

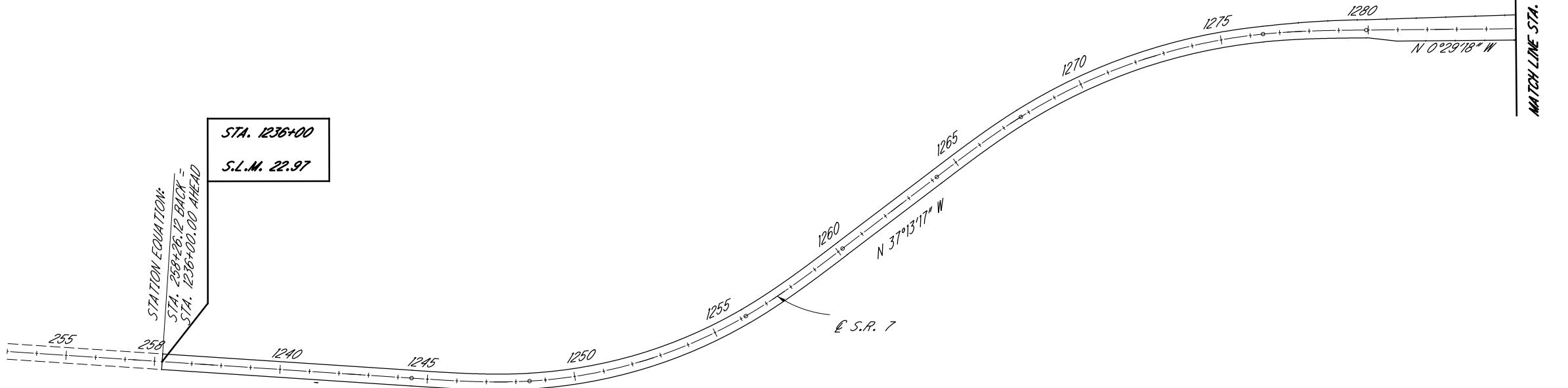
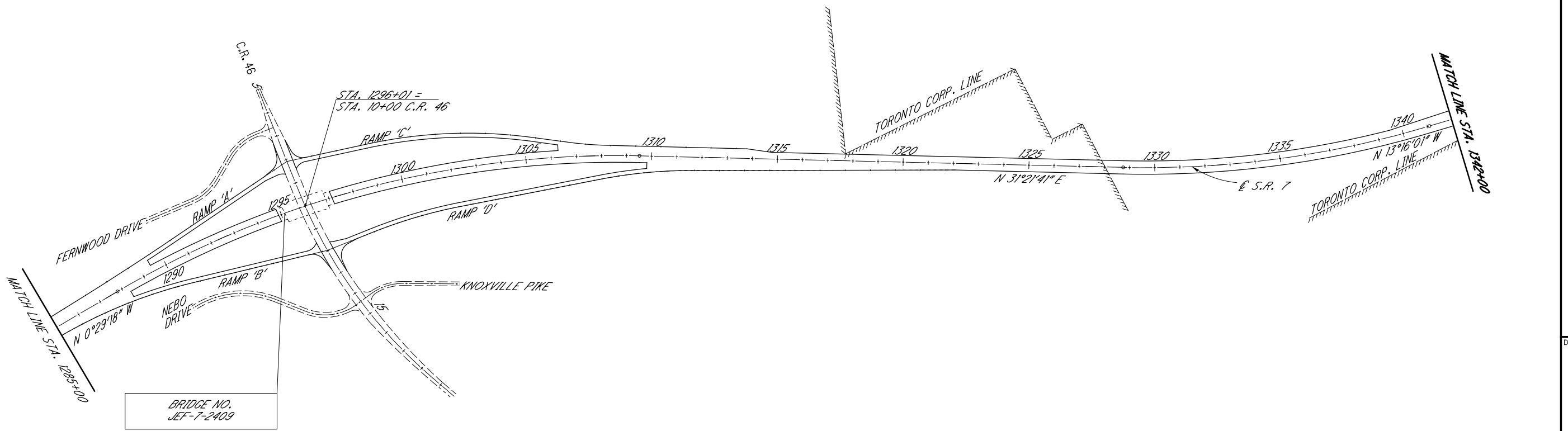
There are no existing landscaped areas within the work limits.

SCHEMATIC PLAN

HORIZONTAL SCALE IN FEET
0 100 200 400



DESIGNER ADK
REVIEWER JPB 06-10-22
PROJECT ID 102458
SHEET TOTAL P.3 39



SCHEMATIC PLAN

HORIZONTAL SCALE IN FEET

400

200

800

DESIGN AGENCY



DESIGNER

ADK

REVIEWER

JPB

06-10-22

PROJECT ID

102458

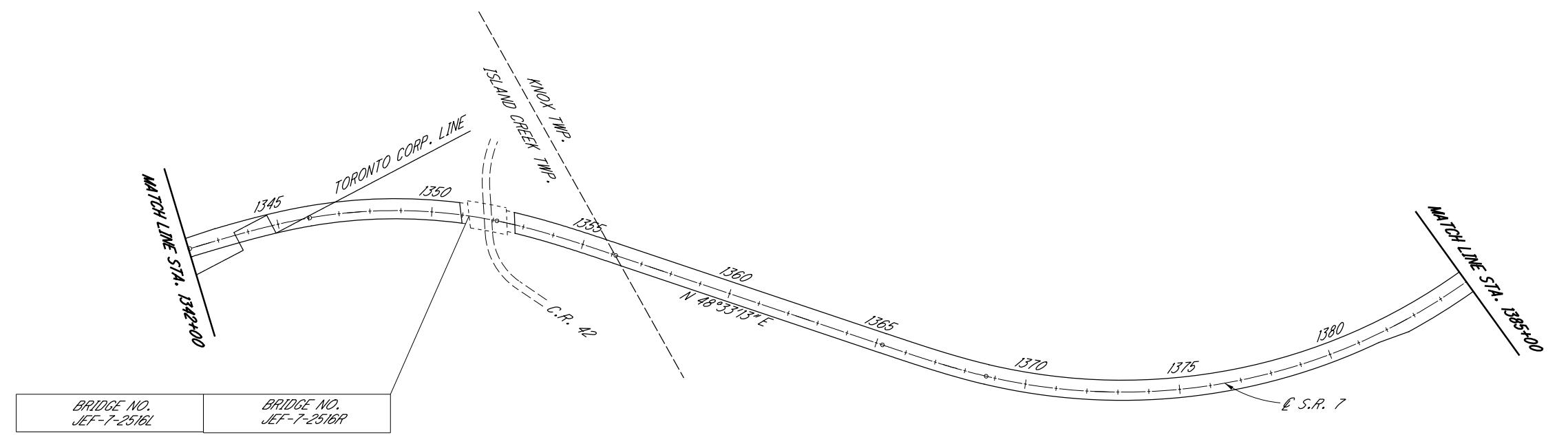
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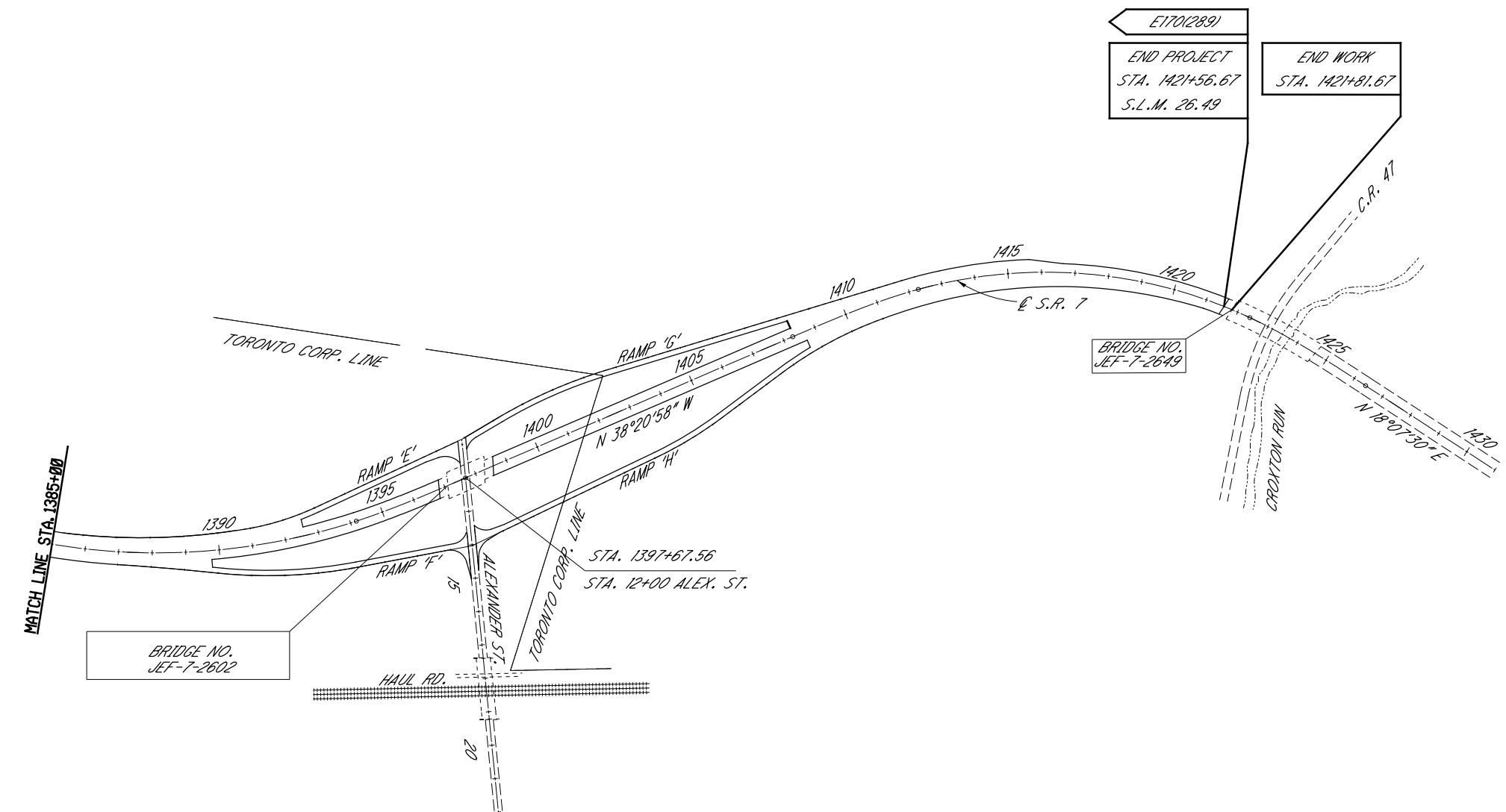
39

There are no existing landscaped areas within the work limits.



SCHEMATIC PLAN

HORIZONTAL SCALE IN FEET
0 200 400 800



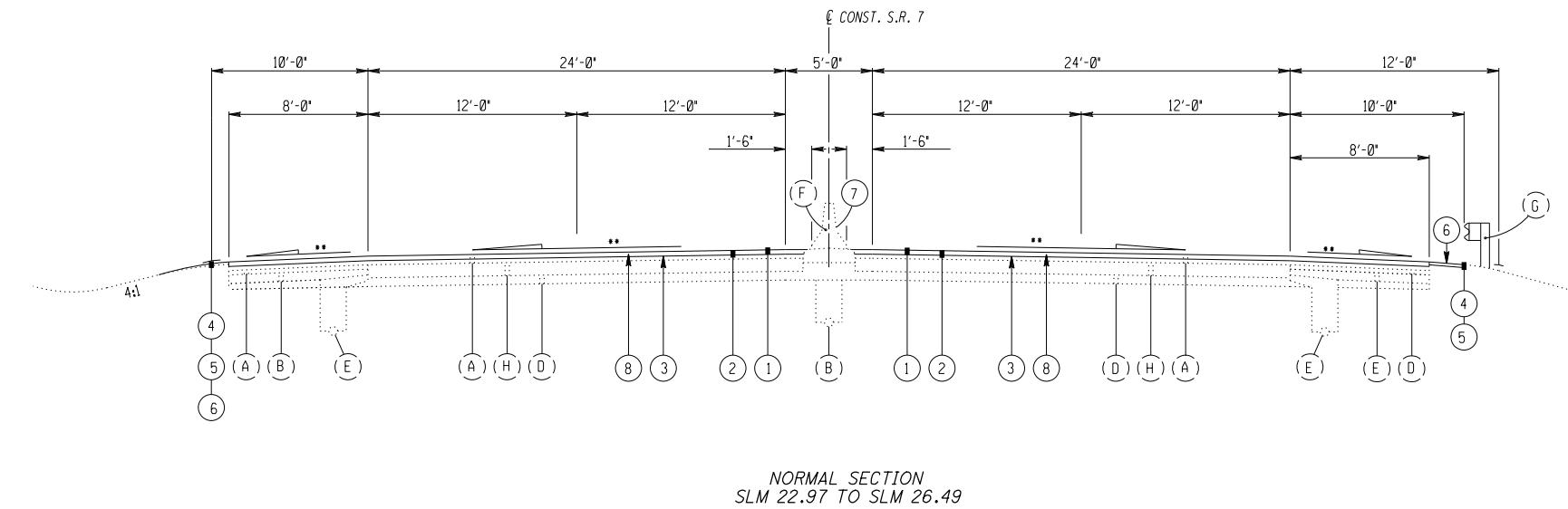
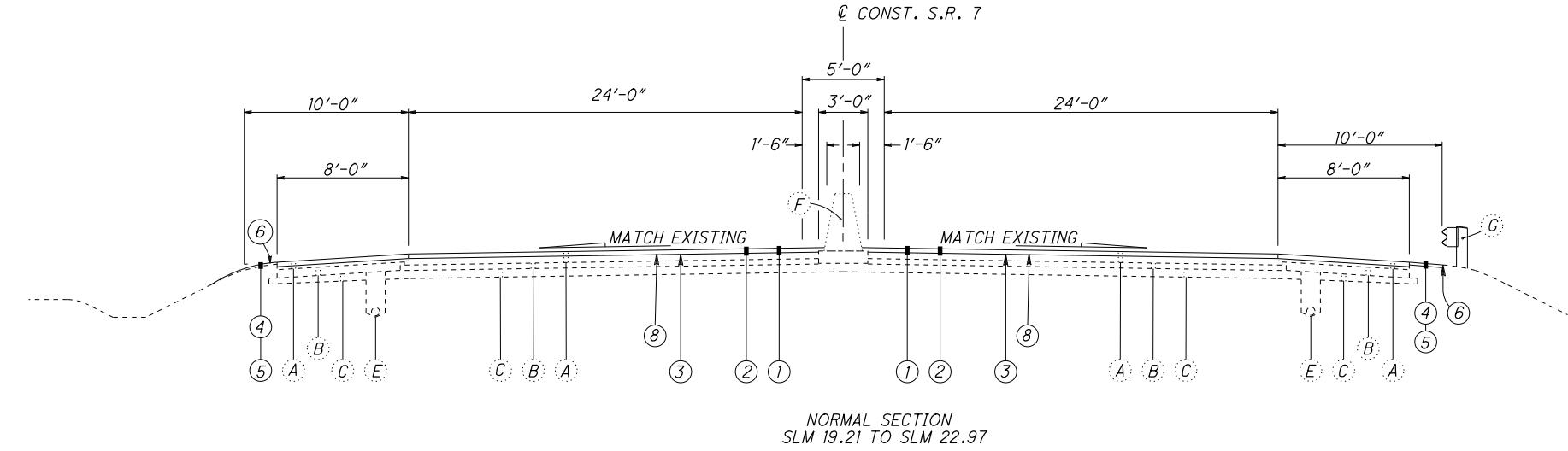
DESIGNER
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JPB 06-10-22

PROJECT ID
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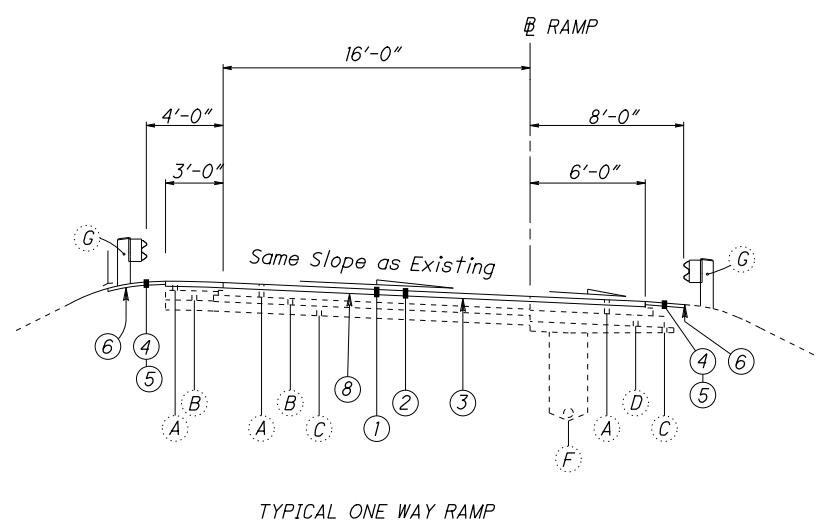
SHEET TOTAL
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TYPICAL SECTIONS



EXISTING LEGEND

- (A) EXISTING ASPHALT CONCRETE
- (B) EXISTING BITUMINOUS AGGREGATE BASE
- (C) EXISTING 6" SUBBASE
- (D) EXISTING AGGREGATE BASE
- (E) EXISTING 6" UNDERDRAIN
- (F) EXISTING CONCRETE BARRIER
- (G) EXISTING GUARDRAIL
- (H) EXISTING 9" REINFORCED CONCRETE PAVEMENT



PROPOSED LEGEND

- (1) ITEM 442 - 1 1/2" ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE B, (447), AS PER PLAN
- (2) ITEM 254 - 1 1/2" PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN
- (3) ITEM 407 - NON-TRACKING TACK COAT
- (4) ITEM 617 - SHOULDER PREPARATION
- (5) ITEM 617 - COMPAKTED AGGREGATE, AS PER PLAN
- (6) ITEM 408 - PRIME COAT, AS PER PLAN
- (7) ITEM 519 - PATCHING CONCRETE STRUCTURE
- (8) ITEM 254 - PATCHING PLANED SURFACE



ITEM 442 - ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE B, (447), AS PER PLAN

THE CONTRACTOR SHALL FOLLOW THE SPECIFICATIONS OF C&MS 703.05, EXCEPT DO NOT USE COARSE AGGREGATE FROM A SOURCE DESIGNATED "SR" OR "SRH" AS DEFINED BY THE OFFICE OF MATERIALS MANAGEMENT (OMM) IN ANY JOB MIX FORMULA (JMF) FOR THIS ITEM OF WORK.

ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN

GRADED SHOULDERS SHALL BE RESHAPED AS PER THE REQUIREMENTS OF ITEM 617, COMPACTED AGGREGATE. GRINDINGS SHALL BE USED IN LIEU OF ITEM 617, COMPACTED AGGREGATE. IF THE AMOUNT OF GRINDINGS ARE NOT SUFFICIENT TO COVER THE COMPACTED AGGREGATE QUANTITY REQUIRED FOR THIS PLAN, THEN ADDITIONAL MATERIAL MEETING SPECIFICATION 617 SHALL BE PROCURED AND USED BY THE CONTRACTOR. ALL GRINDINGS SHALL ADHERE TO THE SPECIFICATIONS STATED IN C&MS 617 AND MEET GRADATIONS CONFORMING TO C&MS 703.18. THE COST FOR STORING THE GRINDINGS ON THE PROJECT AND PLACING THE GRINDINGS SHALL ALSO BE INCLUDED IN THIS ITEM.

COORDINATION OF RESURFACING AND PLANING OPERATIONS

ONCE THE PAVEMENT PLANING OPERATIONS HAVE COMMENCED, THE CONTRACTOR SHALL PLANE CONTINUOUSLY UNTIL ALL ELEMENTS OF WORK ASSOCIATED WITH THE PAVEMENT PLANING OPERATIONS ARE CONCLUDED. THE PAVEMENT PLANING OPERATIONS SHALL BE COMPLETED IN A TIMELY MANNER, OR AS DIRECTED BY THE ENGINEER.

THE CONTRACTOR MUST BEGIN PAVING OPERATIONS NO LATER THAN 4 DAYS AFTER THE START OF THE PAVEMENT PLANING.

IF ASPHALT CONCRETE IS TO BE APPLIED DIRECTLY ONTO PORTLAND CEMENT, CONCRETE, OR BRICK PAVEMENT, THE CONTRACTOR SHALL TACK THE EXISTING PAVEMENT WITH RUBBERIZED ASPHALT EMULSION CONFORMING TO C&MS 702.13.

ALL GRINDINGS SHALL BECOME THE PROPERTY OF THE CONTRACTOR EXCEPT FOR WHAT IS REQUIRED TO BE USED FOR SHOULDER MATERIAL INDICATED IN ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN AND THE 300 TONS DELIVERED TO THE TORONTO OUTPOST.

ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, (1½"), AS PER PLAN

THIS ITEM SHALL INCLUDE HAULING THE GRINDINGS FROM THE PAVEMENT PLANING OPERATION TO THE ADDRESS LISTED BELOW AND STOCKPILING THE MATERIAL IN A MANNER ACCEPTABLE TO THE ENGINEER. CONTINUOUS END DUMPING WILL NOT BE PERMITTED.

ALL PARTS: 300 TOTAL TONS DELIVERED TO:

ODOT TORONTO OUTPOST
940 KINGSDALE RD., STEUBENVILLE, OH 43952
CONTACT: TY JUSTICE 330-308-6580

ALL LABOR, EQUIPMENT, AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK SHALL BE INCLUDED IN THE SQUARE YARD BID PRICE FOR ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN.

ITEM 408 - PRIME COAT, AS PER PLAN

THIS ITEM OF WORK SHALL BE PERFORMED IN ACCORDANCE WITH C&MS "ITEM 408 - PRIME COAT", EXCEPT THE CONTRACTOR SHALL APPLY "MC-70" AT A RATE OF 0.4 GALLONS PER SQUARE YARD, OR AS DETERMINED BY THE ENGINEER, TO THE COMPLETED COMPACTED AGGREGATE SHOULDER, AS PER PLAN.

ITEM 646 - EPOXY PAVEMENT MARKINGS**ITEM 807 - WET REFLECTIVE EPOXY PAVEMENT MARKINGS**

THE CONTRACTOR SHALL REPLACE THE EXISTING PAVEMENT MARKINGS WITHIN THE PROJECT LIMITS WITH NEW PAVEMENT MARKINGS AT THE SAME LOCATIONS AS PER C&MS 641.06. SEE STANDARD DRAWING TC-71.10 AND TC-72.20 FOR PAVEMENT MARKING DETAILS.

PROFILE AND ALIGNMENT

PLACE THE PROPOSED PAVEMENT TO FOLLOW THE PROFILE AND ALIGNMENT OF THE EXISTING PAVEMENT. PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY AS SHOWN ON THE TYPICAL SECTIONS.

SHIELD

THE CONTRACTOR SHALL PROVIDE A SHIELD TO PREVENT THE SPRAYING OR DRIFTING OF LIQUID BITUMINOUS MATERIAL ONTO THE EDGE OF THE PAVEMENT OR EDGELINE. THE CONTRACTOR SHALL ADHERE TO THE PROVISIONS SET FORTH IN C&MS 107.10 CONCERNING THE PROTECTION AND RESTORATION OF ALL PUBLIC AND PRIVATE PROPERTY IMPACTED BY CONSTRUCTION OPERATIONS.

UTILITIES

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER, OR ADJACENT TO, THE WORK AREA.

CLEARING AND GRUBBING

ALTHOUGH THERE ARE NO TREES OR STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE LIMITS OF THE PROJECT, A LUMP SUM QUANTITY IS INCLUDED IN THE GENERAL SUMMARY FOR ITEM 201, CLEARING AND GRUBBING. ALL PROVISIONS AS SET FORTH IN THE SPECIFICATIONS UNDER THIS ITEM ARE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 201, CLEARING AND GRUBBING.

EXISTING PLANS

THE FOLLOWING EXISTING PLANS ARE AVAILABLE FOR REFERENCE AT THE DISTRICT 11 OFFICE OF THE OHIO DEPARTMENT OF TRANSPORTATION, 2201 REISER AVE. S.E., NEW PHILADELPHIA, OHIO, 44663:

PID 23736 (2012)
PID 87025 (2011)

IN ADDITION, THE EXISTING PLANS CAN BE FOUND ON THE DEPARTMENT'S WEBSITE AT THE FOLLOWING ADDRESS:

<http://www.dot.state.oh.us/pub/contracts/attach>

SURFACE COURSE COMPLETION REQUIREMENTS

ANY GIVEN LENGTH OF WORK ON WHICH RESURFACING OPERATIONS HAVE BEEN STARTED IN A CONSTRUCTION SEASON SHALL HAVE THE SURFACE COURSE PLACED THAT SAME SEASON.

ITEM 442 - ANTI-SEGREGATION EQUIPMENT

PROVIDE A MATERIAL TRANSFER VEHICLE IN ACCORDANCE WITH CMS 401.12 WHEN PLACING ASPHALT SURFACE AND INTERMEDIATE COURSES ON MAINLINE LANES AND ACCELERATION AND DECELERATION LANES.

ITEM 519 - PATCHING CONCRETE STRUCTURE

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR THE PATCHING OF CONCRETE BARRIER AT LOCATIONS AS DIRECTED BY THE ENGINEER. APPLY A WHITE CURING COMPOUND TO THE FINISHED PATCHES. THE PATCHING SHALL BE COMPLETED BEFORE THE ASPHALT SURFACE COURSE IS PLACED.

PAYMENT FOR THE ABOVE WORK SHALL BE AT THE UNIT PRICE BID FOR ITEM 519, PATCHING CONCRETE STRUCTURE AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT, AND MATERIALS.

TOTAL CARRIED TO THE GENERAL SUMMARY:

ITEM 519 - PATCHING CONCRETE STRUCTURE 50 SQ FT

ITEM 621 - RAISED PAVEMENT MARKER REMOVED

EXISTING RAISED PAVEMENT MARKERS SHALL BECOME THE PROPERTY OF THE CONTRACTOR FOR DISPOSAL OFF THE PROJECT. IN AREAS OF PAVEMENT PLANING ONLY, THE REQUIREMENT TO FILL THE DEPRESSIONS SHALL BE WAIVED.

AIRWAY/HIGHWAY CLEARANCE FOR AIRPORTS AND HELIPORTS

THIS PROJECT HAS BEEN IDENTIFIED AS BEING WITHIN THE INFLUENCE AREA OF A PUBLIC USE AIRPORT OR HELIPORT. NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT AT MAXIMUM OPERATING HEIGHT SHALL EXCEED A HEIGHT OF 404 FT. IF ANY TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT WILL EXCEED THIS HEIGHT, FURTHER COORDINATION WITH THE FEDERAL AVIATION ADMINISTRATION (FAA), AND ODOT OFFICE OF AVIATION, WILL BE NECESSARY PRIOR TO ERECTING SUCH TEMPORARY STRUCTURES OR OPERATING SUCH EQUIPMENT ON THE PROJECT. THE CONTRACTOR WILL BE REQUIRED TO SUBMIT FORM 7460-1 TO THE FAA. NOTIFY THE ODOT OFFICE OF AVIATION WHEN SUBMITTING FAA FORM 7460-1.

NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT SHALL EXCEED THE PERMISSIBLE HEIGHT, UNTIL A COPY OF THE FAA APPROVAL AND THE ODOT OFFICE OF AVIATION PERMIT HAS BEEN FURNISHED TO THE PROJECT ENGINEER.

FEDERAL AVIATION ADMINISTRATION
SOUTHWEST REGIONAL OFFICE
OBSTRUCTION EVALUATION GROUP
10101 HILLWOOD PARKWAY
FORT WORTH, TX 76177
FAX: (817) 222-5920
<HTTP://CEAAA.FAA.GOV>

OHIO DEPARTMENT OF TRANSPORTATION
OFFICE OF AVIATION
2829 WEST DUBLIN-GRANVILLE ROAD
COLUMBUS, OHIO 43235
OHIO.AIRPORT.PROTECTION@DOT.OHIO.GOV

SPEED MEASUREMENT MARKINGS

THE CONTRACTOR SHALL CONTACT THE DISTRICT 11 SURVEY OPERATIONS MANAGER AT 330-308-7866 WHEN THE PAVING AND LINEAR GRADING IS COMPLETE FOR THE RESTORATION OF THE SPEED MEASUREMENT MARKINGS FROM SLM 21.50 TO SLM 22.50. THE SPEED MEASUREMENT MARKINGS WILL BE APPLIED BY DISTRICT PERSONNEL. THE CONTRACTOR SHALL COORDINATE WITH THE DISTRICT 11 SURVEY OPERATIONS MANAGER TO ENSURE THAT THE SPEED MEASUREMENT MARKINGS CAN BE APPLIED WHILE THE CONTRACTOR'S WORK ZONE MAINTENANCE OF TRAFFIC IS STILL IN EFFECT.



DESIGNER
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ITEM 614 - MAINTAINING TRAFFIC, AS PER PLAN

THE CONTRACTOR SHALL MAINTAIN TRAFFIC AT ALL TIMES IN ACCORDANCE WITH THE REQUIREMENTS OF CMS ITEM 614, THESE MAINTENANCE OF TRAFFIC NOTES AND DETAILS, THE STANDARD CONSTRUCTION DRAWINGS, AND THE TRAFFIC CONTROL DETAILS DESCRIBED IN THESE PLANS.

THE MINIMUM LANE WIDTH FOR TRAFFIC CONTROL SHALL BE 11 FEET AT ALL TIMES. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ORGANIZE HIS WORK IN SUCH A MANNER TO PROVIDE THE MOST SAFETY WITH THE LEAST INCONVENIENCE TO THE TRAVELING PUBLIC.

THE CONTRACTOR IS RESPONSIBLE FOR DESIGNING THE MAINTENANCE OF TRAFFIC SCHEME. THE CONTRACTOR SHALL SUBMIT, IN WRITING, THIS MAINTENANCE OF TRAFFIC SCHEME AND A SCHEDULE OF OPERATIONS TO THE ENGINEER AND RECEIVE APPROVAL BEFORE WORK IS STARTED ON THE PROJECT.

ANY OPEN PAVEMENT TRENCH OR DROPOFF SHALL BE ADEQUATELY MAINTAINED AND PROTECTED. THE PROTECTION USED SHALL MEET THE REQUIREMENTS OF STD DWG MT-101.90.

UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE PERMITTED TO HAVE WORK ZONES WHICH ALTERNATELY CLOSE BOTH THE PASSING AND TRAVEL LANE UNLESS THE DISTANCE BETWEEN THE LANE RESTRICTIONS EXCEEDS 2 MILES.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR SMOOTH AND ORDERLY FLOW OF TRAFFIC THROUGH THE PROJECT AREA 24 HOURS PER DAY FOR THE DURATION OF THE PROJECT. THIS CONSISTS OF NOTIFYING THE OHIO STATE PATROL AFTER ENCOUNTERING ANY ACCIDENTS OR DISABLED VEHICLES OR OBJECTS HINDERING THE FLOW OF TRAFFIC.

THE CONTRACTOR SHALL DESIGNATE TO THE ENGINEER A PERSON RESPONSIBLE FOR MAINTENANCE OF TRAFFIC CONTROL DURING NON-WORK HOURS WHO SHALL BE AVAILABLE WITHIN (30) MINUTES AFTER NOTIFICATION.

PAYMENT FOR PROVIDING WATCHMEN, FURNISHING, ERECTING, MAINTAINING AND REMOVING SIGNS, CONES, MARKERS, SPECIAL LIGHTING, FLOODLIGHTING, WORK ZONE PAVEMENT MARKINGS, WORK ZONE RAISED PAVEMENT MARKERS, ETC., SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614 MAINTAINING TRAFFIC, AS PER PLAN.

THE CONTRACTOR SHALL FURNISH, INSTALL AND MAINTAIN ALL ADDITIONAL SIGNS OR OTHER TRAFFIC CONTROL DEVICES AS REQUIRED ABOVE. ALL COSTS INVOLVED IN FURNISHING, INSTALLING AND MAINTAINING THESE DEVICES SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614, MAINTAINING TRAFFIC, AS PER PLAN.

UNLESS THE ENGINEER DEEMS IT PHYSICALLY IMPOSSIBLE, ALL CONSTRUCTION EQUIPMENT SHALL EXIT ALL WORK ZONES FROM THE DOWNSTREAM END OF THE WORK ZONE OR BY INTERCHANGE RAMPS. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE PERMITTED TO DIRECTLY TRANSPORT OR OPERATE ANY EQUIPMENT ACROSS THE OPEN LANES OF THE ROADWAY.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS. THE RESTRICTIONS SHALL ALSO APPLY TO WORK ON THE RAMPS.

ITEM 614 - MAINTAINING TRAFFIC, AS PER PLAN (CONTINUED)

THE PLANING AND RESURFACING WILL PROCEED CONTINUOUSLY A MINIMUM OF FIVE (5) DAYS PER WEEK, WEATHER PERMITTING, EXCEPTING HOLIDAYS AND EVENTS LISTED BELOW:

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

NEW YEARS	LABOR DAY
MEMORIAL DAY	THANKSGIVING
FOURTH OF JULY	CHRISTMAS

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPENED DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF THE WEEK	TIME ALL LANES MUST BE OPENED TO TRAFFIC
SUNDAY	12:00N FRI THRU 6:00 AM MON
MONDAY	12:00N FRI THRU 6:00 AM TUE
TUESDAY	12:00N MON THRU 6:00 AM WED
WEDNESDAY	12:00N TUE THRU 6:00 AM THU
THURSDAY	12:00N WED THRU 6:00 AM FRI
(THANKSGIVING ONLY)	6:00 AM WED THRU 6:00 AM MON
FRIDAY	12:00N THU THRU 6:00 AM MON
SATURDAY	12:00N FRI THRU 6:00 AM MON

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF \$15,220 FOR EACH HOUR THE ABOVE DESCRIBED LANE CLOSURE RESTRICTIONS ARE VIOLATED.

NO EXTENSIONS OF TIME SHALL BE GRANTED FOR DELAYS IN MATERIAL DELIVERIES, UNLESS SUCH DELAYS ARE INDUSTRY-WIDE, OR FOR LABOR STRIKES, UNLESS SUCH STRIKES ARE AREA-WIDE.

