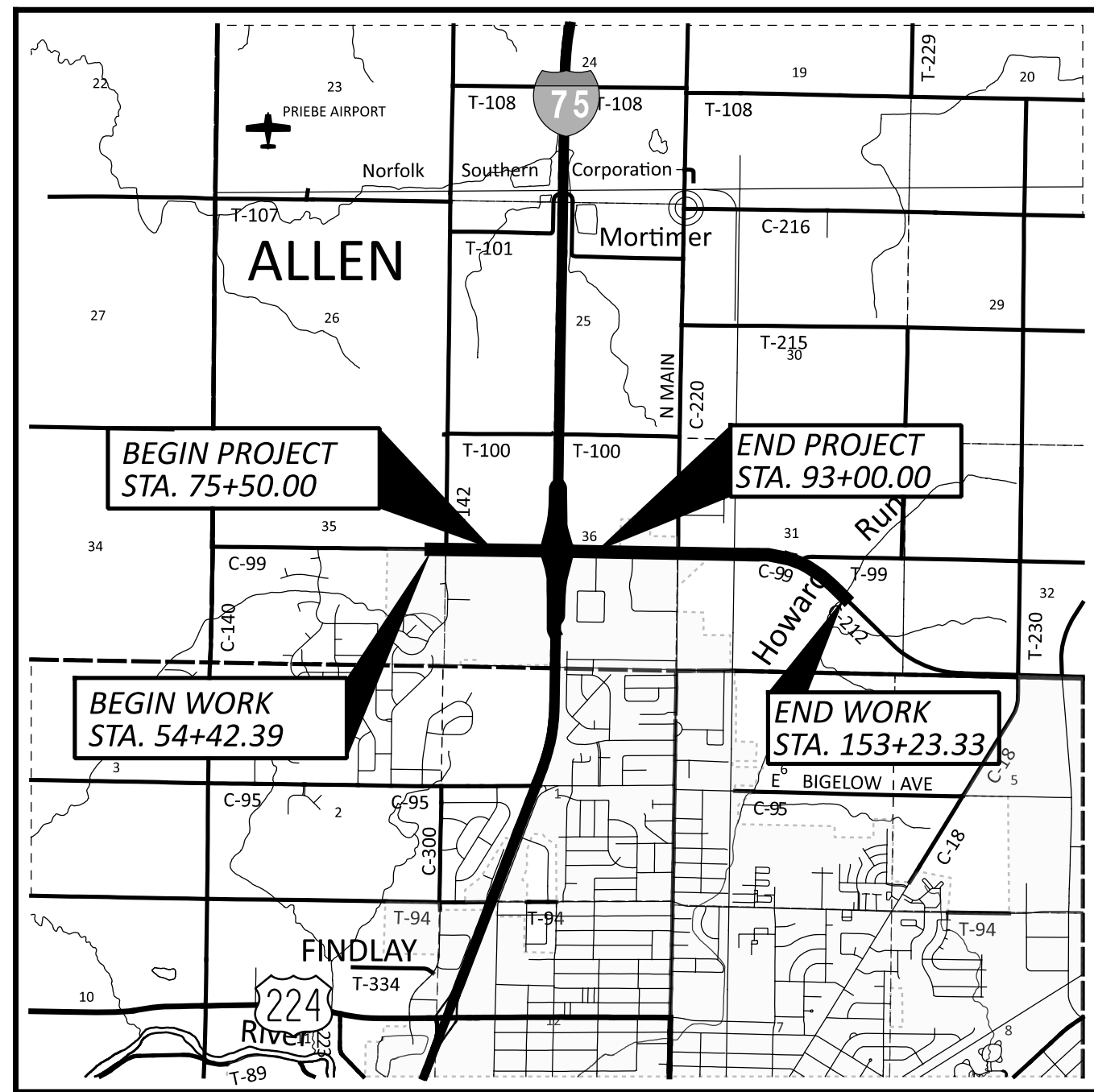
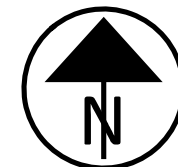


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LOCATION MAP

LATITUDE: 41° 05' 16" LONGITUDE: 83° 39' 36"



PORTION TO BE IMPROVED	—————
INTERSTATE HIGHWAY	=====
FEDERAL ROUTES	=====
STATE ROUTES	=====
COUNTY & TOWNSHIP ROADS	=====
OTHER ROADS	—————

DESIGN DESIGNATION

SEE SHEET 2 FOR DESIGN DESIGNATIONS

DESIGN EXCEPTIONS

NONE

ADA DESIGN WAIVERS

NONE

UNDERGROUND UTILITIES
 Contact Two Working Days
 Before You Dig

OHIO811.org
 Before You Dig

OHIO811, 8-1-1, or 1-800-362-2764
 (Non members must be called directly)

PLAN PREPARED BY:

HDR ENGINEERING, INC.
 9999 CARVER ROAD, SUITE 210
 CINCINNATI, OHIO 45242
 513-984-7500

STATE OF OHIO DEPARTMENT OF TRANSPORTATION

HAN-75/CR99 INTERCHANGE REHAB

ALLEN TOWNSHIP
 HANCOCK COUNTY
 CITY OF FINDLAY

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FEDERAL PROJECT NUMBER

NON-FEDERAL

RAILROAD INVOLVEMENT

CSX TRANSPORTATION, INC.

PROJECT DESCRIPTION

RECONSTRUCT THE INTERCHANGE OF IR 75 AT CR 99 FROM AN EXISTING DIAMOND INTERCHANGE TO A DIVERGING DIAMOND INTERCHANGE. PROJECT INCLUDES THE RECONSTRUCTION AND WIDENING OF THE EXISTING RAMPS AND WIDENING OF CR 99 FROM TECHNOLOGY DRIVE TO MAIN STREET INCLUDING STORM SEWER UPGRADES FROM MAIN STREET TO THE OUTFALL AT HOWARD RUN PROPOSED WORK INCLUDES, SIDEWALK AND SHARED USE PATH, PAVEMENT WIDENING AND FULL DEPTH REPLACEMENT OF ASPHALT AND CONCRETE PAVEMENT, NEW BRIDGE CONSTRUCTION, EXISTING BRIDGE WIDENING, DRAINAGE, EROSION CONTROL, TRAFFIC SIGNALS, TRAFFIC CONTROL, AND LIGHTING.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA:	38.2 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA:	6.0 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA:	44.2 ACRES

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

2023 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

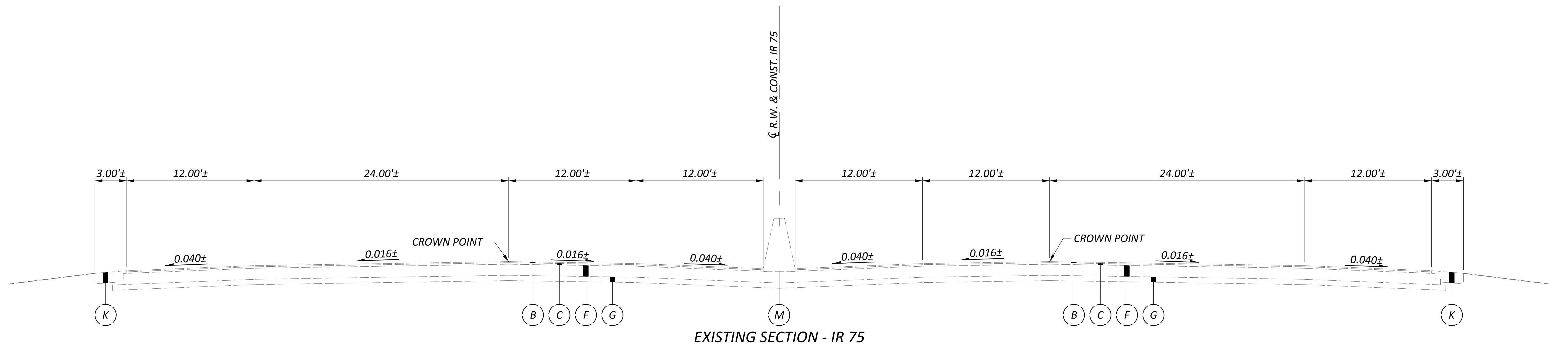
I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

STANDARD CONSTRUCTION DRAWINGS										SUPPLEMENTAL SPECIFICATIONS		SPECIAL PROVISIONS					
BP-2.1	1/21/22	F-1.1	7/19/13	RM-4.5	7/21/17	HL-20.21	1/15/21	ITS-50.12	7/15/22	MT-101.70	4/21/23	TC-65.10	1/17/14	800	7/21/23	SP-1	7/7/21
BP-2.2	1/15/21	F-2.1	7/20/18	RM-4.6	7/19/13	HL-30.11	7/21/23			MT-101.75	7/21/23	TC-65.11	7/15/22	804	1/20/23	ASBESTOS REPORT	
BP-2.3	7/18/14	F-3.1	7/19/13	RM-5.2	7/21/23	HL-30.22	1/15/21	MT-95.31	7/19/19	MT-101.80	1/17/20	TC-71.10	4/21/23	809	7/21/23	SP-2	PENDING
BP-3.1	1/21/22	F-3.3	7/19/13			HL-30.31	7/21/23	MT-95.32	4/19/19	MT-101.90	7/17/20	TC-72.20	7/21/23	813	7/21/23	SPECIAL PROVISION	
BP-4.1	7/19/13	F-3.4	7/19/13	WQ-1.1	7/21/23	HL-40.20	7/21/23	MT-95.40	7/21/23	MT-102.10	7/21/23	TC-73.20	7/21/23	816	10/18/19	WATERWAYS PERMIT	10/6/23
BP-5.1	7/15/22					HL-50.21	7/15/22	MT-95.41	7/21/23	MT-102.20	4/19/19	TC-74.10	7/21/23	825	4/21/23		
BP-7.1	7/21/23	MGS-1.1	7/16/21	AS-1-15	1/20/23	HL-60.11	7/21/17	MT-95.50	7/21/17			TC-81.22	7/21/23	832	7/21/23		
		MGS-2.1	1/19/18	AS-2-15	7/21/23	HL-60.12	7/21/23	MT-95.82	7/19/13	TC-12.31	4/15/22	TC-83.10	1/17/20	904	7/15/22		
CB-2-2A, 2B, 2C	1/20/23	MGS-3.1	1/19/18	GSD-1-19	1/15/21	HL-60.21	7/20/18	MT-97.10	4/19/19	TC-15.116	7/21/23	TC-83.20	7/15/22	907	10/18/19		
CB-2-3, 2-4	1/20/23	MGS-3.2	1/18/13	SBR-1-20	7/21/23	HL-60.31	7/21/23	MT-97.11	1/20/17	TC-16.22	7/21/23	TC-85.10	10/21/22	909	7/21/23		
CB-3	7/16/21	MGS-4.2	7/19/13	SBR-2-20	7/21/23			MT-97.12	1/20/17	TC-21.11	7/16/21	TC-85.20	4/21/23	913	4/16/21		
CB-3A	7/16/21	MGS-5.3	7/15/16	SICD-2-14	1/15/21	ITS-10.10	1/20/23	MT-98.10	1/17/20	TC-21.21	1/20/23			916	7/21/23		
CB-4	7/16/21			VPF-1-90	7/21/23	ITS-10.11	1/20/23	MT-98.11	1/17/20	TC-22.20	1/17/14			921	4/20/12		
		MH-3	7/21/23			ITS-12.10	7/15/22	MT-98.20	4/19/19	TC-41.10	7/19/13						
DM-1.1	7/17/20			HL-10.11	7/21/23	ITS-14.10	4/10/23	MT-98.21	7/21/23	TC-41.20	10/18/13						
DM-1.2	7/16/21	RM-1.1	1/20/23	HL-10.12	7/21/23	ITS-14.11	1/20/23	MT-98.28	1/17/20	TC-41.30	4/21/23						
DM-4.1	7/17/20	RM-4.1	7/21/17	HL-10.13	1/20/23	ITS-15.10	1/20/23	MT-99.20	4/19/19	TC-41.40	10/18/13						
DM-4.4	1/15/16	RM-4.3	1/21/22	HL-10.31	7/15/22	ITS-18.00	7/16/21	MT-99.30	1/17/20	TC-52.10	10/18/13						
		RM-4.4	7/21/23	HL-20.11	7/21/23	ITS-50.10	1/20/23	MT-101.60	4/21/23	TC-52.20	1/15/21						

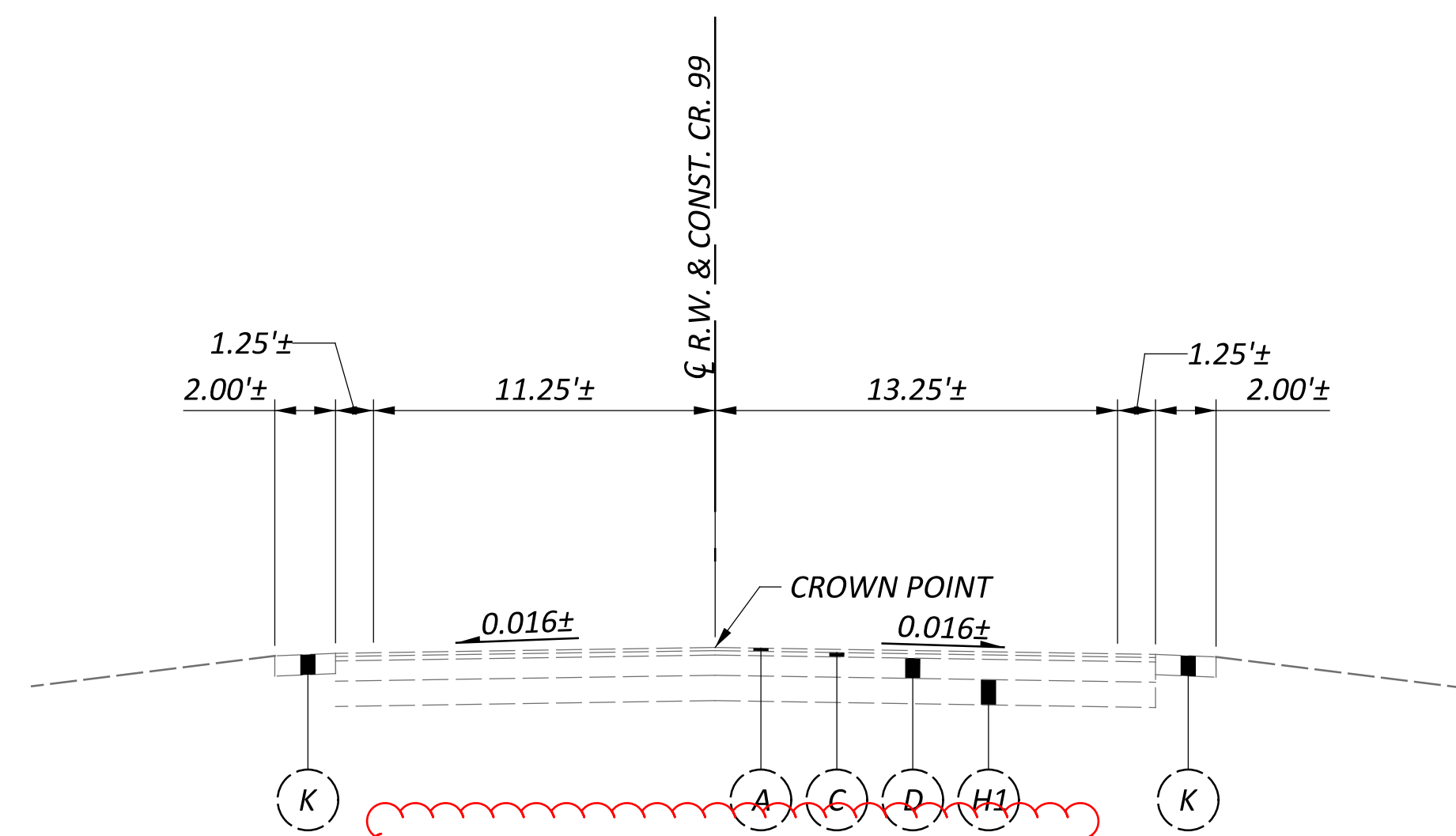
Christopher A. Hughes
 Christopher A. Hughes, P.E.
 District 01 Deputy Director

Jack Marchbanks
 Jack Marchbanks, PhD
 Director, Department of Transportation

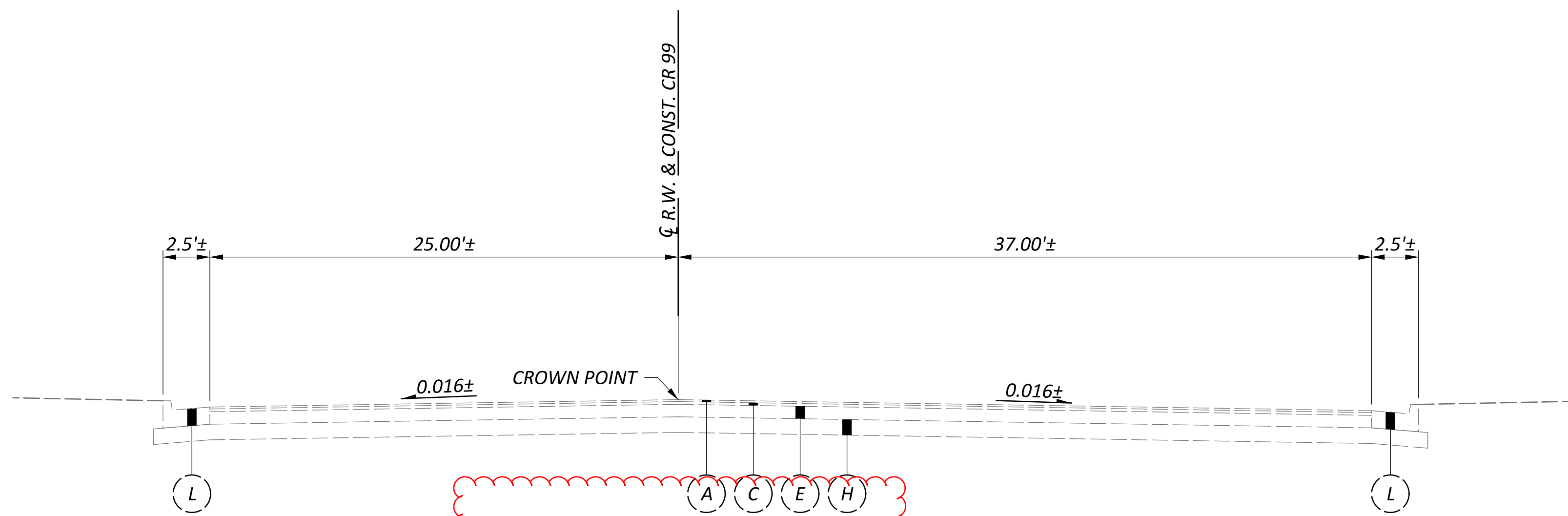
DESIGN AGENCY	
DESIGNER	MJL
REVIEWER	KF
PROJECT ID	12/08/22
SHEET	102375
TOTAL	102375



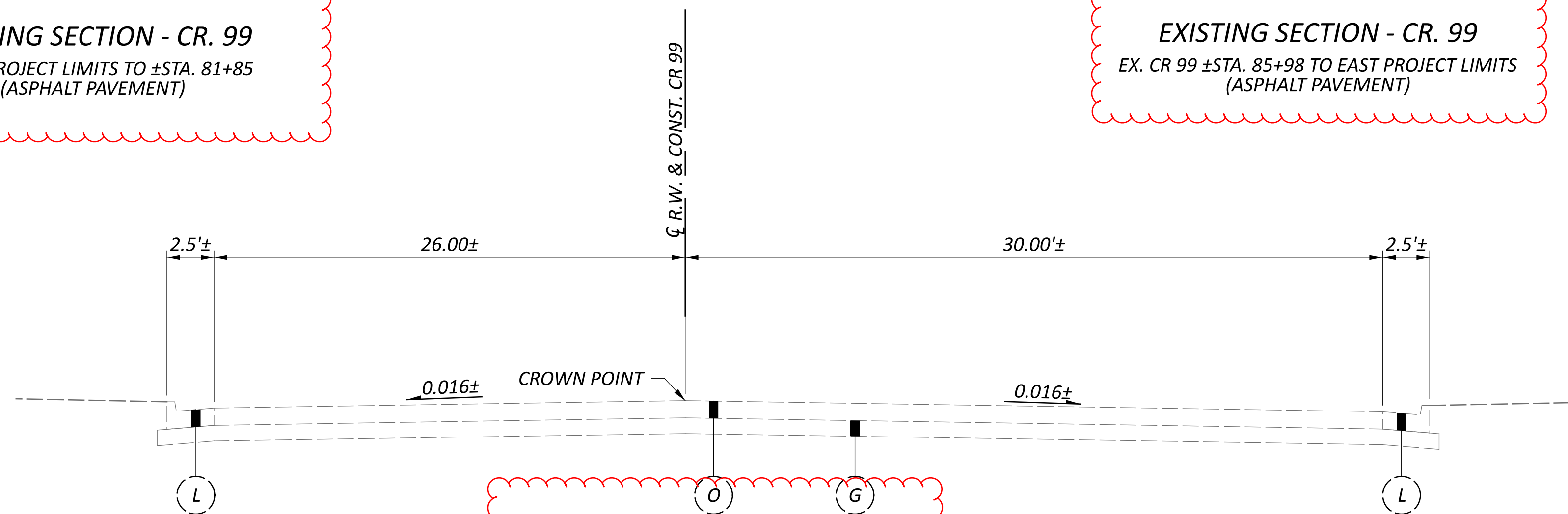
EXISTING SECTION - IR 75



EXISTING SECTION - CR 99
 WEST PROJECT LIMITS TO ±STA. 81+85
 (ASPHALT PAVEMENT)

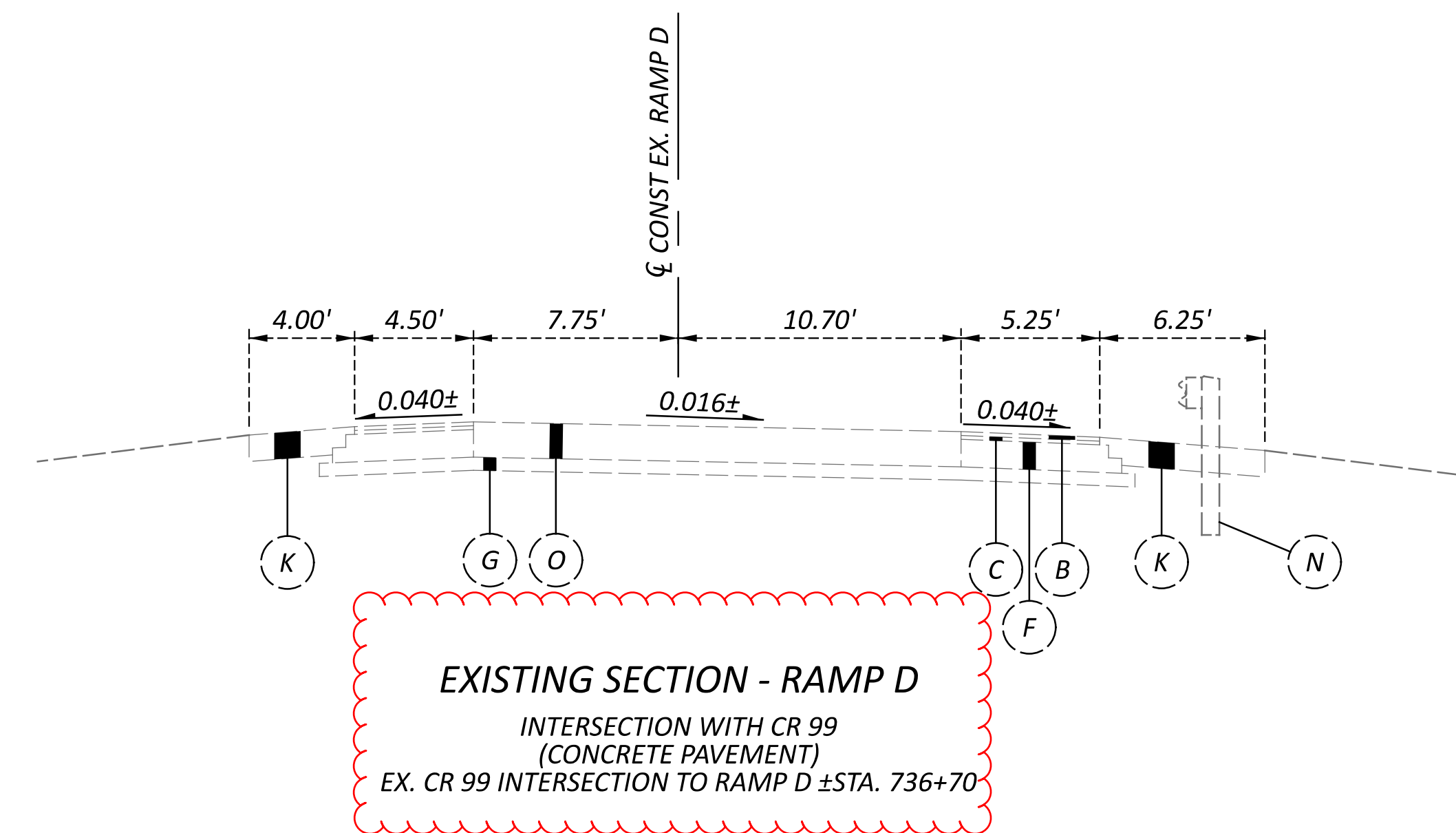
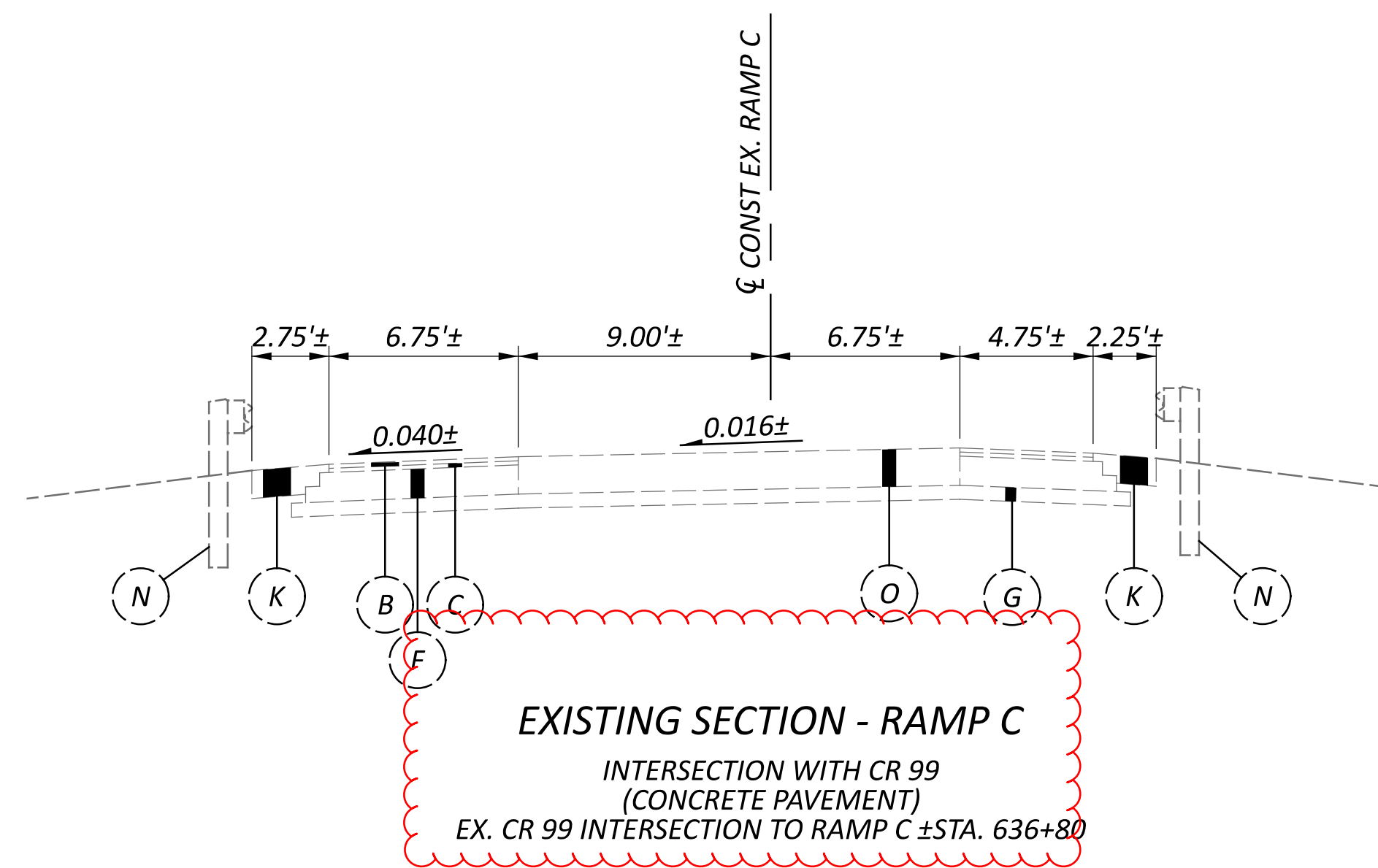
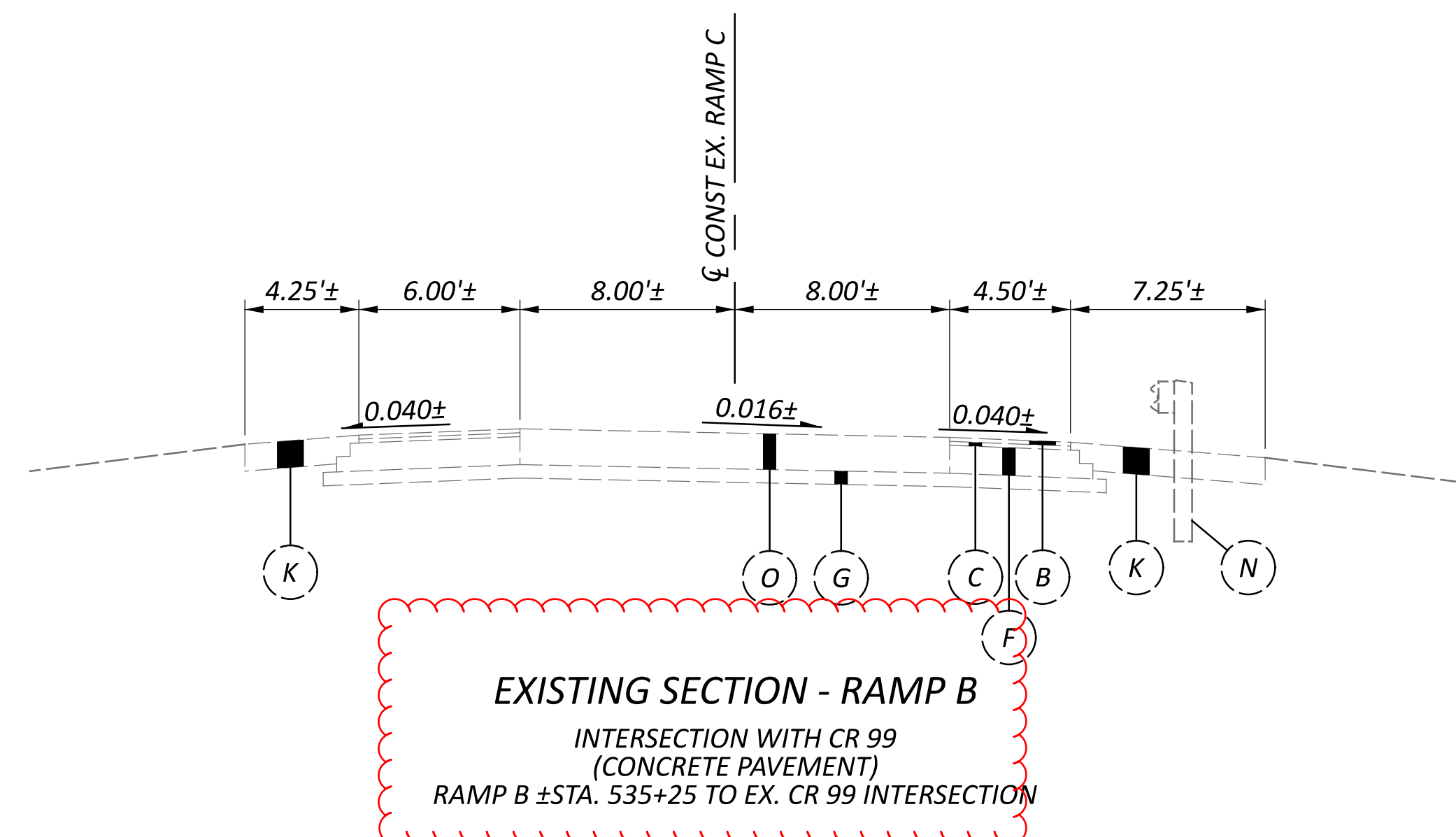
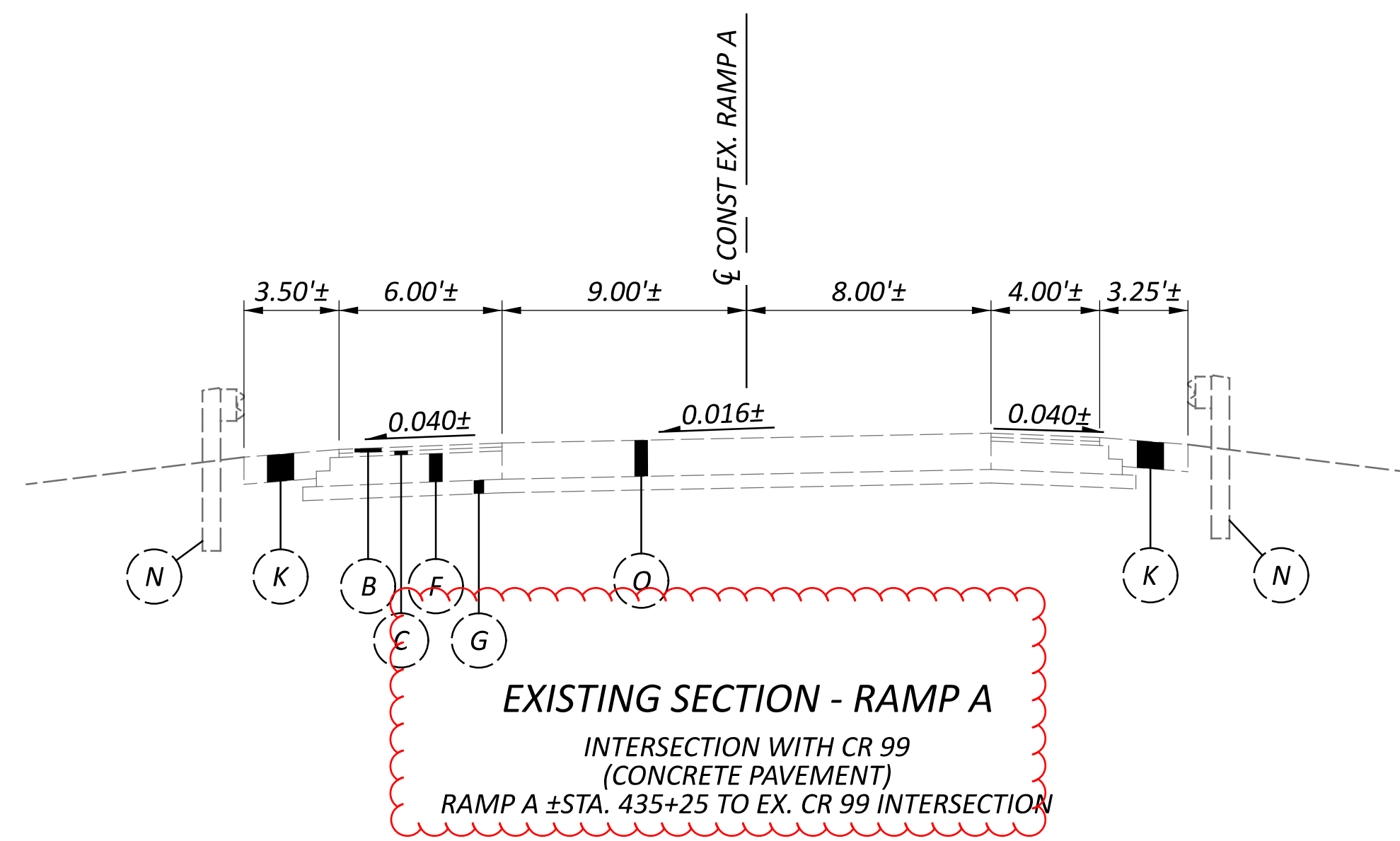


EXISTING SECTION - CR 99
 EX. CR 99 ±STA. 85+98 TO EAST PROJECT LIMITS
 (ASPHALT PAVEMENT)

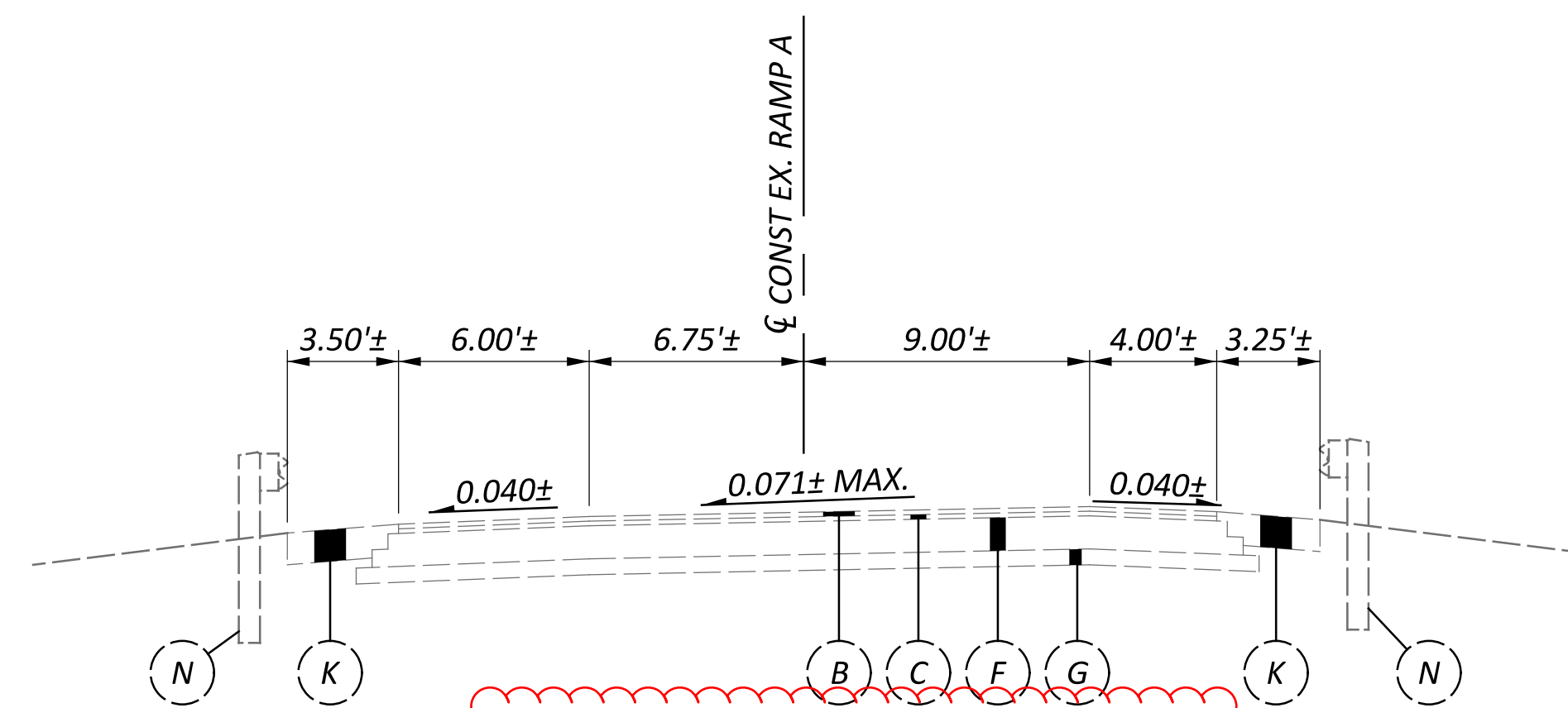


EXISTING SECTION - CR 99 INTERSECTIONS WITH IR 75 RAMPS
 (CONCRETE PAVEMENT)
 EX. CR 99 ±STA. 81+85 TO ±STA. 83+85
 EX. CR 99 ±STA. 85+98 TO ±STA. 88+00

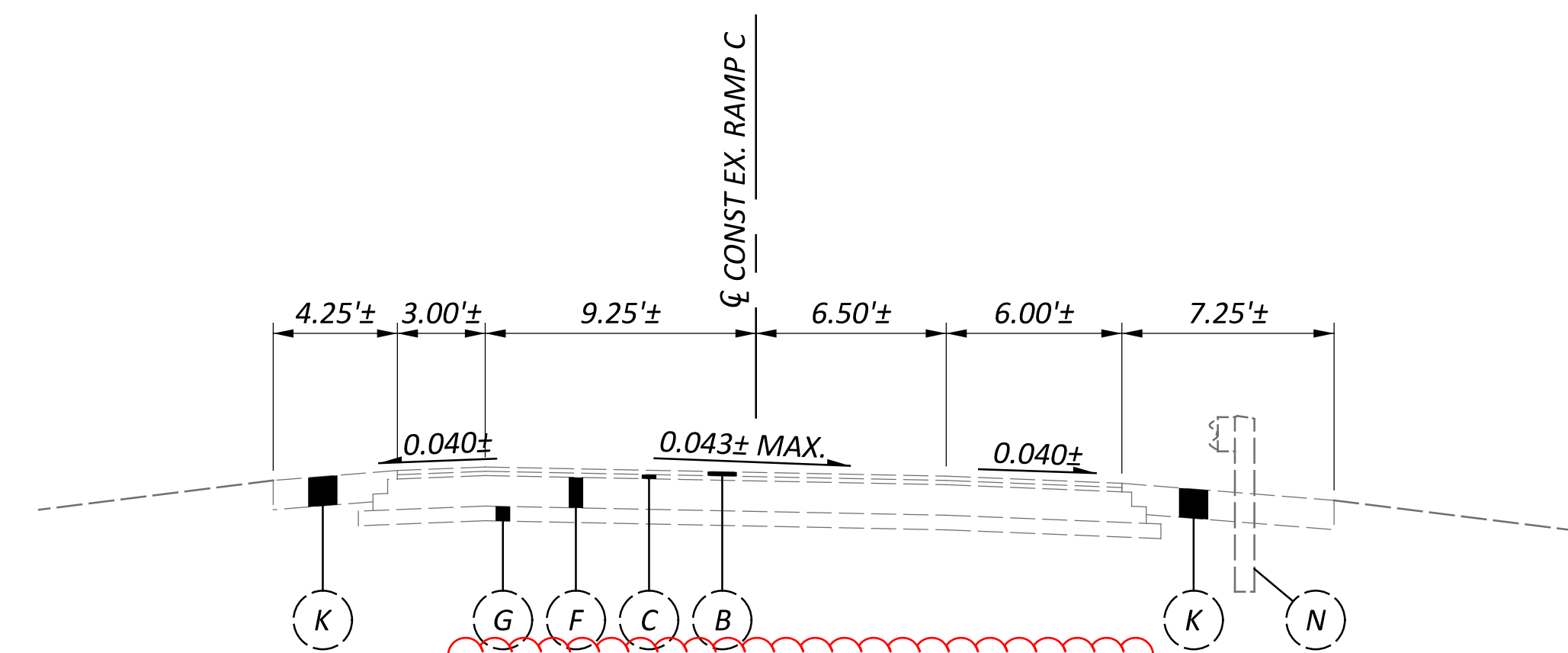
NOTE: EXISTING PAVMENT DEPTHS ARE BASED ON RECORD DRAWINGS AND SOIL BORINGS



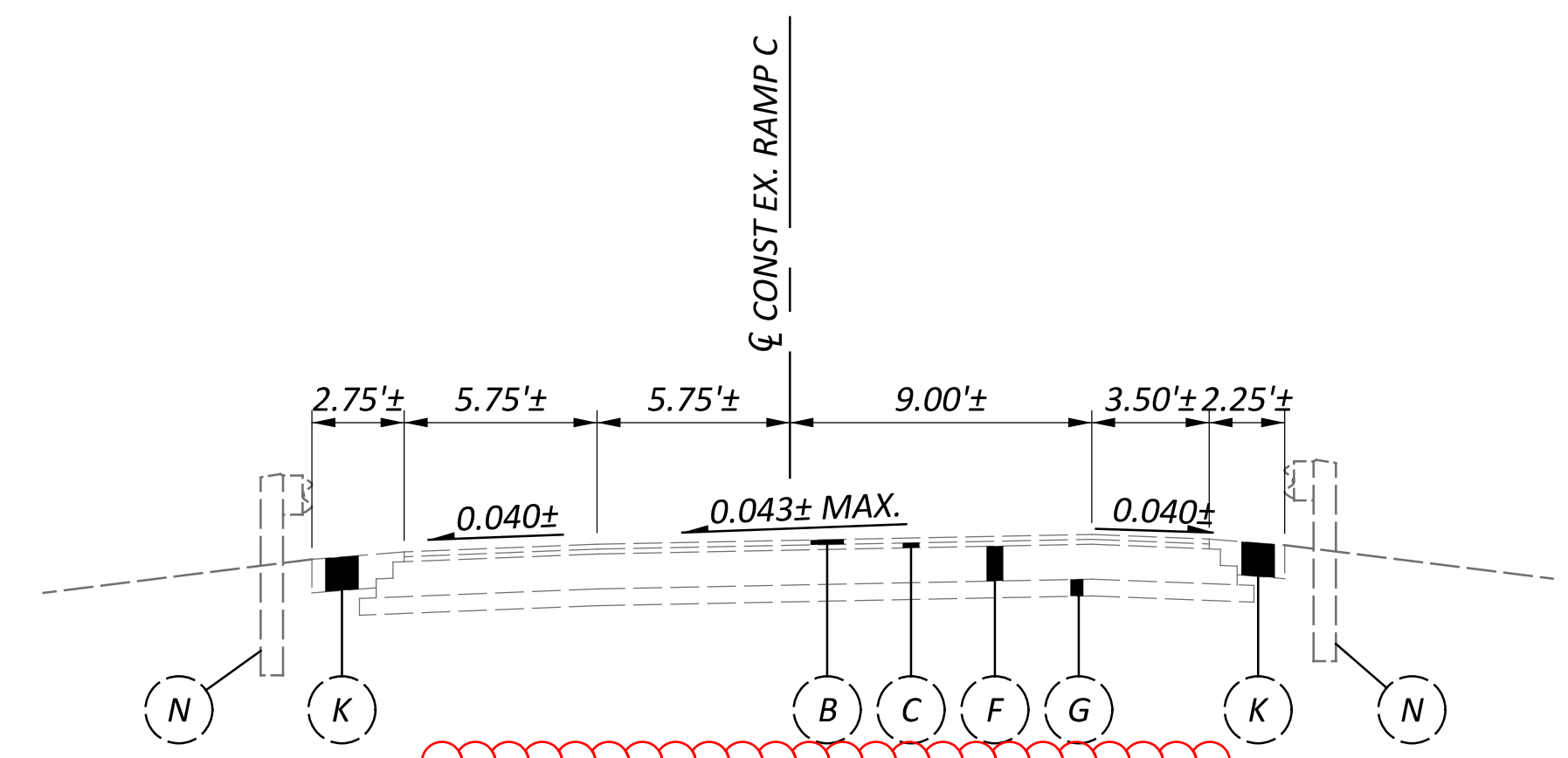
NOTE: EXISTING PAVMENT DEPTHS ARE BASED ON RECORD DRAWINGS AND SOIL BORINGS



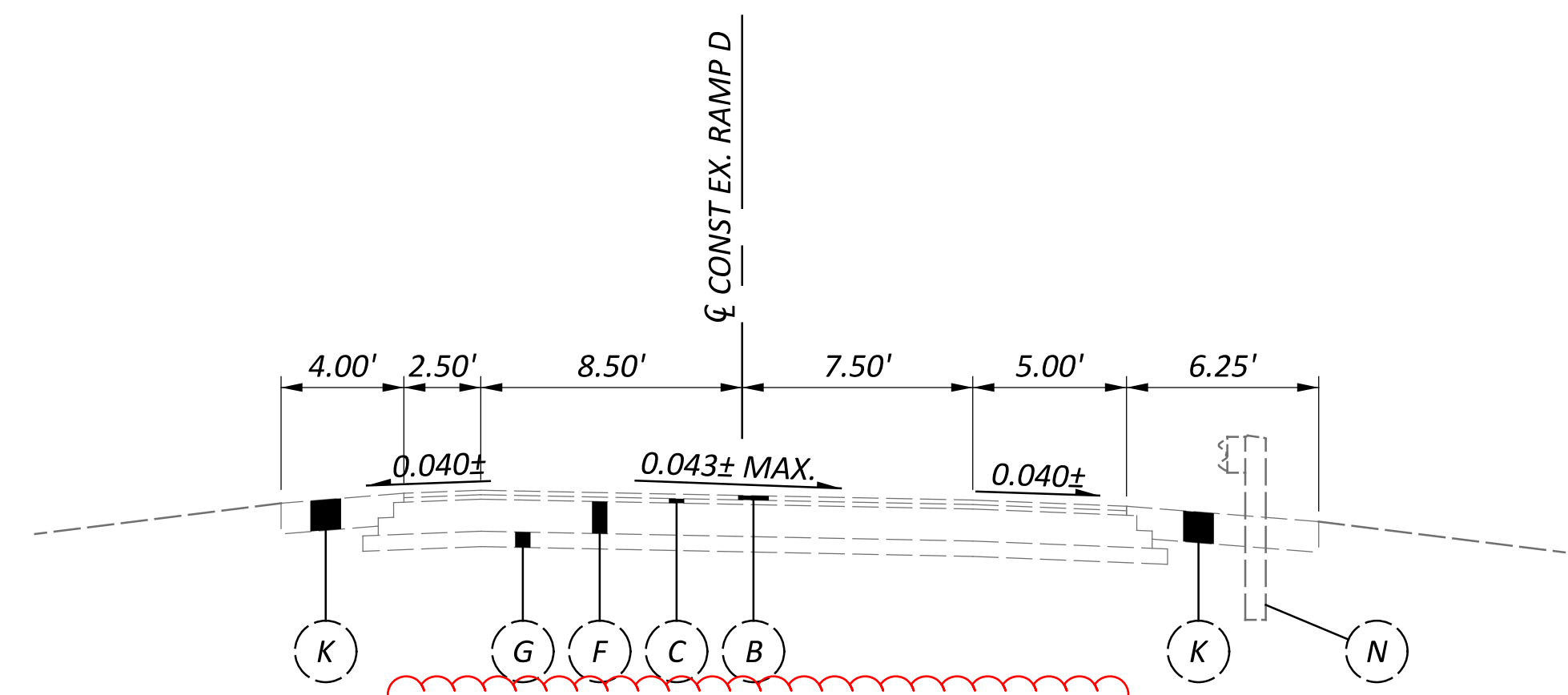
EXISTING SECTION - RAMP A
 SUPERELEVATED SECTION
 (ASPHALT PAVEMENT)
 PROJECT LIMIT TO RAMP A ±STA. 435+25



EXISTING SECTION - RAMP B
 SUPERELEVATED SECTION
 (ASPHALT PAVEMENT)
 PROJECT LIMIT TO RAMP B ±STA. 535+25



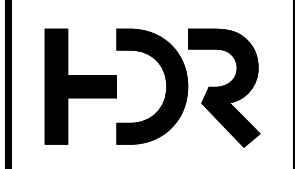
EXISTING SECTION - RAMP C
 SUPERELEVATED SECTION
 (ASPHALT PAVEMENT)
 RAMP C ±STA. 636+80 TO PROJECT LIMIT



EXISTING SECTION - RAMP D
 SUPERELEVATED SECTION
 (ASPHALT PAVEMENT)
 RAMP D ±STA. 736+70 TO PROJECT LIMIT

NOTE: EXISTING PAVMENT DEPTHS ARE BASED ON RECORD DRAWINGS AND SOIL BORINGS

DESIGN AGENCY



DESIGNER
 MJL

REVIEWER
 KF 05/20/22

PROJECT ID
 102375

SHEET TOTAL
 9 | 705

UTILITIES

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS:

ELECTRIC
AMERICAN ELECTRIC POWER
209 N WOOD STREET
FOSTORIA, OHIO 44830
740-348-5322 ATTN: PAUL PAXTON

AMERICAN ELECTRIC POWER (TRANSMISSION)
PUBLIC PROJECTS
8600 SMITHS MILL ROAD
NEW ALBANY, OH 43054
380-205-5072

HANCOCK-WOOD ELECTRIC
1399 BUSINESS PARK RD
NORTH BALTIMORE, OH 45872
800-445-4840 ATTN: ANDY FISHER

WATER
CITY OF FINDLAY
WATER
136 N BLANCHARD STREET
FINDLAY, OHIO 45840
419-424-7121 ATTN: JEREMY KALB

STORM
HANCOCK COUNTY ENGINEERS OFFICE
1900 LIMA AVENUE
FINDLAY, OHIO 45840
419-422-7433
ATTN: DOUG CADE, COUNTY ENGINEER

SANITARY
CITY OF FINDLAY
SEWER MAINTENANCE
1201 RIVER STREET
FINDLAY, OHIO 45840
419-424-7121 ATTN: JEREMY KALB

GAS
COLUMBIA GAS OF OHIO, INC.
1800 BROAD AVENUE
FINDLAY, OHIO 45840
419-427-3219
ATTN: ADAM HAMMAN

COLUMBIA GAS OF OHIO DISTRIBUTION & TRANSMISSION
1804 BROAD AVENUE
FINDLAY, OHIO 45840
419-427-3200
ATTN: JONATHON AMSTUTZ (614) 301-3875
NATHAN BEINING (419) 266-7092

KNG ENERGY (NATURAL GAS)
1700 WESTFIELD DRIVE
FINDLAY, OHIO 45840
419-424-3427
ATTN: SANDY ROLLER OR JEFF ABBOTT

NORTH COAST GAS TRANSMISSION
445 HUTCHINSON AVENUE
SUITE 830
COLUMBUS, OHIO 43235
281-770-3143 ATTN: BILL CAGLE

ITS
ODOT CO ITS
1606 W BROAD ST.
COLUMBUS, OH 43223
614-387-4113
CEN.ITS.LAB@DOT.OHIO.GOV

TELECOMMUNICATIONS

AT&T
130 N. ERIE ST. ROOM 714
TOLEDO, OH 43604
419- 245-5004 ATTN: ROB FEY
RF1281@ATT.COM

BUCKEYE CATV (BROADBAND)
4814 ANGOLA ROAD BUILDING A
TOLEDO, OH 43615
419-724-3713 ATTN: MICHAEL SHEAHAN

CHARTER COMMUNICATIONS (SPECTRUM/TIME WARNER)
205 CRYSTAL AVENUE
FINDLAY, OHIO 45840
419-956-8211 ATTN: DERRICK NEWMAN
DERRICK.NEWMAN@CHARTER.COM

INDEPENDENT FIBER
13888 CR 25A
WAPAKONETA, OHIO 45895
419-739-6941 ATTN: SARA EMANS
SEMANS@CNITEAM.COM

LEVEL 3 COMMUNICATIONS (CENTURYLINK, LUMEN TECHNOLOGIES)
6185 HUNTLEY RD, SUITE E
COLUMBUS, OH 43215
614-255-2112 (OFFICE) 614-554-7071 (CELL) ATTN: STEVE KAUFFMAN
STEVE.KAUFFMAN@LUMEN.COM

OIL
BUCKEYE PIPELINE COMPANY
3321 YORK STREET
OREGON, OH 43616
419-698-8770

SURVEYING PARAMETERS

PRIMARY PROJECT CONTROL MONUMENTS GOVERN ALL POSITIONING ON ODOT PROJECTS. SEE SHEET 3 OF THE PLANS FOR A TABLE CONTAINING PROJECT CONTROL INFORMATION.

