

LOCATION MAP

LATITUDE: N 40° 24' 30" LONGITUDE: W 80° 36' 55"



PORTION TO BE IMPROVED	
INTERSTATE HIGHWAY	
FEDERAL ROUTES	
STATE ROUTES	
COUNTY & TOWNSHIP ROADS	
OTHER ROADS	

DESIGN DESIGNATION	19.21 TO 19.43	19.43 TO 21.03	21.03 TO 22.53	22.53 TO 23.95	23.95 TO 24.11	24.11 TO 25.80	25.80 TO 26.23	26.23 TO 26.49
CURRENT ADT (2023)	13,500	11,500	15,500	13,000	9,000	9,000	8,900	8,800
DESIGN YEAR ADT (2035)	13,500	11,500	17,000	13,000	9,700	9,600	9,200	8,800
DESIGN HOURLY VOLUME (2035)	1,600	1,200	1,700	1,300	950	950	1,200	900
DIRECTIONAL DISTRIBUTION	51.6%	50.8%	51.6%	51.6%	54.2%	56.4%	57.2%	58%
TRUCKS (24 HOUR B&C)	10%	10%	7%	10%	10%	10%	10%	14%
DESIGN SPEED	55 MPH	55 MPH	55 MPH	55 MPH	55 MPH	55 MPH	55 MPH	55 MPH
LEGAL SPEED	55 MPH	55 MPH	55 MPH	55 MPH	55 MPH	55 MPH	55 MPH	55 MPH
DESIGN FUNCTIONAL CLASSIFICATION:								
02 - PRINCIPAL ARTERIAL FREEWAY (RURAL)								
NHS PROJECT	YES	YES	YES	YES	YES	YES	YES	YES

DESIGN EXCEPTIONS

NONE REQUIRED

ADA DESIGN WAIVERS

NONE REQUIRED

UNDERGROUND UTILITIES


Contact Two Working Days
Before You Dig

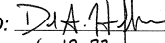

OHIO811.org
Before You Dig

OHIO811, 8-1-1, or 1-800-362-2764
(Non members must be called directly)

PLAN PREPARED BY:
ODOT DISTRICT 11
ENGINEERING
NEW PHILADELPHIA, OHIO

ENGINEER'S SEAL:



SIGNED: 
DATE: 6-10-22

STANDARD CONSTRUCTION DRAWINGS										SUPPLEMENTAL SPECIFICATIONS	SPECIAL PROVISIONS
BP-2.1	1-21-22	DM-4.3	1-15-16	MT-95.30	7-19-19	TC-41.20	10-18-13			800-2019	7-15-22
BP-2.5	1-21-22	DM-4.4	1-15-16	MT-95.45	1-17-20	TC-42.20	10-18-13			807	1-21-22
BP-3.1	1-21-22			MT-95.50	7-21-17	TC-52.10	10-18-13			808	1-18-19
BP-7.1	1-21-22			MT-98.10	1-17-20	TC-52.20	1-15-21			821	4-20-12
BP-9.1	1-18-19			MT-98.11	1-17-20	TC-64.10	7-16-21			832	10-19-18
				MT-98.20	4-19-19	TC-65.10	1-17-14			850	4-15-22
				MT-98.22	1-17-20	TC-65.11	7-21-17			908	10-20-17
				MT-98.28	1-17-20	TC-71.10	7-16-21			921	4-20-12
				MT-99.20	4-19-19	TC-72.20	7-20-18				
				MT-101.90	7-17-20						
				MT-104.10	10-16-15						
				MT-105.10	1-17-20						
				MT-110.10	7-19-13						

FEDERAL PROJECT NUMBER

E170(289)

RAILROAD INVOLVEMENT

NONE

PROJECT DESCRIPTION

IMPROVEMENT OF 7.28 MILES OF S.R. 7 IN JEFFERSON COUNTY BY PLANING AND RESURFACING 1.5" ASPHALT SURFACE COURSE, INCLUDING PAVEMENT REPAIRS AND NEW PAVEMENT MARKINGS.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA: 0.000 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA: 0.000 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA: N/A (NOI NOT REQUIRED)

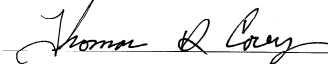
LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

2019 SPECIFICATIONS

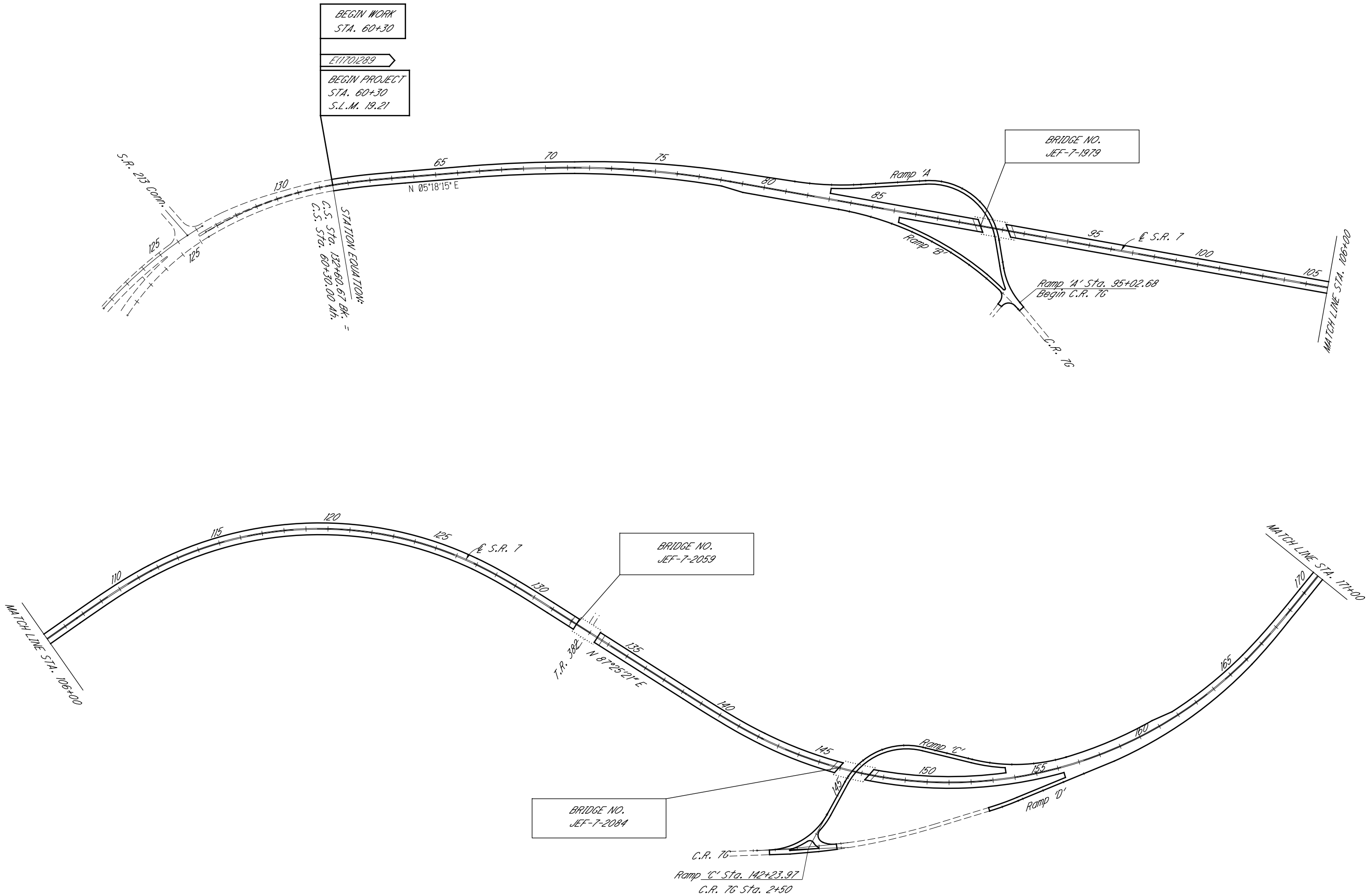
THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

APPROVED 
DATE 6-10-2022 DISTRICT DEPUTY DIRECTOR

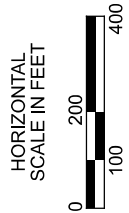
APPROVED _____
DATE _____ DIRECTOR, DEPARTMENT OF TRANSPORTATION





There are no existing landscaped areas within the work limits.

SCHEMATIC PLAN



DESIGN AGENCY



DESIGNER

ADK

REVIEWER

JPB 06-10-22

PROJECT ID

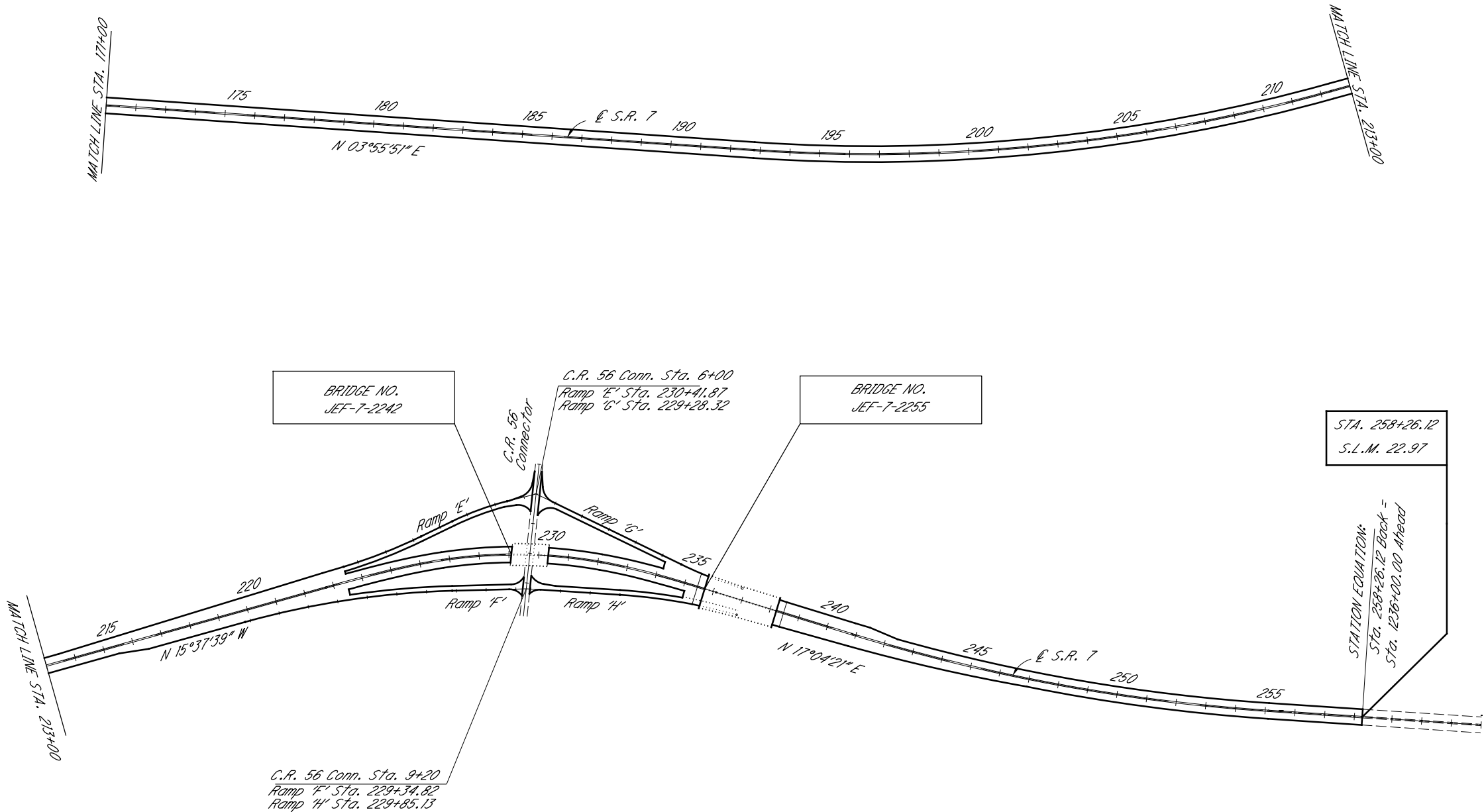
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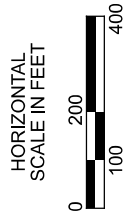
P.2

TOTAL

39



There are no existing landscaped areas within the work limits.



SCHEMATIC PLAN

DESIGN AGENCY



DESIGNER

ADK

REVIEWER

JPB 06-10-22

PROJECT ID

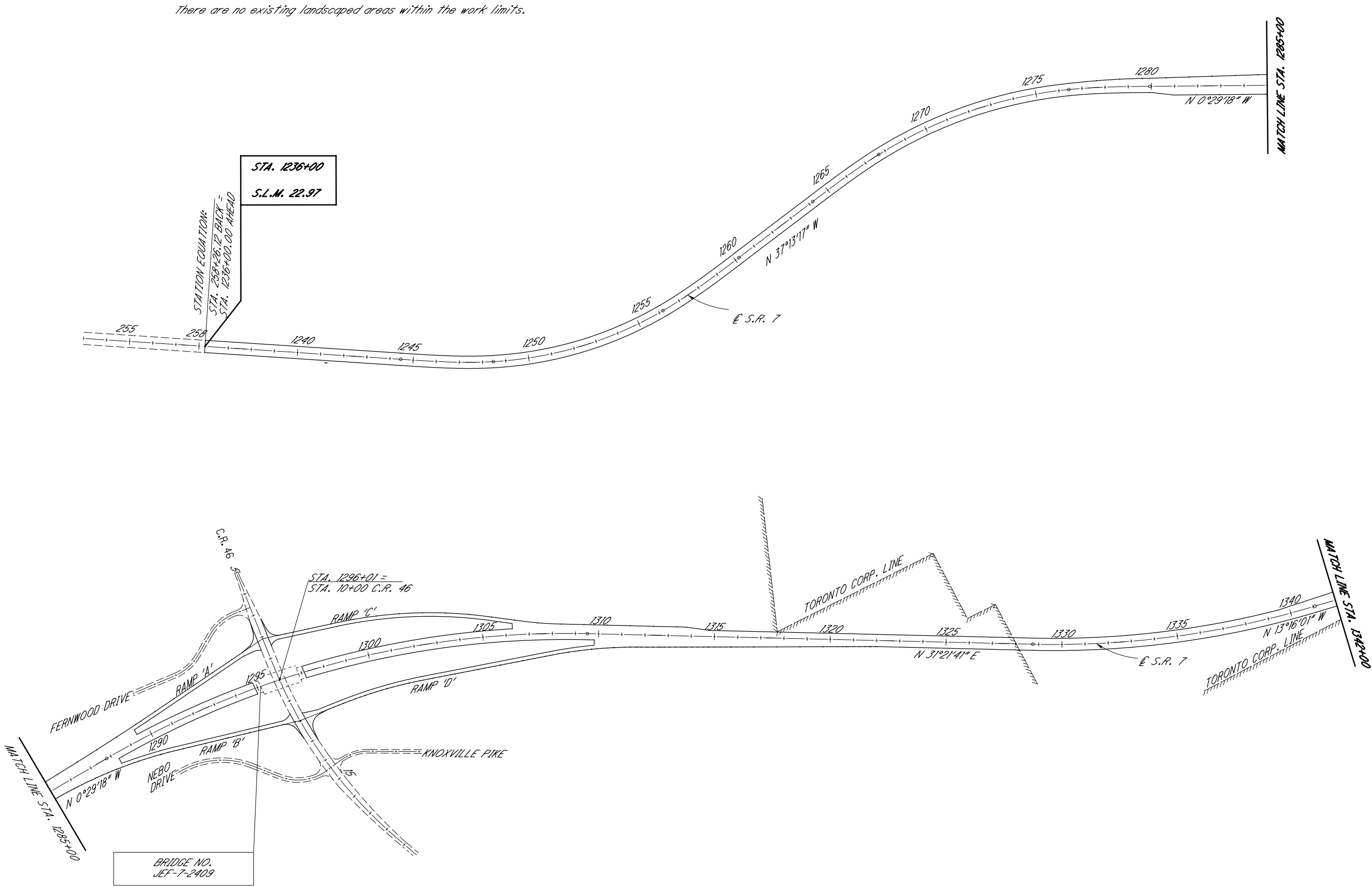
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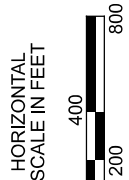
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TOTAL

39



SCHEMATIC PLAN



DESIGN AGENCY



DESIGNER

ADK

REVIEWER

JPB 06-10-22

PROJECT ID

102458

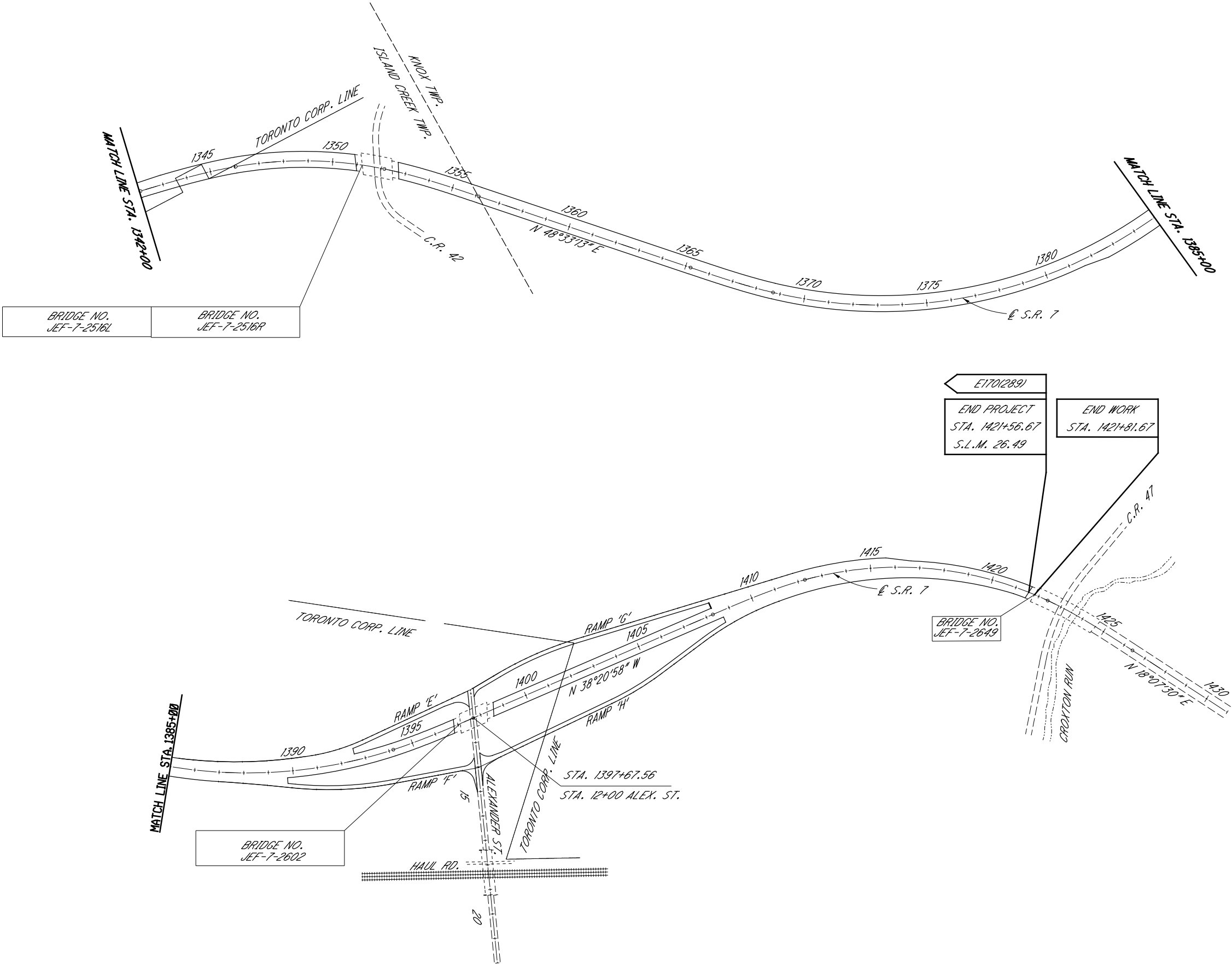
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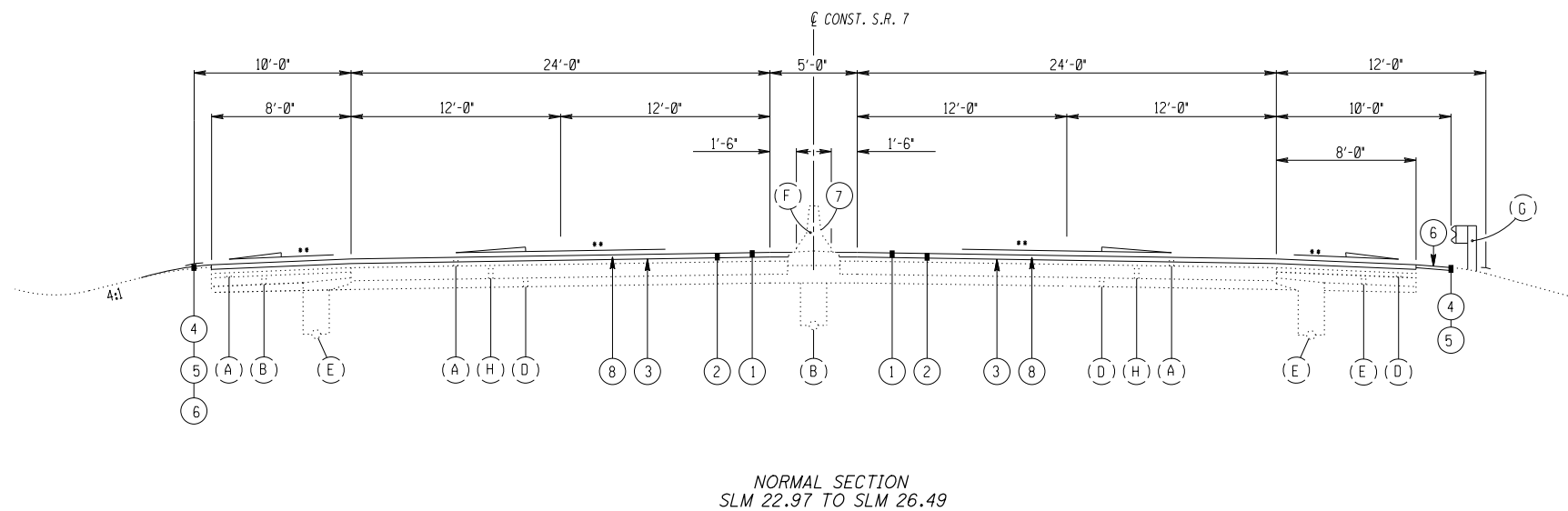
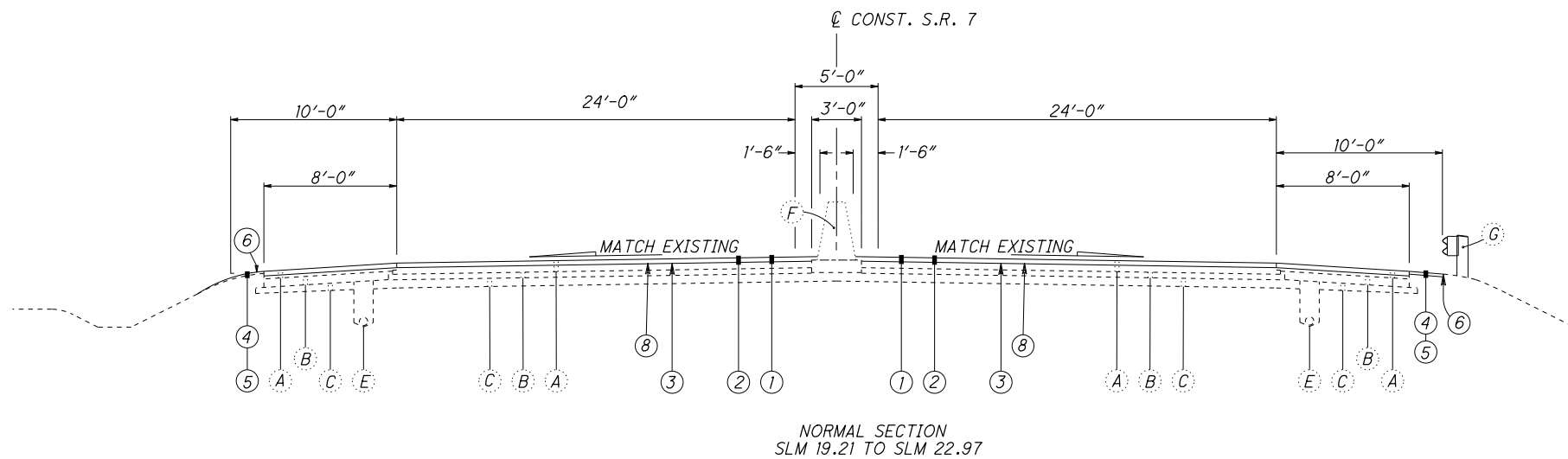
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TOTAL