WHEN RAISED PAVEMENT MARKERS ARE TO BE INSTALLED, THE REQUIRED LANE CLOSURE SHALL REMAIN IN EFFECT UNTIL THE EPOXY IS DRY AND ALL FOREIGN MATTER OR DEBRIS CREATED BY THE INSTALLATION OF THE RPM CASTING IS REMOVED FROM THE ROADWAY.

PEDESTRIAN WALKWAYS CONSTRUCTED BY THE CONTRACTOR SHALL BE KEPT FREE OF ANY OBSTRUCTIONS OR HAZARDS INCLUDING HOLES, DEBRIS AND MUD. OTHER WALKWAYS DAMAGED OR DIRTIED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED OR CLEANED. THE CONTRACTOR MUST TAKE PRECAUTIONS TO PROTECT PEDESTRIANS FROM EXPOSURE TO HAZARDS RESULTING FROM THE CONSTRUCTION OPERATION BY INSTALLING CONSTRUCTION FENCE AND SIGNING.

TEMPORARY ORANGE PLASTIC CONSTRUCTION FENCE HAS BEEN PROVIDED HEREIN AS A TRAFFIC CONTROL DEVICE TO DIVERT AND GUIDE PEDESTRIANS WHOSE PATH WOULD OTHERWISE ENTER THE WORK AREA. THE TEMPORARY ORANGE PLASTIC CONSTRUCTION FENCE SHALL BE PLACED AROUND THE SIDEWALK WORK AREAS. SIDEWALK CLOSED SIGN (R9-9 (30 X 18)) MOUNTED ON A TYPE 3 BARRICADE WITH TYPE A FLASHING LIGHTS SHALL BE PLACED OUTSIDE THE FENCE ON EACH SIDEWALK APPROACH AS SHOWN ON SCD MT-101.60.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, AS PER PLAN, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ITEM 607 - FENCE, MISC.: TEMPORARY ORANGE PLASTIC CONSTRUCTION FENCE

TEMPORARY ORANGE PLASTIC/NYLON CONSTRUCTION FENCE SHALL BE PLACED AROUND NEW CURB RAMPS AND WALK UNTIL ADEQUATELY CURED, OR AS DIRECTED BY THE ENGINEER TO PROTECT PEDESTRIAN TRAFFIC FROM CONSTRUCTION OPERATIONS. THE FENCING MATERIAL SHALL BE SECURELY FASTENED TO EITHER WOOD, OR METAL POSTS WITH A MAXIMUM SPACING NOT TO EXCEED 6 FEET. THE FENCING MATERIAL SHALL HAVE A NOMINAL HEIGHT OF 42 INCHES, AND THE TOP EDGE OF THE FENCING SHALL NOT BE PERMITTED TO SAG BELOW 30 INCHES. THE CONTRACTOR SHALL ENSURE THE FENCE IS IN GOOD CONDITION, PROPERLY PLACED, AND MAINTAINED AT ALL TIMES.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 607 - FENCE MISC.: TEMPORARY ORANGE PLASTIC CONSTRUCTION FENCE

121 FT.

WORK ZONE MARKINGS AND SIGNS

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS IDENTIFIED BY THE ENGINEER FOR WORK ZONE PAVEMENT MARKINGS AND SIGNS PER THE REQUIREMENTS OF C&MS 614.04 AND 614.11.

ITEM 614 - WORK ZONE PAVEMENT MARKINGS, CLASS I, 807 PAINT

ITEM 614 - WORK ZONE PAVEMENT MARKINGS, CLASS III, 642 PAINT

ITEM 614 - WORK ZONE MARKING SIGNS

THE CONTRACTOR SHALL BE REQUIRED TO INSTALL WORK ZONE MARKINGS AND SIGNS AT LOCATIONS IDENTIFIED BY THE ENGINEER PER THE REQUIREMENTS OF CMS 614.04 AND 614.11.

WORK ZONE PAVEMENT MARKINGS FOR MILLED SURFACES SHALL BE CLASS III, 642 PAINT. WORK ZONE PAVEMENT MARKINGS FOR NEWLY PAVED SURFACES SHALL BE CLASS I, 807 PAINT.

PRIOR TO PLACEMENT OF ANY WORK ZONE PAVEMENT MARKINGS, THE CONTRACTOR SHALL COMPLETELY OBLITERATE, AS PER 614.10, ALL EXISTING PAVEMENT MARKINGS THAT WOULD CREATE CONFUSION OR CONFLICT WITH THE WORK ZONE PAVEMENT MARKINGS.

WORK ZONE RAISED PAVEMENT MARKERS CANNOT BE USED TO SIMULATE (REPLACE) ANY TYPE OF WORK ZONE PAVEMENT MARKING.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR WORK ZONE SIGNS:

ITEM 614 - WORK ZONE MARKING SIGN

9 EACH

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

WORK ZONE PAVEMENT MARKINGS TABLE

PART	LOCATION	614							FUNDING
		WORK ZONE EDGE LINE, CLASS I, 6', 807 PAINT	WORK ZONE LANE LINE, CLASS I, 6', 807 PAINT	WORK ZONE CHANNELIZING LINE, CLASS I, 12", 807 PAINT	WORK ZONE STOP LINE, CLASS III, 642 PAINT	WORK ZONE EDGE LINE, CLASS III, 6', 642 PAINT	WORK ZONE LANE LINE, CLASS III, 6', 642 PAINT	WORK ZONE CHANNELIZING LINE, CLASS III, 12", 642 PAINT	
1	S.R. 7	34.14	15.40	6,686	293	34.14	15.40	6,686	
	SUB-TOTALS	34.14	15.40	6,686	293	34.14	15.40	6,686	01/NHS/PV
	TOTALS (CARRIED TO THE GENERAL SUMMARY)	34.14	15.40	6,686	293	34.14	15.40	6,686	

ITEM 614, REPLACEMENT SIGN

FLATSHEET SIGNS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT SIGNS SHALL BE NEW. OTHER MATERIALS MAY BE IN USED, BUT GOOD, CONDITION SUBJECT TO APPROVAL BY THE ENGINEER.

PAYMENT FOR THE NEW SIGNS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT SIGN, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF DAMAGED SIGNS, HARDWARE AND SUPPORTS, AND PROVIDING THE NECESSARY REPLACEMENT HARDWARE, SUPPORTS, ETC.

AN ESTIMATED QUANTITY OF 5 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

ITEM 614, REPLACEMENT DRUM

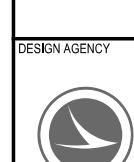
DRUMS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT DRUMS SHALL BE NEW.

PAYMENT FOR THE NEW DRUMS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT DRUM, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGED DRUM, AND PROVIDING AND MAINTAINING THE REPLACEMENT DRUM IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL DRUM.

AN ESTIMATED QUANTITY OF 10 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

MOVEMENT OF DRUMS

THE ROW OF DRUMS ALONG A CLOSED LANE SHALL BE MOVED OUT OF THE OPEN LANE ONTO THE NEW PAVEMENT AS SOON AS PAVING OPERATIONS PERMIT.

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WORK ZONE INCREASED PENALTIES SIGN (R11-H5A)

R11-H5A-48 SIGNS SHALL BE FURNISHED, ERECTED, AND MAINTAINED IN GOOD CONDITION AND/OR REPLACED AS NECESSARY AND SUBSEQUENTLY REMOVED BY THE CONTRACTOR. SIGNS SHALL BE MOUNTED AT THE APPROPRIATE OFFSETS AND ELEVATIONS AS PRESCRIBED BY THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. THEY SHALL BE MAINTAINED ON SUPPORTS MEETING CURRENT SAFETY CRITERIA.

THE SIGNS MAY BE ERECTED OR UNCOVERED NO MORE THAN FOUR HOURS BEFORE THE ACTUAL START OF WORK. THE SIGNS SHALL BE REMOVED OR COVERED NO LATER THAN FOUR HOURS FOLLOWING RESTORATION OF ALL LANES TO TRAFFIC WITH NO RESTRICTIONS, OR SOONER AS DIRECTED BY THE ENGINEER. TEMPORARY SIGN COVERING AND UNCOVERING DUE TO TEMPORARY LANE RESTORATIONS SHALL BE GUIDED BY THE FOUR-HOUR LIMITATIONS STATED ABOVE. SUCH LANE RESTORATIONS SHOULD BE EXPECTED TO REMAIN IN EFFECT FOR 30 OR MORE CONSECUTIVE CALENDAR DAYS, SUCH AS DURING WINTER SHUT-DOWNS.

(THE SIGNS ON THE MAINLINE SHALL BE DUAL MOUNTED UNLESS NOT PHYSICALLY POSSIBLE. THE FIRST SIGN SHALL BE PLACED BETWEEN THE ROAD WORK AHEAD (W20-1) SIGN AND THE NEXT SIGN IN THE SEQUENCE. SIGNS SHALL BE ERECTED ON EACH ENTRANCE RAMP AND EVERY 2 MILES THROUGH THE CONSTRUCTION WORK LIMITS. SIGNS ON THE MAINLINE SHALL BE R11-H5A-48. SIGNS USED ON THE RAMPS SHALL BE R11-H5A-24. R11-H5A-24 SIGNS MAY BE USED IN THE MEDIAN IN LIEU OF R11-H5A-48 SIGNS IF IT IS NOT PHYSICALLY POSSIBLE TO PROVIDE R11-H5A-48 SIGNS IN THE MEDIAN.)

THE R11-H5A-48 SIGNS SHALL BE MOUNTED ON 2 NO. 3 POSTS WHEN LOCATED WITHIN CLEAR ZONES.

THE CONTRACTOR MAY USE SIGNS AND SUPPORTS IN USED, BUT GOOD, CONDITION PROVIDED THE SIGNS MEET CURRENT ODOT SPECIFICATIONS. SIGN FACES SHALL BE RETROREFLECTORIZED WITH TYPE G SHEETING COMPLYING WITH THE REQUIREMENTS OF C&MS 730.19.

WORK ZONE INCREASED PENALTIES SIGNS AND SUPPORTS WILL BE MEASURED AS THE NUMBER OF SIGN INSTALLATIONS, INCLUDING THE SIGN AND NECESSARY SUPPORTS. IF A SIGN AND SUPPORT COMBINATION IS REMOVED AND REERECTED AT ANOTHER LOCATION AS DIRECTED BY THE ENGINEER, IT SHALL BE CONSIDERED ANOTHER UNIT.

PAYMENT FOR ACCEPTED QUANTITIES, COMPLETE, IN PLACE
WILL BE MADE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL
BE FULL COMPENSATION FOR ALL MATERIALS, LABOR,
INCIDENTALS AND EQUIPMENT FOR FURNISHING, ERECTING,
MAINTAINING, COVERING DURING SUSPENSION OF WORK, AND
REMOVAL OF THE SIGN AND SUPPORT.

ITEM 614, WORK ZONE INCREASED PENALTIES SIGN 24 EACH

WORK ZONE INCREASED PENALTIES SIGNS WILL BE PLACED AT THE FOLLOWING LOCATIONS:

BEGIN PROJECT - NORTHBOUND ONLY (2)
C.R. 7G INTERCHANGE - RAMP 'A' (1)
C.R. 7G INTERCHANGE - RAMP 'C' (1)
APPROXIMATE SLM. 20.95 (4)
C.R. 56 CONNECTOR INTERCHANGE - RAMP 'E' (1)
C.R. 56 CONNECTOR INTERCHANGE - RAMP 'H' (1)
APPROXIMATE SLM. 22.90 (4)
C.R. 46 INTERCHANGE - RAMP 'A' (1)
C.R. 46 INTERCHANGE - RAMP 'D' (1)
APPROXIMATE SLM. 24.85 (4)

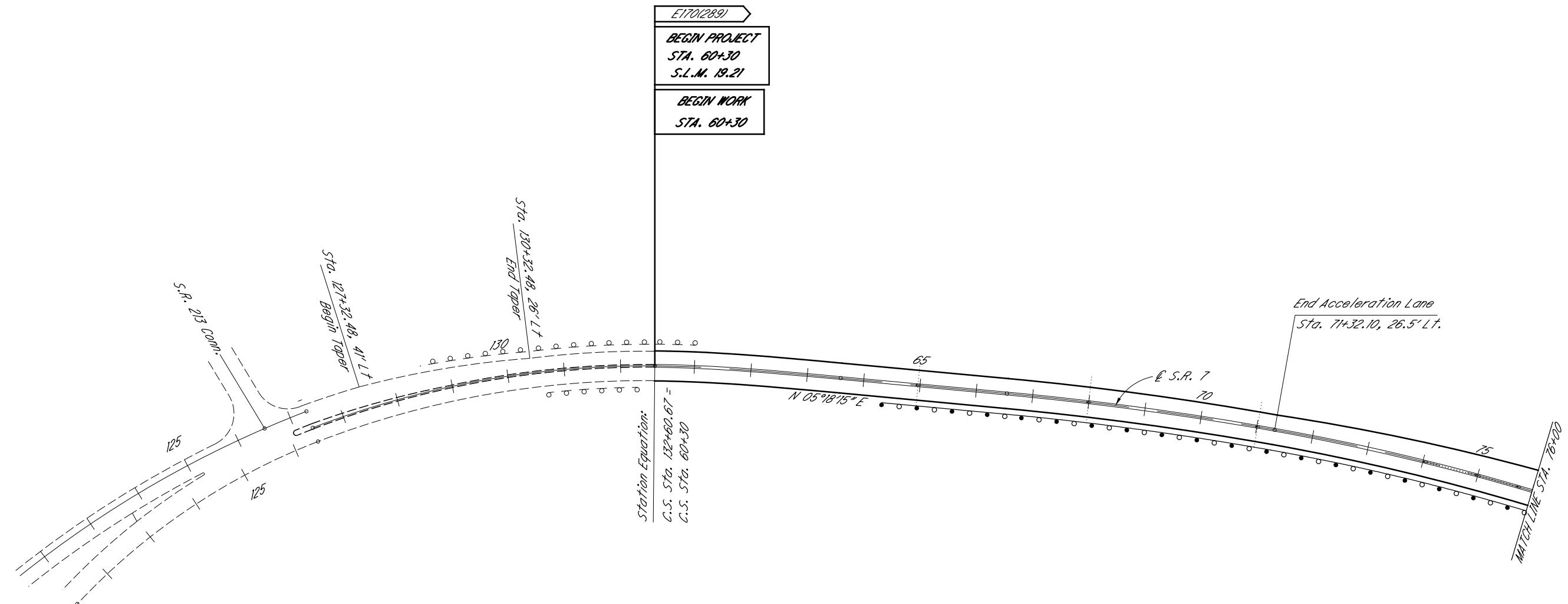
WORK ZONE INCREASED PENALTIES SIGN (R11-H5A) (CONTINUED)

ALEXANDER ST. INTERCHANGE - RAMP 'E' (1)
ALEXANDER ST. INTERCHANGE - RAMP 'H' (1)
END PROJECT - SOUTHBOUND ONLY (2)

CONTRACTOR'S EQUIPMENT - OPERATION AND STORAGE

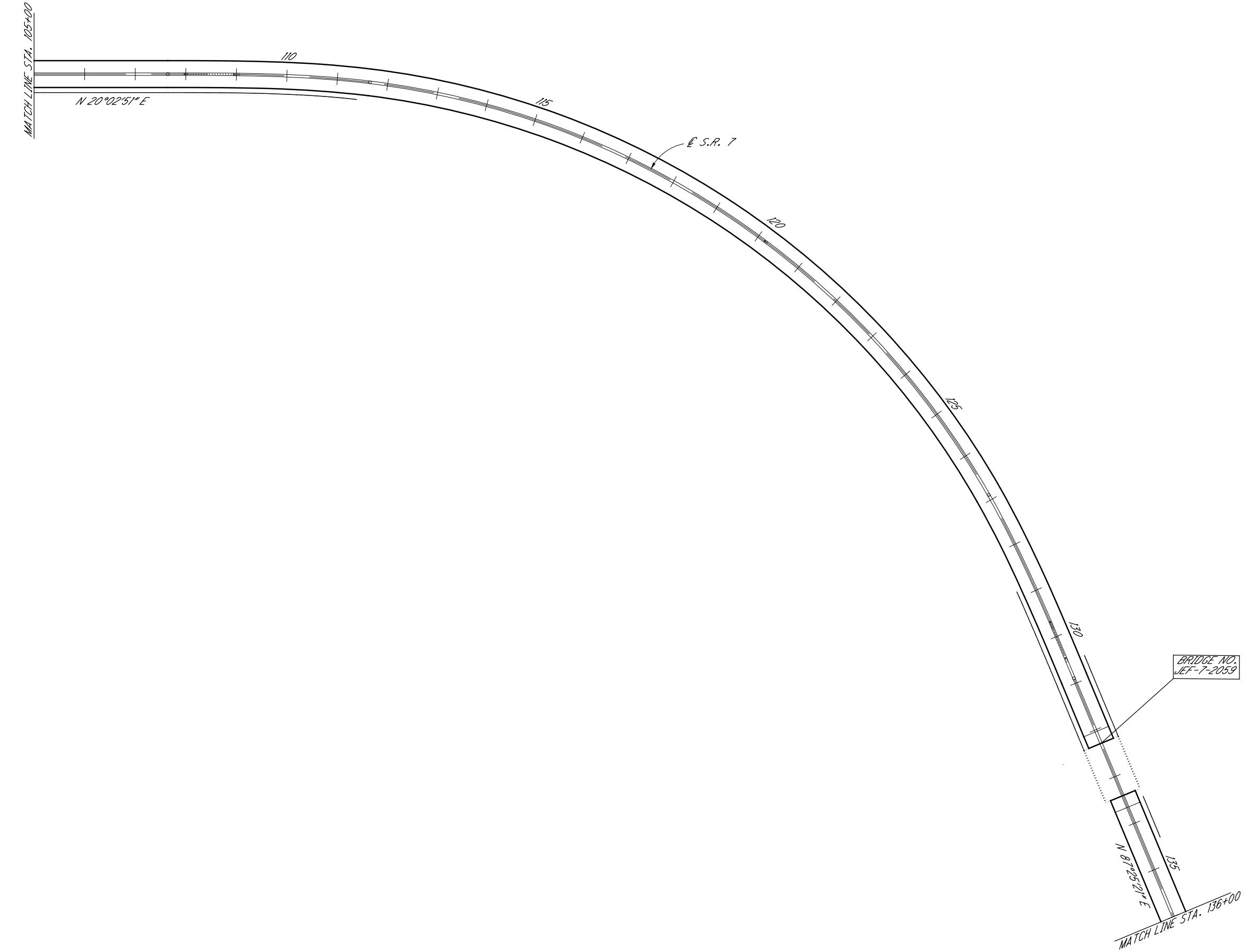
IN ADDITION TO THE REQUIREMENTS OF SECTION 614.03 OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS THE FOLLOWING SHALL APPLY. THE CONTRACTOR'S EQUIPMENT SHALL BE OPERATED IN THE DIRECTION OF TRAFFIC WHERE PRACTICAL. A FLAGGER SHALL BE USED WHERE THE CONTRACTOR'S EQUIPMENT MUST MERGE WITH THE TRAFFIC STREAM. THE CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE EQUIPPED WITH AT LEAST ONE AMBER FLASHING LIGHT.