USE THE FOLLOWING PROJECT CONTROL, VERTICAL POSITIONING, AND HORIZONTAL POSITIONING PARAMETERS FOR ALL SURVEYING:

PROJECT CONTROL

POSITIONING METHOD: ODOT VRS
MONUMENT TYPE: TYPE B

VERTICAL POSITIONING

ORTHOMETRIC HEIGHT DATUM: NAVD88
GEOID: GEOID 12A

HORIZONTAL POSITIONING

REFERENCE FRAME: NAD83 (2011)
ELLIPSOID: GRS80
MAP PROJECTION: LAMBERT CONFORMAL CONIC
COORDINATE SYSTEM: OHIO STATE PLANE, NORTH ZONE
COMBINED SCALE FACTOR: 1.00009569 (GRID TO GROUND)
ORIGIN OF COORDINATE SYSTEM: 0,0

USE THE POSITIONING METHODS AND MONUMENT TYPE USED IN THE ORIGINAL SURVEY TO RESTORE ALL MONUMENTS RELATED TO PRIMARY PROJECT CONTROL THAT ARE DAMAGED OR DESTROYED BY CONSTRUCTION ACTIVITIES. RESTORE THE DAMAGED OR DESTROYED MONUMENTS IN ACCORDANCE WITH CMS 623.

UNITS ARE IN U.S. SURVEY FEET.

ITEM 623 PRE AND POST-CONSTRUCTION SURVEY MONUMENT VERIFICATION AND REPORTS

A **PRE-CONSTRUCTION VERIFICATION REPORT**, COMPLETED BY A REGISTERED SURVEYOR, WHICH IDENTIFIES DISCREPANCIES OR CHANGES TO THE USABILITY OF THE PROJECT CONTROL MONUMENTS SPECIFIED IN THE CONTRACT DOCUMENTS AND IDENTIFIES ALL REQUIRED SURVEY MONUMENTS WITHIN THE RIGHT OF WAY PROJECT WORK AREA, SHALL BE FURNISHED TO THE ENGINEER AND THE DISTRICT SURVEY OPERATIONS MANAGER **PRIOR TO BEGINNING ANY EARTHWORK ACTIVITIES.**

SEE CMS 623.04 FOR FURTHER INFORMATION ON SURVEY MONUMENTS THAT ARE REQUIRED TO BE INCLUDED WITH THIS PAY ITEM AND FOR THE PRE-CONSTRUCTION VERIFICATION REPORT TEMPLATE. SEE CMS 623.11 FOR FURTHER INFORMATION ON BASIS OF PAYMENT FOR THIS ITEM.

SEE SHEET 3 OF THE CONSTRUCTION PLANS FOR A TABLE OF PROJECT CONTROL MONUMENTS THAT NEED INCLUDED AS PART OF THIS VERIFICATION. SEE SHEET 2A OF THE RW PLAN SUBSET (OVERALL SHEET 649) FOR TABLES OF EXISTING SURVEY MONUMENTS WHICH ARE EITHER CURRENTLY WITHIN THE PROJECT WORK AREA OR EXPECTED TO BE DISTURBED THAT NEED INCLUDED AS PART OF THIS VERIFICATION.

A **POST-CONSTRUCTION VERIFICATION REPORT**, COMPLETED BY A REGISTERED SURVEYOR, WHICH ENSURES THAT ALL REQUIRED SURVEY MONUMENTS AS SPECIFIED IN THE CONTRACT DOCUMENTS, AND ADDITIONAL MONUMENTS FOUND DURING THE PRECONSTRUCTION SURVEY MONUMENT VERIFICATION, ARE PRESERVED, SET, RESET, AND/OR ADJUSTED TO GRADE, SHALL BE FURNISHED TO THE ENGINEER AND THE DISTRICT SURVEY OPERATIONS MANAGER **AFTER COMPLETION OF FINAL GRADING AND/OR RESURFACING AND CONSTRUCTION ACTIVITIES.**

SEE CMS 623.04 FOR FURTHER INFORMATION ON THIS PAY ITEM AND FOR THE POST-CONSTRUCTION VERIFICATION REPORT TEMPLATE. SEE CMS 623.11 FOR FURTHER INFORMATION ON BASIS OF PAYMENT FOR THIS ITEM.

SEE SHEET 2A OF THE RW PLAN SUBSET (OVERALL SHEET 649) FOR A TABLE OF SURVEY MONUMENTS TO BE SET DURING CONSTRUCTION THAT WILL NEED INCLUDED AS PART OF THIS VERIFICATION AND FOR TABLES OF EXISTING SURVEY MONUMENTS CURRENTLY WITHIN THE PROJECT WORK AREA OR EXPECTED TO BE DISTURBED THAT NEED INCLUDED AS PART OF THIS VERIFICATION.

CLEARING AND GRUBBING

ALTHOUGH THERE ARE NO TREES OR STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE LIMITS OF THE PROJECT, A LUMP SUM QUANTITY IS INCLUDED IN THE GENERAL SUMMARY FOR ITEM 201, CLEARING AND GRUBBING. A SIGNIFICANT NUMBER OF TREES HAVE BEEN REMOVED BY THE DISTRICT FORCES, HOWEVER STUMPS REMAIN. THE ESTIMATED NUMBER OF STUMPS WITH SIGNIFICANT SIZE ARE AS FOLLOWS. THE REMOVAL OF THESE ITEMS AND ALL PROVISIONS AS SET FORTH IN THE SPECIFICATIONS UNDER THIS ITEM ARE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 201, CLEARING AND GRUBBING.

STUMP SIZE 12" - 18" 15 EA.
STUMP SIZE 19" - 30" 7 EA.
STUMP SIZE 31" - 48" 1 EA.

TREE CLEARING RESTRICTIONS

THE PROJECT IS LOCATED WITHIN THE KNOWN HABITAT RANGES OF THE FEDERALLY ENDANGERED INDIANA BAT AND NORTHERN LONG-EARED BAT, AND THE STATE ENDANGERED LITTLE BROWN AND TRICOLORED BATS. NO TREES SHALL BE REMOVED UNDER THIS PROJECT FROM APRIL 1 THROUGH SEPTEMBER 30. ALL NECESSARY TREE REMOVAL SHALL OCCUR FROM OCTOBER 1 THROUGH MARCH 31. THIS REQUIREMENT IS NECESSARY TO AVOID AND MINIMIZE IMPACTS TO THESE SPECIES AS REQUIRED BY THE ENDANGERED SPECIES ACT AND ORC 1531.25. FOR THE PURPOSES OF THIS NOTE, A TREE IS DEFINED AS A LIVE, DYING, OR DEAD WOODY PLANT, WITH A TRUNK THREE INCHES OR GREATER IN DIAMETER AT A HEIGHT OF 4.5 FEET ABOVE THE GROUND SURFACE, AND WITH A MINIMUM HEIGHT OF 13 FEET.

BENCHING OF FOUNDATION SLOPES

ALTHOUGH CROSS-SECTIONS INDICATE SPECIFIC DIMENSIONS FOR PROPOSED BENCHING OF THE EMBANKMENT FOUNDATIONS IN CERTAIN AREAS, NO WAIVER OF THE SPECIFICATIONS IS INTENDED. BENCH ALL OTHER SLOPED EMBANKMENT AREAS AS SET FORTH IN SECTION 203.05 OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS (C&MS). NO ADDITIONAL PAYMENT WILL BE MADE FOR BENCHING REQUIRED UNDER THE PROVISIONS OF SECTION 203.05.

ROUNDING

THE ROUNDING AT SLOPE BREAKPOINTS SHOWN ON THE TYPICAL SECTIONS APPLIES TO ALL CROSS-SECTIONS, EVEN THOUGH OTHERWISE SHOWN.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

FENCE LENGTHS

THE LENGTHS OF FENCE SHOWN IN THE PLANS ARE HORIZONTAL DIMENSIONS. MEASUREMENTS OF THE FINAL QUANTITIES WILL BE IN ACCORDANCE WITH ITEM 607.

ITEM 607 - FENCE, TYPE CLT, AS PER PLAN BLACK VINYL COATED

ALL MATERIALS NECESSARY FOR THE INSTALLATION OF FENCE, TYPE CLT, AS PER PLAN, INCLUDING ALL POSTS AND RAILS SHALL BE FEDERAL COLOR NUMBER 27038, BLACK. THE PVC COATING FOR THE FABRIC SHALL BE BLACK IN COLOR CLOSELY APPROACHING FEDERAL COLOR NUMBER 27038. ALL LABOR AND MATERIAL FOR COATING SHALL BE INCLUDED WITH THIS ITEM FOR PAYMENT.

ADA RAMP AS PER PLAN

GEOMETRIC DETAILS FOR CURB RAMPS NOT CATEGORIZED PER SCD BP-7.1 ARE PROVIDED ON SHEETS 406 TO 411. BASIS OF PAYMENT FOR THESE RAMPS WILL FOLLOW C&MS SECTION 608 AND WILL BE INCLUDED IN THE TOTAL QUANTITY FOR PAY ITEM - 608 CURB RAMPS.

REMOVAL, MISC.: PRIVATE SIGN

ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH ODOT ITEM 202. THE PRIVATE SIGNS SHALL BE REMOVED FROM THE VARIOUS LOCATIONS INDICATED IN THE PLAN AND SHALL BE DISPOSED. ALL COSTS ASSOCIATED WITH THE REMOVAL AND DISPOSAL OF THESE PRIVATE SIGNS SHALL BE INCLUDED IN THE UNIT PRICE BID PER EACH FOR ITEM 202 REMOVAL, MISC.: PRIVATE SIGN.

REMOVAL, MISC.: METAL GATE ASSEMBLY

ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH ODOT ITEM 202. THE METAL GATE ASSEMBLY SHALL BE REMOVED FROM THE LOCATION INDICATED IN THE PLAN AND SHALL BE DISPOSED. ALL COSTS ASSOCIATED WITH THE REMOVAL AND DISPOSAL OF THIS METAL GATE ASSEMBLY SHALL BE INCLUDED IN THE UNIT PRICE BID PER EACH FOR ITEM 202 REMOVAL, MISC.: METAL GATE ASSEMBLY.

FENCE MISC.: WOOD FENCE

ALL WORK SHALL BE PERFORMED IN ACCORDANCE WITH ODOT ITEM 607 AND STANDARD CONSTRUCTION DRAWING RM-5.2. THIS WOOD BIKEWAY RAILING SHALL BE INSTALLED WHERE INDICATED ON THE PLAN. ALL COSTS ASSOCIATED WITH THE INSTALLATION OF THIS BIKEWAY RAILING SHALL BE INCLUDED IN THE UNIT PRICE BID PER FOOT FOR ITEM 607 FENCE MISC.: WOOD FENCE.

DESIGN AGENCY



DESIGNER
MJL

REVIEWER
KF 05/20/22

PROJECT ID
102375

SHEET TOTAL
30 | 705

**ITEM 452 - NON-REINFORCED CONCRETE PAVEMENT, MISC.:
8" CONCRETE SLAB STAMPED AND STAINED**

THIS ITEM SHALL BE CONSTRUCTED AS PER ITEM 452 EXCEPT THAT THE CONCRETE SHALL BE INTEGRALLY COLORED AND SHALL HAVE A STAMPED SURFACE. CONCRETE SHALL BE INSTALLED FOLLOWING THE INSTRUCTIONS FROM THE STAMPED COLORED CONCRETE PATTERN AND PIGMENT MANUFACTURER.

A CONCRETE COLORING SHALL BE INTEGRALLY ADDED TO THE CONCRETE TO PRODUCE A MEDIUM GRAY COLOR. THE CONTRACTOR SHALL SUBMIT THE TECHNICAL DATA AND A COLOR SAMPLE OF THE PIGMENT TO THE ENGINEER FOR APPROVAL NO LESS THAN 14 DAYS PRIOR TO USE. THE LIQUID COLORANT SHALL CONFORM TO ASTM C979.

THE PATTERN FOR THE STAMPED SURFACE SHALL CONFORM TO A FLAGSTONE PATTERN AND SHALL PRODUCE A SIMULATED STONE TEXTURE. THE STONES SHALL HAVE A MINIMUM NOMINAL DIMENSION OF 8 INCHES BY 8 INCHES AND HAVE A MAXIMUM DIMENSION OF 24 INCHES BY 24 INCHES.

RADIAL JOINTS HAVING A DEPTH OF 6 INCHES SHALL BE SAWED AT 20 FEET SPACING WITHIN 24 HOURS OF CONCRETE PLACEMENT.

AFTER PLACEMENT OF CONCRETE, THE CONTRACTOR SHALL APPLY AND ACRYLIC BASED CONCRETE SEALER AND CURING AGENT. THE SEALER SHALL HAVE A MEDIUM TO HIGH GLOSS AND BE APPLIED IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS. THE CONTRACTOR SHALL SUBMIT THE BRAND NAME AND TECHNICAL DATA FOR THE CONCRETE SEALER TO THE ENGINEER FOR APPROVAL NO LESS THAN 14 DAYS PRIOR TO USE.

PAYMENT SHALL BE MADE AT THE UNIT BID PRICE PER SQUARE YARD AND SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO PERFORM THE WORK NOTES ABOVE.

ITEM 203 - ROADWAY, MISC.: DECORATIVE STONE

DECORATIVE STONE (RIVER ROCK OR SIMILAR) SHALL BE PLACED AT A DEPTH OF 6 INCHES USING A FILTER BLANKET, TYPE B IN ACCORDANCE WITH ODOT SPECIFICATION 721.09 IN THE AREAS SHOWN ON THE PLANS. THE DECORATIVE STONE SHALL BE PLACED 1" BELOW ADJACENT WALKS AND CURBS. THE CONTRACTOR SHALL COORDINATE WITH THE ENGINEER TO SELECT THE TYPE OF DECORATIVE STONE USED. STONE SIZE SHALL MEET A NO. 2 GRADATION; NOMINAL SIZE OF 1 1/2" TO 2 1/2".

ALL EXCAVATION, MATERIALS, LABOR, AND EQUIPMENT REQUIRED TO COMPLETE THE ABOVE WORK SHALL BE INCLUDED IN THE UNIT BID PRICE PER SQUARE YARD FOR ITEM 203 - ROADWAY, MISC.: DECORATIVE STONE.

ITEM 606 - GUARDRAIL, TYPE MGS, AS PER PLAN

IN ADDITION TO THE REQUIREMENTS OF ITEM 606, THIS ITEM REQUIRES STEEL POSTS AND COMPOSITE OR POLYMER ALTERNATIVE BLOCKOUTS. THE BLOCKOUTS SHALL BE FROM THE APPROVED PRODUCTS LIST THAT IS MAINTAINED BY THE OFFICE OF ROADWAY ENGINEERING AND INSTALLED PER CONSTRUCTION AND MATERIALS SPECIFICATION 606 AND ALL PERTINENT STANDARD DRAWINGS AND PLAN INSERT SHEETS. ALL COSTS ASSOCIATED WITH PROVIDING AND INSTALLING STEEL POSTS AND APPROVED ALTERNATIVE MGS BLOCKOUTS SHALL BE INCLUDED IN THE UNIT BIDS FOR THE FOLLOWING ITEMS:

ITEM 606 - GUARDRAIL, TYPE MGS, AS PER PLAN

ITEM 606 - MGS BRIDGE TERMINAL ASSEMBLY, TYPE 1, AS PER PLAN

IN ADDITION TO THE REQUIREMENTS OF ITEM 606 AND STANDARD CONSTRUCTION DRAWING MGS-3.1, THIS ITEM REQUIRES THE USE OF STEEL POSTS. ALL COSTS ASSOCIATED WITH PROVIDING AND INSTALLING STEEL POSTS SHALL BE INCLUDED IN THE UNIT BID FOR ITEM 606, MGS BRIDGE TERMINAL ASSEMBLY, TYPE 1, AS PER PLAN

POST CONSTRUCTION STORM WATER TREATMENT

THIS PLAN UTILIZES STRUCTURAL BEST MANAGEMENT PRACTICES (BMP'S) FOR POST CONSTRUCTION STORM WATER TREATMENT.

REVIEW OF DRAINAGE FACILITIES

PRIOR TO THE START OF WORK AND AGAIN BEFORE FINAL ACCEPTANCE, PERFORM AN INSPECTION WITH REPRESENTATIVES OF THE DEPARTMENT, CONTRACTOR AND LOCALS OF ALL EXISTING DRAINAGE FACILITIES THAT ARE TO REMAIN IN SERVICE WHICH MAY BE AFFECTED BY THE WORK. THE CONDITION OF THE EXISTING CONDUITS AND THEIR APPURTENANCES IS DETERMINED FROM FIELD OBSERVATIONS. RECORDS OF THE INSPECTION ARE MAINTAINED BY THE DEPARTMENT.

CONFIRM ALL EXISTING SEWERS INSPECTED INITIALLY BY THE ABOVE-MENTIONED PARTIES ARE MAINTAINED AND LEFT IN A CONDITION COMPARABLE TO THAT DETERMINED BY THE ORIGINAL INSPECTION. THE CONTRACTOR IS RESPONSIBLE TO CORRECT ANY CHANGE IN THE CONDITION RESULTING FROM THEIR OPERATIONS AS DIRECTED AND APPROVED BY THE ENGINEER.

PAYMENT FOR ALL OPERATIONS DESCRIBED ABOVE IS INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 611 CONDUIT ITEMS.

CROSSINGS AND CONNECTIONS TO EXISTING PIPES AND UTILITIES

WHERE PLANS PROVIDE FOR A PROPOSED CONDUIT TO BE CONNECTED TO, OR CROSS OVER OR UNDER AN EXISTING SEWER OR UNDERGROUND UTILITY, THE CONTRACTOR SHALL LOCATE THE EXISTING PIPES OR UTILITIES BOTH AS TO LINE AND GRADE BEFORE STARTING TO LAY THE PROPOSED CONDUIT.

IF IT IS DETERMINED THAT THE ELEVATION OF THE EXISTING CONDUIT, OR EXISTING APPURTENANCE TO BE CONNECTED, DIFFERS FROM THE PLAN ELEVATION OR RESULTS IN A CHANGE IN THE PLAN CONDUIT SLOPE, THE ENGINEER SHALL BE NOTIFIED BEFORE STARTING CONSTRUCTION OF ANY PORTION OF THE PROPOSED CONDUIT WHICH WILL BE AFFECTED BY THE VARIANCE IN THE EXISTING ELEVATIONS.

IF IT IS DETERMINED THAT THE PROPOSED CONDUIT WILL INTERSECT AN EXISTING SEWER OR UNDERGROUND UTILITY IF CONSTRUCTED AS SHOWN ON THE PLAN, THE ENGINEER SHALL BE NOTIFIED BEFORE STARTING CONSTRUCTION OF ANY PORTION OF THE PROPOSED CONDUIT WHICH WOULD BE AFFECTED BY THE INTERFERENCE WITH AN EXISTING FACILITY.

PAYMENT FOR ALL THE OPERATIONS DESCRIBED ABOVE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 611 CONDUIT ITEM

EXISTING SUBSURFACE DRAINAGE

PROVIDE UNOBSTRUCTED OUTLETS FOR ALL EXISTING UNDERDRAINS OR AGGREGATE DRAINS ENCOUNTERED DURING CONSTRUCTION.

PROVIDE AN OUTLET PER STANDARD CONSTRUCTION DRAWING DM-1.1 FOR ALL UNDERDRAINS THAT OUTLET TO A SLOPE. UNDERDRAINS THAT CAN BE CONNECTED TO THE NEW OR EXISTING UNDERDRAINS AT THE END OF THE PROJECT LIMITS AS WELL AS ALL NECESSARY BENDS OR BRANCHES REQUIRED FOR CONNECTION ARE INCLUDED IN THE BASIS OF PAYMENT FOR UNCLASSIFIED PIPE UNDERDRAINS.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR THE WORK NOTED ABOVE:

- 601, TIED CONCRETE BLOCK MAT, TYPE 1 - 4 SQ. YD.
- 611, 6" CONDUIT, TYPE F - 100 FT.
- 611, PRECAST REINFORCED CONCRETE OUTLET - 2 EACH
- 605, 6" UNCLASSIFIED PIPE UNDERDRAINS 100 FT.

FARM DRAINS

PROVIDE UNOBSTRUCTED OUTLETS TO ALL FARM DRAINS ENCOUNTERED DURING CONSTRUCTION. REPLACE EXISTING COLLECTORS WHICH ARE LOCATED BELOW THE ROADWAY DITCH ELEVATIONS, AND WHICH CROSS THE ROADWAY WITHIN THE (RIGHT OF WAY)(CONSTRUCTION) LIMITS WITH ITEM 611, CONDUIT, TYPE B, ONE COMMERCIAL SIZE LARGER THAN THE EXISTING CONDUIT.

OUTLET EXISTING COLLECTORS AND ISOLATED FARM DRAINS, WHICH ARE ENCOUNTERED ABOVE THE ELEVATION OF ROADWAY DITCHES INTO THE ROADWAY.

DITCH USING ITEM 611, TYPE F CONDUIT. THE OPTIMUM OUTLET ELEVATION IS ONE FOOT ABOVE THE FLOWLINE ELEVATION OF THE DITCH. INTERCEPT LATERAL FIELD TILES WHICH CROSS THE ROADWAY WITH ITEM 611, TYPE E CONDUIT, AND CARRY IN A LONGITUDINAL DIRECTION TO AN ADEQUATE OUTLET OR ROADWAY CROSSING.

THE LOCATION, TYPE, SIZE AND GRADE OF REPLACEMENTS IS DETERMINED BY THE ENGINEER AND PAYMENT MADE ON FINAL MEASUREMENTS.

PROVIDE EROSION CONTROL PADS AT THE OUTLET END OF ALL FARM DRAINS PER STANDARD CONSTRUCTION DRAWING DM-1.1, EXCEPT WHEN THEY OUTLET INTO A DRAINAGE STRUCTURE.

PAYMENT FOR THE EROSION CONTROL PADS AND ANY NECESSARY BENDS OR BRANCHES IS INCLUDED FOR PAYMENT IN THE PERTINENT CONDUIT ITEMS.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR THE WORK NOTED ABOVE:

- 611 6" CONDUIT, TYPE B 100 FT.
- 611 6" CONDUIT, TYPE E 100 FT.
- 611 6" CONDUIT, TYPE F 100 FT.
- 601 ROCK CHANNEL PROTECTION TYPE C WITH FILTER 5 CU. YD.

ITEM 611 - MANHOLE, NO.3, AS PER PLAN

IN ADDITION TO THE REQUIREMENTS OF ITEM 611, THIS ITEM REQUIRES TEMPORARY SHORING. TEMPORARY SHORING PLAN TO BE APPROVED BY CSXT PRIOR TO INSTALLATION. ALL COST ASSOCIATED WITH DEVELOPING A TEMPORARY SHORING PLAN AND MATERIALS REQUIRED SHALL BE INCLUDED IN THE UNIT BID FOR THE FOLLOWING ITEMS:

ITEM 611 MANHOLE NO.3, AS PER PLAN

ITEM SPECIAL - FILL AND PLUG EXISTING CONDUIT

THIS ITEM CONSISTS OF THE CONSTRUCTION OF BULKHEADS IN AN EXISTING 36 INCH DIAMETER CONDUIT AND FILLING THE AREA SEALED OFF WITH ITEM 613, SAND OR OTHER MATERIAL APPROVED BY THE ENGINEER.

LOCATE THE BULKHEADS AT THE LIMITS OF THE AREA TO BE FILLED, AS INDICATED ON THE PLANS. THE BULKHEADS CONSIST OF BRICK OR CONCRETE MASONRY WITH A MINIMUM THICKNESS OF 12 INCHES.

PUMP THE FILL MATERIAL INTO PLACE OR BY OTHER MEANS APPROVED BY THE ENGINEER, SO THAT AFTER SETTLEMENT, AT LEAST 90 PERCENT OF THE CROSS-SECTIONAL AREA OF THE CONDUIT, FOR ITS ENTIRE LENGTH IS FILLED. THE LENGTH OF FILLED AND PLUGGED CONDUIT TO BE PAID FOR IS THE ACTUAL NUMBER OF FEET (MEASURED ALONG THE CENTERLINE OF EACH CONDUIT FROM OUTER FACE TO OUTER FACE OF BULKHEADS) FILLED AND PLUGGED AS DESCRIBED ABOVE.

IN LIEU OF FILLING AND PLUGGING THE EXISTING CONDUIT, THE PIPE MAY BE CRUSHED AND BACKFILLED PER 203, OR IT MAY BE REMOVED. THE LENGTH, MEASURED AS PROVIDED ABOVE, WILL BE PAID FOR AT THE CONTRACT PRICE PER FOOT FOR, ITEM SPECIAL, FILL AND PLUG EXISTING CONDUIT.

ITEM 606 - ANCHOR ASSEMBLY, MGS TYPE E (MASH 2016)

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING ANY OF THE GUARDRAIL END TERMINALS FOR TYPE MGS GUARDRAIL AS LISTED ON ROADWAY ENGINEERING'S WEB PAGE UNDER ROADSIDE SAFETY DEVICES FOR APPROVED GUARDRAIL END TREATMENTS. INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS, IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

THE FACE OF THE TYPE E IMPACT HEAD SHALL BE COVERED WITH A SHEET OF TYPE G REFLECTIVE SHEETING, PER CMS 730.19.

REFER TO THE MANUFACTURER'S INSTRUCTIONS REGARDING THE INSTALLATION OF, AND THE GRADING AROUND THE FOUNDATION TUBES AND GROUND STRUT. THE TOP OF ANY FOUNDATION TUBE SHOULD BE LESS THAN 4 INCHES ABOVE THE GROUND. THE PLACEMENT OF THE FOUNDATION TUBES SHOULD BE AN APPROPRIATE DEPTH BELOW THE LEVEL LINE IN ORDER TO MAINTAIN THE FINISHED GUARDRAIL HEIGHT OF 31 INCHES FROM THE EDGE OF THE SHOULDER.

ON-SITE GRADING IS REQUIRED IF THE TOP OF THE FOUNDATION TUBES OR TOP OF THE GROUND STRUT DOES PROJECT MORE THAN 4 INCHES ABOVE THE GROUND LINE.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID FOR ITEM 606, ANCHOR ASSEMBLY, MGS TYPE E, EACH, AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT A COMPLETE AND FUNCTIONAL ANCHOR ASSEMBLY SYSTEM, INCLUDING ALL RELATED TRANSITIONS, REFLECTIVE SHEETING, HARDWARE, GRADING, EMBANKMENT AND EXCAVATION NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER.



CSX TRANSPORTATION COORDINATION NOTES

REFER TO THE CSX TRANSPORTATION PUBLIC PROJECT INFORMATION MANUAL FOR ADDITIONAL REQUIREMENTS NEEDED FOR WORKING ON/ABOVE/ADJACENT TO CSXT. SPECIFIC SECTIONS THAT PERTAIN TO THIS PROJECT ARE SPECIAL PROVISIONS FOR CONSTRUCTION NEAR CSXT PROPERTY, OVERHEAD BRIDGE CRITERIA, CONSTRUCTION SUBMISSION CRITERIA, AND INSURANCE REQUIREMENTS FOR PUBLIC PROJECTS.

CONTRACTOR ACCESS WILL BE LIMITED TO THE IMMEDIATE PROJECT AREA ONLY. THE CSXT RIGHT-OF-WAY OUTSIDE THE PROJECT AREA MAY NOT BE USED FOR CONTRACTOR ACCESS TO THE PROJECT SITE AND NO TEMPORARY AT-GRADE CROSSINGS WILL BE ALLOWED.

THE CONTRACTOR WILL BE REQUIRED TO ABIDE BY THE PROVISIONS OF THE AGENCY/CSXT AGREEMENT(S). PERIODICALLY, THROUGHOUT THE PROJECT DURATION, THE CONTRACTOR WILL BE REQUIRED TO MEET, DISCUSS AND, IF NECESSARY, TAKE IMMEDIATE ACTION AT THE DISCRETION OF CSXT PERSONNEL AND/OR THEIR AUTHORIZED REPRESENTATIVE, TO COMPLY WITH PROVISIONS OF THAT AGREEMENT AND THESE SPECIFICATIONS.

THE CONTRACTOR MAY NOT USE CSXT RIGHT-OF-WAY FOR STORAGE OF MATERIALS OR EQUIPMENT DURING CONSTRUCTION WITHOUT PRIOR CSXT APPROVAL. THE CSXT RIGHT-OF-WAY MUST ALWAYS REMAIN CLEAR FOR RAILROAD USE. EQUIPMENT MAY NOT BE POSITIONED TO BLOCK THE RAILROAD ACCESS ROAD, TRACK AREA OR ANY PART OF THE CSXT RIGHT-OF-WAY WITHOUT PRIOR CSXT APPROVAL.

CSXT SHALL BE NOTIFIED AT LEAST FIVE (5) DAYS IN ADVANCE OF THE PRE-CONSTRUCTION MEETING.

THE CONTRACTOR SHALL COORDINATE ALL WORK ON, OVER OR ADJACENT TO THE RAILROADS WITHIN THE PROJECT'S LIMITS. THE CONTRACTOR SHALL CONTACT CSX RAILROAD, AT LEAST THIRTY (30) DAYS IN ADVANCE, IN ORDER TO COORDINATE THE NECESSARY WORK. UNDER NO CIRCUMSTANCES SHALL THERE BE ANY WORK WITHIN THE RAILROAD RIGHT-OF-WAY WITHOUT THE PROPER AUTHORIZATION AND/OR FLAG PROTECTION FROM THE RAILROAD.

CSXT REQUIRES THAT THE CONTRACTOR SUBMIT AND RECEIVE ACCEPTANCE OF A COMPREHENSIVE MEANS & METHODS SUBMITTAL (CSXT CONSTRUCTION SUBMISSION CRITERIA, ISSUED APRIL 2022) DETAILING SCOPE WORK WITHIN CSXT TRACKS OR RIGHT-OF-WAY, OR OTHER WORK WHICH PRESENTS THE POTENTIAL TO AFFECT CSXT PROPERTY OR OPERATIONS TO UNDERTAKING THE WORK.

ALL LIFTING EQUIPMENT AND CONNECTION DEVICES SHALL HAVE A CAPACITY FOR 150% OF THE ACTUAL LIFTING LOAD. THE FACTOR OF SAFETY PROVIDED BY THE MANUFACTURER IN THE LIFTING CAPACITY DATA SHALL NOT BE CONSIDERED IN THE 150% REQUIREMENT.

TEMPORARY CONSTRUCTION CLEARANCES (HORIZONTAL & VERTICAL) PROPOSED - FOR EXISTING OR LESS THAN STANDARD CONDITIONS - SHALL BE SUBJECT TO APPROVAL BY CSXT. TYPICALLY REDUCTION IN CONSTRUCTION CLEARANCES ARE NOT PERMITTED.

DURING AND AFTER COMPLETION OF CONSTRUCTION, THE OUTSIDE PARTY OR ITS CONTRACTOR SHALL CLEAR CSXT'S DRAINAGE DITCHES OF ALL DEBRIS TO THE SATISFACTION OF CSXT'S CONSTRUCTION MONITORING REPRESENTATIVE.

A WORK SITE SAFETY PLAN THAT INCLUDES A RECOGNITION TO KEEP ALL PERSONNEL FROM FOULING CSXT RAIL OPERATIONS, A FALL PROTECTION PLAN DESCRIBING THE MEASURES TO BE TAKEN WHEN REQUIRED, AND A FIRE PROTECTION PLAN SHALL BE PRESENTED AND ACCEPTED BY CSXT FOR WORK ON, OVER OR ADJACENT CSXT PROPERTY.

ALL WASTE MATERIALS GENERATED BY THIS PROJECT, INCLUDING WASHING WITH CLEANING SOLVENTS, BLASTING, SCRAPING, BRUSHING AND/OR PAINTING OPERATIONS, SHALL BE THE RESPONSIBILITY OF THE STATE OR ITS CONTRACTOR, AND SHALL BE CONTAINED, COLLECTED AND PROPERLY DISPOSED OF BY THE STATE OR ITS CONTRACTOR. THE STATE AND ITS CONTRACTOR AGREE TO FULLY COMPLY WITH ALL FEDERAL, STATE, AND LOCAL ENVIRONMENTAL LAWS, REGULATIONS, STATUTES AND ORDINANCES AT ALL TIMES.

CSXT MAY REQUIRE FULL TIME RAILROAD FLAGGING FOR ANY PROJECT TASKS THAT MAY HAVE THE POTENTIAL TO FOUL THE TRACK OR CAUSE A HAZARD TO TRAIN MOVEMENTS.

CSXT HAS SOLE AUTHORITY TO DETERMINE THE NEED FOR TRACK PROTECTION REQUIRED TO PROTECT ITS OPERATIONS AND PROPERTY. IN GENERAL, TRACK PROTECTION WILL BE REQUIRED WHENEVER CONTRACTOR OR EQUIPMENT ARE, OR ARE LIKELY TO BE, WORKING WITHIN FIFTY (50) FEET OF TRACK OR OTHER TRACK CLEARANCES AS SPECIFIED BY CSXT.

UPON COMPLETION OF THE WORK ON CSXT PROPERTY, THE CONTRACTOR SHALL REQUEST THE OWNER TO ARRANGE A FINAL INSPECTION OF THE PROJECT WITH THE RAILROAD'S PROJECT ENGINEER OR THEIR AUTHORIZED REPRESENTATIVE.

PERSONAL PROTECTION EQUIPMENT (PPE)

THE CONTRACTOR SHALL FOLLOW ALL REQUIREMENTS OF SECTIONS XXIV AND XXXIV OF THE OHIO DEPARTMENT OF TRANSPORTATION SAFETY & HEALTH STANDARD OPERATING PRECEDURE 220-006 (SP) EFFECTIVE: NOVEMBER 1, 2018 (EXCEPT AS AMENDED BELOW) AND ALL SUBSEQUENT UPDATES POSTED AT THE FOLLOWING WEBSITE:

HTTP://WWW.DOT.STATE.OH.US/POLICY/POLICIESANDSOPS/POLICIES/220-006(SP).PDF

AMENDMENTS TO THE REQUIREMENTS OF THIS DOCUMENTS ARE:

XXIV. HEAD PROTECTION (HARD HATS)

ALL PERSONS WITHIN THE RIGHT-OF-WAY OF ANY HIGHWAY OR ANY OTHER TYPE OF ROADWAY OR CONSTRUCTION SITE WHO ARE EXPOSED TO EITHER TRAFFIC (VEHICLES USING THE HIGHWAY FOR PURPOSES OF TRAVEL) OR CONSTRUCTION EQUIPMENT WITHIN THE WORK AREA, REGARDLESS OF JOB TYPE, SHALL WEAR APPROPRIARE HEAD PROTECTION. ALL HARD HATS MUST MEET OR EXCEED ANSI Z89.1-2009 TYPE 1, CLASS E-G REQUIREMENTS.

XXXIV. SAFETY APPAREL AND VEST (HIGH VISIBILITY)

ALL PERSONS WITHIN THE RIGHT-OF-WAY OF ANY HIGHWAY OR ANY OTHER TYPE OF ROADWAY OR CONSTRUCTION SITE WHO ARE EXPOSED TO EITHER TRAFFIC (VEHICLES USING THE HIGHWAY FOR PURPOSES OF TRAVEL) OR CONSTRUCTION EQUIPMENT WITHIN THE WORK AREA, REGARDLESS OF JOB TYPE, SHALL WEAR A HIGH-VISIBILITY SAFETY VESR THAT MEETS THE PERFORMANCE CLASS II OR CLASS III REQUIREMENTS OF THE ANSI/SEA 107-2015 PUBLICATION ENTITLED "AMERICAN NATIONAL STANDARD FOR HIGH-VISIBILITY SAFETY APPAREL AND ACCESSORIES."

WORKERS MAY WEAR ANSI CLASS II OR ANSI CLAS III APPROVED RAIN SUIT, JACKET OR OTHER APPAREL WITHOUT A SAFETY VEST OVER IT.

INTERIM COMPLETION DATE

THE RERCONSTRUCTED LANES OF CR 99, RECONSTRUCTED IR 75 RAMPS AND INTERSECTING ROADWAYS SHALL BE COMPLETED AND OPEN TO UNRESTRICTED TRAFFIC BY NOVEMBER 30, 2025. COMPLETED AND OPEN TO UNRESTRICTED TRAFFIC IS DEFINED AS ALL TRAFFIC LANES BEING AVAILABLE FOR USE AT THEIR FINAL DESIGN WIDTH WITH ALL PAVEMENT SURFACES COMPLETED AND ALL FINAL MARKINGS, RPM'S, AND SAFETY FEATURES INSTALLED, ALONG WITH NO RESTRICTIONS WITHIN 2 FEET OF THE EDGE LINE ON THE SHOULDERS OR WITHIN 2 FEET OF THE EDGE OF LANES. THE ABOVE MENTIONED DATE SHALL BE CONSIDERED AN INTERIM COMPLETION DATE. IF THE NOTED ROADWAY SECTIONS ARE NOT COMPLETED AND OPEN AS STATED ABOVE BY INTERIM COMPLETION DATE, THE CONTRACTOR SHALL BE SUBJECT TO DAILY DISINCENTIVES OF \$1,600.

THE CONTRACTOR IS STILL SUBJECT TO LIQUIDATED DAMAGES AS OUTLINED IN CMS 108.07 FOR THE REMAINDER OF THE CONTRACT.

DESIGN AGENCY



DESIGNER
MJL

REVIEWER
KF 05/20/22

PROJECT ID
102375

SHEET TOTAL
34A | 705

SEQUENCE OF CONSTRUCTION

PREPHASE PHASE 1

CONSTRUCTION:

- CONSTRUCT TEMPORARY PAVEMENT ALONG EAST SIDE OF TECHNOLOGY DRIVE.
- CONSTRUCT TEMPORARY PAVEMENT ALONG EAST SIDE OF SPEEDWAY DRIVE.
- INSTALL DRAINAGE PIPE ALONG CR 99 EASTBOUND FROM BALL CORPORATION DRIVEWAY NEAR STA. 131+00 TO OUTFALL NEAR STA. 153+10
- * - INSTALL DRAINAGE PIPE CROSSING CR 99 NEAR STA 129+20.
- * - INSTALL DRAINAGE PIPE ALONG CR 99 WESTBOUND FROM NORTH MAIN STREET TO BALL CORPORATION DRIVEWAY NEAR STA 131+00.

TRAFFIC:

- TECHNOLOGY DRIVE WILL REDUCE NORTHBOUND LANE WIDTH FOR CONSTRUCTION OF TEMPORARY PAVEMENT ALONG RIGHT SHOULDER.
- SPEEDWAY DRIVE WILL REDUCE NORTHBOUND LANE WIDTH FOR CONSTRUCTION OF TEMPORARY PAVEMENT ALONG RIGHT SHOULDER.
- USING SHORT-TERM OPERATIONS, CLOSE RIGHT LANE OF CR-99 EASTBOUND, REDUCING TO ONE (1) LANE DURING DRAINAGE PIPE INSTALLATION.
- USING SHORT-TERM OPERATIONS, CLOSE LEFT LANE OF CR-99 EASTBOUND AND WESTBOUND, REDUCING TO ONE (1) LANE IN EACH DIRECTION DURING DRAINAGE PIPE CROSSING INSTALLATION.
- * - USING SHORT-TERM OPERATIONS, CLOSE RIGHT LANE OF CR-99 WESTBOUND, REDUCING TO ONE (1) LANE DURING DRAINAGE PIPE INSTALLATION.

PHASE 1

CONSTRUCTION:

- REMOVE RIGHT SHOULDER RUMBLE STRIPS ALONG NORTHBOUND AND SOUTHBOUND IR 75.
- PATCH OR REPAIR DAMAGED PAVEMENT, INLETS, AND MEDIAN BARRIER ALONG RIGHT SHOULDER PRIOR TO TRAFFIC SHIFT.
- ** - CONSTRUCT CR 99 WESTBOUND BRIDGE. THIS INCLUDES ABUTMENT FOUNDATIONS AND PIER. PRIOR TO WORK ON THE ABUTMENTS, EMBANKMENT IS REQUIRED IN THE AREA DUE TO SETTLEMENT. SEE STRUCTURE PLANS FOR DURATION.
- INSTALL DRAINAGE ALONG RIGHT SHOULDERS AND ABUTMENTS.

TRAFFIC:

- ** - NORTHBOUND AND SOUTHBOUND IR 75 TRAFFIC WILL MAINTAIN THREE (3) LANES, SHIFTING TO THE OUTSIDE SHOULDERS, AND CLOSING THE MEDIAN SHOULDERS FOR PIER AND ABUTMENT CONSTRUCTION.

PHASE 1 STEP 1

CONSTRUCTION:

- INSTALL TEMPORARY SIGNAL POLES AND SIGNALS.
- ** - CONTINUE TO CONSTRUCT CR 99 WESTBOUND BRIDGE. THIS INCLUDES ABUTMENTS, WINGWALLS, AND SUPERSTRUCTURE.
- CONSTRUCT CR 99 EASTBOUND PAVEMENT WIDENING.

- *** - CONSTRUCT RAMP AA AND SOUTHERN PORTION OF RAMP A.
- *** - CONSTRUCT RAMP B AND SOUTHERN PORTION OF RAMP BB.

- CONSTRUCT PAVEMENT AND CURB ON WEST SIDE OF TECHNOLOGY DRIVE.
- CONSTRUCT PAVEMENT, CURB, AND SIDEWALK ON WEST SIDE OF SPEEDWAY DRIVE.
- CONSTRUCT CURB AND ADA RAMPS ON EAST AND WEST SIDES OF VENTURA DRIVE.
- CONSTRUCT CURB, SIDEWALK, AND ADA RAMPS ON SOUTHEAST AND SOUTHWEST QUADRANTS OF NORTH MAIN STREET INTERSECTION.

- REMOVE LEFT SHOULDER RUMBLE STRIPS ALONG NORTHBOUND AND SOUTHBOUND IR 75.
- PATCH OR REPAIR DAMAGED PAVEMENT, INLETS, AND MEDIAN BARRIER ALONG LEFT SHOULDER PRIOR TO TRAFFIC SHIFT.
- CONSTRUCT ACCEL. LANE AND SHOULDER FOR RAMP A.
- CONSTRUCT DECEL. LANE AND SHOULDER FOR RAMP B

TRAFFIC:

- EASTBOUND AND WESTBOUND CR 99 TRAFFIC WILL BE REDUCED TO ONE (1) LANE AND SHIFTED NORTH BETWEEN TECHNOLOGY AND I-75 SB RAMPS. AT THE INTERCHANGE, A WB AND EB LEFT WILL BE ESTABLISHED FOR ENTERING I-75. FROM I-75 NB RAMPS TO NORTH MAIN STREET, A LEFT TURN LANE WILL BE INCLUDED FOR SPEEDWAY DRIVE, VENTURA DRIVE AND NORTH MAIN STREET.

- *** - RAMP A WILL MAINTAIN ONE (1) LANE OF TRAFFIC.
- *** - RAMP B WILL MAINTAIN ONE (1) LANE OF TRAFFIC WITH A REDUCED WIDTH.
- RAMPS C AND D WILL MAINTAIN THEIR EXISTING LANE CONFIGURATION.

- ** - NORTHBOUND AND SOUTHBOUND IR 75 TRAFFIC WILL MAINTAIN THREE (3) LANES, SHIFTING TO THE CENTER MEDIAN AND CLOSING THE RIGHT SHOULDERS.
- REDUCE RAMPS A AND B ACCEL/DECEL LANE LENGTHS.
- RAMPS C AND D WILL MAINTAIN THEIR EXISTING ACCEL/DECEL LANE CONFIGURATIONS.

PHASE 1 STEP 2

CONSTRUCTION:

- CONTINUE TO CONSTRUCT CR 99 WESTBOUND BRIDGE. THIS INCLUDES ABUTMENTS, WINGWALLS, AND SUPERSTRUCTURE.
- CONSTRUCT ROADWAY APPROACH FOR CR 99 WESTBOUND BRIDGE.
- CONSTRUCT PAVEMENT AND CURB ON EAST SIDE OF TECHNOLOGY DRIVE.
- CONSTRUCT PAVEMENT, CURB, AND SIDEWALK ON EAST SIDE OF SPEEDWAY DRIVE.
- REMOVE EXISTING PAVEMENT AND COMPLETE GRADING ALONG RAMP A AND RAMP B.
- CONSTRUCT GORE AREA AND SHOULDERS ALONG RAMP A AND RAMP B.
- CONTINUE TO CONSTRUCT DRAINAGE AND GRADING ALONG RIGHT SHOULDERS FOR IR 75.

TRAFFIC:

- MAINTAIN EASTBOUND AND WESTBOUND CR 99 TRAFFIC FROM PHASE 1 STEP 1.
- SHIFT TRAFFIC TO RAMP A AND RAMP B CONSTRUCTED IN PHASE 1.
- RAMP B WILL MAINTAIN ONE (1) LANE OF TRAFFIC WITH A REDUCED WIDTH.
- RAMP C AND RAMP D WILL MAINTAIN THEIR EXISTING LANE CONFIGURATION.
- SHIFT TECHNOLOGY DRIVE TRAFFIC TO WEST SIDE CONSTRUCTED IN PHASE 1.
- SHIFT SPEEDWAY DRIVE TRAFFIC TO WEST SIDE CONSTRUCTED IN PHASE 1.
- MAINTAIN NORTHBOUND AND SOUTHBOUND IR 75 TRAFFIC FROM PHASE 1 STEP 1.

PHASE 2

CONSTRUCTION:

- MODIFY TEMPORARY SIGNALS FOR NEW TRAFFIC PATTERN.
- REHABILITATE CR 99 EASTBOUND BRIDGE. THIS INCLUDES REMOVING EXISTING BRIDGE DECK.
- CONSTRUCT CR 99 WESTBOUND PAVEMENT WIDENING.
- *** - CONSTRUCT RAMP CC AND NORTHERN PORTION OF RAMP C.
- *** - CONSTRUCT RAMP DD AND NORTHERN PORTION OF RAMP D.
- CONSTRUCT PAVEMENT AND CURB ON NORTH SIDE OF TECHNOLOGY DRIVE.

- CONSTRUCT CURB, SIDEWALK, AND ADA RAMPS ON NORTHEAST AND NORTHWEST QUADRANTS OF NORTH MAIN STREET INTERSECTION.
- CONSTRUCT ACCEL. LANE AND SHOULDER FOR RAMP C.
- CONSTRUCT DECEL. LANE AND SHOULDER FOR RAMP D.

TRAFFIC:

- EASTBOUND AND WESTBOUND CR 99 TRAFFIC WILL BE REDUCED TO ONE (1) LANE AND SHIFTED SOUTH BETWEEN TECHNOLOGY DRIVE AND NORTH MAIN STREET.
- *** - RAMP C WILL MAINTAIN ONE (1) LANE OF TRAFFIC.
- *** - RAMP D WILL MAINTAIN ONE (1) LANE OF TRAFFIC WITH A REDUCED WIDTH.
- NORTHBOUND AND SOUTHBOUND IR 75 TRAFFIC WILL MAINTAIN THREE (3) LANES, SHIFTING TO THE CENTER MEDIAN AND CLOSING THE RIGHT SHOULDERS.
- REDUCE RAMPS C AND D ACCEL/DECEL LANE LENGTHS.
- RAMP A AND RAMP B WILL MAINTAIN THEIR EXISTING ACCEL/DECEL LANE CONFIGURATIONS.

PHASE 2 STEP 1

CONSTRUCTION:

- CONSTRUCT SOUTHERN PORTION OF RAMP C AND RAMP D.
- REMOVE EXISTING PAVEMENT AND COMPLETE GRADING RAMP C AND RAMP D.
- CONSTRUCT CENTER ISLANDS ADJACENT TO RAMP A AND RAMP B.
- REMOVE EXISTING PAVEMENT AND COMPLETE GRADING RAMP C AND RAMP D.
- CONSTRUCT GORE AREA AND SHOULDERS ALONG RAMP C AND RAMP D.

TRAFFIC:

- MAINTAIN EASTBOUND AND WESTBOUND CR 99 TRAFFIC FROM PHASE 2.
- SHIFT TRAFFIC TO RAMP C AND RAMP D CONSTRUCTED IN PHASE 2.
- RAMP A AND RAMP B WILL MAINTAIN THEIR EXISTING LANE CONFIGURATION.
- MAINTAIN NORTHBOUND AND SOUTHBOUND IR 75 TRAFFIC FROM PHASE 2.

PHASE 3

CONSTRUCTION:

- MODIFY TEMPORARY SIGNALS FOR NEW TRAFFIC PATTERN.
- CONSTRUCT CENTER ISLANDS ADJACENT TO RAMP C AND RAMP D.
- INSTALL PERMANENT SIGNAL EQUIPMENT.
- INSTALL FINAL OVERLAY AND PAVEMENT MARKINGS.
- ERECT FINAL TRAFFIC CONTROL SIGNS.

TRAFFIC:

- EASTBOUND AND WESTBOUND CR 99 TRAFFIC WILL OPEN TO TWO (2) LANES THROUGH THE INTERCHANGE.
- RAMPS A, B, C, AND D WILL MAINTAIN THEIR EXISTING LANE CONFIGURATION.
- IR 75 WILL MAINTAIN THE EXISTING LANE CONFIGURATION.

*SEE DETOUR SHEET FOR INSTALLATION OF DRAINAGE PIPE ALONG CR 99 WESTBOUND FROM NORTH MAIN ST ON SHEET 48F.

** SEE MAINTAINING TRAFFIC, CR 99 WESTBOUND BRIDGE CONSTRUCTION NOTE ON SHEET 40, DETOUR SHEET ON 48A, PLAN SHEETS ON 73A - 73S

*** SEE DETOUR SHEETS CREATED FOR INSTALLATION OF DRAINAGE PIPE ACROSS EACH RAMP ON SHEETS 48B, 48C, 48D, 48E.

ITEM 614, MAINTAINING TRAFFIC (NOTICE OF CLOSURE SIGN)

NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. [AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK.]

