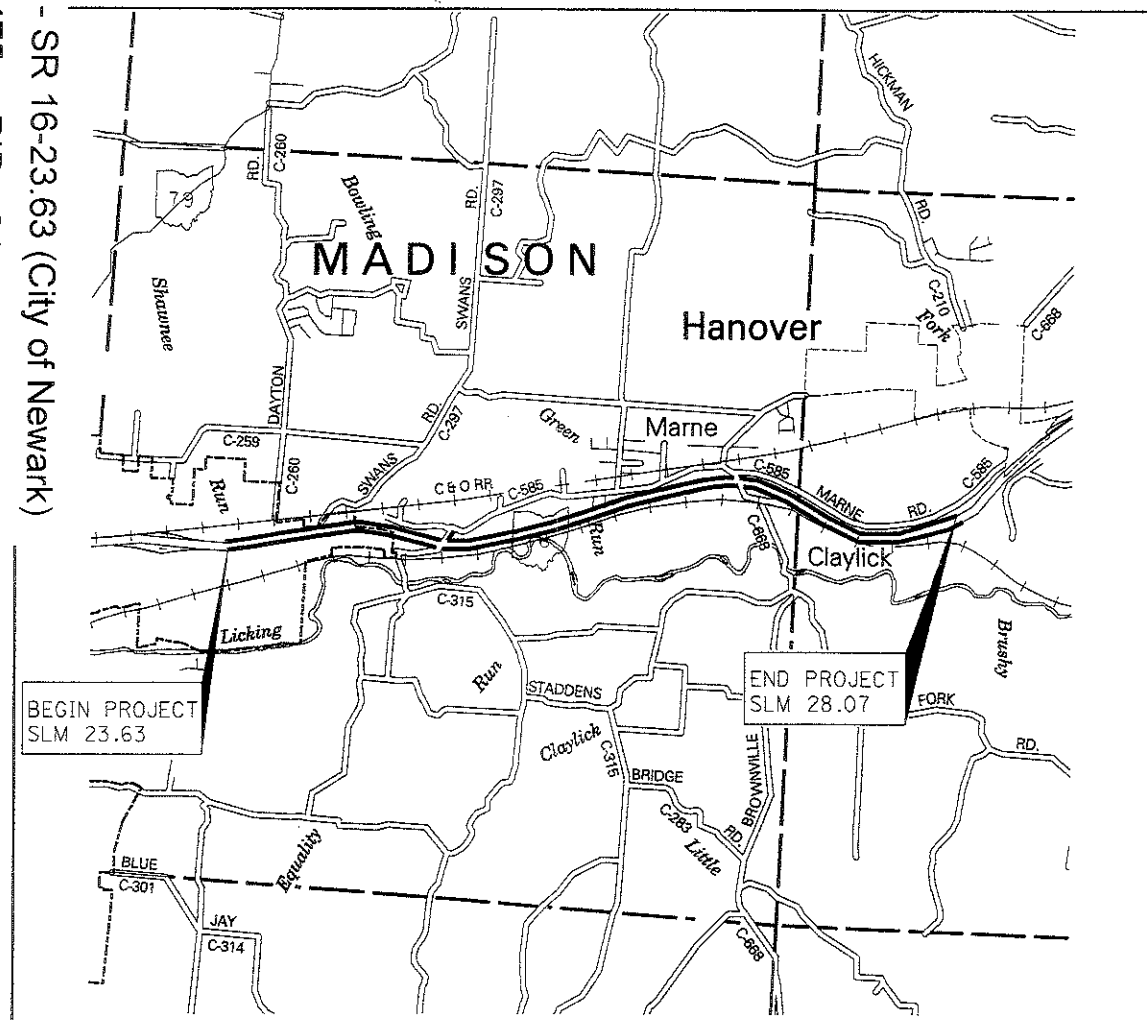


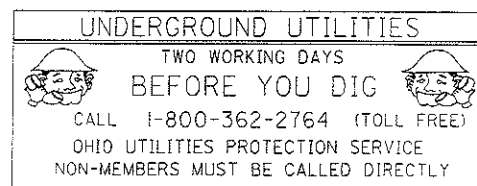
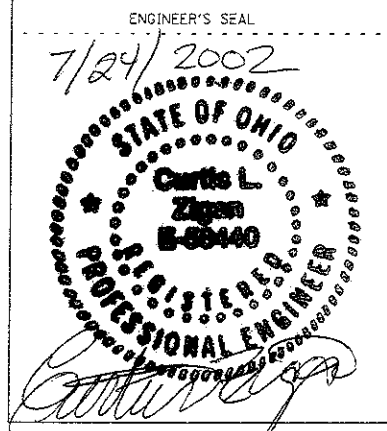
LOCATION MAP



PORTION TO BE IMPROVED

DESIGN EXCEPTIONS: NONE

DESIGN DESIGNATION	
Current ADT (2002)	19300
Design Year ADT (2014)	23400
Design Hourly Volume (2014)	2340
Directional Distribution	50%
Trucks (24 Hour B&C)	15%
Design Speed	55 mph
Legal Speed	55 mph



STANDARD DRAWINGS		STANDARD DRAWINGS		SUPPLEMENTAL SPECIFICATIONS	
BP-2.1	7-28-00	MT-99.20M	1-30-95		
BP-2.2	7-28-00	TC-65.10	10-19-01	908	4-19-02
BP-3.1	7-28-00	TC-65.11	10-19-01		
BP-4.1	7-28-00	TC-65.12	10-19-01		
MT-35.10	4-20-01	TC-71.10	4-19-02		
MT-95.30	4-19-02	TC-73.10	1-19-01		
MT-95.40	4-19-02				
MT-99.10M	1-30-95				

OHIO DEPARTMENT OF TRANSPORTATION

LIC-16-23.63

LICKING COUNTY

MADISON/HANOVER TOWNSHIP

PROJECT DESCRIPTION:

4-LANE ASPHALT CONCRETE RESURFACING AND RELATED WORK

LOCATION	COUNTY	ROUTE	SECTIONS	PROJECT TERMINI		NET LENGTH MILES	CITY
				BEGIN	END		
I	LIC	SR 16	(23.63-24.27)	23.63	28.07	4.44	NEWARK



INDEX OF SHEETS:

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2002 SPECIFICATIONS

THE STANDARD 2002 SPECIFICATIONS OF THE STATE OF OHIO DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND THE PROPOSAL SHALL GOVERN THESE IMPROVEMENTS.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THESE IMPROVEMENTS WILL NOT REQUIRE THE CLOSING OF THE HIGHWAY AND PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS INDICATED IN THE PROPOSAL.

APPROVED Christopher T. Enloe
DATE 7/26/02 DISTRICT DEPUTY DIRECTOR

APPROVED *Gordon Foster JR*
DATE 8-7-02 DIRECTOR, DEPARTMENT OF
TRANSPORTATION

PLAN PREPARED BY:
District
D5
Production

UTILITIES

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT SHOULD NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER OR ADJACENT TO THE WORK AREA. BELOW IS A LIST OF UTILITIES LOCATED WITHIN THE WORK AREA AND IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT OWNERS AND VERIFY LOCATIONS

AEP (OHIO POWER CO.)
850 TECH CENTER DRIVE
GAHANNA, OH. 43230-6605
PHONE:(614)883-6829
ATTN: RICK ECKLE

COLUMBIA GAS OF OHIO, INC.
2429 LINDEN AVE.
P.O. BOX 310
ZANESVILLE, OH. 43701
PHONE: (740)452-5467 EXT. 105
ATTN: JIM DEITRICK

ALLTEL OHIO, INC.
66 NORTH FOURTH ST.
P.O. BOX 3005
NEWARK, OH. 43058-3005
PHONE: (740)3498827
ATTN: CHARLIE WADE

NATIONAL GAS & OIL CORP.
1500 GRANVILLE RD.
P.O. BOX 4970
NEWARK, OH. 43055
PHONE: (740)344-2102 EXT. 1254
ATTN: GREG WILSON

NOTIFICATION OF ROAD CLOSURE OR RESTRICTION

IN ORDER FOR ODOT TO PROPERLY PERMIT OVERSIZE LOADS, PREPARE PROPER SIGNING WHEN REQUIRED AND FURTHER TO NOTIFY THE GENERAL MOTORING PUBLIC, THE CONTRACTOR SHALL NOTIFY (IN WRITING) THE DISTRICT 5 HIGHWAY MANAGEMENT ADMINISTRATOR WITH COPIES FOR THE DISTRICT 5 ROADWAY SERVICES MANAGER AND PROJECT ENGINEER NOT LESS THAN 21 DAYS BEFORE SUCH CLOSURE OR LANE RESTRICTIONS.

SEND NOTIFICATION TO:

DISTRICT 5 HIGHWAY MANAGEMENT ADMINISTRATOR
P.O. BOX 306
JACKSONTOWN, OH. 43030
PHONE: (740) 323-4400 EXT. 5241

CONTINGENCY QUANTITIES

THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED "AS DIRECTED BY THE ENGINEER" UNLESS AUTHORIZED BY THE ENGINEER. THE ACTUAL WORK LOCATIONS AND QUANTITIES USED FOR SUCH ITEMS SHALL BE INCORPORATED INTO THE FINAL CHANGE ORDER GOVERNING COMPLETION OF THIS PROJECT.