EQUIPMENT MAY BE PARKED IN AREAS ALONG THE HIGHWAY, THIRTY FEET (30') FROM THE EDGE OF TRAVELED HIGHWAY UNLESS BEHIND GUARDRAIL, WHEN VARIOUS OPERATIONS ARE SCHEDULED TO CONTINUE THE NEXT WORKDAY. ON WEEKENDS OR AT OTHER TIMES OF SUSPENSION OF WORK, THE EQUIPMENT SHALL BE STORED AT A STORAGE AREA REMOVED FROM THE STATE ROUTE RIGHT OF WAY. NO EQUIPMENT SHALL BE PARKED IN THE MEDIAN OF THE HIGHWAY. ADEQUATE BARRICADES AND LIGHT SHALL BE PLACED ON THE PAVEMENT SIDE OF THE EQUIPMENT TO IDENTIFY THE LIMITS OF THE EQUIPMENT. ALL OTHER EQUIPMENT, INCLUDING PRIVATE VEHICLES, SHALL BE STORED AT THE APPROVED CONTRACTOR'S STORAGE AREA.



JEF-7-19.21

MODEL: Sheet1 PAPER SIZE: 17x11 (In.) DATE: 6/27/2022 TIME: 1:39:05 PM USER: whitman
DW: \mhdot-dw.bentley.com\mhdot-dw\02\Documents\01 Active Projects\01 Jefferson\02458-400-Engineering\Roadway\Sheets\02458.GP003.dgn



DESIGN AGENCY

DESIGNER

ADK

REVIEWER

JPB 06-10-22

PROJECT ID

102458

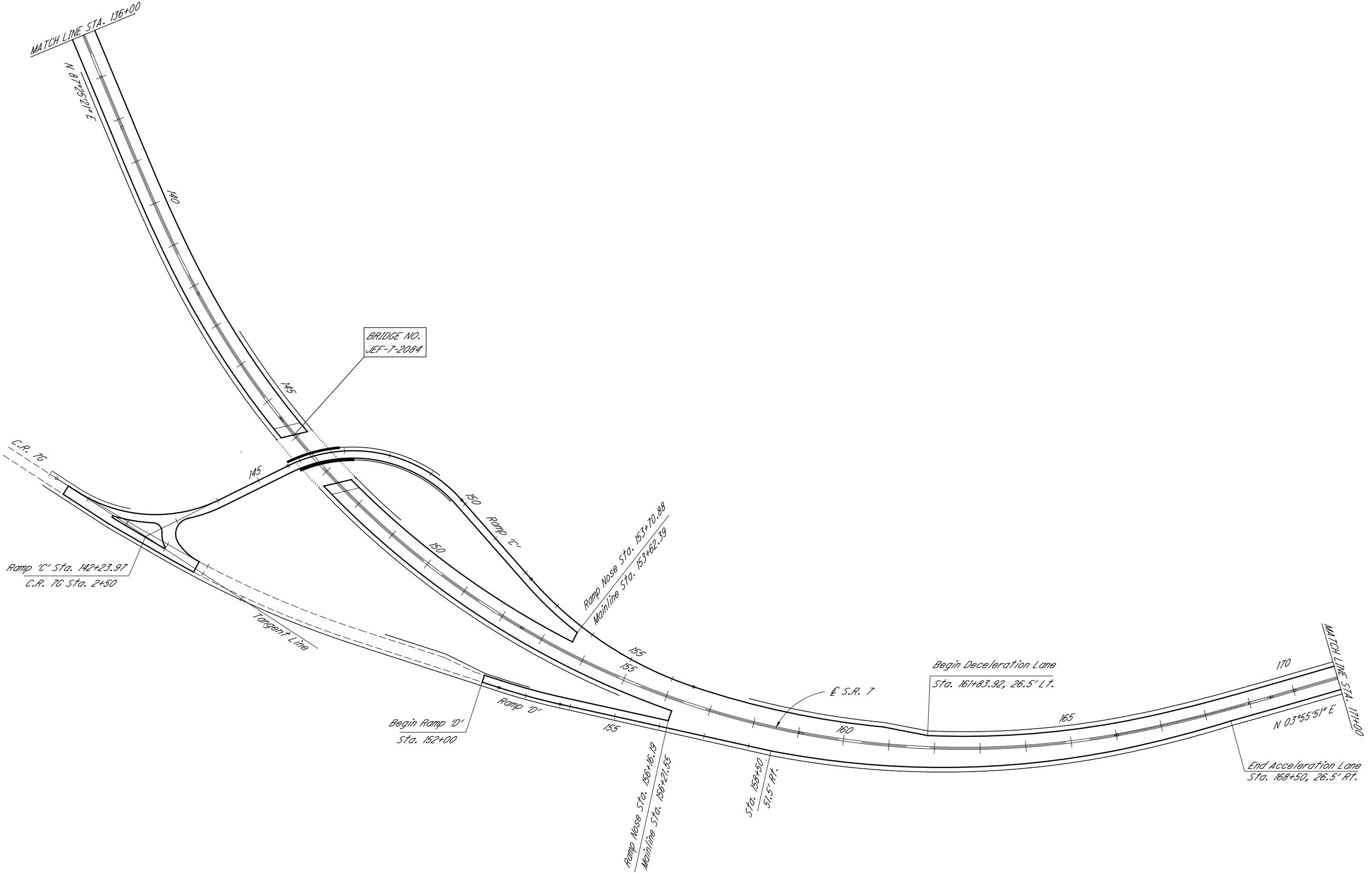
SHEET TOTAL

P.15 39

HORIZONTAL
SCALE IN FEET

0 50 100 200

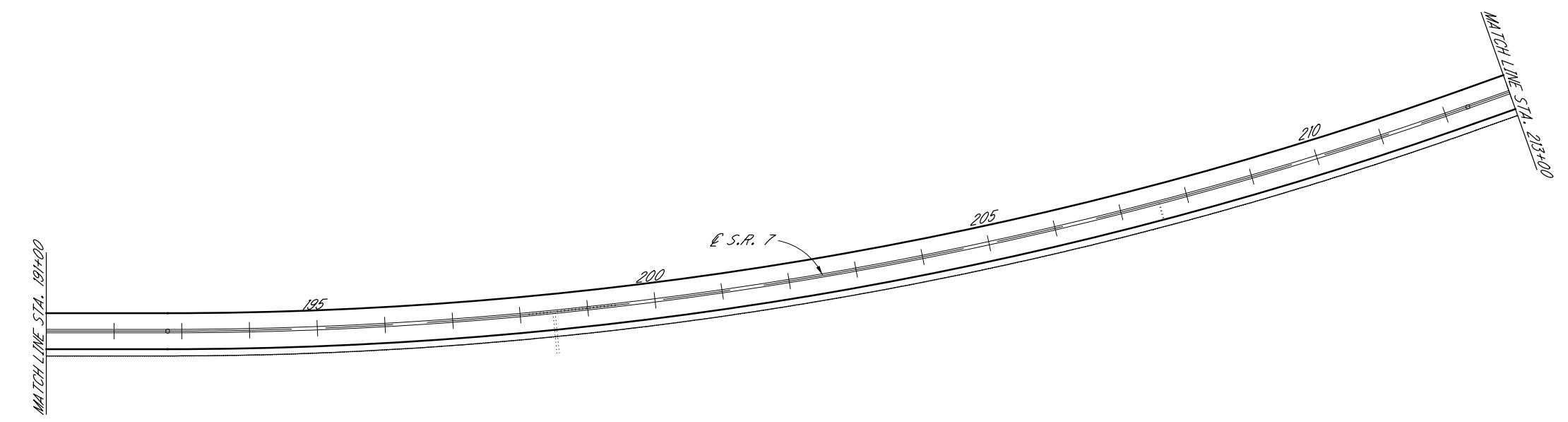
PLAN SHEET
STA 105+00 TO STA 136+00



DESIGNER ADK
REVIEWER JPB 06-10-22
PROJECT ID 102458
SHEET TOTAL P.16 39

PLAN SHEET
STA 136+00 STA 171+00

HORIZONTAL SCALE IN FEET
0 50 100 200



MATCH LINE STA. 171+00

E.S.R. 7

185

200

205

210

15' 7"