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD/RAMP FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.

NOTICE OF CLOSURE SIGN TIME TABLE		
ITEM	DURATION OF CLOSURE	SIGN DISPLAYED TO PUBLIC
RAMP & ROAD CLOSURES	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	7 CALENDAR DAYS PRIOR TO CLOSURE
	<= 12 HOURS	2 BUSINESS DAYS PRIOR TO CLOSURE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION. THIS IS TO BE A SPECIFIC OFFICE WITHIN THE DISTRICT RATHER THAN THE GENERAL SWITCHBOARD NUMBER.

ITEM 614, MAINTAINING TRAFFIC (LANES OPEN DURING HOLIDAYS OR SPECIAL EVENTS)

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

- NEW YEARS
- TOTAL SOLAR ECLIPSE (04/08/24)
- MEMORIAL DAY
- FORTH OF JULY
- LABOR DAY
- GENERAL/REGULAR ELECTION DAY (NOV)
- THANKSGIVING
- CHRISTMAS

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY OR EVENT	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00AM TUESDAY
TOTAL SOLAR ECLIPSE	12:00N FRIDAY THROUGH 6:AM WEDNESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00AM WEDNESDAY
GEN./REG. ELECTION	5:00AM TUESDAY THROUGH 12:00AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00AM FRIDAY
THANKSGIVING	6:00AM WEDNESDAY THROUGH 6:00AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00AM MONDAY

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT (PN 127).

DESIGN AGENCY



DESIGNER
CO

REVIEWER
KF 05/20/22

PROJECT ID
102375

SHEET TOTAL
35 | 705

OVERHEAD-MOUNTED WORK ZONE SIGNALS

SIGNALS SHALL BE OVERHEAD MOUNTED IN ACCORDANCE WITH THE DETAILS SHOWN ON TRAFFIC SCD MT-96.20.

LIGHTING

LIGHTING SHALL BE PROVIDED AT EACH END OF THE LANE CLOSURE FOR THE CLOSING OF ONE LANE OF A TWO LANE HIGHWAY.

LIGHTING SHALL BE BY CONVENTIONAL METHODS, WITH LUMINAIRE ARMS ATTACHED TO THE SIGNAL SUPPORTS. AREA ILLUMINATION SHALL BE PROVIDED BY USING AN 8000-LUMEN LED, 150 WATT MINIMUM HIGH PRESSURE SODIUM LUMINARIES OR 250 WATT MINIMUM MERCURY LUMINARIES. THE MINIMUM HEIGHT OF THE LUMINAIRE SHALL BE 27 FT FROM THE GROUND SURFACE.

PAYMENT FOR LIGHTING SHALL INCLUDE DELIVERY, ERECTION, MAINTENANCE AND REMOVAL AS CALLED FOR IN THE PLANS. PAYMENT SHALL BE PER EACH.

MAINTENANCE OF TRAFFIC SIGNAL/FLASHER INSTALLATION

THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING TRAFFIC SIGNAL/FLASHER INSTALLATIONS WITHIN THE PROJECT UNDER THE FOLLOWING CONDITIONS:

1. EXISTING SIGNAL/FLASHER INSTALLATIONS WHICH THE PLANS REQUIRE THE CONTRACTOR TO ADJUST, MODIFY, ADD ONTO OR REMOVE, OR WHICH THE CONTRACTOR ACTUALLY ADJUSTS, MODIFIES OR OTHERWISE DISTURBS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE ENTIRE INSTALLATION (AT AN INTERSECTION) FROM THE TIME HIS OPERATIONS FIRST DISTURB THE INSTALLATION UNTIL THE INSTALLATION HAS BEEN SUBSEQUENTLY REMOVED OR MODIFIED AND THE WORK IS ACCEPTED.
2. NEW OR REUSED SIGNAL/FLASHER INSTALLATIONS OR DEVICES, INSTALLED BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE OF THESE FROM THE TIME OF INSTALLATION UNTIL THE WORK IS ACCEPTED.

THE CONTRACTOR SHALL CORRECT AS QUICKLY AS POSSIBLE ALL OUTAGES OR MALFUNCTIONS. HE SHALL PROVIDE THE MAINTAINING AGENCY AND THE ENGINEER SUCH ADDRESSES AND PHONE NUMBERS WHERE HIS MAINTENANCE FORCES CAN BE CONTACTED. THE CONTRACTOR SHALL PROVIDE ONE OR MORE PERSONS TO RECEIVE ALL CALLS AND DISPATCH THE NECESSARY MAINTENANCE FORCES TO CORRECT OUTAGES. SUCH A PERSON OR PERSONS MAY BE USED TO PERFORM OTHER DUTIES AS LONG AS PROMPT ATTENTION IS GIVEN TO THESE CALLS AND A PERSON IS READILY AVAILABLE CONTINUOUSLY 24 HOURS A DAY, 7 DAYS A WEEK. ALL LAMP OUTAGES, CABLE OUTAGES, ELECTRICAL FAILURES, EQUIPMENT MALFUNCTIONS AND MISALIGNED SIGNAL HEADS SHALL BE CORRECTED TO THE SATISFACTION OF THE ENGINEER WITH THE SIGNAL BACK TO SERVICE WITHIN FOUR HOURS AFTER THE CONTRACTOR HAS BEEN NOTIFIED OF THE OUTAGE.

IN THE EVENT NEW SIGNALS ARE DAMAGED PRIOR TO ACCEPTANCE, ALL DAMAGED EQUIPMENT EXCEPT POLES AND CONTROL EQUIPMENT SHALL BE REPLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER WITH THE SIGNAL BACK IN SERVICE WITHIN 8 HOURS AFTER THE CONTRACTOR'S NOTIFICATION OF THE OUTAGE. THE CONTRACTOR SHALL ARRANGE FOR FULL TRAFFIC CONTROL UNTIL THE SIGNAL IS BACK IN OPERATION. IF POLES AND/OR CONTROL EQUIPMENT ARE DAMAGED AND MUST BE REPLACED, THE CONTRACTOR SHALL MAKE TEMPORARY REPAIRS AS

NECESSARY TO BRING THE SIGNAL BACK INTO FULL OPERATION WITHIN THE ALLOWED 8-HOUR PERIOD, AND SHALL MAKE PERMANENT REPAIRS OR REPLACEMENT AS SOON THEREAFTER AS POSSIBLE.

NONE OF THE ABOVE SHALL BE CONSTRUED AS COLLECTIVE OR CONSECUTIVE OUTAGE TIME PERIODS AT ANY ONE LOCATION. THAT IS, WHERE MORE THAN ONE OUTAGE OCCURS AT ANY ONE LOCATION THEN THE ALLOTTED TIME LIMIT SHALL BE FOR THE WORST SINGLE OUTAGE.

WHERE OUTAGES ARE THE DIRECT RESULT OF A VEHICLE ACCIDENT THE RESPONSE OF THE CONTRACTOR SHALL BE AS OUTLINED ABOVE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COLLECTION OF ANY COMPENSATION FOR THIS WORK FROM THOSE PARTIES RESPONSIBLE FOR THE DAMAGE.

WHERE THE CONTRACTOR HAS FAILED TO, OR CANNOT RESPOND TO, AN OUTAGE OR SIGNAL EQUIPMENT MALFUNCTION, AT THESE LOCATIONS WITHIN HIS RESPONSIBILITY, WITHIN PERIODS AS SPECIFIED ABOVE, THE ENGINEER MAY INVOKE THE PROVISIONS OF SECTION 105.15 AND ANY SUBSEQUENT BILLINGS TO THE STATE OR THE CITY OF FINDLAY FOR POLICE SERVICES AND MAINTENANCE SERVICES BY CITY FORCES SHALL BE DEDUCTED FROM MONIES DUE OR TO BECOME DUE THE CONTRACTOR IN ACCORDANCE WITH PROVISIONS OF SECTION 105.15.

THE CONTRACTOR SHALL PROVIDE THE MAINTENANCE SERVICE ENTIRELY WITH HIS FORCES OR HE MAY CHOOSE TO ENTER INTO A COOPERATIVE UNDERSTANDING WITH THE LOCAL MAINTAINING AGENCY TO PROVIDE THE MAINTENANCE. THE CONTRACTOR SHALL INFORM THE ENGINEER, IN WRITING, OF THE MAINTENANCE METHOD SELECTED.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO ANY TRAFFIC SIGNAL COMPONENTS REQUIRED TO BE HANDLED DURING THE RELOCATION OF POLES AND REVISIONS TO THE SIGNAL SYSTEM. WHEN A TRAFFIC SIGNAL MUST BE TAKEN OUT OF SERVICE BY THE CONTRACTOR, DUE TO CONSTRUCTION PROCEDURES, THIS OUTAGE SHALL NOT EXCEED 9 HOURS AND SHALL NOT INCLUDE THE HOURS OF 6 AM TO 9 PM. ANY SIGNALIZED INTERSECTION, WHERE THE SIGNAL IS OUT OF SERVICE DUE TO CONSTRUCTION PROCEDURES, OR DUE TO AN OUTAGE OR MALFUNCTION OF EQUIPMENT AS DESCRIBED ABOVE, SHALL BE PROTECTED, BY THE CONTRACTOR, BY THE INSTALLATION OF TEMPORARY "STOP" SIGNS, EXCEPT FOR THE FOLLOWING INTERSECTIONS WHICH SHALL BE PROTECTED BY OFF-DUTY CITY OF FINDLAY POLICE, HIRED BY THE CONTRACTOR:

1. CR 99 & SB RAMP
2. CR 99 & NB RAMP
3. CR 99 & SPEEDWAY DR

ANY VEHICULAR TRAFFIC SIGNAL HEAD, EITHER NEW OR EXISTING WHICH WILL BE OUT OF OPERATION SHALL BE COVERED IN THE MANNER DESCRIBED IN 632.25.

THE CONTRACTOR SHALL MAINTAIN COMPLETE RECORDS OF MALFUNCTIONS INCLUDING:

1. TIME OF NOTIFICATION OF MALFUNCTION;
2. TIME OF WORK CREWS ARRIVAL TO CORRECT THE MALFUNCTION;
3. ACTIONS TAKEN TO CORRECT THE MALFUNCTION, INCLUDING A LIST OF PARTS REPAIRED OR REPLACED;
4. A DIAGNOSIS OF REASON FOR THE MALFUNCTION AND PROBABILITY OF REOCCURRENCE;

5. TIME OF COMPLETION OF THE REPAIR AND SYSTEM RESTORED TO FULL SERVICE.

A COPY OF THESE RECORDS SHALL BE PROVIDED TO THE ENGINEER WITHIN THREE (3) WORKING DAYS FOLLOWING COMPLETION OF EACH REPAIR.

ALL COSTS RESULTING FROM THE ABOVE REQUIREMENTS SHALL BE CONSIDERED TO BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614, MAINTAINING TRAFFIC.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

THE PROBABLE PCMS LOCATIONS AND WORK LIMITS FOR THOSE LOCATIONS ARE SHOWN ON SHEET(S) _____ OF THE PLAN. PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

(THE CONTRACTOR SHALL IMPLEMENT A SYSTEM WHEREBY CHANGEABLE MESSAGES WILL BE IMPLEMENTED WITHIN _____ HOURS FOLLOWING TELEPHONE NOTIFICATION FROM THE PROJECT ENGINEER TO A DESIGNATED PHONE.)

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED

AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

(THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL (IN ACTIVE CELLULAR PHONE AREAS) ALLOW REMOTE SIGN ACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS AND REVISIONS TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES. ONE REMOTE DATA INPUT DEVICE (LAPTOP COMPUTER PLUS MODEM OR EQUIVALENT) SHALL BE FURNISHED FOR USE BY THE DISTRICT TRAFFIC ENGINEER, OR EQUIVALENT, AND SHALL BE INSURED AGAINST THEFT.) THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614, PORTABLE MESSAGE SIGN, AS PER PLAN
4 SIGNS, ASSUMING 24 MONTHS

ALTERNATE METHODS

IF THE CONTRACTOR SO ELECTS, THEY MAY SUBMIT ALTERNATE METHODS FOR MAINTENANCE OF TRAFFIC PROVIDED THE INTENT OF THE ABOVE PROVISIONS ARE FOLLOWED AND NO ADDITIONAL INCONVENIENCE TO THE TRAVELING PUBLIC RESULTS THEREFROM. NO ALTERNATE PLAN SHALL BE PLACED INTO EFFECT UNTIL APPROVAL HAS BEEN GRANTED, IN WRITING, BY THE DISTRICT CONSTRUCTION ENGINEER. ALLOW 2 WEEKS FOR REVIEW WITH NO DELAY TO THE PROJECT. NO ADDITIONAL MONEY WILL BE PAID FOR ALTERNATE METHODS.

DESIGN AGENCY



DESIGNER
CO

REVIEWER
KF 05/20/22

PROJECT ID
102375

SHEET TOTAL
37 | 705

ITEM 614, WORK ZONE TRAFFIC SIGNAL

THE CONTRACTOR SHALL PROVIDE AND INSTALL ALL WOOD POLES, DOWN GUYS, CONDUIT RISER, WEATHERHEAD, METER BASE, DISCONNECT SWITCH, MESSENGER WIRE (WITH ACCESSORIES), SIGNAL HEADS, CONTROLLER, WIRING AND ALL OTHER NECESSARY ITEMS IN ACCORDANCE WITH PLAN DETAILS FOR A COMPLETE WORK ZONE INSTALLATION AT THE NORTH RAMP INTERSECTION. MAINTENANCE OF ALL WORK ZONE TRAFFIC CONTROL EQUIPMENT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR.

THE WORK ZONE SIGNALS ARE DESIGNED AND EXPECTED TO BE USED DURING EACH CONSTRUCTION STAGE.

DURING STAGE 1, THE CONTRACTOR SHALL:

INSTALL 45' TEMPORARY WOOD POLES AND APPURTENANCES AS SHOWN ON THE PLAN.

INSTALL 3/8" MESSENGER WIRE BETWEEN THE TEMPORARY WOOD POLES. MESSENGER WIRE ATTACHMENT HEIGHTS AT THE TEMPORARY WOOD POLES SHALL BE ADEQUATE TO PROVIDE A MINIMUM OF 16' CLEARANCE FOR SIGNAL HEADS WHEN INSTALLED.

INSTALL THE TEMPORARY CONTROLLER AND APPURTENANCES AT THE LOCATION SHOWN IN THE PLAN. REFER TO THE SCD TC-83.10 POWER SERVICE AND CONTROLLER MOUNTING ON WOOD POLES FOR MORE INFORMATION.

INSTALL THE TEMPORARY WORK ZONE SIGNAL HEADS AS SHOWN ON THE PLAN. COVER THE SIGNAL HEADS UNTIL NEEDED.

INSTALL THE SIGNAL CABLE FOR THE WORK ZONE SIGNAL HEADS IN ACCORDANCE WITH THE WIRING DIAGRAM. CABLE SHALL BE PROVIDED FROM THE HEAD TO THE CONTROLLER. COIL EXTRA WIRE FOR MOVING HEADS TO SERVE OTHER CONSTRUCTION STAGE PLACEMENTS.

PLACE THE WORK ZONE INSTALLATION INTO SERVICE.

THE CONTRACTOR IS RESPONSIBLE FOR REMOVING WORK ZONE INSTALLATION FROM SERVICE UPON COMPLETION OF THE PROJECT.

ALL MATERIALS FURNISHED BY THE CONTRACTOR SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND REMOVED FROM THE PROJECT SITE.

THE FOLLOWING QUANTITY HAS BEEN INCLUDED IN THE PLANS AND CARRIED TO THE GENERAL SUMMARY:

ITEM 614, WORK ZONE TRAFFIC SIGNAL 4 EACH

ITEM 615, PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN

PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A SHALL CONFORM TO C&MS 615 AND AS SPECIFIES HEREIN.

PAYMENT FOR ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN INCLUDES THE INSTALLATION, MAINTENANCE AND REMOVAL OF ALL FENCING, EARTHWORK, GUARDRAIL, SIDEWALK, AND ALL OTHER ITEMS AS NECESSARY TO PROVIDE A COMPLETE, FUNCTIONAL, AND SAFE INSTALLATION FOR PUBLIC USE.

PHASE	ALIGNMENT	STATION LIMITS		AREA (SY)
PREPHASE 1	TECHNOLOGY DR.	5+61	9+55	694
	SPEEDWAY DR.	6+09	9+65	482
PHASE 1 STEP 1	CR 99	73+10	78+75	492
		81+60	82+50	1,139
		81+45	82+40	37
		87+40	88+55	501
		87+55	89+00	41
PHASE 2	IR 75	1022+60	1025+40	182
		82+10	82+40	294
	CR 99	87+45	88+10	160
		1042+70	1043+25	12
		1045+90	1046+90	32

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE PLANS AND CARRIED TO THE GENERAL SUMMARY:

ITEM 615, PAVEMENT FOR MAINTANING TRAFFIC, CLASS A, AS PER PLAN
4,350 SY

ITEM 614, MAINTAINING TRAFFIC, CR 99 WESTBOUND BRIDGE CONSTRUCTION

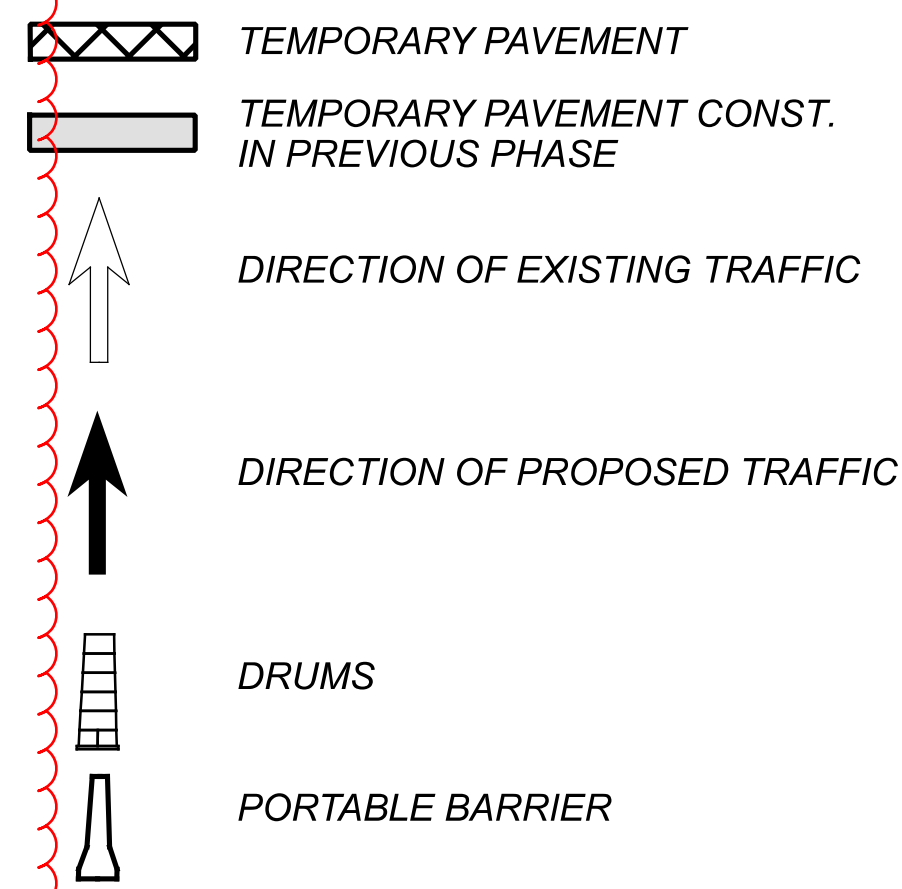
DURING THE CONSTRUCTION OF CR 99 WESTBOUND BRIDGE, LANES ON IR 75 MAY BE CLOSED FOR SHORT DURATIONS IN ACCORDANCE WITH THE FOLLOWING PHASING. THE CONSTRUCTION ACTIVITIES MAY INLCUDE THE ERECTION OF BRIDGE GIRDERS, DECK PLACEMENT, DECK REMOVAL, AND FALSEWORK. CONTRACTOR IS REQUIRED TO GET APPROVAL FROM THE PROJECT ENGINEER FOR CLOSURES AND COORDINATE WITH THE ODOT DISTRICT, COUNTY AND THE CITY OF FINDLAY.

FOR WORK, OVER THE IR 75 SB LANES, ALL IR 75 SB LANES MAY BE CLOSED. SB TRAFFIC WILL BE REDUCED TO ONE LANE IN ACCORDANCE WITH ODOT STANDARD DRAWINGS AND BE DIVERTED TO THE IR 75 SB EXIT RAMP TO CR 99 AND TO THE IR 75 SB ENTRANCE RAMP FROM CR 99. CR 99 WILL BE CLOSED TO THROUGH TRAFFIC IN EACH DIRECTION. TYPE III BARRICADES WILL BE INSTALLED, SIGNAL ADJUSTED TO AN ALL GREEN PHASE FOR IR 75 TRAFFIC. IR 75 NB LANES MAY BE REDUCED TO ONE LANE IN ACCORDANCE WITH ODOT STANDARD DRAWINGS AND SHIFTED TO THE EXISTING I-75 NB SHOULDER.

FOR WORK OVER THE IR 75 NB LANES, ALL IR 75 NB LANES MAY BE CLOSED. NB TRAFFIC WILL BE REDUCED TO ONE LANE IN ACCORDANCE WITH ODOT STANDARD DRAWINGS AND BE DIVERTED TO THE IR 75 NB EXIT RAMP TO CR 99, THE PROCEED THROUGH THE INTERSECTION WITH CR 99 AND TO THE IR 75 NB ENTRANCE RAMP FROM CR 99. CR 99 WILL BE CLOSED TO THROUGH TRAFFIC IN EACH DIRECTION. TYPE III BARRICADES WILL BE INTALLED, SIGNAL ADJUSTED TO AN ALL GREEN PHASE FOR IR 75 TRAFFIC. IR 75 SB LANES MAY BE REDUCED TO ONE LANE IN ACCORDANCE WITH ODOT STANDARD DRAWINGS AND SHIFTED TO THE EXISTING IR 75 SB SHOULDER.

THE LIMITATIONS FOR THIS SHORT DURATION CLOSURE SHALL BE IN ACCORDANCE WITH THE PERMITTED LANE CLOSURE SCHEDULE (PLCS). THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE AS DESIGNATED IN THE IR 75 CLOSURE TABLE FOR EACH MINUTE THAT IR 75 REMAINS CLOSED TO TRAFFIC BEYOND THE SPECIFIED PERIOD.

MAINTENANCE OF TRAFFIC TYPICAL PLANS LEGEND



DESIGN AGENCY



DESIGNER

CO

REVIEWER

KF 05/20/22

PROJECT ID

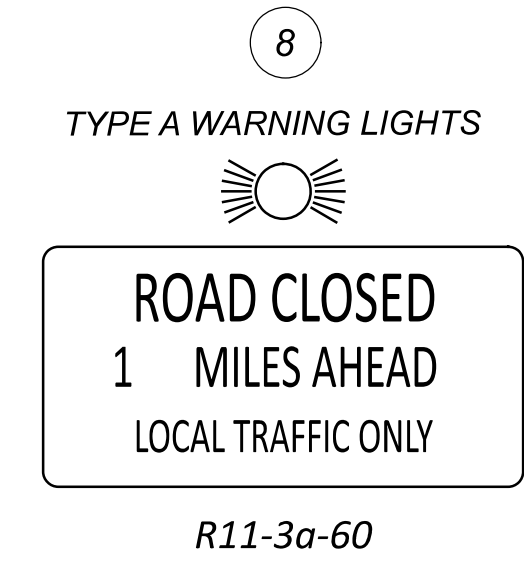
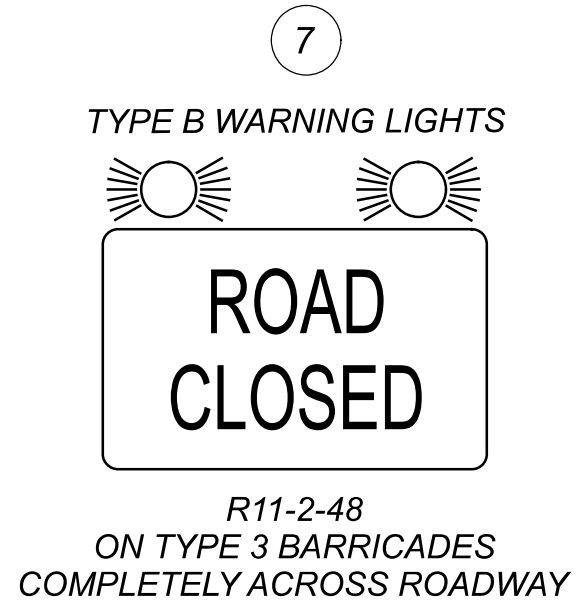
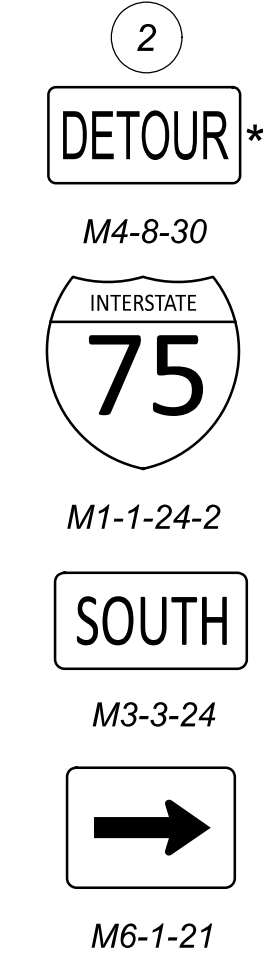
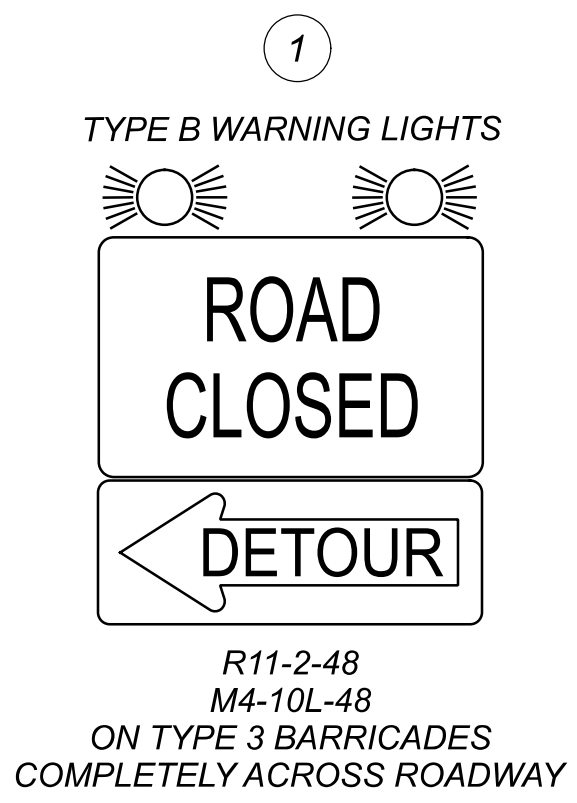
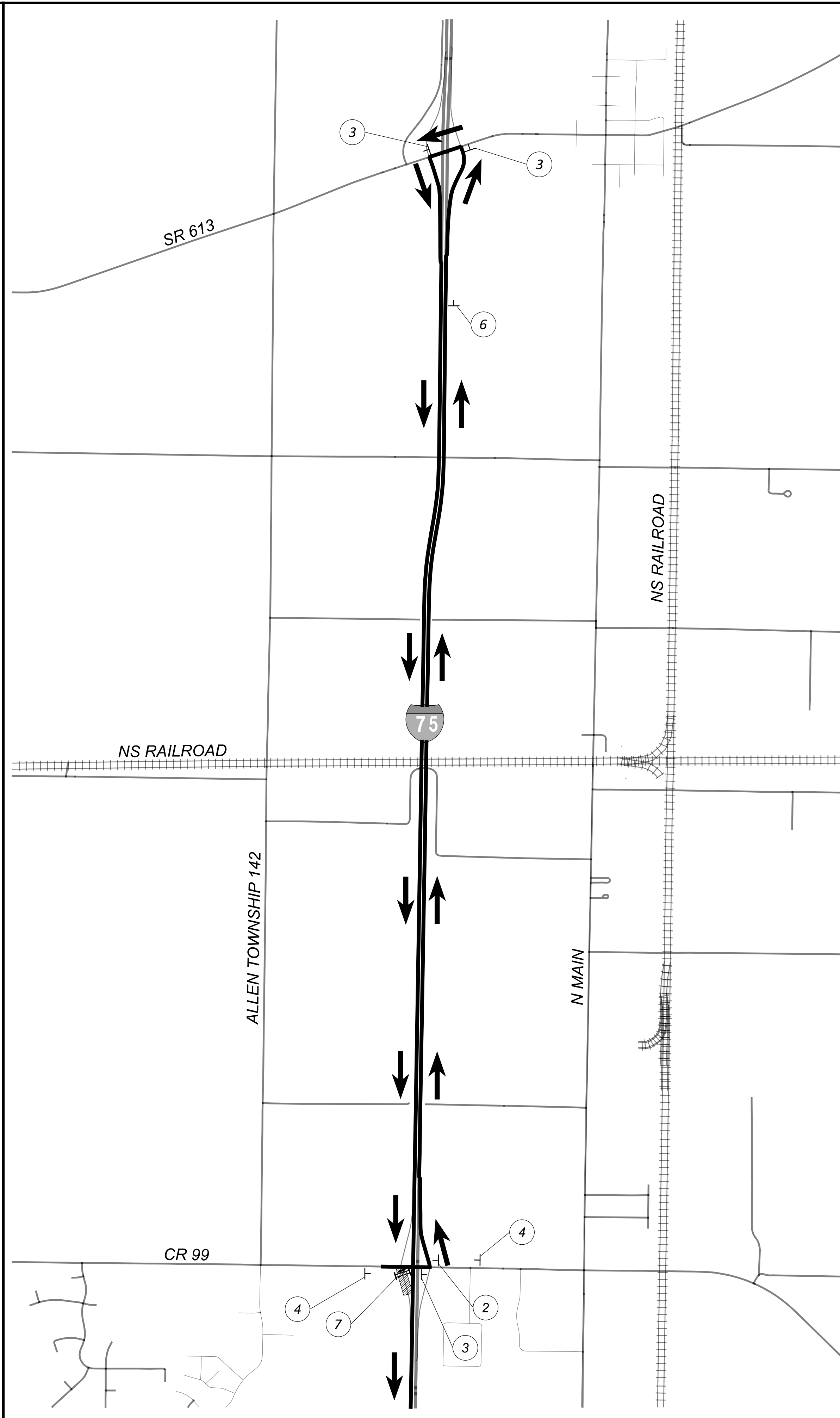
102375

SHEET

40

TOTAL

705



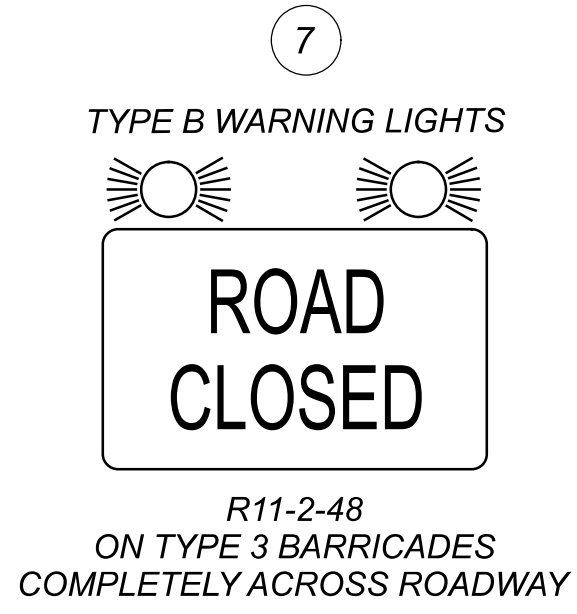
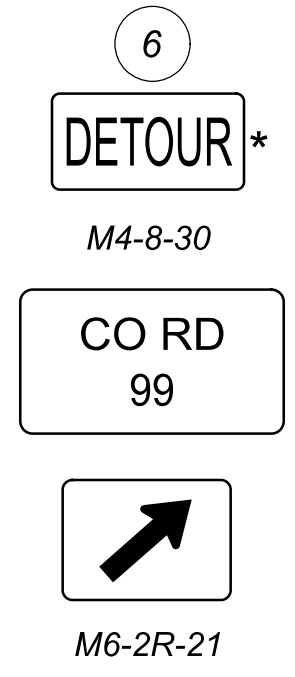
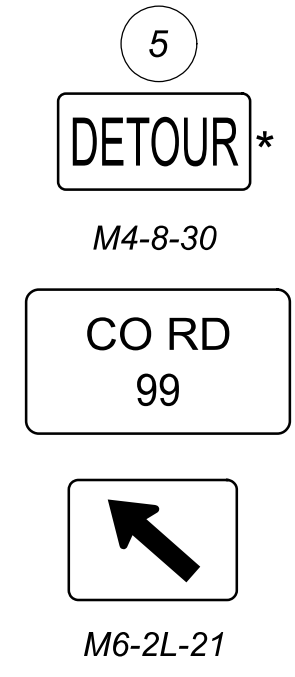
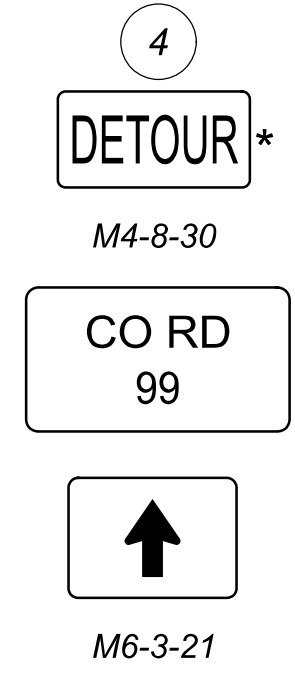
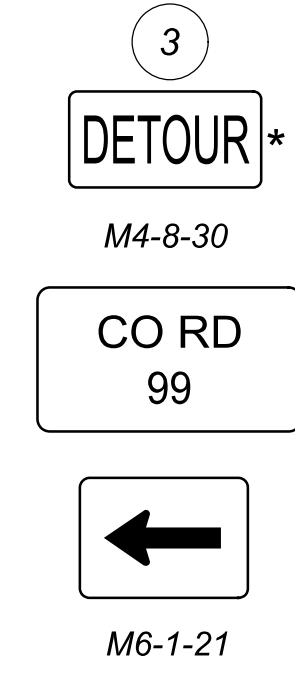
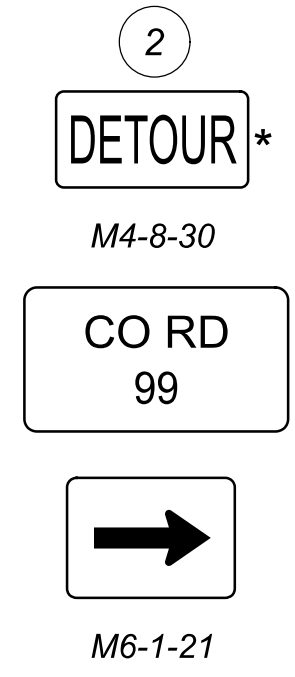
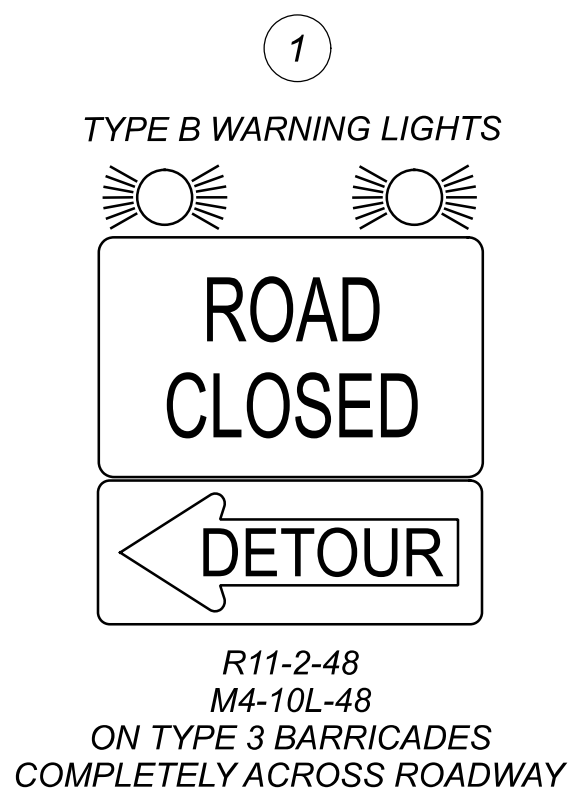
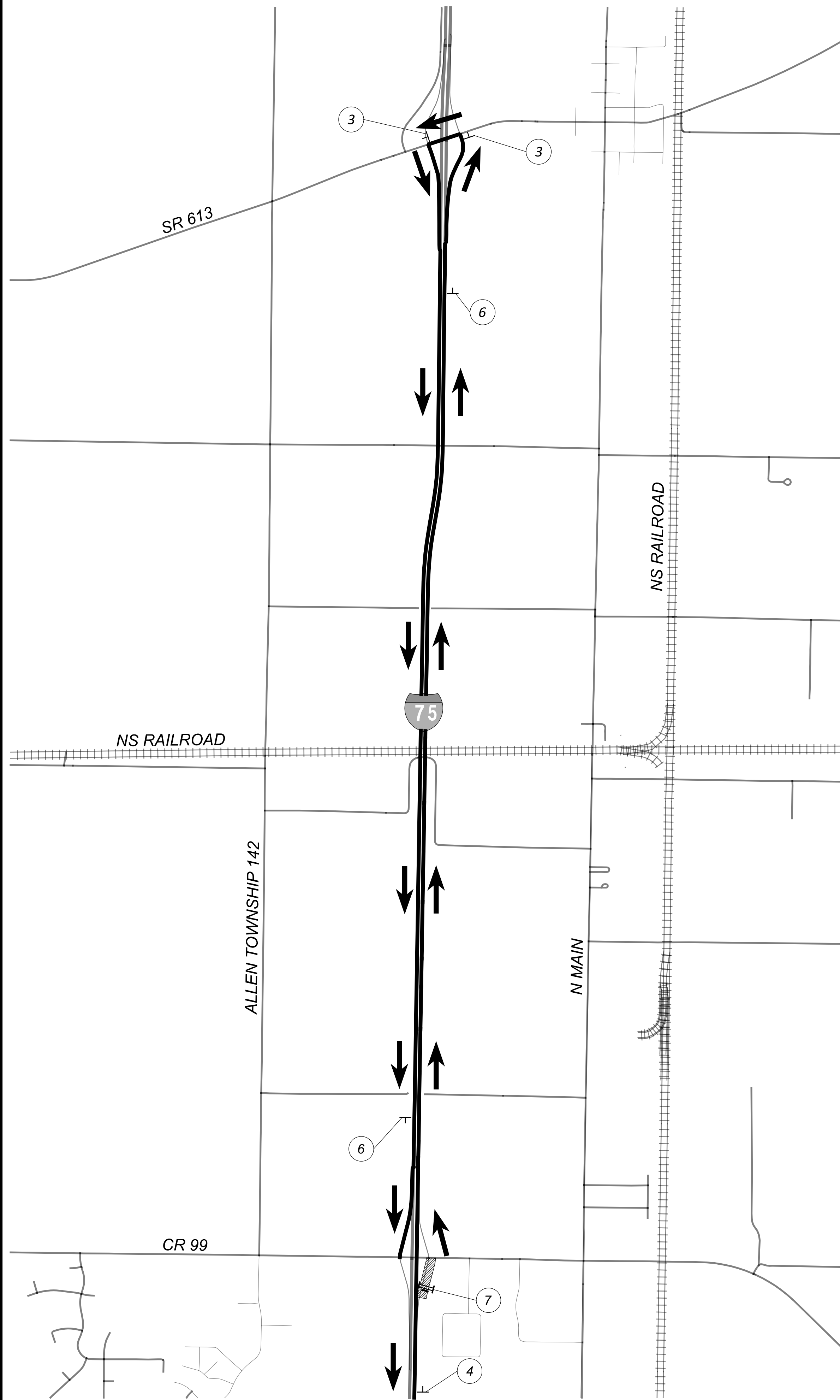
LEGEND

- PROJECT WORK AREA
- DETOUR ROUTE
- TYPE III BARRICADE
- * DENOTES BLACK ON ORANGE SIGN
- PCMS PORTABLE CHANGABLE MESSAGE SIGN

NOTES:

RAMP CLOSURES WILL BE PERMITTED FOR 3 DAYS FOR PIPE INSTALLATION OR FOR AT THE DIRECTION OF THE ENGINEER. THE RAMP CLOSURES WILL BE NON-CONCURRENT AND MUST BE COORDINATED WITH THE CONCRETE REPAIR WORK SCHEDULED FOR SR 613.

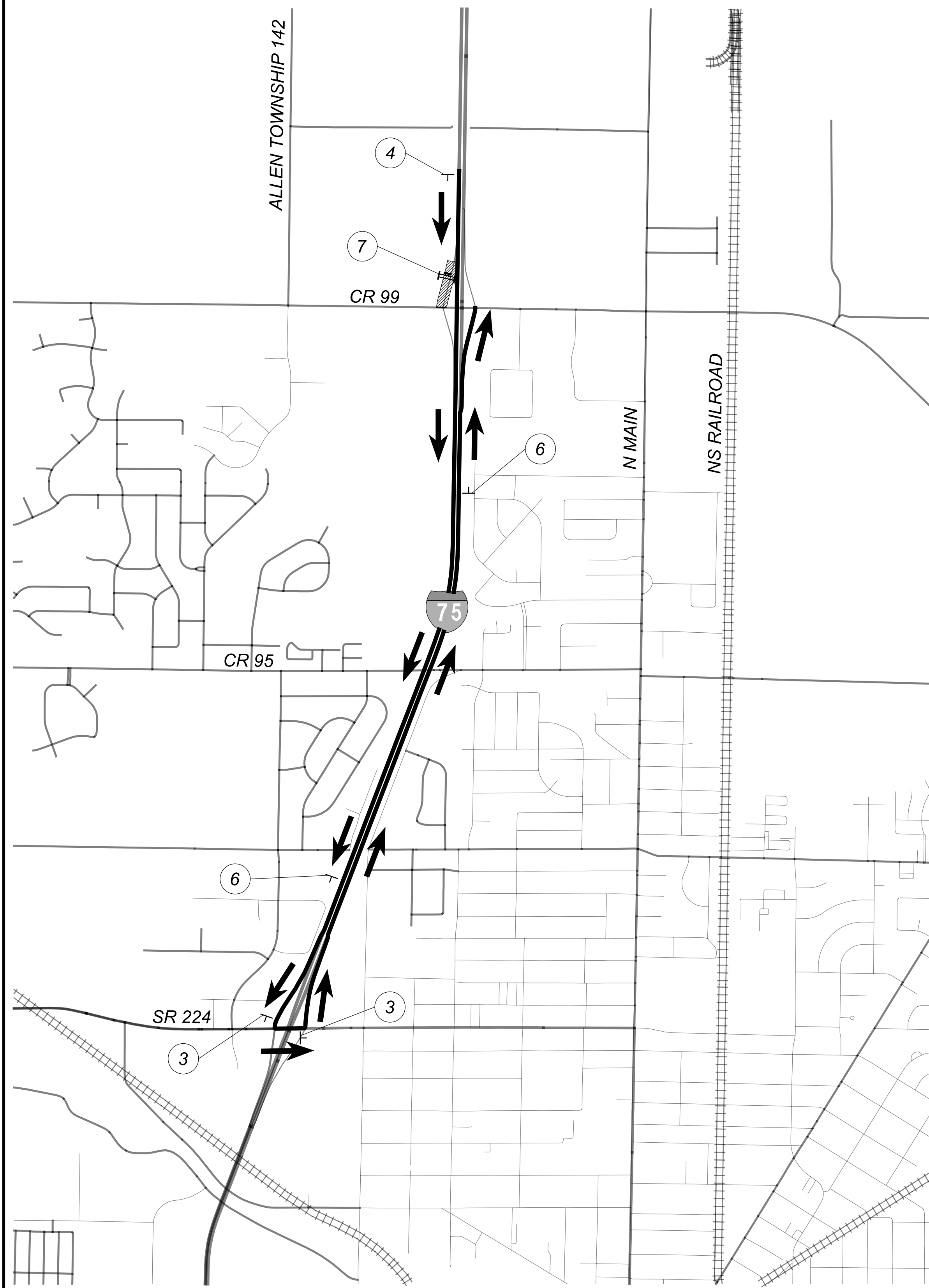




- LEGEND**
- PROJECT WORK AREA
 - DETOUR ROUTE
 - TYPE III BARRICADE
 - * DENOTES BLACK ON ORANGE SIGN
 - PCMS PORTABLE CHANGABLE MESSAGE SIGN

NOTES:
 RAMP CLOSURES WILL BE PERMITTED FOR 3 DAYS FOR PIPE INSTALLATION OR FOR AT THE DIRECTION OF THE ENGINEER. THE RAMP CLOSURES WILL BE NON-CONCURRENT AND MUST BE COORDINATED WITH THE CONCRETE REPAIR WORK SCHEDULED FOR SR 613.

DESIGN AGENCY	
DESIGNER	CO
REVIEWER	KF 02/12/24
PROJECT ID	102375
SHEET	TOTAL
48C	705



- LEGEND**
- PROJECT WORK AREA
 - DETOUR ROUTE
 - TYPE III BARRICADE
 - | * DENOTES BLACK ON ORANGE SIGN
 - PORTABLE CHANGABLE MESSAGE SIGN

①
 TYPE B WARNING LIGHTS

ROAD CLOSED

 R11-2-48
 M4-10L-48
 ON TYPE 3 BARRICADES
 COMPLETELY ACROSS ROADWAY

②
DETOUR*
 M4-8-30
CO RD 99

 M6-1-21

③
DETOUR*
 M4-8-30
CO RD 99

 M6-1-21

④
DETOUR*
 M4-8-30
CO RD 99

 M6-3-21

⑤
DETOUR*
 M4-8-30
CO RD 99

 M6-2L-21

⑥
DETOUR*
 M4-8-30
CO RD 99

 M6-2R-21

⑦
 TYPE B WARNING LIGHTS

ROAD CLOSED
 R11-2-48
 ON TYPE 3 BARRICADES
 COMPLETELY ACROSS ROADWAY

⑧
 TYPE A WARNING LIGHTS

ROAD CLOSED
 1 MILES AHEAD
 LOCAL TRAFFIC ONLY
 R11-3a-60

NOTES:

RAMP CLOSURES WILL BE PERMITTED FOR 3 DAYS FOR PIPE INSTALLATION OR FOR AT THE DIRECTION OF THE ENGINEER. THE RAMP CLOSURES WILL BE NON-CONCURRENT AND MUST BE COORDINATED WITH THE CONCRETE REPAIR WORK SCHEDULED FOR SR 613.