FEATHERING

FEATHERING OF THE ASPHALT CONCRETE WHERE REQUIRED SHALL BE ACCORDING TO DRAWING BP-3.1, 7-28-00.

ITEM 617, COMPACTED AGGREGATE, AS PER PLAN

THIS ITEM SHALL BE USED IN CONJUNCTION WITH SHOULDER PREPARATION ITEM TO RESTORE ROADWAY SHOULDERS. THE CONTRACTOR SHALL PLACE RECYCLED ASPHALT CONCRETE PAVEMENT. THE QUANTITIES SHOWN ON GENERAL SUMMARY ARE CONTINGENCY QUANTITIES AND SHALL BE USED AS DIRECTED BY THE ENGINEER TO MINIMIZE SHOULDER DROP-OFFS AFTER THE PAVING OPERATION. ALL GRADATION REQUIREMENTS OF 703.18 SHALL BE WAIVED. THE MAXIMUM SIZE OF CRUSHED MATERIAL SHALL BE NO GREATER THAN 1".

PROFILE AND ALIGNMENT

THE PROPOSED PAVEMENT RESURFACING SHALL FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT.

TACK COAT

THE RATE OF APPLICATION OF THE 407 TACK COAT SHALL BE SUBJECT TO ADJUSTMENT AS DIRECTED BY THE ENGINEER. PLAN QUANTITIES INDICATE AN AVERAGE APPLICATION RATE OF 0.075 GALLONS PER SQUARE YARD FOR ESTIMATING PURPOSES ONLY.

CONVERSION OF STANDARD CONSTRUCTION DRAWINGS

THE METRIC STANDARD DRAWINGS REFERENCED IN THIS PLAN SHALL BE CONVERTED TO ENGLISH UNITS USING THE SI (METRIC) TO ENGLISH CONVERSION FACTORS PROVIDED IN SECTION 109.02 OF THE 2002 CONSTRUCTION AND MATERIALS SPECIFICATIONS. THE APPENDIX OF IEEE/ASTM SI 10 SHALL BE UTILIZED FOR ANY ADDITIONAL CONVERSION FACTORS REQUIRED. CONVERSIONS SHALL BE APPROPRIATELY PRECISE AND SHALL REFLECT STANDARD INDUSTRY ENGLISH VALUES WHERE SUITABLE.

ITEM 614 WORK ZONE MARKING SIGNS

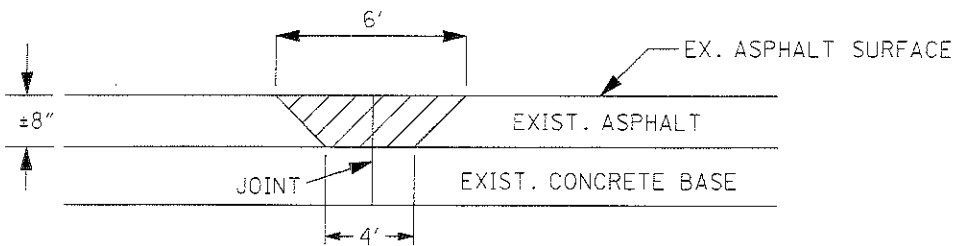
A QUANTITY OF WORK ZONE MARKING SIGNS HAS BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER.

WORK ZONE MARKING SIGNS	
OW-167 (NO EDGE LINES)	5
OW-128 (BEGIN ROAD CONSTRUCTION AHEAD)	6
OC-8 (END ROAD CONSTRUCTION)	6
TOTAL	17

ITEM 202 WEARING COURSE REMOVED, AS PER PLAN

THIS ITEM SHALL BE USED TO REMOVE THE ASPHALT OVER JOINTS FROM SLM 23.63-28.07 WHERE THE ROADWAY HAS AN EXISTING CONCRETE BASE. AN ESTIMATION OF 40 JOINTS SHALL BE REPAIRED BY MILLING DOWN TO THE CONCRETE BASE AND PLACING ITEM 301 ASPHALT CONCRETE BASE AS PER DETAIL BELOW. TACK COAT FOR FACE OF TRENCH SHALL BE INCLUDED FOR PAYMENT WITH ITEM 301. THIS WORK SHALL BE PERFORMED PRIOR TO PLANING THE ROADWAY. THE FOLLOWING QUANTITIES ARE CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER:

ITEM 202 WEARING COURSE REMOVED, AS PER PLAN 640 SQ.YD.
ITEM 301 ASPHALT COCNRETE BASE, PG 64-22 71 CU.YD.

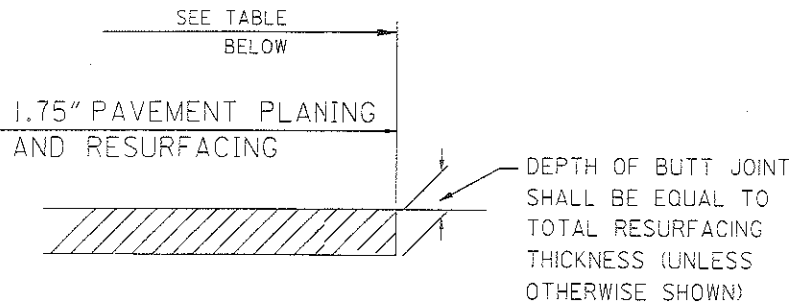


ITEM 451 REINFORCED CONCRETE PAVEMENT

ALL CONTRACTION AND EXPANSION JOINTS IN PROPOSED CONCRETE PAVEMENT SHALL BE PLACED TO MATCH EXISTING JOINTS IN ADJACENT CONCRETE TURN LANES (SEE DETAIL SHEETS 18 & 19). PROPOSED JOINTS SHALL BE AS PER STANDARD CONSTRUCTION DRAWINGS BP-2.1, 7-28-00 AND BP-2.2, 7-28-00.

BUTT JOINT

A BUTT JOINT WILL BE REQUIRED AT LOCATIONS SPECIFIED BELOW AND AT EXTRA AREAS WITH WEARING COURSE REMOVED. THE JOINT SHALL BE AS PER STANDARD CONSTRUCTION DRAWING BP-3.1, 4-19-C2.



ROUTE	DESCRIPTION	SLM
SR 16	EASTBOUND	
	BEGIN WORK	23.76
	LIC-16-2591 APPROACH SLABS	25.91
SR 16	END WORK	28.07
	WESTBOUND	
	BEGIN WORK	23.63
	BEGIN PROPOSED CONCRETE	23.72
	END PROPOSED CONCRETE	23.86
	LIC-16-259 BRIDGE DECK	25.91
	END WORK	28.07

202 PAVEMENT REMOVED, AS PER PLAN

THIS ITEM SHALL BE USED TO REMOVE PAVEMENT BETWEEN SLM 23.72 AND 23.86 WESTBOUND FOR PLACING PROPOSED 451 REINFORCED CONCRETE PAVEMENT. EXISTING BUILD-UP CONSIST OF 8" CONTINUOUSLY REINFORCED CONCRETE AND ±8" ASPHALT CONCRETE. AREA TO BE REMOVED SHALL BE SAW CUT FULL DEPTH AND REMOVED BY A LIFTING METHOD APPROVED BY THE ENGINEER SO AS NOT TO DAMAGE EXISTING SUBGRADE MATERIAL. CARE SHALL BE TAKEN NOT TO DAMAGE ADJACENT CONCRETE PAVEMENT. DEPTH OF REMOVAL SHALL BE 18". ALL MATERIALS, LABOR, EQUIPMENT AND INCIDENTALS NEEDED TO COMPLETE THE WORK DESCRIBED ABOVE SHALL BE INCLUDED FOR PAYMENT.

QUANTITES FOR REMOVAL SHOWN ON SHEETS 15 & 16.

SHOULDER RESTORATION

IN ORDER TO PROVIDE POSITIVE DRAINAGE FROM THE ROADWAY SURFACE TO THE SHOULDER BREAK, THE EXISTING ROADWAY SHOULDERS SHALL BE GRADED AND SHAPED USING A GRADER OF ADEQUATE SIZE TO PERFORM THE WORK TO THE SATISFACTION OF THE ENGINEER. PAYMENT FOR ALL OF THE ABOVE GRADING AND SHAPING WORK, INCLUDING LABOR AND INCIDENTALS, SHALL BE THE UNIT PRICE BID FOR ITEM SPECIAL - GRADER RENTAL, AND SHALL BE PAID FOR THE ACTUAL NUMBER OF GRADER HOURS WORKED.