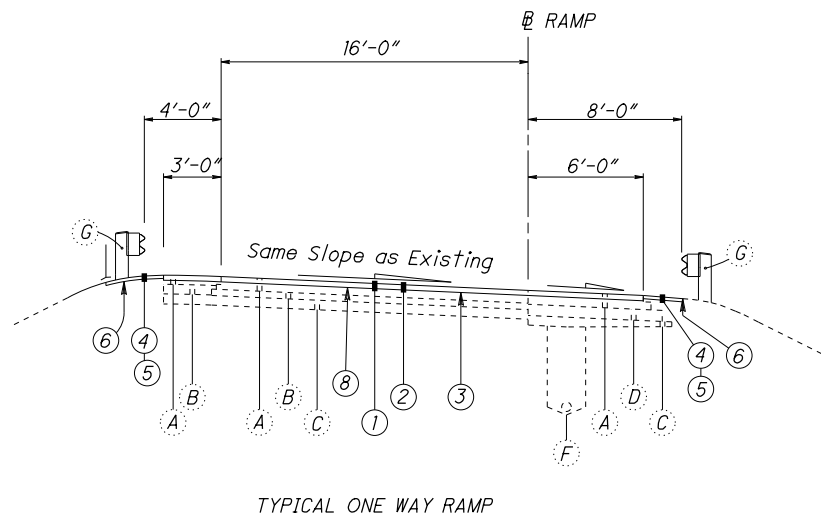
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There are no existing landscaped areas within the work limits.



**EXISTING LEGEND**

- (A) — EXISTING ASPHALT CONCRETE
- (B) — EXISTING BITUMINOUS AGGREGATE BASE
- (C) — EXISTING 6" SUBBASE
- (D) — EXISTING AGGREGATE BASE
- (E) — EXISTING 6" UNDERDRAIN
- (F) — EXISTING CONCRETE BARRIER
- (G) — EXISTING GUARDRAIL
- (H) — EXISTING 9" REINFORCED CONCRETE PAVEMENT

**PROPOSED LEGEND**

- ① ITEM 442 - 1 1/2" ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE B, (447), AS PER PLAN
- ② ITEM 254 - 1 1/2" PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN
- ③ ITEM 407 - NON-TRACKING TACK COAT
- ④ ITEM 617 - SHOULDER PREPARATION
- ⑤ ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN
- ⑥ ITEM 408 - PRIME COAT, AS PER PLAN
- ⑦ ITEM 519 - PATCHING CONCRETE STRUCTURE
- ⑧ ITEM 254 - PATCHING PLANED SURFACE

SUPERELEVATED SECTIONS NOT SHOWN FOR SIMPLICITY



ITEM 442 - ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE B, (447), AS PER PLAN

THE CONTRACTOR SHALL FOLLOW THE SPECIFICATIONS OF C&MS 703.05, EXCEPT DO NOT USE COARSE AGGREGATE FROM A SOURCE DESIGNATED "SR" OR "SRH" AS DEFINED BY THE OFFICE OF MATERIALS MANAGEMENT (OMM) IN ANY JOB MIX FORMULA (JMF) FOR THIS ITEM OF WORK.

ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN

GRADED SHOULDERS SHALL BE RESHAPED AS PER THE REQUIREMENTS OF ITEM 617, COMPACTED AGGREGATE. GRINDINGS SHALL BE USED IN LIEU OF ITEM 617, COMPACTED AGGREGATE. IF THE AMOUNT OF GRINDINGS ARE NOT SUFFICIENT TO COVER THE COMPACTED AGGREGATE QUANTITY REQUIRED FOR THIS PLAN, THEN ADDITIONAL MATERIAL MEETING SPECIFICATION 617 SHALL BE PROCURED AND USED BY THE CONTRACTOR. ALL GRINDINGS SHALL ADHERE TO THE SPECIFICATIONS STATED IN C&MS 617 AND MEET GRADATIONS CONFORMING TO C&MS 703.18. THE COST FOR STORING THE GRINDINGS ON THE PROJECT AND PLACING THE GRINDINGS SHALL ALSO BE INCLUDED IN THIS ITEM.

COORDINATION OF RESURFACING AND PLANING OPERATIONS

ONCE THE PAVEMENT PLANING OPERATIONS HAVE COMMENCED, THE CONTRACTOR SHALL PLANE CONTINUOUSLY UNTIL ALL ELEMENTS OF WORK ASSOCIATED WITH THE PAVEMENT PLANING OPERATIONS ARE CONCLUDED. THE PAVEMENT PLANING OPERATIONS SHALL BE COMPLETED IN A TIMELY MANNER, OR AS DIRECTED BY THE ENGINEER.

THE CONTRACTOR MUST BEGIN PAVING OPERATIONS NO LATER THAN 4 DAYS AFTER THE START OF THE PAVEMENT PLANING.

IF ASPHALT CONCRETE IS TO BE APPLIED DIRECTLY ONTO PORTLAND CEMENT, CONCRETE, OR BRICK PAVEMENT, THE CONTRACTOR SHALL TACK THE EXISTING PAVEMENT WITH RUBBERIZED ASPHALT EMULSION CONFORMING TO C&MS 702.13.

ALL GRINDINGS SHALL BECOME THE PROPERTY OF THE CONTRACTOR EXCEPT FOR WHAT IS REQUIRED TO BE USED FOR SHOULDER MATERIAL INDICATED IN ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN AND THE 300 TONS DELIVERED TO THE. TORONTO OUTPOST.

ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, (1½"), AS PER PLAN

THIS ITEM SHALL INCLUDE HAULING THE GRINDINGS FROM THE PAVEMENT PLANING OPERATION TO THE ADDRESS LISTED BELOW AND STOCKPILING THE MATERIAL IN A MANNER ACCEPTABLE TO THE ENGINEER. CONTINUOUS END DUMPING WILL NOT BE PERMITTED.

ALL PARTS: 300 TOTAL TONS DELIVERED TO:

ODOT TORONTO OUTPOST
940 KINGSDALE RD., STEUBENVILLE, OH 43952
CONTACT: TY JUSTICE 330-308-6580

ALL LABOR, EQUIPMENT, AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK SHALL BE INCLUDED IN THE SQUARE YARD BID PRICE FOR ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN.

ITEM 408 - PRIME COAT, AS PER PLAN

THIS ITEM OF WORK SHALL BE PERFORMED IN ACCORDANCE WITH C&MS "ITEM 408 - PRIME COAT", EXCEPT THE CONTRACTOR SHALL APPLY "MC-70" AT A RATE OF 0.4 GALLONS PER SQUARE YARD, OR AS DETERMINED BY THE ENGINEER, TO THE COMPLETED COMPACTED AGGREGATE SHOULDER, AS PER PLAN.

ITEM 646 - EPOXY PAVEMENT MARKINGS
ITEM 807 - WET REFLECTIVE EPOXY PAVEMENT MARKINGS

THE CONTRACTOR SHALL REPLACE THE EXISTING PAVEMENT MARKINGS WITHIN THE PROJECT LIMITS WITH NEW PAVEMENT MARKINGS AT THE SAME LOCATIONS AS PER C&MS 641.06. SEE STANDARD DRAWING TC-71.10 AND TC-72.20 FOR PAVEMENT MARKING DETAILS.

PROFILE AND ALIGNMENT

PLACE THE PROPOSED PAVEMENT TO FOLLOW THE PROFILE AND ALIGNMENT OF THE EXISTING PAVEMENT. PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY AS SHOWN ON THE TYPICAL SECTIONS.

SHIELD

THE CONTRACTOR SHALL PROVIDE A SHIELD TO PREVENT THE SPRAYING OR DRIFTING OF LIQUID BITUMINOUS MATERIAL ONTO THE EDGE OF THE PAVEMENT OR EDGELINE. THE CONTRACTOR SHALL ADHERE TO THE PROVISIONS SET FORTH IN C&MS 107.10 CONCERNING THE PROTECTION AND RESTORATION OF ALL PUBLIC AND PRIVATE PROPERTY IMPACTED BY CONSTRUCTION OPERATIONS.

UTILITIES

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER, OR ADJACENT TO, THE WORK AREA.

CLEARING AND GRUBBING

ALTHOUGH THERE ARE NO TREES OR STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE LIMITS OF THE PROJECT, A LUMP SUM QUANTITY IS INCLUDED IN THE GENERAL SUMMARY FOR ITEM 201, CLEARING AND GRUBBING. ALL PROVISIONS AS SET FORTH IN THE SPECIFICATIONS UNDER THIS ITEM ARE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 201, CLEARING AND GRUBBING.

EXISTING PLANS

THE FOLLOWING EXISTING PLANS ARE AVAILABLE FOR REFERENCE AT THE DISTRICT 11 OFFICE OF THE OHIO DEPARTMENT OF TRANSPORTATION, 2201 REISER AVE. S.E., NEW PHILADELPHIA, OHIO, 44663:

PID 23736 (2012)
PID 87025 (2011)

IN ADDITION, THE EXISTING PLANS CAN BE FOUND ON THE DEPARTMENT'S WEBSITE AT THE FOLLOWING ADDRESS:

<http://www.dot.state.oh.us/pub/contracts/attach>

SURFACE COURSE COMPLETION REQUIREMENTS

ANY GIVEN LENGTH OF WORK ON WHICH RESURFACING OPERATIONS HAVE BEEN STARTED IN A CONSTRUCTION SEASON SHALL HAVE THE SURFACE COURSE PLACED THAT SAME SEASON.

ITEM 442 - ANTI-SEGREGATION EQUIPMENT

PROVIDE A MATERIAL TRANSFER VEHICLE IN ACCORDANCE WITH CMS 401.12 WHEN PLACING ASPHALT SURFACE AND INTERMEDIATE COURSES ON MAINLINE LANES AND ACCELERATION AND DECELERATION LANES.

ITEM 519 - PATCHING CONCRETE STRUCTURE

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR THE PATCHING OF CONCRETE BARRIER AT LOCATIONS AS DIRECTED BY THE ENGINEER. APPLY A WHITE CURING COMPOUND TO THE FINISHED PATCHES. THE PATCHING SHALL BE COMPLETED BEFORE THE ASPHALT SURFACE COURSE IS PLACED.

PAYMENT FOR THE ABOVE WORK SHALL BE AT THE UNIT PRICE BID FOR ITEM 519, PATCHING CONCRETE STRUCTURE AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT, AND MATERIALS.

TOTAL CARRIED TO THE GENERAL SUMMARY:

ITEM 519 - PATCHING CONCRETE STRUCTURE 50 SQ FT

ITEM 621 - RAISED PAVEMENT MARKER REMOVED

EXISTING RAISED PAVEMENT MARKERS SHALL BECOME THE PROPERTY OF THE CONTRACTOR FOR DISPOSAL OFF THE PROJECT. IN AREAS OF PAVEMENT PLANING ONLY, THE REQUIREMENT TO FILL THE DEPRESSIONS SHALL BE WAIVED.

AIRWAY/HIGHWAY CLEARANCE FOR AIRPORTS AND HELIPORTS

THIS PROJECT HAS BEEN IDENTIFIED AS BEING WITHIN THE INFLUENCE AREA OF A PUBLIC USE AIRPORT OR HELIPORT. NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT AT MAXIMUM OPERATING HEIGHT SHALL EXCEED A HEIGHT OF 404 FT. IF ANY TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT WILL EXCEED THIS HEIGHT, FURTHER COORDINATION WITH THE FEDERAL AVIATION ADMINISTRATION (FAA), AND ODOT OFFICE OF AVIATION, WILL BE NECESSARY PRIOR TO ERECTING SUCH TEMPORARY STRUCTURES OR OPERATING SUCH EQUIPMENT ON THE PROJECT. THE CONTRACTOR WILL BE REQUIRED TO SUBMIT FORM 7460-1 TO THE FAA. NOTIFY THE ODOT OFFICE OF AVIATION WHEN SUBMITTING FAA FORM 7460-1.

NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT SHALL EXCEED THE PERMISSIBLE HEIGHT, UNTIL A COPY OF THE FAA APPROVAL AND THE ODOT OFFICE OF AVIATION PERMIT HAS BEEN FURNISHED TO THE PROJECT ENGINEER.

FEDERAL AVIATION ADMINISTRATION
SOUTHWEST REGIONAL OFFICE
OBSTRUCTION EVALUATION GROUP
10101 HILLWOOD PARKWAY
FORT WORTH, TX 76177
FAX: (817) 222-5920
HTTP://CEAAA.FAA.GOV

OHIO DEPARTMENT OF TRANSPORTATION
OFFICE OF AVIATION
2829 WEST DUBLIN-GRANVILLE ROAD
COLUMBUS, OHIO 43235
OHIO.AIRPORT.PROTECTION@DOT.OHIO.GOV

SPEED MEASUREMENT MARKINGS

THE CONTRACTOR SHALL CONTACT THE DISTRICT 11 SURVEY OPERATIONS MANAGER AT 330-308-7866 WHEN THE PAVING AND LINEAR GRADING IS COMPLETE FOR THE RESTORATION OF THE SPEED MEASUREMENT MARKINGS FROM SLM 21.50 TO SLM 22.50. THE SPEED MEASUREMENT MARKINGS WILL BE APPLIED BY DISTRICT PERSONNEL. THE CONTRACTOR SHALL COORDINATE WITH THE DISTRICT 11 SURVEY OPERATIONS MANAGER TO ENSURE THAT THE SPEED MEASUREMENT MARKINGS CAN BE APPLIED WHILE THE CONTRACTOR'S WORK ZONE MAINTENANCE OF TRAFFIC IS STILL IN EFFECT.

DESIGN AGENCY



DESIGNER

ADK

REVIEWER

JPB 06-10-22

PROJECT ID

102458

SHEET

P.7

TOTAL

39

ITEM 614 - MAINTAINING TRAFFIC, AS PER PLAN

THE CONTRACTOR SHALL MAINTAIN TRAFFIC AT ALL TIMES IN ACCORDANCE WITH THE REQUIREMENTS OF CMS ITEM 614, THESE MAINTENANCE OF TRAFFIC NOTES AND DETAILS, THE STANDARD CONSTRUCTION DRAWINGS, AND THE TRAFFIC CONTROL DETAILS DESCRIBED IN THESE PLANS.

THE MINIMUM LANE WIDTH FOR TRAFFIC CONTROL SHALL BE 11 FEET AT ALL TIMES. IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO ORGANIZE HIS WORK IN SUCH A MANNER TO PROVIDE THE MOST SAFETY WITH THE LEAST INCONVENIENCE TO THE TRAVELING PUBLIC.

THE CONTRACTOR IS RESPONSIBLE FOR DESIGNING THE MAINTENANCE OF TRAFFIC SCHEME. THE CONTRACTOR SHALL SUBMIT, IN WRITING, THIS MAINTENANCE OF TRAFFIC SCHEME AND A SCHEDULE OF OPERATIONS TO THE ENGINEER AND RECEIVE APPROVAL BEFORE WORK IS STARTED ON THE PROJECT.

ANY OPEN PAVEMENT TRENCH OR DROPOFF SHALL BE ADEQUATELY MAINTAINED AND PROTECTED. THE PROTECTION USED SHALL MEET THE REQUIREMENTS OF STD DWG MT-101.90.

UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE PERMITTED TO HAVE WORK ZONES WHICH ALTERNATELY CLOSE BOTH THE PASSING AND TRAVEL LANE UNLESS THE DISTANCE BETWEEN THE LANE RESTRICTIONS EXCEEDS 2 MILES.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR SMOOTH AND ORDERLY FLOW OF TRAFFIC THROUGH THE PROJECT AREA 24 HOURS PER DAY FOR THE DURATION OF THE PROJECT. THIS CONSISTS OF NOTIFYING THE OHIO STATE PATROL AFTER ENCOUNTERING ANY ACCIDENTS OR DISABLED VEHICLES OR OBJECTS HINDERING THE FLOW OF TRAFFIC.

THE CONTRACTOR SHALL DESIGNATE TO THE ENGINEER A PERSON RESPONSIBLE FOR MAINTENANCE OF TRAFFIC CONTROL DURING NON-WORK HOURS WHO SHALL BE AVAILABLE WITHIN (30) MINUTES AFTER NOTIFICATION.

PAYMENT FOR PROVIDING WATCHMEN, FURNISHING, ERECTING, MAINTAINING AND REMOVING SIGNS, CONES, MARKERS, SPECIAL LIGHTING, FLOODLIGHTING, WORK ZONE PAVEMENT MARKINGS, WORK ZONE RAISED PAVEMENT MARKERS, ETC., SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614 MAINTAINING TRAFFIC, AS PER PLAN.

THE CONTRACTOR SHALL FURNISH, INSTALL AND MAINTAIN ALL ADDITIONAL SIGNS OR OTHER TRAFFIC CONTROL DEVICES AS REQUIRED ABOVE. ALL COSTS INVOLVED IN FURNISHING, INSTALLING AND MAINTAINING THESE DEVICES SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614, MAINTAINING TRAFFIC, AS PER PLAN.

UNLESS THE ENGINEER DEEMS IT PHYSICALLY IMPOSSIBLE, ALL CONSTRUCTION EQUIPMENT SHALL EXIT ALL WORK ZONES FROM THE DOWNSTREAM END OF THE WORK ZONE OR BY INTERCHANGE RAMPS. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE PERMITTED TO DIRECTLY TRANSPORT OR OPERATE ANY EQUIPMENT ACROSS THE OPEN LANES OF THE ROADWAY.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS. THE RESTRICTIONS SHALL ALSO APPLY TO WORK ON THE RAMPS.

ITEM 614 - MAINTAINING TRAFFIC, AS PER PLAN (CONTINUED)

THE PLANING AND RESURFACING WILL PROCEED CONTINUOUSLY A MINIMUM OF FIVE (5) DAYS PER WEEK, WEATHER PERMITTING, EXCEPTING HOLIDAYS AND EVENTS LISTED BELOW:

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

NEW YEARS	LABOR DAY
MEMORIAL DAY	THANKSGIVING
FOURTH OF JULY	CHRISTMAS

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPENED DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF THE WEEK	TIME ALL LANES MUST BE OPENED TO TRAFFIC
SUNDAY	12:00N FRI THRU 6:00 AM MON
MONDAY	12:00N FRI THRU 6:00 AM TUE
TUESDAY	12:00N MON THRU 6:00 AM WED
WEDNESDAY	12:00N TUE THRU 6:00 AM THU
THURSDAY	12:00N WED THRU 6:00 AM FRI
(THANKSGIVING ONLY)	6:00 AM WED THRU 6:00 AM MON
FRIDAY	12:00N THU THRU 6:00 AM MON
SATURDAY	12:00N FRI THRU 6:00 AM MON

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF \$15,220 FOR EACH HOUR THE ABOVE DESCRIBED LANE CLOSURE RESTRICTIONS ARE VIOLATED.

NO EXTENSIONS OF TIME SHALL BE GRANTED FOR DELAYS IN MATERIAL DELIVERIES, UNLESS SUCH DELAYS ARE INDUSTRY-WIDE, OR FOR LABOR STRIKES, UNLESS SUCH STRIKES ARE AREA-WIDE.

WHEN RAISED PAVEMENT MARKERS ARE TO BE INSTALLED, THE REQUIRED LANE CLOSURE SHALL REMAIN IN EFFECT UNTIL THE EPOXY IS DRY AND ALL FOREIGN MATTER OR DEBRIS CREATED BY THE INSTALLATION OF THE RPM CASTING IS REMOVED FROM THE ROADWAY.

PEDESTRIAN WALKWAYS CONSTRUCTED BY THE CONTRACTOR SHALL BE KEPT FREE OF ANY OBSTRUCTIONS OR HAZARDS INCLUDING HOLES, DEBRIS AND MUD. OTHER WALKWAYS DAMAGED OR DIRTIED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED OR CLEANED. THE CONTRACTOR MUST TAKE PRECAUTIONS TO PROTECT PEDESTRIANS FROM EXPOSURE TO HAZARDS RESULTING FROM THE CONSTRUCTION OPERATION BY INSTALLING CONSTRUCTION FENCE AND SIGNING.