MATCH LINE STA. 191+00

E.S.R. 7

175

180

185

190

N 03°55'51"E

MATCH LINE STA. 171+00

MATCH LINE STA. 191+00



DESIGN AGENCY

DESIGNER

ADK

REVIEWER

JPB 06-10-22

PROJECT ID

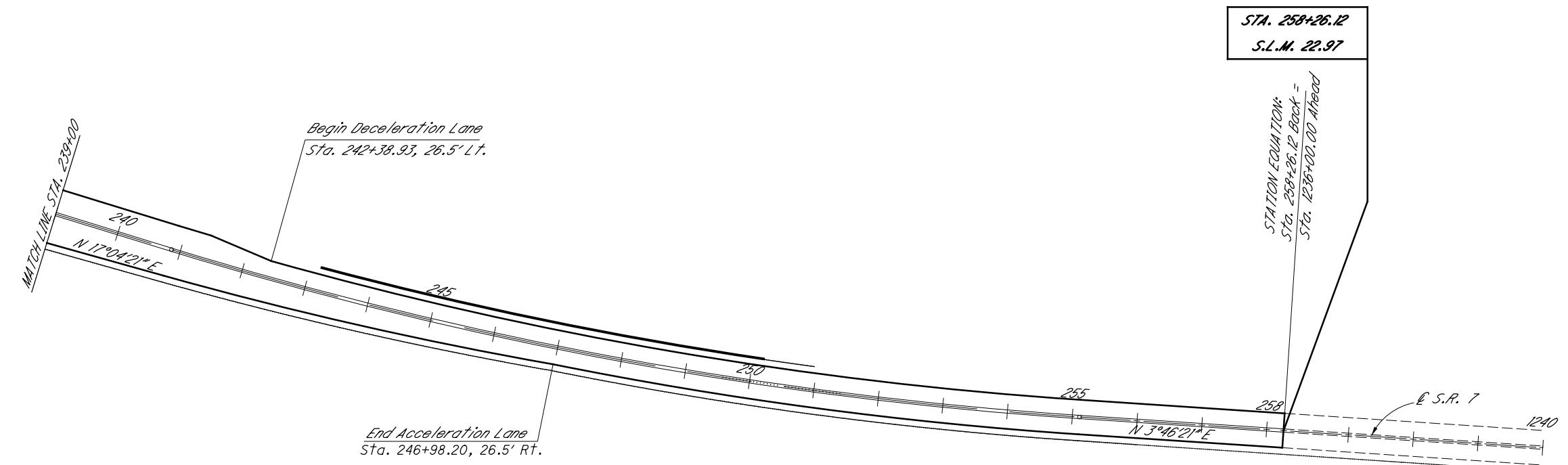
102458

SHEET TOTAL

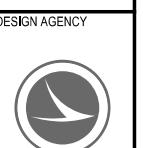
P.17

39

PLAN SHEET
STA 171+00 TO STA 213+00HORIZONTAL
SCALE IN FEET
0 50 100 200

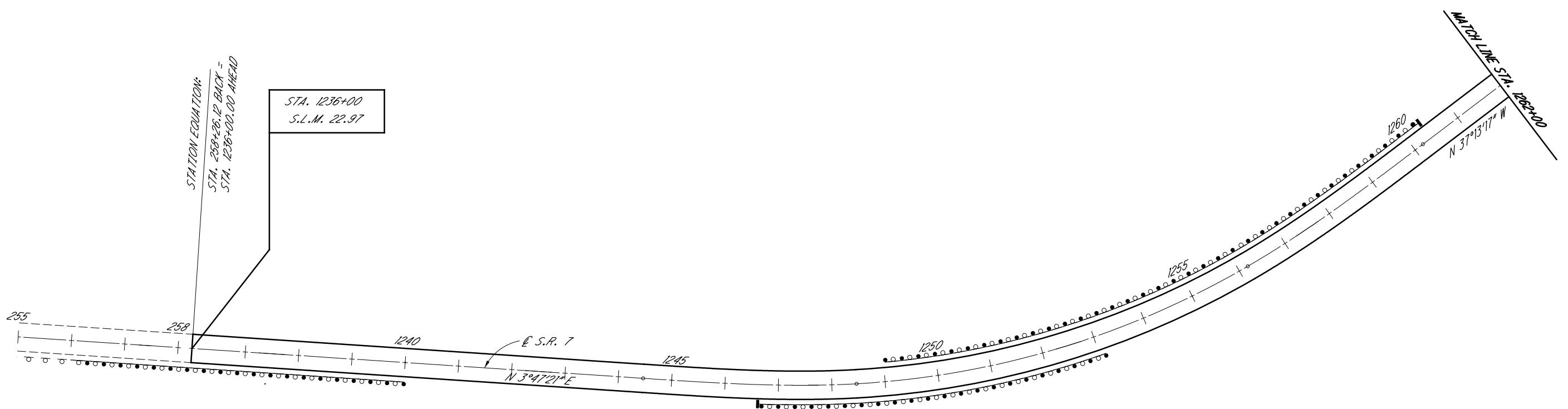


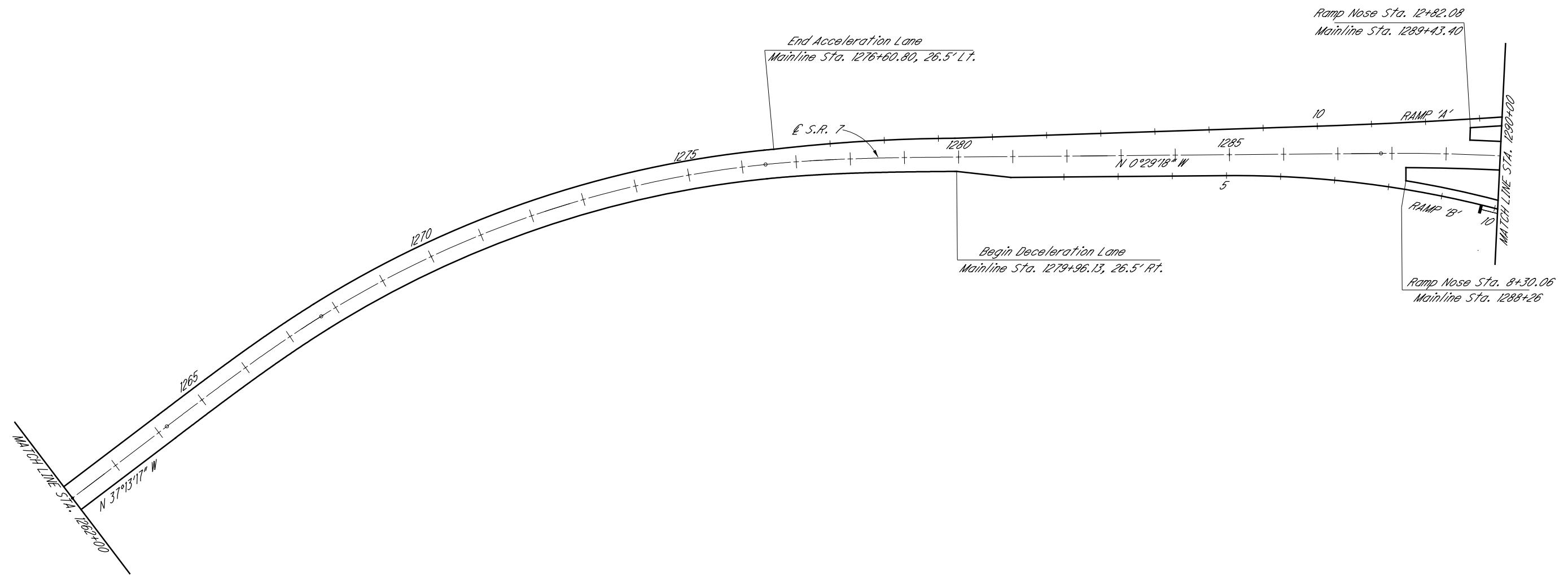
DESIGNER
ADK
REVIEWER
JPB 06-10-22
PROJECT ID
102458
SHEET TOTAL
P.19 39



PLAN SHEET
STA. 239+00 TO STA. 258+26.12

HORIZONTAL
SCALE IN FEET
0 50 100 200





PLAN SHEET
STA 1266+00 TO STA 1290+00



DESIGN AGENCY

DESIGNER

ADK

REVIEWER

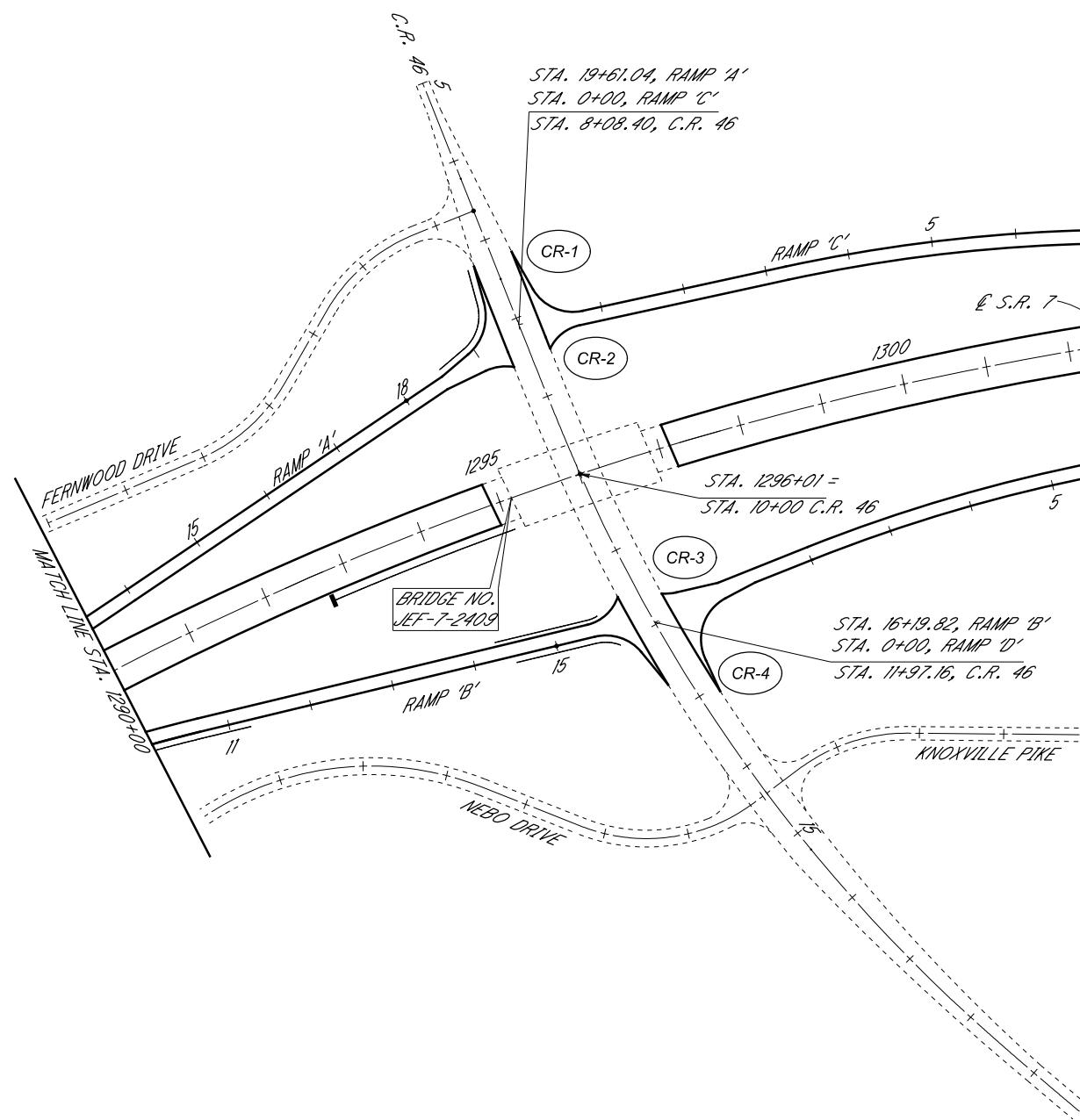
JPB 06-10-22

PROJECT ID

102458

SHEET TOTAL

P.21 39



STA. 19+61.04, RAMP 'A'
STA. 0+00, RAMP 'C'
STA. 8+08.40, C.R. 46

STA. 1296+01 =
STA. 10+00 C.R. 46
STA. 16+19.82, RAMP 'B'
STA. 0+00, RAMP 'D'
STA. 11+97.16, C.R. 46

Ramp Nose Sta. 10+90.44
Mainline Sta. 1306+28.88

Begin Deceleration Lane
Mainline Sta. 1314+77.65, 26.5' Lt.

Ramp Nose Sta. 13+12.54
Mainline Sta. 1309+81.65

PLAN SHEET
STA 1290+00 TO STA 1318+00



DESIGN AGENCY

DESIGNER

ADK

REVIEWER

JPB 06-10-22

PROJECT ID

102458

SHEET TOTAL

P.22 39

FOR CURB RAMP DETAILS AND QUANTITIES, SEE SHEET 37.

HORIZONTAL
SCALE IN FEET
0 50 100 200

MATCH LINE STA. 1318+00

TORONTO CORP. LINE

1320

1325

1330

E.S.P. 7

TORONTO CORP. LINE

N 13°16'01" W

1340

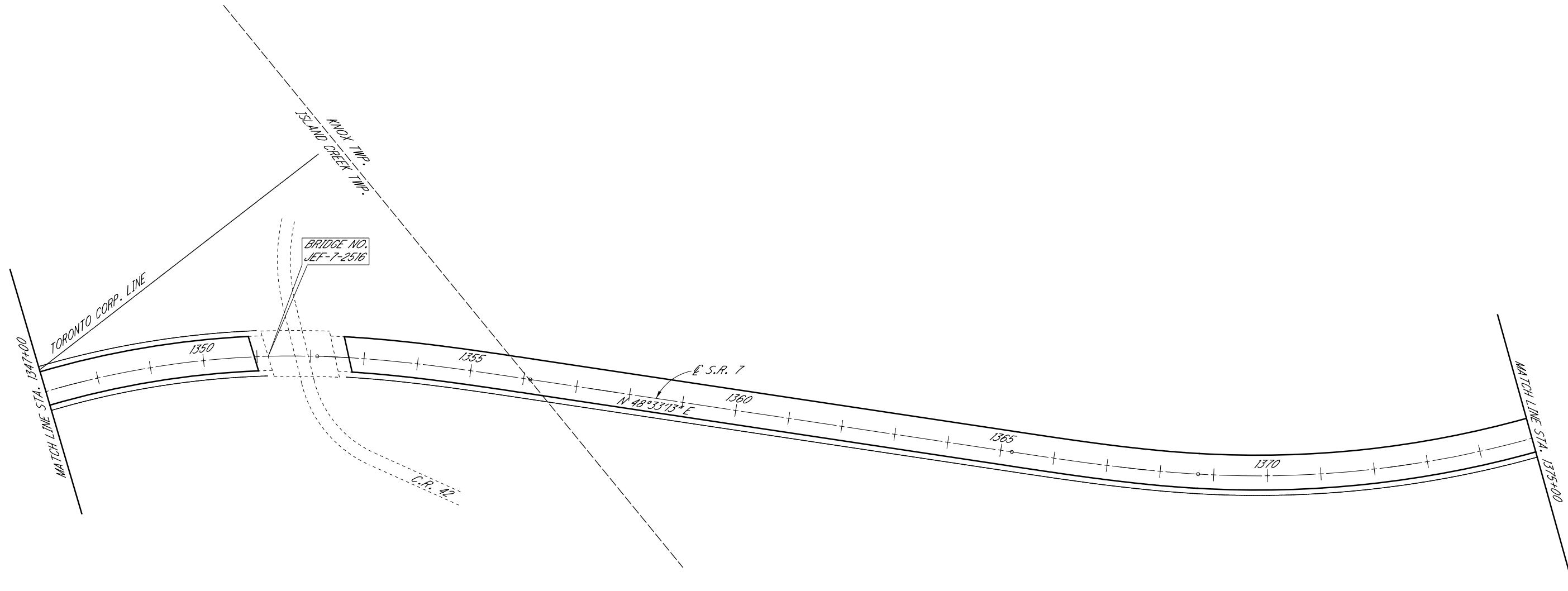
1345

MATCH LINE STA. 1347+00

End Acceleration Lane
Mainline Sta. 1322+07.20, 26.5' RT.Lane Transition
Mainline Sta. 1343+00, 38.5' LT.Lane Transition
Mainline Sta. 1339+50, 26.5' LT.DESIGNER
ADKREVIEWER
JPB 06-10-22PROJECT ID
102458SHEET TOTAL
P.23 39HORIZONTAL
SCALE IN FEET
0 50 100 200PLAN SHEET
STA 1318+00 TO STA 1347+00

JEF-7-19.21

MODEL: Sheet PAPER SIZE: 17x11 (In.) DATE: 6/27/2022 TIME: 1:40:10 PM USER: iwhitman
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DESIGN AGENCY

DESIGNER

ADK

REVIEWER

JPB 06-10-22

PROJECT ID

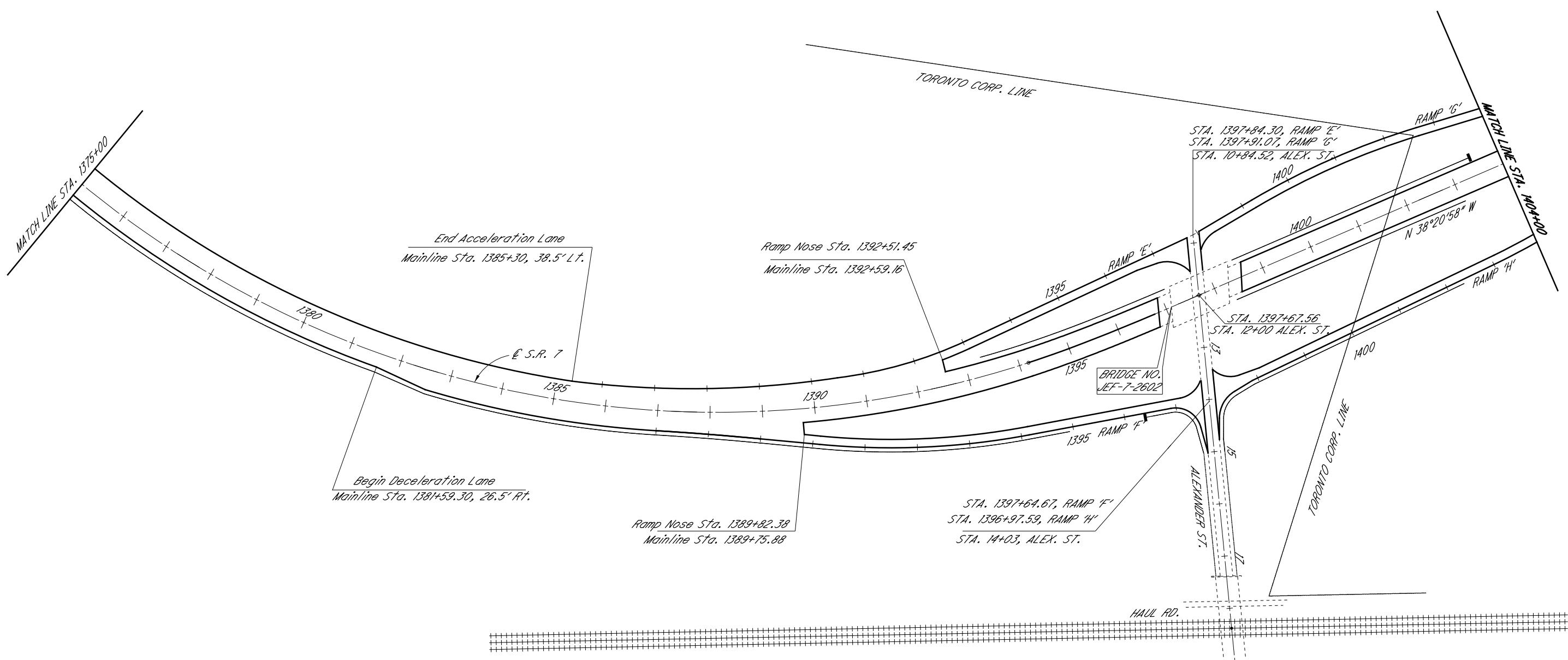
102458

SHEET TOTAL

P.24 39

PLAN SHEET
STA 1347+00 TO STA 1375+00

HORIZONTAL
SCALE IN FEET
0 50 100 200



PLAN SHEET
STA 1375+00 TO STA 1404+00

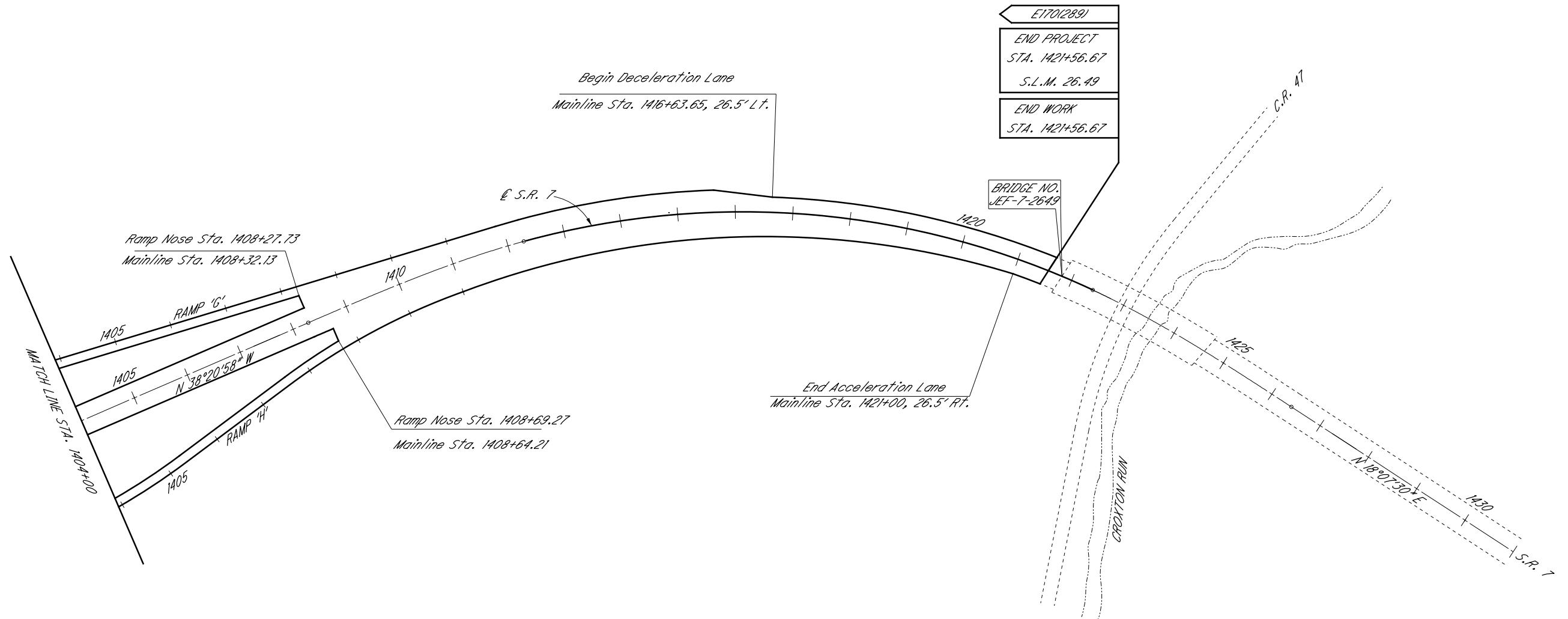
SIGNER

ADR
REVIEWER
B 06-10-2

JECT ID

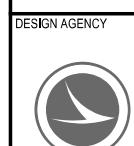
102-100

.25



LOCATION	STATION	LENGTH	WIDTH	TYPICAL	PAVEMENT AREA		254		407	442		REMARKS
							IN.	PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN (1-1/2")	PATCHING PLANED SURFACE (3%)	NON-TRACKING TACK COAT @ 0.085 GAL./S.Y.	IN.	ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE B (447), AS PER PLAN
NORTHBOUND	FROM	TO	FT	FT	SQ YD		SQ YD	SQ YD	GAL	CU YD	CU. YD.	
S.R. 7 MAINLINE												
	60+30.00	89+75.00	2,945.00	24.00	7,853	1 1/2	7,853	236	668	1 1/2	327	327
BR. NO.JEF-7-1979	OMIT (179')											OMIT BRIDGE DECK & APPROACH SLABS
	91+54.00	132+00.00	4,046.00	24.00	10,789	1 1/2	10,789	324	917	1 1/2	450	450
BR. NO. JEF-7-2059	OMIT (163')											OMIT BRIDGE DECK & APPROACH SLABS
	133+63.00	145+65.00	1,202.00	24.00	3,205	1 1/2	3,205	96	272	1 1/2	134	134
BR. NO. JEF-7-2084	OMIT (192')											OMIT BRIDGE DECK & APPROACH SLABS
	147+57.00	229+20.00	8,163.00	24.00	21,768	1 1/2	21,768	653	1,850	1 1/2	907	907
BR. NO. JEF-7-2242	OMIT (174')											OMIT BRIDGE DECK & APPROACH SLABS
	230+94.00	235+45.00	451.00	24.00	1,203	1 1/2	1,203	36	102	1 1/2	50	50
BR. NO. JEF-7-2255	OMIT (301')											OMIT BRIDGE DECK & APPROACH SLABS
	238+46.00	258+26.12	1,980.12	24.00	5,280	1 1/2	5,280	158	449	1 1/2	220	220
DECCEL. LANE RAMP 'B'	78+07.72	86+28.35	820.63	VARIES	1,306	1 1/2	1,306	39	111	1 1/2	54	54
ACCEL. LANE RAMP 'D'	156+21.55	168+50.00	1,228.45	VARIES	2,190	1 1/2	2,190	66	186	1 1/2	91	91
DECCEL. LANE RAMP 'F'	215+46.54	223+45.67	799.13	VARIES	1,308	1 1/2	1,308	39	111	1 1/2	55	55
ACCEL. LANE RAMP 'H'	235+06.56	235+69.92	63.36	VARIES	248	1 1/2	248	7	21	1 1/2	10	10
	238+21.42	246+98.20	876.78	VARIES	1,070	1 1/2	1,070	32	91	1 1/2	45	45
TOTALS CARRIED TO GENERAL SUMMARY							56,220	1,687	4,778	2,343	2,343	01/NHS/PV