DESIGN AGENCY

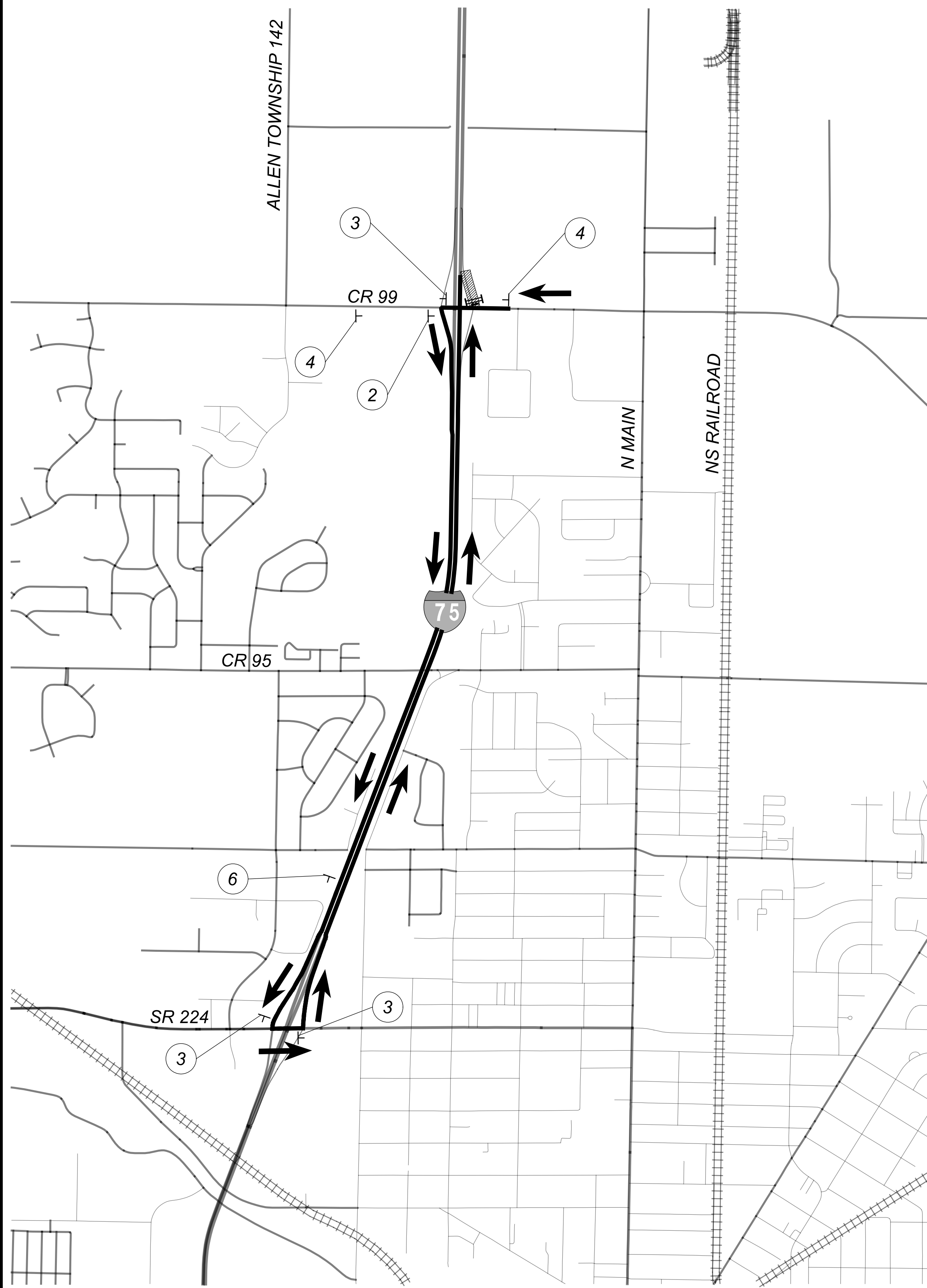


DESIGNER
CO

REVIEWER
KF 02/12/24

PROJECT ID
102375

SHEET TOTAL
48D | 705



- LEGEND**
- PROJECT WORK AREA
 - DETOUR ROUTE
 - TYPE III BARRICADE
 - | * DENOTES BLACK ON ORANGE SIGN
 - PCMS PORTABLE CHANGABLE MESSAGE SIGN

①
 TYPE B WARNING LIGHTS

 ROAD CLOSED

 DETOUR
 R11-2-48
 M4-10L-48
 ON TYPE 3 BARRICADES
 COMPLETELY ACROSS ROADWAY

⑦
 TYPE B WARNING LIGHTS

 ROAD CLOSED
 R11-2-48
 ON TYPE 3 BARRICADES
 COMPLETELY ACROSS ROADWAY

②
 DETOUR*
 M4-8-30

 INTERSTATE 75
 M1-1-24-2
 NORTH
 M3-1-24

 M6-1-21

③
 DETOUR*
 M4-8-30

 INTERSTATE 75
 M1-1-24-2
 NORTH
 M3-1-24

 M6-1-21

④
 DETOUR*
 M4-8-30

 INTERSTATE 75
 M1-1-24-2
 NORTH
 M3-1-24

 M6-3-21

⑤
 DETOUR*
 M4-8-30

 INTERSTATE 75
 M1-1-24-2
 NORTH
 M3-1-24

 M6-2L-21

⑥
 DETOUR*
 M4-8-30

 INTERSTATE 75
 M1-1-24-2
 NORTH
 M3-3-24

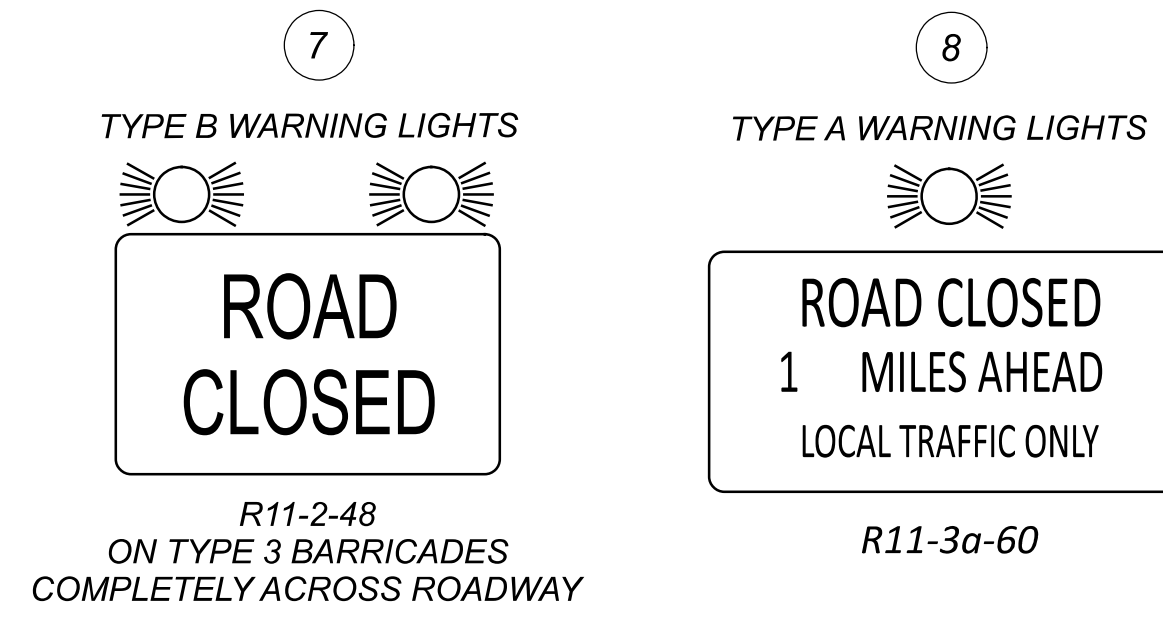
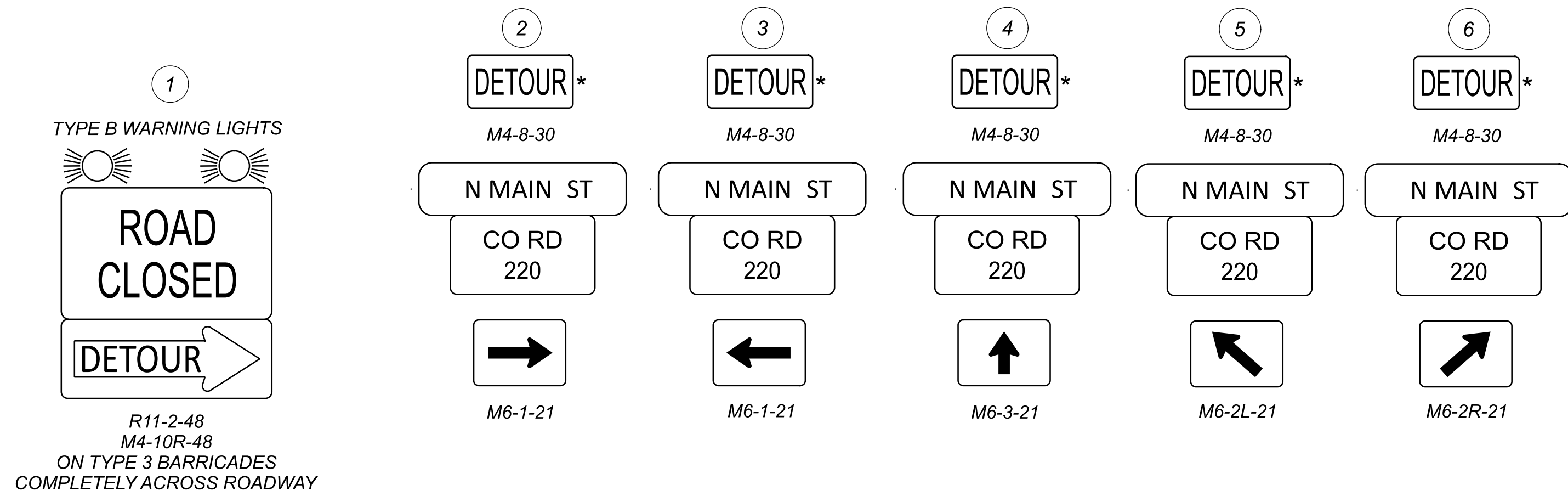
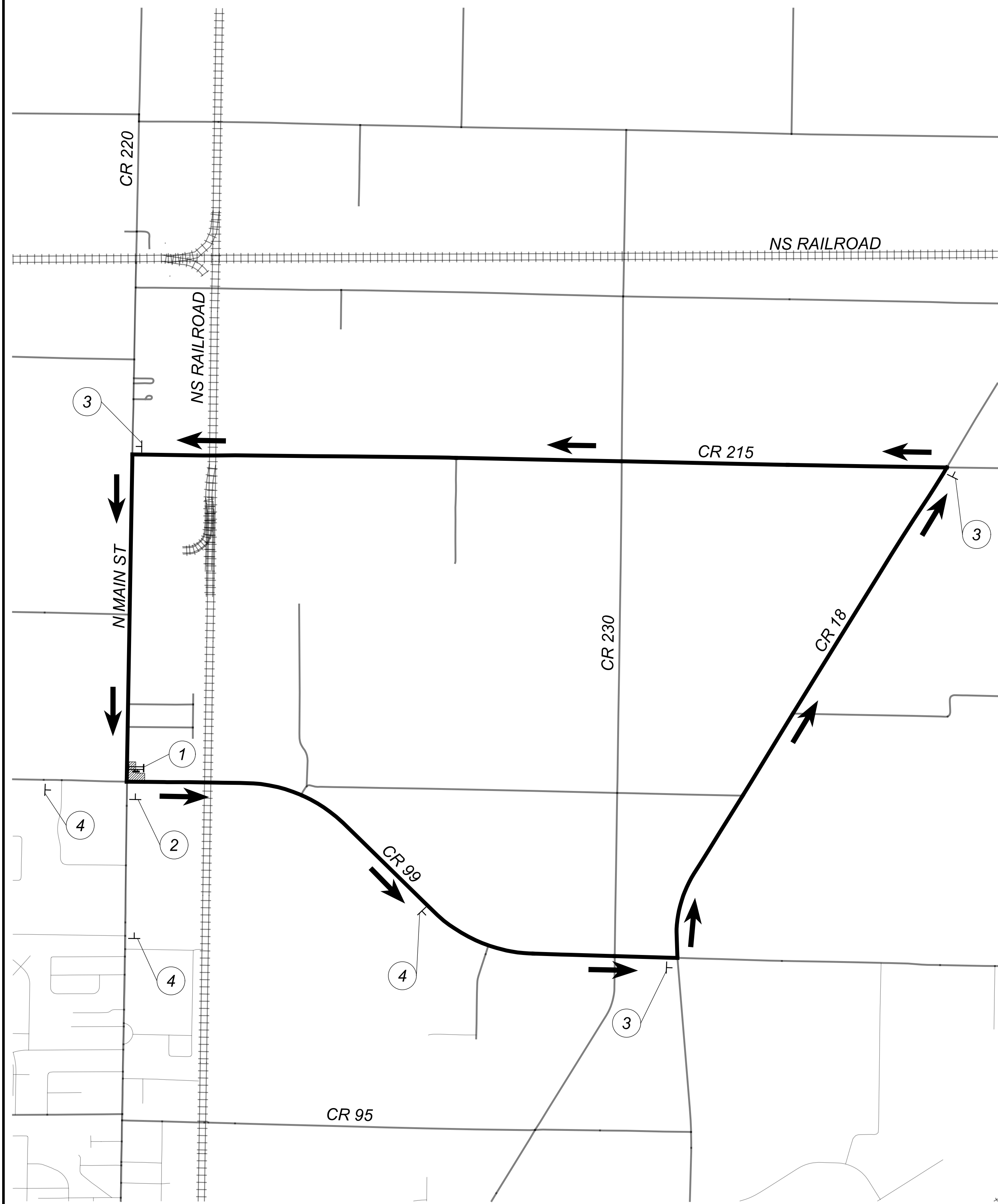
 M6-2R-21

⑧
 TYPE A WARNING LIGHTS

 ROAD CLOSED
 1 MILES AHEAD
 LOCAL TRAFFIC ONLY
 R11-3a-60

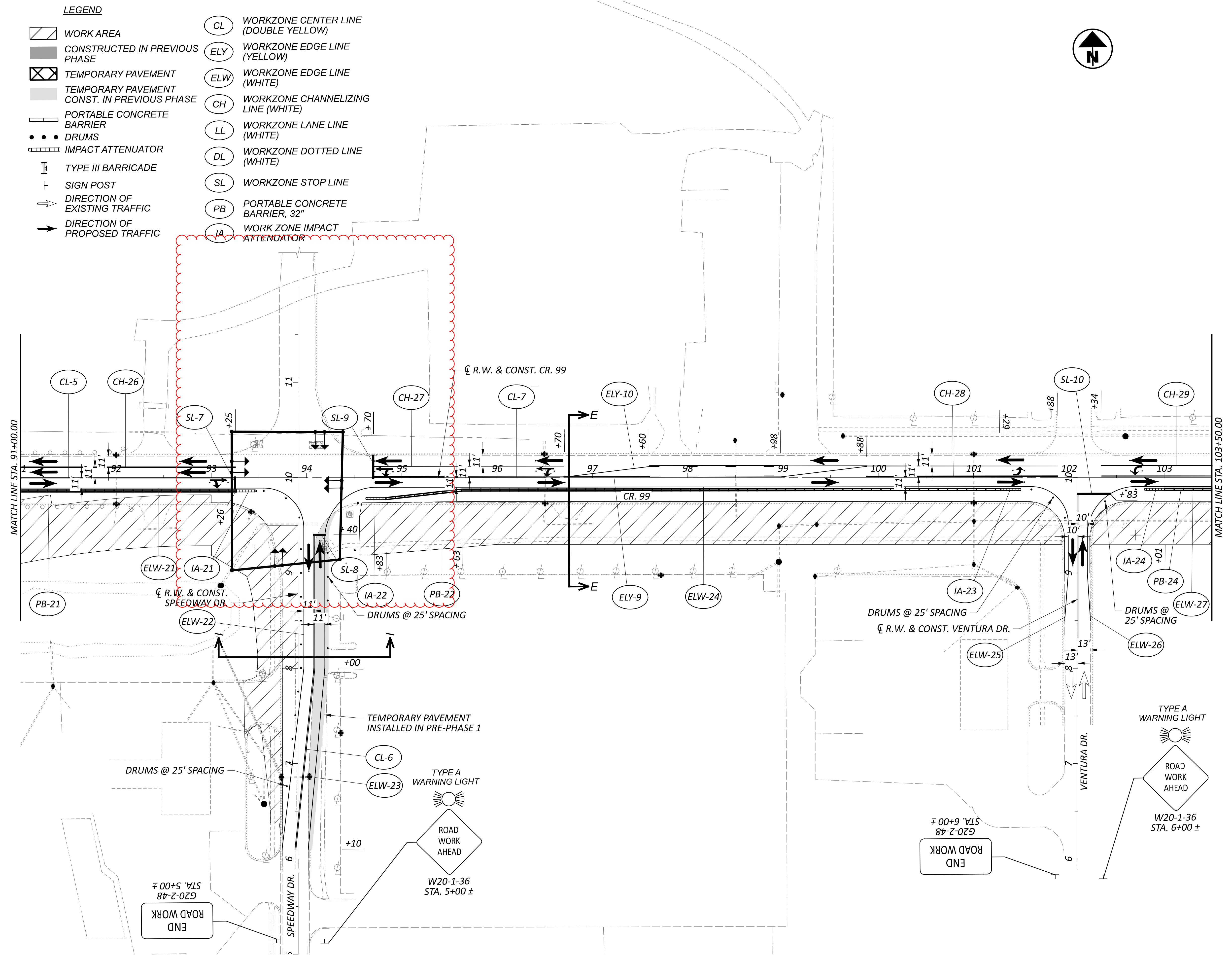
NOTES:
 RAMP CLOSURES WILL BE PERMITTED FOR 3 DAYS FOR PIPE INSTALLATION OR FOR AT THE DIRECTION OF THE ENGINEER. THE RAMP CLOSURES WILL BE NON-CONCURRENT AND MUST BE COORDINATED WITH THE CONCRETE REPAIR WORK SCHEDULED FOR SR 613.

DESIGN AGENCY	
DESIGNER	CO
REVIEWER	KF 02/12/24
PROJECT ID	102375
SHEET	TOTAL
48E	705



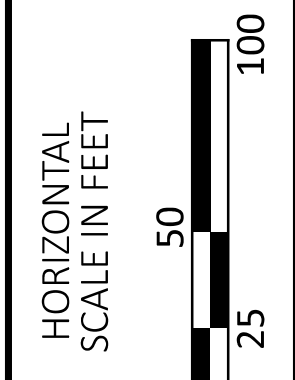
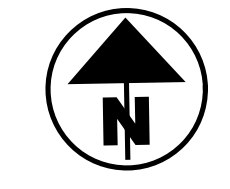
- LEGEND**
- PROJECT WORK AREA
 - DETOUR ROUTE
 - TYPE III BARRICADE
 - + * DENOTES BLACK ON ORANGE SIGN
 - PCMS PORTABLE CHANGABLE MESSAGE SIGN

NOTES:
 N MAIN ST. CLOSURE WILL BE PERMITTED FOR MAXIMUM OF 5 DAYS FOR THE 42" STORM SEWER AND ANY RELATED WORK OR FOR AT THE DIRECTION OF THE ENGINEER.



LEGEND

- WORK AREA
- CONSTRUCTED IN PREVIOUS PHASE
- TEMPORARY PAVEMENT
- TEMPORARY PAVEMENT CONST. IN PREVIOUS PHASE
- PORTABLE CONCRETE BARRIER
- DRUMS
- IMPACT ATTENUATOR
- TYPE III BARRICADE
- SIGN POST
- DIRECTION OF EXISTING TRAFFIC
- DIRECTION OF PROPOSED TRAFFIC
- WORKZONE CENTER LINE (DOUBLE YELLOW)
- WORKZONE EDGE LINE (YELLOW)
- WORKZONE EDGE LINE (WHITE)
- WORKZONE CHANNELIZING LINE (WHITE)
- WORKZONE LANE LINE (WHITE)
- WORKZONE DOTTED LINE (WHITE)
- WORKZONE STOP LINE
- PORTABLE CONCRETE BARRIER, 32"
- WORK ZONE IMPACT ATTENUATOR



**MAINTENANCE OF TRAFFIC
 PHASE 1 STEP 1 CR 99**

DESIGN AGENCY



DESIGNER
 CO

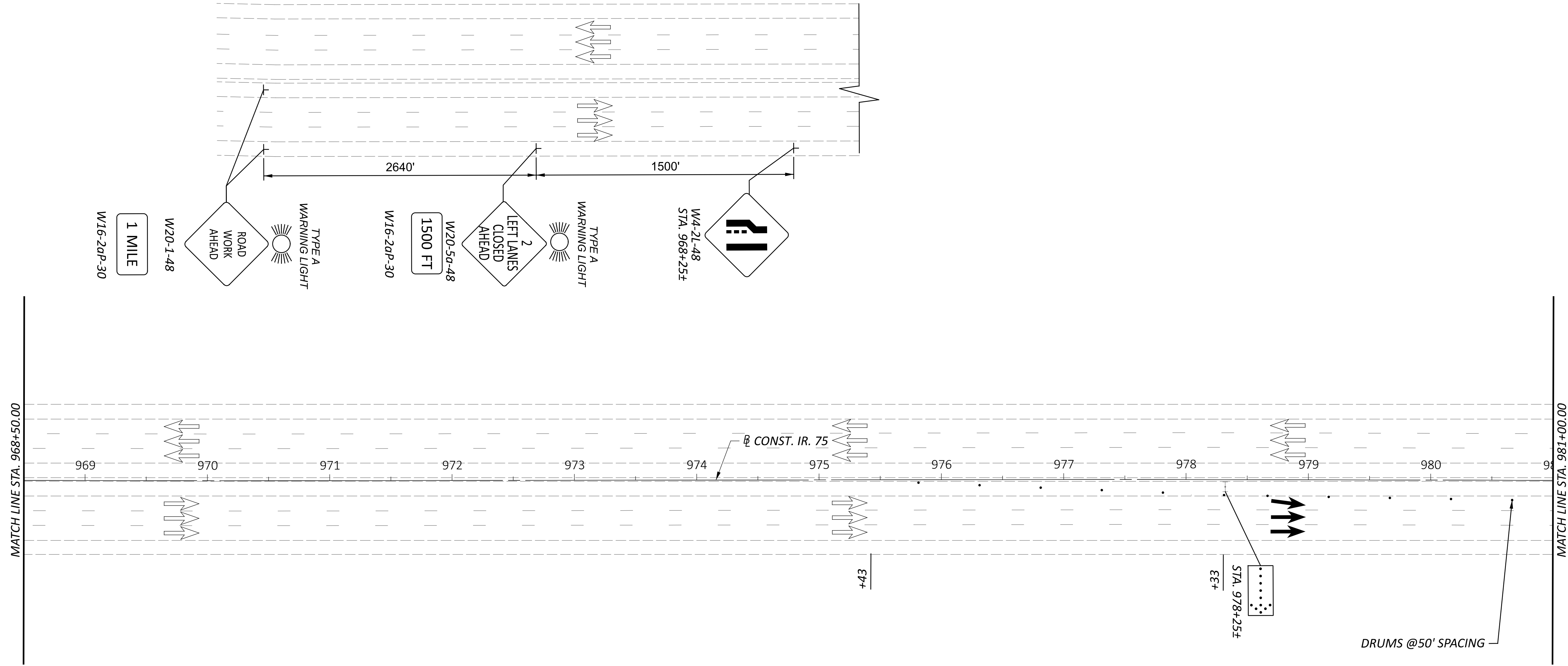
REVIEWER
 KF 05/20/22

PROJECT ID
 102375

SHEET TOTAL
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HAN-75/CR99 INTERCHANGE REHAB

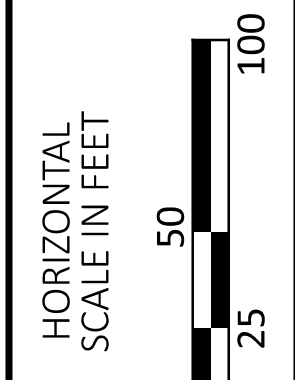
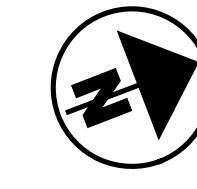
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LEGEND

- | | |
|---|--------------------------------------|
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| CONSTRUCTED IN PREVIOUS PHASE | WORKZONE EDGE LINE (YELLOW) |
| TEMPORARY PAVEMENT | WORKZONE EDGE LINE (WHITE) |
| TEMPORARY PAVEMENT CONST. IN PREVIOUS PHASE | WORKZONE CHANNELIZING LINE (WHITE) |
| PORTABLE CONCRETE BARRIER | WORKZONE LANE LINE (WHITE) |
| DRUMS | WORKZONE DOTTED LINE (WHITE) |
| IMPACT ATTENUATOR | WORKZONE STOP LINE |
| TYPE III BARRICADE | PORTABLE CONCRETE BARRIER, 32" |
| SIGN POST | WORK ZONE IMPACT ATTENUATOR |
| DIRECTION OF EXISTING TRAFFIC | |
| DIRECTION OF PROPOSED TRAFFIC | |

DRUMS @ 50' SPACING



**MAINTENANCE OF TRAFFIC
 CLOSURE OF NORTHBOUND IR 75**

DESIGN AGENCY



DESIGNER

CO

REVIEWER

KF 02/12/24

PROJECT ID

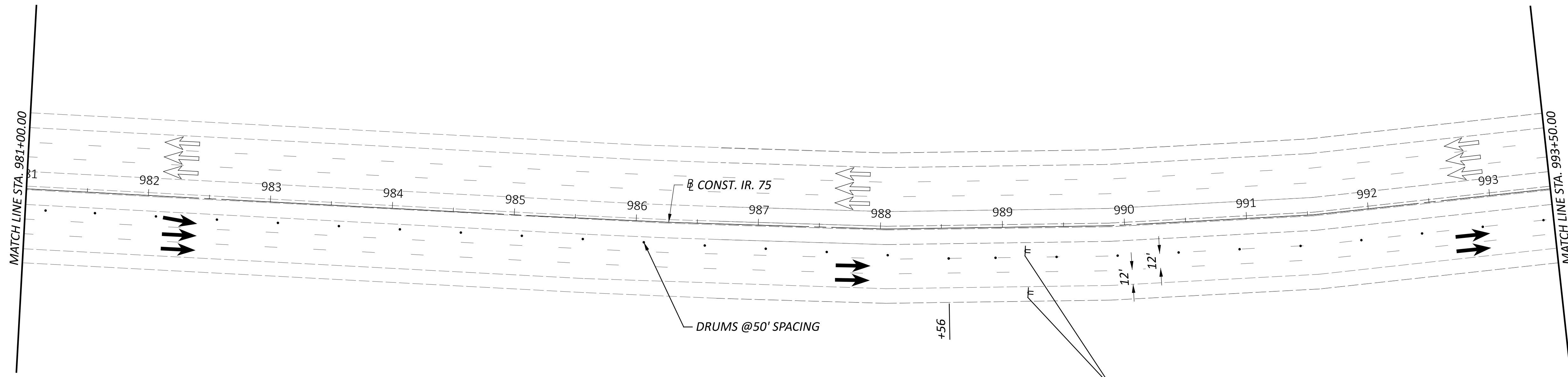
102375

SHEET

73A

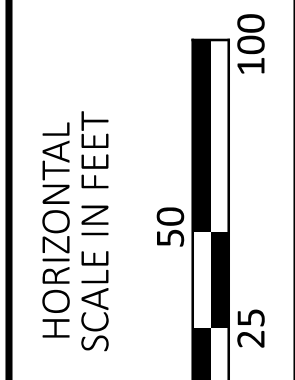
TOTAL

705



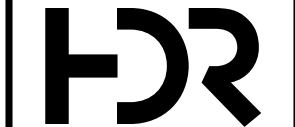
LEGEND

- | | | | |
|--|---|--|--------------------------------------|
| | WORK AREA | | WORKZONE CENTER LINE (DOUBLE YELLOW) |
| | CONSTRUCTED IN PREVIOUS PHASE | | WORKZONE EDGE LINE (YELLOW) |
| | TEMPORARY PAVEMENT | | WORKZONE EDGE LINE (WHITE) |
| | TEMPORARY PAVEMENT CONST. IN PREVIOUS PHASE | | WORKZONE CHANNELIZING LINE (WHITE) |
| | PORTABLE CONCRETE BARRIER | | WORKZONE LANE LINE (WHITE) |
| | DRUMS | | WORKZONE DOTTED LINE (WHITE) |
| | IMPACT ATTENUATOR | | WORKZONE STOP LINE |
| | TYPE III BARRICADE | | PORTABLE CONCRETE BARRIER, 32" |
| | SIGN POST | | WORK ZONE IMPACT ATTENUATOR |
| | DIRECTION OF EXISTING TRAFFIC | | |
| | DIRECTION OF PROPOSED TRAFFIC | | |



MAINTENANCE OF TRAFFIC
 CLOSURE OF NORTHBOUND IR 75

DESIGN AGENCY

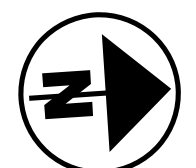
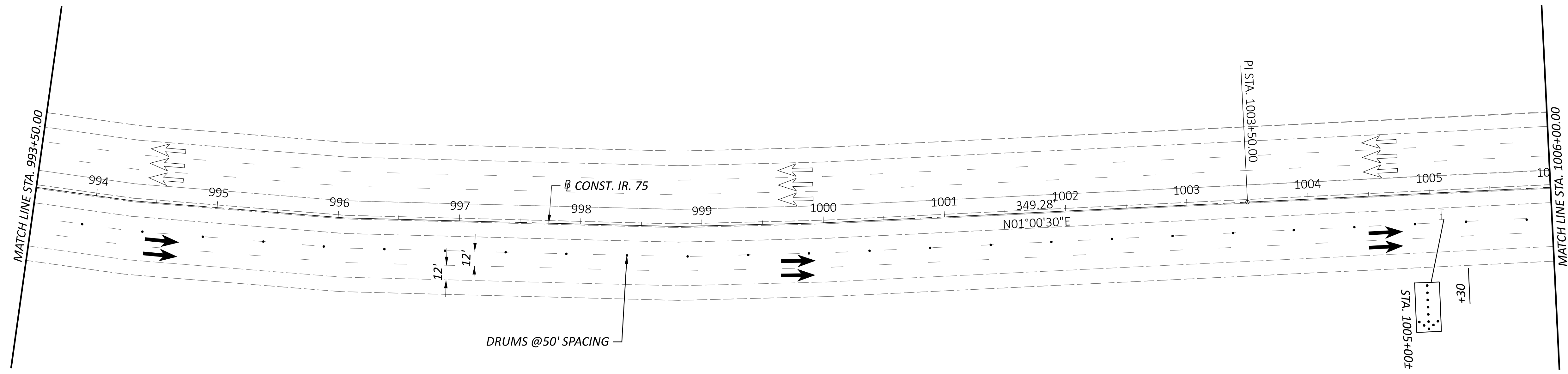


DESIGNER
 CO

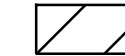









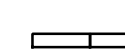



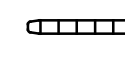




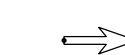
REVIEWER
 KF 02/12/24

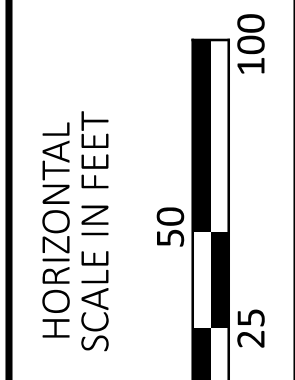
PROJECT ID
 102375

SHEET	TOTAL
73B	705



LEGEND

- | | |
|---|--|
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|  CONSTRUCTED IN PREVIOUS PHASE |  WORKZONE EDGE LINE (YELLOW) |
|  TEMPORARY PAVEMENT |  WORKZONE EDGE LINE (WHITE) |
|  TEMPORARY PAVEMENT CONST. IN PREVIOUS PHASE |  WORKZONE CHANNELIZING LINE (WHITE) |
|  PORTABLE CONCRETE BARRIER |  WORKZONE LANE LINE (WHITE) |
|  DRUMS |  WORKZONE DOTTED LINE (WHITE) |
|  IMPACT ATTENUATOR |  WORKZONE STOP LINE |
|  TYPE III BARRICADE |  PORTABLE CONCRETE BARRIER, 32" |
|  SIGN POST |  WORK ZONE IMPACT ATTENUATOR |
|  DIRECTION OF EXISTING TRAFFIC | |
|  DIRECTION OF PROPOSED TRAFFIC | |



**MAINTENANCE OF TRAFFIC
 CLOSURE OF NORTHBOUND IR 75**

DESIGN AGENCY



DESIGNER
 CO

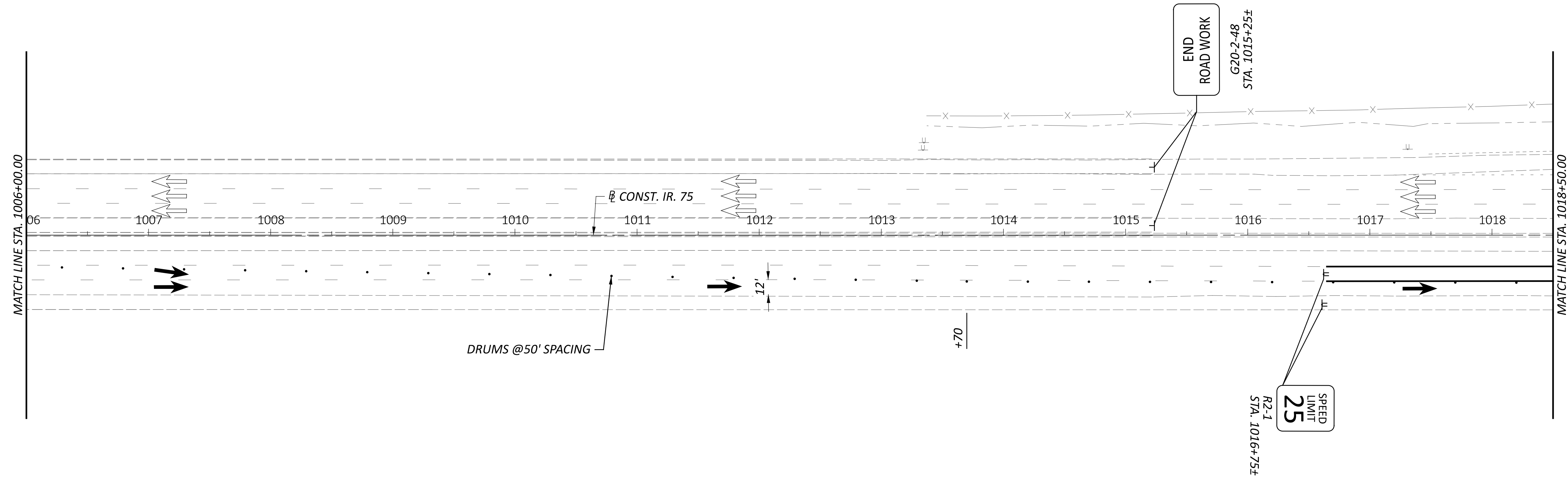
REVIEWER
 KF 02/12/24

PROJECT ID
 102375

SHEET TOTAL
 73C | 705

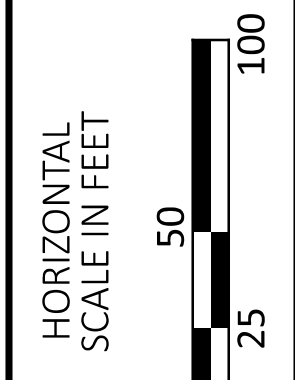
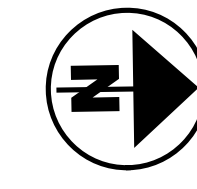
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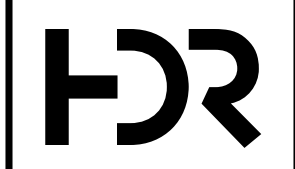
LEGEND

- | | | | |
|--|---|--|--------------------------------------|
| | WORK AREA | | WORKZONE CENTER LINE (DOUBLE YELLOW) |
| | CONSTRUCTED IN PREVIOUS PHASE | | WORKZONE EDGE LINE (YELLOW) |
| | TEMPORARY PAVEMENT | | WORKZONE EDGE LINE (WHITE) |
| | TEMPORARY PAVEMENT CONST. IN PREVIOUS PHASE | | WORKZONE CHANNELIZING LINE (WHITE) |
| | PORTABLE CONCRETE BARRIER | | WORKZONE LANE LINE (WHITE) |
| | DRUMS | | WORKZONE DOTTED LINE (WHITE) |
| | IMPACT ATTENUATOR | | WORKZONE STOP LINE |
| | TYPE III BARRICADE | | PORTABLE CONCRETE BARRIER, 32" |
| | SIGN POST | | WORK ZONE IMPACT ATTENUATOR |
| | DIRECTION OF EXISTING TRAFFIC | | |
| | DIRECTION OF PROPOSED TRAFFIC | | |



**MAINTENANCE OF TRAFFIC
 CLOSURE OF NORTHBOUND IR 75**

DESIGN AGENCY



DESIGNER
 CO

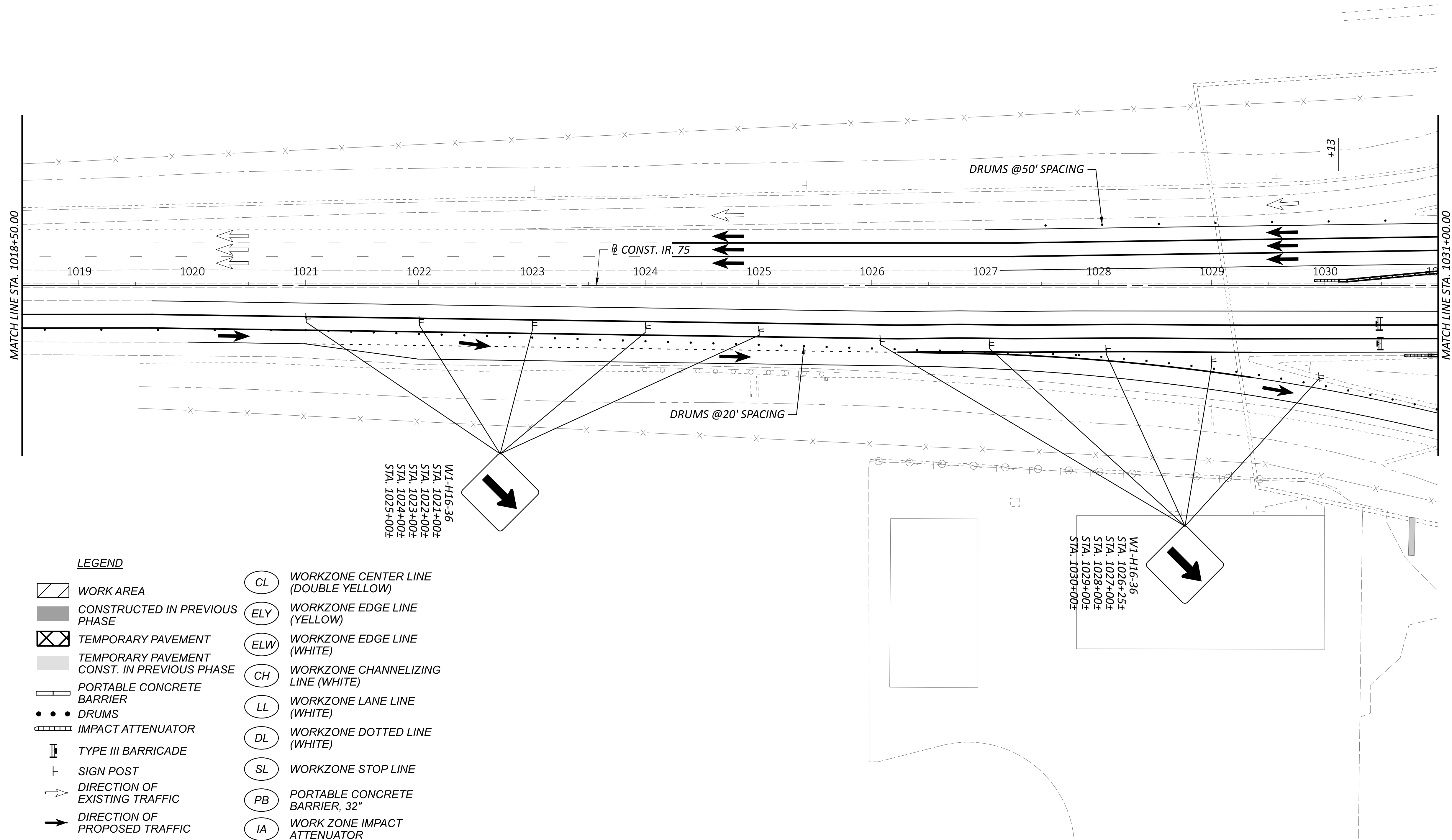
REVIEWER
 KF 02/12/24

PROJECT ID
 102375

SHEET TOTAL
 73D | 705

HAN-75/CR99 INTERCHANGE REHAB

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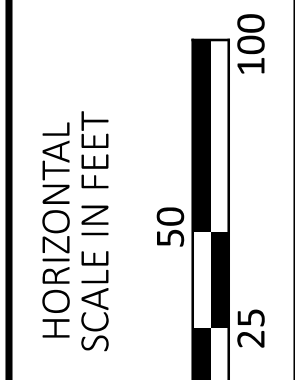


LEGEND

- | | | | |
|--|---|--|--------------------------------------|
| | WORK AREA | | WORKZONE CENTER LINE (DOUBLE YELLOW) |
| | CONSTRUCTED IN PREVIOUS PHASE | | WORKZONE EDGE LINE (YELLOW) |
| | TEMPORARY PAVEMENT | | WORKZONE EDGE LINE (WHITE) |
| | TEMPORARY PAVEMENT CONST. IN PREVIOUS PHASE | | WORKZONE CHANNELIZING LINE (WHITE) |
| | PORTABLE CONCRETE BARRIER | | WORKZONE LANE LINE (WHITE) |
| | DRUMS | | WORKZONE DOTTED LINE (WHITE) |
| | IMPACT ATTENUATOR | | WORKZONE STOP LINE |
| | TYPE III BARRICADE | | PORTABLE CONCRETE BARRIER, 32" |
| | SIGN POST | | WORK ZONE IMPACT ATTENUATOR |
| | DIRECTION OF EXISTING TRAFFIC | | |
| | DIRECTION OF PROPOSED TRAFFIC | | |

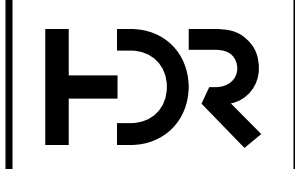
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 STA. 1025+00#

W1-H16-36
 STA. 1026+25#
 STA. 1027+00#
 STA. 1028+00#
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**MAINTENANCE OF TRAFFIC
 CLOSURE OF NORTHBOUND IR 75**

DESIGN AGENCY



DESIGNER
 CO

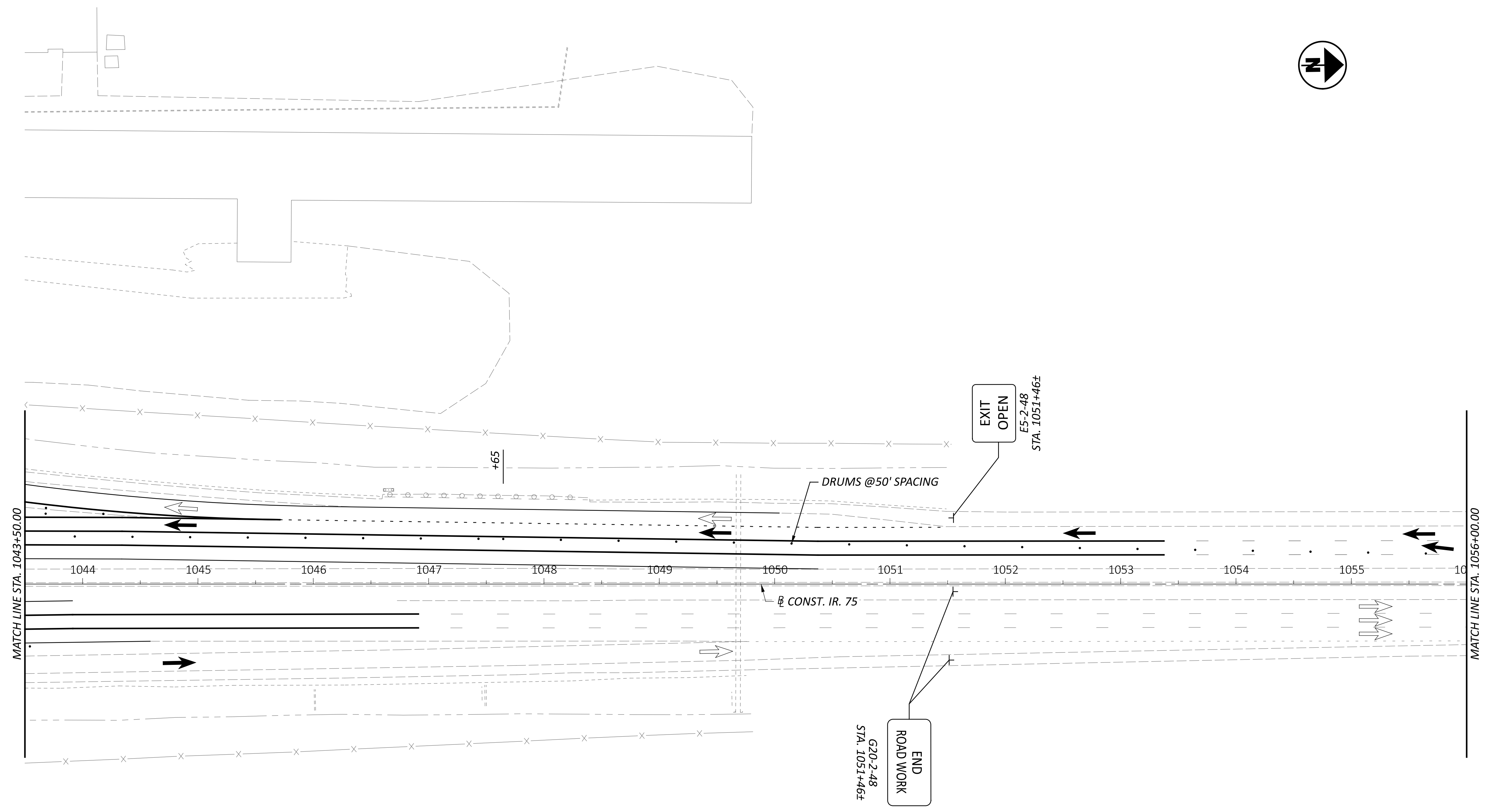
REVIEWER
 KF 02/12/24

PROJECT ID
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SHEET	TOTAL
73E	705

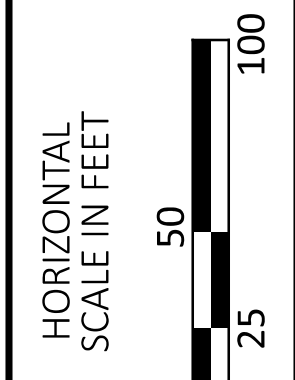
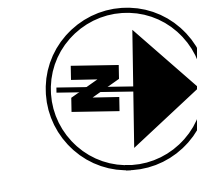
HAN-75/CR99 INTERCHANGE REHAB

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LEGEND

- | | | | |
|--|---|--|--------------------------------------|
| | WORK AREA | | WORKZONE CENTER LINE (DOUBLE YELLOW) |
| | CONSTRUCTED IN PREVIOUS PHASE | | WORKZONE EDGE LINE (YELLOW) |
| | TEMPORARY PAVEMENT | | WORKZONE EDGE LINE (WHITE) |
| | TEMPORARY PAVEMENT CONST. IN PREVIOUS PHASE | | WORKZONE CHANNELIZING LINE (WHITE) |
| | PORTABLE CONCRETE BARRIER | | WORKZONE LANE LINE (WHITE) |
| | DRUMS | | WORKZONE DOTTED LINE (WHITE) |
| | IMPACT ATTENUATOR | | WORKZONE STOP LINE |
| | TYPE III BARRICADE | | PORTABLE CONCRETE BARRIER, 32" |
| | SIGN POST | | WORK ZONE IMPACT ATTENUATOR |
| | DIRECTION OF EXISTING TRAFFIC | | |
| | DIRECTION OF PROPOSED TRAFFIC | | |



**MAINTENANCE OF TRAFFIC
 CLOSURE OF NORTHBOUND IR 75**

DESIGN AGENCY



DESIGNER
 CO

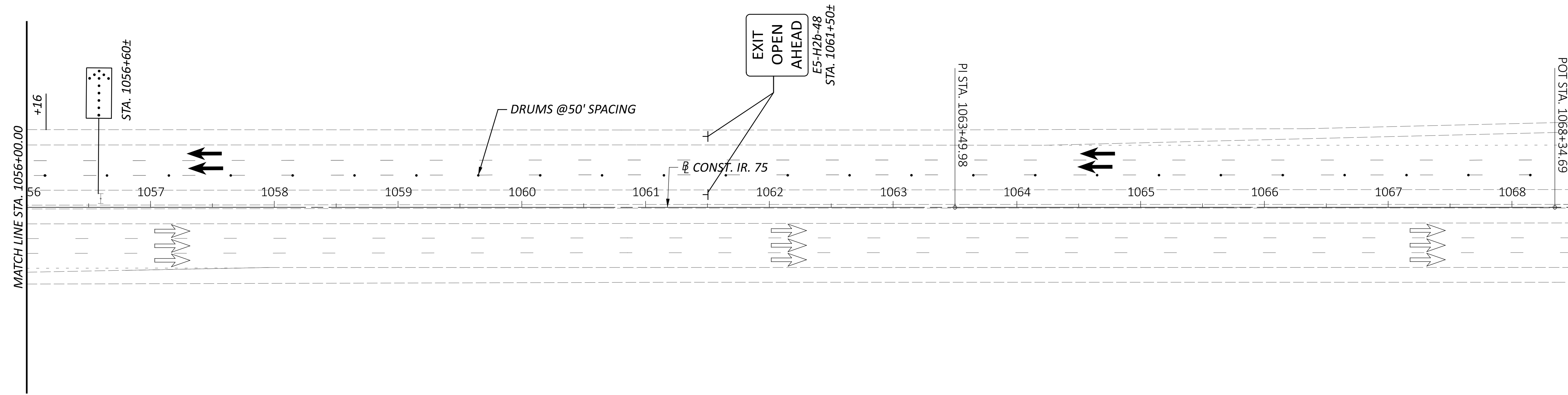
REVIEWER
 KF MM-DD-YY

PROJECT ID
 102375

SHEET TOTAL
 73G 705

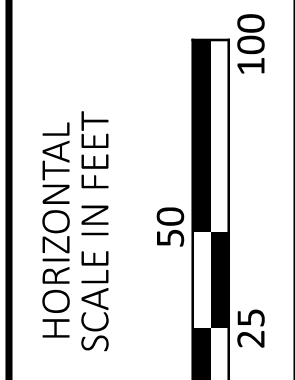
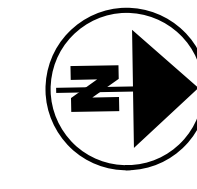
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LEGEND

- | | | | |
|--|---|--|--------------------------------------|
| | WORK AREA | | WORKZONE CENTER LINE (DOUBLE YELLOW) |
| | CONSTRUCTED IN PREVIOUS PHASE | | WORKZONE EDGE LINE (YELLOW) |
| | TEMPORARY PAVEMENT | | WORKZONE EDGE LINE (WHITE) |
| | TEMPORARY PAVEMENT CONST. IN PREVIOUS PHASE | | WORKZONE CHANNELIZING LINE (WHITE) |
| | PORTABLE CONCRETE BARRIER | | WORKZONE LANE LINE (WHITE) |
| | DRUMS | | WORKZONE DOTTED LINE (WHITE) |
| | IMPACT ATTENUATOR | | WORKZONE STOP LINE |
| | TYPE III BARRICADE | | PORTABLE CONCRETE BARRIER, 32" |
| | SIGN POST | | WORK ZONE IMPACT ATTENUATOR |
| | DIRECTION OF EXISTING TRAFFIC | | |
| | DIRECTION OF PROPOSED TRAFFIC | | |



**MAINTENANCE OF TRAFFIC
 CLOSURE OF NORTHBOUND IR 75**

DESIGN AGENCY



DESIGNER
 CO

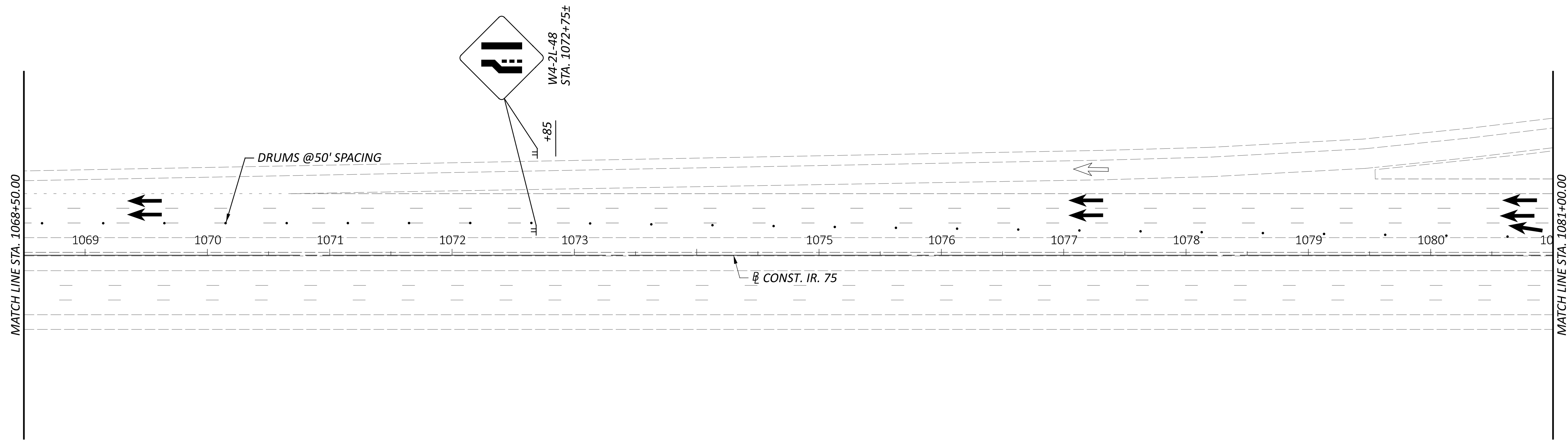
REVIEWER
 KF 02/12/24

PROJECT ID
 102375

SHEET TOTAL
 73H 705

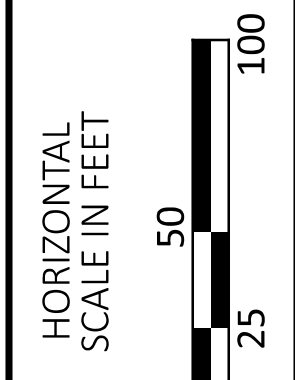
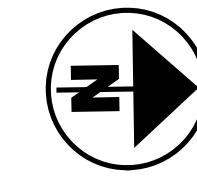
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LEGEND

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| | CONSTRUCTED IN PREVIOUS PHASE | | WORKZONE EDGE LINE (YELLOW) |
| | TEMPORARY PAVEMENT | | WORKZONE EDGE LINE (WHITE) |
| | TEMPORARY PAVEMENT CONST. IN PREVIOUS PHASE | | WORKZONE CHANNELIZING LINE (WHITE) |
| | PORTABLE CONCRETE BARRIER | | WORKZONE LANE LINE (WHITE) |
| | DRUMS | | WORKZONE DOTTED LINE (WHITE) |
| | IMPACT ATTENUATOR | | WORKZONE STOP LINE |
| | TYPE III BARRICADE | | PORTABLE CONCRETE BARRIER, 32" |
| | SIGN POST | | WORK ZONE IMPACT ATTENUATOR |
| | DIRECTION OF EXISTING TRAFFIC | | |
| | DIRECTION OF PROPOSED TRAFFIC | | |



**MAINTENANCE OF TRAFFIC
 CLOSURE OF NORTHBOUND IR 75**

DESIGN AGENCY



DESIGNER
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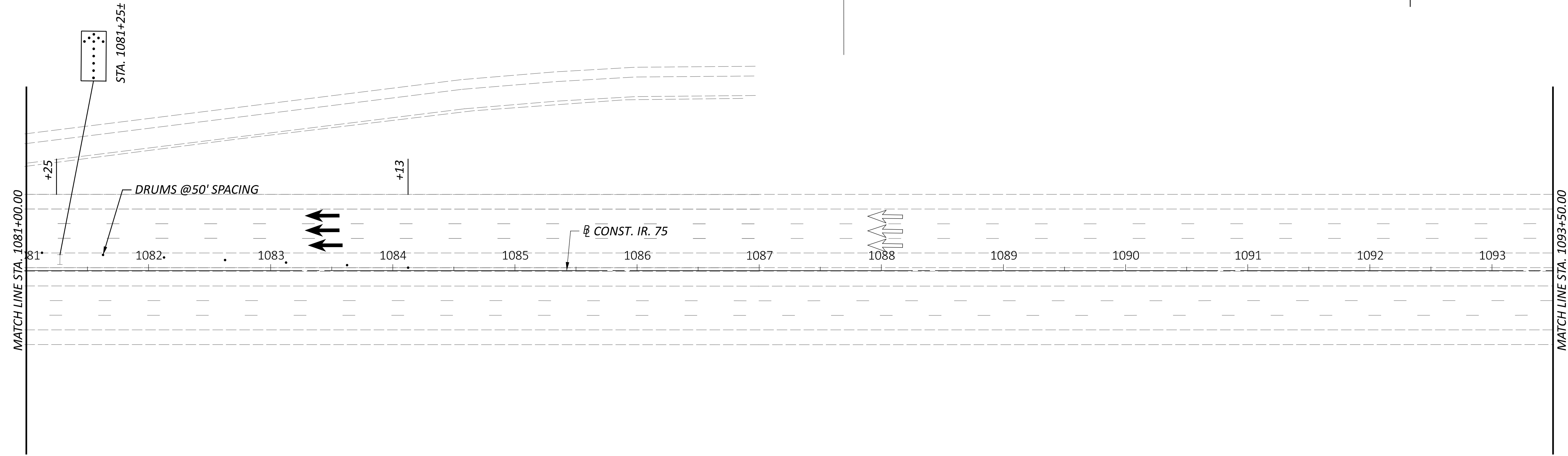
REVIEWER
 KF 02/12/24