TEMPORARY ORANGE PLASTIC CONSTRUCTION FENCE HAS BEEN PROVIDED HEREIN AS A TRAFFIC CONTROL DEVICE TO DIVERT AND GUIDE PEDESTRIANS WHOSE PATH WOULD OTHERWISE ENTER THE WORK AREA. THE TEMPORARY ORANGE PLASTIC CONSTRUCTION FENCE SHALL BE PLACED AROUND THE SIDEWALK WORK AREAS. SIDEWALK CLOSED SIGN (R9-9 (30 X 18)) MOUNTED ON A TYPE 3 BARRICADE WITH TYPE A FLASHING LIGHTS SHALL BE PLACED OUTSIDE THE FENCE ON EACH SIDEWALK APPROACH AS SHOWN ON SCD MT-101.60.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, AS PER PLAN, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ITEM 607 - FENCE, MISC.: TEMPORARY ORANGE PLASTIC CONSTRUCTION FENCE

TEMPORARY ORANGE PLASTIC/NYLON CONSTRUCTION FENCE SHALL BE PLACED AROUND NEW CURB RAMPS AND WALK UNTIL ADEQUATELY CURED, OR AS DIRECTED BY THE ENGINEER TO PROTECT PEDESTRIAN TRAFFIC FROM CONSTRUCTION OPERATIONS. THE FENCING MATERIAL SHALL BE SECURELY FASTENED TO EITHER WOOD, OR METAL POSTS WITH A MAXIMUM SPACING NOT TO EXCEED 6 FEET. THE FENCING MATERIAL SHALL HAVE A NOMINAL HEIGHT OF 42 INCHES, AND THE TOP EDGE OF THE FENCING SHALL NOT BE PERMITTED TO SAG BELOW 30 INCHES. THE CONTRACTOR SHALL ENSURE THE FENCE IS IN GOOD CONDITION, PROPERLY PLACED, AND MAINTAINED AT ALL TIMES.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 607 - FENCE MISC.: TEMPORARY ORANGE PLASTIC CONSTRUCTION FENCE	121 FT.
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WORK ZONE MARKINGS AND SIGNS

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS IDENTIFIED BY THE ENGINEER FOR WORK ZONE PAVEMENT MARKINGS AND SIGNS PER THE REQUIREMENTS OF C&MS 614.04 AND 614.11.

ITEM 614 - WORK ZONE PAVEMENT MARKINGS, CLASS I, 807 PAINT
ITEM 614 - WORK ZONE PAVEMENT MARKINGS, CLASS III, 642 PAINT
ITEM 614 - WORK ZONE MARKING SIGNS

THE CONTRACTOR SHALL BE REQUIRED TO INSTALL WORK ZONE MARKINGS AND SIGNS AT LOCATIONS IDENTIFIED BY THE ENGINEER PER THE REQUIREMENTS OF CMS 614.04 AND 614.11.

WORK ZONE PAVEMENT MARKINGS FOR MILLED SURFACES SHALL BE CLASS III, 642 PAINT. WORK ZONE PAVEMENT MARKINGS FOR NEWLY PAVED SURFACES SHALL BE CLASS I, 807 PAINT.

PRIOR TO PLACEMENT OF ANY WORK ZONE PAVEMENT MARKINGS, THE CONTRACTOR SHALL COMPLETELY OBLITERATE, AS PER 614.10, ALL EXISTING PAVEMENT MARKINGS THAT WOULD CREATE CONFUSION OR CONFLICT WITH THE WORK ZONE PAVEMENT MARKINGS.

WORK ZONE RAISED PAVEMENT MARKERS CANNOT BE USED TO SIMULATE (REPLACE) ANY TYPE OF WORK ZONE PAVEMENT MARKING.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR WORK ZONE SIGNS:

ITEM 614 - WORK ZONE MARKING SIGN	9 EACH
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THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

WORK ZONE PAVEMENT MARKINGS TABLE									
PART	LOCATION	614							FUNDING
		WORK ZONE EDGE LINE, CLASS I, 6", 807 PAINT	WORK ZONE LANE LINE, CLASS I, 6", 807 PAINT	WORK ZONE CHANNELIZING LINE, CLASS I, 12", 807 PAINT	WORK ZONE STOP LINE, CLASS III, 642 PAINT	WORK ZONE EDGE LINE, CLASS III, 6", 642 PAINT	WORK ZONE LANE LINE, CLASS III, 6", 642 PAINT	WORK ZONE CHANNELIZING LINE, CLASS III, 12", 642 PAINT	
		MILE	MILE	FT.	FT.	MILE	MILE	FT.	
1	S.R. 7	34.14	15.40	6,686	293	34.14	15.40	6,686	
SUB-TOTALS		34.14	15.40	6,686	293	34.14	15.40	6,686	01/NHS/PV
TOTALS (CARRIED TO THE GENERAL SUMMARY)		34.14	15.40	6,686	293	34.14	15.40	6,686	

ITEM 614, REPLACEMENT SIGN

FLATSHEET SIGNS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT SIGNS SHALL BE NEW. OTHER MATERIALS MAY BE IN USED, BUT GOOD, CONDITION SUBJECT TO APPROVAL BY THE ENGINEER.

PAYMENT FOR THE NEW SIGNS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT SIGN, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF DAMAGED SIGNS, HARDWARE AND SUPPORTS, AND PROVIDING THE NECESSARY REPLACEMENT HARDWARE, SUPPORTS, ETC.

AN ESTIMATED QUANTITY OF 5 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

ITEM 614, REPLACEMENT DRUM

DRUMS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT DRUMS SHALL BE NEW.

PAYMENT FOR THE NEW DRUMS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT DRUM, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGED DRUM, AND PROVIDING AND MAINTAINING THE REPLACEMENT DRUM IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL DRUM.

AN ESTIMATED QUANTITY OF 10 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

MOVEMENT OF DRUMS

THE ROW OF DRUMS ALONG A CLOSED LANE SHALL BE MOVED OUT OF THE OPEN LANE ONTO THE NEW PAVEMENT AS SOON AS PAVING OPERATIONS PERMIT.

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION OF TRAFFIC RESTRICTIONS TIME TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS & PIO
RAMP & ROAD CLOSURES	>= 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	<= 12 HOURS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS (CONTINUED)

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

FOR OPERATIONS WITHOUT POSITIVE PROTECTION OCCURRING WITHIN 10 FEET OF AN OPEN TRAVELED LANE THAT MEET ALL OF THE FOLLOWING CRITERIA:

- ON A MULTI-LANE DIVIDED INTERSTATE, OTHER FREEWAY OR EXPRESSWAY; AND
- AN AUTHORIZED SPEED LIMIT OF 45 MPH OR GREATER THAT IS IN EFFECT AT THE TIME OF THE OPERATION; AND,
- AADT OF 50,000 (OR AADT OF 30,000 WITH 25% OR HIGHER PERCENT TRUCKS)

"WITHOUT POSITIVE PROTECTION" MEANS USE OF DRUMS, CONES, SHADOW VEHICLE, ETC, WITHOUT PROTECTION FROM PORTABLE BARRIER OR OTHER RIGID BARRIER ALONG THE WORK AREA. THIS PHRASE DOES NOT APPLY TO CASES WHERE POSITIVE PROTECTION IS REQUIRED. MOBILE OPERATIONS ARE REGARDED AS "WITHOUT POSITIVE PROTECTION". FOR WORK ZONES USING A COMBINATION OF BARRIER AND TEMPORARY TRAFFIC CONTROL DEVICES (CONES, DRUMS, ETC), THE DESIGNATION SHALL BE BASED UPON THE TYPE OF DEVICES USED IN THE AREA THAT WORKERS ARE LOCATED.

IF MULTIPLE ACTIVE LOCALIZED QUALIFYING WORK AREAS OCCUR WITHOUT POSITIVE PROTECTION, PER MAINLINE TRAFFIC DIRECTION, PROVIDE A UNIFORMED LEO AND OFFICIAL PATROL CAR IN ADVANCE OF:

- THE FIRST ACTIVE WORK AREA THAT DRIVERS WILL ENCOUNTER; OR
- THE ACTIVE WORK AREA Laterally CLOSEST TO THE OPEN TRAVELED LANE; OR
- OTHER LOCATION AS APPROVED BY THE ENGINEER.

THE UNIFORMED LEO AND OFFICIAL PATROL CAR MAY RELOCATE AMONG THE LISTED LOCATIONS AS APPROPRIATE AS THE OPERATIONS PROCEED IN THE LOCALIZED QUALIFYING WORK AREAS.

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION (OR AT THE POINT OF ROAD CLOSURE), AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS (CONTINUED)

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE THAT SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 250 HOURS (01/NHS/PV)

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF A LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

WORK ZONE SPEED ZONES (WZSZS)

THE FOLLOWING WORK ZONE SPEED ZONE (WZSZ) SPEED LIMIT REVISION(S) HAVE BEEN APPROVED FOR USE ON THIS PROJECT WHEN WORK ZONE CONDITIONS AND FACTORS ARE MET AS DESCRIBED BELOW:

WZSZ REVISION NUMBER	CTY-RTE-SECT	DIRECTION
WZ-60670	JEF-7-18.71 TO JEF-7-27.00	NB & SB

POTENTIAL WZSZ LOCATIONS SHALL HAVE AN ORIGINAL (PRE-CONSTRUCTION) POSTED SPEED LIMIT OF 55 MPH OR GREATER, A QUALIFYING WORK ZONE CONDITION OF AT LEAST 0.5 MILE IN LENGTH, AN EXPECTED WORK DURATION OF AT LEAST THREE HOURS, AND A WORK ZONE CONDITION IN PLACE THAT REDUCES THE EXISTING FUNCTIONALITY OF THE TRAVEL LANES OR SHOULDERS (I.E., LANE CLOSURE, LANE SHIFT, CROSSOVER, CONTRAFLOW AND/OR SHOULDER CLOSURE). THE LENGTH OF THE WORK ZONE CONDITION IS MEASURED FROM THE BEGINNING OF THE TAPER FOR THE SUBJECT WORK ZONE CONDITION IMPACTING THE TRAVEL LANES AND/OR SHOULDER TO THE END OF THE DOWNSTREAM TAPER, WHERE DRIVERS ARE RETURNED TO TYPICAL ALIGNMENT. AN EXPECTED WORK DURATION OF AT LEAST THREE HOURS IS REQUIRED TO BALANCE THE ADDITIONAL EXPOSURE CREATED BY INSTALLING AND REMOVING WZSZ SIGNING WITH THE TIME NEEDED TO COMPLETE THE WORK.

IF THE WORK ZONE MEETS THESE MINIMUM CRITERIA, IT SHALL BE ANALYZED FURTHER USING TABLE 1 BELOW TO DETERMINE IF AND WHEN IT QUALIFIES FOR A SPEED LIMIT REDUCTION. DEPENDING ON THE ORIGINAL POSTED SPEED LIMIT, THE TYPE OF TEMPORARY TRAFFIC CONTROL USED, AND WHETHER OR NOT WORKERS ARE PRESENT, A WARRANTED WZSZ WILL VARY IN THE APPROVED SPEED LIMIT TO BE POSTED OVER TIME.

WORK ZONE SPEED ZONES (WZSZS) (CONTINUED)

C&MS ITEM 614, PARAGRAPH 614.02(B), INDICATES THAT TWO DIRECTIONS OF A DIVIDED HIGHWAY ARE CONSIDERED SEPARATE HIGHWAY SECTIONS. THEREFORE, IF THE WORK ON A MULTI-LANE DIVIDED HIGHWAY IS LIMITED TO ONLY ONE DIRECTION, A SPEED LIMIT REDUCTION IN THE DIRECTION OF THE WORK DOES NOT AUTOMATICALLY CONSTITUTE A SPEED LIMIT REDUCTION IN THE OPPOSITE DIRECTION. EACH DIRECTION SHALL BE ANALYZED INDEPENDENTLY FROM EACH OTHER.

ALL WZSZS FLUCTUATE BETWEEN TWO APPROVED REDUCED SPEED LIMITS OR BETWEEN AN APPROVED REDUCED SPEED LIMIT AND THE ORIGINAL POSTED SPEED LIMIT. ONLY ONE OF TWO SIGNING STRATEGIES SHALL BE USED TO IMPLEMENT A WZSZ.

[WZSZS USING DSL SIGN ASSEMBLIES SHALL BE IN ACCORDANCE WITH THIS NOTE, APPROVED LIST, SUPPLEMENTAL SPECIFICATIONS (SS) 808 AND 908, AND TRAFFIC SCD MT-104.10.]

[WZSZS USING TEMPORARY FLATSHEET SPEED LIMIT SIGNS SHALL BE IN ACCORDANCE WITH THIS NOTE AND SCD MT-104.10. ADDITIONALLY PAYMENT MAY BE REMOVED, OR A DISINCENTIVE APPLIED, FOR WZSZS USING TEMPORARY FLATSHEET SPEED LIMIT SIGNS THE SAME AS DESCRIBED IN THE MOST RECENT PUBLICATION OF SS 808 IN REGARDS TO WZSZS USING DSL SIGN ASSEMBLIES (SEE SS 808.06 PARAGRAPHS 4 THROUGH 7, INCLUDING TABLE 1).]

ONLY ONE WARRANTED SPEED LIMIT APPLIES AT ANY ONE TIME; SPEED LIMIT REDUCTIONS ARE NOT CUMULATIVE. WZSZS SHALL NOT BE USED FOR MOVING/MOBILE ACTIVITIES, AS DEFINED IN OMUTCD PART 6.

WHEN LOOKING UP THE WARRANTED WORK ZONE SPEED LIMITS, ALWAYS USE THE ORIGINAL, PRECONSTRUCTION, POSTED SPEED LIMIT. DO NOT USE A PRIOR OR CURRENT WORK ZONE SPEED LIMIT AS A LOOK UP VALUE IN THE TABLE. POSITIVE PROTECTION IS GENERALLY REGARDED AS PORTABLE BARRIER OR OTHER RIGID BARRIER IN USE ALONG THE WORK AREA WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WITHOUT POSITIVE PROTECTION IS GENERALLY REGARDED AS USING DRUMS, CONES, SHADOW VEHICLE, ETC., ALONG THE WORK AREA WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WORKERS ARE CONSIDERED AS BEING PRESENT WHEN ON-SITE, WORKING WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WHEN THE WORK ZONE CONDITION REDUCING THE EXISTING FUNCTIONALITY OF THE TRAVEL LANES OR SHOULDERS IS REMOVED, THE SPEED LIMIT DISPLAYED SHALL RETURN TO THE ORIGINAL POSTED SPEED LIMIT.

TABLE 1: WARRANTED WORK ZONE SPEED LIMITS (MPH) FOR WORK ZONES IN HIGH-SPEED (55 MPH OR GREATER) MULTI-LANE HIGHWAYS				
ORIGINAL POSTED SPEED LIMIT	WITH POSITIVE PROTECTION		WITHOUT POSITIVE PROTECTION	
	WORKERS PRESENT	WORKERS NOT PRESENT	WORKERS PRESENT	WORKERS NOT PRESENT
70	60	65	55	65
65	55	60	50	60
60	55	60	50	60
55	50	55	45	55

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 808, DIGITAL SPEED LIMIT (DSL) SIGN ASSEMBLY 66 SIGN MNTH

[ASSUMING 22 DSL SIGN ASSEMBLIES FOR 3 MONTHS]

DESIGN AGENCY



DESIGNER	ADK
REVIEWER	JPB 06-10-22
PROJECT ID	102458
SHEET	P.9
TOTAL	39

WORK ZONE INCREASED PENALTIES SIGN (R11-H5A)

R11-H5A-48 SIGNS SHALL BE FURNISHED, ERECTED, AND MAINTAINED IN GOOD CONDITION AND/OR REPLACED AS NECESSARY AND SUBSEQUENTLY REMOVED BY THE CONTRACTOR. SIGNS SHALL BE MOUNTED AT THE APPROPRIATE OFFSETS AND ELEVATIONS AS PRESCRIBED BY THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. THEY SHALL BE MAINTAINED ON SUPPORTS MEETING CURRENT SAFETY CRITERIA.

THE SIGNS MAY BE ERECTED OR UNCOVERED NO MORE THAN FOUR HOURS BEFORE THE ACTUAL START OF WORK. THE SIGNS SHALL BE REMOVED OR COVERED NO LATER THAN FOUR HOURS FOLLOWING RESTORATION OF ALL LANES TO TRAFFIC WITH NO RESTRICTIONS, OR SOONER AS DIRECTED BY THE ENGINEER. TEMPORARY SIGN COVERING AND UNCOVERING DUE TO TEMPORARY LANE RESTORATIONS SHALL BE GUIDED BY THE FOUR-HOUR LIMITATIONS STATED ABOVE. SUCH LANE RESTORATIONS SHOULD BE EXPECTED TO REMAIN IN EFFECT FOR 30 OR MORE CONSECUTIVE CALENDAR DAYS, SUCH AS DURING WINTER SHUT-DOWNS.

(THE SIGNS ON THE MAINLINE SHALL BE DUAL MOUNTED UNLESS NOT PHYSICALLY POSSIBLE. THE FIRST SIGN SHALL BE PLACED BETWEEN THE ROAD WORK AHEAD (W20-1) SIGN AND THE NEXT SIGN IN THE SEQUENCE. SIGNS SHALL BE ERECTED ON EACH ENTRANCE RAMP AND EVERY 2 MILES THROUGH THE CONSTRUCTION WORK LIMITS. SIGNS ON THE MAINLINE SHALL BE R11-H5A-48. SIGNS USED ON THE RAMPS SHALL BE R11-H5A-24. R11-H5A-24 SIGNS MAY BE USED IN THE MEDIAN IN LIEU OF R11-H5A-48 SIGNS IF IT IS NOT PHYSICALLY POSSIBLE TO PROVIDE R11-H5A-48 SIGNS IN THE MEDIAN.)

THE R11-H5A-48 SIGNS SHALL BE MOUNTED ON 2 NO. 3 POSTS WHEN LOCATED WITHIN CLEAR ZONES.

THE CONTRACTOR MAY USE SIGNS AND SUPPORTS IN USED, BUT GOOD, CONDITION PROVIDED THE SIGNS MEET CURRENT ODOT SPECIFICATIONS. SIGN FACES SHALL BE RETROREFLECTORIZED WITH TYPE G SHEETING COMPLYING WITH THE REQUIREMENTS OF C&MS 730.19.

WORK ZONE INCREASED PENALTIES SIGNS AND SUPPORTS WILL BE MEASURED AS THE NUMBER OF SIGN INSTALLATIONS, INCLUDING THE SIGN AND NECESSARY SUPPORTS. IF A SIGN AND SUPPORT COMBINATION IS REMOVED AND REERECTED AT ANOTHER LOCATION AS DIRECTED BY THE ENGINEER, IT SHALL BE CONSIDERED ANOTHER UNIT.

PAYMENT FOR ACCEPTED QUANTITIES, COMPLETE, IN PLACE WILL BE MADE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIALS, LABOR, INCIDENTALS AND EQUIPMENT FOR FURNISHING, ERECTING, MAINTAINING, COVERING DURING SUSPENSION OF WORK, AND REMOVAL OF THE SIGN AND SUPPORT.

ITEM 614, WORK ZONE INCREASED PENALTIES SIGN 24 EACH

WORK ZONE INCREASED PENALTIES SIGNS WILL BE PLACED AT THE FOLLOWING LOCATIONS:

- BEGIN PROJECT - NORTHBOUND ONLY (2)
- C.R. 7G INTERCHANGE - RAMP 'A' (1)
- C.R. 7G INTERCHANGE - RAMP 'C' (1)
- APPROXIMATE SLM. 20.95 (4)
- C.R. 56 CONNECTOR INTERCHANGE - RAMP 'E' (1)
- C.R. 56 CONNECTOR INTERCHANGE - RAMP 'H' (1)
- APPROXIMATE SLM. 22.90 (4)
- C.R. 46 INTERCHANGE - RAMP 'A' (1)
- C.R. 46 INTERCHANGE - RAMP 'D' (1)
- APPROXIMATE SLM. 24.85 (4)

WORK ZONE INCREASED PENALTIES SIGN (R11-H5A) (CONTINUED)

- ALEXANDER ST. INTERCHANGE - RAMP 'E' (1)
- ALEXANDER ST. INTERCHANGE - RAMP 'H' (1)
- END PROJECT - SOUTHBOUND ONLY (2)

CONTRACTOR'S EQUIPMENT - OPERATION AND STORAGE

IN ADDITION TO THE REQUIREMENTS OF SECTION 614.03 OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS THE FOLLOWING SHALL APPLY. THE CONTRACTOR'S EQUIPMENT SHALL BE OPERATED IN THE DIRECTION OF TRAFFIC WHERE PRACTICAL. A FLAGGER SHALL BE USED WHERE THE CONTRACTOR'S EQUIPMENT MUST MERGE WITH THE TRAFFIC STREAM. THE CONTRACTOR'S VEHICLES AND EQUIPMENT SHALL BE EQUIPPED WITH AT LEAST ONE AMBER FLASHING LIGHT.

EQUIPMENT MAY BE PARKED IN AREAS ALONG THE HIGHWAY, THIRTY FEET (30') FROM THE EDGE OF TRAVELED HIGHWAY UNLESS BEHIND GUARDRAIL, WHEN VARIOUS OPERATIONS ARE SCHEDULED TO CONTINUE THE NEXT WORKDAY. ON WEEKENDS OR AT OTHER TIMES OF SUSPENSION OF WORK, THE EQUIPMENT SHALL BE STORED AT A STORAGE AREA REMOVED FROM THE STATE ROUTE RIGHT OF WAY. NO EQUIPMENT SHALL BE PARKED IN THE MEDIAN OF THE HIGHWAY. ADEQUATE BARRICADES AND LIGHT SHALL BE PLACED ON THE PAVEMENT SIDE OF THE EQUIPMENT TO IDENTIFY THE LIMITS OF THE EQUIPMENT. ALL OTHER EQUIPMENT, INCLUDING PRIVATE VEHICLES, SHALL BE STORED AT THE APPROVED CONTRACTOR'S STORAGE AREA.