ESTIMATED QUANTITIES



DESIGNER
ADK
REVIEWER
JPB 06-10-22
PROJECT ID
102458
SHEET TOTAL
P.27 39

LOCATION	STATION		LENGTH	WIDTH	TYPICAL	PAVEMENT AREA	IN.	254		407	442		442	REMARKS
	FROM	TO						SQ YD	SQ YD		SQ YD	SQ YD	GAL	
SOUUTHBOUND														
S.R. 7 MAINLINE														
	60+30.00	89+75.00	2,945.00	24.00		7,853	1 1/2	7,853	236	668	1 1/2	327	327	
BR. NO.JEF-7-1979	OMIT (179')													OMIT BRIDGE DECK & APPROACH SLABS
	91+54.00	132+00.00	4,046.00	24.00		10,789	1 1/2	10,789	324	917	1 1/2	450	450	
BR. NO. JEF-7-2059	OMIT (163')													OMIT BRIDGE DECK & APPROACH SLABS
	133+63.00	145+65.00	1,202.00	24.00		3,205	1 1/2	3,205	96	272	1 1/2	134	134	
BR. NO. JEF-7-2084	OMIT (192')													OMIT BRIDGE DECK & APPROACH SLABS
	147+57.00	229+20.00	8,163.00	24.00		21,768	1 1/2	21,768	653	1,850	1 1/2	907	907	
BR. NO. JEF-7-2242	OMIT (174')													OMIT BRIDGE DECK & APPROACH SLABS
	230+94.00	235+45.00	451.00	24.00		1,203	1 1/2	1,203	36	102	1 1/2	50	50	
BR. NO. JEF-7-2255	OMIT (301')													OMIT BRIDGE DECK & APPROACH SLABS
	238+46.00	258+26.12	1,980.12	24.00		5,280	1 1/2	5,280	158	449	1 1/2	220	220	
ACCEL. RAMP 'A'	71+32.10	83+00.00	1,167.90	VARIES		1,961	1 1/2	1,961	59	167	1 1/2	82	82	
DECCEL. RAMP 'C'	153+62.39	161+83.92	821.53	VARIES		1,297	1 1/2	1,297	39	110	1 1/2	54	54	
ACCEL. RAMP 'E'	213+50.00	223+50.00	1,000.00	VARIES		1,388	1 1/2	1,388	42	118	1 1/2	58	58	
DECCEL. RAMP 'G'	234+17.63	235+69.92	152.29	VARIES		454	1 1/2	454	14	39	1 1/2	19	19	
	238+21.42	242+38.93	417.51	VARIES		491	1 1/2	491	15	42	1 1/2	20	20	
TOTALS CARRIED TO GENERAL SUMMARY								55,690	1,671	4,733	2,321	2,321	01/NHS/PV	

ESTIMATED QUANTITIES



DESIGNER

ADK

REVIEWER

JPB 06-10-22

PROJECT ID

102458

SHEET TOTAL

P.28 39

LOCATION	STATION	LENGTH	WIDTH	TYPICAL	PAVEMENT AREA	254		407	442		REMARKS	
						IN.	PAVEMENT PLANING, ASPHALT CONCRETE, (1-1/2"), AS PER PLAN	PATCHING PLANED SURFACE (3%)	NON-TRACKING TACK COAT @ 0.085 GAL./S.Y.	IN.	ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE B (447), AS PER PLAN	
	FROM	TO	FT	FT	SQ YD	SQ YD	GAL	CU YD	CU YD			
NORTHBOUND												
S.R. 7 MAINLINE												
	1236+00.00	1294+89.74	5,889.74	24.00	15,706	1 1/2	15,706	471	1,335	1 1/2	654	654
BR. NO. JEF-7-2409	OMIT (223')											OMIT BRIDGE DECK & APPROACH SLABS
	1297+12.74	1350+95.99	5,383.25	24.00	14,355	1 1/2	14,355	431	1,220	1 1/2	598	598
BR. NO. JEF-7-2516	OMIT (175')											OMIT BRIDGE DECK & APPROACH SLABS
	1352+70.99	1396+84.82	4,413.83	24.00	11,770	1 1/2	11,770	353	1,000	1 1/2	490	490
BR. NO. JEF-7-2602	OMIT (169')											OMIT BRIDGE DECK & APPROACH SLABS
	1398+53.82	1421+56.67	2,302.85	24.00	6,141	1 1/2	6,141	184	522	1 1/2	256	256
DECCEL LANE - RAMP B	1279+96.13	1288+26.00	829.87	VARIES	1,384	1 1/2	1,384	42	118	1 1/2	58	58
ACCEL LANE - RAMP D	1309+81.65	1322+07.20	1,225.55	VARIES	2,197	1 1/2	2,197	66	187	1 1/2	92	92
DECCEL LANE - RAMP F	1381+59.30	1389+75.88	816.58	VARIES	1,331	1 1/2	1,331	40	113	1 1/2	55	55
ACCEL LANE - RAMP H	1408+64.21	1421+00.00	1,235.79	VARIES	2,440	1 1/2	2,440	73	207	1 1/2	102	102
SOUTHBOUND												
	1236+00.00	1294+89.74	5,889.74	24.00	15,706	1 1/2	15,706	471	1,335	1 1/2	654	654
BR. NO. JEF-7-2409	OMIT (223')											OMIT BRIDGE DECK & APPROACH SLABS
	1297+12.74	1350+95.99	5,383.25	24.00	14,355	1 1/2	14,355	431	1,220	1 1/2	598	598
BR. NO. JEF-7-2516	OMIT (175')											OMIT BRIDGE DECK & APPROACH SLABS
	1352+70.99	1396+84.82	4,413.83	24.00	11,770	1 1/2	11,770	353	1,000	1 1/2	490	490
BR. NO. JEF-7-2602	OMIT (169')											OMIT BRIDGE DECK & APPROACH SLABS
	1398+53.82	1421+56.67	2,302.85	24.00	6,141	1 1/2	6,141	184	522	1 1/2	256	256
ACCEL LANE - RAMP A	1276+60.80	1289+43.40	1,282.60	VARIES	2,347	1 1/2	2,347	70	200	1 1/2	98	98
DECCEL LANE - RAMP C	1306+28.88	1314+77.65	848.77	VARIES	1,336	1 1/2	1,336	40	114	1 1/2	56	56
ACCEL LANE - RAMP E	1385+30.00	1392+59.16	729.16	VARIES	761	1 1/2	761	23	65	1 1/2	32	32
DECCEL LANE - RAMP G	1408+32.13	1416+63.65	831.52	VARIES	2,440	1 1/2	2,440	73	207	1 1/2	102	102
TOTALS CARRIED TO GENERAL SUMMARY						110,179	3,305	9,365	4,591	4,591	01/NHS/PV	
ESTIMATED QUANTITIES												
DESIGN AGENCY												
DESIGNER ADK												
REVIEWER JPB 06-10-22												
PROJECT ID 102458												
SHEET TOTAL P.29 39												

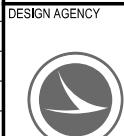
LOCATION	STATION	LENGTH	WIDTH	TYPICAL	PAVEMENT AREA	254		407	408	442		617			618	REMARKS	
						IN.	PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN, (1-1/2")	PATCHING PLANED SURFACE (3%)	NON-TRACKING TACK COAT @ 0.085 GAL./S.Y.	PRIME COAT, AS PER PLAN @ 0.4 GAL./S.Y.	IN.	ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE B (447), AS PER PLAN	COMPACTED AGGREGATE WIDTH	IN.	COMPACTED AGGREGATE, AS PER PLAN	SHOULDER PREPARATION	RUMBLE STRIPS, SHOULDER (ASPHALT CONCRETE)
							SQ YD	SQ YD	GAL	GAL		CY YD	FT.		CY YD	SQ YD	FT.
S.R. 7 N.B.	FROM	TO	FT	FT													
INSIDE SHOULDER																	
	60+30.00	89+75.00	2945	1.5	491	1 1/2	491	15	42		1 1/2	20					
BR. NO.JEF-7-1979	OMIT(179')																
	91+54.00	132+00.00	4046	1.5	674	1 1/2	674	20	57		1 1/2	28.08					
BR. NO. JEF-7-2059	OMIT (163')																
	133+63.00	145+65.00	1202	1.5	200	1 1/2	200	6	17		1 1/2	8					
BR. NO. JEF-7-2084	OMIT (192')																
	147+57.00	229+20.00	8163	1.5	1,361	1 1/2	1,361	41	116		1 1/2	57					
BR. NO. JEF-7-2242	OMIT (174')																
	230+94.00	235+45.00	451	1.5	75	1 1/2	75	2	6		1 1/2	3					
BR. NO. JEF-7-2255	OMIT (301')																
	238+46.00	258+26.12	1980.12	1.5	330	1 1/2	330	10	28		1 1/2	14					
OUTSIDE SHOULDER																	
	60+30.00	89+75.00	2945	8	2,618	1 1/2	2,618	79	223	262	1 1/2	109	2	1 1/2	27	654	2945
BR. NO.JEF-7-1979	OMIT(179')																
	91+54.00	132+00.00	4046	8	3,596	1 1/2	3,596	108	306	360	1 1/2	149.83	2	1 1/2	37	899	4046
BR. NO. JEF-7-2059	OMIT (163')																
	133+63.00	145+65.00	1202	8	1,068	1 1/2	1,068	32	91	107	1 1/2	45	2	1 1/2	11	267	1202
BR. NO. JEF-7-2084	OMIT (192')																
	147+57.00	229+20.00	8163	8	7,256	1 1/2	7,256	218	617	726	1 1/2	302	2	1 1/2	76	1814	8163
BR. NO. JEF-7-2242	OMIT (174')																
	230+94.00	235+45.00	451	8	401	1 1/2	401	12	34	40	1 1/2	17	2	1 1/2	4	100	451
BR. NO. JEF-7-2255	OMIT (301')																
	238+46.00	258+26.12	1980.12	8	1,760	1 1/2	1,760	53	150	176	1 1/2	73	2	1 1/2	18	440	1980
TOTALS CARRIED TO GENERAL SUMMARY						19,830	595	1,686	1,670	826		174	4,175	18,787	01/NHS/PV		

ESTIMATED QUANTITIES

DESIGNER
ADKREVIEWER
JPB 06-10-22PROJECT ID
102458SHEET TOTAL
P.30 39

LOCATION	STATION	LENGTH	WIDTH	TYPICAL	PAVEMENT AREA	254		407	408	442		617			618	REMARKS		
						IN.	PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN, (1-1/2")	PATCHING PLANED SURFACE (3%)	NON-TRACKING TACK COAT @ 0.085 GAL./S.Y.	PRIME COAT, AS PER PLAN @ 0.4 GAL./S.Y.	IN.	ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE B (447), AS PER PLAN	COMPACTED AGGREGATE WIDTH	IN.	COMPACTED AGGREGATE, AS PER PLAN	SHOULDER PREPARATION	RUMBLE STRIPS, SHOULDER (ASPHALT CONCRETE)	
S.R. 7 S.B.	FROM	TO	FT	FT		SQ YD	SQ YD	SQ YD	GAL	GAL		CU YD	FT.	CU YD	SQ YD	FT.		
INSIDE SHOULDER																		
	60+30.00	89+75.00	2945	1.5		491	1 1/2	491	15	42		1 1/2	20					
BR. NO.JEF-7-1979	OMIT(179')																	
	91+54.00	132+00.00	4046	1.5		674	1 1/2	674	20	57		1 1/2	28.08					
BR. NO. JEF-7-2059	OMIT (163')																	
	133+63.00	145+65.00	1202	1.5		200	1 1/2	200	6	17		1 1/2	8					
BR. NO. JEF-7-2084	OMIT (192')																	
	147+57.00	229+20.00	8163	1.5		1,361	1 1/2	1,361	41	116		1 1/2	57					
BR. NO. JEF-7-2242	OMIT (174')																	
	230+94.00	235+45.00	451	1.5		75	1 1/2	75	2	6		1 1/2	3					
BR. NO. JEF-7-2255	OMIT (301')																	
	238+46.00	258+26.12	1980.12	1.5		330	1 1/2	330	10	28		1 1/2	14					
OUTSIDE SHOULDER																		
	60+30.00	89+75.00	2945	8		2,618	1 1/2	2,618	79	223	262	1 1/2	109	2	1 1/2	27	654	
BR. NO.JEF-7-1979	OMIT(179')																	
	91+54.00	132+00.00	4046	8		3,596	1 1/2	3,596	108	306	360	1 1/2	149.83	2	1 1/2	37	899	
BR. NO. JEF-7-2059	OMIT (163')																	
	133+63.00	145+65.00	1202	8		1,068	1 1/2	1,068	32	91	107	1 1/2	45	2	1 1/2	11	267	
BR. NO. JEF-7-2084	OMIT (192')																	
	147+57.00	229+20.00	8163	8		7,256	1 1/2	7,256	218	617	726	1 1/2	302	2	1 1/2	76	1814	
BR. NO. JEF-7-2242	OMIT (174')																	
	230+94.00	235+45.00	451	8		401	1 1/2	401	12	34	40	1 1/2	17	2	1 1/2	4	100	
BR. NO. JEF-7-2255	OMIT (301')																	
	238+46.00	258+26.12	1980.12	8		1,760	1 1/2	1,760	53	150	176	1 1/2	73	2	1 1/2	18	440	
TOTALS CARRIED TO GENERAL SUMMARY							19,830	595	1,686	1,670		826			174	4,175	18,787	01/NHS/PV
TOTAL																	P.31	
39																		

ESTIMATED QUANTITIES

DESIGNER
ADKREVIEWER
JPB 06-10-22PROJECT ID
102458SHEET TOTAL
P.31 39

LOCATION	STATION	LENGTH	WIDTH	TYPICAL	PAVEMENT AREA	254		407	408	442		617			618	REMARKS		
						IN.	PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN, (1-1/2")	PATCHING PLANED SURFACE (3%)	NON-TRACKING TACK COAT @ 0.085 GAL./S.Y.	PRIME COAT, AS PER PLAN @ 0.4 GAL./S.Y.	IN.	ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE B (447), AS PER PLAN	COMPACTED AGGREGATE WIDTH	IN.	COMPACTED AGGREGATE, AS PER PLAN	SHOULDER PREPARATION	RUMBLE STRIPS, SHOULDER (ASPHALT CONCRETE)	
S.R. 7 N.B.	FROM	TO	FT	FT	SQ YD	SQ YD	SQ YD	SQ YD	GAL	GAL	CU YD	FT.	CU YD	SQ YD	FT.	REMARKS		
INSIDE SHOULDER																		
	1236+00.00	1294+89.74	5889.74	1.5	982	1 1/2	982	29	83	1 1/2	41							
BR. NO. JEF-7-2409	OMIT (223')																	
	1297+12.74	1350+95.99	5383.25	1.5	897	1 1/2	897	27	76	1 1/2	37							
BR. NO. JEF-7-2516	OMIT (175')																	
	1352+70.99	1396+84.82	4413.83	1.5	736	1 1/2	736	22	63	1 1/2	31							
BR. NO. JEF-7-2602	OMIT (169')																	
	1398+53.82	1421+56.67	2302.85	1.5	384	1 1/2	384	12	33	1 1/2	16							
OUTSIDE SHOULDER																		
	1236+00.00	1294+89.74	5889.74	8	5,235	1 1/2	5,235	157	445	524	1 1/2	218	2	1 1/2	55	1309	5890	
BR. NO. JEF-7-2409	OMIT (223')																	
	1297+14.74	1350+95.99	5381.25	8	4,783	1 1/2	4,783	144	407	478	1 1/2	199	2	1 1/2	50	1196	5381	
BR. NO. JEF-7-2516	OMIT (175')																	
	1352+70.99	1396+84.82	4413.83	8	3,923	1 1/2	3,923	118	333	392	1 1/2	163	2	1 1/2	41	981	4414	
BR. NO. JEF-7-2602	OMIT (169')																	
	1398+53.82	1421+56.67	2302.85	8	2,047	1 1/2	2,047	61	174	205	1 1/2	85	2	1 1/2	21	512	2303	
TOTALS CARRIED TO GENERAL SUMMARY							18,987	570	1,614	1,599		791			167	3,997	17,988	01/NHS/PV