ALL EXCESS MATERIAL REMAINING AROUND GUARDRAIL AND OTHER AREAS AFTER THE GRADER WORK IS COMPLETED AND NOT DISPOSED OF ON THE SITE, SHALL BE REMOVED AND DISPOSED OF BY THE CONTRACTOR. PAYMENT FOR ALL OF THE ABOVE REMOVAL WORK SHALL BE PAID FOR AT THE UNIT PRICE BID FOR ITEM SPECIAL - LOADER RENTAL, AND SHALL BE FOR THE ACTUAL NUMBER OF LOADER HOURS WORKED. ANY OTHER EQUIPMENT, LABOR OR INCIDENTALS REQUIRED TO COMPLETE THIS ITEM SHALL BE INCLUDED THEREIN FOR PAYMENT.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR THE ABOVE PURPOSES.

ITEM SPECIAL	
GRADER RENTAL (HOURS)	10
LOADER RENTAL (HOURS)	5

DETECTOR LOOP, AS PER PLAN

ALL DETECTOR LOOPS SHALL BE CUT INTO THE EXISTING ROADWAY SURFACE AND PLACED TO A DEPTH OF 4" SO AS TO ALLOW FOR PLANING OF THE ROADWAY WITHOUT DISTURBING DETECTOR LOOPS. THIS WORK SHALL BE PERFORMED AT NIGHT. PLACEMENT SHALL BE AS PER SPECIFICATION 632.II. ALL MATERIALS (INCLUDING SPLICE KITS), LABOR, EQUIPMENT AND INCIDENTALS NECESSARY TO FURNISH A COMPLETED, IN PLACE, WORKING DETECTOR LOOP SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 632 DETECTOR LOOP, AS PER PLAN. ALL LOCATIONS, SIZES AND ORIENTATIONS SHALL BE VERIFIED AND SUPPLIED TO THE CONTRACTOR BEFORE CONSTRUCTION.

ITEM 632 DETECTOR LOOP, AS PER PLAN - 15 EACH

RAISED PAVEMENT MARKER REMOVED FOR STORAGE, AS PER PLAN

REMOVAL OF RAISED PAVEMENT MARKERS SHALL CONFORM WITH SECTION NO. 202.10 IN THE CONSTRUCTION AND MATERIAL SPECIFICATIONS MANUAL EXCEPT FOR THE FOLLOWING:

ONCE PAVEMENT MARKERS HAVE BEEN REMOVED THE OPENING THAT REMAINS IN THE ROADWAY SHALL BE CLEANED FREE OF ALL DEBRIS, TACKED AND FILLED WITH ASPHALT CONCRETE BY THE END OF THE SAME CONSTRUCTION DAY. AFTER PAVEMENT MARKERS HAVE BEEN REMOVED BY THE CONTRACTOR, HE WILL THEN BE RESPONSIBLE TO TAKE THE REMOVED MARKERS TO A STATE GARAGE THAT WILL BE DESIGNATED BY THE ENGINEER. THE PROJECT ENGINEER SHALL GIVE THE DISTRICT MAINTENANCE ENGINEER 24 HOUR NOTICE PRIOR TO DELIVERY AND THE PROJECT ENGINEER SHALL BE RESPONSIBLE FOR FURNISHING ALL NECESSARY TRANSFER DOCUMENTATION WITH ALL DELIVERIES. PAYMENT FOR ALL WORK DESCRIBED ABOVE SHALL BE PAID FOR UNDER ITEM 202 RAISED PAVEMENT MARKERS REMOVED FOR STORAGE, AS PER PLAN.

ITEM 202 - RAISED PAVEMENT MARKER REMOVED FOR STORAGE, AS PER PLAN
586 EACH - ESTIMATED QUANTITY CARRIED TO GENERAL SUMMARY

ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE

THE ENTIRE ROADWAY SURFACE SHALL BE PLANED TO A DEPTH OF ± 1.75 INCHES AS DIRECTED BY THE ENGINEER. THE ROADWAY SHALL BE PLANED SUCH THAT A MINIMUM SLOPE OF 0.0156 FT/FT IS CREATED FROM THE LANE LINE TO THE EDGE OF PAVEMENT. ALL SPECIFICATIONS OF ITEM 254 SHALL APPLY.

THE QUANTITIES FOR ITEM 254 PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN ARE SHOWN ON SHEETS 15 & 16 AND ARE CARRIED TO THE GENERAL SUMMARY FOR THE ABOVE DESCRIBED PURPOSE.

MAINTENANCE OF TRAFFIC

PLACING OF THE ITEM 442 ASPHALT CONCRETE, 12.5MM, TYPE A SHALL OCCUR AS CLOSE BEHIND THE PLANING OPERATION AS POSSIBLE SUCH THAT TRAFFIC SHALL NOT BE MAINTAINED ON THE MILLED SURFACE AT THE END OF THE WORK DAY.

WORK RESTRICTIONS AND LANE CLOSURES
FOR ASPHALT CONCRETE PLANING AND RESURFACING

PERMITTED ONE LANE CLOSURES FOR ASPHALT CONCRETE PLANING AND
PAVING OPERATION SHALL BE AS FOLLOWS:

- 7:00 P.M. TO 5:00 A.M. MON.-FRI.
- 7:00 P.M. FRI. TO 8 A.M. SAT.
- 7:00 P.M SAT. TO 11 A.M SUN.
- 7:00 P.M. SUN. TO 5 A.M. MON.

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO
TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:
MEMORIAL DAY, JULY 4, LABOR DAY AND THANKSGIVING
THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE
DAY OF THE WEEK ON WHICH THE HOLIDAY FALLS. THE FOLLOWING SCHEDULE
SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF THE WEEK	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 12:00N MONDAY
MONDAY	12:00N FRIDAY THROUGH 12:00N TUESDAY
TUESDAY	12:00N MONDAY THROUGH 12:00N WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 12:00N THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 12:00N MONDAY
FRIDAY	12:00N THURSDAY THROUGH 12:00N MONDAY
SATURDAY	12:00N THURSDAY THROUGH 12:00N MONDAY

NO EXTENSIONS OF TIME SHALL BE GRANTED FOR DELAYS IN MATERIAL
DELIVERIES, UNLESS SUCH DELAYS ARE INDUSTRY-WIDE, OR FOR LABOR
STRIKES, UNLESS SUCH STRIKES ARE AREA-WIDE.
SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS,
THE CONTRACTOR SHALL BE ASSESSED LIQUIDATED DAMAGES IN ACCOR-
DANCE WITH CMS 108.07 EXCEPT AS FOLLOWS.
LANE CLOSURES BEFORE THE ALLOWABLE TIME OR FAILURE TO REOPEN ALL
LANES TO TRAFFIC AS DESIGNATED IN THE PLANS SHALL RESULT IN A LIQUIDATION
OF DAMAGES CLAIM PAYABLE TO THE OHIO DEPARTMENT OF TRANSPORTATION AT
A RATE OF \$1500.00 (FIFTEEN HUNDRED DOLLARS) PER HOUR UNTIL CONDITION
IS CORRECTED FOR EACH INFRACTION.
EACH INFRACTION OVER ONE HOUR IS TO BE ROUNDED UP TO THE NEXT HOUR.
PAYMENT FOR THE ABOVE MENTIONED WORK SHALL BE INCLUDED IN THE
LUMP SUM BID PRICE FOR ITEM 614 MAINTAINING TRAFFIC - LUMP SUM

FLOODLIGHTING

FLOODLIGHTING FOR THE ASPHALT CONCRETE PLANING AND PAVING OPERATION
DURING NIGHT TIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS
DO NOT CAUSE GLARE TO THE DRIVERS ON THE HIGHWAYS. TO INSURE THE
ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER
SHALL DRIVE THROUGH THE WCRK SITE EACH NIGHT WHEN THE LIGHTING IS IN
PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS
DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO
THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS. THE
CONTRACTOR SHALL SUBMIT A COPY OF THEIR FLOODLIGHTING PLACEMENT
PLAN TO THE DISTRICT OFFICE FOR APPROVAL BY THE AREA ENGINEER
BEFORE ANY WORK IS TO COMMENCE . THE COST OF FLOODLIGHTING WILL BE
INCLUDED IN THE LUMP SUM COST OF ITEM 614 MAINTAINING TRAFFIC.

ITEM 614, LAW ENFORCEMENT OFFICER
WITH PATROL CAR

IN ADDITION TO THE REQUIREMENTS OF 614 AND THE LATEST EDITION OF THE
OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD), A UNIFORMED

LAW ENFORCEMENT OFFICER AND OFFICIAL PATROL CAR WITH WORKING TOP MOUNTED
EMERGENCY FLASHING LIGHTS SHALL BE PROVIDED FOR CONTROLLING TRAFFIC FOR
THE FOLLOWING TASKS:
FOR LANE CLOSURES: DURING INITIAL SETUP PERIODS, TEAR DOWN PERIODS,
SUBSTANTIAL SHIFTS OF A CLOSURE POINT, WHEN NEW LANE CLOSURE
ARRANGEMENTS ARE INITIATED AND FOR FULL DEPTH REMOVAL AND REPLACEMENT
AT DAYTON ROAD INTERSECTION.