DESIGN AGENCY



DESIGNER

DAH

REVIEWER

JPB 06-10-22

PROJECT ID

102458

SHEET

P.10

TOTAL

39

SHEET NUM.														PART. 01/NHS/PV	ITEM ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
7	8	27	28	29	30	31	32	33	34	35	36	37	39							
																			ROADWAY	
LS														LS	201	11000	LS		CLEARING AND GRUBBING	
												110		110	202	30000	110	SF	WALK REMOVED	
	121													121	607	98000	121	FT	FENCE, MISC.: TEMPORARY ORANGE PLASTIC CONSTRUCTION FENCE	8
												110		110	608	52000	110	SF	CURB RAMP	
																			EROSION CONTROL	
												3		3	659	98000	3	SY	SEEDING, MISC.: CURB RAMP GRADING RESTORATION	37
														1,000	832	30000	1,000	EACH	EROSION CONTROL	
																			PAVEMENT	
											100			100	251	01010	100	CY	PARTIAL DEPTH PAVEMENT REPAIR (441)	
		56,220	55,690	110,179	19,830	19,830	18,987	18,987	18,509	21,421				339,653	254	01001	339,653	SY	PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN (1-1/2")	7
		1,687	1,671	3,305	595	595	570	570	555	643				10,191	254	01600	10,191	SY	PATCHING PLANED SURFACE	
											300			300	255	10011	300	SY	FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC1, AS PER PLAN	36
											1,000			1,000	255	20000	1,000	FT	FULL DEPTH PAVEMENT SAWING	
		4,778	4,733	9,365	1,686	1,686	1,614	1,614	1,573	1,821				28,870	407	20000	28,870	GAL	NON-TRACKING TACK COAT	
					1,670	1,670	1,599	1,599	1,123	1,299				8,960	408	10001	8,960	GAL	PRIME COAT, AS PER PLAN	7
		2,343	2,321	4,591										9,255	442	00100	9,255	CY	ANTI-SEGREGATION EQUIPMENT	
		2,343	2,321	4,591	826	826	791	791	786	893				14,168	442	10351	14,168	CY	ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE B (447), AS PER PLAN	7
					174	174	167	167	125	135				942	617	10101	942	CY	COMPACTED AGGREGATE, AS PER PLAN	7
					4,175	4,175	3,997	3,997	2,807	3,246				22,397	617	20000	22,397	SY	SHOULDER PREPARATION	
					18,787	18,787	17,988	17,988						73,550	618	40100	73,550	FT	RUMBLE STRIPS, SHOULDER (ASPHALT CONCRETE)	
																			TRAFFIC CONTROL	
													1,000	1,000	621	00100	1,000	EACH	RPM	
													1,000	1,000	621	54000	1,000	EACH	RAISED PAVEMENT MARKER REMOVED	
													0.09	0.09	646	10200	0.09	MILE	CENTER LINE	
													293	293	646	10400	293	FT	STOP LINE	
													110	110	646	10800	110	SF	ISLAND MARKING	
													1	1	646	20300	1	EACH	LANE ARROW	
													16	16	646	20320	16	EACH	WRONG WAY ARROW	
													34.14	34.14	807	12010	34.14	MILE	WET REFLECTIVE EPOXY PAVEMENT MARKING, EDGE LINE, 6"	
													15.4	15.4	807	12110	15.4	MILE	WET REFLECTIVE EPOXY PAVEMENT MARKING, LANE LINE, 6"	
													6,686	6,686	807	12310	6,686	FT	WET REFLECTIVE EPOXY PAVEMENT MARKING, CHANNELIZING LINE, 12"	
													11,465	11,465	807	12410	11,465	FT	WET REFLECTIVE EPOXY PAVEMENT MARKING, DOTTED LINE, 6"	
													49.54	49.54	850	10010	49.54	MILE	GROOVING FOR 6" RECESSED PAVEMENT MARKING, (ASPHALT)	
													11,465	11,465	850	10110	11,465	FT	GROOVING FOR 6" RECESSED PAVEMENT MARKING, (ASPHALT)	
													6,686	6,686	850	10130	6,686	FT	GROOVING FOR 12" RECESSED PAVEMENT MARKING, (ASPHALT)	
																			MISCELLANEOUS STRUCTURE	
50														50	519	11100	50	SF	PATCHING CONCRETE STRUCTURE	7

GENERAL SUMMARY

DESIGN AGENCY


DESIGNER
DAH

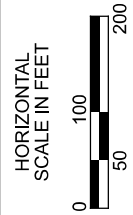
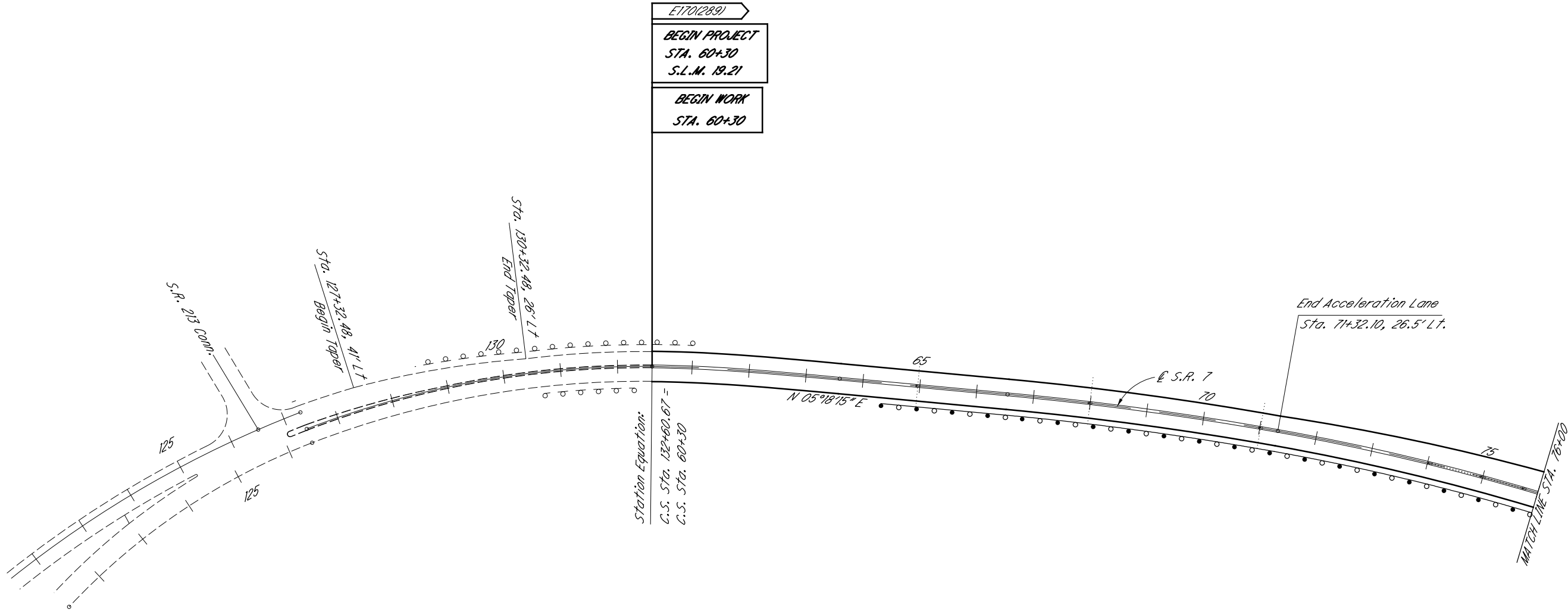
REVIEWER
JPB 06-10-22

PROJECT ID
102458

SHEET
P.11

TOTAL
39

<div style="text-align: center;"> <h1>GENERAL SUMMARY</h1> </div>	
DESIGN AGENCY	
	
DESIGNER	
DAH	
REVIEWER	
JPB	06-10-22
PROJECT ID	
102458	
SHEET	TOTAL
P.12	39

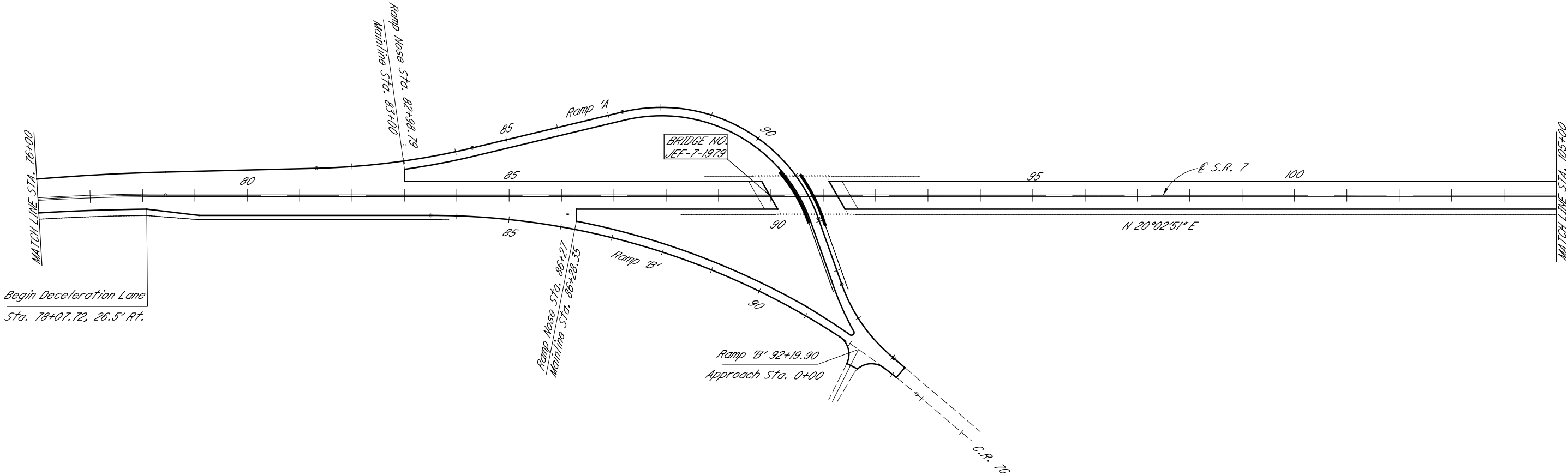


PLAN SHEET
STA 121+00 TO STA 76+00

DESIGN AGENCY



DESIGNER	ADK
REVIEWER	JPB 06-10-22
PROJECT ID	102458
SHEET	P.13
TOTAL	39



DESIGN AGENCY

DESIGNER
ADK

REVIEWER
JPB 06-10-22

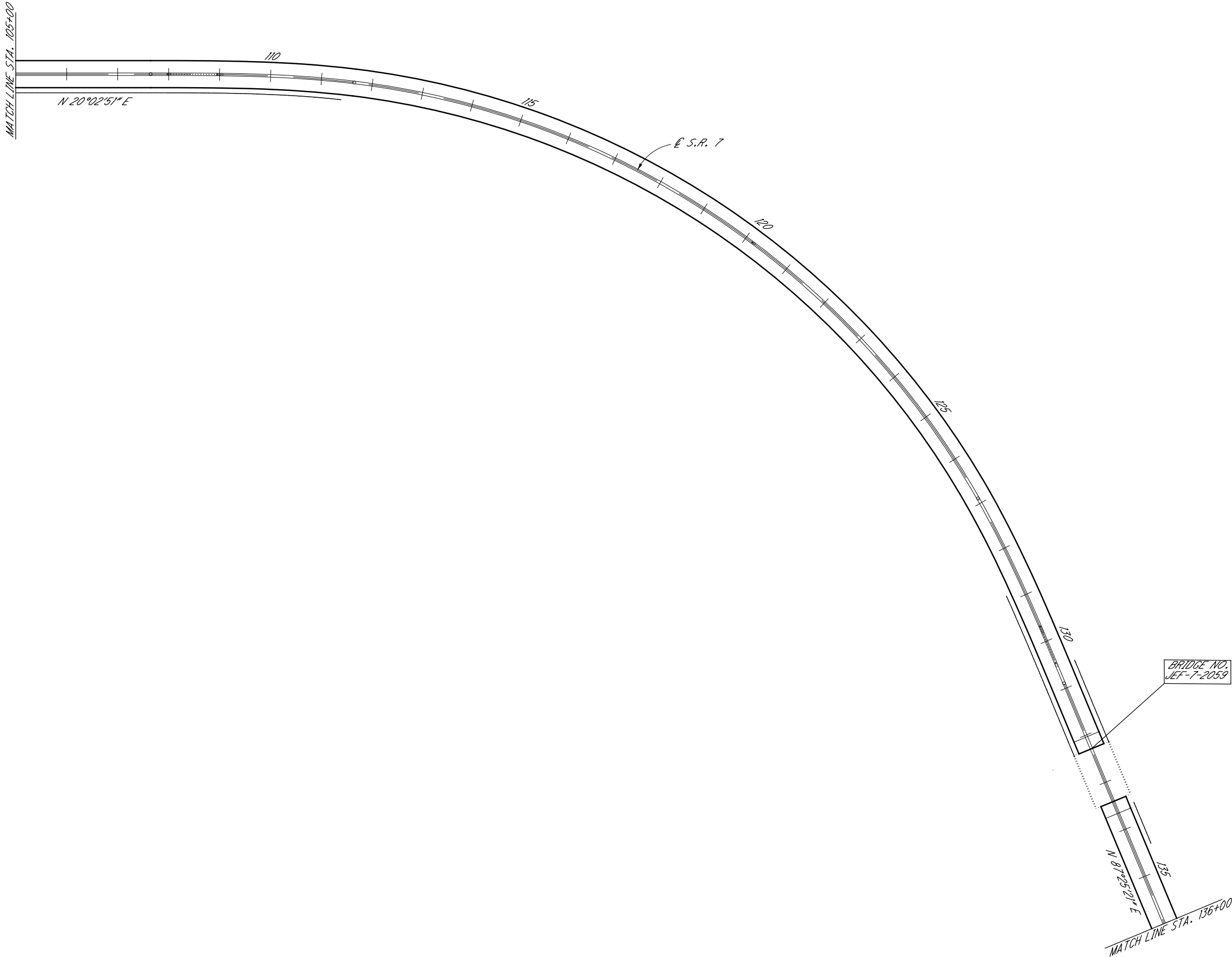
PROJECT ID
102458

SHEET
P.14

TOTAL
39

PLAN SHEET
STA 76+00 TO STA 105+00

HORIZONTAL
SCALE IN FEET
0 50 100 200



DESIGN AGENCY

DESIGNER
ADK

REVIEWER
JPB 06-10-22

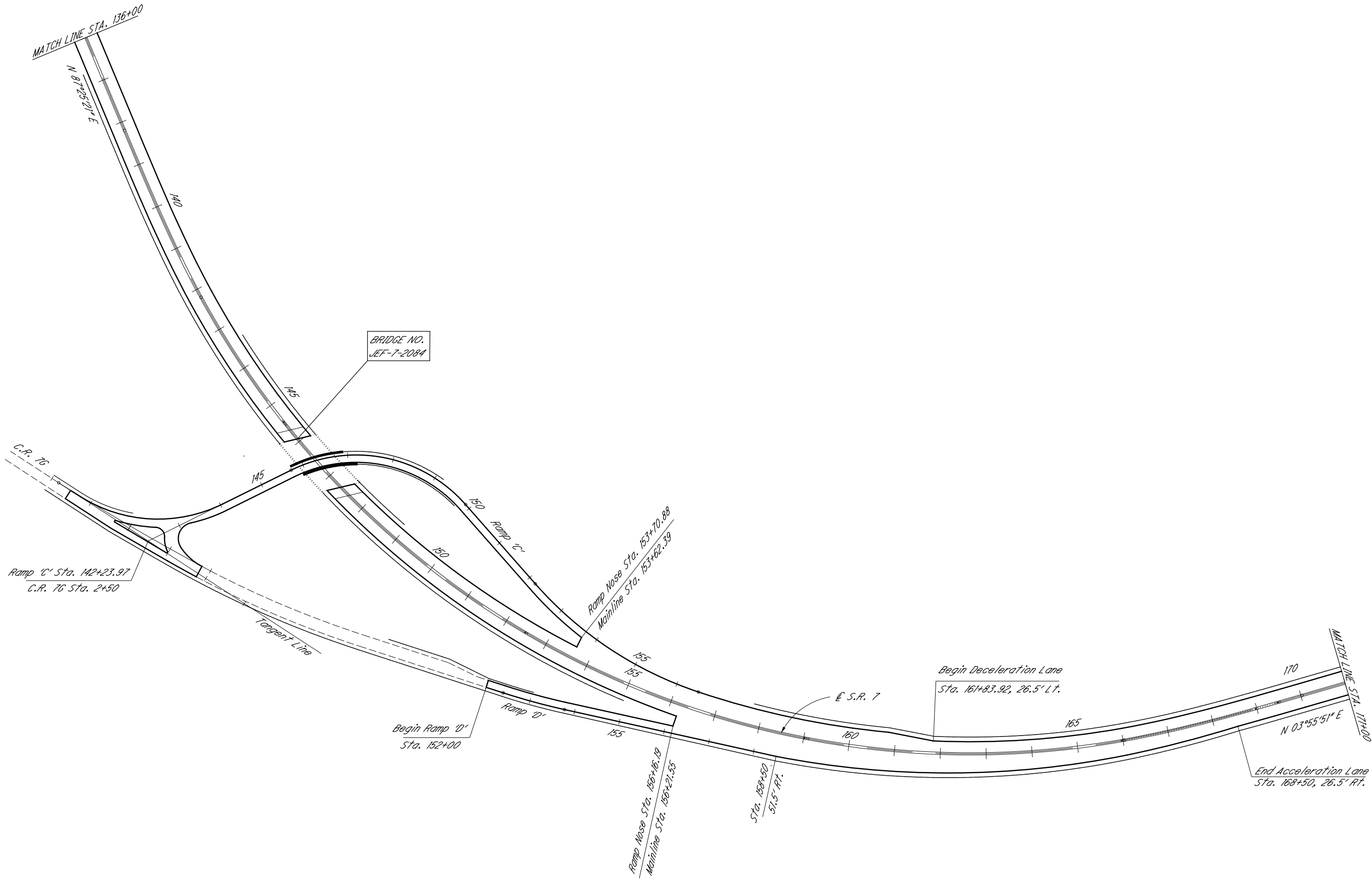
PROJECT ID
102458

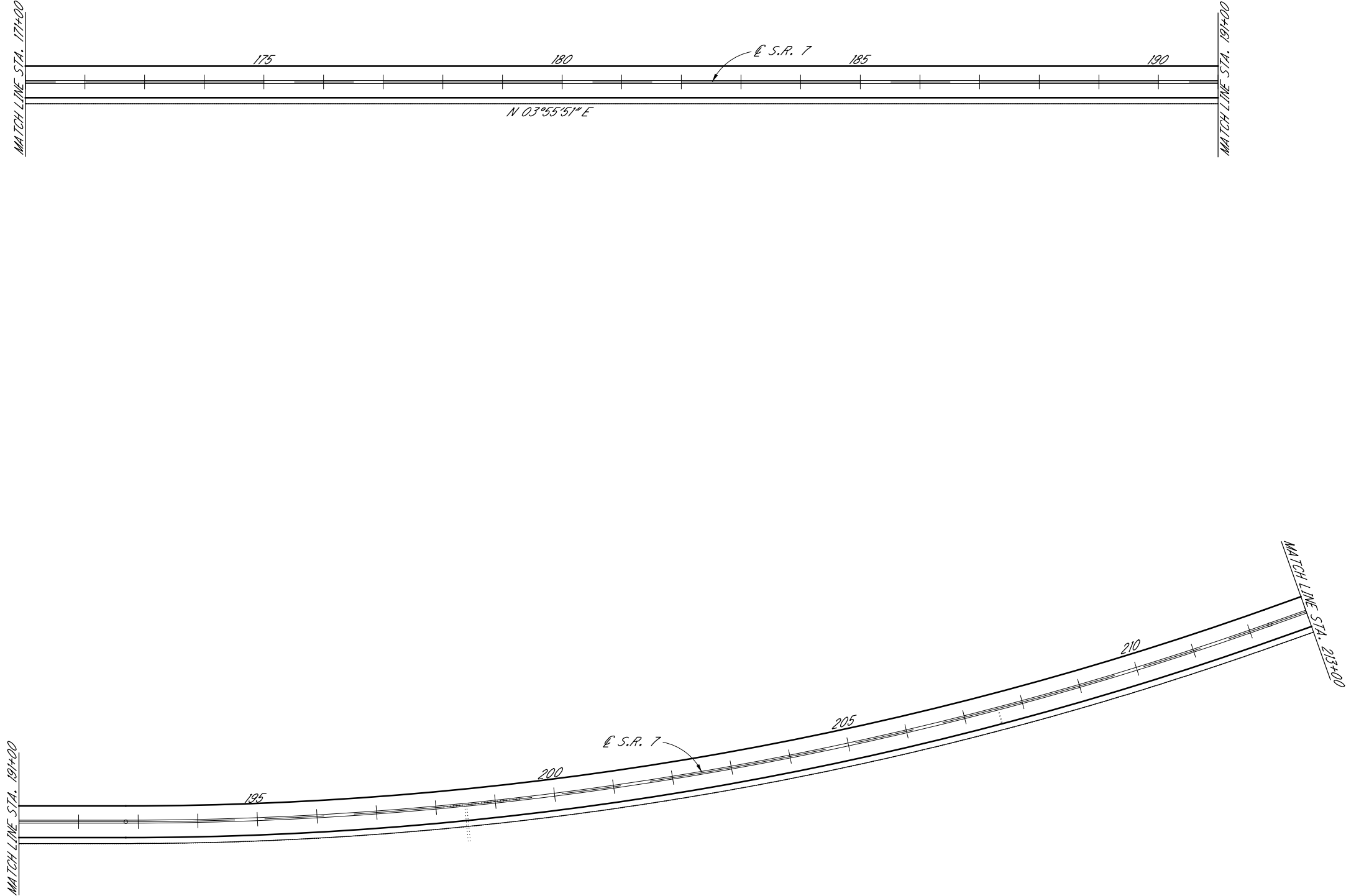
SHEET P.15	TOTAL 39
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PLAN SHEET
STA 105+00 TO STA 136+00



HORIZONTAL
SCALE IN FEET





DESIGN AGENCY

DESIGNER

ADK

REVIEWER

JPB 06-10-22

PROJECT ID

102458

SHEET

P.17

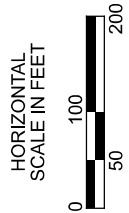
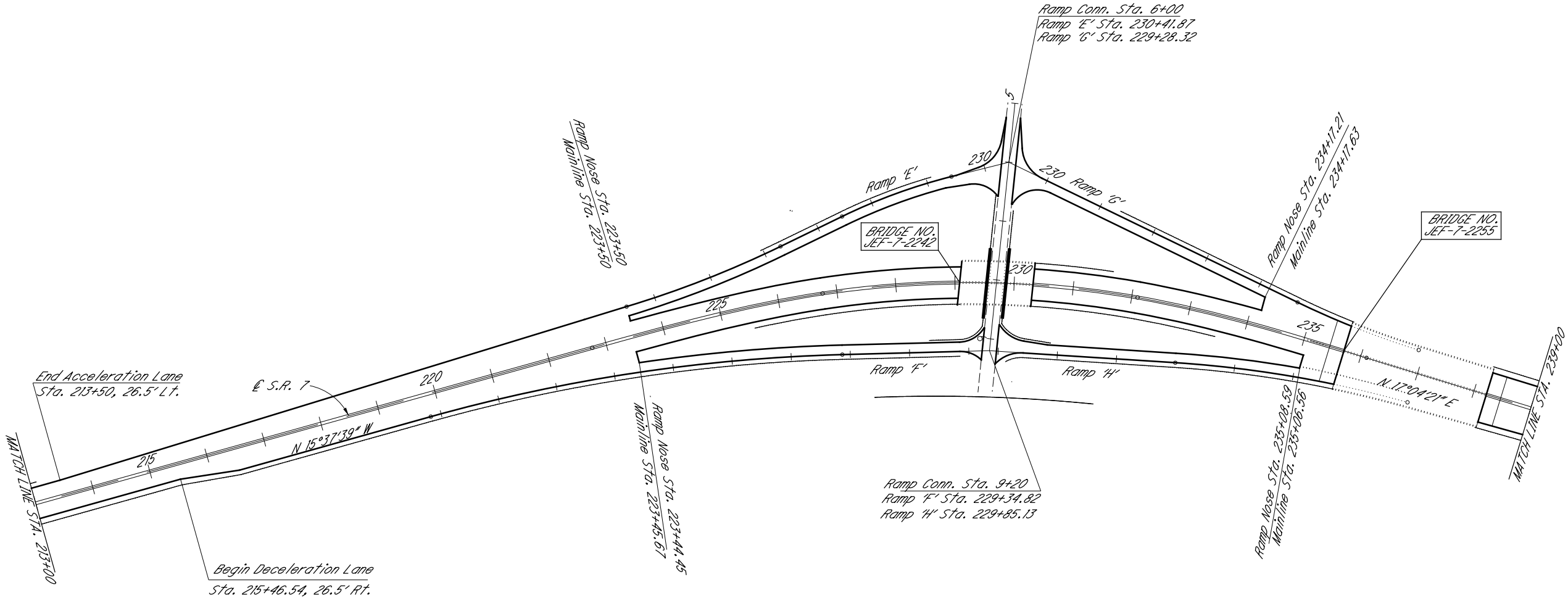
TOTAL