ESTIMATED QUANTITIES



DESIGNER

ADK

REVIEWER

JPB 06-10-22

PROJECT ID

102458

SHEET TOTAL

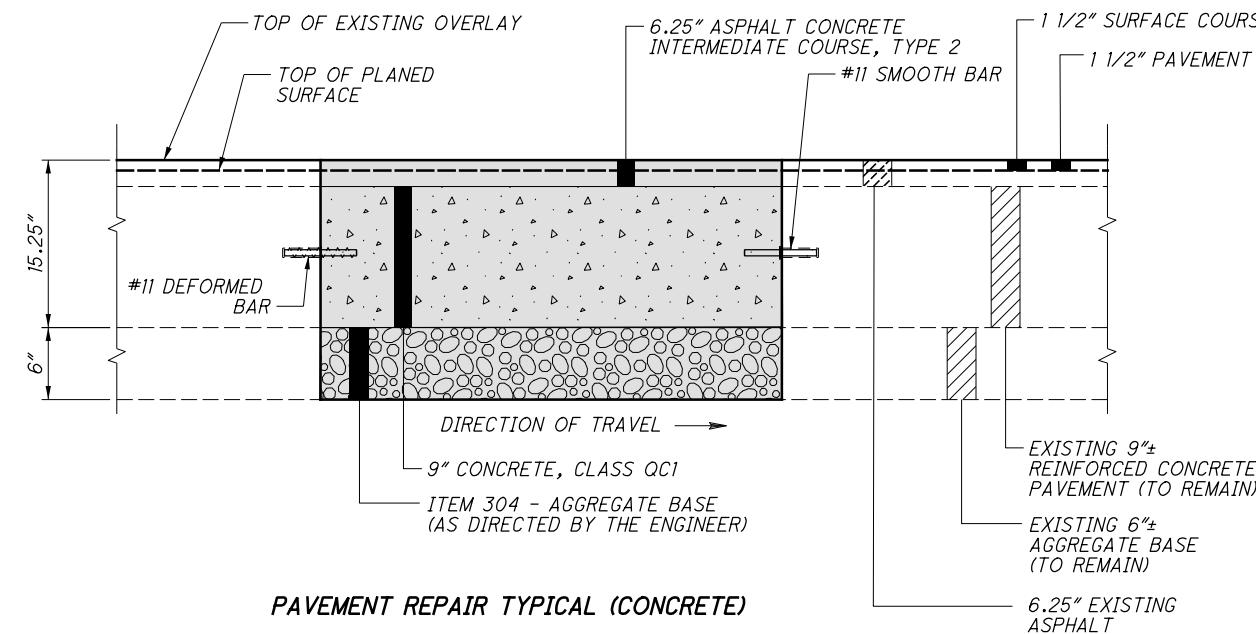
P.32 39

LOCATION	STATION	LENGTH	WIDTH	TYPICAL	PAVEMENT AREA	254		407	408	442		617			618	REMARKS		
						IN.	PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN, (1-1/2")	PATCHING PLANED SURFACE (3%)	NON-TRACKING TACK COAT @ 0.085 GAL./S.Y.	PRIME COAT, AS PER PLAN @ 0.4 GAL./S.Y.	IN.	ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE B (447), AS PER PLAN	COMPACTED AGGREGATE WIDTH	IN.	COMPACTED AGGREGATE, AS PER PLAN	SHOULDER PREPARATION	RUMBLE STRIPS, SHOULDER (ASPHALT CONCRETE)	
S.R. 7 S.B.	FROM	TO	FT	FT	SQ YD	SQ YD	SQ YD	SQ YD	GAL	GAL	CU YD	FT.	CU YD	SQ YD	FT.			
INSIDE SHOULDER																		
	1236+00.00	1294+89.74	5889.74	1.5	982	1 1/2	982	29	83		1 1/2	41						
BR. NO. JEF-7-2409	OMIT (223')																	
	1297+12.74	1350+95.99	5383.25	1.5	897	1 1/2	897	27	76		1 1/2	37						
BR. NO. JEF-7-2516	OMIT (175')																	
	1352+70.99	1396+84.82	4413.83	1.5	736	1 1/2	736	22	63		1 1/2	31						
BR. NO. JEF-7-2602	OMIT (169')																	
	1398+53.82	1421+56.67	2302.85	1.5	384	1 1/2	384	12	33		1 1/2	16						
OUTSIDE SHOULDER																		
	1236+00.00	1294+89.74	5889.74	8	5,235	1 1/2	5,235	157	445	524	1 1/2	218	2	1 1/2	55	1309		
BR. NO. JEF-7-2409	OMIT (223')																	
	1297+14.74	1350+95.99	5381.25	8	4,783	1 1/2	4,783	144	407	478	1 1/2	199	2	1 1/2	50	1196		
BR. NO. JEF-7-2516	OMIT (175')																	
	1352+70.99	1396+84.82	4413.83	8	3,923	1 1/2	3,923	118	333	392	1 1/2	163	2	1 1/2	41	981		
BR. NO. JEF-7-2602	OMIT (169')																	
	1398+53.82	1421+56.67	2302.85	8	2,047	1 1/2	2,047	61	174	205	1 1/2	85	2	1 1/2	21	512		
TOTALS CARRIED TO GENERAL SUMMARY						18,987	570	1,614	1,599		791			167	3,997	17,988	01/NHS/PV	

ESTIMATED QUANTITIES

DESIGNER
ADKREVIEWER
JPB 06-10-22PROJECT ID
102458SHEET TOTAL
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LOCATION	STATION	LENGTH	WIDTH	TYPICAL	PAVEMENT AREA	254		407	408	442		617		REMARKS	
						IN.	PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN (1 1/2")	PATCHING PLANED SURFACE (3%)	NON- TRACKING TACK COAT @ 0.085 GAL./S.Y., AS PER PLAN	IN.	ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE B (447), AS PER PLAN	COMPACTED AGGREGATE WIDTH	IN.	COMPACTED AGGREGATE, AS PER PLAN	SHOULDER PREPARATION
RAMPS	FROM	TO	FT	FT	SQ YD	SQ YD	SQ YD	GAL	GAL	CU YD	FT	CU YD	SQ YD		
CO RD. 46 INTERCHANGE															
RAMP A															
MAINLINE	12+82.08	18+48.00	565.92	16.00		1,006	1 1/2	1,006	30	86	1 1/2	41.92			
MAINLINE	18+48.00	19+36.47	88.47	VARIES		339	1 1/2	339	10	29	1 1/2	14.14			
LEFT SHOULDER	12+82.08	18+48.00	565.92	3.00		189	1 1/2	189	6	16	1 1/2	7.86	2	1 1/2	
LEFT SHOULDER	18+48.00	19+36.47	88.47	3.00		29	1 1/2	29	1	3	1 1/2	1.23	2	1 1/2	
RIGHT SHOULDER	12+82.08	18+48.00	565.92	6.00		377	1 1/2	377	11	32	1 1/2	15.72	2	1 1/2	
RIGHT SHOULDER	18+48.00	19+36.47	88.47	6.00		59	1 1/2	59	2	5	1 1/2	2.46	2	1 1/2	
RAMP B															
MAINLINE	8+30.06	15+41.66	711.60	16.00		1,265	1 1/2	1,265	38	108	1 1/2	52.71			
MAINLINE	15+41.66	15+94.86	53.20	VARIES		209	1 1/2	209	6	18	1 1/2	8.69			
LEFT SHOULDER	8+30.06	15+41.66	711.60	3.00		237	1 1/2	237	7	20	63	1 1/2	9.88	2	1 1/2
LEFT SHOULDER	15+41.66	15+94.86	53.20	3.00		18	1 1/2	18	1	2	1 1/2	0.74	2	1 1/2	
RIGHT SHOULDER	7+94.93	8+44.93	50.00	7.00		39	1 1/2	39	1	3	1 1/2	1.62	2	1 1/2	
RIGHT SHOULDER	8+44.93	15+41.66	696.73	6.00		464	1 1/2	464	14	39	62	1 1/2	19.35	2	1 1/2
RIGHT SHOULDER	15+41.66	15+94.86	53.20	6.00		35	1 1/2	35	1	3	5	1 1/2	1.48	2	1 1/2
RAMP C															
MAINLINE	0+24.33	0+77.44	53.11	VARIES		217	1 1/2	217	7	18	1 1/2	9.03			
MAINLINE	0+77.44	10+90.44	1,013.00	16.00		1,801	1 1/2	1,801	54	153	1 1/2	75.04			
LEFT SHOULDER	0+24.33	0+77.44	53.11	3.00		18	1 1/2	18	1	2	5	1 1/2	0.74	2	1 1/2
LEFT SHOULDER	0+77.44	10+90.44	1,013.00	3.00		338	1 1/2	338	10	29	90	1 1/2	14.07	2	1 1/2
RIGHT SHOULDER	0+24.33	0+77.44	53.11	6.00		35	1 1/2	35	1	3	5	1 1/2	1.48	2	1 1/2
RIGHT SHOULDER	0+77.44	10+62.87	985.43	6.00		657	1 1/2	657	20	56	88	1 1/2	27.37	2	1 1/2
RIGHT SHOULDER	10+62.87	11+12.87	50.00	7.00		39	1 1/2	39	1	3	4	1 1/2	1.62	2	1 1/2
RAMP D															
MAINLINE	0+24.23	1+28.50	104.27	VARIES		418	1 1/2	418	13	36	1 1/2	17.42			
MAINLINE	1+28.50	13+12.54	1,184.04	16.00		2,105	1 1/2	2,105	63	179	1 1/2	87.71			
LEFT SHOULDER	0+24.23	1+28.50	104.27	3.00		35	1 1/2	35	1	3	9	1 1/2	1.45	2	1 1/2
LEFT SHOULDER	1+28.50	13+12.54	1,184.04	3.00		395	1 1/2	395	12	34	105	1 1/2	16.45	2	1 1/2
RIGHT SHOULDER	0+24.23	1+28.50	104.27	6.00		70	1 1/2	70	2	6	9	1 1/2	2.90	2	1 1/2
RIGHT SHOULDER	1+28.50	13+12.54	1,184.04	6.00		789	1 1/2	789	24	67	105	1 1/2	32.89	2	1 1/2
ALEXANDER ST. INTERCHANGE															
RAMP E															
MAINLINE	1392+51.45	1396+58.61	407.16	16.00		724	1 1/2	724	22	62	1 1/2	30.16			
MAINLINE	1396+58.61	1397+71.58	112.97	VARIES		358	1 1/2	358	11	30	1 1/2	14.90			
LEFT SHOULDER	1392+51.45	1396+58.61	407.16	3.00		136	1 1/2	136	4	12	36	1 1/2	5.65	2	1 1/2
LEFT SHOULDER	1396+58.61	1397+71.58	112.97	3.00		38	1 1/2	38	1	3	10	1 1/2	1.57	2	1 1/2
RIGHT SHOULDER	1392+51.45	1397+71.58	520.13	6.00		347	1 1/2	347	10	29	46	1 1/2	14.45	2	1 1/2
RAMP F															
MAINLINE	1389+82.38	1396+89.80	707.42	16.00		1,258	1 1/2	1,258	38	107	1 1/2	52.40			
MAINLINE	1396+89.80	1397+52.62	62.82	VARIES		289	1 1/2	289	9	25	1 1/2	12.05			
LEFT SHOULDER	1389+82.38	1396+89.80	707.42	3.00		236	1 1/2	236	7	20	63	1 1/2	9.83	2	1 1/2
LEFT SHOULDER	1396+89.80	1397+52.62	62.82	3.00		21	1 1/2	21	1	2	6	1 1/2	0.87	2	1 1/2
RIGHT SHOULDER	1389+59.30	1390+09.30	50.00	7.00		39	1 1/2	39	1	3	4	1 1/2	1.62	2	1 1/2
RIGHT SHOULDER	1390+09.30	1396+89.80	680.50	6.00		454	1 1/2	454	14	39	60	1 1/2	18.90	2	1 1/2
RIGHT SHOULDER	1396+89.80	1397+52.62	62.82	6.00		42	1 1/2	42	1	4	6	1 1/2	1		



FOR DETAILS NOT SHOWN SEE STANDARD CONSTRUCTION DRAWING BP-2.5
FOR TRANSVERSE JOINT REPAIR DETAILS AND BP-2.1 FOR LONGITUDINAL JOINT DETAILS

ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC1, AS PER PLAN (SLM 22.97 TO SLM 26.49 ONLY)

THE ESTIMATED QUANTITIES ARE TO BE CONSIDERED APPROXIMATE. A FINAL FIELD REVIEW WILL BE PERFORMED BY ODOT PRIOR TO CONSTRUCTION AND FINAL LOCATIONS WILL BE GIVEN TO THE CONTRACTOR PRIOR TO CONSTRUCTION.

THIS WORK CONSISTS OF REMOVING THE EXISTING ASPHALT CONCRETE, REINFORCED CONCRETE, AND THE AGGREGATE BASE COURSES; SHAPING AND COMPACTING THE EXPOSED MATERIAL; PLACING ITEM 304 AGGREGATE BASE; THEN INSTALLING DOWEL RODS FOLLOWED BY CONCRETE PAVEMENT, CLASS QC1. POUR CONCRETE TO MATCH THE CONCRETE BASE, PAVE WITH ASPHALT INTERMEDIATE TO THE EXISTING SURFACE, AND MILL 1.5" OF ASPHALT WHEN MILLING THE ADJACENT ASPHALT PAVEMENT. THIS WORK SHALL BE COMPLETED BEFORE MILLING AND RESURFACING BEGINS.

ALL OTHER PROVISIONS OF STANDARD CONSTRUCTION DRAWINGS BP-2.1 AND BP-2.5 APPLY.

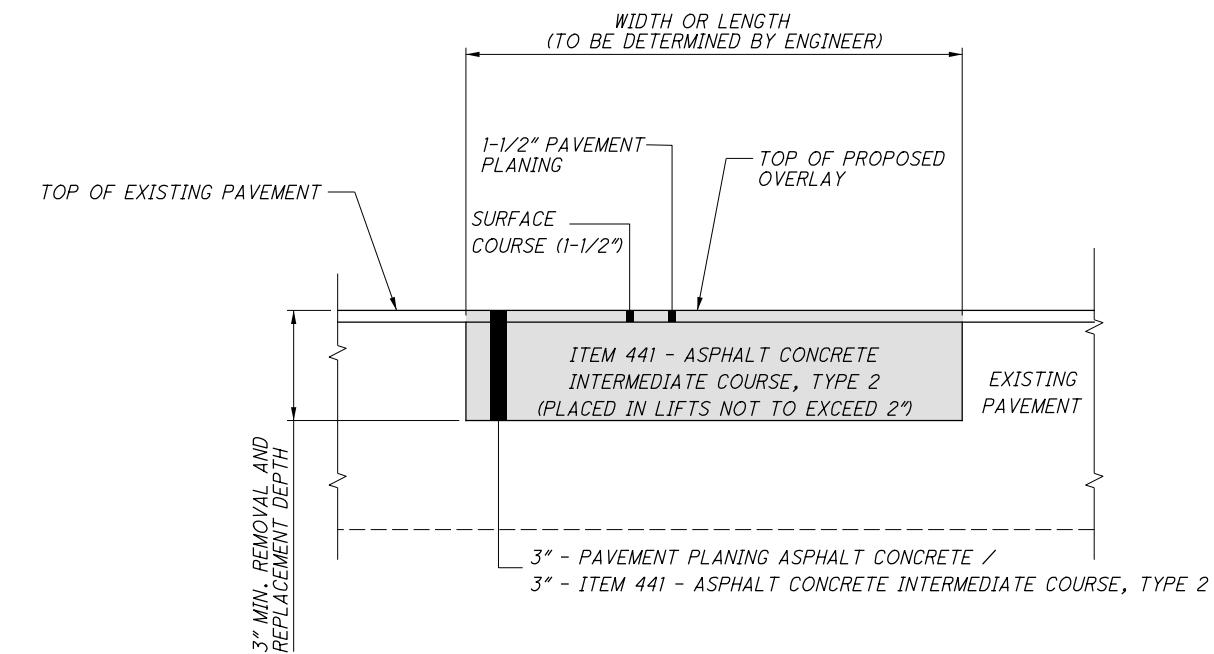
THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN PROVIDED FOR INFORMATION ONLY.