PROJECT ID
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SHEET TOTAL
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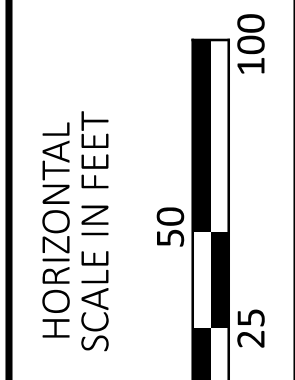
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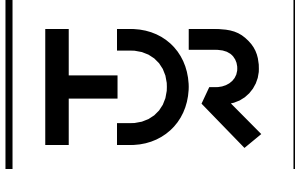
LEGEND

- | | |
|---|--------------------------------------|
| WORK AREA | WORKZONE CENTER LINE (DOUBLE YELLOW) |
| CONSTRUCTED IN PREVIOUS PHASE | WORKZONE EDGE LINE (YELLOW) |
| TEMPORARY PAVEMENT | WORKZONE EDGE LINE (WHITE) |
| TEMPORARY PAVEMENT CONST. IN PREVIOUS PHASE | WORKZONE CHANNELIZING LINE (WHITE) |
| PORTABLE CONCRETE BARRIER | WORKZONE LANE LINE (WHITE) |
| DRUMS | WORKZONE DOTTED LINE (WHITE) |
| IMPACT ATTENUATOR | WORKZONE STOP LINE |
| TYPE III BARRICADE | PORTABLE CONCRETE BARRIER, 32" |
| SIGN POST | WORK ZONE IMPACT ATTENUATOR |
| DIRECTION OF EXISTING TRAFFIC | |
| DIRECTION OF PROPOSED TRAFFIC | |



**MAINTENANCE OF TRAFFIC
 CLOSURE OF NORTHBOUND IR 75**

DESIGN AGENCY

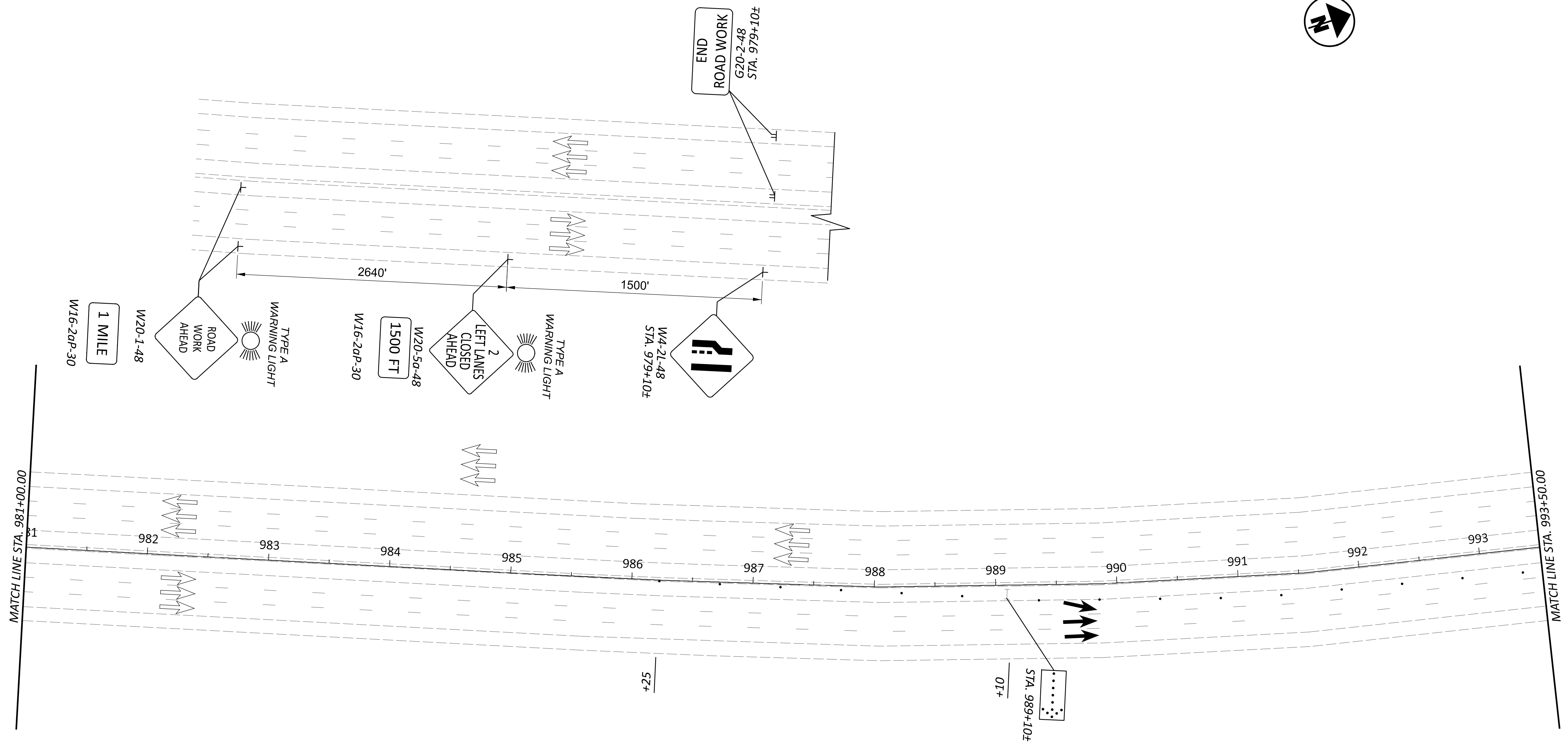


DESIGNER
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REVIEWER
 KF 02/12/24

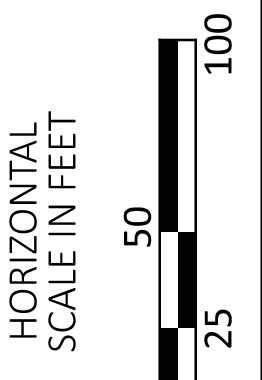
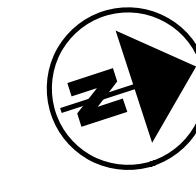
PROJECT ID
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SHEET TOTAL
 73J 705



LEGEND

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| | CONSTRUCTED IN PREVIOUS PHASE | | WORKZONE EDGE LINE (YELLOW) |
| | TEMPORARY PAVEMENT | | WORKZONE EDGE LINE (WHITE) |
| | TEMPORARY PAVEMENT CONST. IN PREVIOUS PHASE | | WORKZONE CHANNELIZING LINE (WHITE) |
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| | DRUMS | | WORKZONE DOTTED LINE (WHITE) |
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| | SIGN POST | | WORK ZONE IMPACT ATTENUATOR |
| | DIRECTION OF EXISTING TRAFFIC | | |
| | DIRECTION OF PROPOSED TRAFFIC | | |



MAINTENANCE OF TRAFFIC
 CLOSURE OF SOUTHBOUND IR 75

DESIGN AGENCY

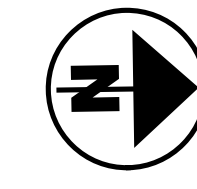
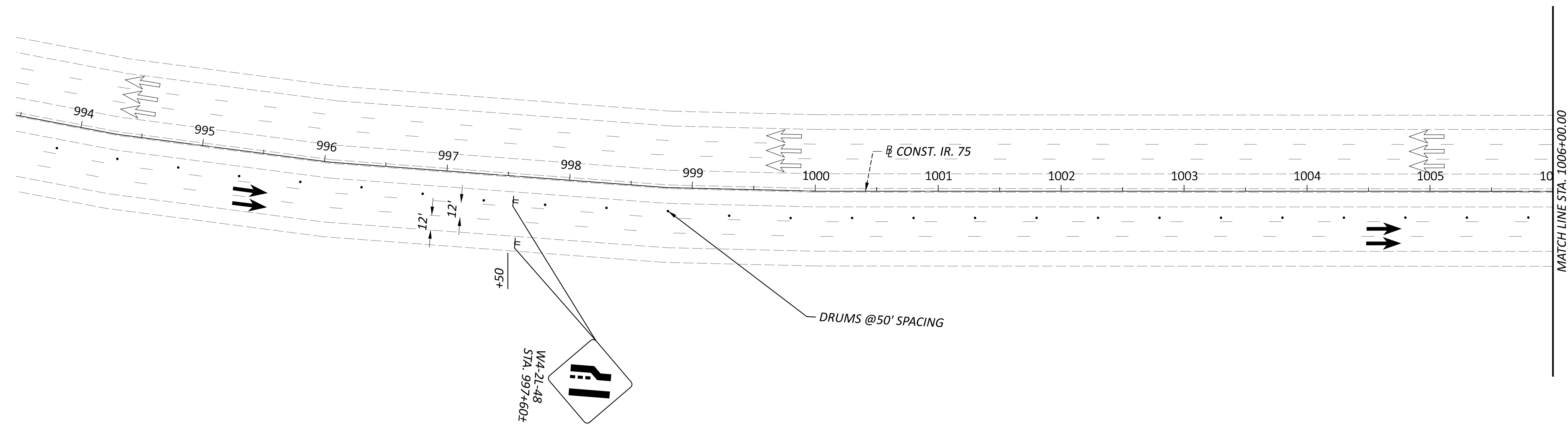


DESIGNER
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REVIEWER
 KF 02/12/24

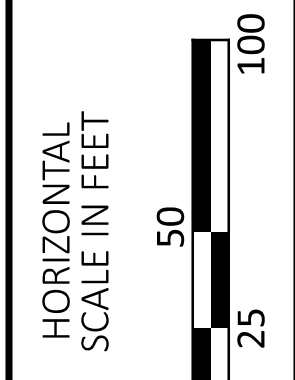
PROJECT ID
 102375

SHEET TOTAL
 73K 705



LEGEND

	WORK AREA		WORKZONE CENTER LINE (DOUBLE YELLOW)
	CONSTRUCTED IN PREVIOUS PHASE		WORKZONE EDGE LINE (YELLOW)
	TEMPORARY PAVEMENT		WORKZONE EDGE LINE (WHITE)
	TEMPORARY PAVEMENT CONST. IN PREVIOUS PHASE		WORKZONE CHANNELIZING LINE (WHITE)
	PORTABLE CONCRETE BARRIER		WORKZONE LANE LINE (WHITE)
	DRUMS		WORKZONE DOTTED LINE (WHITE)
	IMPACT ATTENUATOR		WORKZONE STOP LINE
	SIGN POST		PORTABLE CONCRETE BARRIER, 32"
	DIRECTION OF EXISTING TRAFFIC		WORK ZONE IMPACT ATTENUATOR
	DIRECTION OF PROPOSED TRAFFIC		



**MAINTENANCE OF TRAFFIC
 CLOSURE OF SOUTHBOUND IR 75**

DESIGN AGENCY



DESIGNER
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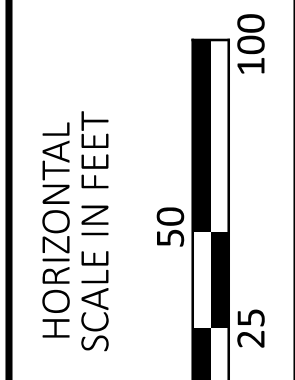
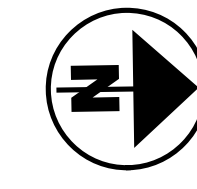
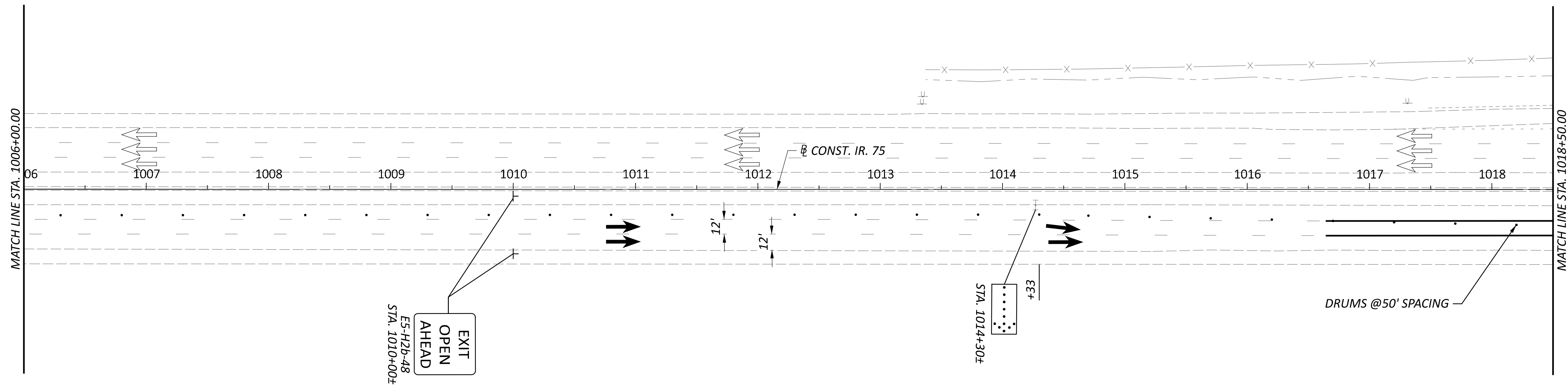
REVIEWER
 KF 02/12/24

PROJECT ID
 102375

SHEET TOTAL
 73L 705

HAN-75/CR99 INTERCHANGE REHAB

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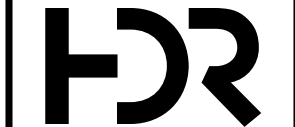


LEGEND

	WORK AREA		WORKZONE CENTER LINE (DOUBLE YELLOW)
	CONSTRUCTED IN PREVIOUS PHASE		WORKZONE EDGE LINE (YELLOW)
	TEMPORARY PAVEMENT		WORKZONE EDGE LINE (WHITE)
	TEMPORARY PAVEMENT CONST. IN PREVIOUS PHASE		WORKZONE CHANNELIZING LINE (WHITE)
	PORTABLE CONCRETE BARRIER		WORKZONE LANE LINE (WHITE)
	DRUMS		WORKZONE DOTTED LINE (WHITE)
	IMPACT ATTENUATOR		WORKZONE STOP LINE
	TYPE III BARRICADE		PORTABLE CONCRETE BARRIER, 32"
	SIGN POST		WORK ZONE IMPACT ATTENUATOR
	DIRECTION OF EXISTING TRAFFIC		
	DIRECTION OF PROPOSED TRAFFIC		

**MAINTENANCE OF TRAFFIC
 CLOSURE OF SOUTHBOUND IR 75**

DESIGN AGENCY

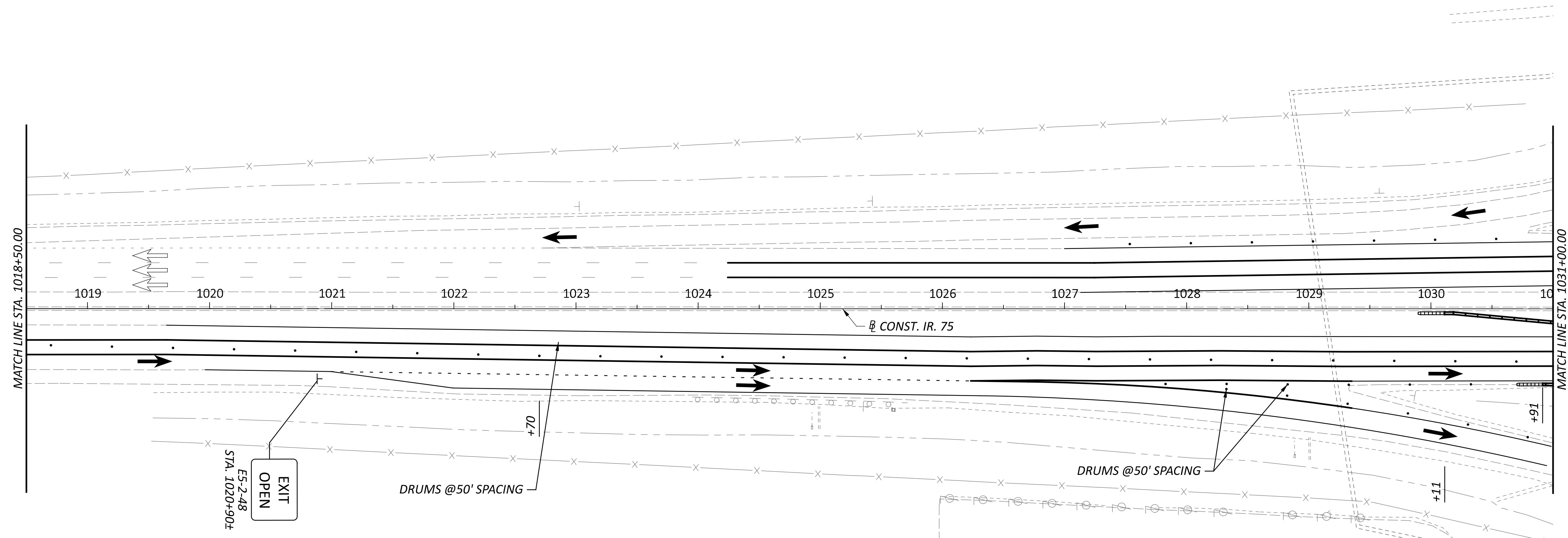


DESIGNER
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REVIEWER
 KF 02/12/24

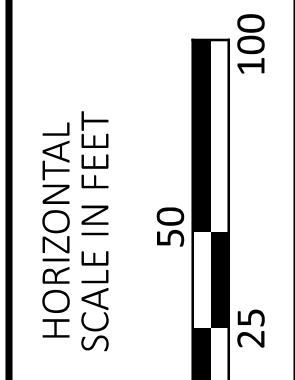
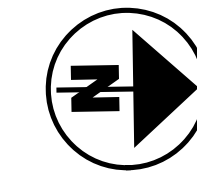
PROJECT ID
 102375

SHEET TOTAL
 73M 705



LEGEND

- | | | | |
|--|---|--|--------------------------------------|
| | WORK AREA | | WORKZONE CENTER LINE (DOUBLE YELLOW) |
| | CONSTRUCTED IN PREVIOUS PHASE | | WORKZONE EDGE LINE (YELLOW) |
| | TEMPORARY PAVEMENT | | WORKZONE EDGE LINE (WHITE) |
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| | DRUMS | | WORKZONE DOTTED LINE (WHITE) |
| | IMPACT ATTENUATOR | | WORKZONE STOP LINE |
| | TYPE III BARRICADE | | PORTABLE CONCRETE BARRIER, 32" |
| | SIGN POST | | WORK ZONE IMPACT ATTENUATOR |
| | DIRECTION OF EXISTING TRAFFIC | | |
| | DIRECTION OF PROPOSED TRAFFIC | | |



**MAINTENANCE OF TRAFFIC
 CLOSURE OF SOUTHBOUND IR 75**

DESIGN AGENCY



DESIGNER
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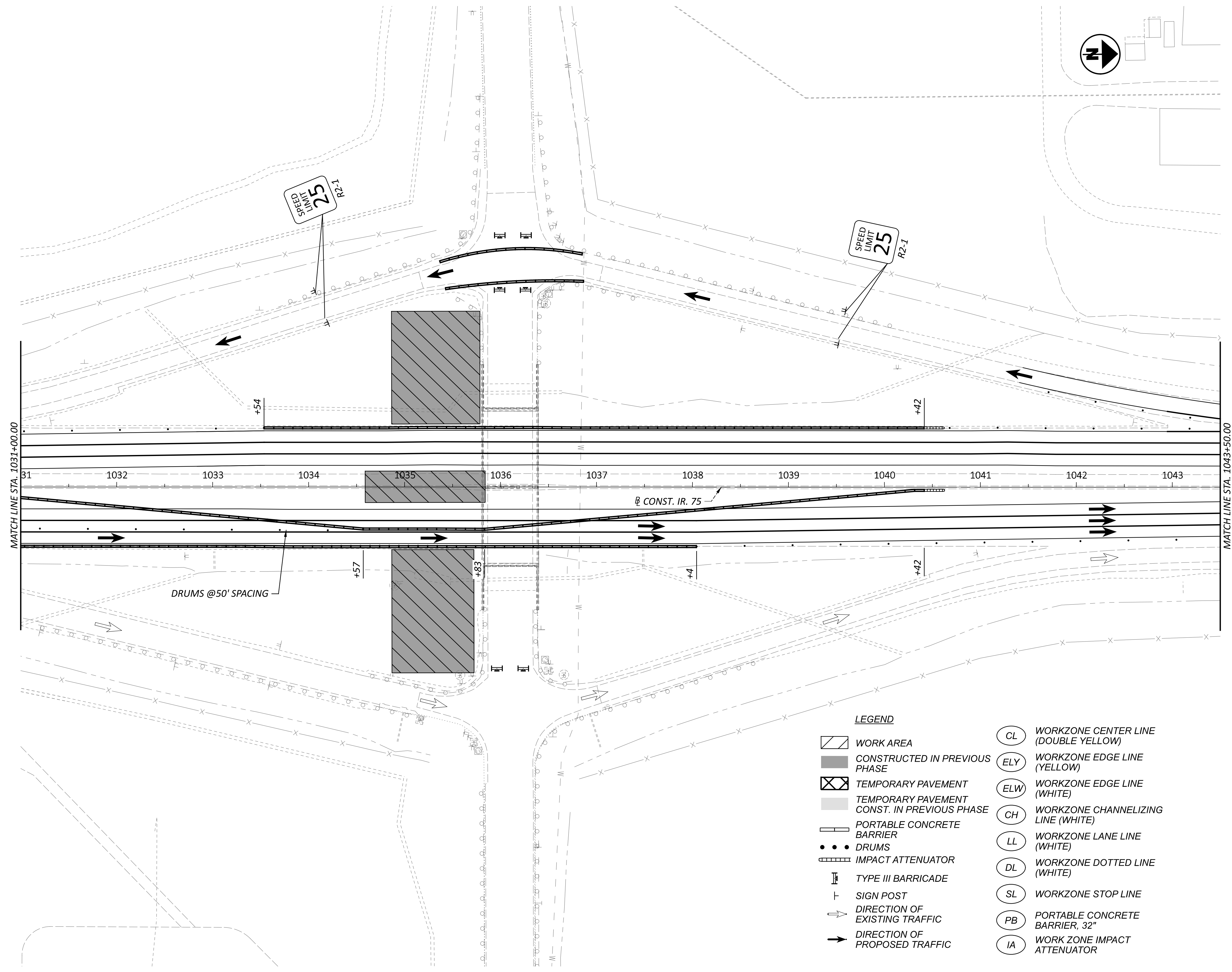
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 KF 02/12/24

PROJECT ID
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SHEET	TOTAL
73N	705

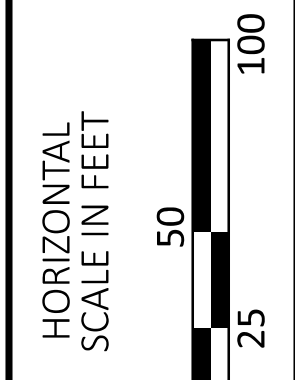
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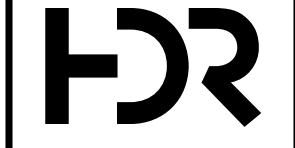
LEGEND

WORK AREA	WORKZONE CENTER LINE (DOUBLE YELLOW)
CONSTRUCTED IN PREVIOUS PHASE	WORKZONE EDGE LINE (YELLOW)
TEMPORARY PAVEMENT	WORKZONE EDGE LINE (WHITE)
TEMPORARY PAVEMENT CONST. IN PREVIOUS PHASE	WORKZONE CHANNELIZING LINE (WHITE)
PORTABLE CONCRETE BARRIER	WORKZONE LANE LINE (WHITE)
DRUMS	WORKZONE DOTTED LINE (WHITE)
IMPACT ATTENUATOR	WORKZONE STOP LINE
TYPE III BARRICADE	PORTABLE CONCRETE BARRIER, 32"
DIRECTION OF EXISTING TRAFFIC	WORK ZONE IMPACT ATTENUATOR
DIRECTION OF PROPOSED TRAFFIC	



**MAINTENANCE OF TRAFFIC
 CLOSURE OF SOUTHBOUND IR 75**

DESIGN AGENCY



DESIGNER
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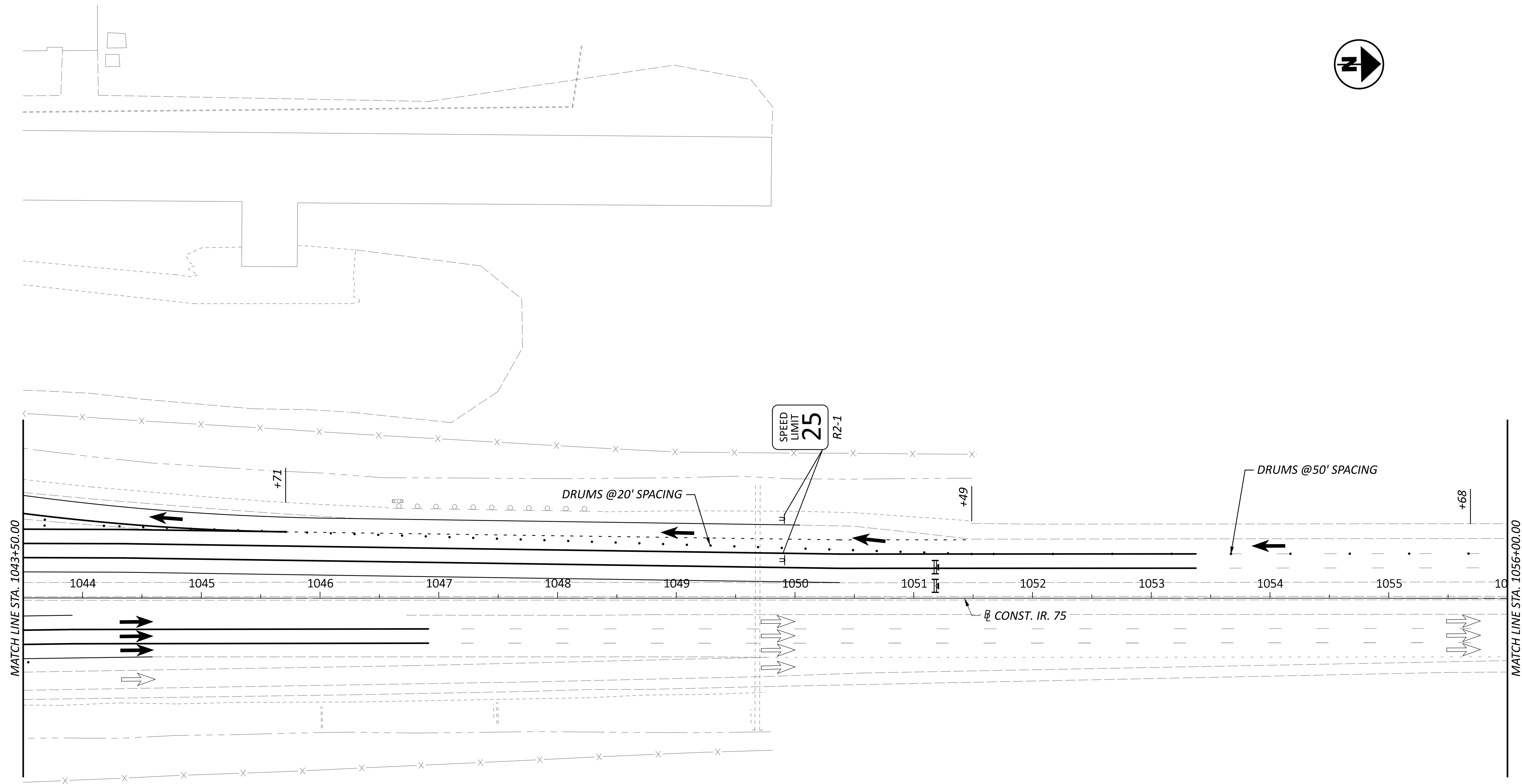
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PROJECT ID
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SHEET	TOTAL
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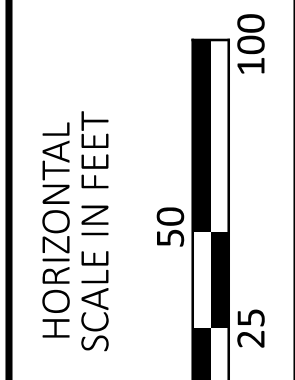
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LEGEND

- | | | | |
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| | CONSTRUCTED IN PREVIOUS PHASE | | WORKZONE EDGE LINE (YELLOW) |
| | TEMPORARY PAVEMENT | | WORKZONE EDGE LINE (WHITE) |
| | TEMPORARY PAVEMENT CONST. IN PREVIOUS PHASE | | WORKZONE CHANNELIZING LINE (WHITE) |
| | PORTABLE CONCRETE BARRIER | | WORKZONE LANE LINE (WHITE) |
| | DRUMS | | WORKZONE DOTTED LINE (WHITE) |
| | IMPACT ATTENUATOR | | WORKZONE STOP LINE |
| | TYPE III BARRICADE | | PORTABLE CONCRETE BARRIER, 32" |
| | SIGN POST | | WORK ZONE IMPACT ATTENUATOR |
| | DIRECTION OF EXISTING TRAFFIC | | |
| | DIRECTION OF PROPOSED TRAFFIC | | |



**MAINTENANCE OF TRAFFIC
 CLOSURE OF SOUTHBOUND IR 75**

DESIGN AGENCY



DESIGNER

CO

REVIEWER

KF 02/12/24

PROJECT ID

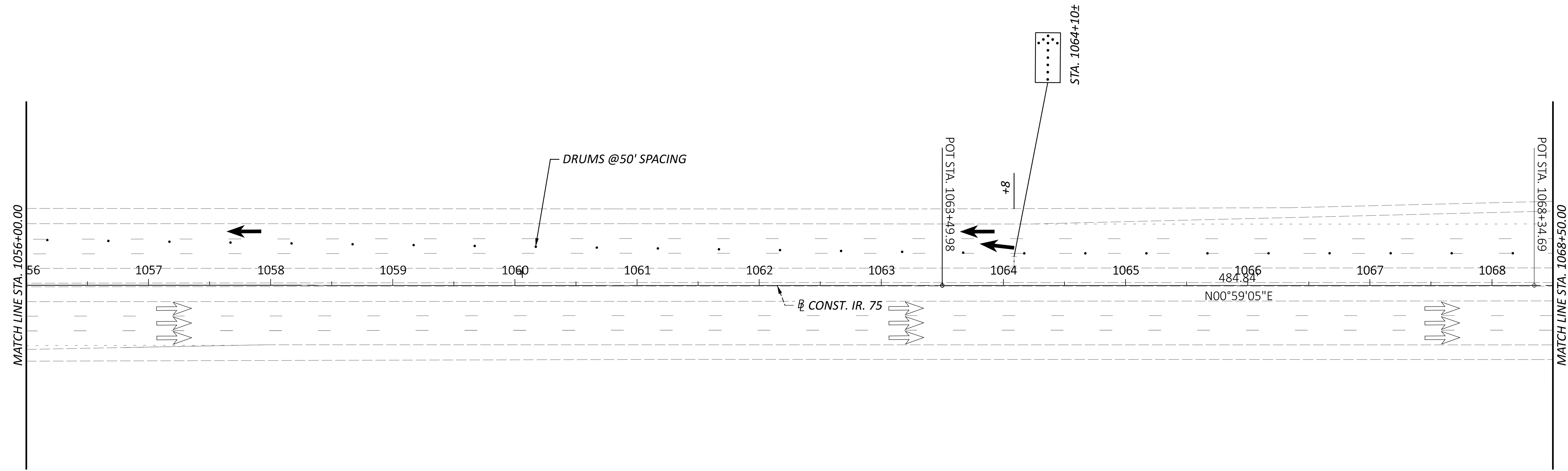
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SHEET TOTAL

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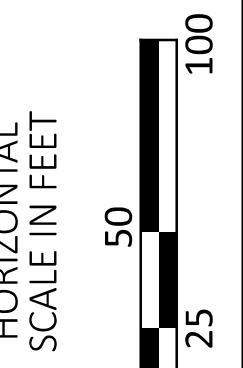
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LEGEND

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| | TEMPORARY PAVEMENT | | WORKZONE EDGE LINE (WHITE) |
| | TEMPORARY PAVEMENT CONST. IN PREVIOUS PHASE | | WORKZONE CHANNELIZING LINE (WHITE) |
| | PORTABLE CONCRETE BARRIER | | WORKZONE LANE LINE (WHITE) |
| | DRUMS | | WORKZONE DOTTED LINE (WHITE) |
| | IMPACT ATTENUATOR | | WORKZONE STOP LINE |
| | TYPE III BARRICADE | | PORTABLE CONCRETE BARRIER, 32" |
| | SIGN POST | | WORK ZONE IMPACT ATTENUATOR |
| | DIRECTION OF EXISTING TRAFFIC | | |
| | DIRECTION OF PROPOSED TRAFFIC | | |



**MAINTENANCE OF TRAFFIC
 CLOSURE OF SOUTHBOUND IR 75**

DESIGN AGENCY



DESIGNER
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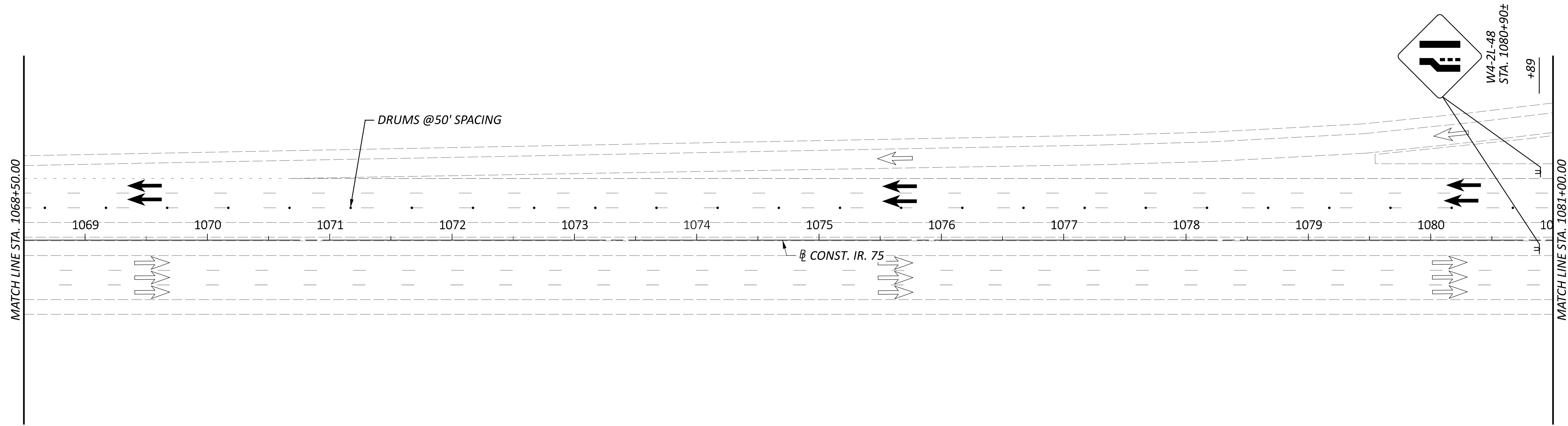
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PROJECT ID
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SHEET	TOTAL
73Q	705

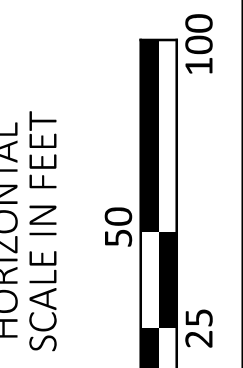
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LEGEND

- | | | | |
|--|---|--|--------------------------------------|
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| | DIRECTION OF EXISTING TRAFFIC | | |
| | DIRECTION OF PROPOSED TRAFFIC | | |



**MAINTENANCE OF TRAFFIC
 CLOSURE OF SOUTHBOUND IR 75**

DESIGN AGENCY

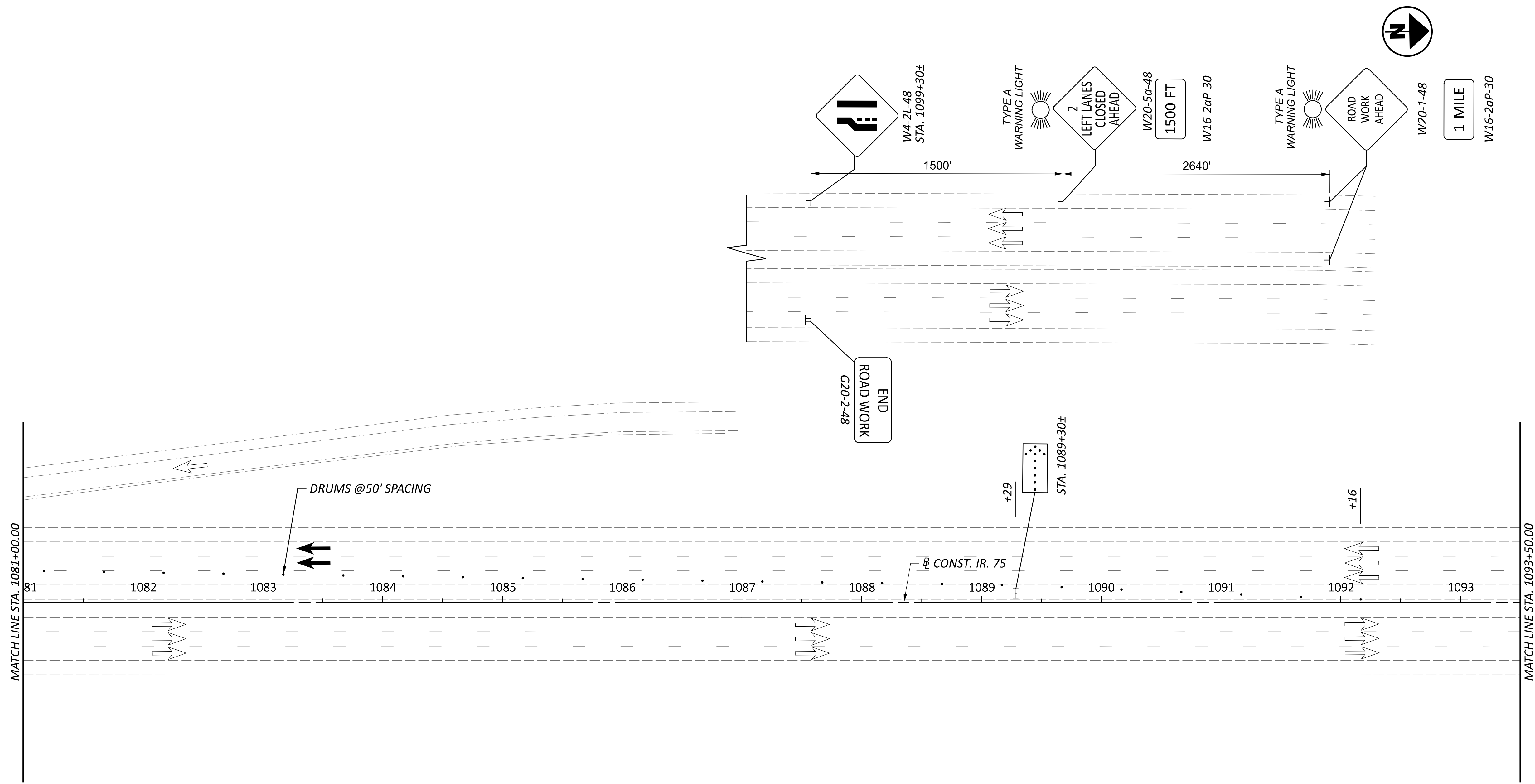


DESIGNER
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REVIEWER
 KF 02/12/24

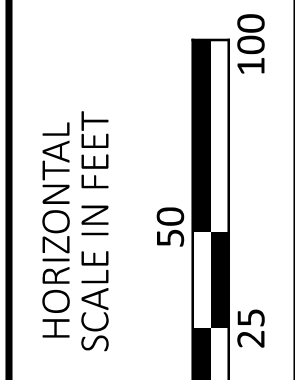
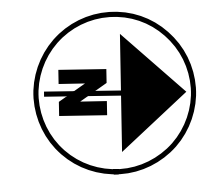
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SHEET	TOTAL
73R	705



LEGEND

- | | |
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| WORK AREA | WORKZONE CENTER LINE (DOUBLE YELLOW) |
| CONSTRUCTED IN PREVIOUS PHASE | WORKZONE EDGE LINE (YELLOW) |
| TEMPORARY PAVEMENT | WORKZONE EDGE LINE (WHITE) |
| TEMPORARY PAVEMENT CONST. IN PREVIOUS PHASE | WORKZONE CHANNELIZING LINE (WHITE) |
| PORTABLE CONCRETE BARRIER | WORKZONE LANE LINE (WHITE) |
| DRUMS | WORKZONE DOTTED LINE (WHITE) |
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| SIGN POST | WORK ZONE IMPACT ATTENUATOR |
| DIRECTION OF EXISTING TRAFFIC | |
| DIRECTION OF PROPOSED TRAFFIC | |



**MAINTENANCE OF TRAFFIC
 CLOSURE OF SOUTHBOUND IR 75**

DESIGN AGENCY

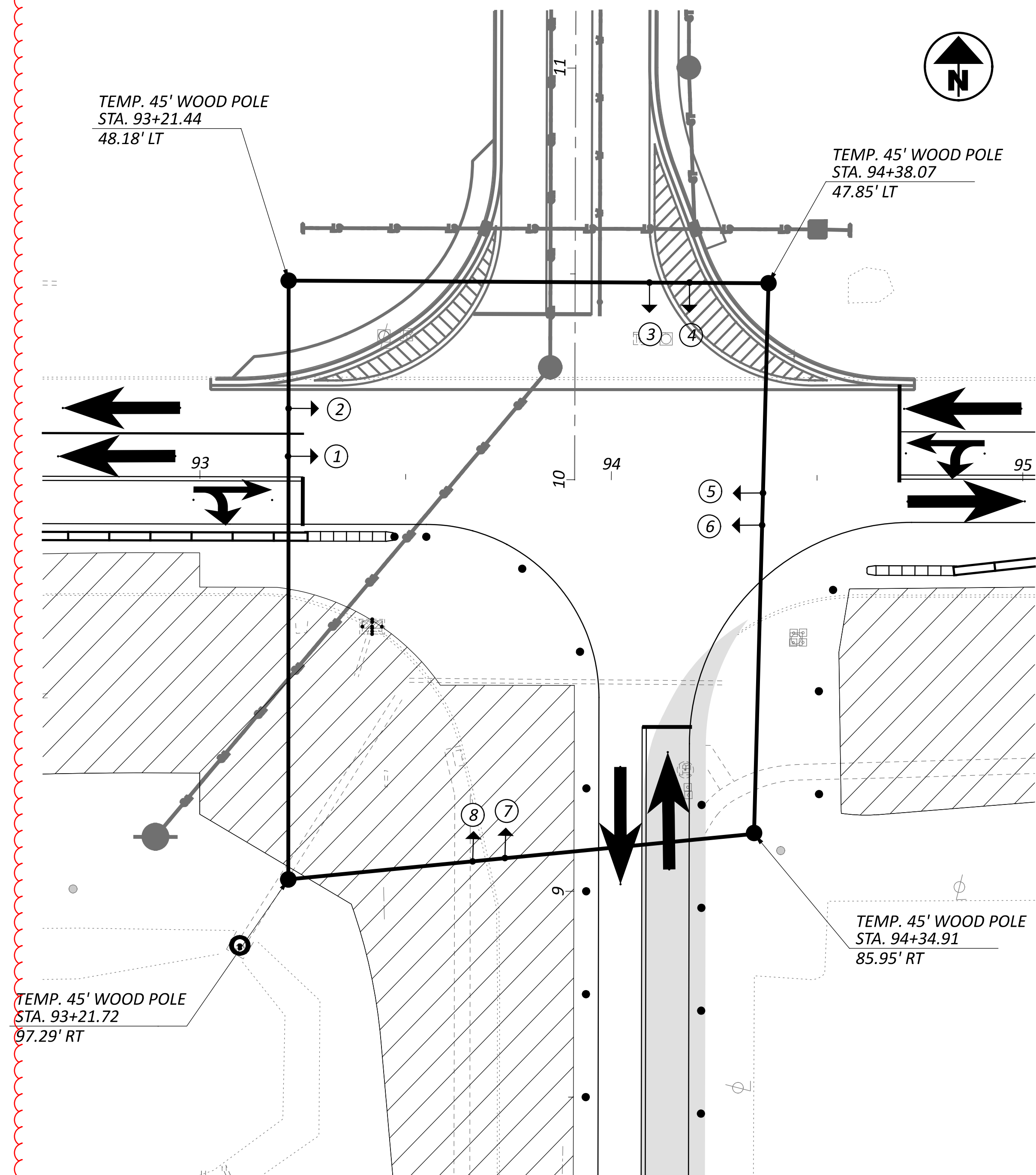


DESIGNER
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REVIEWER
 KF 02/12/24

PROJECT ID
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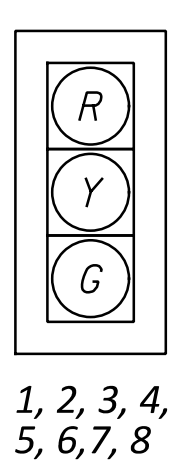


NOTE: EXISTING SIGNAL AT SPEEDWAY DR TO BE REMOVED DURING STEP 1 PHASE 1 OF CONSTRUCTION.