39

PLAN SHEET
STA 171+00 TO STA 213+00

HORIZONTAL
SCALE IN FEET





PLAN SHEET
STA. 213+00 TO STA. 239+00

DESIGN AGENCY

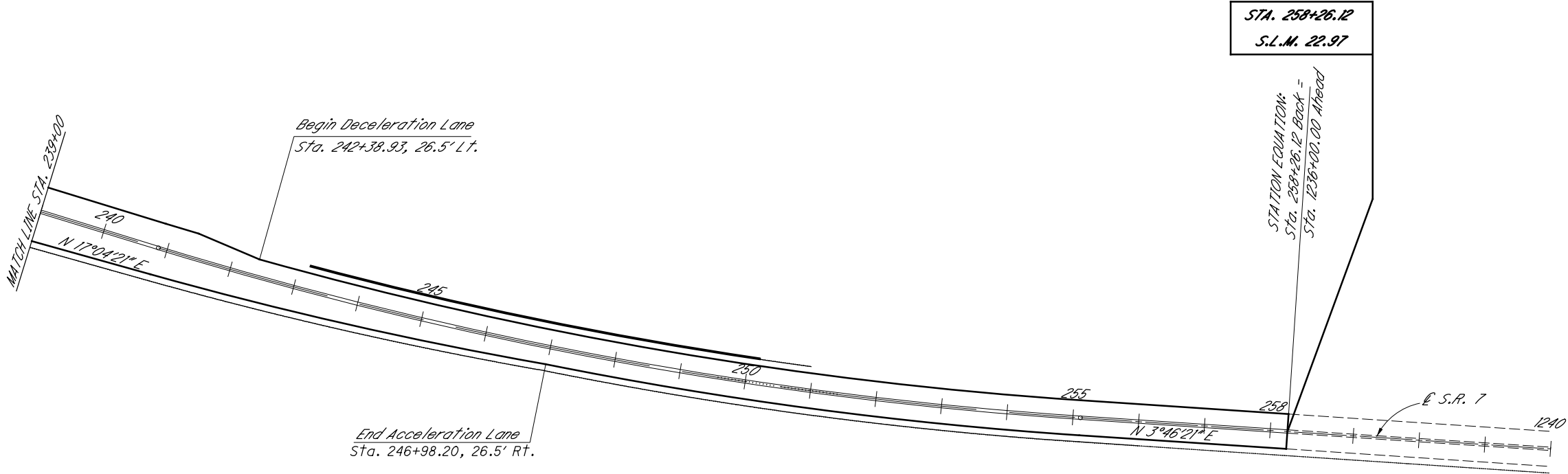


DESIGNER
ADK

REVIEWER
JPB 06-10-22

PROJECT ID
102458

SHEET TOTAL
P.18 39



DESIGN AGENCY

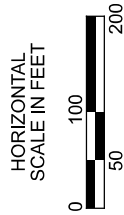
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ADK

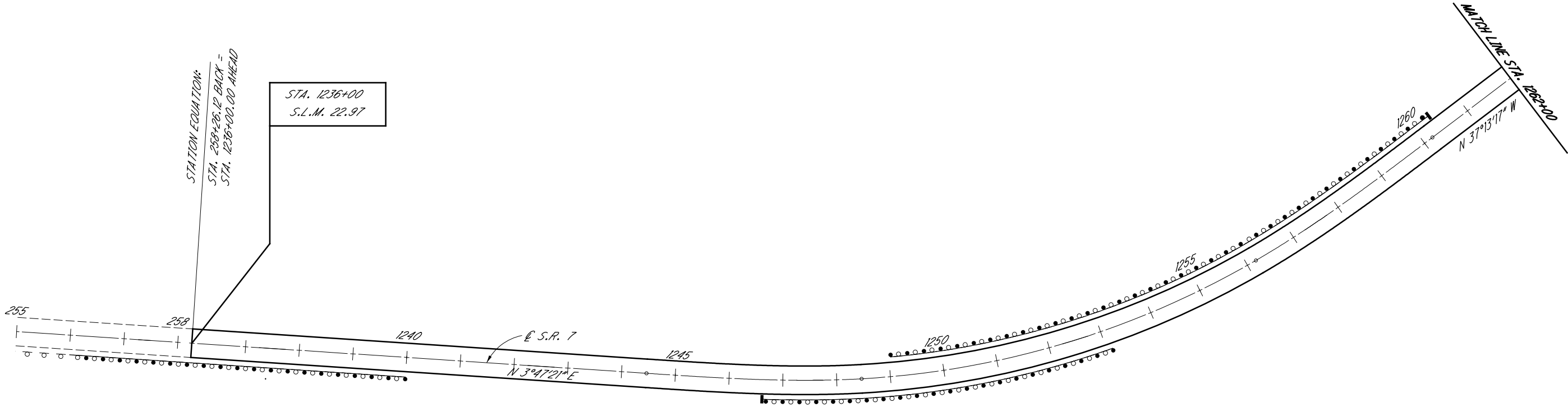
REVIEWER
JPB 06-10-22

PROJECT ID
102458

SHEET	TOTAL
P.19	39

PLAN SHEET
STA. 239+00 TO STA. 258+26.12





DESIGN AGENCY

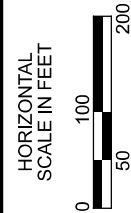
DESIGNER
ADK

REVIEWER
JPB 06-10-22

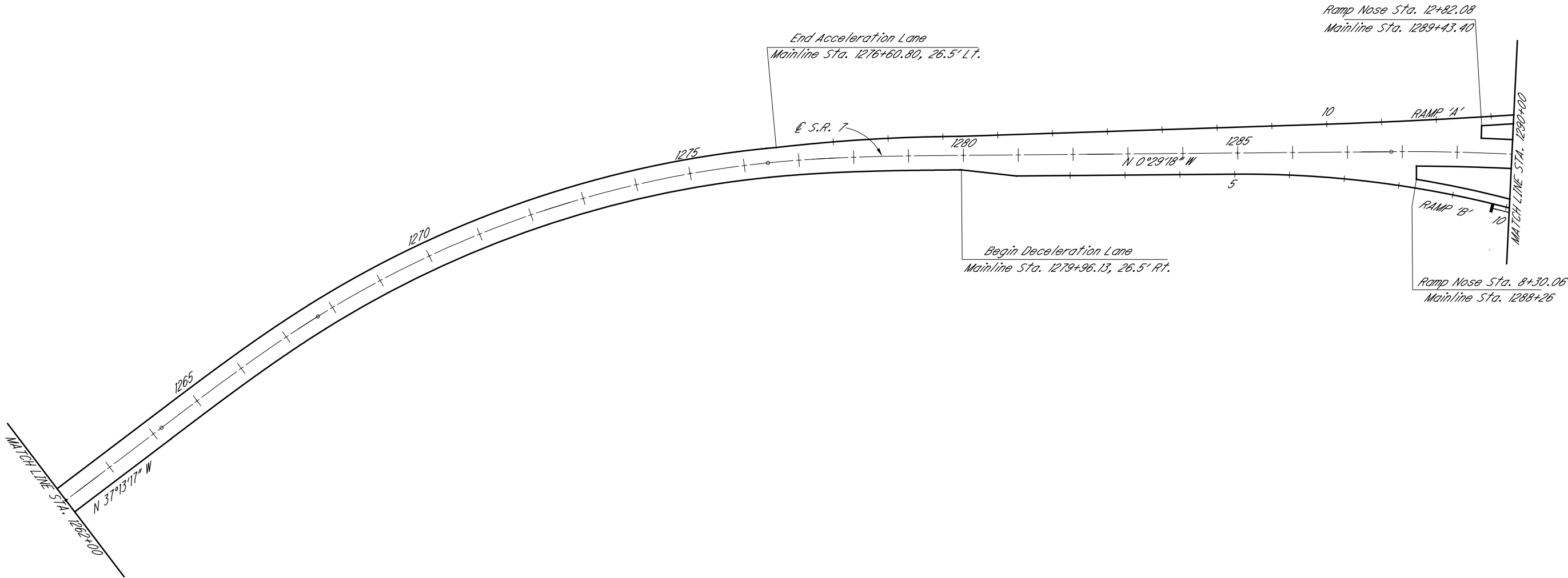
PROJECT ID
102458

SHEET	TOTAL
P.20	39

PLAN SHEET
STA 1236+00 TO STA 1262+00



HORIZONTAL
SCALE IN FEET



DESIGN AGENCY

DESIGNER

ADK

REVIEWER

JPB 06-10-22

PROJECT ID

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SHEET

P.21

TOTAL

39

PLAN SHEET

STA 1262+00 TO STA 1290+00

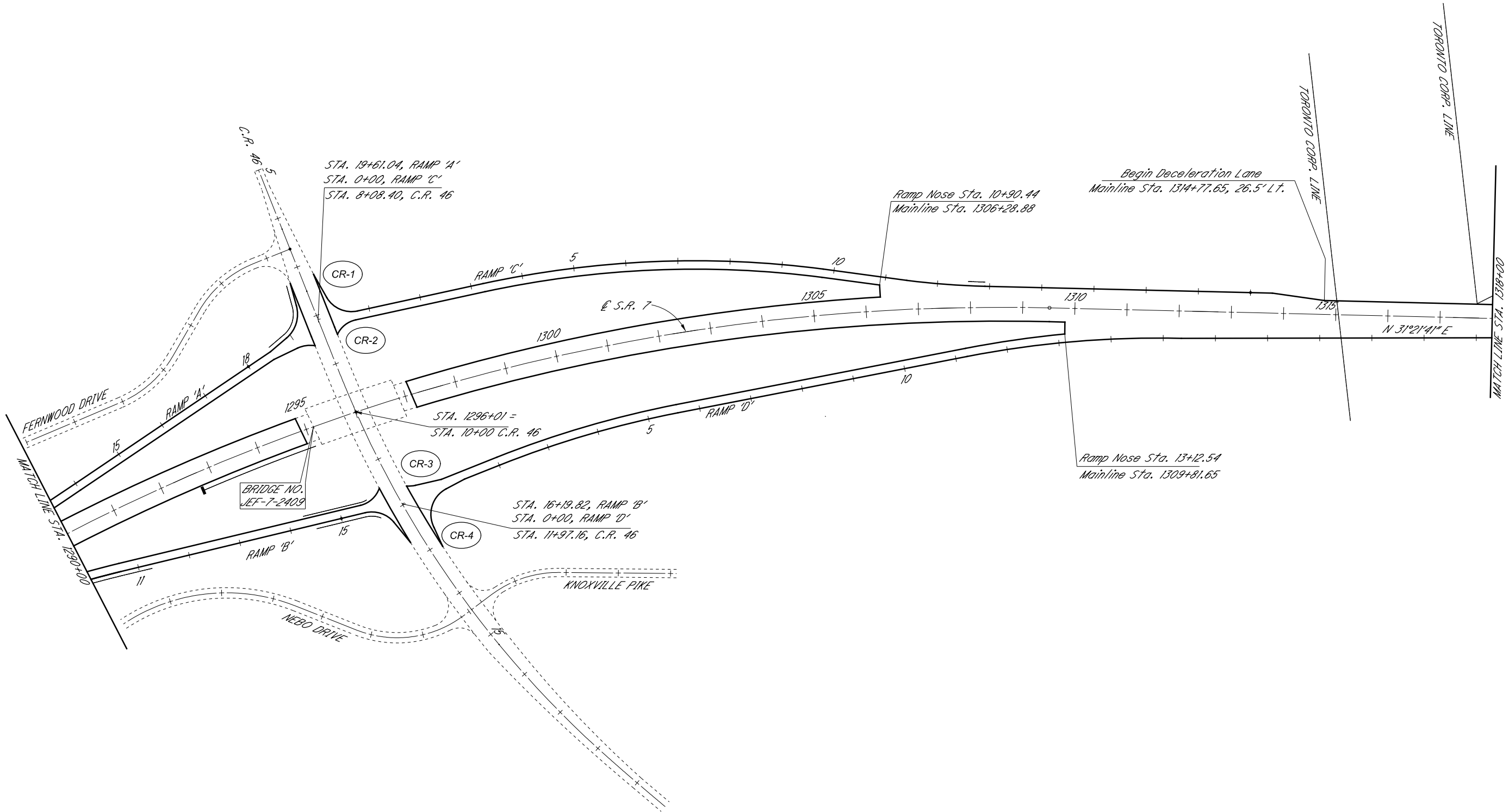
HORIZONTAL SCALE IN FEET

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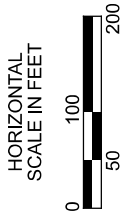
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100

200



FOR CURB RAMP DETAILS AND QUANTITIES, SEE SHEET 37.



PLAN SHEET
STA 1290+00 TO STA 1318+00

DESIGN AGENCY



DESIGNER

ADK

REVIEWER

JPB 06-10-22

PROJECT ID

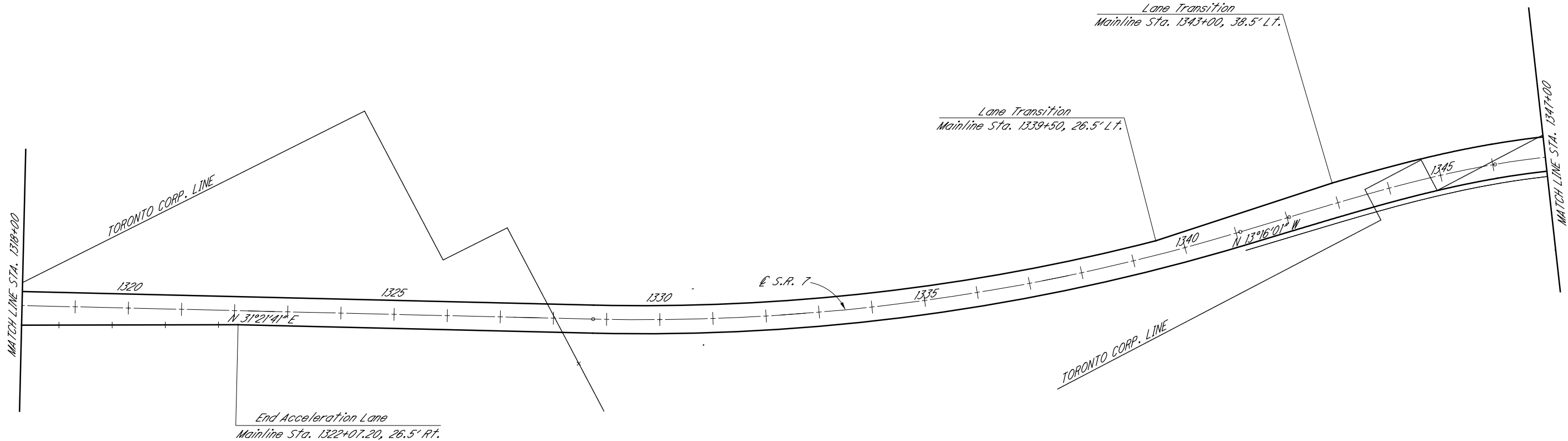
102458

SHEET

P.22

TOTAL

39



DESIGN AGENCY

DESIGNER

ADK

REVIEWER

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PROJECT ID

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SHEET

P.23

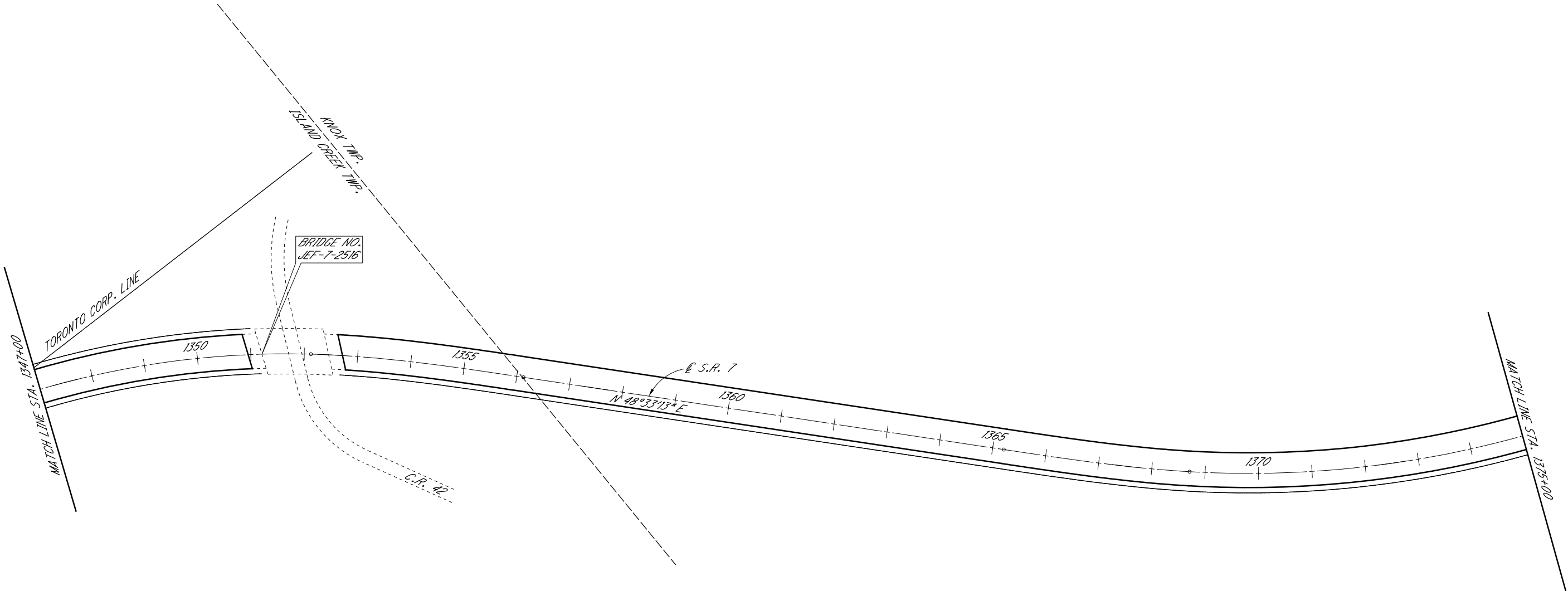
TOTAL

39

PLAN SHEET
STA 1318+00 TO STA 1347+00

HORIZONTAL
SCALE IN FEET





PLAN SHEET
STA 1347+00 TO STA 1375+00

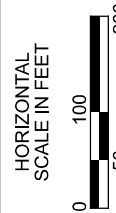


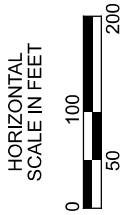
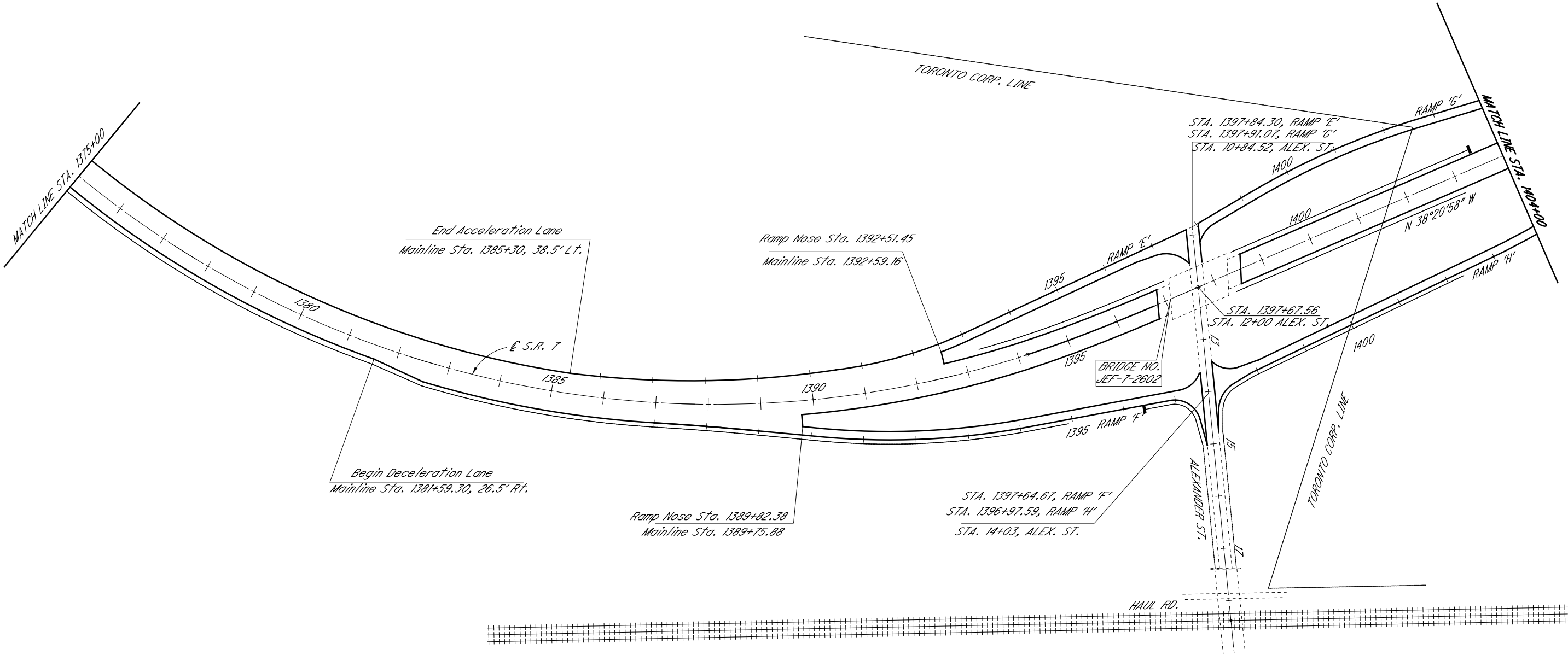
DESIGNER
ADK