9" CONCRETE, QC1	75 CU. YD.
ITEM 304 - AGGREGATE BASE	38 CU. YD.
ITEM 441 - ASPHALT CONCRETE INTERMEDIATE COURSE	52 CU. YD.
ITEM 509 - EPOXY COATED REINFORCING	1,984 POUNDS
ITEM 510 - DOWEL HOLES WITH NONSHRINK, NONMETALLIC GROUT	320 EACH

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER. FINAL PAYMENT FOR THESE ITEMS SHALL BE FOR THE ACCEPTED QUANTITY COMPLETED IN PLACE.

(SLM 22.97 TO SLM 26.49 ONLY)
ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC1, AS PER PLAN - 300 SQ. YD. (01/NHS/PV)

ITEM 255 - FULL DEPTH PAVEMENT SAWING ----- 1,000 FT. (01/NHS/PV)



PARTIAL DEPTH PAVEMENT REPAIR TYPICAL

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (441)

THIS ITEM OF WORK SHALL ADHERE TO THE REQUIREMENTS DETAILED IN CMS 251 USING ITEM 441 - ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2 FOR THE PAVEMENT LIFTS.

ODOT SHALL PERFORM A FINAL FIELD REVIEW PRIOR TO CONSTRUCTION AND AT THAT TIME, THE ENGINEER WILL DETERMINE AND MARK THE SIZE AND LOCATION FOR EACH REPAIR AND PROVIDE THAT INFORMATION TO THE CONTRACTOR. FINAL PAYMENT FOR THESE LOCATIONS SHALL BE FOR THE ACCEPTED QUANTITY COMPLETED IN PLACE.

ALL PARTIAL DEPTH REPAIRS SHALL BE COMPLETED PRIOR TO COMMENCING PAVING OPERATIONS.

PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS SHALL BE INCLUDED IN THE CONTRACT PRICE FOR ITEM 251 PARTIAL DEPTH REPAIR (441).

ESTIMATED QUANTITIES

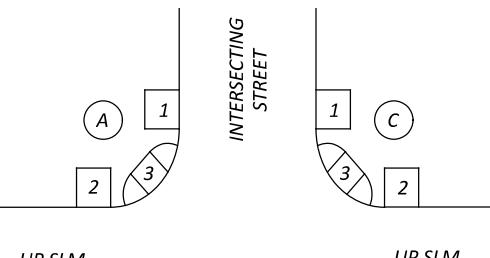
ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (441) - 100 CU YD

TOTAL USE: 100 CU YD (01/NHS/PV)

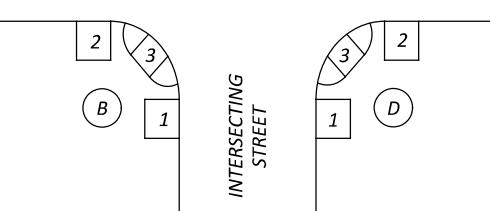


REF. NO.	PART	COUNTY	ROUTE	SLM	INTERSECTING PUBLIC ROAD	LOCATION (SEE LEGEND)	CURB RAMP TYPE	202		608	659	FUNDING	REMARKS
								FT	FT	FT	SQ FT		
CR-1	1	JEF	C.R. 46		RAMP 'C'	A1	D11-A	8		4	32		
CR-2	1	JEF	C.R. 46		RAMP 'C'	C1	D11-A	7		4	28		
CR-3	1	JEF	C.R. 46		RAMP 'D'	A1	D11-C	7.5	5.5	4	26		
CR-4	1	JEF	C.R. 46		RAMP 'D'	C1	D11-A	6		4	24		
CONVERT SQ. FT. TO SQ. YD.													
TOTALS CARRIED TO GENERAL SUMMARY													
								110		110	3		
											28.5		
											SQ YD		

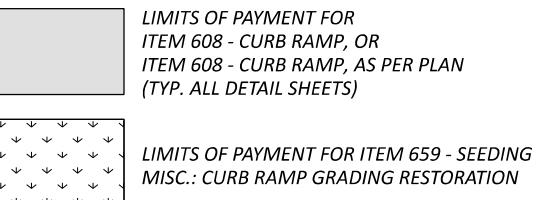
CURB RAMP DETAILS



RAMP LOCATION DETAIL



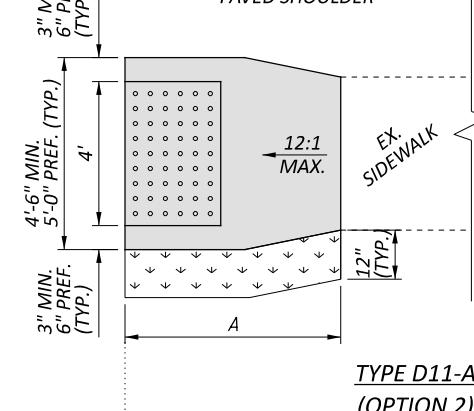
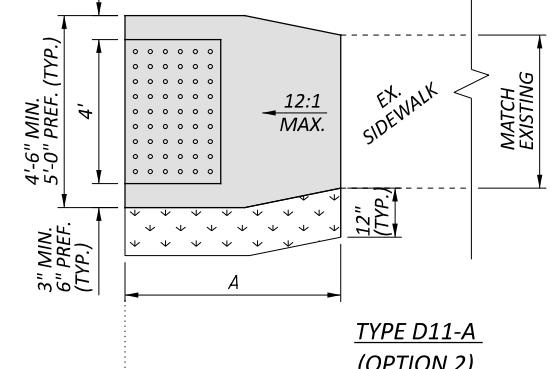
LEGEND



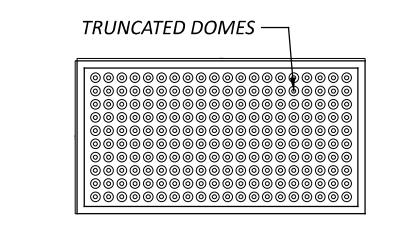
INTERSECTING STREET

INTERSECTING STREET

INTERSECTING STREET

TYPE D11-A
(OPTION 2)

TYPE D11-C



DETECTABLE WARNING INSERT DETAIL

ITEM 659 - SEEDING MISC.: CURB RAMP GRADING RESTORATION

THIS ITEM OF WORK CONSISTS OF REWORKING, OR RESHAPING THE GRADING ADJACENT TO THE NEW CURB RAMPS AND/OR WALK. THE CONTRACTOR SHALL SEED AND MULCH AS PER ITEM 659, AND PROVIDE ALL ADDITIONAL MATERIALS AND EQUIPMENT NECESSARY TO RESTORE THE GRADING TO THE SATISFACTION OF THE ENGINEER.

A QUANTITY OF ONE SQUARE FOOT PER LINEAR FOOT, PER SIDE WITH ACCOMPANYING GRADED BORDER, OF NEW RAMP, AND/OR WALK SHALL BE CALCULATED FOR THIS ITEM OF WORK. FINAL CONVERSION OF QUANTITIES FROM SQUARE FOOT TO SQUARE YARDS SHALL BE PERFORMED IN THE SUMMARY LEVEL. PAYMENT FOR THE AFOREMENTIONED WORK SHALL BE MADE AT THE UNIT PRICE BID FOR ITEM 659, SEEDING MISC.: CURB RAMP GRADING RESTORATION, SQ. YD., AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE ALL WORK TO THE SATISFACTION OF THE ENGINEER.

NOTES:

- SEE SCD BP-7.1 FOR ADDITIONAL DETAILS, SECTIONS, NOTES, AND OTHER CURB RAMP DESIGNS NOT PROVIDED ON THIS SHEET.
- THE NEW CURB RAMP SHALL BE 4'-6" MINIMUM (5'-0" PREFERRED) WIDE. THE DETECTABLE WARNING INSERT SHALL HAVE A THREE INCH MINIMUM BORDER ON THE SIDES. IF THE EXISTING SIDEWALK IS LESS THAN 4'-6" WIDE THE RAMP SHALL TAPER TO MEET THE EXISTING SIDEWALK WIDTH BEGINNING A MINIMUM OF SIX INCHES BEYOND THE BACKSIDE OF THE DETECTABLE WARNING INSERT AS DETAILED IN TYPE D11-A, OPTION 2.

DESIGNER
DAHREVIEWER
JPB 06-10-22PROJECT ID
102458SHEET TOTAL
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PART		SHEET NO.		ROUTE		STATION TO STATION		LANE OR RAMP		SIDE OF LANE OR RAMP		621	646	807	850	FUNDING													
												RPM	RAISED PAVEMENT MARKER REMOVED	CENTER LINE	STOP LINE	CHEVRON MARKING	LANE ARROW	WROUGHT IRON											
												SPACING	2-WAY	2-WAY	1-WAY	FT.	EACH	EACH	EACH	SQ FT	MILE	MILE	MILE	FT.	FT.	FT.	FT.		
													WHITE/RED	YELLOW/RED	WHITE														
1	JEF	S.R. 7	60+30.00	258+26.12	NORTH BOUND	LT.																							
1	JEF	S.R. 7	CR 7G INTERCHANGE			LANE	120			166	166																		
1	JEF	S.R. 7	CR 7G INTERCHANGE			RT.																							
1	JEF	S.R. 7	CR 7G INTERCHANGE		RAMP B	LT.	80	8		8	8																		
1	JEF	S.R. 7	CR 7G INTERCHANGE			GORE	40	9		9	9																		
1	JEF	S.R. 7	CR 7G INTERCHANGE			RT.																							
1	JEF	S.R. 7	CR 7G INTERCHANGE		RAMP D	CTR.					0.05								2										
1	JEF	S.R. 7	CR 7G INTERCHANGE			LT.	80	6		6	6																		
1	JEF	S.R. 7	CR 7G INTERCHANGE			GORE	40	9		9	9																		
1	JEF	S.R. 7	CR 56 CONNECTOR INTERCHANGE		RAMP F	RT.																							
1	JEF	S.R. 7	CR 56 CONNECTOR INTERCHANGE			LT.	80	8		8	8																		
1	JEF	S.R. 7	CR 56 CONNECTOR INTERCHANGE			GORE	40	11		11	11																		
1	JEF	S.R. 7	CR 56 CONNECTOR INTERCHANGE		RAMP H	RT.																							
1	JEF	S.R. 7	CR 56 CONNECTOR INTERCHANGE			CTR.																							
1	JEF	S.R. 7	CR 56 CONNECTOR INTERCHANGE			LT.	80	7		7	7																		
1	JEF	S.R. 7	CR 56 CONNECTOR INTERCHANGE		RAMP H	GORE	40	8		8	8																		
1	JEF	S.R. 7	CR 56 CONNECTOR INTERCHANGE			RT.																							
1	JEF	S.R. 7	CR 56 CONNECTOR INTERCHANGE			LT.	120			166	166																		
1	JEF	S.R. 7	CR 7G INTERCHANGE		RAMP A	LANE																							
1	JEF	S.R. 7	CR 7G INTERCHANGE			RT.																							
1	JEF	S.R. 7	CR 7G INTERCHANGE			LT.	80	15		15	15																		
1	JEF	S.R. 7	CR 7G INTERCHANGE		RAMP C	GORE	40	8		8	8																		
1	JEF	S.R. 7	CR 7G INTERCHANGE			RT.																							
1	JEF	S.R. 7	CR 7G INTERCHANGE			CTR.																							
1	JEF	S.R. 7	CR 7G INTERCHANGE		RAMP C CONNECTOR	LT.	80	15		15	15																		
1	JEF	S.R. 7	CR 7G INTERCHANGE			GORE	40	9		9	9																		
1	JEF	S.R. 7	CR 7G INTERCHANGE			RT.																							
1	JEF	S.R. 7	CR 56 CONNECTOR INTERCHANGE		RAMP E	CTR.																							
1	JEF	S.R. 7	CR 56 CONNECTOR INTERCHANGE			RT.																							
1	JEF	S.R. 7	CR 56 CONNECTOR INTERCHANGE			LT.	80	9		9	9																		
1	JEF	S.R. 7	CR 56 CONNECTOR INTERCHANGE		RAMP G	GORE	40	3		3	3																		
1	JEF	S.R. 7	CR 56 CONNECTOR INTERCHANGE			RT.																							
1	JEF	S.R. 7	CR 56 CONNECTOR INTERCHANGE			CTR.																							
SUB-TOTALS: (CARRIED TO SHEET 38)										473	473	0.09	73			1	8	110	17.25	7.50	3546	6022	24.75	6022	3546	01/NHS/PV			

TRAFFIC CONTROL SUB-SUMMARY

PART		SHEET NO.		STATION TO STATION		LANE OR RAMP	SIDE OF LANE OR RAMP	621	646	807	850	FUNDING														
								RPM	RAISED PAVEMENT MARKER REMOVED	CENTER LINE	STOP LINE	CHEVRON MARKINGS	LANE ARROW	WRONG WAY ARROW	ISLAND MARKING	WET REFLECTIVE EPOXY PAVEMENT MARKING, EDGE LINE 6" (YELLOW)	WET REFLECTIVE EPOXY PAVEMENT MARKING, EDGE LINE 6" (WHITE)	WET REFLECTIVE EPOXY PAVEMENT MARKING, CHANNELIZING LINE 12"	GROOVING FOR 6" RECESSED PAVEMENT MARKING (ASPHALT)	GROOVING FOR 6" RECESSED PAVEMENT MARKING (ASPHALT)	GROOVING FOR 12" RECESSED PAVEMENT MARKING (ASPHALT)					
								2-WAY	2-WAY	1-WAY	RAISED PAVEMENT MARKER REMOVED	MILE	FT.	EACH	EACH	SQ FT	MILE	FT.	MILE	FT.	MILE	FT.				
								WHITE/RED	YELLOW/RED	WHITE	FT.	EACH	EACH	EACH	FT.	FT.	FT.	FT.	FT.	FT.	FT.					
								FT.	EACH	EACH	MILE	FT.	FT.	FT.	FT.	MILE	FT.	FT.	FT.	FT.	FT.	FT.				
1	JEF	S.R. 7	1236+00.00	1421+56.67	NORTH BOUND	LT.											3.51							3.51		
						LANE	120			156	156							3.51							3.51	
						RT.												3.51							3.51	
1	JEF	S.R. 7	CR 46 INTERCHANGE		RAMP B	LT.	80		10		10							0.14				627	0.14	627		
						GORE	40	11			11							0.16				412			0.16	
						RT.												0.25				849	0.25	849		
1	JEF	S.R. 7	CR 46 INTERCHANGE		RAMP D	LT.	80		17		17							0.26				377			0.26	
						GORE	40	10			10							0.25				849	0.25	849		
						RT.												0.26				377			0.26	
1	JEF	S.R. 7	ALEXANDER ST.		RAMP F	LT.	80		10		10							0.15				644	0.15	644		
						GORE	40	9			9							0.16				353			0.16	
						RT.												0.16				353			0.16	
1	JEF	S.R. 7	ALEXANDER ST.		RAMP H	LT.	80		15		15							0.22				748	0.22	748		
						GORE	40	13			13							0.23				480			0.23	
						RT.												0.23				480			0.23	
1	JEF	S.R. 7	1236+00.00	1421+56.67	SOUTH BOUND	LT.												3.51							3.51	
						LANE	120			156	156							3.51							3.51	
						RT.												3.51							3.51	
1	JEF	S.R. 7	1343+00.00	1389+29.52	SOUTH BOUND	LANE	120			40	40							0.88				890	0.12	890		
						LT.	80		9		9						0.12				394			0.14		
						GORE	40	10			10						0.14				394			0.14		
1	JEF	S.R. 7	CR 46 INTERCHANGE		RAMP A	LT.	80		14		14							0.2				690	0.20	690		
						GORE	40	7			7							0.22				322			0.22	
						RT.												0.22				322			0.22	
1	JEF	S.R. 7	CR 46 INTERCHANGE		RAMP C	LT.	80		7		7							0.1				400	0.10	400		
						GORE	40	9			9							0.10				324			0.10	
						RT.												0.10				324			0.10	
1	JEF	S.R. 7	ALEXANDER ST.		RAMP E	LT.	80		13		13							0.2				595	0.20	595		
						GORE	40	11			11							0.20				478			0.20	
						RT.												0.20				478			0.20	
1	JEF	S.R. 7	ALEXANDER ST.		RAMP G	LT.												2								
						GORE												2								
						RT.												2								
						CTR.																				
SUB TOTALS (FROM THIS SHEET):									527		527	0	220		0	8	0	16.89	7.90	3140	5443	24.79	5443	3140	01/NHS/PV	
SUB TOTALS (FROM SHEET 37):									473		473	0.09	73		1	8	110	17.25	7.50	3546	6022	24.75	6022	3546	01/NHS/PV	
TOTALS CARRIED TO GENERAL SUMMARY									1,000		1,000	0.09	293		1	16	110	34.14	15.40	6,686	11,465	49.54	11,465	6,686	01/NHS/PV	

TRAFFIC CONTROL SUB-SUMMARY

DESIGN AGENCY
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