LEGEND

- TRAFFIC SIGNAL, 3 UNIT HEAD, 12"
- SIGNAL SUPPORT POLE
- WORK AREA
- TEMPORARY PAVEMENT
- CONSTRUCTED IN PREVIOUS PHASE
- TEMPORARY PAVEMENT CONST. IN PREVIOUS PHASE
- DIRECTION OF EXISTING TRAFFIC
- DIRECTION OF PROPOSED TRAFFIC

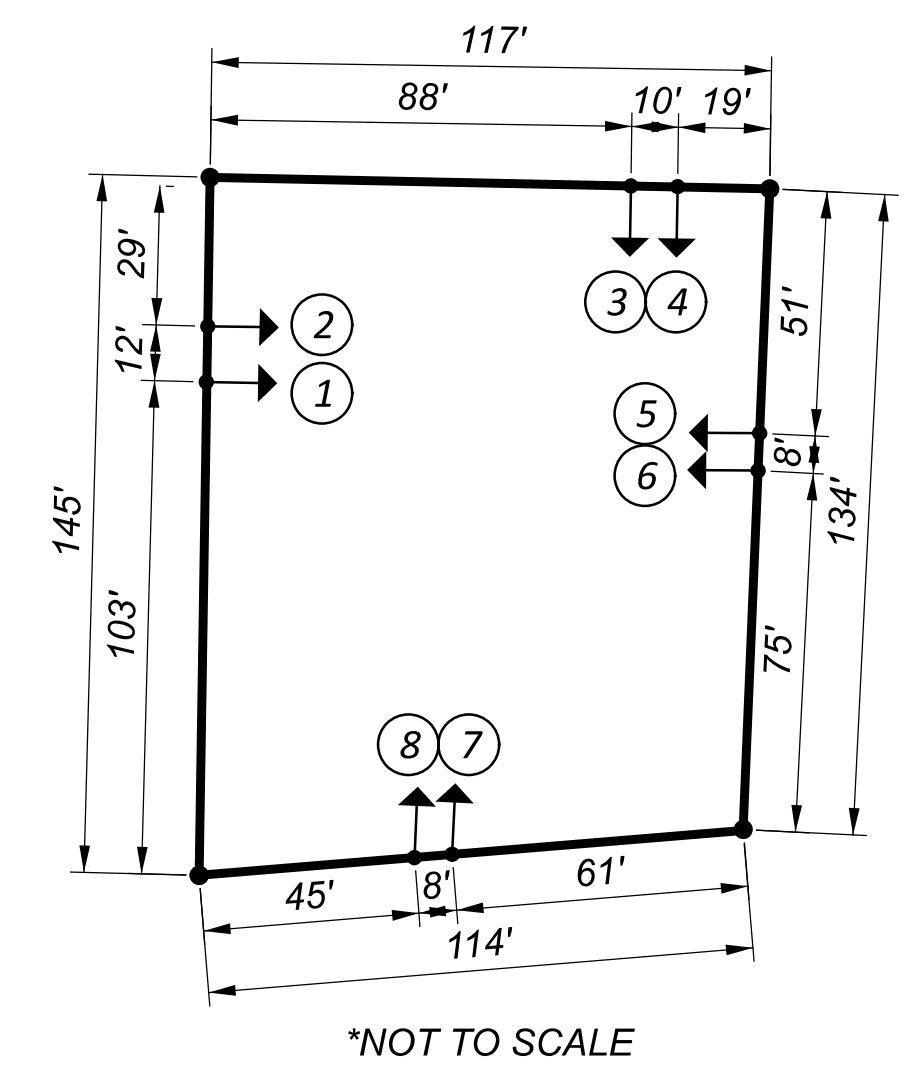
TEMPORARY SIGNAL HEADS



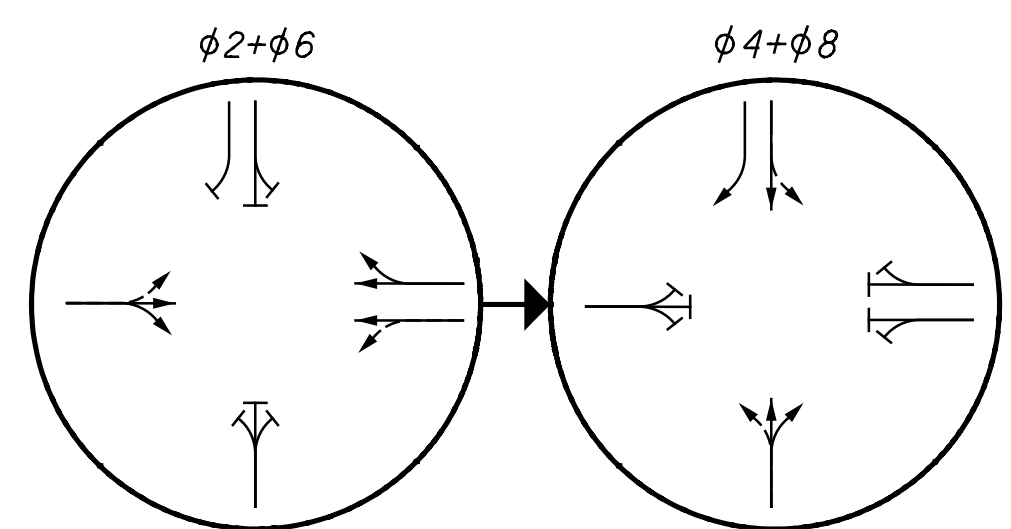
TEMPORARY FIELD WIRING HOOK-UP CHART

SIGNAL HEAD	INDICATION	FIELD TERMINAL	FLASH
1,2	R	Ø6 Y	R
	Y	Ø6 G	
	G	Ø6 R	
3,4	R	Ø8 R	R
	Y	Ø8 Y	
	G	Ø8 G	
5,6	R	Ø2 R	R
	Y	Ø2 Y	
	G	Ø2 G	
7,8	R	Ø4 R	R
	Y	Ø4 Y	
	G	Ø4 G	

TEMPORARY SIGNAL SPAN DIAGRAM

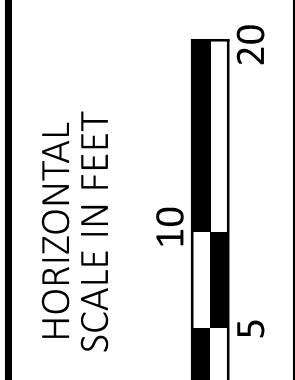


TEMPORARY PHASE DIAGRAM



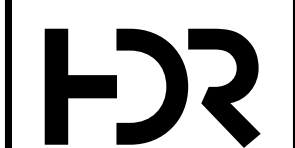
TEMPORARY SIGNAL TIMING CHART

INTERSECTION: CR99 & SPEEDWAY DR								
MAINTAINING AGENCY:								
START UP	DUAL ENTRY:		PHASES:					
	REST IN RED:	RING 1	RING 2					
START IN:	-	-	-	-	-	-	-	
TIME FOR: FLASH, ALL RED (SEC.):	-	9, 6	-	-	-	-	-	
FIRST PHASE(S):	2 & 6	-	-	-	-	-	-	
COLOR DISPLAYED:	-	-	-	-	-	-	-	
INTERVAL OR FEATURE	CONTROLLER MOVEMENT NO.							
INTERSECTION MOVEMENT (PHASE)	1	2	3	4	5	6	7	8
DIRECTION	-	EB	-	SB	-	WB	-	NB
MINIMUM GREEN (INITIAL) (SEC.)	-	30	-	20	-	30	-	20
ADDED INITIAL *(SEC./ACTUATION)	-	-	-	-	-	-	-	-
MAXIMUM INITIAL *(SEC.)	-	-	-	-	-	-	-	-
PASSAGE TIME (PRESET GAP) (SEC.)	-	3	-	3	-	3	-	3
TIME BEFORE REDUCTION *(SEC.)	-	-	-	-	-	-	-	-
MINIMUM GAP *(SEC.)	-	-	-	-	-	-	-	-
TIME TO REDUCE *(SEC.)	-	-	-	-	-	-	-	-
MAXIMUM GREEN I (SEC.)	-	60	-	55	-	60	-	55
MAXIMUM GREEN II (SEC.)	-	60	-	55	-	60	-	55
YELLOW CHANGE (SEC.)	-	3.4	-	3.4	-	3.4	-	3.4
ALL RED CLEARANCE (SEC.)	-	1.0	-	1.0	-	1.0	-	1.0
DELAYED GREEN (LPI) (SEC.)	-	-	-	-	-	-	-	-
FLASHING YELLOW ARROW DELAY (SEC.)	-	-	-	-	-	-	-	-
WALK (SEC.)	-	-	-	-	-	-	-	-
PEDESTRIAN CLEARANCE (SEC.)	-	-	-	-	-	-	-	-
RECALL	MAXIMUM (ON/OFF)	-	-	-	-	-	-	-
	MINIMUM (ON/OFF)	-	ON	-	-	ON	-	-
	PEDESTRIAN (ON/OFF)	-	-	-	-	-	-	-
MEMORY	(ON/OFF)	-	-	-	-	-	-	-

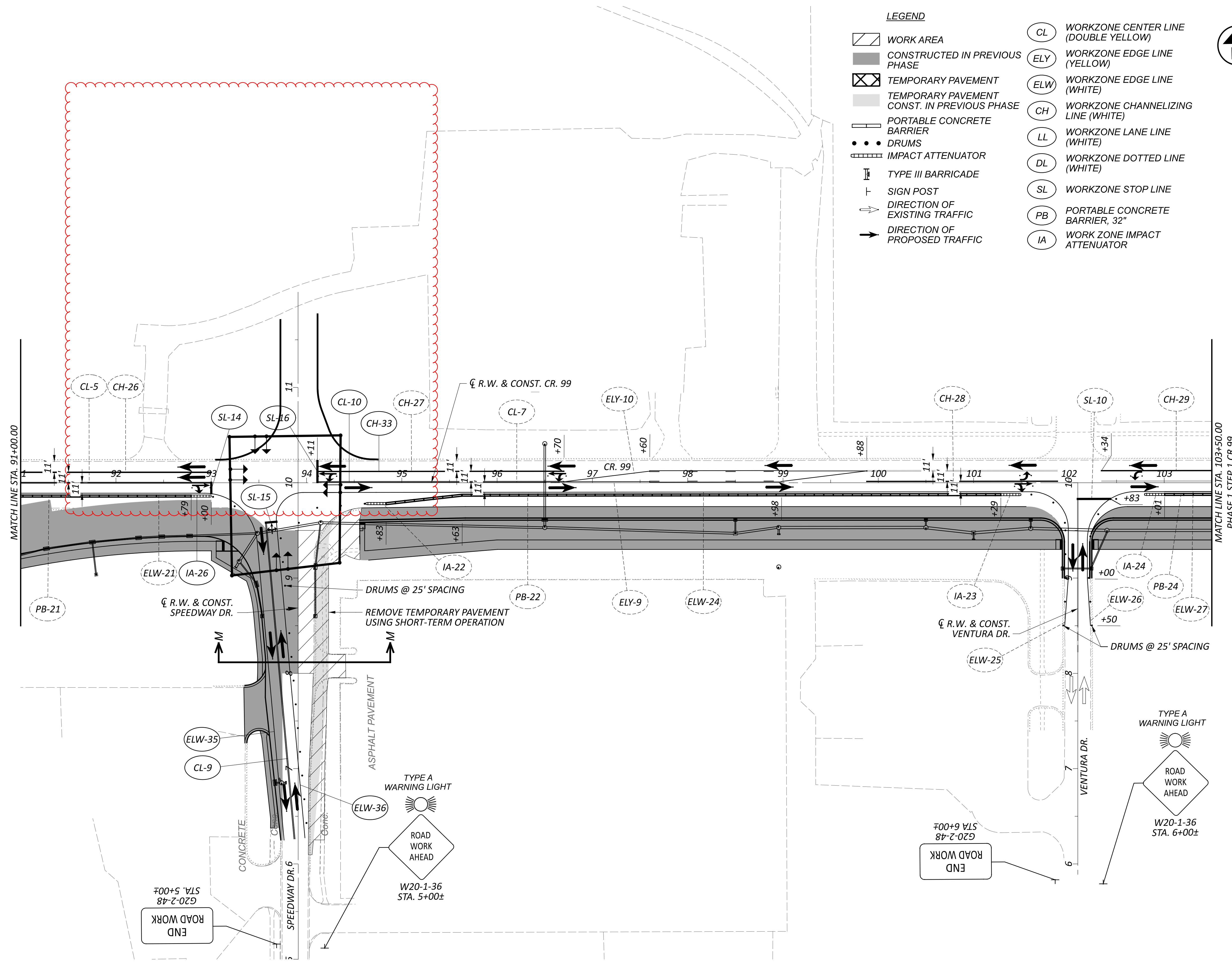


MAINTENANCE OF TRAFFIC - SPEEDWAY DR
 SIGNAL PLAN STAGING - PHASE 1 STEP 1

DESIGN AGENCY

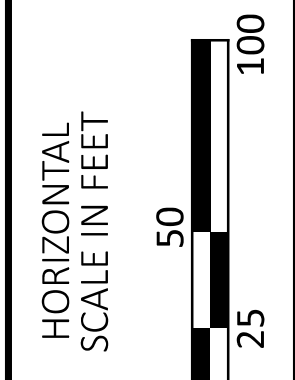
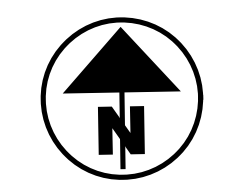


DESIGNER: JLL
 REVIEWER: DWB 05/20/22
 PROJECT ID: 102375
 SHEET: 76 TOTAL: 705



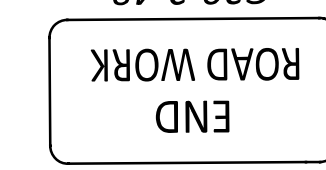
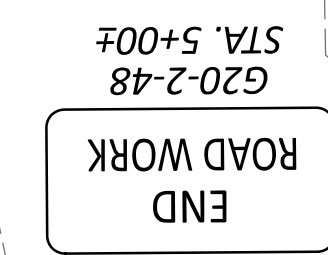
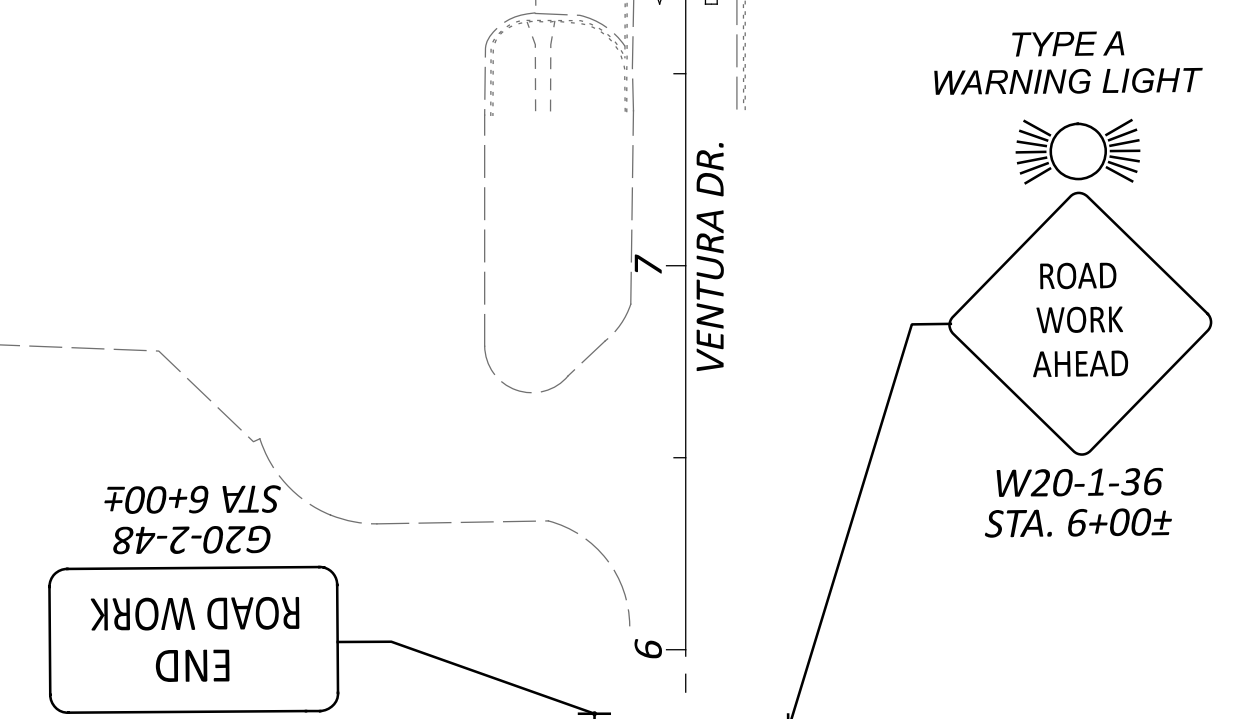
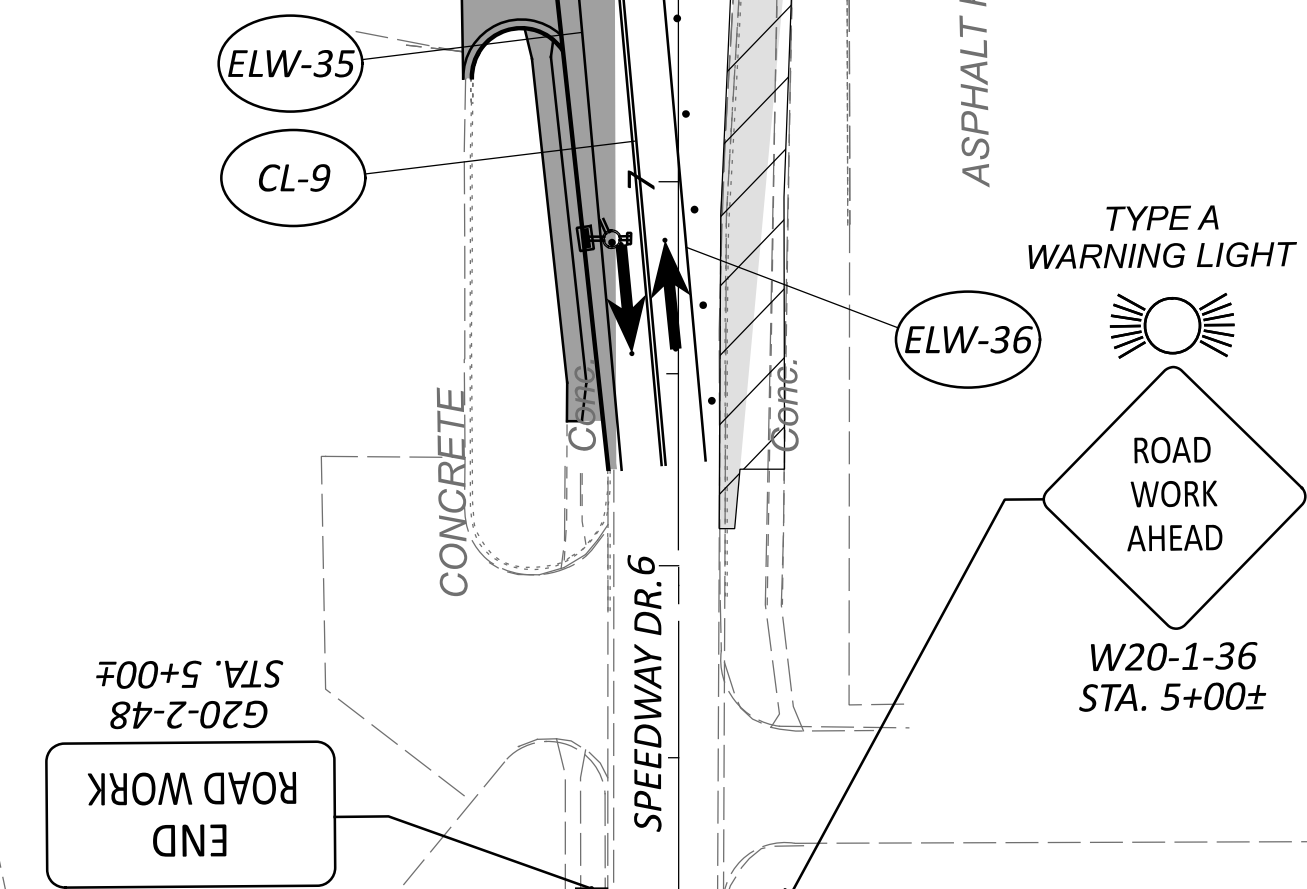
LEGEND

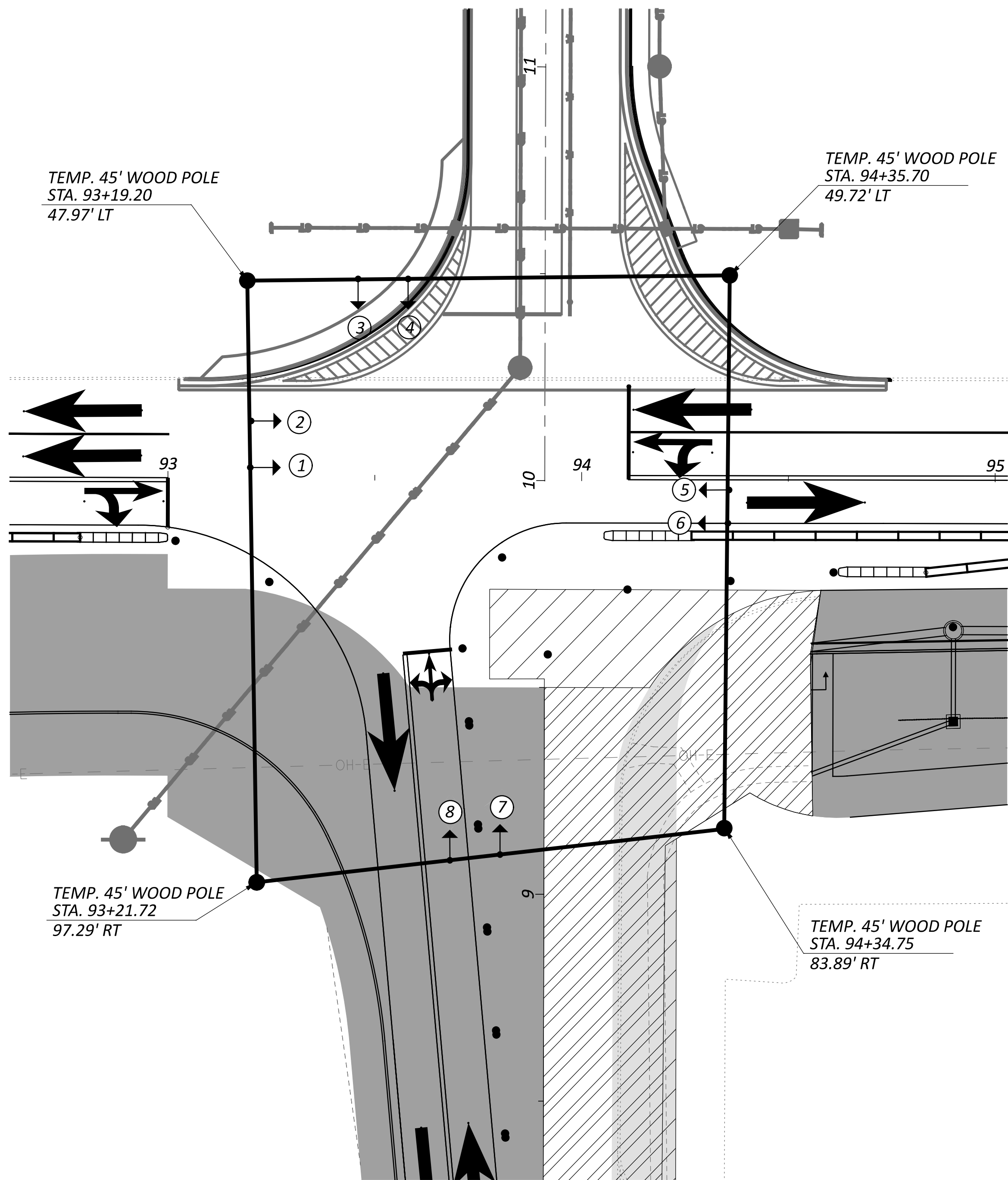
	WORK AREA		WORKZONE CENTER LINE (DOUBLE YELLOW)
	CONSTRUCTED IN PREVIOUS PHASE		WORKZONE EDGE LINE (YELLOW)
	TEMPORARY PAVEMENT		WORKZONE EDGE LINE (WHITE)
	TEMPORARY PAVEMENT CONST. IN PREVIOUS PHASE		WORKZONE CHANNELIZING LINE (WHITE)
	PORTABLE CONCRETE BARRIER		WORKZONE LANE LINE (WHITE)
	DRUMS		WORKZONE DOTTED LINE (WHITE)
	IMPACT ATTENUATOR		WORKZONE STOP LINE
	TYPE III BARRICADE		PORTABLE CONCRETE BARRIER, 32"
	SIGN POST		WORK ZONE IMPACT ATTENUATOR
	DIRECTION OF EXISTING TRAFFIC		
	DIRECTION OF PROPOSED TRAFFIC		



**MAINTENANCE OF TRAFFIC
 PHASE 1 STEP 2 CR 99**

DESIGN AGENCY	
DESIGNER	CO
REVIEWER	KF 05/20/22
PROJECT ID	102375
SHEET TOTAL	79 705

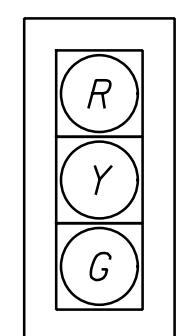




LEGEND

- TRAFFIC SIGNAL, 3 UNIT HEAD, 12"
- SIGNAL SUPPORT POLE
- WORK AREA
- TEMPORARY PAVEMENT
- CONSTRUCTED IN PREVIOUS PHASE
- TEMPORARY PAVEMENT CONST. IN PREVIOUS PHASE
- DIRECTION OF EXISTING TRAFFIC
- DIRECTION OF PROPOSED TRAFFIC

TEMPORARY SIGNAL HEADS

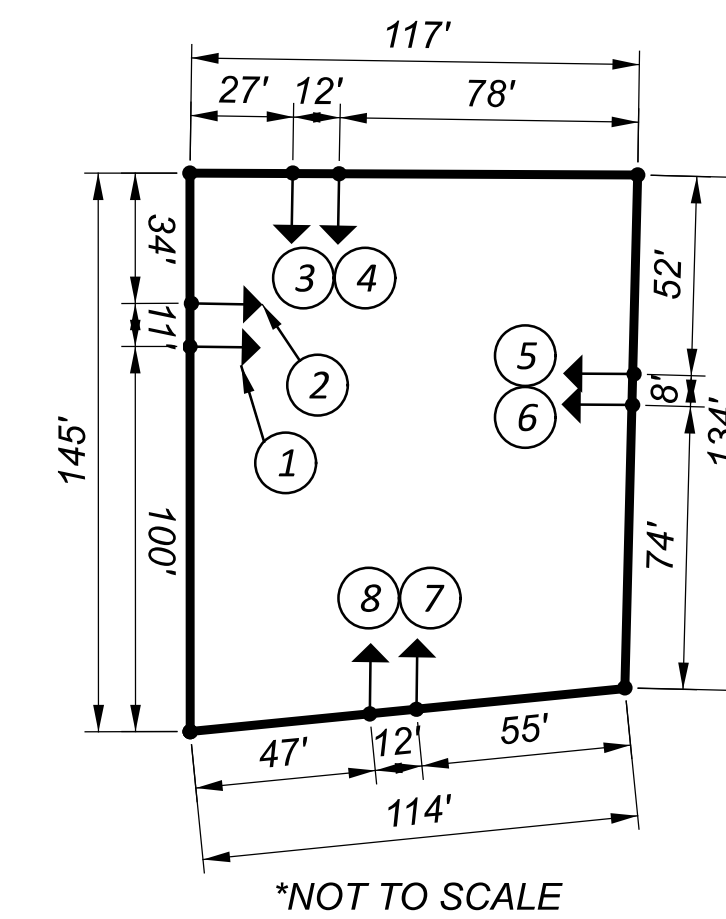


1, 2, 3, 4,
5, 6, 7, 8

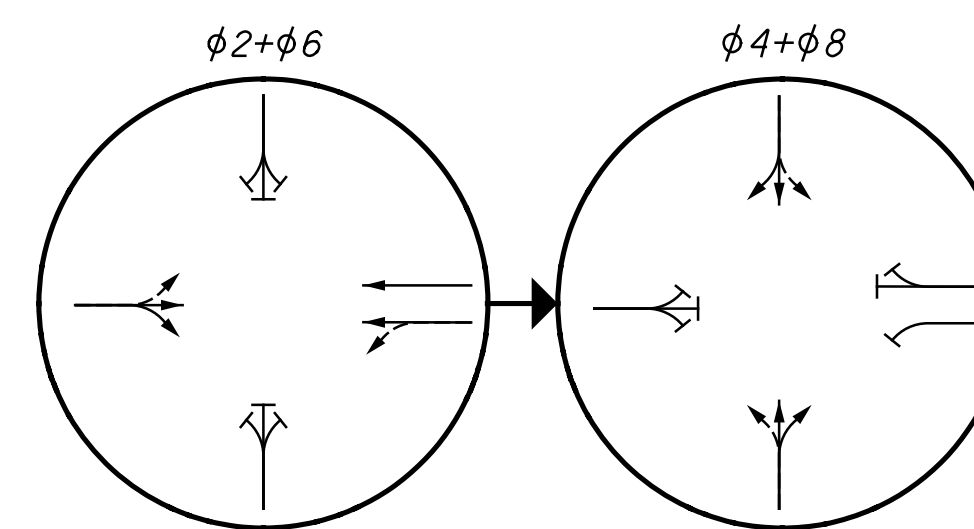
TEMPORARY FIELD WIRING HOOK-UP CHART

SIGNAL HEAD	INDICATION	FIELD TERMINAL	FLASH
1,2	R	Ø6 G	R
	Y	Ø6 Y	
	G	Ø6 G	
3,4	R	Ø8 R	R
	Y	Ø8 Y	
	G	Ø8 G	
5,6	R	Ø2 R	R
	Y	Ø2 Y	
	G	Ø2 G	
7,8	R	Ø4 R	R
	Y	Ø4 Y	
	G	Ø4 G	

TEMPORARY SIGNAL SPAN DIAGRAM

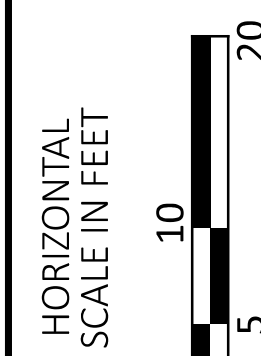


TEMPORARY PHASE DIAGRAM



TEMPORARY SIGNAL TIMING CHART

INTERSECTION: CR99 & SPEEDWAY DR								
MAINTAINING AGENCY:								
START UP	DUAL ENTRY:		PHASES:					
	REST IN RED:		RING 1		RING 2			
START IN:	-	9, 6						
TIME FOR: FLASH, ALL RED (SEC.):	-	9, 6						
FIRST PHASE(S):	2 & 6							
COLOR DISPLAYED:	-							
INTERVAL OR FEATURE	CONTROLLER MOVEMENT NO.							
INTERSECTION MOVEMENT (PHASE)	1	2	3	4	5	6	7	8
DIRECTION	-	EB	-	SB	-	WB	-	NB
MINIMUM GREEN (INITIAL) (SEC.)	-	30	-	20	-	30	-	20
ADDED INITIAL *(SEC./ACTUATION)	-	-	-	-	-	-	-	-
MAXIMUM INITIAL *(SEC.)	-	-	-	-	-	-	-	-
PASSAGE TIME (PRESET GAP) (SEC.)	-	3	-	3	-	3	-	3
TIME BEFORE REDUCTION *(SEC.)	-	-	-	-	-	-	-	-
MINIMUM GAP *(SEC.)	-	-	-	-	-	-	-	-
TIME TO REDUCE *(SEC.)	-	-	-	-	-	-	-	-
MAXIMUM GREEN I (SEC.)	-	60	-	55	-	60	-	55
MAXIMUM GREEN II (SEC.)	-	60	-	55	-	60	-	55
YELLOW CHANGE (SEC.)	-	3.4	-	3.4	-	3.4	-	3.4
ALL RED CLEARANCE (SEC.)	-	1.0	-	1.0	-	1.0	-	1.0
DELAYED GREEN (LPI) (SEC.)	-	-	-	-	-	-	-	-
FLASHING YELLOW ARROW DELAY (SEC.)	-	-	-	-	-	-	-	-
WALK (SEC.)	-	-	-	-	-	-	-	-
PEDESTRIAN CLEARANCE (SEC.)	-	-	-	-	-	-	-	-
RECALL	MAXIMUM (ON/OFF)	-	-	-	-	-	-	-
	MINIMUM (ON/OFF)	-	ON	-	-	-	ON	-
	PEDESTRIAN (ON/OFF)	-	-	-	-	-	-	-
MEMORY (ON/OFF)	-	-	-	-	-	-	-	-



MAINTENANCE OF TRAFFIC - SPEEDWAY DR
 SIGNAL PLAN STAGIN - PHASE 1 STEP 2

DESIGN AGENCY



DESIGNER
JLL

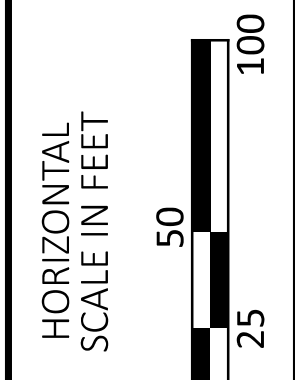
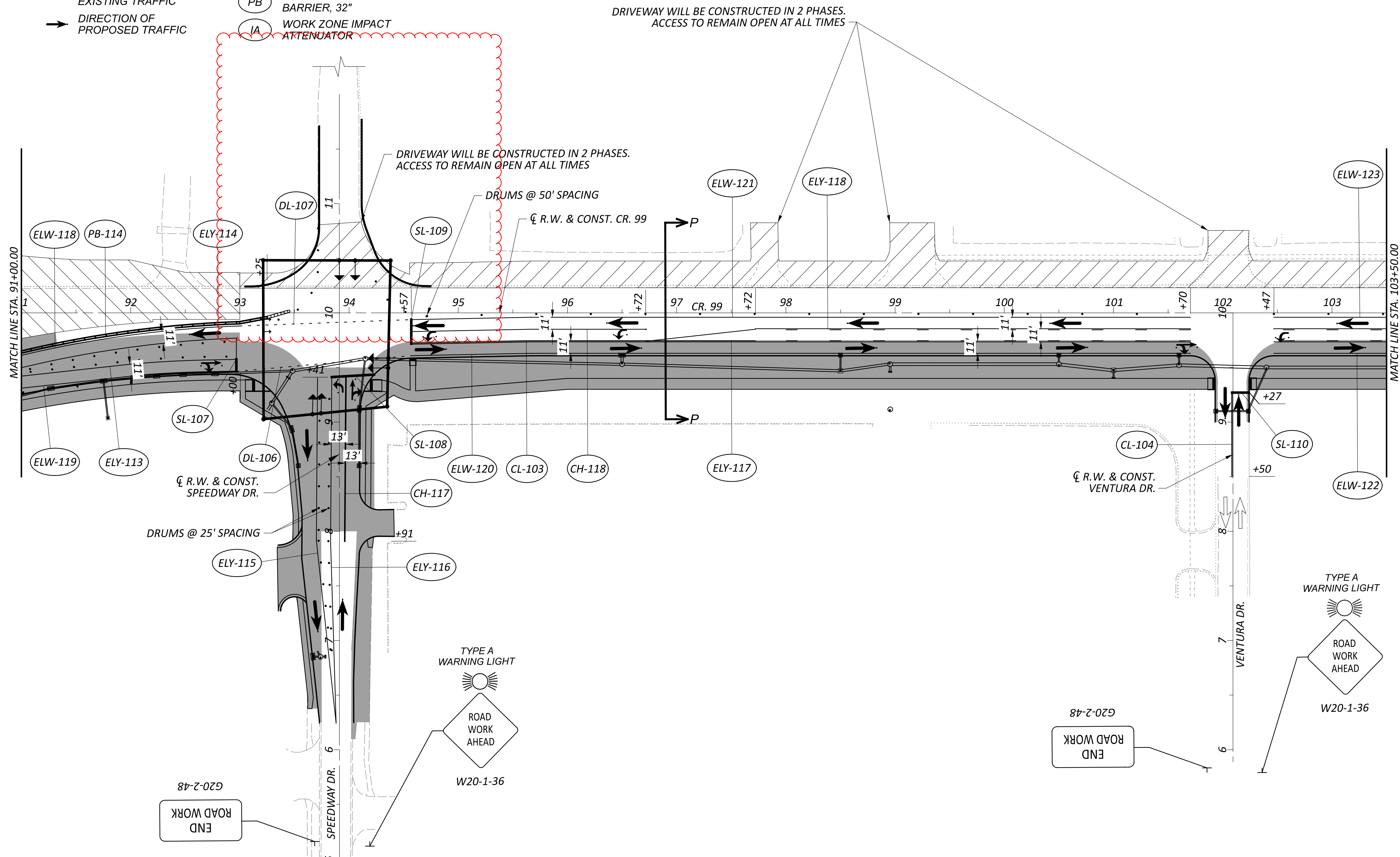
REVIEWER
DWB 05/20/22

PROJECT ID
102375

SHEET TOTAL
88 | 705

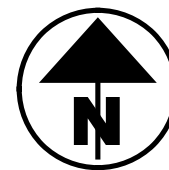
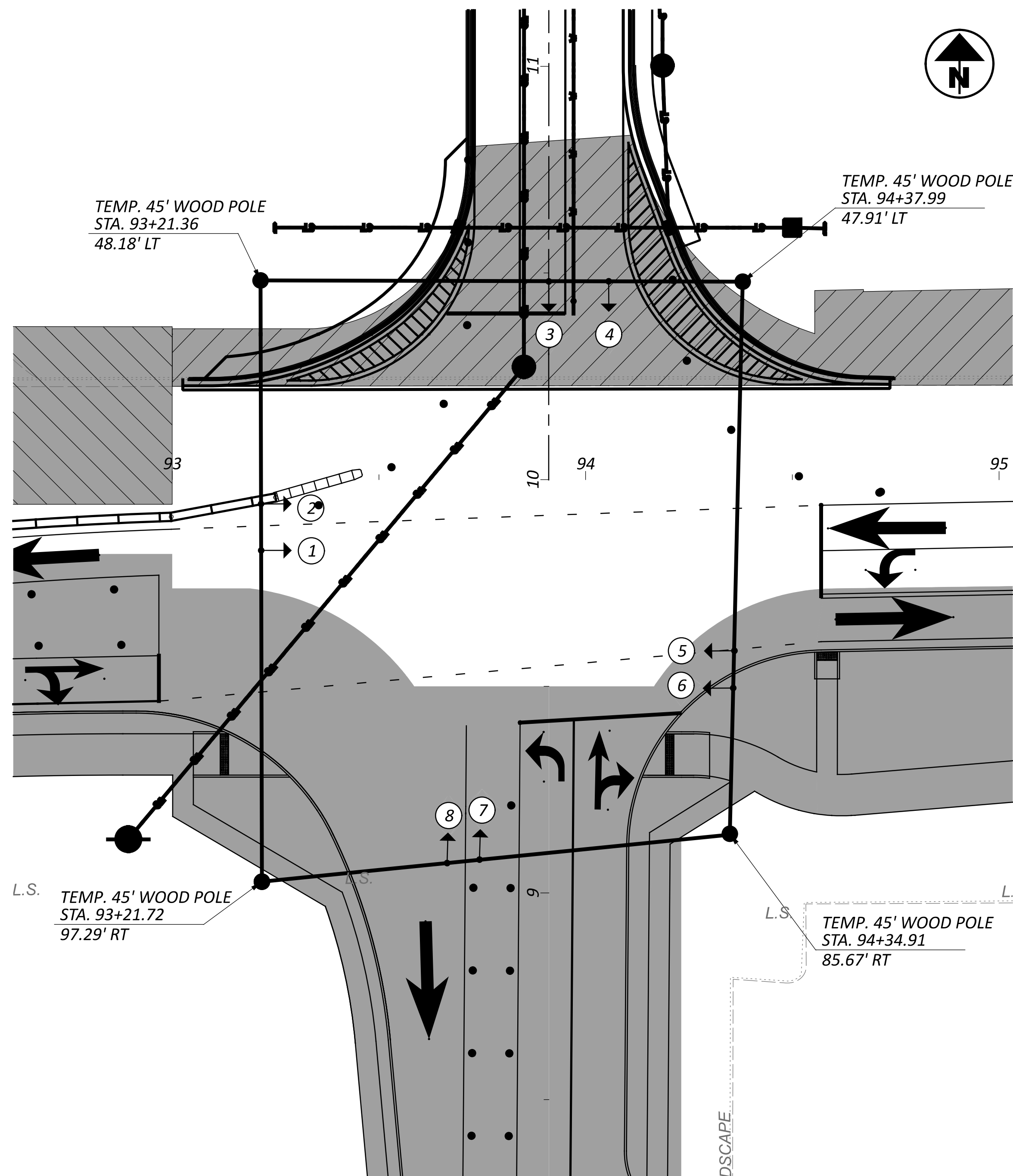
- LEGEND**
- WORK AREA
 - CONSTRUCTED IN PREVIOUS PHASE
 - TEMPORARY PAVEMENT
 - TEMPORARY PAVEMENT CONST. IN PREVIOUS PHASE
 - PORTABLE CONCRETE BARRIER
 - DRUMS
 - IMPACT ATTENUATOR
 - TYPE III BARRICADE
 - SIGN POST
 - DIRECTION OF EXISTING TRAFFIC
 - DIRECTION OF PROPOSED TRAFFIC

- CL WORKZONE CENTER LINE (DOUBLE YELLOW)
- ELY WORKZONE EDGE LINE (YELLOW)
- ELW WORKZONE EDGE LINE (WHITE)
- CH WORKZONE CHANNELIZING LINE (WHITE)
- LL WORKZONE LANE LINE (WHITE)
- DL WORKZONE DOTTED LINE (WHITE)
- SL WORKZONE STOP LINE
- PB PORTABLE CONCRETE BARRIER, 32"
- IA WORK ZONE IMPACT ATTENUATOR



MAINTENANCE OF TRAFFIC
 PHASE 2 CR 99

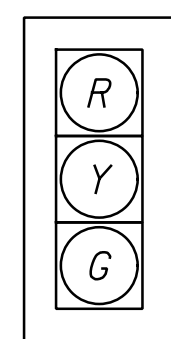
DESIGN AGENCY	
DESIGNER	CO
REVIEWER	KF 05/20/22
PROJECT ID	102375
SHEET	93
TOTAL	705



LEGEND

- TRAFFIC SIGNAL, 3 UNIT HEAD, 12"
- TRAFFIC SIGNAL, 3 UNIT HEAD, 12" WITH ARROWS
- SIGNAL SUPPORT POLE
- WORK AREA
- TEMPORARY PAVEMENT
- CONSTRUCTED IN PREVIOUS PHASE
- TEMPORARY PAVEMENT CONST. IN PREVIOUS PHASE
- DIRECTION OF EXISTING TRAFFIC
- DIRECTION OF PROPOSED TRAFFIC

TEMPORARY SIGNAL HEADS

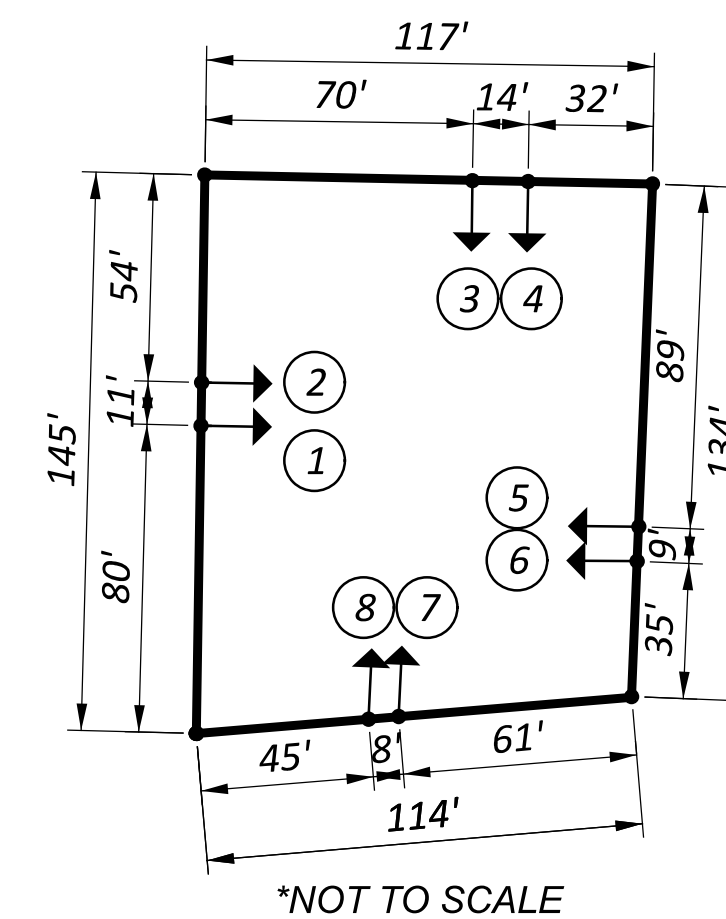


1, 2, 3, 4,
5, 6, 7, 8

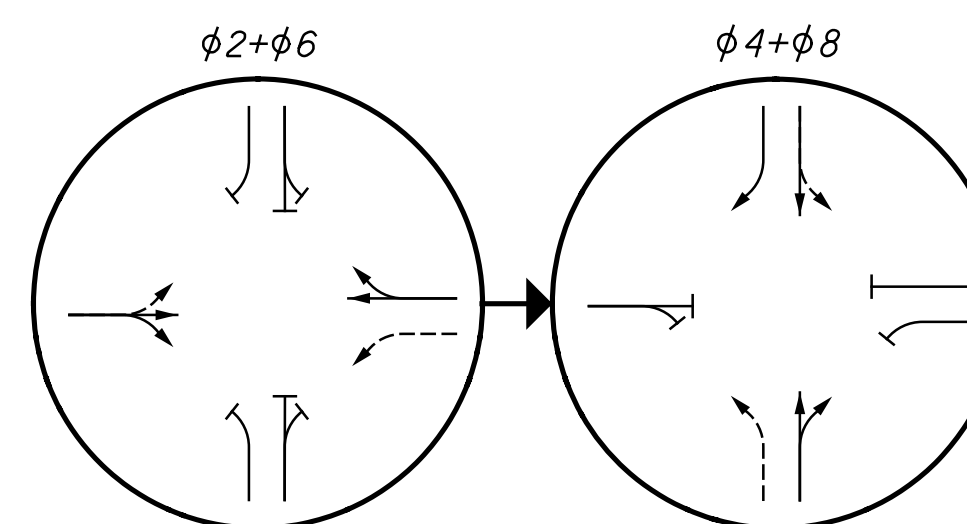
TEMPORARY FIELD WIRING HOOK-UP CHART

SIGNAL HEAD	INDICATION	FIELD TERMINAL	FLASH
1,2	R	Ø6 R	R
	Y	Ø6 Y	
	G	Ø6 G	
3,4	R	Ø8 R	R
	Y	Ø8 Y	
	G	Ø8 G	
5,6	R	Ø2 R	R
	Y	Ø2 Y	
	G	Ø2 G	
7,8	R	Ø4 R	R
	Y	Ø4 Y	
	G	Ø4 G	

TEMPORARY SIGNAL SPAN DIAGRAM

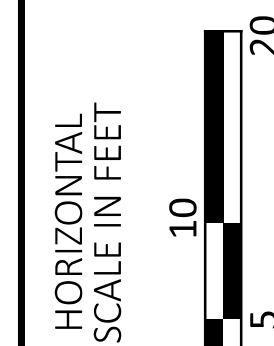


TEMPORARY PHASE DIAGRAM



TEMPORARY SIGNAL TIMING CHART

INTERSECTION: CR99 & SPEEDWAY DR								
MAINTAINING AGENCY:								
START UP	DUAL ENTRY:		PHASES:					
	REST IN RED:		RING 1		RING 2			
START IN:	-	-	-	-	-	-	-	-
TIME FOR: FLASH, ALL RED (SEC.):	-	9, 6	-	-	-	-	-	-
FIRST PHASE(S):	2 & 6	-	-	-	-	-	-	-
COLOR DISPLAYED:	-	-	-	-	-	-	-	-
INTERVAL OR FEATURE	CONTROLLER MOVEMENT NO.							
INTERSECTION MOVEMENT (PHASE)	1	2	3	4	5	6	7	8
DIRECTION	-	EB	-	SB	-	WB	-	NB
MINIMUM GREEN (INITIAL) (SEC.)	-	30	-	20	-	30	-	20
ADDED INITIAL *(SEC./ACTUATION)	-	-	-	-	-	-	-	-
MAXIMUM INITIAL *(SEC.)	-	-	-	-	-	-	-	-
PASSAGE TIME (PRESET GAP) (SEC.)	-	3	-	3	-	3	-	3
TIME BEFORE REDUCTION *(SEC.)	-	-	-	-	-	-	-	-
MINIMUM GAP *(SEC.)	-	-	-	-	-	-	-	-
TIME TO REDUCE *(SEC.)	-	-	-	-	-	-	-	-
MAXIMUM GREEN I (SEC.)	-	60	-	55	-	60	-	55
MAXIMUM GREEN II (SEC.)	-	60	-	55	-	60	-	55
YELLOW CHANGE (SEC.)	-	3.4	-	3.4	-	3.4	-	3.4
ALL RED CLEARANCE (SEC.)	-	1.0	-	1.0	-	1.0	-	1.0
DELAYED GREEN (LPI) (SEC.)	-	-	-	-	-	-	-	-
FLASHING YELLOW ARROW DELAY (SEC.)	-	-	-	-	-	-	-	-
WALK (SEC.)	-	-	-	-	-	-	-	-
PEDESTRIAN CLEARANCE (SEC.)	-	-	-	-	-	-	-	-
RECALL	MAXIMUM (ON/OFF)	-	-	-	-	-	-	-
	MINIMUM (ON/OFF)	-	ON	-	-	ON	-	-
	PEDESTRIAN (ON/OFF)	-	-	-	-	-	-	-
MEMORY (ON/OFF)	-	-	-	-	-	-	-	-



MAINTENANCE OF TRAFFIC - SPEEDWAY DR
 SIGNAL PLAN STAGING - PHASE 2

DESIGN AGENCY



DESIGNER

JLL

REVIEWER

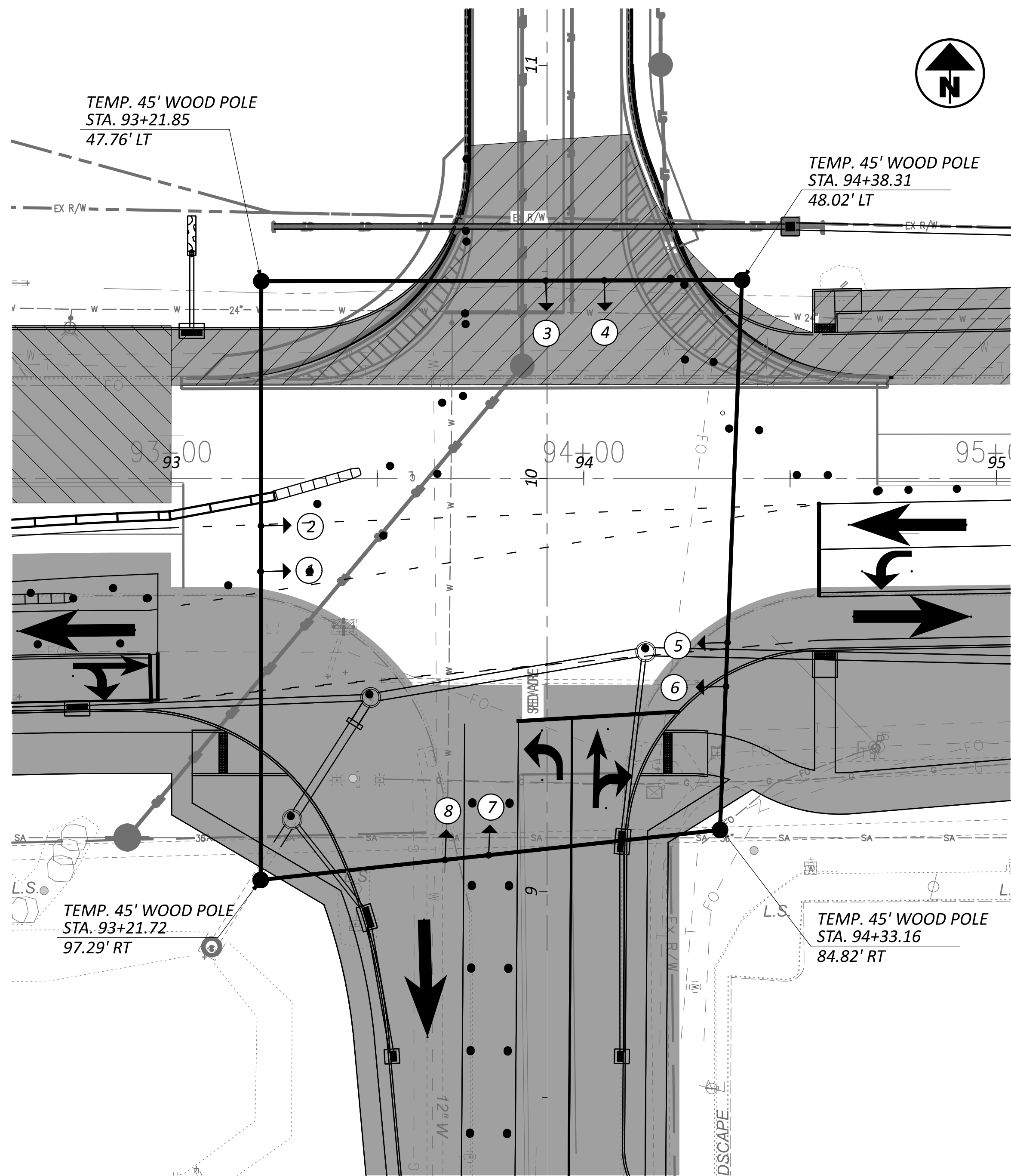
DWB 05/20/22

PROJECT ID

102375

SHEET TOTAL

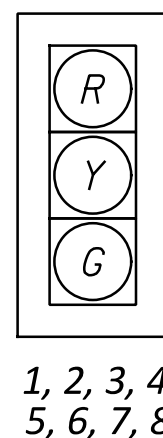
103 705



LEGEND

- TRAFFIC SIGNAL, 3 UNIT HEAD, 12"
- TRAFFIC SIGNAL, 3 UNIT HEAD, 12" WITH ARROWS
- SIGNAL SUPPORT POLE
- WORK AREA
- TEMPORARY PAVEMENT
- CONSTRUCTED IN PREVIOUS PHASE
- TEMPORARY PAVEMENT CONST. IN PREVIOUS PHASE
- DIRECTION OF EXISTING TRAFFIC
- DIRECTION OF PROPOSED TRAFFIC

TEMPORARY SIGNAL HEADS

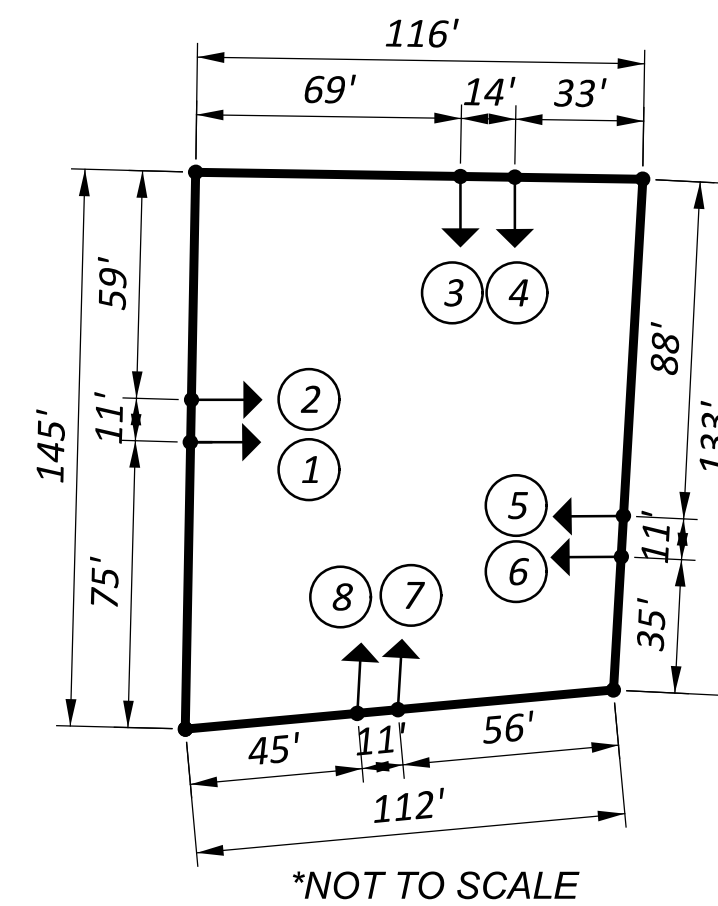


1, 2, 3, 4
5, 6, 7, 8

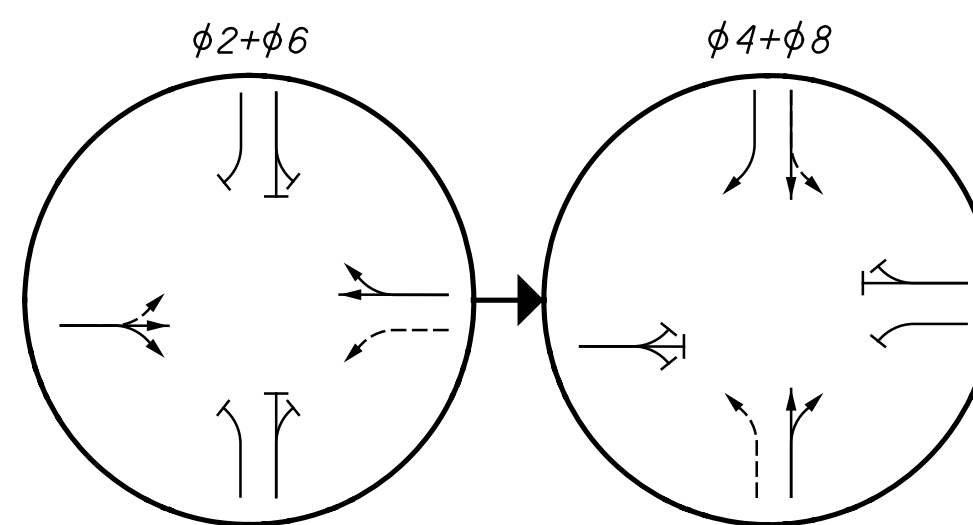
TEMPORARY FIELD WIRING HOOK-UP CHART

SIGNAL HEAD	INDICATION	FIELD TERMINAL	FLASH
1,2	R	Ø6 R	R
	Y	Ø6 Y	
	G	Ø6 G	
3,4	R	Ø8 R	R
	Y	Ø8 Y	
	G	Ø8 G	
5,6	R	Ø2 R	R
	Y	Ø2 Y	
	G	Ø2 G	
7,8	R	Ø4 R	R
	Y	Ø4 Y	
	G	Ø4 G	

TEMPORARY SIGNAL SPAN DIAGRAM

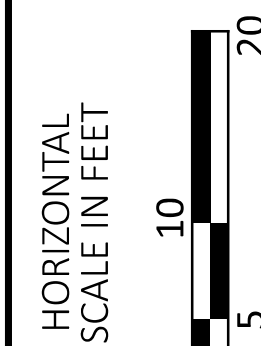


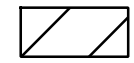



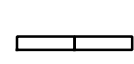


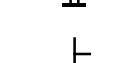
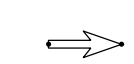