REVIEWER
JPB 06-10-22

PROJECT ID
102458

SHEET	TOTAL
P.24	39





PLAN SHEET
STA 1375+00 TO STA 1404+00

DESIGN AGENCY



DESIGNER

ADK

REVIEWER

JPB 06-10-22

PROJECT ID

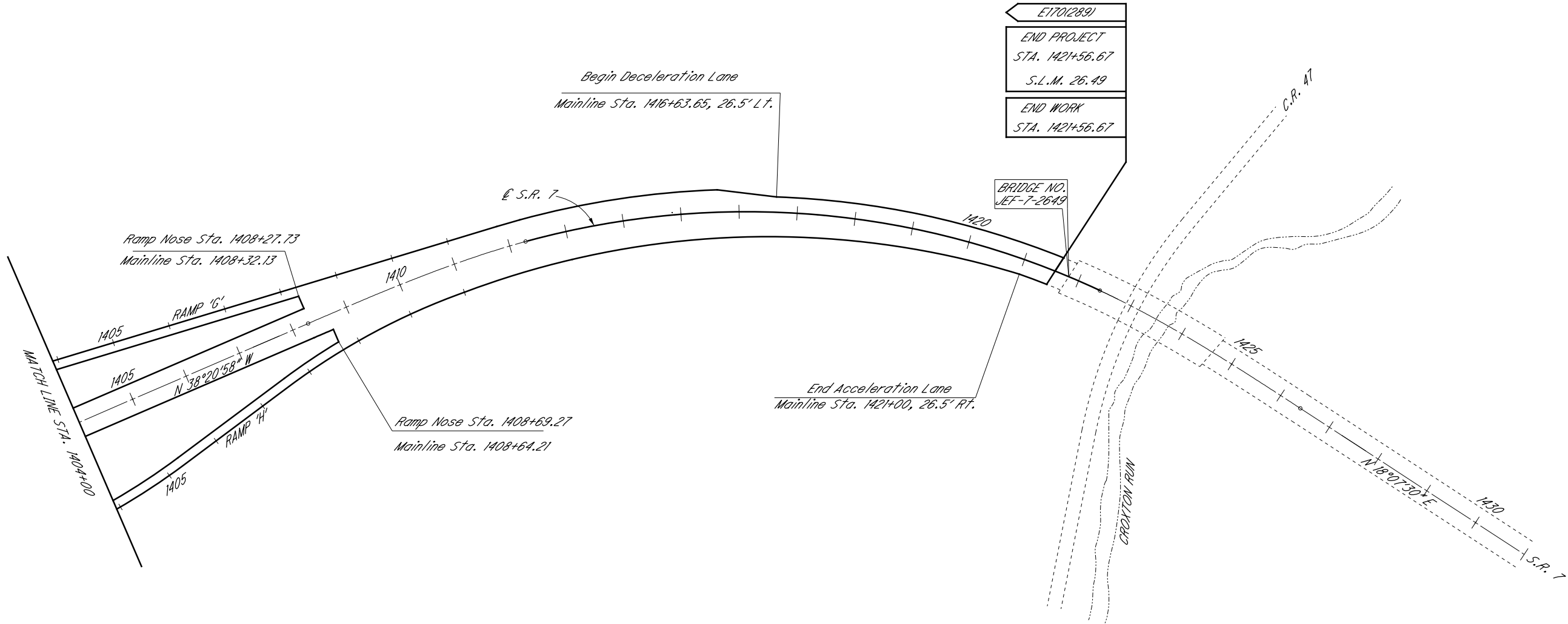
102458

SHEET

P.25

TOTAL

39



PLAN SHEET
STA 1404+00 TO STA 1430+00



DESIGNER	ADK
REVIEWER	JPB
PROJECT ID	102458
SHEET	P.26
TOTAL	39

LOCATION	STATION		LENGTH	WIDTH	TYPICAL	PAVEMENT AREA		254			407	442		442	REMARKS
								IN.	PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN (1-1/2")	PATCHING PLANED SURFACE (3%)	NON-TRACKING TACK COAT @ 0.085 GAL./S.Y.	IN.	ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE B (447), AS PER PLAN	ANTI- SEGREGATION EQUIPMENT	
						SQ YD		SQ YD	SQ YD	GAL		CU YD	CU. YD.		
NORTHBOUND	FROM	TO	FT	FT		SQ YD									
S.R. 7 MAINLINE															
	60+30.00	89+75.00	2,945.00	24.00		7,853		1 ½	7,853	236	668	1 ½	327	327	
BR. NO.JEF-7-1979	OMIT(179')														OMIT BRIDGE DECK & APPROACH SLABS
	91+54.00	132+00.00	4,046.00	24.00		10,789		1 ½	10,789	324	917	1 ½	450	450	
BR. NO. JEF-7-2059	OMIT (163')														OMIT BRIDGE DECK & APPROACH SLABS
	133+63.00	145+65.00	1,202.00	24.00		3,205		1 ½	3,205	96	272	1 ½	134	134	
BR. NO. JEF-7-2084	OMIT (192')														OMIT BRIDGE DECK & APPROACH SLABS
	147+57.00	229+20.00	8,163.00	24.00		21,768		1 ½	21,768	653	1,850	1 ½	907	907	
BR. NO. JEF-7-2242	OMIT (174')														OMIT BRIDGE DECK & APPROACH SLABS
	230+94.00	235+45.00	451.00	24.00		1,203		1 ½	1,203	36	102	1 ½	50	50	
BR. NO. JEF-7-2255	OMIT (301')														OMIT BRIDGE DECK & APPROACH SLABS
	238+46.00	258+26.12	1,980.12	24.00		5,280		1 ½	5,280	158	449	1 ½	220	220	
DECEL. LANE RAMP 'B'	78+07.72	86+28.35	820.63	VARIES		1,306		1 ½	1,306	39	111	1 ½	54	54	
ACCEL. LANE RAMP 'D'	156+21.55	168+50.00	1,228.45	VARIES		2,190		1 ½	2,190	66	186	1 ½	91	91	
DECEL. LANE RAMP 'F'	215+46.54	223+45.67	799.13	VARIES		1,308		1 ½	1,308	39	111	1 ½	55	55	
ACCEL. LANE RAMP 'H'	235+06.56	235+69.92	63.36	VARIES		248		1 ½	248	7	21	1 ½	10	10	
	238+21.42	246+98.20	876.78	VARIES		1,070		1 ½	1,070	32	91	1 ½	45	45	
TOTALS CARRIED TO GENERAL SUMMARY									56,220	1,687	4,778		2,343	2,343	01/NHS/PV

ESTIMATED QUANTITIES

DESIGN AGENCY



DESIGNER

ADK

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SHEET

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TOTAL

39

LOCATION	STATION		LENGTH	WIDTH	TYPICAL	PAVEMENT AREA		254			407	442		442	REMARKS
								IN.	PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN, (1-1/2")	PATCHING PLANED SURFACE (3%)	NON-TRACKING TACK COAT @ 0.085 GAL./S.Y.	IN.	ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE B (447), AS PER PLAN	ANTI- SEGREGATION EQUIPMENT	
						SQ YD		SQ YD	SQ YD	GAL		CU YD	CU. YD.		
SOUUTHBOUND	FROM	TO	FT	FT		SQ YD									
S.R. 7 MAINLINE															
	60+30.00	89+75.00	2,945.00	24.00		7,853		1 ½	7,853	236	668	1 ½	327	327	
BR. NO.JEF-7-1979	OMIT(179')														OMIT BRIDGE DECK & APPROACH SLABS
	91+54.00	132+00.00	4,046.00	24.00		10,789		1 ½	10,789	324	917	1 ½	450	450	
BR. NO. JEF-7-2059	OMIT (163')														OMIT BRIDGE DECK & APPROACH SLABS
	133+63.00	145+65.00	1,202.00	24.00		3,205		1 ½	3,205	96	272	1 ½	134	134	
BR. NO. JEF-7-2084	OMIT (192')														OMIT BRIDGE DECK & APPROACH SLABS
	147+57.00	229+20.00	8,163.00	24.00		21,768		1 ½	21,768	653	1,850	1 ½	907	907	
BR. NO. JEF-7-2242	OMIT (174')														OMIT BRIDGE DECK & APPROACH SLABS
	230+94.00	235+45.00	451.00	24.00		1,203		1 ½	1,203	36	102	1 ½	50	50	
BR. NO. JEF-7-2255	OMIT (301')														OMIT BRIDGE DECK & APPROACH SLABS
	238+46.00	258+26.12	1,980.12	24.00		5,280		1 ½	5,280	158	449	1 ½	220	220	
ACCEL. RAMP 'A'	71+32.10	83+00.00	1,167.90	VARIES		1,961		1 ½	1,961	59	167	1 ½	82	82	
DECEL. RAMP 'C'	153+62.39	161+83.92	821.53	VARIES		1,297		1 ½	1,297	39	110	1 ½	54	54	
ACCEL. RAMP 'E'	213+50.00	223+50.00	1,000.00	VARIES		1,388		1 ½	1,388	42	118	1 ½	58	58	
DECEL. RAMP 'G'	234+17.63	235+69.92	152.29	VARIES		454		1 ½	454	14	39	1 ½	19	19	
	238+21.42	242+38.93	417.51	VARIES		491		1 ½	491	15	42	1 ½	20	20	
TOTALS CARRIED TO GENERAL SUMMARY									55,690	1,671	4,733		2,321	2,321	01/NHS/PV

ESTIMATED QUANTITIES

DESIGN AGENCY



DESIGNER

ADK

REVIEWER

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TOTAL

39

LOCATION	STATION		LENGTH	WIDTH	TYPICAL	PAVEMENT AREA	254			407	IN.	442		REMARKS
							IN.	PAVEMENT PLANING, ASPHALT CONCRETE, (1-1/2"), AS PER PLAN	PATCHING PLANED SURFACE (3%)	NON- TRACKING TACK COAT @ 0.085 GAL./S.Y.		ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE B (447), AS PER PLAN	ANTI- SEGREGATION EQUIPMENT	
NORTHBOUND	FROM	TO	FT	FT		SQ YD		SQ YD	SQ YD	GAL		CU YD	CU YD	
S.R. 7 MAINLINE														
	1236+00.00	1294+89.74	5,889.74	24.00		15,706	1 ½	15,706	471	1,335	1 ½	654	654	
BR. NO. JEF-7-2409	OMIT (223')													OMIT BRIDGE DECK & APPROACH SLABS
	1297+12.74	1350+95.99	5,383.25	24.00		14,355	1 ½	14,355	431	1,220	1 ½	598	598	
BR. NO. JEF-7-2516	OMIT (175')													OMIT BRIDGE DECK & APPROACH SLABS
	1352+70.99	1396+84.82	4,413.83	24.00		11,770	1 ½	11,770	353	1,000	1 ½	490	490	
BR. NO. JEF-7-2602	OMIT (169')													OMIT BRIDGE DECK & APPROACH SLABS
	1398+53.82	1421+56.67	2,302.85	24.00		6,141	1 ½	6,141	184	522	1 ½	256	256	
DECEL LANE - RAMP B	1279+96.13	1288+26.00	829.87	VARIES		1,384	1 ½	1,384	42	118	1 ½	58	58	
ACCEL LANE - RAMP D	1309+81.65	1322+07.20	1,225.55	VARIES		2,197	1 ½	2,197	66	187	1 ½	92	92	
DECEL LANE - RAMP F	1381+59.30	1389+75.88	816.58	VARIES		1,331	1 ½	1,331	40	113	1 ½	55	55	
ACCEL LANE - RAMP H	1408+64.21	1421+00.00	1,235.79	VARIES		2,440	1 ½	2,440	73	207	1 ½	102	102	
SOUTHBOUND														
	1236+00.00	1294+89.74	5,889.74	24.00		15,706	1 ½	15,706	471	1,335	1 ½	654	654	
BR. NO. JEF-7-2409	OMIT (223')													OMIT BRIDGE DECK & APPROACH SLABS
	1297+12.74	1350+95.99	5,383.25	24.00		14,355	1 ½	14,355	431	1,220	1 ½	598	598	
BR. NO. JEF-7-2516	OMIT (175')													OMIT BRIDGE DECK & APPROACH SLABS
	1352+70.99	1396+84.82	4,413.83	24.00		11,770	1 ½	11,770	353	1,000	1 ½	490	490	
BR. NO. JEF-7-2602	OMIT (169')													OMIT BRIDGE DECK & APPROACH SLABS
	1398+53.82	1421+56.67	2,302.85	24.00		6,141	1 ½	6,141	184	522	1 ½	256	256	
ACCEL LANE - RAMP A	1276+60.80	1289+43.40	1,282.60	VARIES		2,347	1 ½	2,347	70	200	1 ½	98	98	
DECEL LANE - RAMP C	1306+28.88	1314+77.65	848.77	VARIES		1,336	1 ½	1,336	40	114	1 ½	56	56	
ACCEL LANE - RAMP E	1385+30.00	1392+59.16	729.16	VARIES		761	1 ½	761	23	65	1 ½	32	32	
DECEL LANE - RAMP G	1408+32.13	1416+63.65	831.52	VARIES		2,440	1 ½	2,440	73	207	1 ½	102	102	
TOTALS CARRIED TO GENERAL SUMMARY								110,179	3,305	9,365		4,591	4,591	01/NHS/PV

ESTIMATED QUANTITIES

DESIGN AGENCY

DESIGNER
ADK

REVIEWER
JPB 06-10-22

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SHEET
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TOTAL
39

LOCATION	STATION		LENGTH	WIDTH	TYPICAL	PAVEMENT AREA	254			407	408	442		617			618	REMARKS	
							IN.	PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN, (1-1/2")	PATCHING PLANED SURFACE (3%)	NON- TRACKING TACK COAT @ 0.085 GAL./S.Y.	PRIME COAT,AS PER PLAN @ 0.4 GAL./S.Y.	IN.	ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE B (447), AS PER PLAN	COMPACTED AGGREGATE WIDTH	IN.	COMPACTED AGGREGATE, AS PER PLAN	SHOULDER PREPARATION		RUMBLE STRIPS, SHOULDER (ASPHALT CONCRETE)
								SQ YD	SQ YD	GAL	GAL		CU YD	FT.		CU YD	SQ YD		FT.
S.R. 7 N.B.	FROM	TO	FT	FT		SQ YD													
INSIDE SHOULDER																			
	60+30.00	89+75.00	2945	1.5		491	1 ½	491	15	42		1 ½	20						
BR. NO.JEF-7-1979	OMIT(179')																		
	91+54.00	132+00.00	4046	1.5		674	1 ½	674	20	57		1 ½	28.08						
BR. NO. JEF-7-2059	OMIT (163')																		
	133+63.00	145+65.00	1202	1.5		200	1 ½	200	6	17		1 ½	8						
BR. NO. JEF-7-2084	OMIT (192')																		
	147+57.00	229+20.00	8163	1.5		1,361	1 ½	1,361	41	116		1 ½	57						
BR. NO. JEF-7-2242	OMIT (174')																		
	230+94.00	235+45.00	451	1.5		75	1 ½	75	2	6		1 ½	3						
BR. NO. JEF-7-2255	OMIT (301')																		
	238+46.00	258+26.12	1980.12	1.5		330	1 ½	330	10	28		1 ½	14						
OUTSIDE SHOULDER																			
	60+30.00	89+75.00	2945	8		2,618	1 ½	2,618	79	223	262	1 ½	109	2	1 ½	27	654	2945	
BR. NO.JEF-7-1979	OMIT(179')																		
	91+54.00	132+00.00	4046	8		3,596	1 ½	3,596	108	306	360	1 ½	149.83	2	1 ½	37	899	4046	
BR. NO. JEF-7-2059	OMIT (163')																		
	133+63.00	145+65.00	1202	8		1,068	1 ½	1,068	32	91	107	1 ½	45	2	1 ½	11	267	1202	
BR. NO. JEF-7-2084	OMIT (192')																		
	147+57.00	229+20.00	8163	8		7,256	1 ½	7,256	218	617	726	1 ½	302	2	1 ½	76	1814	8163	
BR. NO. JEF-7-2242	OMIT (174')																		
	230+94.00	235+45.00	451	8		401	1 ½	401	12	34	40	1 ½	17	2	1 ½	4	100	451	
BR. NO. JEF-7-2255	OMIT (301')																		
	238+46.00	258+26.12	1980.12	8		1,760	1 ½	1,760	53	150	176	1 ½	73	2	1 ½	18	440	1980	
TOTALS CARRIED TO GENERAL SUMMARY								19,830	595	1,686	1,670		826			174	4,175	18,787	01/NHS/PV



LOCATION	STATION		LENGTH	WIDTH	TYPICAL	PAVEMENT AREA	254			407	408	442		617				618	REMARKS
							IN.	PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN, (1-1/2")	PATCHING PLANED SURFACE (3%)	NON- TRACKING TACK COAT @ 0.085 GAL./S.Y.	PRIME COAT,AS PER PLAN @ 0.4 GAL./S.Y.	IN.	ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE B (447), AS PER PLAN	COMPACTED AGGREGATE WIDTH	IN.	COMPACTED AGGREGATE, AS PER PLAN	SHOULDER PREPARATION	RUMBLE STRIPS, SHOULDER (ASPHALT CONCRETE)	
								SQ YD	SQ YD	GAL	GAL		CU YD	FT.		CU YD	SQ YD	FT.	
S.R. 7 S.B.	FROM	TO	FT	FT		SQ YD													
INSIDE SHOULDER																			
	60+30.00	89+75.00	2945	1.5		491	1 ½	491	15	42		1 ½	20						
BR. NO.JEF-7-1979	OMIT(179')																		
	91+54.00	132+00.00	4046	1.5		674	1 ½	674	20	57		1 ½	28.08						
BR. NO. JEF-7-2059	OMIT (163')																		
	133+63.00	145+65.00	1202	1.5		200	1 ½	200	6	17		1 ½	8						
BR. NO. JEF-7-2084	OMIT (192')																		
	147+57.00	229+20.00	8163	1.5		1,361	1 ½	1,361	41	116		1 ½	57						
BR. NO. JEF-7-2242	OMIT (174')																		
	230+94.00	235+45.00	451	1.5		75	1 ½	75	2	6		1 ½	3						
BR. NO. JEF-7-2255	OMIT (301')																		
	238+46.00	258+26.12	1980.12	1.5		330	1 ½	330	10	28		1 ½	14						
OUTSIDE SHOULDER																			
	60+30.00	89+75.00	2945	8		2,618	1 ½	2,618	79	223	262	1 ½	109	2	1 ½	27	654	2945	
BR. NO.JEF-7-1979	OMIT(179')																		
	91+54.00	132+00.00	4046	8		3,596	1 ½	3,596	108	306	360	1 ½	149.83	2	1 ½	37	899	4046	
BR. NO. JEF-7-2059	OMIT (163')																		
	133+63.00	145+65.00	1202	8		1,068	1 ½	1,068	32	91	107	1 ½	45	2	1 ½	11	267	1202	
BR. NO. JEF-7-2084	OMIT (192')																		
	147+57.00	229+20.00	8163	8		7,256	1 ½	7,256	218	617	726	1 ½	302	2	1 ½	76	1814	8163	
BR. NO. JEF-7-2242	OMIT (174')																		
	230+94.00	235+45.00	451	8		401	1 ½	401	12	34	40	1 ½	17	2	1 ½	4	100	451	
BR. NO. JEF-7-2255	OMIT (301')																		
	238+46.00	258+26.12	1980.12	8		1,760	1 ½	1,760	53	150	176	1 ½	73	2	1 ½	18	440	1980	
TOTALS CARRIED TO GENERAL SUMMARY								19,830	595	1,686	1,670		826			174	4,175	18,787	01/NHS/PV

ESTIMATED QUANTITIES

DESIGN AGENCY



DESIGNER

ADK

REVIEWER

JPB 06-10-22

PROJECT ID

102458

SHEET

P.31

TOTAL

39

LOCATION	STATION		LENGTH	WIDTH	TYPICAL	PAVEMENT AREA	254			407	408	442		617				618	REMARKS
							IN.	PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN, (1-1/2")	PATCHING PLANED SURFACE (3%)	NON- TRACKING TACK COAT @ 0.085 GAL./S.Y.	PRIME COAT,AS PER PLAN @ 0.4 GAL./S.Y.	IN.	ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE B (447), AS PER PLAN	COMPACTED AGGREGATE WIDTH	IN.	COMPACTED AGGREGATE, AS PER PLAN	SHOULDER PREPARATION	RUMBLE STRIPS, SHOULDER (ASPHALT CONCRETE)	
								SQ YD	SQ YD	GAL	GAL		CU YD	FT.		CU YD	SQ YD	FT.	
S.R. 7 N.B.	FROM	TO	FT	FT		SQ YD													
INSIDE SHOULDER																			
	1236+00.00	1294+89.74	5889.74	1.5		982	1 ½	982	29	83		1 ½	41						
BR. NO. JEF-7-2409	OMIT(223')																		
	1297+12.74	1350+95.99	5383.25	1.5		897	1 ½	897	27	76		1 ½	37						
BR. NO. JEF-7-2516	OMIT (175')																		
	1352+70.99	1396+84.82	4413.83	1.5		736	1 ½	736	22	63		1 ½	31						
BR. NO. JEF-7-2602	OMIT (169')																		
	1398+53.82	1421+56.67	2302.85	1.5		384	1 ½	384	12	33		1 ½	16						
OUTSIDE SHOULDER																			
	1236+00.00	1294+89.74	5889.74	8		5,235	1 ½	5,235	157	445	524	1 ½	218	2	1 ½	55	1309	5890	
BR. NO. JEF-7-2409	OMIT(223')																		
	1297+14.74	1350+95.99	5381.25	8		4,783	1 ½	4,783	144	407	478	1 ½	199	2	1 ½	50	1196	5381	
BR. NO. JEF-7-2516	OMIT (175')																		
	1352+70.99	1396+84.82	4413.83	8		3,923	1 ½	3,923	118	333	392	1 ½	163	2	1 ½	41	981	4414	
BR. NO. JEF-7-2602	OMIT (169')																		
	1398+53.82	1421+56.67	2302.85	8		2,047	1 ½	2,047	61	174	205	1 ½	85	2	1 ½	21	512	2303	
TOTALS CARRIED TO GENERAL SUMMARY								18,987	570	1,614	1,599		791			167	3,997	17,988	01/NHS/PV

