

ITEM 614- MAINTAINING TRAFFIC

WESSELMAN ROAD

MAINTAIN A MINIMUM OF 1 LANE OF TWO-WAY TRAFFIC USING FLAGGERS DURING WORK HOURS.

I-74

MAINTAIN ALL EXISTING LANES IN EACH DIRECTION AT ALL TIMES, EXCEPT LANE CLOSURES ARE PERMITTED IN ACCORDANCE WITH THE LANE VALUE CONTRACT TABLE, BY USE OF THE EXISTING PAVEMENT.

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR SPECIAL EVENTS:

NEW YEAR'S (OBSERVED)	GENERAL/REGULAR ELECTION DAY ((NOV)
MEMORIAL DAY	THANKSGIVING
FOURTH OF JULY (OBSERVED)	CHRISTMAS (OBSERVED)
LABOR DAY	(OTHER HOLIDAY OR SPECIAL EVENT)

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR SPECIAL EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY OR SPECIAL EVENT	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
TUESDAY	(GEN./REG. ELECTION) 5:00 AM TUESDAY THROUGH 12:00 AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY
THURSDAY	(THANKSGIVING ONLY) 6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

DURING THE SAME PERIODS, MAINTAIN PEDESTRAIN ACCESS IF PEDESTRIAN ACCESS WAS PRESENT PRIOR TO CONSTRUCTION. SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT (PN 127).

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DETERMINED BY THE ENGINEER FOR THE MAINTENANCE OF TRAFFIC TO CLOSE A SHOULDER AND LANE USING PORTABLE BARRIER PER MT-95.40 AND MT-95.45 AT HAM-74-0838L/R AND HAM-74-0911L. PORTABLE BARRIER SHALL BE USED TO PROTECT EQUIPMENT STAGING IN THE MEDIAN.

- ITEM 614 - WORK ZONE IMPACT ATTENUATOR = 4 EACH
- ITEM 622 - PORTABLE BARRIER = 4,100 FT
- ITEM 202 - BRIDGE TERMINAL ASSEMBLY REMOVED = 3 EACH
- ITEM 202 - GUARDRAIL REMOVED = 500 FT
- ITEM 202 - IMPACT ATTENUATOR REMOVED = 2 EACH
- ITEM 202 - ANCHOR ANCHOR ASSEMBLY REMOVED = 2 EACH
- ITEM 606 - IMPACT ATTENUATOR, TYPE 1 (CAT SYSTEM) = 2 EACH
- ITEM 606 - BRIDGE TERMINAL ASSEMBLY, TYPE 1 = 3 EACH
- ITEM 606 - GUARDRAIL, TYPE 5 = 500 FT
- ITEM 606 - ANCHOR ASSEMBLY, TYPE 'T' (TEMPORARY) = 2 EACH

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

CONTRACTOR SITE ACCESS/STAGING

ACCESS TO/FROM THE MEDIAN EAST OF THE STRUCTURE (BETWEEN THE GREAT MIAMI RIVER AND E MIAMI RIVER ROAD) IS PROHIBITED WHEN ALL LANES ARE OPEN. IF ACCESS TO THIS AREA IS NEEDED, THE LEFT LANE SHALL BE CLOSED TO PROVIDE DECELERATION/ACCELERATION SIMILAR TO MT-103.10.

ACCESS TO/FROM THE MEDIAN WEST OF THE STRUCTURE (BETWEEN THE SR 128 AND THE GREAT MIAMI RIVER) SHALL BE MADE BY ACCELERATING/DECELERATING ALONG THE EXISTING SHOULDER(S) SIMILAR TO MT-103.10. WRONG WAY TRAVEL ON THE OPEN SHOULDER IS PROHIBITED; THERE ARE NO TRAVEL RESTRICTIONS ON THE GRASSED MEDIAN.

PROVIDE PORTABLE BARRIER ALONG ONE OR BOTH MEDIAN SHOULDERS TO PROTECT EQUIPMENT PER 614.035. THE EXISTING MEDIAN GUARDRAIL MAY BE REMOVED AND REINSTALLED TO IMPROVE SITE ACCESS. IF GUARDRAIL IS REMOVED, THE EASTBOUND LEFT SHOULDER SHALL BE CLOSED WITH PORTABLE BARRIER AS SHOWN ON SHEET 5. PAYMENT FOR PORTABLE BARRIER AND GUARDRAIL AS DESCRIBED IN THIS NOTE SHALL BE MADE AT THE UNIT PRICE BID FOR THESE ITEMS.