TEMPORARY PHASE DIAGRAM

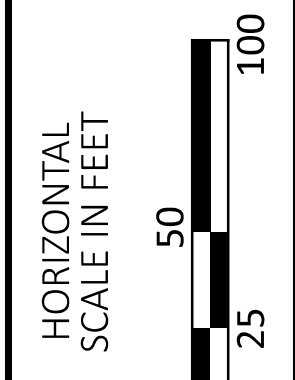
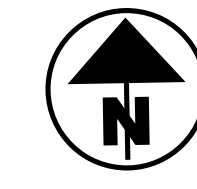
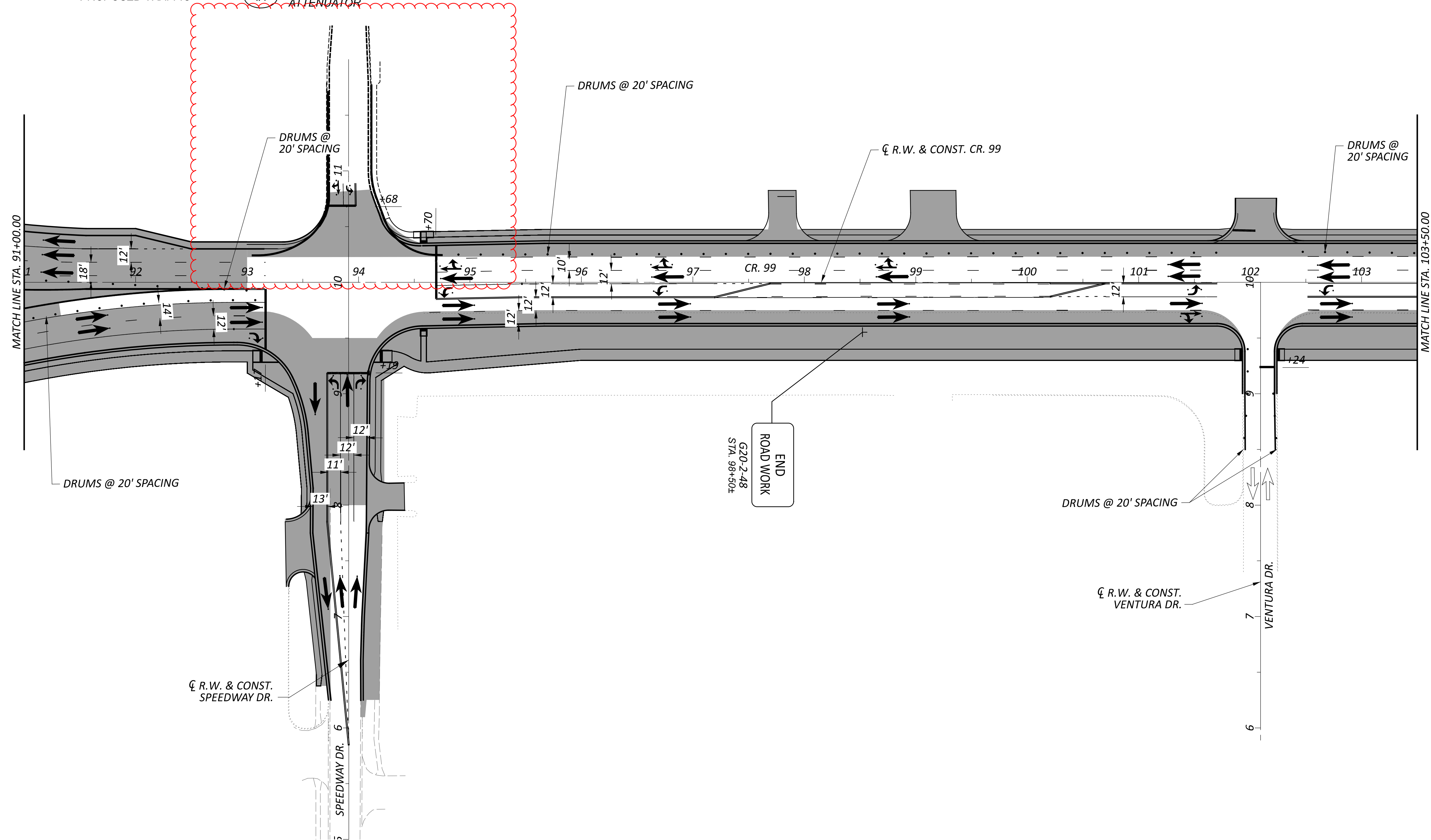


TEMPORARY SIGNAL TIMING CHART

INTERSECTION: CR99 & SPEEDWAY DR								
MAINTAINING AGENCY:								
START UP	DUAL ENTRY:		PHASES:					
	REST IN RED:	RING 1	RING 2					
START IN:	-	-	-	-	A	B	C	D
TIME FOR: FLASH, ALL RED (SEC.):	9, 6	OVERLAP						
FIRST PHASE(S):	2 & 6	PHASES						
COLOR DISPLAYED:	-							
INTERVAL OR FEATURE	CONTROLLER MOVEMENT NO.							
INTERSECTION MOVEMENT (PHASE)	1	2	3	4	5	6	7	8
DIRECTION	-	EB	-	SB	-	WB	-	NB
MINIMUM GREEN (INITIAL) (SEC.)	-	30	-	20	-	30	-	20
ADDED INITIAL *(SEC./ACTUATION)	-	-	-	-	-	-	-	-
MAXIMUM INITIAL *(SEC.)	-	-	-	-	-	-	-	-
PASSAGE TIME (PRESET GAP) (SEC.)	-	3	-	3	-	3	-	3
TIME BEFORE REDUCTION *(SEC.)	-	-	-	-	-	-	-	-
MINIMUM GAP *(SEC.)	-	-	-	-	-	-	-	-
TIME TO REDUCE *(SEC.)	-	-	-	-	-	-	-	-
MAXIMUM GREEN I (SEC.)	-	60	-	55	-	60	-	55
MAXIMUM GREEN II (SEC.)	-	60	-	55	-	60	-	55
YELLOW CHANGE (SEC.)	-	3.4	-	3.4	-	3.4	-	3.4
ALL RED CLEARANCE (SEC.)	-	1.4	-	1.0	-	1.4	-	1.0
DELAYED GREEN (LPI) * (SEC.)	-	-	-	-	-	-	-	-
FLASHING YELLOW ARROW DELAY^ (SEC.)	-	-	-	-	-	-	-	-
WALK (SEC.)	-	-	-	-	-	-	-	-
PEDESTRIAN CLEARANCE (SEC.)	-	-	-	-	-	-	-	-
RECALL	MAXIMUM (ON/OFF)	-	-	-	-	-	-	-
	MINIMUM (ON/OFF)	-	ON	-	-	ON	-	-
	PEDESTRIAN (ON/OFF)	-	-	-	-	-	-	-
MEMORY (ON/OFF)	-	-	-	-	-	-	-	-



- LEGEND**
-  WORK AREA
 -  CONSTRUCTED IN PREVIOUS PHASE
 -  TEMPORARY PAVEMENT
 -  TEMPORARY PAVEMENT CONST. IN PREVIOUS PHASE
 -  PORTABLE CONCRETE BARRIER
 -  DRUMS
 -  IMPACT ATTENUATOR
 -  TYPE III BARRICADE
 -  SIGN POST
 -  DIRECTION OF EXISTING TRAFFIC
 -  DIRECTION OF PROPOSED TRAFFIC
 -  CL WORKZONE CENTER LINE (DOUBLE YELLOW)
 -  ELY WORKZONE EDGE LINE (YELLOW)
 -  ELW WORKZONE EDGE LINE (WHITE)
 -  CH WORKZONE CHANNELIZING LINE (WHITE)
 -  LL WORKZONE LANE LINE (WHITE)
 -  DL WORKZONE DOTTED LINE (WHITE)
 -  SL WORKZONE STOP LINE
 -  PB PORTABLE CONCRETE BARRIER, 32"
 -  IA WORK ZONE IMPACT ATTENUATOR



**MAINTENANCE OF TRAFFIC
 PHASE 3 CR 99**

DESIGN AGENCY



DESIGNER
 CO

REVIEWER
 KF 05/20/22

PROJECT ID
 102375

SHEET TOTAL
 117 705

SHEET NUM.											PART.				ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
31	32	33	130	144	149	154	463	464	489		01/SK5/03	02/IMS/08	03/IMS/13	04/SK5/04						
			25								25				626	00102	25	EACH	ROADWAY CONT.	
			35								35				626	00110	35	EACH	BARRIER REFLECTOR, TYPE 1 (ONE WAY)	
			4											4	SPECIAL	69050350	4	EACH	BARRIER REFLECTOR, TYPE 2 (ONE WAY)	
		1,250									1,250				SPECIAL	69065016	1,250	TON	MAILBOX REMOVED AND RESET	
																			WORK INVOLVING PETROLEUM CONTAMINATED SOIL	
	5			44				4	5		38			20	601	32200	58	CY	EROSION CONTROL	
				188							188				601	37500	188	FT	ROCK CHANNEL PROTECTION, TYPE C WITH FILTER	
2											2				659	00100	2	EACH	PAVED GUTTER, TYPE 1-2	
14,630											14,630				659	00300	14,630	CY	SOIL ANALYSIS TEST	
131,802									5,912		131,802			5,912	659	10000	137,714	SY	TOPSOIL	
																			SEEDING AND MULCHING	
6,590											6,590				659	14000	6,590	SY	REPAIR SEEDING AND MULCHING	
6,590											6,590				659	15000	6,590	SY	INTER-SEEDING	
18.39											18.39				659	20000	18.39	TON	COMMERCIAL FERTILIZER	
27.23											27.23				659	31000	27.23	ACRE	LIME	
730											730				659	35000	730	MGAL	WATER	
297											297				659	40000	297	MSF	MOWING	
									4,710					4,710	670	00500	4,710	SY	SLOPE EROSION PROTECTION	
				235							235				670	00720	235	SY	DITCH EROSION PROTECTION MAT, TYPE B	
											LS				832	15000	LS		STORM WATER POLLUTION PREVENTION PLAN	
											LS				832	15002	LS		STORM WATER POLLUTION PREVENTION INSPECTIONS	
											LS				832	15010	LS		STORM WATER POLLUTION PREVENTION INSPECTION SOFTWARE	
											300,000				832	30000	300,000	EACH	EROSION CONTROL	
	4					44					48				601	21050	48	SY	DRAINAGE	
															601	21060	60	SY	TIED CONCRETE BLOCK MAT WITH TYPE 1 UNDERLAYMENT	
					22.1		1.4				16.5				602	20000	25.2	CY	TIED CONCRETE BLOCK MAT WITH TYPE 2 UNDERLAYMENT	
						857					564				605	05200	857	FT	CONCRETE MASONRY	
						628					628				605	11100	628	FT	4" UNCLASSIFIED PIPE UNDERDRAINS	
																			6" SHALLOW PIPE UNDERDRAINS, (24" DEEP)	
						10,238					10,238				605	11100	10,238	FT	6" SHALLOW PIPE UNDERDRAINS, (30" DEEP)	
	100					1,599					1,699				605	13300	1,699	FT	6" UNCLASSIFIED PIPE UNDERDRAINS	
						18,163					18,163				605	14000	18,163	FT	6" BASE PIPE UNDERDRAINS	
						489					288			201	605	31100	489	FT	AGGREGATE DRAINS	
						413					413				611	00510	413	FT	6" CONDUIT, TYPE F FOR UNDERDRAIN OUTLETS	
	100										100				611	00900	100	FT	6" CONDUIT, TYPE B	
	100										100				611	01400	100	FT	6" CONDUIT, TYPE E	
	200										200				611	01500	200	FT	6" CONDUIT, TYPE F	
					1,254						1,253			1	611	01500	1,254	FT	6" CONDUIT, TYPE F, 707.33	
							70				70				611	01800	70	FT	8" CONDUIT, TYPE B, 707.70	
					10									10	611	02000	10	FT	8" CONDUIT, TYPE C	
					1,512						1,507			5	611	04400	1,512	FT	12" CONDUIT, TYPE B	
					951						800			151	611	04600	951	FT	12" CONDUIT, TYPE C	
					1,433						1,276			157	611	05900	1,433	FT	15" CONDUIT, TYPE B	
					843						655			188	611	06100	843	FT	15" CONDUIT, TYPE C	
						118					118				611	06700	118	FT	15" CONDUIT, TYPE F 707.05, TYPE C 707.21 OR 707.33	
					249						249				611	07400	249	FT	18" CONDUIT, TYPE B	
					703						628			75	611	07600	703	FT	18" CONDUIT, TYPE C	
					80						80				611	08900	80	FT	21" CONDUIT, TYPE B	
					1,095						1,095				611	09100	1,095	FT	21" CONDUIT, TYPE C	
					417						417				611	10400	417	FT	24" CONDUIT, TYPE B	
					706				5		689			22	611	10600	711	FT	24" CONDUIT, TYPE C	
					14									14	611	11900	14	FT	27" CONDUIT, TYPE B	
					427						427				611	12100	427	FT	27" CONDUIT, TYPE C	
					46						46				611	13600	46	FT	30" CONDUIT, TYPE C	
					205						205				611	16200	205	FT	36" CONDUIT, TYPE A, 706.02, 707.21, 707.22, 707.33, 707.85	
							241				241				611	16200	241	FT	36" CONDUIT, TYPE A, 706.02, 707.01, 707.02, 707.03 (0.188), 707.33	
					1,022						1,022				611	16400	1,022	FT	36" CONDUIT, TYPE B	
					2,108						2,132				611	16600	2,132	FT	36" CONDUIT, TYPE C	

GENERAL SUMMARY

DESIGN AGENCY



DESIGNER
MJL

REVIEWER
PHF 11/22/22

PROJECT ID
102375

SHEET TOTAL
120 | 705

HAN-75/CR99 INTERCHANGE REHAB

MODEL: GENERAL SUMMARY 7 PAPER SIZE: 34x22 (in.) DATE: 2/15/2024 TIME: 12:48:42 PM USER: MLORENZ
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SHEET NUM.						PART.				ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
36	37	38	39	40	47	01/SK5/03	02/IMS/08	03/IMS/13	04/SK5/04						
		525				525				254	01000	525	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 1.5"	
250						250				410	12000	250	CY	TRAFFIC COMPACTED SURFACE, TYPE A OR B	
			400			400				614	11110	400	HOUR	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	
				4		4				SPECIAL	61411300	4	EACH	WORK ZONE TRAFFIC SIGNAL	40
					26	26				614	12380	26	EACH	WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS, (UNIDIRECTIONAL)	
25						LS				614	12420	LS		DETOUR SIGNING	
						25				614	12500	25	EACH	REPLACEMENT SIGN	
		4,920				4,920				614	12800	4,920	EACH	WORK ZONE RAISED PAVEMENT MARKER	
		700				700				614	13310	700	EACH	BARRIER REFLECTOR, TYPE 1 (ONE WAY)	
		700				700				614	13350	700	EACH	OBJECT MARKER, ONE WAY	
	96					96				614	18601	96	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN	37
					1.97	1.97				614	20100	1.97	MILE	WORK ZONE LANE LINE, CLASS 1, 4", 642 PAINT	
					1.86	1.86				614	21100	1.86	MILE	WORK ZONE CENTER LINE, CLASS I, 642 PAINT	
					12.2	11.75			0.45	614	22100	12.2	MILE	WORK ZONE EDGE LINE, CLASS I, 4", 642 PAINT, WHITE	
					8.18	7.73			0.45	614	22100	8.18	MILE	WORK ZONE EDGE LINE, CLASS I, 4", 642 PAINT, YELLOW	
					55,521	52,973			2,548	614	23200	55,521	FT	WORK ZONE CHANNELIZING LINE, CLASS I, 8", 642 PAINT	
					8,366	8,044			322	614	24200	8,366	FT	WORK ZONE DOTTED LINE, CLASS I, 4", 642 PAINT	
					853	776			77	614	26200	853	FT	WORK ZONE STOP LINE, CLASS I, 642 PAINT	
4,350		11				11				614	40050	11	EACH	BUSINESS ENTRANCE SIGN	
200						4,350				615	20001	4,350	SY	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS A, AS PER PLAN	40
						200				616	10000	200	MGAL	WATER	
					29,031	29,031				622	41100	29,031	FT	PORTABLE BARRIER, UNANCHORED	
					2	2				622	41060	2	EACH	DUAL PORTABLE BARRIER TRANSITION/TERMINATION	
						LS				614	11000	LS		INCIDENTALS	
						24				619	16020	24	MNTH	MAINTAINING TRAFFIC	
						LS				623	10000	LS		FIELD OFFICE, TYPE C	
						LS				623	11000	LS		CONSTRUCTION LAYOUT STAKES AND SURVEYING	
						LS				624	10000	LS		PROVIDING ELECTRONIC INSTRUMENTATION	
						LS								MOBILIZATION	

GENERAL SUMMARY

DESIGN AGENCY



DESIGNER

MJL

REVIEWER

PHF 11/22/22

PROJECT ID

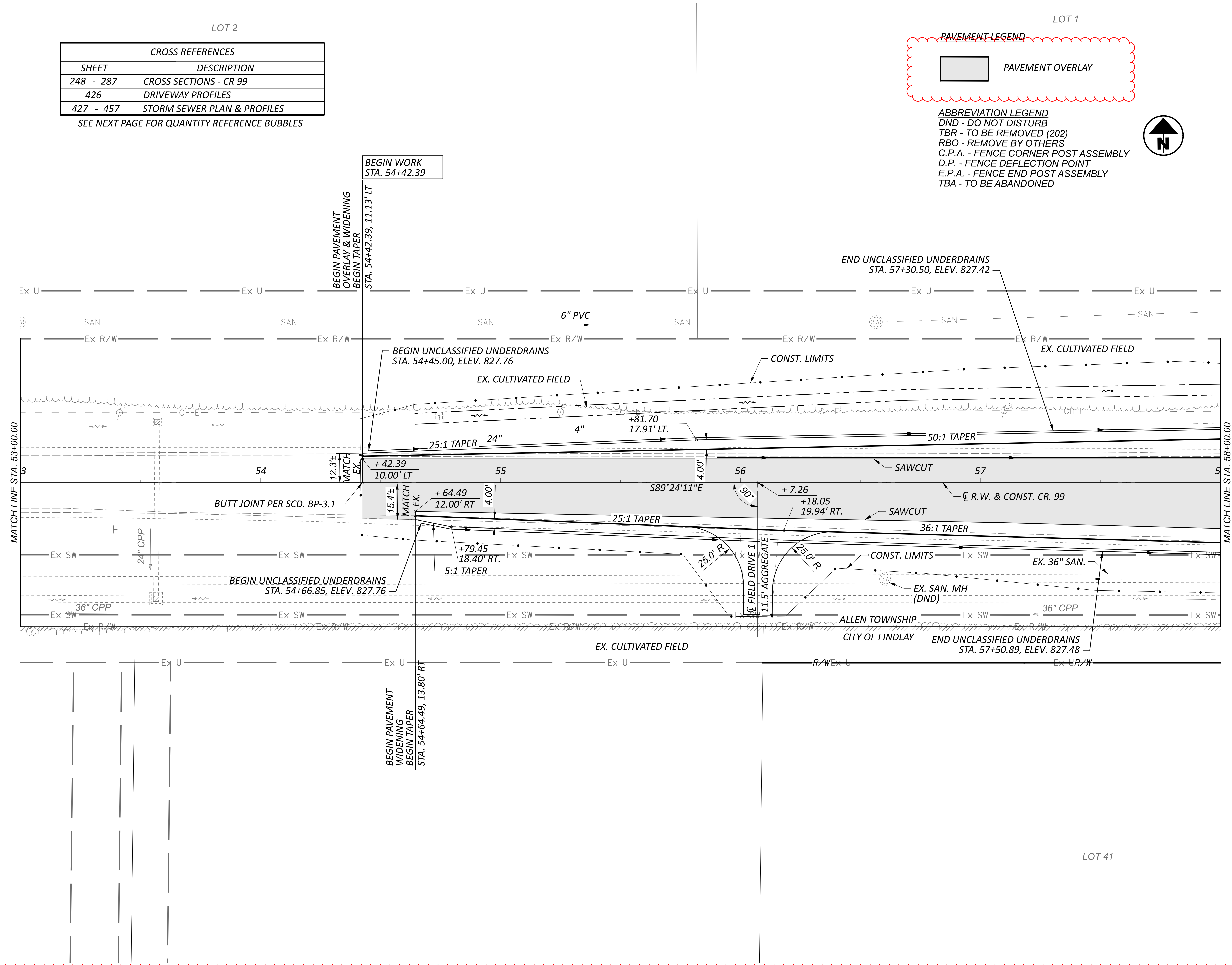
102375

SHEET TOTAL

125 | 705

CROSS REFERENCES	
SHEET	DESCRIPTION
248 - 287	CROSS SECTIONS - CR 99
426	DRIVEWAY PROFILES
427 - 457	STORM SEWER PLAN & PROFILES

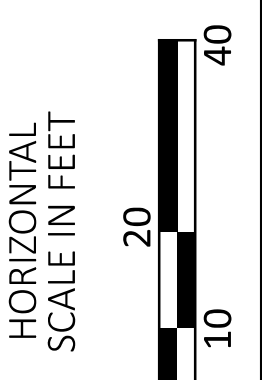
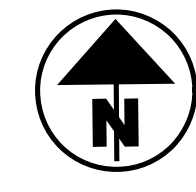
SEE NEXT PAGE FOR QUANTITY REFERENCE BUBBLES



PAVEMENT LEGEND

PAVEMENT OVERLAY

ABBREVIATION LEGEND
 DND - DO NOT DISTURB
 TBR - TO BE REMOVED (202)
 RBO - REMOVE BY OTHERS
 C.P.A. - FENCE CORNER POST ASSEMBLY
 D.P. - FENCE DEFLECTION POINT
 E.P.A. - FENCE END POST ASSEMBLY
 TBA - TO BE ABANDONED



PLAN SHEET - CR 99
 STA. 53+00.00 TO STA. 58+00.00

DESIGN AGENCY

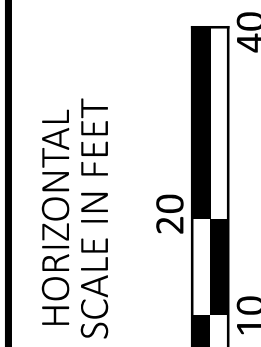
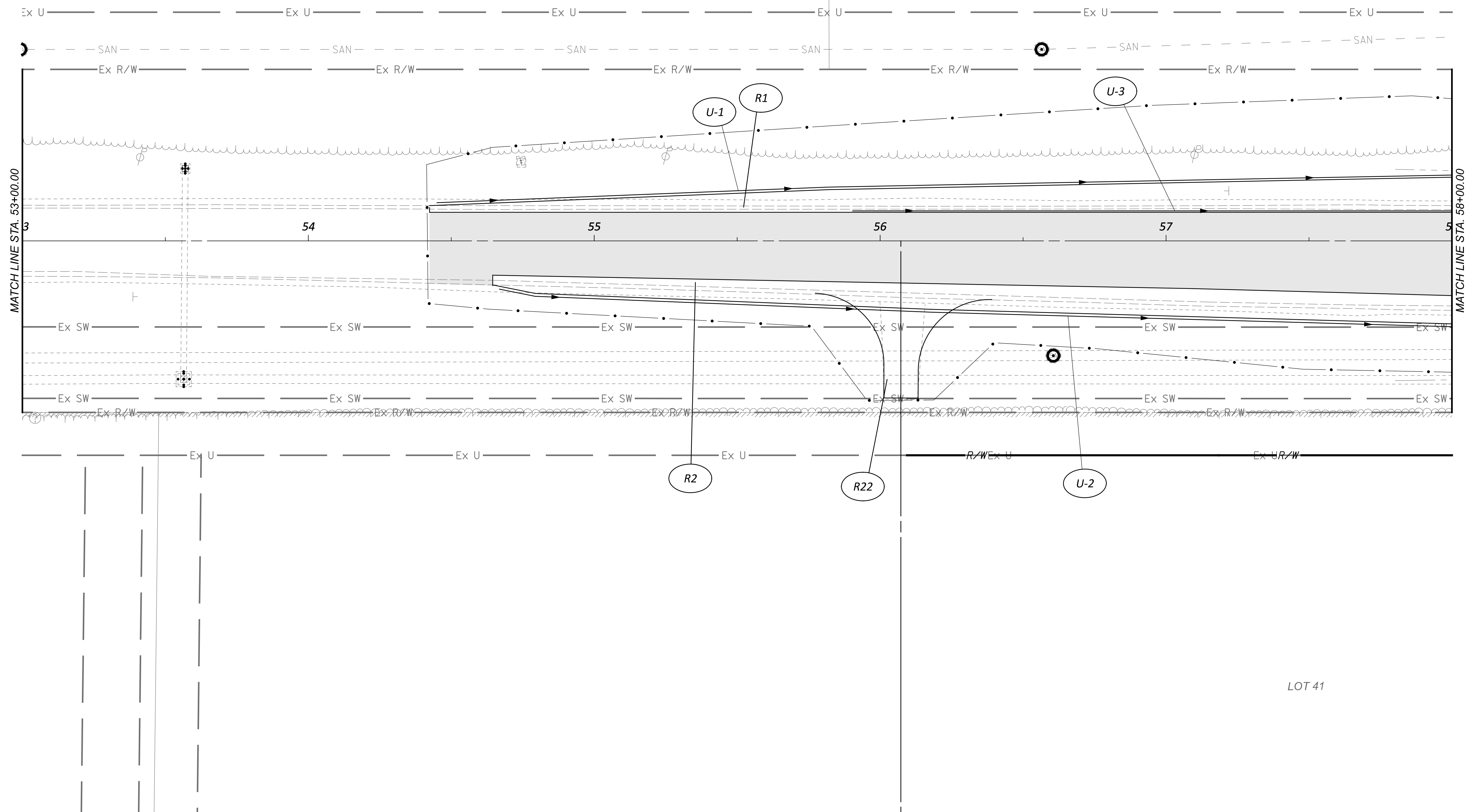


DESIGNER
 MJL

REVIEWER
 KF 05/20/22

PROJECT ID
 102375

SHEET TOTAL
 160 705



QUANTITY PLAN SHEET - CR 99
STA. 53+00.00 TO STA. 58+00.00

DESIGN AGENCY



DESIGNER

MJL

REVIEWER

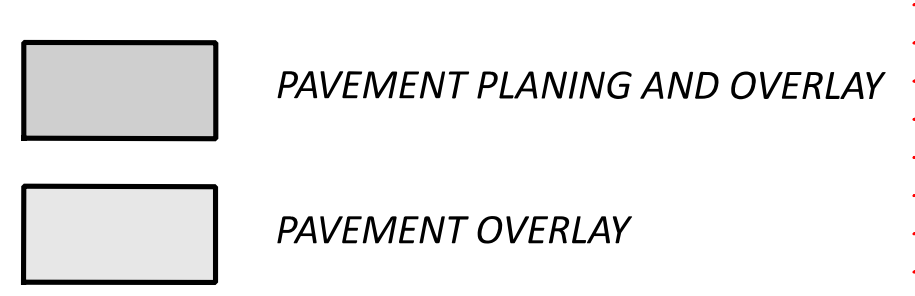
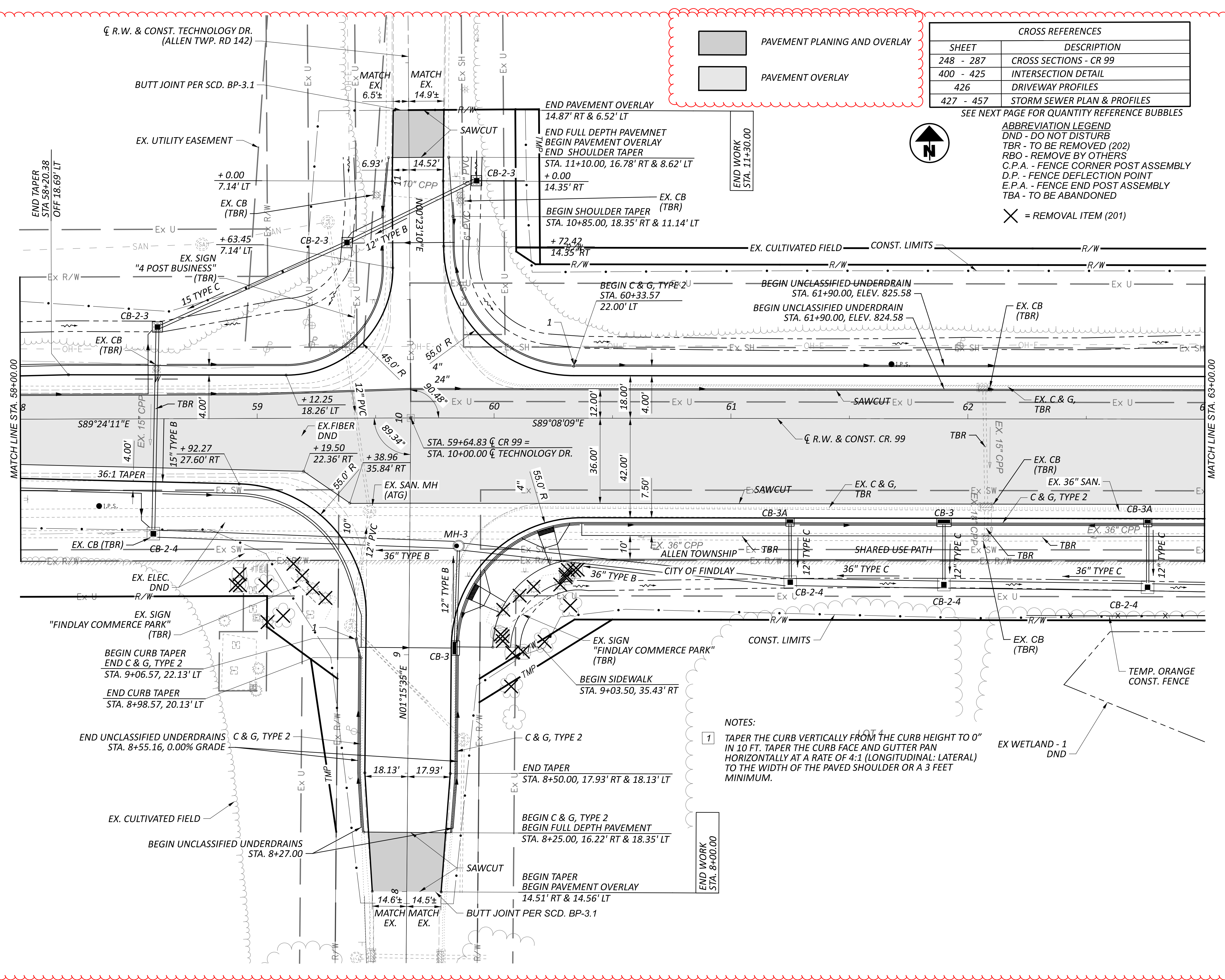
KF 05/20/22

PROJECT ID

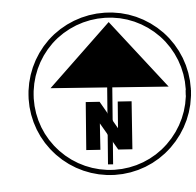
102375

SHEET TOTAL

161 705

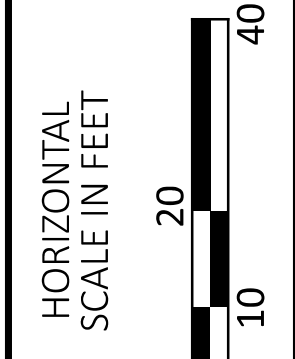


CROSS REFERENCES	
SHEET	DESCRIPTION
248 - 287	CROSS SECTIONS - CR 99
400 - 425	INTERSECTION DETAIL
426	DRIVEWAY PROFILES
427 - 457	STORM SEWER PLAN & PROFILES



SEE NEXT PAGE FOR QUANTITY REFERENCE BUBBLES

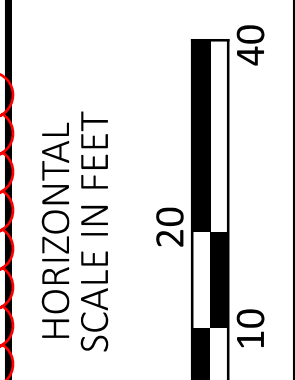
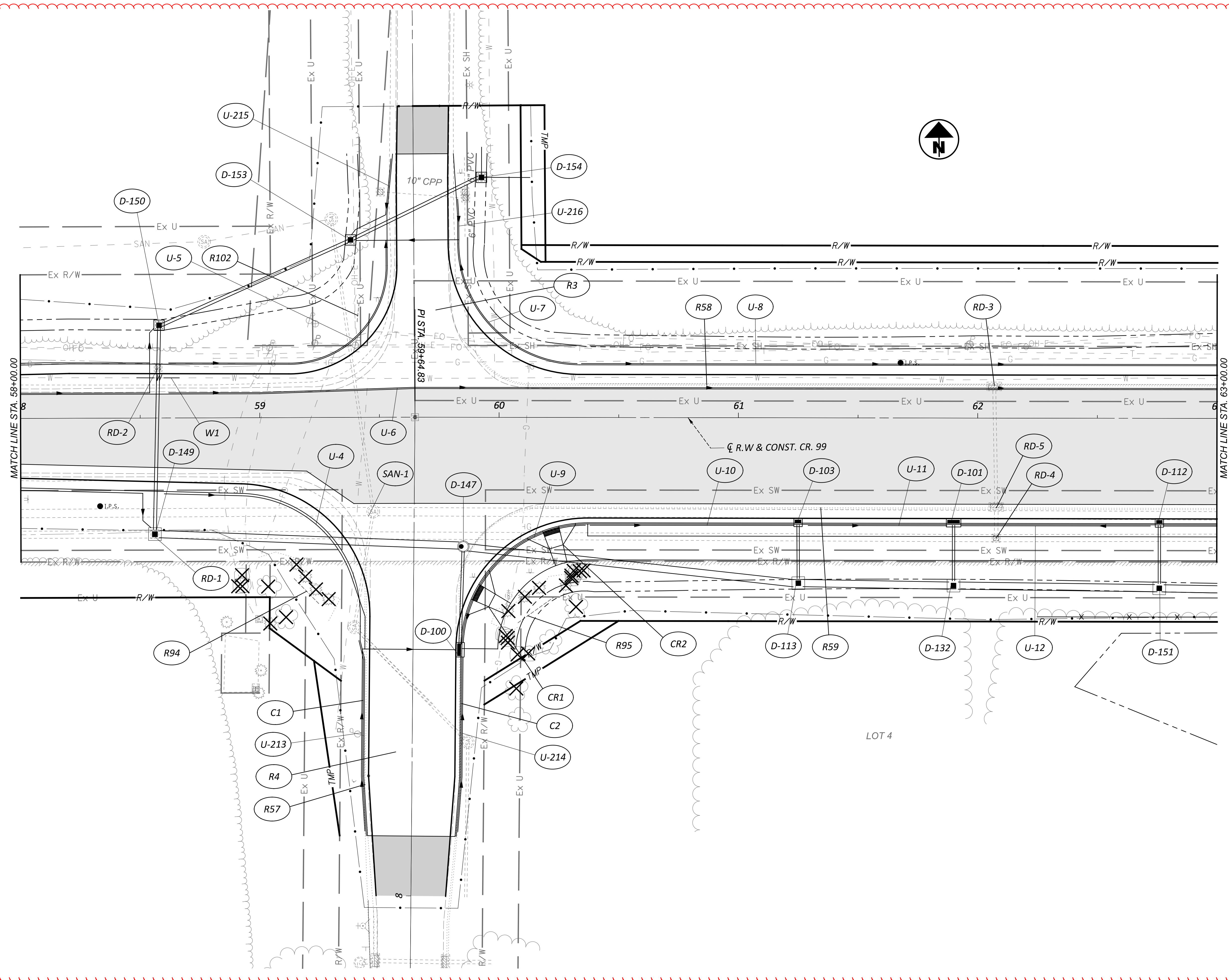
ABBREVIATION LEGEND
 DND - DO NOT DISTURB
 TBR - TO BE REMOVED (202)
 RBO - REMOVE BY OTHERS
 C.P.A. - FENCE CORNER POST ASSEMBLY
 D.P. - FENCE DEFLECTION POINT
 E.P.A. - FENCE END POST ASSEMBLY
 TBA - TO BE ABANDONED
 X = REMOVAL ITEM (201)



PLAN SHEET - CR 99
 STA. 58+00.00 TO STA. 63+00.00

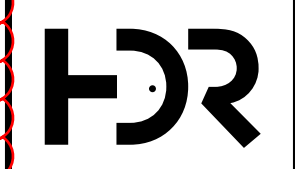
NOTES:
 1 TAPER THE CURB VERTICALLY FROM THE CURB HEIGHT TO 0" IN 10 FT. TAPER THE CURB FACE AND GUTTER PAN HORIZONTALLY AT A RATE OF 4:1 (LONGITUDINAL: LATERAL) TO THE WIDTH OF THE PAVED SHOULDER OR A 3 FEET MINIMUM.

DESIGN AGENCY	HR
DESIGNER	MJL
REVIEWER	KF 05/20/22
PROJECT ID	102375
SHEET	163
TOTAL	705

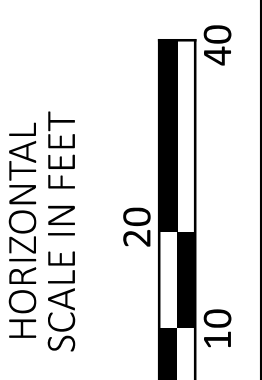
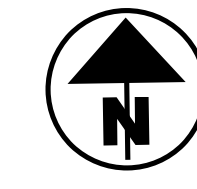
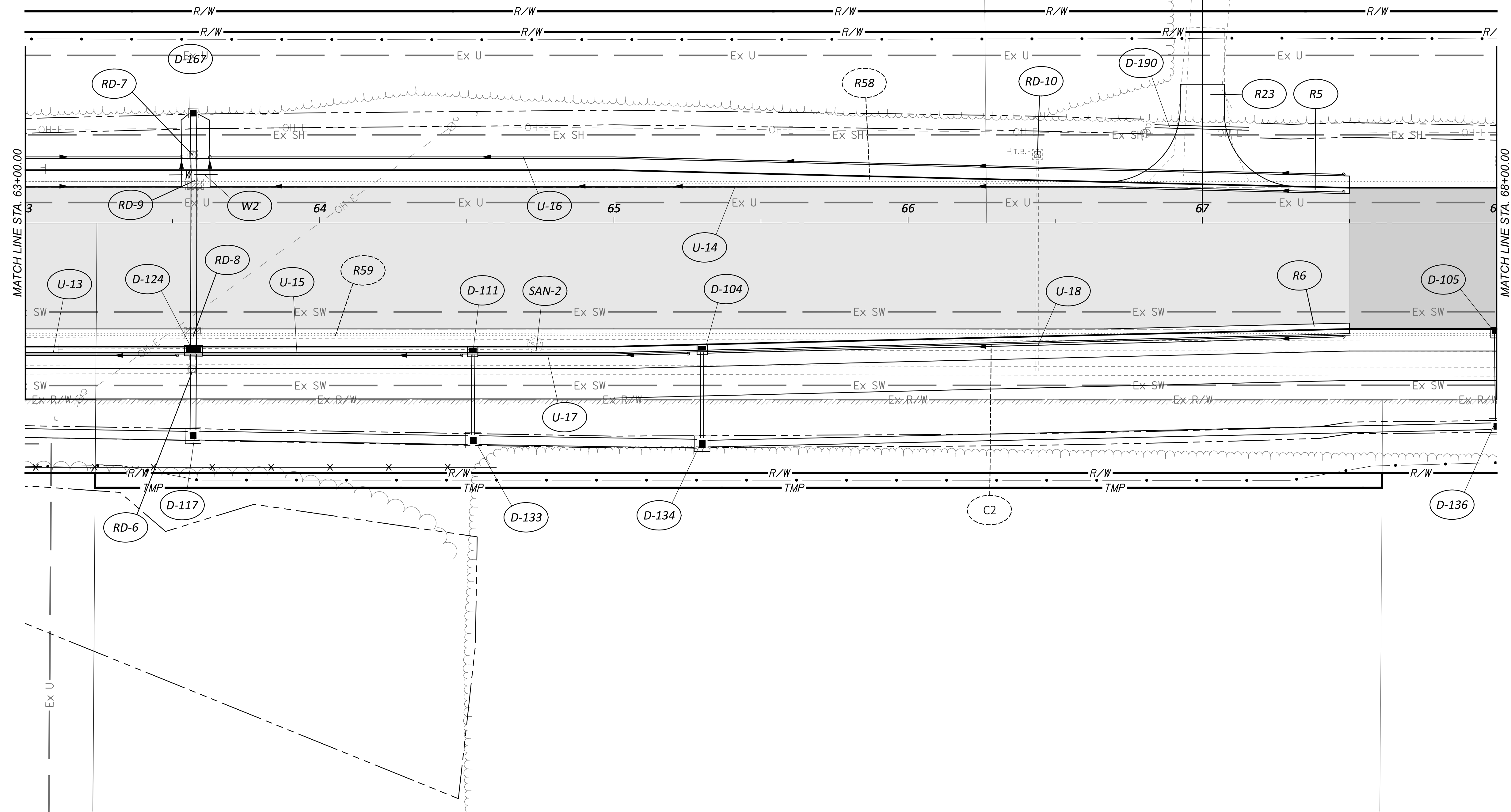


QUANTITY PLAN SHEET - CR 99
 STA. 58+00.00 TO STA. 63+00.00

DESIGN AGENCY



DESIGNER	MJL
REVIEWER	KF 05/20/22
PROJECT ID	102375
SHEET	164
TOTAL	705



QUANTITY PLAN SHEET - CR 99
STA. 63+00.00 TO STA. 68+00.00

DESIGN AGENCY



DESIGNER
MJL

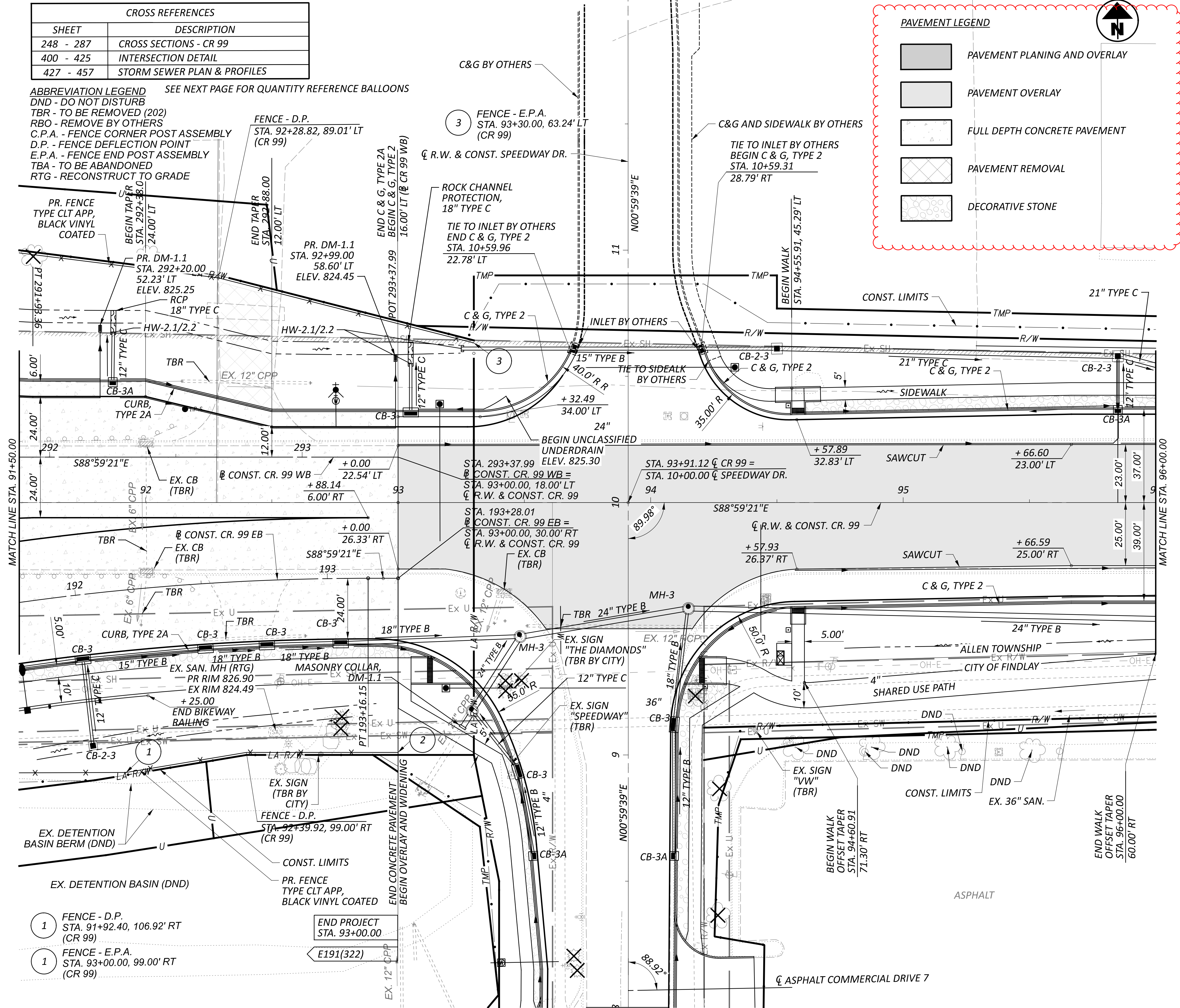
REVIEWER
KF 05/20/22

PROJECT ID
102375

SHEET	TOTAL
167	705

CROSS REFERENCES	
SHEET	DESCRIPTION
248 - 287	CROSS SECTIONS - CR 99
400 - 425	INTERSECTION DETAIL
427 - 457	STORM SEWER PLAN & PROFILES

ABBREVIATION LEGEND SEE NEXT PAGE FOR QUANTITY REFERENCE BALLOONS
 DND - DO NOT DISTURB
 TBR - TO BE REMOVED (202)
 RBO - REMOVE BY OTHERS
 C.P.A. - FENCE CORNER POST ASSEMBLY
 D.P. - FENCE DEFLECTION POINT
 E.P.A. - FENCE END POST ASSEMBLY
 TBA - TO BE ABANDONED
 RTG - RECONSTRUCT TO GRADE

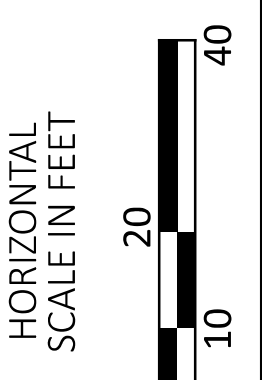


PAVEMENT LEGEND

- PAVEMENT PLANING AND OVERLAY
- PAVEMENT OVERLAY
- FULL DEPTH CONCRETE PAVEMENT
- PAVEMENT REMOVAL
- DECORATIVE STONE

- 1 FENCE - D.P.
STA. 91+92.40, 106.92' RT
(CR 99)
- 1 FENCE - E.P.A.
STA. 93+00.00, 99.00' RT
(CR 99)

END PROJECT
 STA. 93+00.00
 E191(322)



PLAN SHEET - CR 99
 STA. 91+50.00 TO STA. 96+00.00

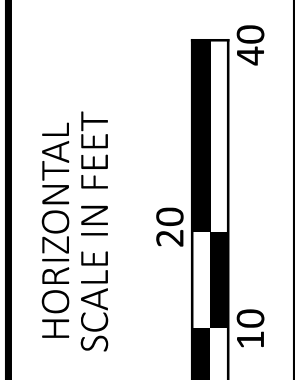
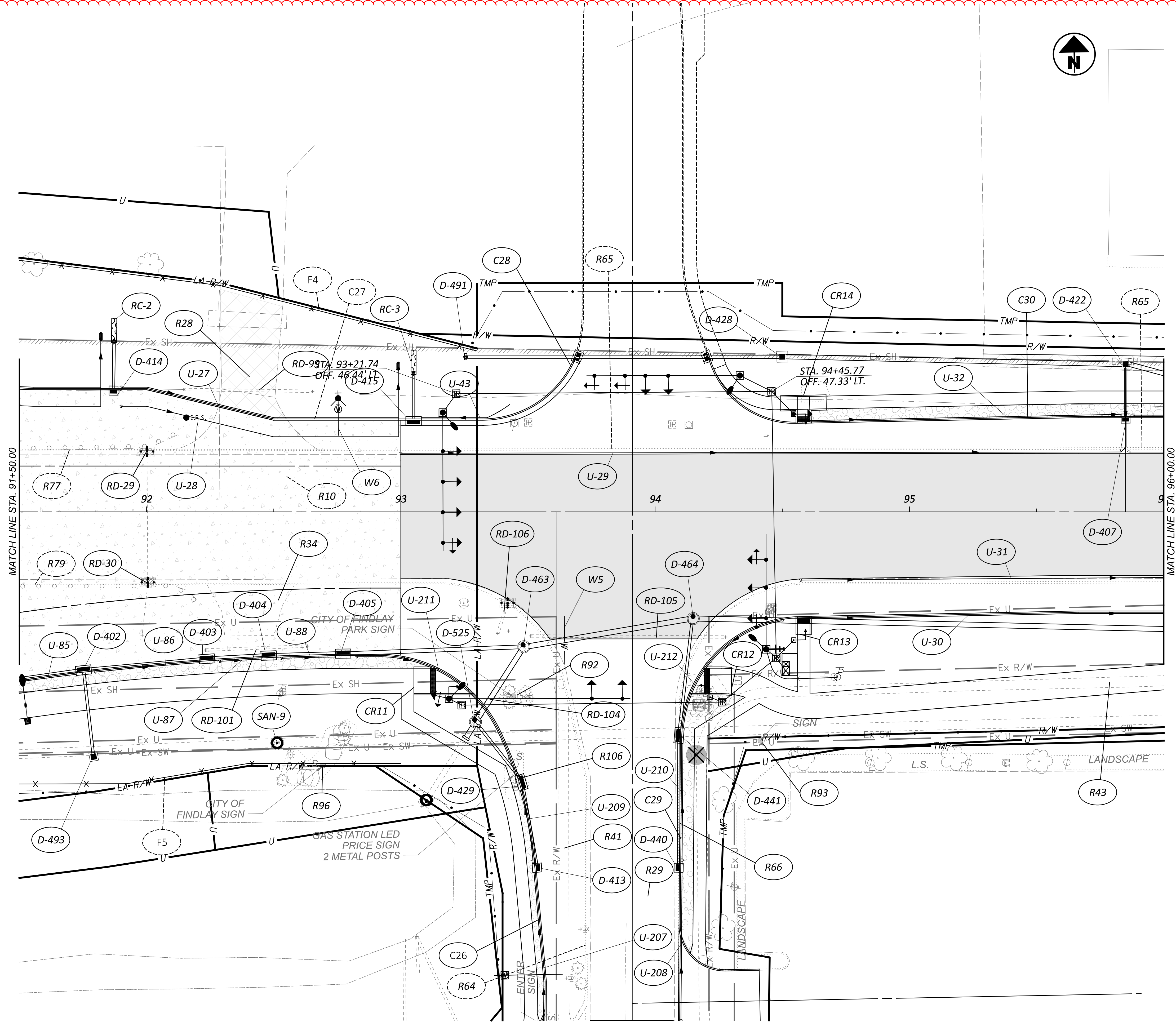
DESIGN AGENCY



DESIGNER	MJL
REVIEWER	KF 05/20/22
PROJECT ID	102375
SHEET	189
TOTAL	705

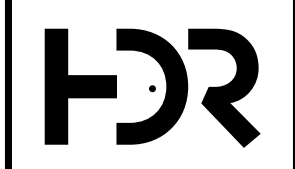
HAN-75/CR99 INTERCHANGE REHAB

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QUANTITY PLAN SHEET - CR 99
 STA. 91+50.00 TO STA. 96+00.00

DESIGN AGENCY



DESIGNER	MJL
REVIEWER	KF 05/20/22
PROJECT ID	102375
SHEET	190
TOTAL	705

CROSS REFERENCES	
SHEET	DESCRIPTION
248 - 287	CROSS SECTIONS - CR 99
426	DRIVEWAY PROFILES
427 - 457	STORM SEWER PLAN & PROFILES
500 - 502	WATER MAIN RELOCATION