ESTIMATED QUANTITIES

DESIGN AGENCY



DESIGNER

ADK

REVIEWER

JPB 06-10-22

PROJECT ID

102458

SHEET

P.32

TOTAL

39

LOCATION	STATION		LENGTH	WIDTH	TYPICAL	PAVEMENT AREA	254			407	408	442		617				618	REMARKS
							IN.	PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN, (1-1/2")	PATCHING PLANED SURFACE (3%)	NON- TRACKING TACK COAT @ 0.085 GAL./S. Y.	PRIME COAT,AS PER PLAN @ 0.4 GAL./S. Y.	IN.	ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE B (447), AS PER PLAN	COMPACTED AGGREGATE WIDTH	IN.	COMPACTED AGGREGATE, AS PER PLAN	SHOULDER PREPARATION	RUMBLE STRIPS, SHOULDER (ASPHALT CONCRETE)	
S.R. 7 S.B.	FROM	TO	FT	FT		SQ YD													
INSIDE SHOULDER																			
	1236+00.00	1294+89.74	5889.74	1.5		982	1 ½	982	29	83		1 ½	41						
BR. NO. JEF-7-2409	OMIT(223')																		
	1297+12.74	1350+95.99	5383.25	1.5		897	1 ½	897	27	76		1 ½	37						
BR. NO. JEF-7-2516	OMIT (175')																		
	1352+70.99	1396+84.82	4413.83	1.5		736	1 ½	736	22	63		1 ½	31						
BR. NO. JEF-7-2602	OMIT (169')																		
	1398+53.82	1421+56.67	2302.85	1.5		384	1 ½	384	12	33		1 ½	16						
OUTSIDE SHOULDER																			
	1236+00.00	1294+89.74	5889.74	8		5,235	1 ½	5,235	157	445	524	1 ½	218	2	1 ½	55	1309	5890	
BR. NO. JEF-7-2409	OMIT(223')																		
	1297+14.74	1350+95.99	5381.25	8		4,783	1 ½	4,783	144	407	478	1 ½	199	2	1 ½	50	1196	5381	
BR. NO. JEF-7-2516	OMIT (175')																		
	1352+70.99	1396+84.82	4413.83	8		3,923	1 ½	3,923	118	333	392	1 ½	163	2	1 ½	41	981	4414	
BR. NO. JEF-7-2602	OMIT (169')																		
	1398+53.82	1421+56.67	2302.85	8		2,047	1 ½	2,047	61	174	205	1 ½	85	2	1 ½	21	512	2303	
TOTALS CARRIED TO GENERAL SUMMARY								18,987	570	1,614	1,599		791			167	3,997	17,988	01/NHS/PV

ESTIMATED QUANTITIES

DESIGN AGENCY



DESIGNER

ADK

REVIEWER

JPB 06-10-22

PROJECT ID

102458

SHEET

P.33

TOTAL

39

LOCATION	STATION		LENGTH	WIDTH	TYPICAL	PAVEMENT AREA	254			407	408	442		617			618	REMARKS
							IN.	PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN (1 1/2")	PATCHING PLANED SURFACE (3%)	NON-TRACKING TACK COAT @ 0.085 GAL./S. Y.	PRIME COAT @ 0.40 GAL./S. Y., AS PER PLAN	IN.	ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE B (447), AS PER PLAN	COMPACTED AGGREGATE WIDTH	IN.	COMPACTED AGGREGATE, AS PER PLAN	SHOULDER PREPERATION	
RAMPS	FROM	TO	FT	FT		SQ YD		SQ YD	SQ YD	GAL	GAL		CU YD	FT		CU YD	SQ YD	
C.R. 7G INTERCHANGE																		
RAMP A																		
MAINLINE	82+98.79	93+15.00	1,016.21	16.00		1,807	1 ½	1,807	54	154		1 ½	75.27					
MAINLINE	93+15.00	94+21.00	106.00	17.00		339	1 ½	339	10	29		1 ½	14.14					
LEFT SHOULDER	83+00.00	94+21.00	1,121.00	3.00		374	1 ½	374	11	32	100	1 ½	15.57	2	1 ½	10.38	249	
RIGHT SHOULDER	81+32.10	82+12.10	80.00	7.00		62	½	62	2	5	4	½	0.86	1	½	0.12	9	
RIGHT SHOULDER	82+12.10	82+98.79	86.69	6.00		58	1 ½	58	2	5	8	1 ½	2.41	2	1 ½	0.80	19	
RIGHT SHOULDER	82+98.79	95+02.68	1,203.89	6.00		803	1 ½	803	24	68	107	1 ½	33.44	2	1 ½	11.15	268	
RIGHT SHOULDER	95+09.85	95+37.57	27.72	6.00		18	2 ½	18	1	2	4	2 ½	1.28	3	2 ½	0.64	9	
RAMP B																		
MAINLINE	86+27.00	90+90.95	463.95	16.00		825	1 ½	825	25	70		1 ½	34.37					
MAINLINE	90+90.95	91+90.95	100.00	15.00		209	1 ½	209	6	18		1 ½	8.69					
MAINLINE	91+90.95	93+10.00	119.05	VARIES		404	1 ½	404	12	34		1 ½	16.85					
APPROACH	0+00.00	0+34.15	34.15	VARIES		159	1 ½	159	5	14	3	1 ½	6.63	2	1 ½	0.32	8	
LEFT SHOULDER	86+27.00	91+90.95	563.95	3.00		188	1 ½	188	6	16	50	1 ½	7.83	2	1 ½	5.22	125	
RIGHT SHOULDER	86+27.00	86+32.00	5.00	8.00		4	1 ½	4	0	0	0	1 ½	0.19	2	1 ½	0.05	1	
RIGHT SHOULDER	86+32.00	86+72.00	40.00	7.00		31	1 ½	31	1	3	4	1 ½	1.30	2	1 ½	0.37	9	
RIGHT SHOULDER	86+72.00	91+76.04	504.04	6.00		336	2 ½	336	10	29	45	1 ½	14.00	2	1 ½	4.67	112	
APP. RADIUS	91+76.04	92+29.36	53.32	6.00		36	2 ½	36	1	3	5	1 ½	1.48	2	1 ½	0.49	12	
APP. RADIUS	92+29.36	92+80.09	50.73	6.00		34	2 ½	34	1	3	5	1 ½	1.41	2	1 ½	0.47	11	
APP. RADIUS	92+80.09	93+10.00	29.91	6.00		20	2 ½	20	1	2	3	1 ½	0.83	2	1 ½	0.28	7	
RAMP C																		
MAINLINE	142+23.97	143+25.01	101.04	VARIES		1,326	1 ½	1,326	40	113		1 ½	55.26					
MAINLINE	143+25.01	153+70.88	1,045.87	16.00		1,859	1 ½	1,859	56	158		1 ½	77.47					
LEFT SHOULDER	3+78.92	143+25.01	126.35	3.00		42	1 ½	42	1	4	11	1 ½	1.75	2	1 ½	1.17	28	
LEFT SHOULDER	143+25.01	153+70.88	1,045.87	3.00		349	1 ½	349	10	30	93	1 ½	14.53	2	1 ½	9.68	232	
RIGHT SHOULDER	0+48.38	3+78.92	330.54	6.00		220	1 ½	220	7	19	29	1 ½	9.18	2	1 ½	3.06	73	
RIGHT SHOULDER	0+48.38	144+05.91	328.71	6.00		219	1 ½	219	7	19	29	1 ½	9.13	2	1 ½	3.04	73	
RIGHT SHOULDER	144+05.92	153+41.03	935.11	6.00		623	1 ½	623	19	53	83	1 ½	25.98	2	1 ½	8.66	208	
RIGHT SHOULDER	153+43.03	153+83.03	40.00	7.00		31	1 ½	31	1	3	4	1 ½	1.30	2	1 ½	0.37	9	
RAMP D																		
MAINLINE	152+00.00	156+16.19	416.19	16.00		740	1 ½	740	22	63		1 ½	30.83					
LEFT SHOULDER	152+00.00	156+16.19	416.19	3.00		139	1 ½	139	4	12	37	1 ½	5.78	2	1 ½	3.85	92	
RIGHT SHOULDER	152+00.00	156+16.19	416.19	6.00		277	1 ½	277	8	24	37	1 ½	11.56	2	1 ½	3.85	92	
RIGHT SHOULDER	156+16.19	157+70.00	153.81	6.00		103	1 ½	103	3	9	14	1 ½	4.27	2	1 ½	1.42	34	
RIGHT SHOULDER	157+70.00	158+50.00	80.00	7.00		62	2 ½	62	2	5	11	2 ½	4.32	3	2 ½	1.85	27	
C.R. 56 INTERCHANGE																		
RAMP E																		
MAINLINE	223+50.00	229+28.02	578.02	16.00		1,028	1 ½	1,028	31	87		1 ½	42.82					
MAINLINE	229+28.02	230+29.10	101.08	VARIES		357	1 ½	357	11	30		1 ½	14.89					
LEFT SHOULDER	223+50.00	229+28.02	578.02	3.00		193	1 ½	193	6	16	51	1 ½	8.03	2	1 ½	5.35	128	
LEFT SHOULDER	229+28.02	230+29.10	101.08	3.00		34	1 ½	34	1	3	9	1 ½	1.40	2	1 ½	0.94	22	
RIGHT SHOULDER	223+50.00	224+30.00	80.00	6.00		53	1 ½	53	2	5	7	1 ½	2.22	2	1 ½	0.74	18	
RIGHT SHOULDER	224+30.00	229+28.02	498.02	6.00		332	1 ½	332	10	28	66	2 ½	23.06	3	2 ½	11.53	166	
RIGHT SHOULDER	229+28.02	230+29.10	101.08	6.00		67	1 ½	67	2	6	18	3 ½	6.55	4	3 ½	4.37	45	
RAMP F																		
MAINLINE	223+44.45	228+84.91	540.46	16.00		961	1 ½	961	29	82		1 ½	40.03					
MAINLINE	228+84.91	229+22.72	37.81	VARIES		119	1 ½	119	4	10		1 ½	4.97					
LEFT SHOULDER	223+44.45	228+84.91	540.46	3.00		180	1 ½	180	5	15	48	1 ½	7.51	2	1 ½	5.00	120	
LEFT SHOULDER	228+84.91	229+22.72	37.81	3.00		13	1 ½	13	0	1	3	1 ½	0.53	2	1 ½	0.35	8	
RIGHT SHOULDER	223+44.45	223+84.45	40.00	6.00		27	1 ½	27	1	2	4	1 ½	1.11	2	1 ½	0.37	9	
RIGHT SHOULDER	223+84.45	228+84.91	500.46	6.00		334	1 ½	334	10	28	44	1 ½	13.90	2	1 ½	4.63	111	
RIGHT SHOULDER	228+84.91	229+22.72	37.81	6.00		25	1 ½	25	1	2	3	1 ½	1.05	2	1 ½	0.35	8	
RAMP G																		
MAINLINE	229+41.09	230+14.06	72.97	VARIES		312	1 ½	312	9	27		1 ½	12.99					
MAINLINE	230+14.06	234+17.21	403.15	16.00		717	1 ½	717	22	61		1 ½	29.86					
LEFT SHOULDER	229+41.09	230+14.06	72.97	3.00		24	1 ½	24	1	2	6	1 ½	1.01	2	1 ½	0.68	16	
LEFT SHOULDER	230+14.06	234+17.21	403.15	3.00		134	1 ½	134	4	11	36	1 ½	5.60	2	1 ½	3.73	90	
RIGHT SHOULDER	229+41.09	229+90.81	49.72	6.00		33	1 ½	33	1	3	4	1 ½	1.38	2	1 ½	0.46	11	
RIGHT SHOULDER	229+90.81	234+00.38	409.57	6.00		273	1 ½	273	8	23	36	1 ½	11.38	2	1 ½	3.79	91	
RIGHT SHOULDER	234+00.38	234+40.38	40.00	7.00		31	2 ½	31	1	3	5	2 ½	2.16	3	2 ½	0.93	13	
RAMP H																		
MAINLINE	229+97.14	230+82.26	85.12	VARIES		254	1 ½	254	8	22		1 ½	10.58					
MAINLINE	230+82.26	235+08.59	426.33	16.00		758	1 ½	758	23	64		1 ½	31.58					
LEFT SHOULDER	229+97.14	230+82.26	85.12	3.00		28	1 ½	28	1	2	8							

ESTIMATED QUANTITIES

DESIGN AGENCY



DESIGNER

ADK

REVIEWER

JPB 06-10-22

PROJECT ID

102458

SHEET

P.34

TOTAL

39

LOCATION	STATION		LENGTH	WIDTH	TYPICAL	PAVEMENT AREA	254			407	408	442		617			REMARKS
							IN.	PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN (1 1/2")	PATCHING PLANED SURFACE (3%)	NON-TRACKING TACK COAT @ 0.085 GAL./S.Y.	PRIME COAT @ 0.40 GAL./S.Y., AS PER PLAN	IN.	ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE B (447), AS PER PLAN	COMPACTED AGGREGATE WIDTH	IN.	COMPACTED AGGREGATE, AS PER PLAN	
	SQ YD	SQ YD	GAL	GAL		CU YD											
RAMPS	FROM	TO	FT	FT		SQ YD											
CO RD. 46 INTERCHANGE																	
RAMP A																	
MAINLINE	12+82.08	18+48.00	565.92	16.00		1,006	1 ½	1,006	30	86		1 ½	41.92				
MAINLINE	18+48.00	19+36.47	88.47	VARIES		339	1 ½	339	10	29		1 ½	14.14				
LEFT SHOULDER	12+82.08	18+48.00	565.92	3.00		189	1 ½	189	6	16	50	1 ½	7.86	2	1 ½	5.24	126
LEFT SHOULDER	18+48.00	19+36.47	88.47	3.00		29	1 ½	29	1	3	8	1 ½	1.23	2	1 ½	0.82	20
RIGHT SHOULDER	12+82.08	18+48.00	565.92	6.00		377	1 ½	377	11	32	50	1 ½	15.72	2	1 ½	5.24	126
RIGHT SHOULDER	18+48.00	19+36.47	88.47	6.00		59	1 ½	59	2	5	8	1 ½	2.46	2	1 ½	0.82	20
RAMP B																	
MAINLINE	8+30.06	15+41.66	711.60	16.00		1,265	1 ½	1,265	38	108		1 ½	52.71				
MAINLINE	15+41.66	15+94.86	53.20	VARIES		209	1 ½	209	6	18		1 ½	8.69				
LEFT SHOULDER	8+30.06	15+41.66	711.60	3.00		237	1 ½	237	7	20	63	1 ½	9.88	2	1 ½	6.59	158
LEFT SHOULDER	15+41.66	15+94.86	53.20	3.00		18	1 ½	18	1	2	5	1 ½	0.74	2	1 ½	0.49	12
RIGHT SHOULDER	7+94.93	8+44.93	50.00	7.00		39	1 ½	39	1	3	4	1 ½	1.62	2	1 ½	0.46	11
RIGHT SHOULDER	8+44.93	15+41.66	696.73	6.00		464	1 ½	464	14	39	62	1 ½	19.35	2	1 ½	6.45	155
RIGHT SHOULDER	15+41.66	15+94.86	53.20	6.00		35	1 ½	35	1	3	5	1 ½	1.48	2	1 ½	0.49	12
RAMP C																	
MAINLINE	0+24.33	0+77.44	53.11	VARIES		217	1 ½	217	7	18		1 ½	9.03				
MAINLINE	0+77.44	10+90.44	1,013.00	16.00		1,801	1 ½	1,801	54	153		1 ½	75.04				
LEFT SHOULDER	0+24.33	0+77.44	53.11	3.00		18	1 ½	18	1	2	5	1 ½	0.74	2	1 ½	0.49	12
LEFT SHOULDER	0+77.44	10+90.44	1,013.00	3.00		338	1 ½	338	10	29	90	1 ½	14.07	2	1 ½	9.38	225
RIGHT SHOULDER	0+24.33	0+77.44	53.11	6.00		35	1 ½	35	1	3	5	1 ½	1.48	2	1 ½	0.49	12
RIGHT SHOULDER	0+77.44	10+62.87	985.43	6.00		657	1 ½	657	20	56	88	1 ½	27.37	2	1 ½	9.12	219
RIGHT SHOULDER	10+62.87	11+12.87	50.00	7.00		39	1 ½	39	1	3	4	1 ½	1.62	2	1 ½	0.46	11
RAMP D																	
MAINLINE	0+24.23	1+28.50	104.27	VARIES		418	1 ½	418	13	36		1 ½	17.42				
MAINLINE	1+28.50	13+12.54	1,184.04	16.00		2,105	1 ½	2,105	63	179		1 ½	87.71				
LEFT SHOULDER	0+24.23	1+28.50	104.27	3.00		35	1 ½	35	1	3	9	1 ½	1.45	2	1 ½	0.97	23
LEFT SHOULDER	1+28.50	13+12.54	1,184.04	3.00		395	1 ½	395	12	34	105	1 ½	16.45	2	1 ½	10.96	263
RIGHT SHOULDER	0+24.23	1+28.50	104.27	6.00		70	1 ½	70	2	6	9	1 ½	2.90	2	1 ½	0.97	23
RIGHT SHOULDER	1+28.50	13+12.54	1,184.04	6.00		789	1 ½	789	24	67	105	1 ½	32.89	2	1 ½	10.96	263
ALEXANDER ST. INTERCHANGE																	
RAMP E																	
MAINLINE	1392+51.45	1396+58.61	407.16	16.00		724	1 ½	724	22	62		1 ½	30.16				
MAINLINE	1396+58.61	1397+71.58	112.97	VARIES		358	1 ½	358	11	30		1 ½	14.90				
LEFT SHOULDER	1392+51.45	1396+58.61	407.16	3.00		136	1 ½	136	4	12	36	1 ½	5.65	2	1 ½	3.77	90
LEFT SHOULDER	1396+58.61	1397+71.58	112.97	3.00		38	1 ½	38	1	3	10	1 ½	1.57	2	1 ½	1.05	25
RIGHT SHOULDER	1392+51.45	1397+71.58	520.13	6.00		347	1 ½	347	10	29	46	1 ½	14.45	2	1 ½	4.82	116
RAMP F																	
MAINLINE	1389+82.38	1396+89.80	707.42	16.00		1,258	1 ½	1,258	38	107		1 ½	52.40				
MAINLINE	1396+89.80	1397+52.62	62.82	VARIES		289	1 ½	289	9	25		1 ½	12.05				
LEFT SHOULDER	1389+82.38	1396+89.80	707.42	3.00		236	1 ½	236	7	20	63	1 ½	9.83	2	1 ½	6.55	157
LEFT SHOULDER	1396+89.80	1397+52.62	62.82	3.00		21	1 ½	21	1	2	6	1 ½	0.87	2	1 ½	0.58	14
RIGHT SHOULDER	1389+59.30	1390+09.30	50.00	7.00		39	1 ½	39	1	3	4	1 ½	1.62	2	1 ½	0.46	11
RIGHT SHOULDER	1390+09.30	1396+89.80	680.50	6.00		454	1 ½	454	14	39	60	1 ½	18.90	2	1 ½	6.30	151
RIGHT SHOULDER	1396+89.80	1397+52.62	62.82	6.00		42	1 ½	42	1	4	6	1 ½	1.75	2	1 ½	0.58	14
RAMP G																	
MAINLINE	1398+04.31	1398+30.66	26.35	VARIES		70	1 ½	70	2	6		1 ½	2.93				
MAINLINE	1398+30.66	1408+27.73	997.07	16.00		1,773	1 ½	1,773	53	151		1 ½	73.86				
LEFT SHOULDER	1398+04.31	1398+30.66	26.35	3.00		9	1 ½	9	0	1	2	1 ½	0.37	2	1 ½	0.24	6
LEFT SHOULDER	1398+30.66	1408+27.73	997.07	3.00		332	1 ½	332	10	28	89	1 ½	13.85	2	1 ½	9.23	222
RIGHT SHOULDER	1398+04.31	1408+13.65	1,009.34	6.00		673	1 ½	673	20	57	90	1 ½	28.04	2	1 ½	9.35	224
RIGHT SHOULDER	1408+13.65	1408+63.65	50.00	8.00		44	1 ½	44	1	4	4	1 ½	1.85	2	1 ½	0.46	11
RAMP H																	
MAINLINE	1397+10.36	1398+11.21	100.85	VARIES		357	1 ½	357	11	30		1 ½	14.88				
MAINLINE	1398+11.21	1408+69.27	1,058.06	16.00		1,881	1 ½	1,881	56	160		1 ½	78.37				
LEFT SHOULDER	1397+10.36	1398+11.21	100.85	3.00		34	1 ½	34	1	3	9	1 ½	1.40	2	1 ½	0.93	22
LEFT SHOULDER	1398+11.21	1408+69.27	1,058.06	3.00		353	1 ½	353	11	30	94	1 ½	14.70	2	1 ½	9.80	235
RIGHT SHOULDER	1397+10.36	1398+11.21	100.85	6.00		67	1 ½	67	2	6	9	1 ½	2.80	2	1 ½	0.93	22
RIGHT SHOULDER	1398+11.21	1408+69.27	1,058.06	6.00		705	1 ½	705	21	60	94	1 ½	29.39	2	1 ½	9.80	2

ESTIMATED QUANTITIES

DESIGN AGENCY

DESIGNER

ADK

REVIEWER

JPB 06-10-22

PROJECT ID

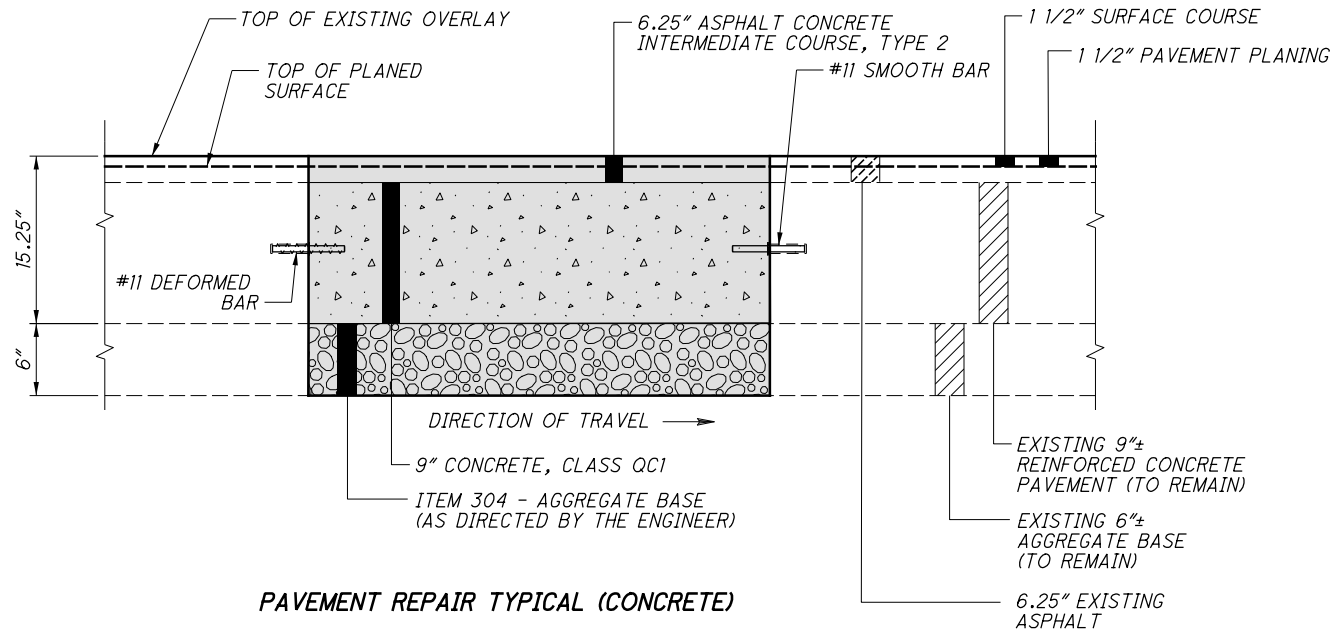
102458

SHEET

P.35

TOTAL

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PAVEMENT REPAIR TYPICAL (CONCRETE)

FOR DETAILS NOT SHOWN SEE STANDARD CONSTRUCTION DRAWING BP-2.5
FOR TRANSVERSE JOINT REPAIR DETAILS AND BP-2.1 FOR LONGITUDINAL JOINT DETAILS

ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC1, AS PER PLAN
(SLM 22.97 TO SLM 26.49 ONLY)

THE ESTIMATED QUANTITIES ARE TO BE CONSIDERED APPROXIMATE. A FINAL FIELD REVIEW WILL BE PERFORMED BY ODOT PRIOR TO CONSTRUCTION AND FINAL LOCATIONS WILL BE GIVEN TO THE CONTRACTOR PRIOR TO CONSTRUCTION.