LAW ENFORCEMENT OFFICERS (L.E.O.'S) SHOULD NOT BE USED WHERE THE OMUTCD
INTENDS THAT FLAGGERS BE USED. THE LEO'S ARE CONSIDERED TO BE EMPLOYED
BY THE CONTRACTOR AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THEIR
ACTIONS. ALTHOUGH THEY ARE EMPLOYED BY THE CONTRACTOR, THE PROJECT
ENGINEER SHALL HAVE CONTROL OVER THEIR PLACEMENT. THE OFFICIAL PATROL
CAR SHALL BE A PUBLIC SAFETY VEHICLE AS REQUIRED BY THE OHIO REVISED
CODE.

THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR THESE SERVICES WITH:
THE OHIO HIGHWAY PATROL
660 EAST MAIN STREET
COLUMBUS, OHIO
TELEPHONE: (614) 466-2660

LAW ENFORCEMENT OFFICERS WITH PATROL CAR REQUIRED BY THE TRAFFIC
MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY)
BASIS UNDER ITEM 614-LAW ENFORCEMENT OFFICER WITH PATROL CAR. THE
FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL
SUMMARY:

LAW ENFORCEMENT OFFICER WITH PATROL CAR. LOCATION 1 - 260 HOURS

THE HOURS PAID SHALL INCLUDE MINIMUM SHOW-UP TIME REQUIRED BY THE LAW
ENFORCEMENT AGENCY INVOLVED.

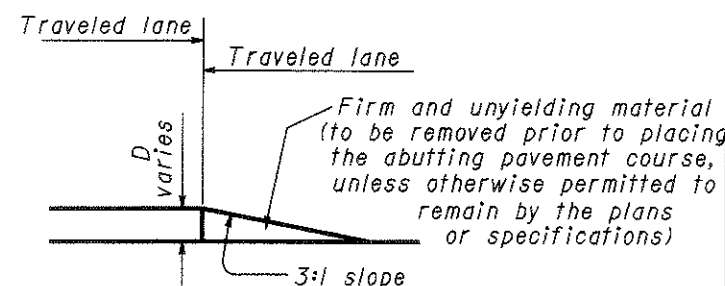
IF THE CONTRACTOR WISHES TC UTILIZE LEO'S FOR FLAGGING AND TRAFFIC
CONTROL OTHER THAN FOR THAT REQUIRED IN THESE PLANS, HE MAY DO SO AT
HIS OWN EXPENSE. PAYMENT FOR THE EXCESS ABOVE THE CONTRACT REQUIREMENTS
WILL BE INCLUDED UNDER ITEM 614 MAINTAINING TRAFFIC.

GENERAL NOTES

1. It is intended that this drawing be used for treatment of drop-offs that develop during construction operations, and that are not otherwise provided for in the construction plans. Where the plans do not provide specific items for labor, equipment, or materials to implement the drop-off treatments specified hereon, they shall be included for payment in the lump sum bid for Item 614 - Maintaining Traffic.
2. While the need for certain advisory signing is noted hereon, it is not intended that this be indicative of all signing that may be required to advise or warn motorists, and all requirements of the Ohio Manual of Uniform Traffic Control Devices (OMUTCD) must be fulfilled.
3. In urban or otherwise heavily developed areas where pedestrians and/or bicyclists may be present in significant numbers, additional signing and protective measures other than those shown hereon may be required.
4. The drop-off treatment selected for use at any given location shall be as appropriate for the prevailing conditions at the site.
5. Where concrete barrier is specified, it shall be in accordance with Standard Construction Drawing MC-9.2 and Item 622.
6. When drums are specified for a dropoff condition, a minimum number of four drums shall be used. Spacing shall be as indicated in the plans or as specified in the OMUTCD.
7. When OW-151 (Low Shoulder) signs or OW-171 (Uneven Lanes) and OWP-171 signs are required, they shall be placed 750' in advance of the condition, on all intersecting entrance ramps within the limits of the condition and immediately beyond all intersecting roadways within the limits of the condition. When the dropoff condition extends more than one-half mile, additional signs should be erected at intervals of one mile or less.
8. For locations, such as at ramps, lane shifts, lane closures, etc., where traffic is required to negotiate any difference in elevation between pavements, a 3:1 slope treatment similar to the Optional Wedge Treatment shall be provided.
9. Portable concrete barrier shall be placed on the same level as the traffic surface and shall not encroach on lane width(s) designated as the minimum required for traffic use. Where drums are used, and their presence would reduce traveled lane widths to less than 10', drums may be placed on the opposite level from that of traffic provided the dropoff depth does not exceed 5" and approval is granted by the Project Engineer.
10. Pavement Repairs (or similar work):
 - a. Lengths greater than 60 feet - utilize appropriate treatment from Condition I.
 - b. Lengths of 60 feet or less - repairs shall be effected in accordance with 255.08. Drums may be used as a separator adjacent to the traveled lane.

OPTIONAL WEDGE TREATMENT (MILLING OR RESURFACING)

1. This treatment may be used when permitted for Condition I only.
2. OW-171 and OWP-171 signs required.



CONDITION I DROPOFFS BETWEEN TRAVELED LANES

1. These treatments are to be used for resurfacing, pavement planing, excavation, etc. between or within traveled lanes.

D (In.)	Treatment
≤ 1/2	Erect OW-171 and OWP-171 signs.
> 1/2 - 3	1) Lane closure utilizing drums* as shown below OR 2) Optional Wedge Treatment
> 3 - 5	Lane closure utilizing drums as shown below.
> 5	Lane closure utilizing portable concrete barrier as shown below.

*Cones may be used for daytime only conditions.

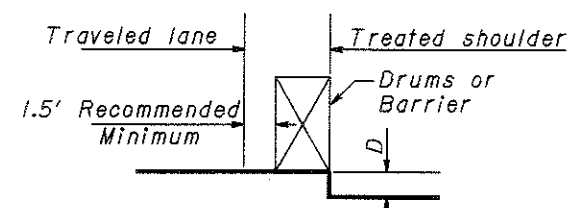


CONDITION II DROPOFFS WITHIN GRADED SHOULDER AREA

1. The treatments indicated below are for use in conjunction with resurfacing, planing, or excavations within the graded shoulder area.
2. The graded shoulder area is that flat or gradually sloping area between the edge of a normally traveled lane and the more steeply sloping ditch foreslope or embankment slope. Its surface may be soil or turf, and/or it may be inclusive of a "treated" area (improved with aggregates, asphaltic materials, or concrete). For the purposes herein, its maximum width shall be considered to be twelve (12) feet.

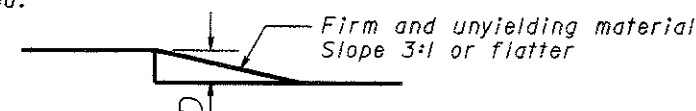
D (In.)	Treatment
≤ 1/2	1) If edgelines are present, no treatment necessary OR 2) Erect OW-171 and OWP-171 signs.
> 1/2 - 5	1) If min. lane width requirements can be met, maintain lanes utilizing drums as shown below OR 2) If min. lane width requirements cannot be met, close adjacent lane utilizing drums OR 3) Optional Shoulder Treatment.
> 5 - 12 Daylight only	If min. lane width requirements can be met, maintain lanes utilizing drums as shown below.
> 5 - 24	1) If min. lane width requirements can be met, maintain lanes utilizing portable concrete barrier as shown below. OR 2) If min. lane width requirements cannot be met, close adjacent lane utilizing drums.
> 24	Lane closure utilizing portable concrete barrier as shown below.

* Minimum lane widths shall be 10' unless otherwise specified in the plans.



OPTIONAL SHOULDER TREATMENT

1. This treatment may not be used within a bituminous shoulder where a hot longitudinal joint per 401.15 is required.
2. OW-151 signs required.



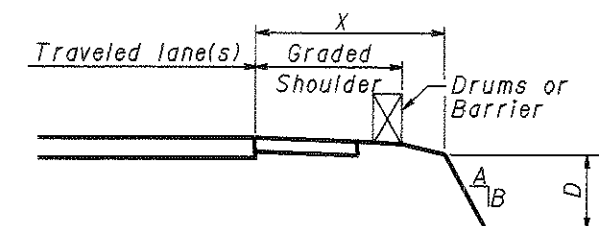
LIC-16-23.63

CONDITION III DROPOFFS BEYOND GRADED SHOULDER OR BACK OF CURB

1. See Note 2 under Condition II.
2. Use Chart A or B below, as applicable.

CHART A

- USE FOR:
1. Uncurbed Facilities.
 2. Curbed Facilities, where:
 - a. Curbs are less than 6" in height.
 - b. Curbs are 6" or greater in height and the legal speed is greater than 40 mph.