SEE NEXT PAGE FOR QUANTITY REFERENCE BALLOONS

LOT 374 LOT 375 LOT 376 LOT 377

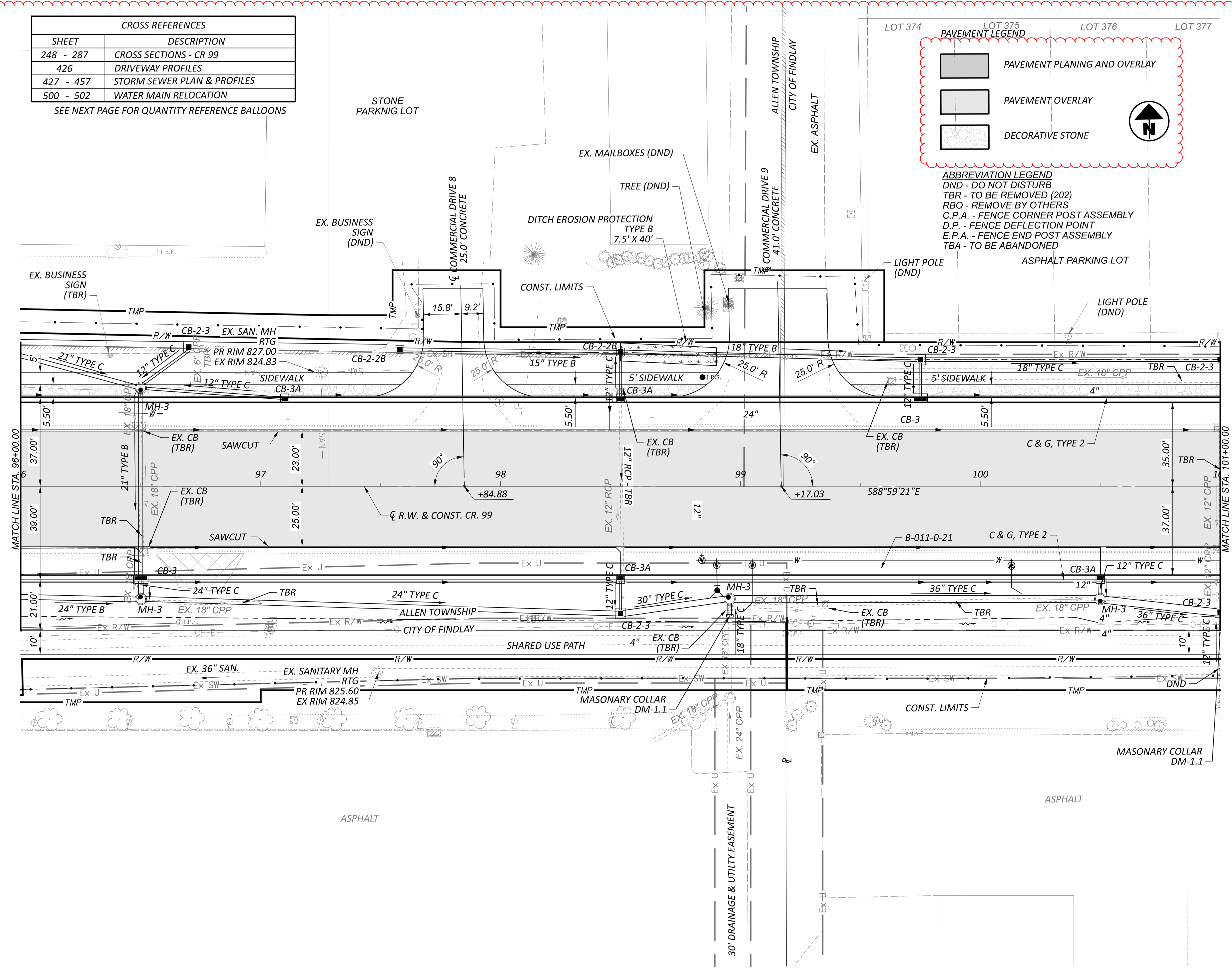
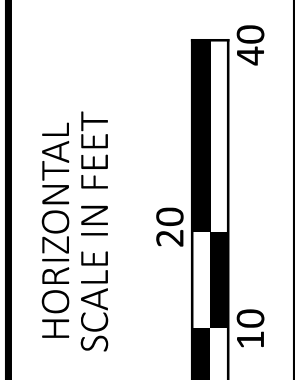
PAVEMENT LEGEND

- PAVEMENT PLANING AND OVERLAY
- PAVEMENT OVERLAY
- DECORATIVE STONE

ABBREVIATION LEGEND

- DND - DO NOT DISTURB
- TBR - TO BE REMOVED (202)
- RBO - REMOVE BY OTHERS
- C.P.A. - FENCE CORNER POST ASSEMBLY
- D.P. - FENCE DEFLECTION POINT
- E.P.A. - FENCE END POST ASSEMBLY
- TBA - TO BE ABANDONED

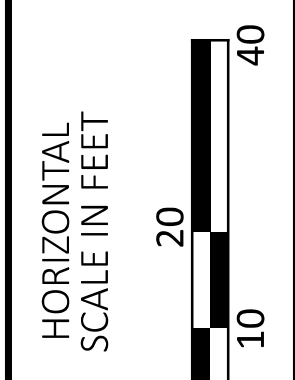
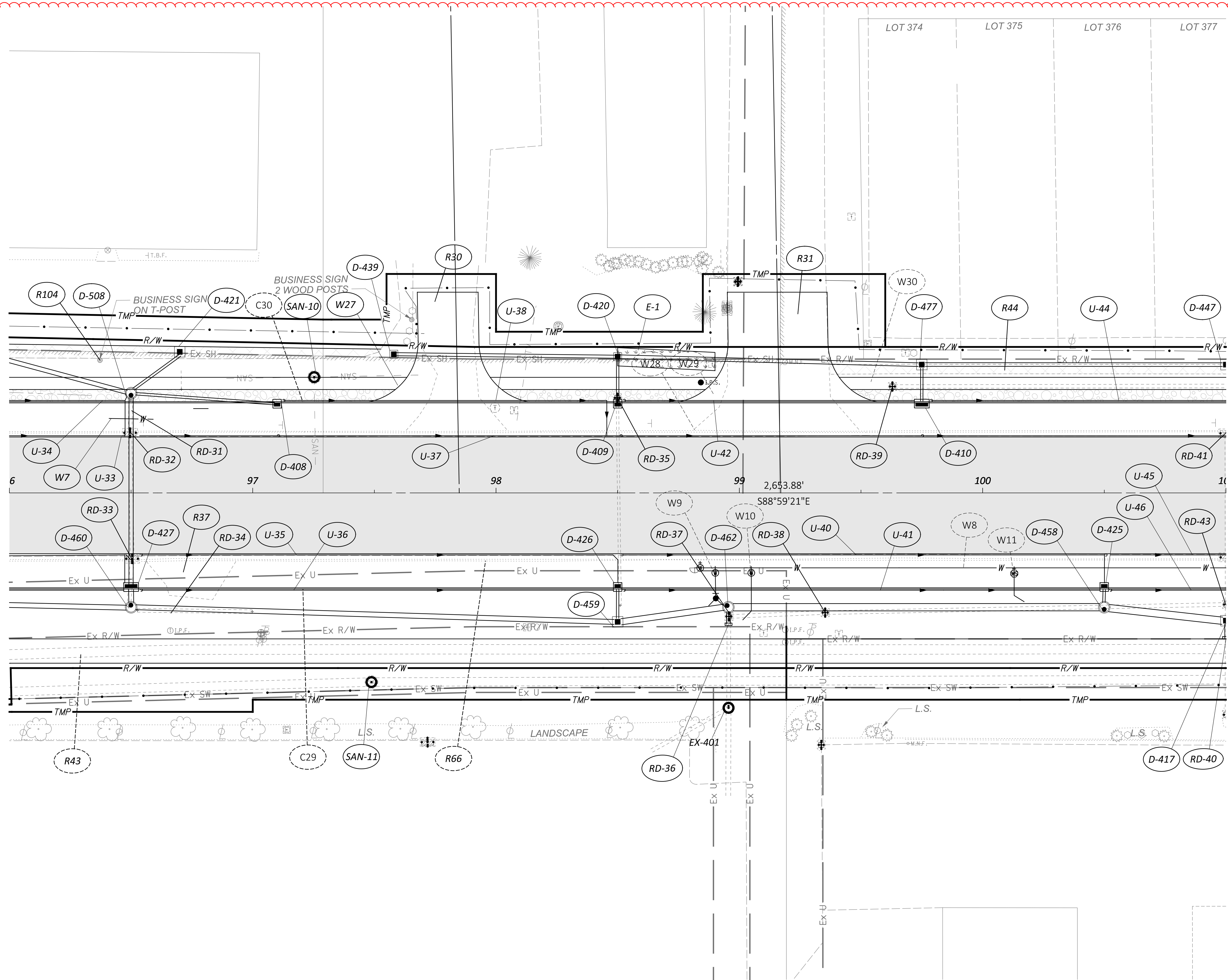
ASPHALT PARKING LOT



PLAN SHEET - CR 99
 STA. 96+00.00 TO STA. 101+00.00

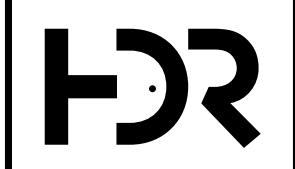
DESIGN AGENCY

DESIGNER: MJL
 REVIEWER: KF 05/20/22
 PROJECT ID: 102375
 SHEET: 194 TOTAL: 705



QUANTITY PLAN SHEET - CR 99
STA. 96+00.00 TO STA. 101+00.00

DESIGN AGENCY



DESIGNER	MJL
REVIEWER	KF 05/20/22
PROJECT ID	102375
SHEET	TOTAL
195	705

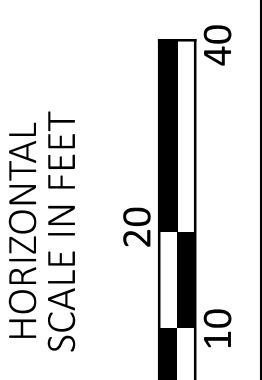
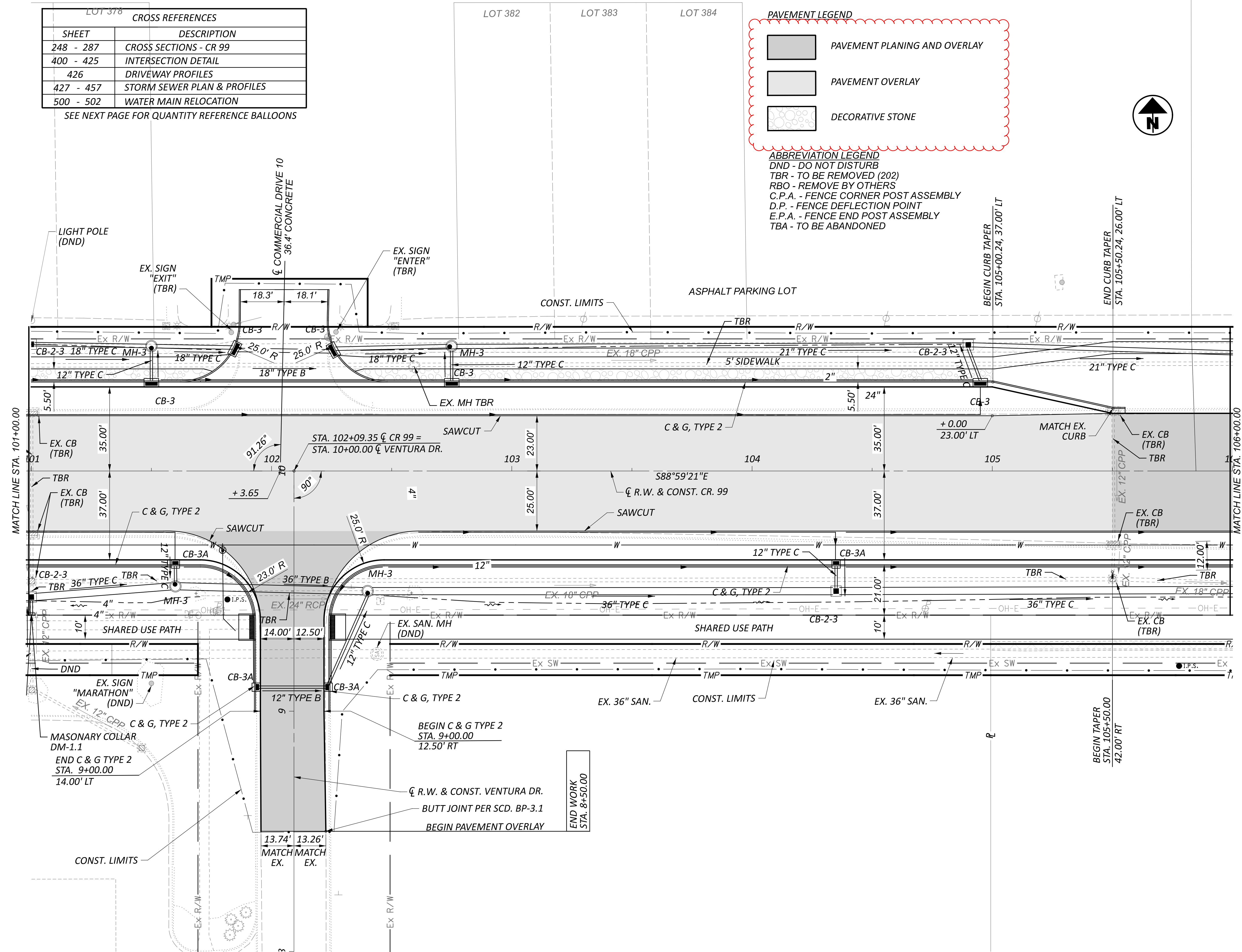
CROSS REFERENCES	
SHEET	DESCRIPTION
248 - 287	CROSS SECTIONS - CR 99
400 - 425	INTERSECTION DETAIL
426	DRIVEWAY PROFILES
427 - 457	STORM SEWER PLAN & PROFILES
500 - 502	WATER MAIN RELOCATION

SEE NEXT PAGE FOR QUANTITY REFERENCE BALLOONS

PAVEMENT LEGEND

- PAVEMENT PLANING AND OVERLAY
- PAVEMENT OVERLAY
- DECORATIVE STONE

ABBREVIATION LEGEND
 DND - DO NOT DISTURB
 TBR - TO BE REMOVED (202)
 RBO - REMOVE BY OTHERS
 C.P.A. - FENCE CORNER POST ASSEMBLY
 D.P. - FENCE DEFLECTION POINT
 E.P.A. - FENCE END POST ASSEMBLY
 TBA - TO BE ABANDONED

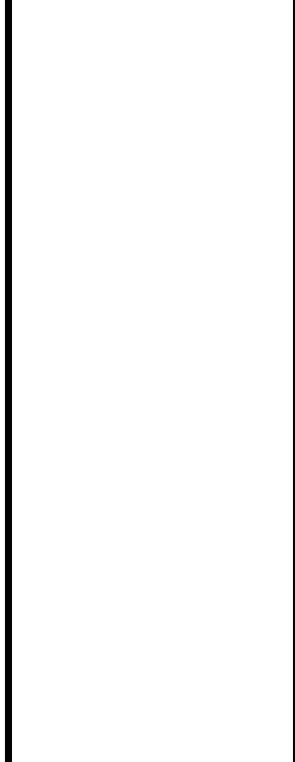
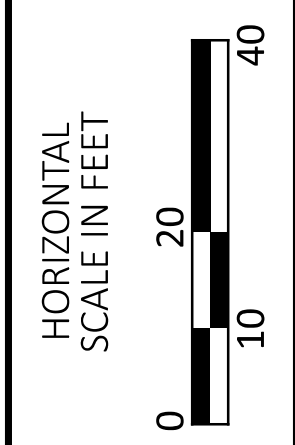
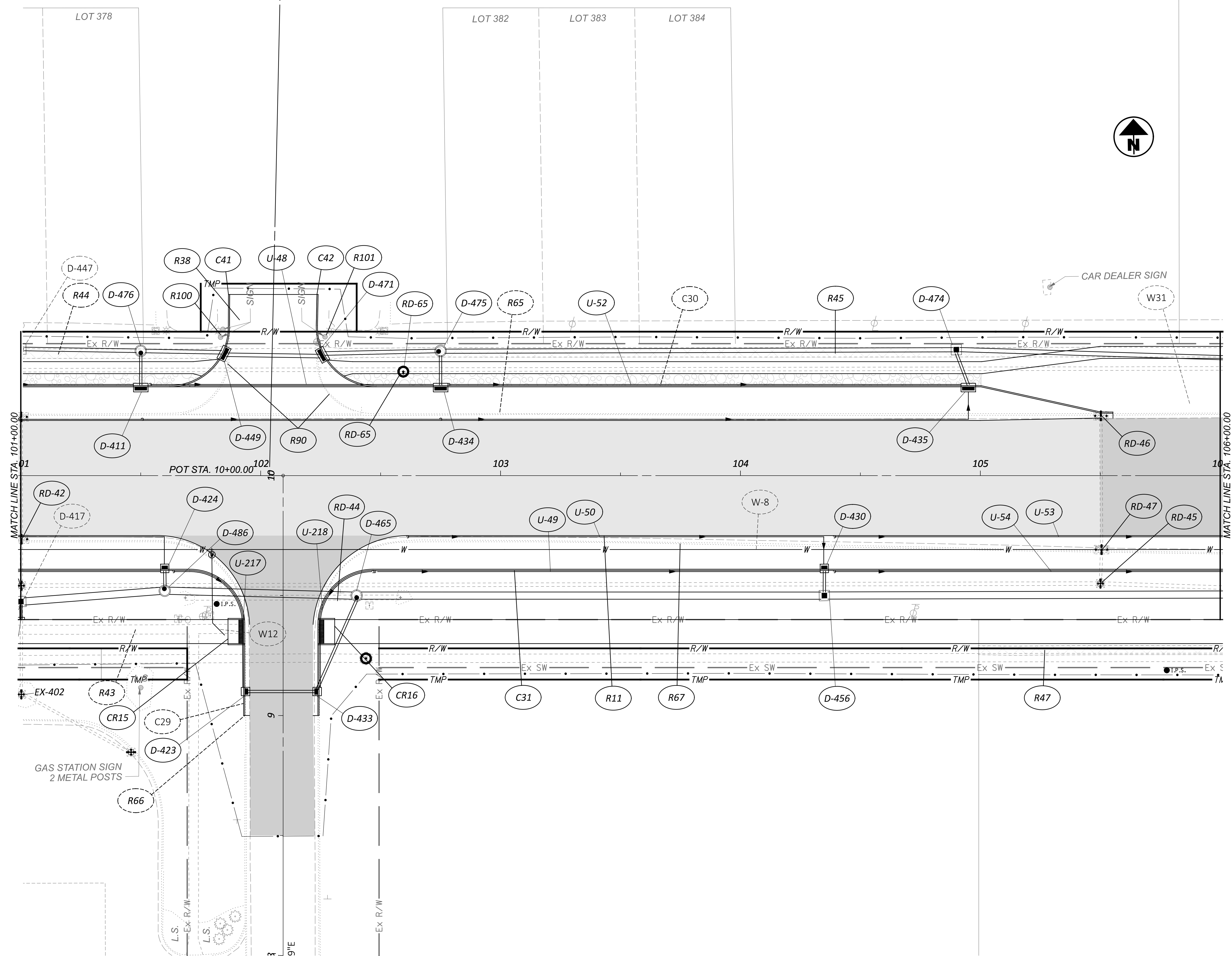


PLAN SHEET - CR 99
 STA. 101+00.00 TO STA. 106+00.00

DESIGN AGENCY

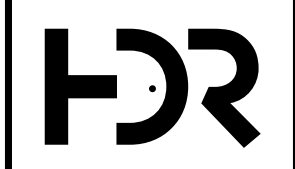


DESIGNER	MJL
REVIEWER	KF 05/20/22
PROJECT ID	102375
SHEET	197
TOTAL	705



QUANTITY PLAN SHEET - CR 99
STA. 101+00.00 TO STA. 106+00.00

DESIGN AGENCY



DESIGNER
MJL

REVIEWER
KF 05/20/22

PROJECT ID
102375




SHEET	TOTAL
198	705

CROSS REFERENCES	
SHEET	DESCRIPTION
325 - 340	CROSS SECTIONS - RAMP C & CC
427 - 457	STORM SEWER PLAN & PROFILES

CURVE 19
 CONST. RAMP C
 P.I. = Sta. 641+93.78
 $\Delta = 07^{\circ}02'11''$ LT
 $Dc = 03^{\circ}30'00''$
 $R = 1,637.00'$
 $T = 100.65'$
 $L = 201.04'$
 $E = 3.09'$
 $DS = 45$ MPH
 $eMAX = 0.043$

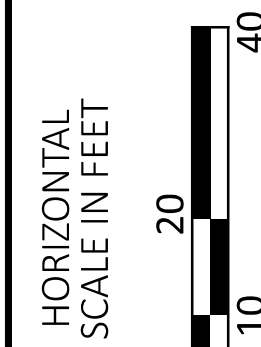
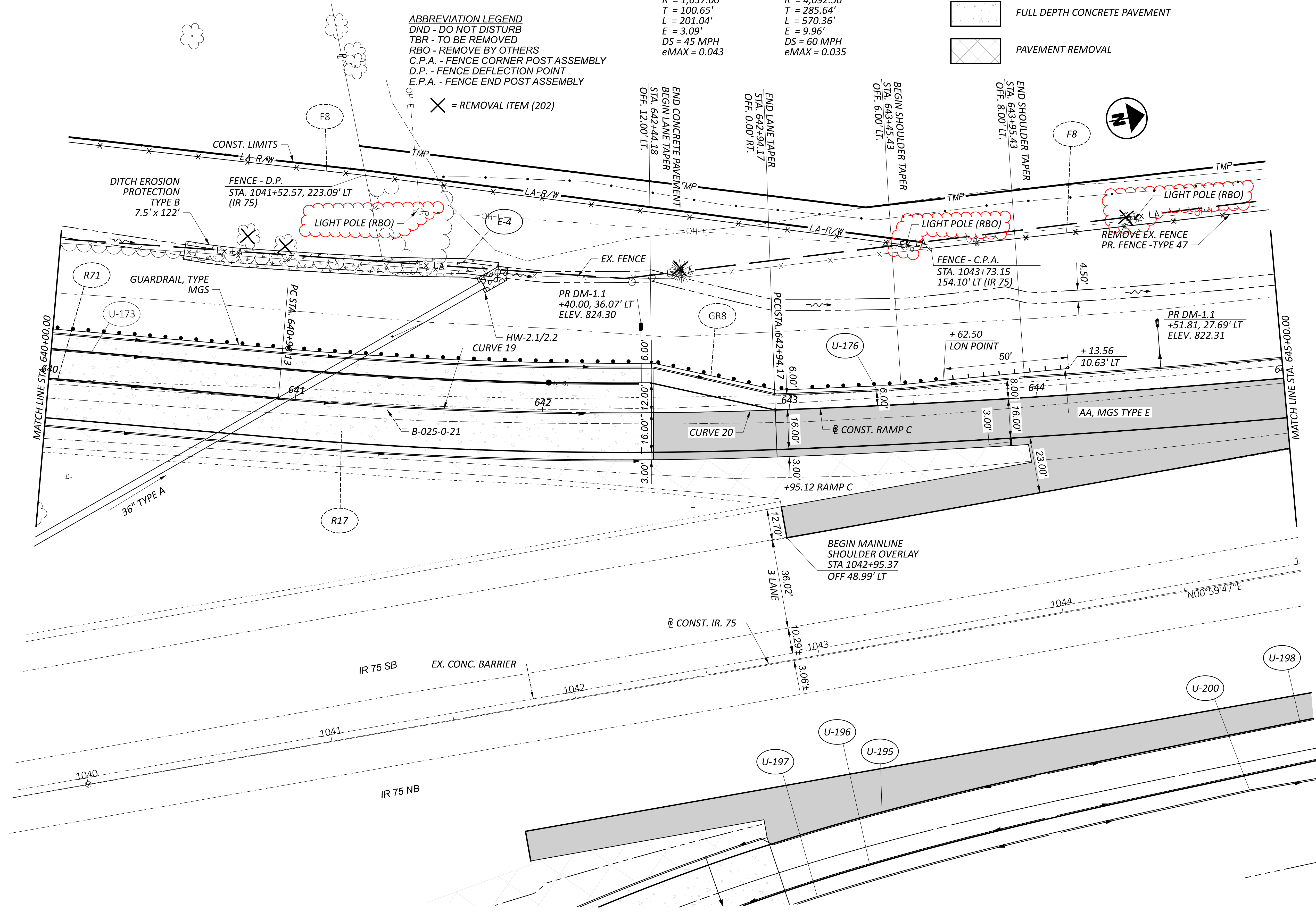
CURVE 20
 CONST. RAMP C
 P.I. = Sta. 645+79.82
 $\Delta = 07^{\circ}59'06''$ LT
 $Dc = 01^{\circ}24'00''$
 $R = 4,092.56'$
 $T = 285.64'$
 $L = 570.36'$
 $E = 9.96'$
 $DS = 60$ MPH
 $eMAX = 0.035$

PAVEMENT LEGEND

-  PAVEMENT PLANING AND OVERLAY
-  FULL DEPTH CONCRETE PAVEMENT
-  PAVEMENT REMOVAL

ABBREVIATION LEGEND
 DND - DO NOT DISTURB
 TBR - TO BE REMOVED
 RBO - REMOVE BY OTHERS
 C.P.A. - FENCE CORNER POST ASSEMBLY
 D.P. - FENCE DEFLECTION POINT
 E.P.A. - FENCE END POST ASSEMBLY

X = REMOVAL ITEM (202)



PLAN SHEET - RAMP C
 STA. 640+00.00 TO STA. 645+00.00

DESIGN AGENCY



DESIGNER
 MJL

REVIEWER
 KF 05/20/22

PROJECT ID
 102375

SHEET TOTAL
 228 705

CURVE 20
 CONST. RAMP C
 P.I. = Sta. 645+79.82
 $\Delta = 07^{\circ}59'06''$ LT
 $Dc = 01^{\circ}24'00''$
 $R = 4,092.56'$
 $T = 285.64'$
 $L = 570.36'$
 $E = 9.96'$
 $DS = 60$ MPH
 $eMAX = 0.035$

CROSS REFERENCES	
SHEET	DESCRIPTION
325 - 340	CROSS SECTIONS - RAMP C & CC

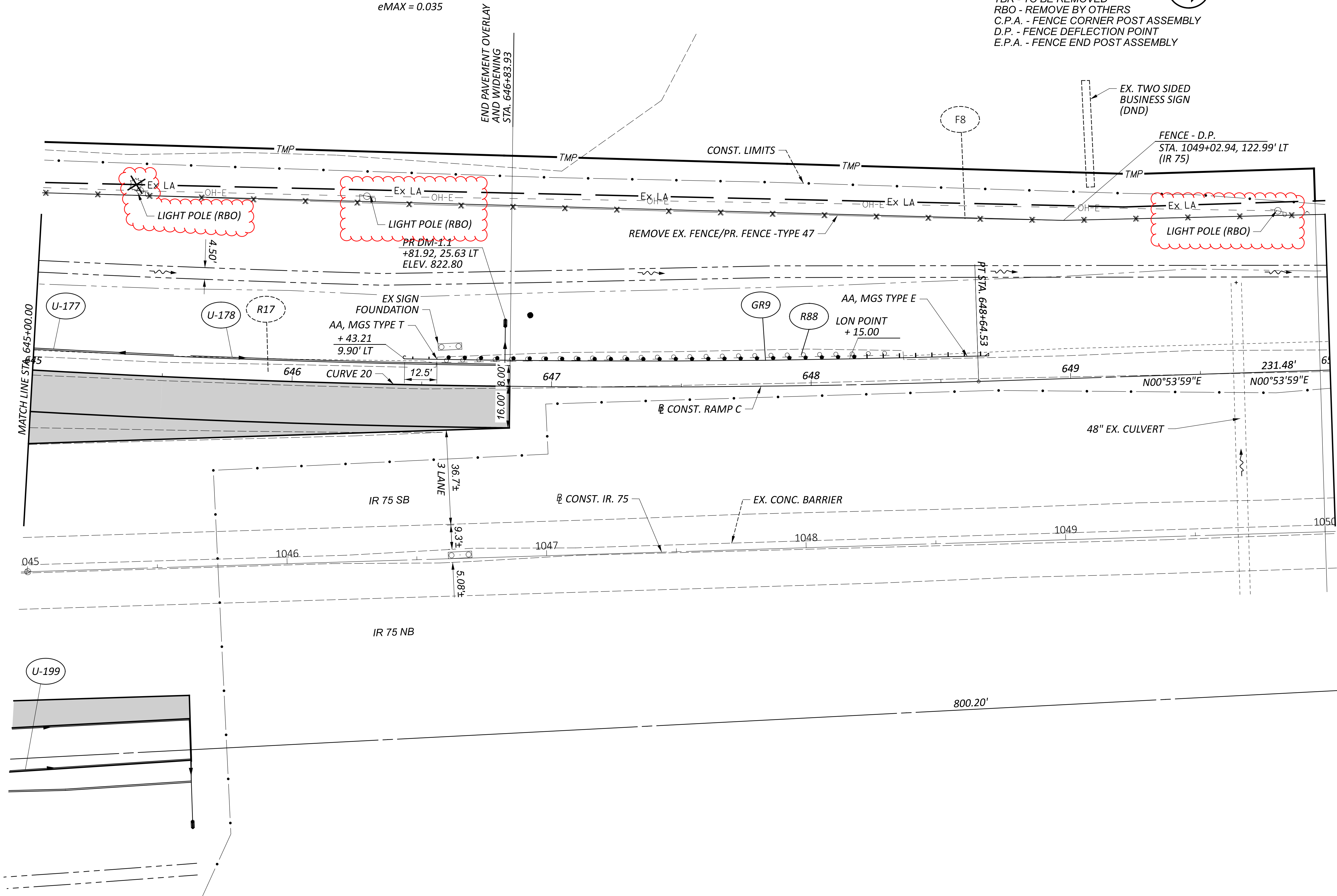
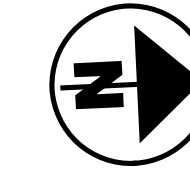
PAVEMENT LEGEND

 PAVEMENT PLANING AND OVERLAY

 = REMOVAL ITEM (202)

ABBREVIATION LEGEND

DND - DO NOT DISTURB
 TBR - TO BE REMOVED
 RBO - REMOVE BY OTHERS
 C.P.A. - FENCE CORNER POST ASSEMBLY
 D.P. - FENCE DEFLECTION POINT
 E.P.A. - FENCE END POST ASSEMBLY

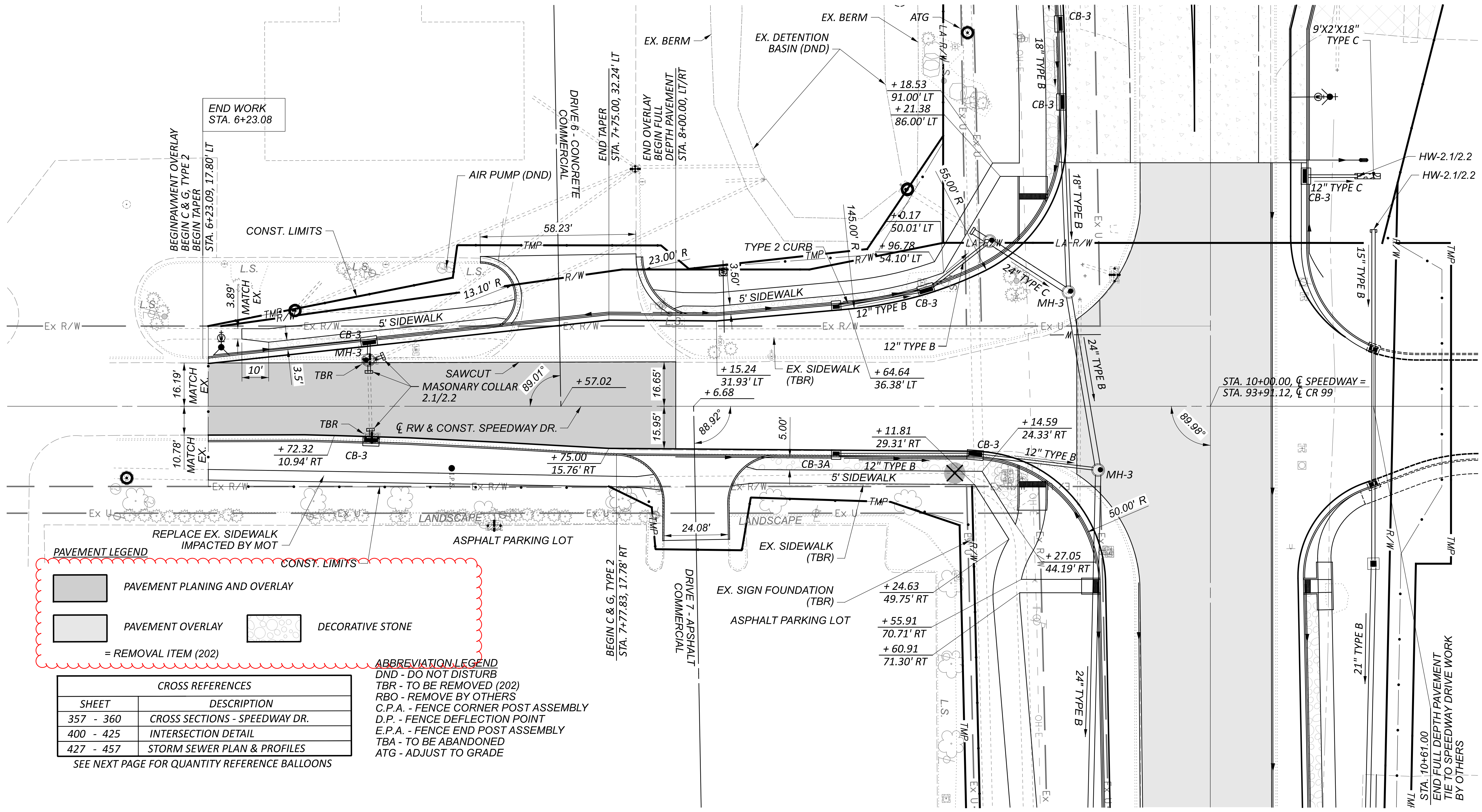


PLAN SHEET - RAMP C
 STA. 645+00.00 TO STA. 650+00.00

DESIGN AGENCY



DESIGNER	MJL
REVIEWER	KF 05/20/22
PROJECT ID	102375
SHEET	TOTAL
230	705



PAVEMENT LEGEND

- PAVEMENT PLANNING AND OVERLAY
- PAVEMENT OVERLAY
- DECORATIVE STONE

= REMOVAL ITEM (202)

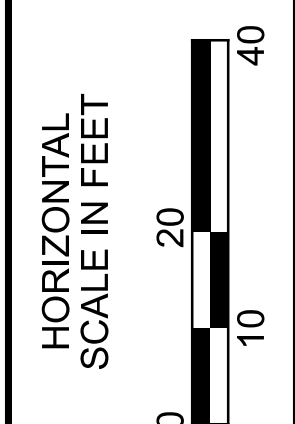
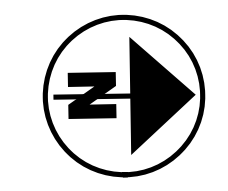
CROSS REFERENCES

SHEET	DESCRIPTION
357 - 360	CROSS SECTIONS - SPEEDWAY DR.
400 - 425	INTERSECTION DETAIL
427 - 457	STORM SEWER PLAN & PROFILES

SEE NEXT PAGE FOR QUANTITY REFERENCE BALLOONS

ABBREVIATION LEGEND

- DND - DO NOT DISTURB
- TBR - TO BE REMOVED (202)
- RBO - REMOVE BY OTHERS
- C.P.A. - FENCE CORNER POST ASSEMBLY
- D.P. - FENCE DEFLECTION POINT
- E.P.A. - FENCE END POST ASSEMBLY
- TBA - TO BE ABANDONED
- ATG - ADJUST TO GRADE



PLAN - SPEEDWAY AVE.
 STA. 5+50.00 TO STA. 10+00.00

DESIGN AGENCY



DESIGNER
MJJ

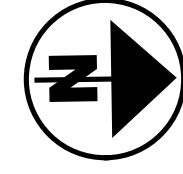
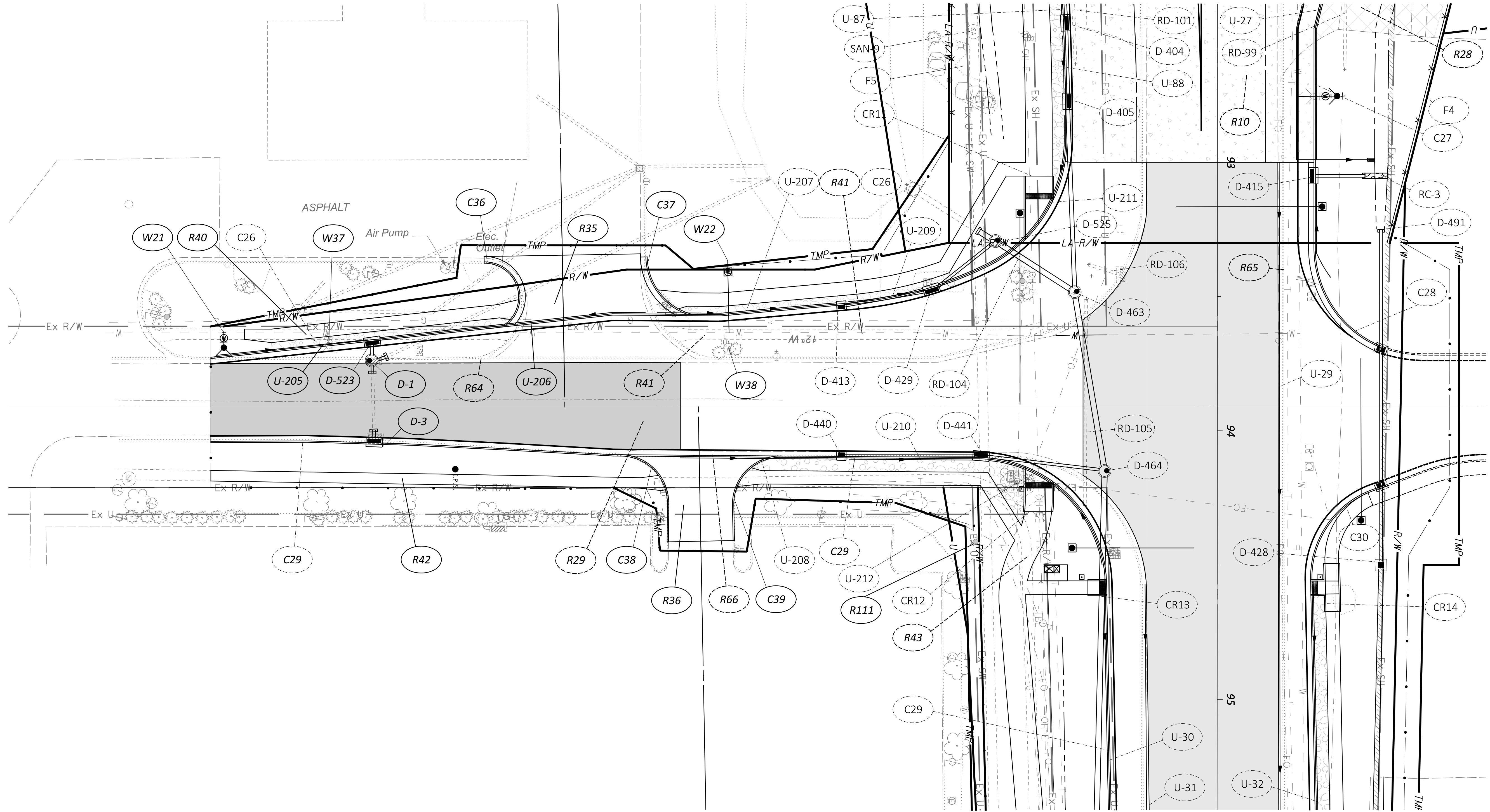
REVIEWER
KF 05/20/22

PROJECT ID
102375

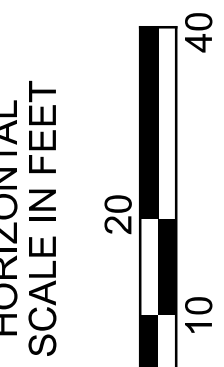
SHEET TOTAL
242 705

HAN-75/CR99 INTERCHANGE REHAB

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QUANTITY PLAN - SPEEDWAY AVE.
STA. 5+50.00 TO STA. 10+00.00



DESIGN AGENCY



DESIGNER
MJL

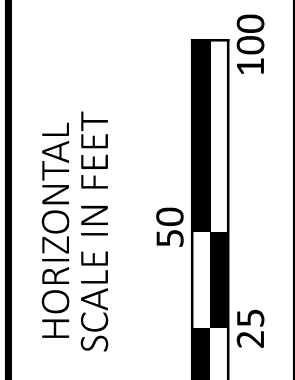
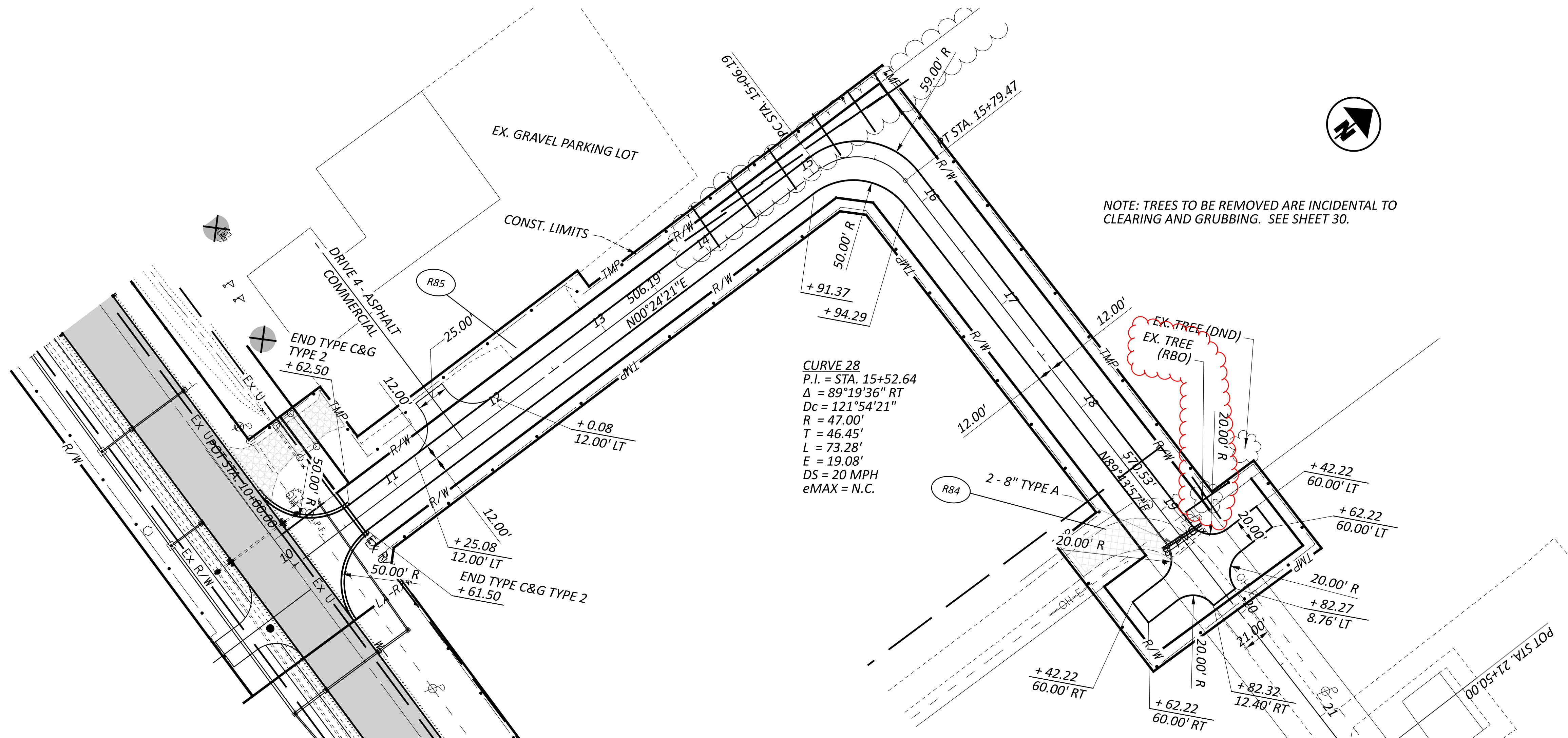
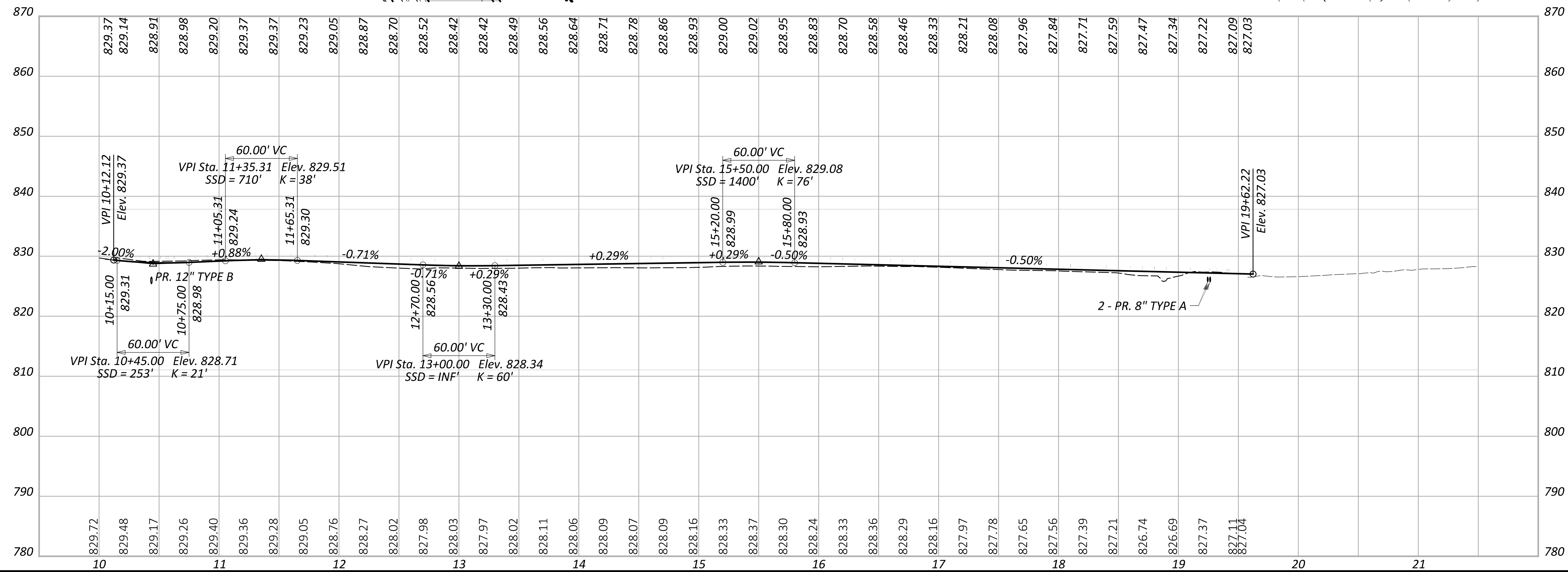
REVIEWER
KF 05/20/22

PROJECT ID
102375

SHEET	TOTAL
243	705

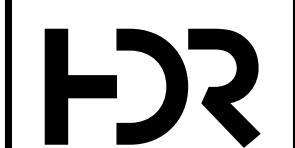
HAN-75/CR99 INTERCHANGE REHAB

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**PLAN AND PROFILE
 CR 99 SERVICE ROAD**

DESIGN AGENCY

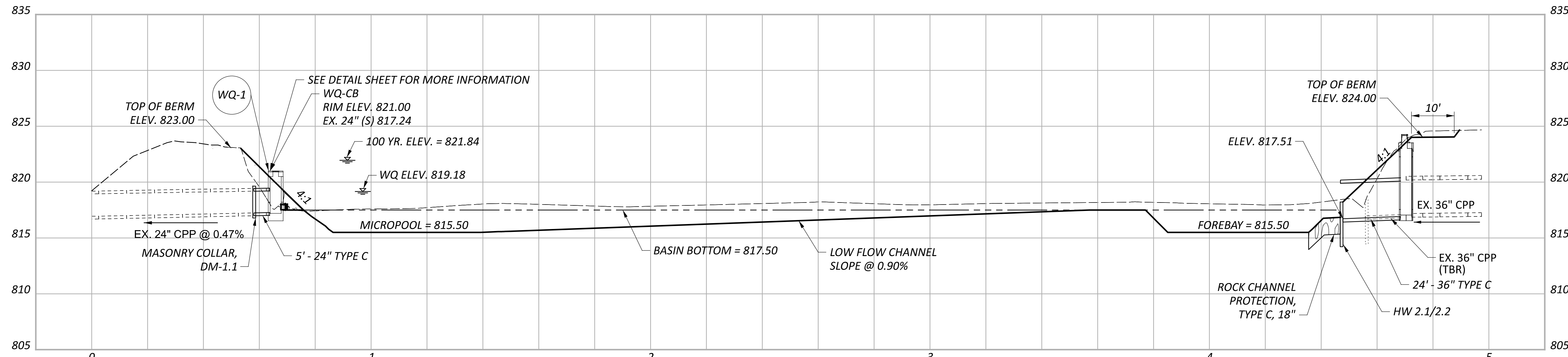
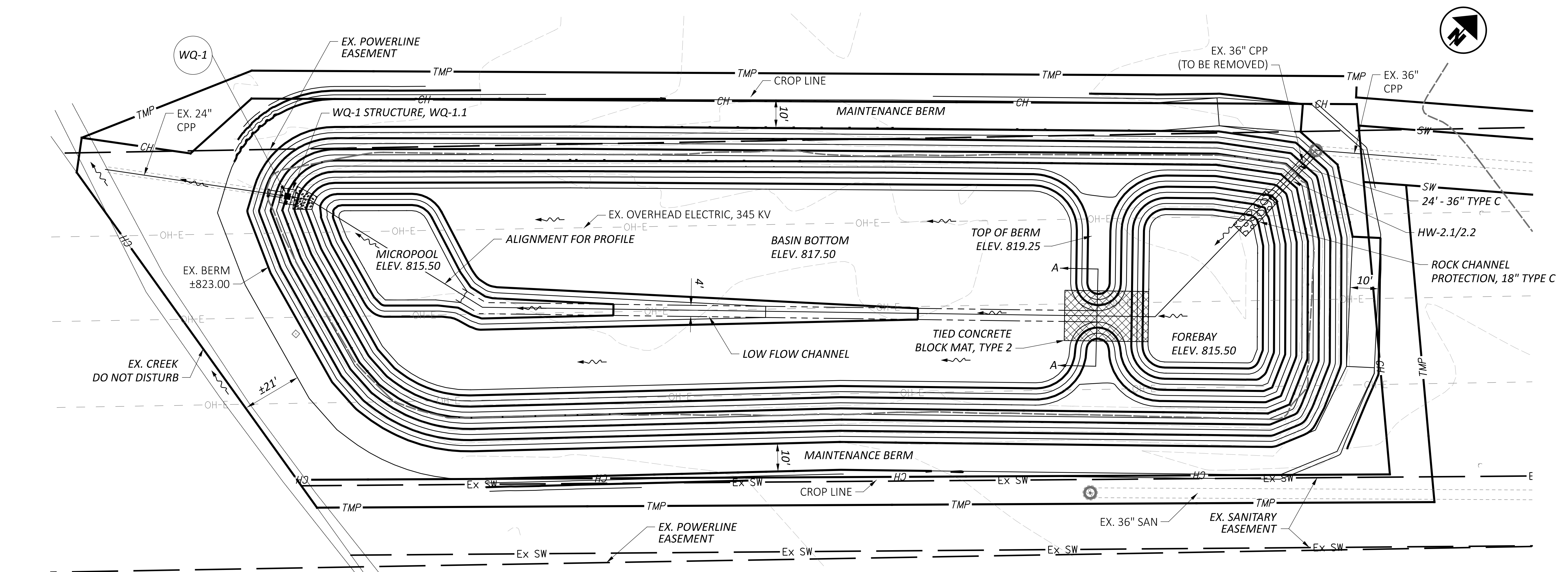


DESIGNER
MJL

REVIEWER
 KF 05/20/22

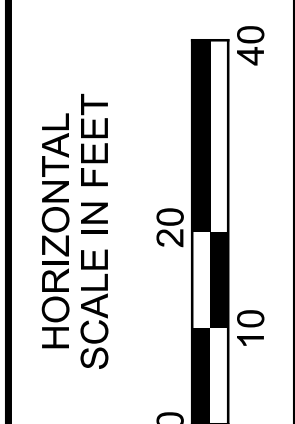
PROJECT ID
 102375

SHEET TOTAL
 247 705



202	202	202	203	203	601	601	602	611	611	611	659	670
HEADWALL REMOVED	PIPE REMOVED, 24" AND UNDER	PIPE REMOVED, OVER 24"	EXCAVATION	EMBANKMENT, USING NATURAL SOILES, 703.16.A	TIED CONCRETE BLOCK MAT, TYPE 2	ROCK CHANNEL PROTECTION, TYPE C	CONCRETE MASONRY	24" CONDUIT, TYPE C	36" CONDUIT, TYPE C	WATER QUALITY BASIN, DETENTION	SEEDING AND MULCHING	SLOPE EROSION PROTECTION
EACH	FT	FT	CY	CY	SY	CY	CY	FT	FT	EACH	SY	SY
1	5	15	959	415	60	5	1.7	5	24	1	5,912	4,710

TOTALS CARRIED TO GENERAL SUMMARY



COUNTY DETENTION BASIN - REGRADING
 PLAN AND PROFILE

DESIGN AGENCY



DESIGNER
 ARG

REVIEWER
 KAG 11/22/22

PROJECT ID
 102375

SHEET TOTAL
 489 705