ACCESS BEHIND GUARDRAIL

REMOVE AND RE-ERECT GUARDRAIL AS NECESSARY FOR ACCESS TO A WORK LOCATION. REMOVE GUARDRAIL ONLY WHEN IT CAN BE REPLACED ON THE SAME DAY. OBTAIN APPROVAL FROM THE ENGINEER FOR EACH LOCATION, PRIOR TO PERFORMING THE WORK. THIS WORK INCLUDES REMOVAL OF EXISTING GUARDRAIL AND POSTS AND RE-ERECTION OF THE SAME MATERIALS. EXISTING RAIL ELEMENTS AND BARRIER REFLECTORS MAY BE RE-USED. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS NECESSARY TO PERFORM THIS WORK SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER. PAYMENT FOR ALL LABOR, EQUIPMENT BEFORE WORK PROCEEDS. AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME TABLE ITEM DURATION OF:		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO OFFICE OF COMMUNICATIONS
RAMP & ROAD CLOSURES	>= 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HOURS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURE & RESTRICTIONS	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	2 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTICE TO OFFICE OF COMMUNICATIONS TIME TABLE.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE

LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

(THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL (IN ACTIVE CELLULAR PHONE AREAS) ALLOW REMOTE SIGN ACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS AND REVISIONS TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES. ONE REMOTE DATA INPUT DEVICE (LAPTOP COMPUTER PLUS MODEM OR EQUIVALENT) SHALL BE FURNISHED FOR USE BY THE DISTRICT TRAFFIC ENGINEER, OR EQUIVALENT, AND SHALL BE INSURED AGAINST THEFT.) THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE, AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 6 SIGN MONTHS ASSUMING 3 PCMS SIGN(S) FOR 2 MONTH(S) PLACE A SIGN AT THE I-275 WB & I-74 WB APPROACH TO HAM-74-9.11L. PLACE A SIGN AT THE I-275 EB APPROACH TO HAM-74-8.38R.