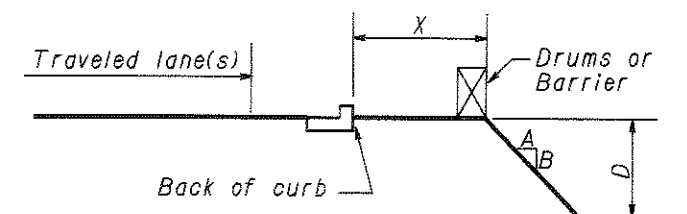


X (Ft.)	D (In.)	A/B	Treatment Required	
			Day	Night
0-4	Any	Any	(a)	(a)
4-30	Any	3:1 or Flatter	None	None
4-12	< 3	Steeper than 3:1	None	None
4-12	> 3 - < 12	Steeper than 3:1	Drums	Drums
4-12	> 12	Steeper than 3:1	Drums	Barrier
> 12 - 20	< 12	Steeper than 3:1	None	None
> 12 - 20	> 12 - < 24	Steeper than 3:1	Drums	Drums
> 12 - 20	> 24	Steeper than 3:1	Drums	Barrier
> 20 - 30	< 24	Steeper than 3:1	None	Drums
> 20 - 30	> 24	Steeper than 3:1	Drums	Barrier
> 30	Any	Any	None	None

(a) Use treatment specified under Condition II.

CHART B

- USE FOR: Curbed facilities, where the curb is 6" or greater in height and the legal speed is 40 mph or less.



X (Ft.)	D (In.)	A/B	Treatment Required	
			Day	Night
0-10	< 12	Any	None	Drums
0-10	> 12	Any	Drums	Drums
> 10	Any	Any	None	None

STATE OF OHIO
DEPARTMENT OF TRANSPORTATION
BUREAU OF LOCATION AND DESIGN

DROPOFFS IN WORK ZONES

DESIGNED DRAWN TRACED CHECKED REVIEWED DATE REVISED

RPM General Notes

Materials Supplied by The Department

All materials are to be Contractor furnished, except that the Department shall supply RPM materials in the quantities shown herein to the Contractor. Pay items for the Department supplied materials shall be indicated as "Installation Only". The quantity and type of Department supplied materials are shown on sheet 29 of this plan.

The Contractor shall pick up the department supplied RPM materials at the direction of the Project Engineer.

For some projects having quantities of less than 20 RPMs, the contractor may pick up RPM materials at the District Offices. Quantities over 20 RPMs will be picked up at the Recycler's Warehouse or as arranged with the District. The Contractor shall pick up Department supplied RPM materials at the specified location(s) for transport to the work site or to the Contractor's storage facility. The Recycled Raised Pavement Marker (RPM) Authorization Form is to be signed by the District Construction Engineer prior to pick up of the RPMs. The Contractor shall notify the District and / or the parties listed on the authorization form in writing at least five (5) calendar days prior to pick up of the department supplied materials. The contractor shall store the RPMs without damage or contamination with foreign matter. A deduction in the amount of the actual cost to the Department shall be made for materials damaged by the Contractor or for castings received by the Contractor which were not installed and were not returned to the Department.

Return of Non-performed Raised Pavement Marker Materials Supplied by the Department

Raised Pavement Marker Materials Supplied by the Department, that are non-performed shall be carefully repacked or packed in the boxes in the same style and quantity as originally received from the Department. Casting styles shall not be mixed within any one container. The Contractor shall clearly mark on the outside of each container, the color of the prismatic retro-reflector, the style of casting. Boxes shall be placed on skids or pallets in the same style (Low Profile or Conventional, reflectorised or non reflectorised) and no more than 420 RPMs (or 21 Boxes) on one skid.

Only use the boxes supplied by the Raised Pavement Marker Recycler. Boxes must be marked with the recycler's part or catalog number and the project number. The recycler's catalog or part numbers may be obtained from the Office of Traffic Engineering in Columbus, Ohio or from the recycler. Boxes not marked with the proper recycler's catalog or part numbers, and the department's project number will not be accepted at the recycler's warehouse. Non Performed Materials will be returned to the location as specified by the District Construction Engineer within 30 Days of the completion of the project.

The above work including all labor, equipment and material needed to perform the work, shall be considered incidental to the respective pay item.

If the department has to repackage the RPMs correctly, the Contractor will be assessed the actual cost for repackaging the Materials by the Department's Forces.

Loading of Materials Supplied by the Department at the Recycler's Warehouse

Trucks shall have a loading height of 48 inches and be able to back up flush to the loading dock.

Trucks shall not have any obstructions or protrusions that prevent the loading by a standard forklift or lift truck.

Semi trucks or 20 foot commercial trucks are the most appropriate trucks for loads in excess of 4 pallets (one pallet = 21 boxes = 2000 LBS).

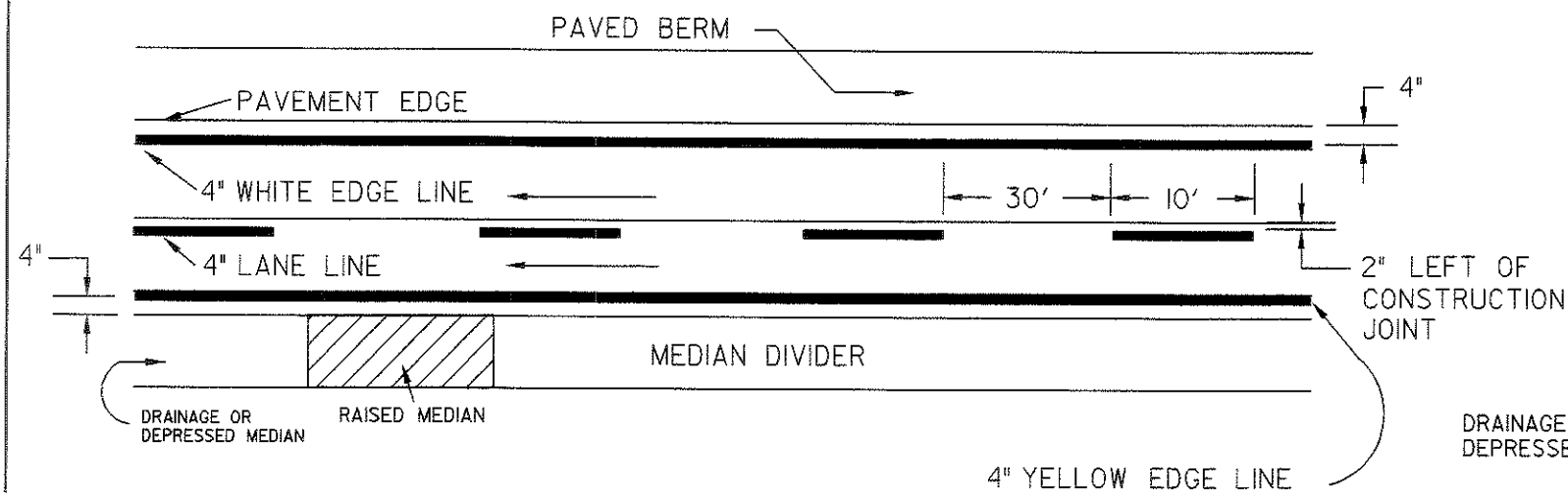
Stake body trucks are appropriate to load less than 4 pallets, provided the truck is rated for the load and the load can be safely secured for transport by chaining or strapping down as needed.

Pickup trucks are appropriate for loads of approximately one pallet, provided the pickup truck is rated for the load and the load can be safely secured for transport.

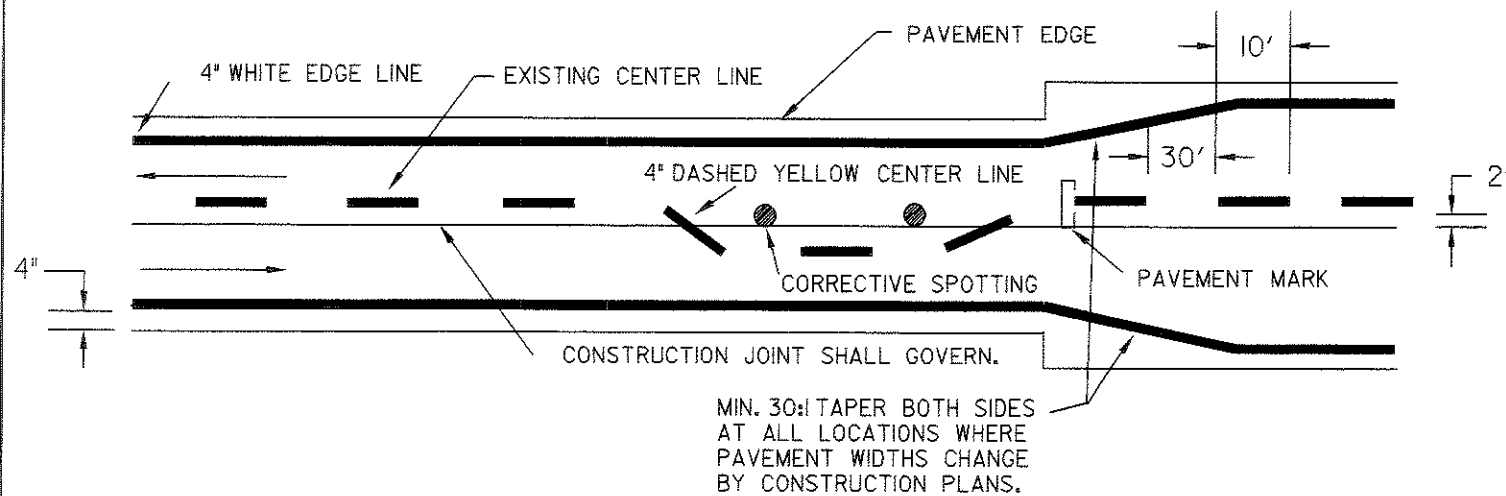
Dump trucks, tilt bed trucks, and non commercial moving vans will not be loaded by the recyclers warehouse.