THIS WORK CONSISTS OF REMOVING THE EXISTING ASPHALT CONCRETE, REINFORCED CONCRETE, AND THE AGGREGATE BASE COURSES; SHAPING AND COMPACTING THE EXPOSED MATERIAL; PLACING ITEM 304 AGGREGATE BASE; THEN INSTALLING DOWEL RODS FOLLOWED BY CONCRETE PAVEMENT, CLASS QC1. POUR CONCRETE TO MATCH THE CONCRETE BASE, PAVE WITH ASPHALT INTERMEDIATE TO THE EXISTING SURFACE, AND MILL 1.5" OF ASPHALT WHEN MILLING THE ADJACENT ASPHALT PAVEMENT. THIS WORK SHALL BE COMPLETED BEFORE MILLING AND RESURFACING BEGINS.

ALL OTHER PROVISIONS OF STANDARD CONSTRUCTION DRAWINGS BP-2.1 AND BP-2.5 APPLY.

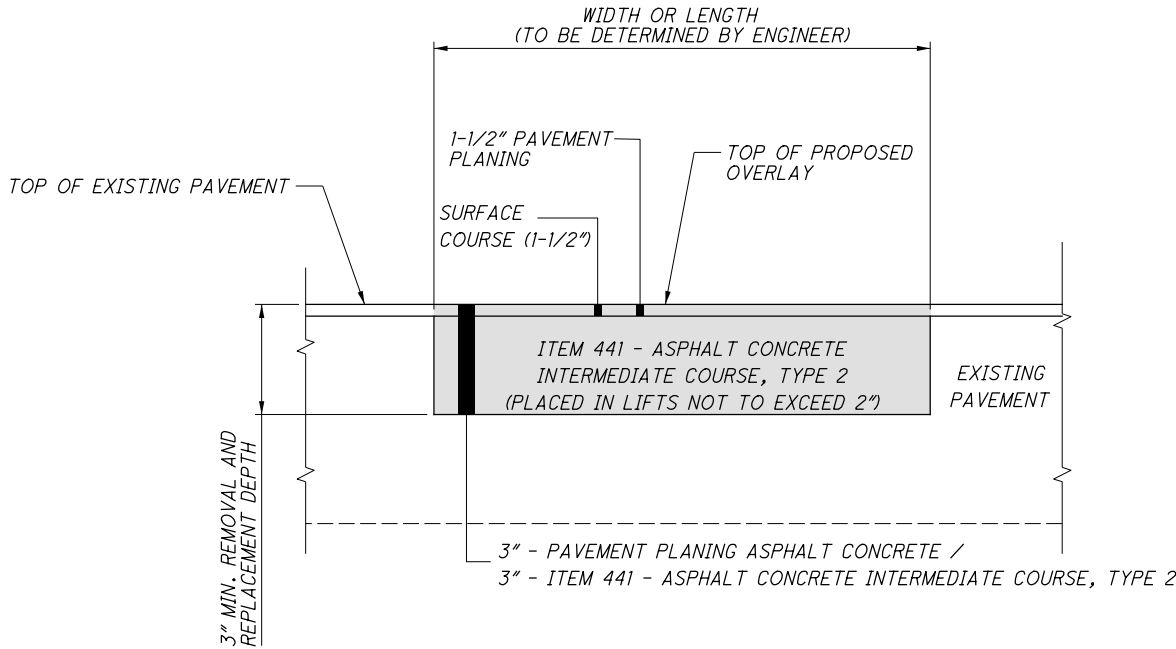
THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN PROVIDED FOR INFORMATION ONLY.

9" CONCRETE, QC1	75 CU. YD.
ITEM 304 - AGGREGATE BASE	38 CU. YD.
ITEM 441 - ASPHALT CONCRETE INTERMEDIATE COURSE	52 CU. YD.
ITEM 509 - EPOXY COATED REINFORCING	1,984 POUNDS
ITEM 510 - DOWEL HOLES WITH NONSHRINK, NONMETALLIC GROUT	320 EACH

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER. FINAL PAYMENT FOR THESE ITEMS SHALL BE FOR THE ACCEPTED QUANTITY COMPLETED IN PLACE.

(SLM 22.97 TO SLM 26.49 ONLY)
ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC1, AS PER PLAN - 300 SQ. YD. (01/NHS/PV)

ITEM 255 - FULL DEPTH PAVEMENT SAWING ----- 1,000 FT. (01/NHS/PV)



PARTIAL DEPTH PAVEMENT REPAIR TYPICAL

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (441)

THIS ITEM OF WORK SHALL ADHERE TO THE REQUIREMENTS DETAILED IN CMS 251 USING ITEM 441 - ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2 FOR THE PAVEMENT LIFTS.

ODOT SHALL PERFORM A FINAL FIELD REVIEW PRIOR TO CONSTRUCTION AND AT THAT TIME, THE ENGINEER WILL DETERMINE AND MARK THE SIZE AND LOCATION FOR EACH REPAIR AND PROVIDE THAT INFORMATION TO THE CONTRACTOR. FINAL PAYMENT FOR THESE LOCATIONS SHALL BE FOR THE ACCEPTED QUANTITY COMPLETED IN PLACE.

ALL PARTIAL DEPTH REPAIRS SHALL BE COMPLETED PRIOR TO COMMENCING PAVING OPERATIONS.

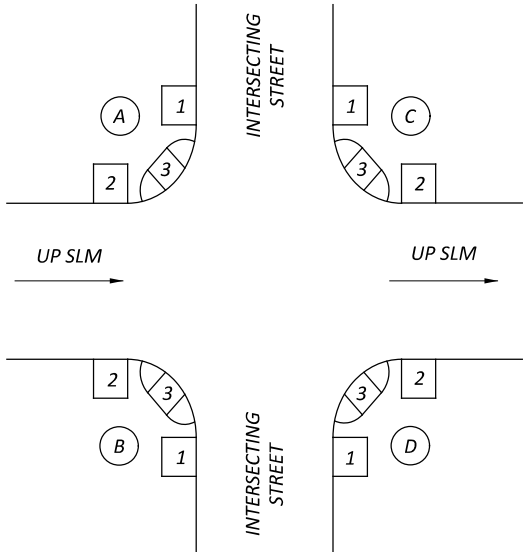
PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS SHALL BE INCLUDED IN THE CONTRACT PRICE FOR ITEM 251 PARTIAL DEPTH REPAIR (441).

ESTIMATED QUANTITIES

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (441) - 100 CU YD
TOTAL USE: 100 CU YD (01/NHS/PV)

REF. NO.	PART	COUNTY	ROUTE	SLM	INTERSECTING PUBLIC ROAD	LOCATION (SEE LEGEND)	CURB RAMP TYPE	DIMENSION A, OR LANDING WIDTH	DIMENSION B	AVG. WIDTH OF SIDEWALK OR LANDING	202		608		659	FUNDING	REMARKS
											WALK REMOVED		CURB RAMP		SEEDING MISC.: CURB RAMP GRADING RESTORATION		
								FT	FT	FT	SQ FT		SQ FT		SQ FT		
CR-1	1	JEF	C.R. 46		RAMP 'C'	A1	D11-A	8		4	32		32		8		
CR-2	1	JEF	C.R. 46		RAMP 'C'	C1	D11-A	7		4	28		28		7		
CR-3	1	JEF	C.R. 46		RAMP 'D'	A1	D11-C	7.5	5.5	4	26		26		7.5		
CR-4	1	JEF	C.R. 46		RAMP 'D'	C1	D11-A	6		4	24		24		6		
CONVERT SQ. FT. TO SQ. YD.															28.5		
															SQ YD		
TOTALS CARRIED TO GENERAL SUMMARY											110		110		3		

CURB RAMP DETAILS

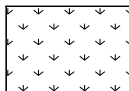


RAMP LOCATION
DETAIL

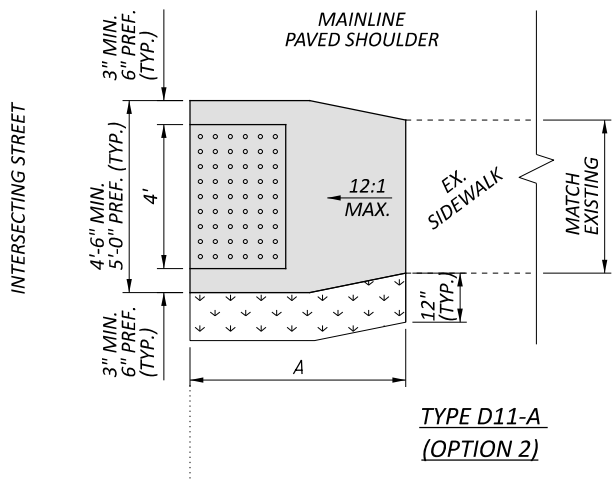
LEGEND



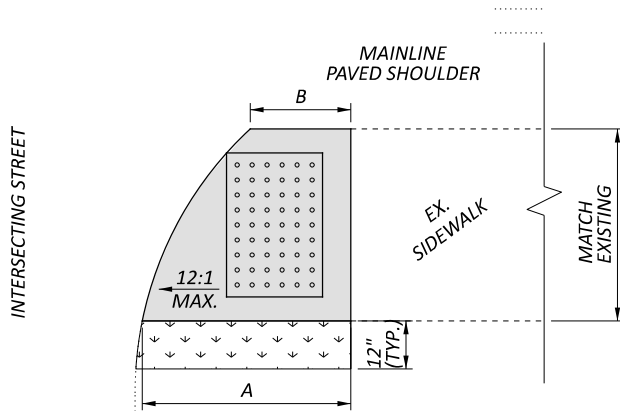
LIMITS OF PAYMENT FOR
ITEM 608 - CURB RAMP, OR
ITEM 608 - CURB RAMP, AS PER PLAN
(TYP. ALL DETAIL SHEETS)



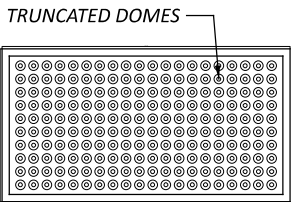
LIMITS OF PAYMENT FOR ITEM 659 - SEEDING
MISC.: CURB RAMP GRADING RESTORATION



TYPE D11-A
(OPTION 2)



TYPE D11-C



DETECTABLE WARNING INSERT DETAIL

ITEM 659 - SEEDING MISC.: CURB RAMP GRADING RESTORATION

THIS ITEM OF WORK CONSISTS OF REWORKING, OR RESHAPING THE GRADING ADJACENT TO THE NEW CURB RAMPS AND/OR WALK. THE CONTRACTOR SHALL SEED AND MULCH AS PER ITEM 659, AND PROVIDE ALL ADDITIONAL MATERIALS AND EQUIPMENT NECESSARY TO RESTORE THE GRADING TO THE SATISFACTION OF THE ENGINEER.

A QUANTITY OF ONE SQUARE FOOT PER LINEAR FOOT, PER SIDE WITH ACCOMPANYING GRADED BORDER, OF NEW RAMP, AND/OR WALK SHALL BE CALCULATED FOR THIS ITEM OF WORK. FINAL CONVERSION OF QUANTITIES FROM SQUARE FOOT TO SQUARE YARDS SHALL BE PERFORMED IN THE SUMMARY LEVEL. PAYMENT FOR THE AFOREMENTIONED WORK SHALL BE MADE AT THE UNIT PRICE BID FOR ITEM 659, SEEDING MISC.: CURB RAMP GRADING RESTORATION, SQ. YD., AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE ALL WORK TO THE SATISFACTION OF THE ENGINEER.

NOTES:

- SEE SCD BP-7.1 FOR ADDITIONAL DETAILS, SECTIONS, NOTES, AND OTHER CURB RAMP DESIGNS NOT PROVIDED ON THIS SHEET.
- THE NEW CURB RAMP SHALL BE 4'-6" MINIMUM (5'-0" PREFERRED) WIDE. THE DETECTABLE WARNING INSERT SHALL HAVE A THREE INCH MINIMUM BORDER ON THE SIDES. IF THE EXISTING SIDEWALK IS LESS THAN 4'-6" WIDE THE RAMP SHALL TAPER TO MEET THE EXISTING SIDEWALK WIDTH BEGINNING A MINIMUM OF SIX INCHES BEYOND THE BACKSIDE OF THE DETECTABLE WARNING INSERT AS DETAILED IN TYPE D11-A, OPTION 2.

DESIGN AGENCY



DESIGNER

DAH

REVIEWER

JPB 06-10-22

PROJECT ID

102458

SHEET

P.37

TOTAL

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TRAFFIC CONTROL SUBSUMMARY																														
PART	SHEET NO.	COUNTY	ROUTE	STATION TO STATION		LANE OR RAMP	SIDE OF LANE OR RAMP	621				RAISED PAVEMENT MARKER REMOVED	CENTER LINE	STOP LINE	646					807					850			FUNDING		
								RPM							CHEVRON MARKING	LANE ARROW	WRONG WAY ARROW	ISLAND MARKING	WET REFLECTIVE EPOXY PAVEMENT MARKING, EDGE LINE 6" (YELLOW)	WET REFLECTIVE EPOXY PAVEMENT MARKING, EDGE LINE 6" (WHITE)	WET REFLECTIVE EPOXY PAVEMENT MARKING, LANE LINE, 6"	WET REFLECTIVE EPOXY PAVEMENT MARKING, CHANNELIZING LINE 12"	WET REFLECTIVE EPOXY PAVEMENT MARKING, DOTTED LINE 6" (WHITE)	GROOVING FOR 6" RECESSED PAVEMENT MARKING (ASPHALT)	GROOVING FOR 6" RECESSED PAVEMENT MARKING (ASPHALT)	GROOVING FOR 12" RECESSED PAVEMENT MARKING (ASPHALT)				
								SPACING	2-WAY	2-WAY	1-WAY																			
				WHITE/RED	YELLOW/RED				WHITE																					
				FROM	TO					FT.	EACH	EACH	EACH	EACH	MILE	FT.	FT.	EACH	EACH	SQ FT	MILE	MILE	MILE	FT.	FT.	MILE	FT.	FT		
1		JEF	S.R. 7	60+30.00	258+26.12	NORTH BOUND	LT. LANE	120			166	166							3.75						3.75					
							RT.															3.75			3.75					
1		JEF	S.R. 7	CR 7G INTERCHANGE		RAMP B	LT.	80		8		8						0.11					614	0.11	614					
							GORE	40	9			9											343			0.13			343	
							RT.																			0.13				
							CTR.						0.05				2										0.13			
1		JEF	S.R. 7	CR 7G INTERCHANGE		RAMP D	LT.	80		6		6						0.08					880	0.08	880					
							GORE	40	9			9											710			0.08			710	
							RT.																			0.08				
							CTR.																							
1		JEF	S.R. 7	CR 56 CONNECTOR INTERCHANGE		RAMP F	LT.	80		8		8						0.11					587	0.11	587					
							GORE	40	11			11											425			0.11			425	
							RT.																			0.11				
							CTR.							18			2													
1		JEF	S.R. 7	CR 56 CONNECTOR INTERCHANGE		RAMP H	LT.	80		7		7						0.1					880	0.10	880					
							GORE	40	8			8											624			0.10			624	
							RT.																			0.10				
							CTR.																							
1		JEF	S.R. 7	60+30.00	258+26.12	SOUTH BOUND	LT. LANE	120			166	166							3.75						3.75					
							RT.																		3.75					
1		JEF	S.R. 7	CR 7G INTERCHANGE		RAMP A	LT.	80		15		15						0.21					880	0.21	880					
							GORE	40	8			8											576			0.23			576	
							RT.																			0.23				
							CTR.																							
1		JEF	S.R. 7	CR 7G INTERCHANGE		RAMP C	LT.	80		15		15						0.22					645	0.22	645					
							GORE	40	9			9											350			0.31			350	
							RT.																			0.31				
							CTR.						0.02				2	110												
1		JEF	S.R. 7	CR 7G INTERCHANGE		RAMP C CONNECTOR	CTR.						0.02																	
							RT.							15		1														
1		JEF	S.R. 7	CR 56 CONNECTOR INTERCHANGE		RAMP E	LT.	80		9		9						0.13					880	0.13	880					
							GORE	40	3			3											240			0.14			240	
							RT.																			0.14				
							CTR.																							
1		JEF	S.R. 7	CR 56 CONNECTOR INTERCHANGE		RAMP G	LT.	80		7		7						0.09					656	0.09	656					
							GORE	40	9			9											278			0.10			278	
							RT.																			0.10				
							CTR.							40			2													
SUB-TOTALS: (CARRIED TO SHEET 38)									473			473	0.09	73		1	8	110	17.25		7.50	3546	6022	24.75	6022	3546	01/NHS/PV			

FOR PAVEMENT MARKING DETAILS, SEE EXISTING PLANS, AND STANDARD DRAWINGS TC - 71.10 AND TC - 72.20

TRAFFIC CONTROL SUB-SUMMARY

DESIGN AGENCY



DESIGNER

DAH

REVIEWER

JPB 06-10-22

PROJECT ID

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SHEET

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TOTAL

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TRAFFIC CONTROL SUBSUMMARY																													
PART	SHEET NO.	COUNTY	ROUTE	STATION TO STATION		LANE OR RAMP	SIDE OF LANE OR RAMP	621					646							807						850			FUNDING
								RPM				RAISED PAVEMENT MARKER REMOVED	CENTER LINE	STOP LINE	CHEVERON MARKINGS	LANE ARROW	WRONG WAY ARROW	ISLAND MARKING	WET REFLECTIVE EPOXY PAVEMENT MARKING, EDGE LINE 6" (YELLOW)	WET REFLECTIVE EPOXY PAVEMENT MARKING, EDGE LINE 6" (WHITE)	WET REFLECTIVE EPOXY PAVEMENT MARKING, LANE LINE, 6"	WET REFLECTIVE EPOXY PAVEMENT MARKING, CHANNELIZING LINE 12"	WET REFLECTIVE EPOXY PAVEMENT MARKING, DOTTED LINE 6" (WHITE)	GROOVING FOR 6" RECESSED PAVEMENT MARKING (ASPHALT)	GROOVING FOR 6" RECESSED PAVEMENT MARKING (ASPHALT)	GROOVING FOR 12" RECESSED PAVEMENT MARKING (ASPHALT)			
								SPACING	2-WAY	2-WAY	1-WAY																		
				WHITE/RED	YELLOW/RED				WHITE																				
				FROM	TO			FT.	EACH	EACH	EACH	EACH	MILE	FT.		EACH	EACH	SQ FT	MILE	MILE	MILE	FT.	FT.	MILE	FT.	FT			
1		JEF	S.R. 7	1236+00.00	1421+56.67	NORTH BOUND	LT. LANE	120			156	156							3.51			3.51			3.51				
							RT.													3.51				3.51					
1		JEF	S.R. 7	CR 46 INTERCHANGE		RAMP B	LT.	80		10		10							0.14				627	0.14	627				
							GORE	40	11			11										412				412			
							RT.													0.16				0.16					
							CTR.							60				2											
1		JEF	S.R. 7	CR 46 INTERCHANGE		RAMP D	LT.	80		17		17							0.25					849	0.25	849			
							GORE	40	10			10										377				377			
							RT.													0.26				0.26					
1		JEF	S.R. 7	ALEXANDER ST.		RAMP F	LT.	80		10		10							0.15					644	0.15	644			
							GORE	40	9			9										353				353			
							RT.													0.16				0.16					
							CTR.																						
1		JEF	S.R. 7	ALEXANDER ST.		RAMP H	LT.	80		15		15			80				2					748	0.22	748			
							GORE	40	13			13										480				480			
							RT.													0.23				0.23					
1		JEF	S.R. 7	1236+00.00	1421+56.67	SOUTH BOUND	LT. LANE	120			156	156							3.51			3.51			3.51				
							RT.													3.51				3.51					
1		JEF	S.R. 7	1343+00.00	1389+29.52	SOUTH BOUND	LANE	120			40	40										0.88			0.88				
							LT.	80		9		9							0.12				890	0.12	890				
1		JEF	S.R. 7	CR 46 INTERCHANGE		RAMP A	GORE	40	10			10										394				394			
							RT.													0.14				0.14					
1		JEF	S.R. 7	CR 46 INTERCHANGE		RAMP C	LT.	80		14		14							0.2					690	0.20	690			
							GORE	40	7			7										322				322			
							RT.													0.22				0.22					
							CTR.							60				2											
1		JEF	S.R. 7	ALEXANDER ST.		RAMP E	LT.	80		7		7							0.1					400	0.10	400			
							GORE	40	9			9										324				324			
							RT.													0.10				0.10					
1		JEF	S.R. 7	ALEXANDER ST.		RAMP G	LT.	80		13		13							0.2					595	0.20	595			
							GORE	40	11			11										478				478			
							RT.													0.20				0.20					
							CTR.							20				2											
SUB TOTALS (FROM THIS SHEET):									527			527	0	220		0	8	0	16.89		7.90	3140	5443	24.79	5443	3140	01/NHS/PV		
SUB TOTALS (FROM SHEET 37):									473			473	0.09	73		1	8	110	17.25		7.50	3546	6022	24.75	6022	3546	01/NHS/PV		
TOTALS CARRIED TO GENERAL SUMMARY									1,000			1,000	0.09	293		1	16	110	34.14		15.40	6,686	11,465	49.54	11,465	6,686	01/NHS/PV		

TRAFFIC CONTROL SUB-SUMMARY

DESIGN AGENCY



DESIGNER

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REVIEWER

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FOR PAVEMENT MARKING DETAILS, SEE EXISTING PLANS, AND STANDARD DRAWINGS TC - 71.10 AND TC - 72.20.