DESIGN AGENCY



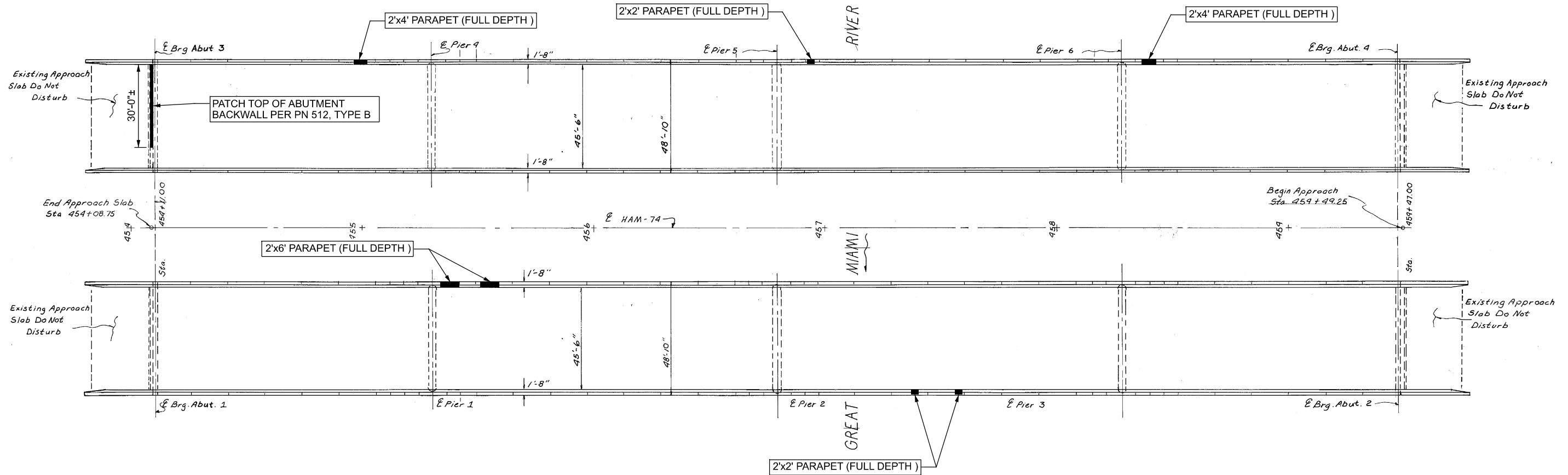
DESIGNER CAH

REVIEWER SK MM-DD-YY

PROJECT ID 114650

SHEET 03

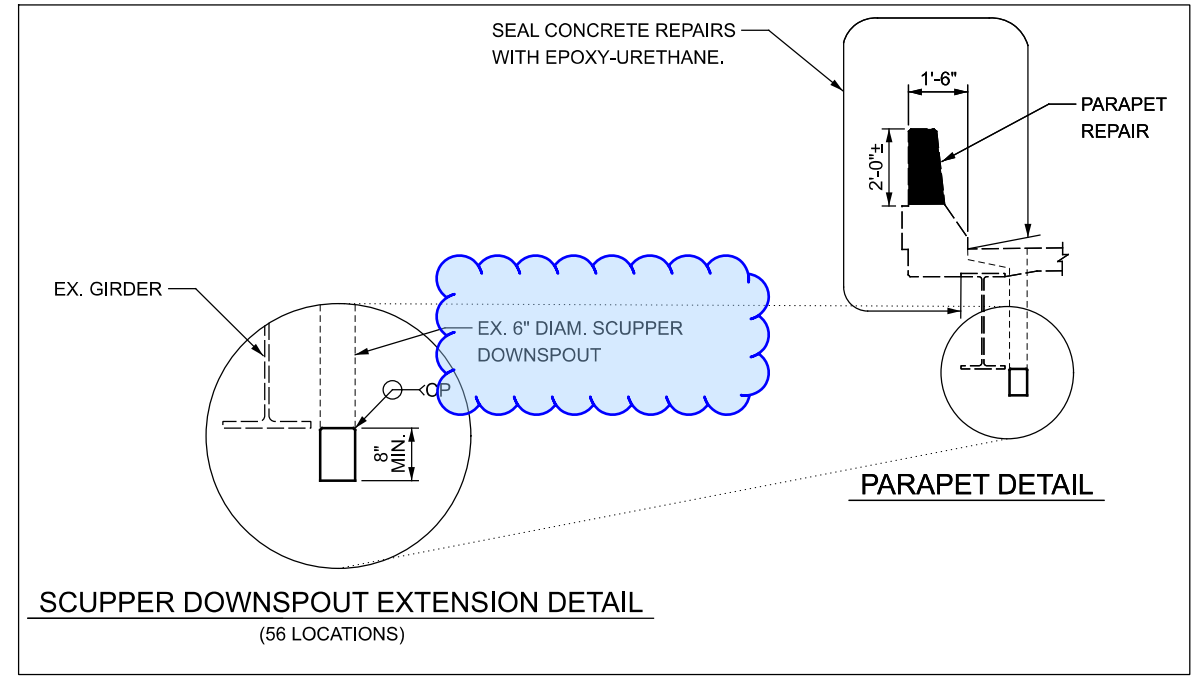
TOTAL 22



PLAN

NOTES:

1. PARAPET REPAIR DIMENSIONS ARE CALLED OUT AS DEPTH FROM TOP OF PARPET BY LENGTH ALONG PARAPET.
2. REMOVE ALL EXISTING CONCRETE SEALER ON PARAPETS. RE-SEAL CONCRETE PARAPETS WITH EPOXY-URETHANE PER CMS 512.
3. EXTEND EXISTING SCUPPER DOWNSPOUTS TO 8" MIN. BELOW BOTTOM FLANGE. MATCH EXISTING PIPE TYPE AND DIAMETER.
4. DETAILS ON THIS SHEET ARE TAKEN FROM EXISTING PLANS AND SHOULD BE USED FOR INFORMATION PURPOSES ONLY.
5. PERFORM ONLY THE WORK AS INDICATED IN THE FRAMED TEXT AND/OR DESCRIBED IN THE GENERAL NOTES.



SCUPPER DOWNSPOUT EXTENSION DETAIL
(56 LOCATIONS)

6. REMOVALS PAID FOR UNDER ITEM 202 - PORTIONS OF STRUCTURE REMOVED, OVER 20 FOOT SPAN, AS PER PLAN FOR PAYMENT.
7. CAREFULLY REMOVE CONCRETE FROM PARAPETS. SALVAGE EXISTING REBAR FOR REUSE. CUTTING EXISTING REBAR AND DOWELING IN OF NEW REBAR SHALL NOT BE ALLOWED.
8. CONCRETE FOR PARAPET REPAIRS PAID FOR UNDER ITEM 513 - CLASS QC2 CONCRETE, SUPERSTRUCTURE.

LEGEND

■ QC 2 CONCRETE REPAIR

SFN	3108252
SFN	3108287
DESIGN AGENCY	



DESIGNER	CHECKER
CAH	GTF
REVIEWER	AMS
PROJECT ID	114650
SUBSET	TOTAL
4	4
SHEET	TOTAL
19	22