The warehouse supervisor will refuse to load any truck that is unsafe to load or unsuitable for the load being placed on the truck.

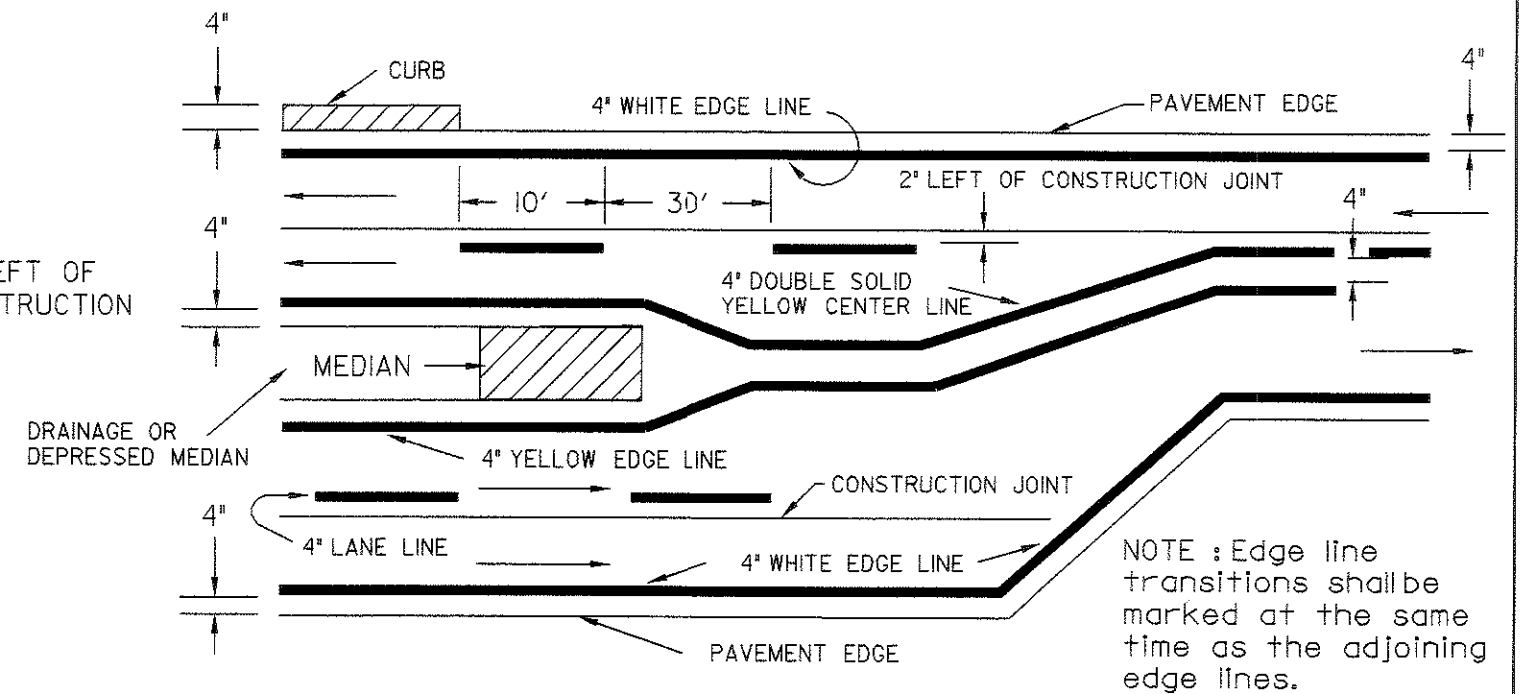
FREEWAY & EXPRESSWAY MAINLINE MARKINGS



TWO LANE MARKINGS



MULTILANE DIVIDED & UNDIVIDED HIGHWAY MARKINGS



NOTES:

1. The distance from the pavement edge to the nearside edge of the edgeline may be increased with the approval of the engineer in order to maintain uniform lane width.
2. See TC-72.20 for entrance and exit ramp markings.
3. The cycle length for dashed lines shall be 40 feet plus or minus 6 inches. The minimum length of dash shall be sufficiently long to maintain a 3:1 ratio between length of gap and length of dash.

Ohio Department of Transportation

Pavement Marking
Typical Details

DATE
11-80
9-86
9-91

PAVEMENT MARKING TYPICALS

LIC-16-23.63

8
31

ITEM 614 PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, CHANGEABLE MESSAGE SIGNS, ON SITE, FOR THE DURATION OF THE PROJECT. THE SIGNS SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS MAINTAINED BY THE DIRECTOR. THE LIST CURRENTLY CONTAINS CLASS III WITH A MINIMUM LEGIBILITY DISTANCE OF 650 FT.

EACH SIGN SHALL BE TRAILER MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM TO DIM THE SIGN DURING DARKNESS AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLE-SHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. SURFACES OF A 9-INCH BY 15-INCH MINIMUM SIZE FACING TRAFFIC.

THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE LINK WHICH WILL ALLOW REMOTE SIGN ACTIVATION, DEACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS AND REVISIONS TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES.

OPTION 1:
THE CONTRACTOR SHALL PROVIDE THE ENGINEER WITH A LAP TOP COMPUTER PLUS MODEM TO CONTROL THE PCMS REMOTELY. THE SIGN SHALL BE INSURED AGAINST THEFT.

OPTION 2:
THE CONTRACTOR SHALL PROVIDE TO THE ENGINEER THE SOFTWARE NECESSARY TO CONTROL THE PCMS REMOTELY.

OPTION 3:
THE PCMS SHALL BE EQUIPPED WITH A MYRIAD SAFETY BEAM OR AN APPROVED EQUAL AS DETERMINED BY THE ENGINEER. THE MYRIAD SAFETY BEAM SENDS OUT A SIGNAL THAT ACTIVATES RADAR DETECTORS. THE BEAM IS APPROVED BY THE F.C.C. THE MYRIAD SAFETY BEAM SHALL USE THE SAME POWER SUPPLY AS THE PCMS. THE MYRIAD SAFETY BEAM SHALL BE ABLE TO BE ACTIVATED WITH THE PCMS RUNNING OR NOT.

THE MYRIAD SAFETY BEAM IS DISTRIBUTED BY:
THE TRIPLEX GROUP, INC.
P.O. BOX 428, NEW HOPE, PA. 18938
PHONE (215) 862-5077

PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS WILL BE OFF, FACING AWAY FROM ALL TRAFFIC AND SHALL DISPLAY ONE OR MORE HIGH INTENSITY YELLOW REFLECTIVE SHEETING SURFACES OF A 9-INCH BY 15-INCH MINIMUM SIZE FACING TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT AND TO REVISE SIGN MESSAGES, IF NECESSARY.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PREPROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OF PREPROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ONBOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHOULD BE EMPLOYED. ALTHOUGH THREE-PHRASES MAY BE USED IN USUAL CONDITIONS. THE PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST ONCE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF 614.03(C). THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC AND THE ENTIRE COST TO CONTROL TRAFFIC ACCRUED BY THE DEPARTMENT WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE TO THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24 HOUR PER DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

THE REQUIREMENT TO FURNISH, INSTALL, MAINTAIN AND REMOVE A PCMS UNIT ON THIS PROJECT SHALL NOT IN ANY WAY RELIEVE THE CONTRACTOR OF HIS RESPONSIBILITIES AS OUTLINED IN 104.04.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE BID PER SIGN-MONTH FOR EACH ITEM 614 PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN AND SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE, AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

THIS PROJECT SHALL REQUIRE 4(FOUR) ITEM 614 PORTABLE CHANGEABLE MESSAGE SIGNS.

THE FOLLOWING QUANTITY HAS BEEN INCLUDED IN THE GENERAL SUMMARY TO PERFORM THE WORK AS DESCRIBED ABOVE.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN,
AS PER PLAN, 4 SIGN MONTH

ITEM 614 MAINTAINING TRAFFIC, AS PER PLAN

TRAFFIC SHALL BE MAINTAINED AS PER STD. DWG. MT-95.40 AND AS OUTLINED IN THE CONSTRUCTION AND MAINTENANCE OPERATIONS SECTIONS OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS LATEST REVISION. IN ADDITION, THE FOLLOWING REQUIREMENTS SHALL APPLY:

THE CONTRACTOR SHALL SUBMIT, IN WRITING A SCHEDULE OF OPERATIONS TO THE DISTRICT DEPUTY DIRECTOR AND RECEIVE APPROVAL BEFORE WORK IS STARTED ON THE PROJECT.

BEFORE WORK BEGINS, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER THE NAMES AND TELEPHONE NUMBERS OF A PERSON OR PERSONS WHO CAN BE CONTACTED 24 HOURS A DAY BY THE OHIO DEPARTMENT OF TRANSPORTATION AND ALL INTERESTED POLICE AGENCIES. THIS PERSON OR PERSONS SHALL BE RESPONSIBLE FOR REPLACING NECESSARY TRAFFIC CONTROL DEVICES IMMEDIATELY, AS PER 64.03(C).

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR THE PHYSICAL CONSTRUCTION ONLY. THE INSTALLATION AND OPERATION OF ALL REQUIRED TRAFFIC CONTROL DEVICES SHALL BE PROVIDED BY THE CONTRACTOR WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

ALL DRUMS SHALL BE PROPERLY REFLECTORIZED PLASTIC DRUMS AND WEIGHTED. ANY REPLACEMENT DRUMS NEEDED DURING THE CONSTRUCTION OF S.R. 16 AND DAYTON ROAD APPROACH SHALL BE PAID FOR UNDER THE LUMP SUM BID FOR ITEM 614 MAINTAINING TRAFFIC, AS PER PLAN.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL TRAFFIC CONTROL INVOLVED IN PLACING AND REMOVING ALL PORTABLE CONCRETE BARRIER NEEDED DURING THIS PROJECT. ANY PORTABLE CONCRETE BARRIER, 32", OBJECT MARKERS AND BARRIER REFLECTORS NEEDED DURING THE CONSTRUCTION OF S.R. 16 AND DAYTON ROAD APPROACH SHALL BE PAID FOR UNDER THE LUMP SUM BID FOR ITEM 614 MAINTAINING TRAFFIC, AS PER PLAN.

ANY TIMING ADJUSTMENT MADE TO THE SIGNAL AT S.R. 16 AND DAYTON ROAD SHALL BE PAID FOR UNDER THE LUMP SUM BID FOR ITEM 614 MAINTAINING TRAFFIC, AS PER PLAN.

ANY EXISTING PAVEMENT MARKINGS NEEDING REMOVED AND ANY ADDITIONAL TEMPORARY PAVEMENT MARKINGS NEEDED DURING THE CONSTRUCTION OF S.R. 16 AND DAYTON ROAD APPROACH SHALL BE PAID FOR UNDER THE LUMP SUM BID FOR ITEM 614 MAINTAINING TRAFFIC, AS PER PLAN.

THE CONTRACTOR SHALL ARRANGE HIS OPERATIONS SO AS TO PREVENT ANY INTERFERENCE TO THE CONTINUOUS FLOW OF TRAFFIC. ALL VEHICLES, EQUIPMENT, WORKERS AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIME TO ONE SIDE OF THE PAVEMENT UNLESS OTHERWISE APPROVED BY THE PROJECT ENGINEER.

FOR SAFETY PURPOSES NO EQUIPMENT, VEHICLES OR MATERIAL SHALL BE PARKED OR STORED WITHIN THIRTY (30) FEET FROM THE EDGE OF THE PAVEMENT OF THE TRAVELED LANES.

EXISTING SIGNS OR CONTRACTOR SUPPLIED SIGNS SHALL BE USED TO MAINTAIN TRAFFIC DURING CONSTRUCTION.

ANY CONFLICTING SIGNS WHETHER INSIDE OR OUTSIDE THE WORK LIMITS SHALL BE REMOVED OR COVERED AND TEMPORARY SIGNS ERECTED WHEN APPLICABLE BY THE CONTRACTOR.

THE ENGINEER SHALL RECORD INSTALLATION AND REMOVAL OF PROPOSED SIGNS, COVERED OR REMOVED AND UNCOVERED OR REERECTED SIGNS IN THE PROJECT DIARY.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS AS DESCRIBED ABOVE SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614 MAINTAINING TRAFFIC, AS PER PLAN UNLESS SEPARATELY ITEMIZED IN THE PLAN.

THE FOLLOWING QUANTITY HAS BEEN INCLUDED IN THE GENERAL SUMMARY:

ITEM 614 MAINTAINING TRAFFIC, AS PER PLAN LUMP

SEQUENCE OF OPERATIONS

PHASE 1

STEP 1
(DRIVING LANE OF WESTBOUND S.R. 16 AND NORTHBOUND LANE OF DAYTON ROAD NORTH APPROACH TO S.R. 16)

INSTALL ALL NECESSARY TRAFFIC CONTROL DEVICES AS PER TYPICAL SECTIONS AND STD. CONST. DWG. MT-95.40.

CLOSE THE DRIVING LANE IN THE WESTBOUND LANES OF S.R. 16 AND THE RIGHT TURN TO THE DAYTON ROAD NORTH APPROACH. MAINTAIN ONE LANE OF TRAFFIC IN THE PASSING LANE FOR S.R. 16 WESTBOUND TRAFFIC DURING THIS STEP OF CONSTRUCTION. CONSTRUCT THE PROPOSED CONCRETE PAVEMENT AS PER TYPICAL SECTIONS AND DETAIL SHEETS.

CLOSE THE NORTHBOUND LANE OF DAYTON ROAD NORTH APPROACH, CONSTRUCT THE PROPOSED CONCRETE PAVEMENT AS PER TYPICAL SECTIONS AND DETAIL SHEETS.

NOTE:
TRAFFIC TRAVELING WESTBOUND ON S.R. 16 SHALL BE PERMITTED TO TURN RIGHT ON TO THE DAYTON ROAD NORTH APPROACH FROM THE PASSING LANE OF S.R. 16 DURING THIS STEP OF CONSTRUCTION.

NOTE:
THE TRAFFIC SIGNAL PHASING MAY NEED ADJUSTED, IF DETERMINED BY THE PROJECT ENGINEER, TO MINIMIZE ONE LANE TRAFFIC CONGESTION. BRIAN MOOREHEAD, CITY OF NEWARK, (740)349-6621 SHALL BE CONTACTED TO ADJUST THE PHASING.

NOTE:
THE LEFT TURN LANE IN THE WESTBOUND LANES OF S.R. 16 SHALL BE OPENED AT ALL TIMES DURING THIS PHASE OF CONSTRUCTION.

NOTE:
TWO WAY TRAFFIC ON THE DAYTON ROAD NORTH APPROACH SHALL BE MAINTAINED AT ALL TIMES DURING THIS PHASE OF CONSTRUCTION.

NOTE:
A LAW ENFORCEMENT OFFICER WITH PATROL CAR(LEO) MAY BE UTILIZED FOR THE INTERSECTION TRAFFIC OF DAYTON ROAD NORTH APPROACH AND S.R. 16.

STEP 2
(RIGHT LANE OF WESTBOUND S.R. 16 AND SOUTHBOUND LANE OF DAYTON ROAD NORTH APPROACH TO S.R. 16)

INSTALL ALL NECESSARY TRAFFIC CONTROL DEVICES AS PER TYPICAL SECTIONS AND DETAIL SHEETS.

CLOSE THE DRIVING LANE IN THE WESTBOUND LANES OF S.R. 16 JUST WEST OF THE DAYTON ROAD NORTH APPROACH. MAINTAIN ONE LANE OF TRAFFIC IN THE PASSING LANE OF S.R. 16 WESTBOUND TRAFFIC DURING THIS STEP OF CONSTRUCTION. CONSTRUCT THE PROPOSED CONCRETE PAVEMENT AS PER TYPICAL SECTIONS AND DETAIL SHEETS.

CLOSE THE SOUTHBOUND LANE OF DAYTON ROAD NORTH APPROACH, CONSTRUCT THE PROPOSED CONCRETE PAVEMENT AS PER TYPICAL SECTIONS AND DETAIL SHEETS.

NOTE:
TRAFFIC TRAVELING WESTBOUND ON S.R. 16 SHALL BE PERMITTED TO TURN RIGHT ON TO THE DAYTON ROAD NORTH APPROACH FROM THE LEFT(PASSING) LANE OF S.R. 16 DURING THIS STEP OF CONSTRUCTION.

NOTE:
THE TRAFFIC SIGNAL PHASING MAY NEED ADJUSTED, IF DETERMINED BY THE PROJECT ENGINEER, TO MINIMIZE ONE LANE TRAFFIC CONGESTION. BRIAN MOOREHEAD, CITY OF NEWARK, (740)349-6621 SHALL BE CONTACTED TO ADJUST THE PHASING.

NOTE:
THE LEFT TURN LANE IN THE WESTBOUND LANES OF S.R. 16 SHALL BE OPENED AT ALL TIMES DURING THIS PHASE OF CONSTRUCTION.

NOTE:
TWO WAY TRAFFIC ON THE DAYTON ROAD NORTH APPROACH SHALL BE MAINTAINED AT ALL TIMES DURING THIS PHASE OF CONSTRUCTION.

NOTE:
A LAW ENFORCEMENT OFFICER WITH PATROL CAR(LEO) MAY BE UTILIZED FOR THE INTERSECTION TRAFFIC OF DAYTON ROAD NORTH APPROACH AND S.R. 16.

PHASE 2

STEP 1
(PASSING LANE OF WESTBOUND S.R. 16 AND THE INTERSECTION OF DAYTON ROAD NORTH APPROACH TO S.R. 16)

INSTALL ALL NECESSARY TRAFFIC CONTROL DEVICES AS PER TYPICAL SECTIONS AND DETAIL SHEETS.

CLOSE THE PASSING LANE IN THE WESTBOUND LANES OF S.R. 16 AND THE LEFT TURN LANE TO THE DAYTON ROAD SOUTH APPROACH. ALSO, CLOSE THE LEFT TURN LANE IN THE EASTBOUND LANES OF S.R. 16 TO THE DAYTON ROAD NORTH APPROACH. MAINTAIN ONE LANE OF TRAFFIC IN THE DRIVING LANE FOR S.R. 16 WESTBOUND TRAFFIC DURING THIS STEP OF CONSTRUCTION. CONSTRUCT THE PROPOSED CONCRETE PAVEMENT AS PER TYPICAL SECTIONS AND DETAIL SHEETS.

CLOSE THE NORTHBOUND LANE OF DAYTON ROAD NORTH APPROACH, CONSTRUCT THE PROPOSED CONCRETE PAVEMENT AS PER TYPICAL SECTIONS AND DETAIL SHEETS.

NOTE:
TRAFFIC TRAVELING WESTBOUND ON S.R. 16 SHALL NOT BE PERMITTED TO TURN LEFT ON TO THE DAYTON ROAD SOUTH APPROACH FROM THE DRIVING LANE OF S.R. 16 DURING THIS STEP OF CONSTRUCTION.

NOTE:
TRAFFIC FOR THE LEFT TURN LANES IN BOTH THE EASTBOUND AND WESTBOUND LANES OF S.R. 16 SHALL USE AN ALTERNATE ROUTE DURING THIS PHASE OF CONSTRUCTION.

NOTE:
THE TRAFFIC SIGNAL PHASING MAY NEED ADJUSTED, IF DETERMINED BY THE PROJECT ENGINEER, TO MINIMIZE ONE LANE TRAFFIC CONGESTION. BRIAN MOOREHEAD, CITY OF NEWARK, (740)349-6621 SHALL BE CONTACTED TO ADJUST THE PHASING.

NOTE:
TRAFFIC TRAVELING EASTBOUND ON S.R. 16 SHALL NOT BE PERMITTED TO TURN LEFT ON TO THE DAYTON ROAD NORTH APPROACH DUE TO THE CLOSURE OF THE LEFT TURN LANE DURING THIS PHASE OF CONSTRUCTION.

NOTE:
THE RIGHT TURN LANE IN THE WESTBOUND LANES OF S.R. 16 SHALL BE OPENED AT ALL TIMES DURING THIS PHASE OF CONSTRUCTION.

NOTE:
TWO WAY TRAFFIC ON THE DAYTON ROAD NORTH APPROACH SHALL BE MAINTAINED AT ALL TIMES DURING THIS PHASE OF CONSTRUCTION.

NOTE:
A LAW ENFORCEMENT OFFICER WITH PATROL CAR(LEO) MAY BE UTILIZED FOR THE INTERSECTION TRAFFIC OF DAYTON ROAD NORTH APPROACH AND S.R. 16.

STEP 2
(PASSING LANE OF WESTBOUND S.R. 16 AND THE INTERSECTION OF DAYTON ROAD NORTH APPROACH TO S.R. 16)

INSTALL ALL NECESSARY TRAFFIC CONTROL DEVICES AS PER TYPICAL SECTIONS AND DETAIL SHEETS.

CLOSE THE PASSING LANE IN THE WESTBOUND LANES OF S.R. 16 AND THE LEFT TURN LANE TO THE DAYTON ROAD SOUTH APPROACH. ALSO, CLOSE THE LEFT TURN LANE IN THE EASTBOUND LANES OF S.R. 16 TO THE DAYTON ROAD NORTH APPROACH. MAINTAIN ONE LANE OF TRAFFIC IN THE DRIVING LANE FOR S.R. 16 WESTBOUND TRAFFIC DURING THIS STEP OF CONSTRUCTION. CONSTRUCT THE PROPOSED CONCRETE PAVEMENT AS PER TYPICAL SECTIONS AND DETAIL SHEETS.

CLOSE THE SOUTHBOUND LANE OF DAYTON ROAD NORTH APPROACH, CONSTRUCT THE PROPOSED CONCRETE PAVEMENT AS PER TYPICAL SECTIONS AND DETAIL SHEETS.

NOTE:
TRAFFIC TRAVELING WESTBOUND ON S.R. 16 SHALL NOT BE PERMITTED TO TURN LEFT ON TO THE DAYTON ROAD SOUTH APPROACH FROM THE DRIVING LANE OF S.R. 16 DURING THIS STEP OF CONSTRUCTION.

NOTE:
TRAFFIC TRAVELING EASTBOUND ON S.R. 16 SHALL NOT BE PERMITTED TO TURN LEFT ON TO THE DAYTON ROAD NORTH APPROACH DUE TO THE CLOSURE OF THE TURN LANE DURING THIS PHASE OF CONSTRUCTION.

PHASE 2 CONT.

NOTE:
TRAFFIC FOR THE LEFT TURN LANES IN BOTH THE EASTBOUND AND WESTBOUND LANES OF S.R. 16 SHALL USE AN ALTERNATE ROUTE DURING THIS PHASE OF CONSTRUCTION.

NOTE:
THE TRAFFIC SIGNAL PHASING MAY NEED ADJUSTED, IF DETERMINED BY THE PROJECT ENGINEER, TO MINIMIZE ONE LANE TRAFFIC CONGESTION. BRIAN MOOREHEAD, CITY OF NEWARK, (740)349-6621 SHALL BE CONTACTED TO ADJUST THE PHASING.

NOTE:
THE RIGHT TURN LANE IN THE WESTBOUND LANES OF S.R. 16 SHALL BE OPENED AT ALL TIMES DURING THIS PHASE OF CONSTRUCTION.

NOTE:
TWO WAY TRAFFIC ON THE DAYTON ROAD NORTH APPROACH SHALL BE MAINTAINED AT ALL TIMES DURING THIS PHASE OF CONSTRUCTION.

NOTE:
A LAW ENFORCEMENT OFFICER WITH PATROL CAR(LEO) MAY BE UTILIZED FOR THE INTERSECTION TRAFFIC OF DAYTON ROAD NORTH APPROACH AND S.R. 16.

ATTENTION CONTRACTOR:
IT IS THE INTENT TO DISRUPT TRAFFIC IN THE WESTBOUND LANES OF S.R. 16 AND THE DAYTON ROAD NORTH APPROACH FOR A TOTAL OF TEN(10) CONSECUTIVE CALENDAR DAYS. THE TIME STARTS FROM THE FIRST TIME TRAFFIC IS ALTERED FROM ITS NORMAL CONDITION AND CONTINUES UNTIL THE TRAFFIC IS RETURNED TO ITS NORMAL CONDITION. FOR ADDITIONAL DETAILS OF THE INCENTIVE/DISINCENTIVE TERMS AND PAYMENT, SEE SHEET 11/31.

GENERAL:
IT IS THE INTENT OF THIS SEQUENCE OF OPERATIONS TO PROVIDE A WORK AREA FOR THE CONTRACTOR WHILE ALSO MAINTAINING TRAFFIC IN A MANNER WHICH IS SAFE FOR THE TRAVELING PUBLIC.

THE CONTRACTOR SHALL NOT BE ALLOWED TO START WORK ON ANY PHASE OF CONSTRUCTION UNTIL APPROVAL HAS BEEN GRANTED BY THE PROJECT ENGINEER.

ALTERNATE METHODS:
IF THE CONTRACTOR SO ELECTS, HE MAY SUBMIT ALTERNATE METHODS FOR THE MAINTENANCE OF TRAFFIC, PROVIDED THE INTENT OF THE ABOVE PROVISIONS IS FOLLOWED AND NO ADDITIONAL INCONVENIENCE TO THE TRAVELING PUBLIC RESULTS THEREFROM. NO ALTERNATE PLAN SHALL BE PLACED INTO EFFECT UNTIL APPROVAL HAS BEEN GRANTED, IN WRITING, BY THE DIRECTOR.

WORK ZONE INSPECTIONS:
THE CONTRACTOR SHALL DESIGNATE AN INDIVIDUAL, OTHER THAN THE SUPERINTENDENT, AND SUBJECT TO THE APPROVAL OF THE ENGINEER, TO INSPECT ALL TRAFFIC CONTROL DEVICES IN THE WORK ZONE AT THE BEGINNING AND AT THE END OF EACH WORK DAY. DAILY, A RECORD OF THE REVIEW SHALL BE GIVEN TO THE PROJECT ENGINEER, IN WRITING, AND SHALL INCLUDE A RECORD OF DEFICIENCIES AND RESOLUTION OF THE DEFICIENCIES. A MINIMUM OF TWO REVIEWS EACH WEEK SHALL BE PERFORMED DURING THE HOURS OF DARKNESS. ONE OF THE NIGHT INSPECTIONS SHALL BE PERFORMED ON A SATURDAY OR SUNDAY NIGHT.

THE FOLLOWING ITEMS SHALL BE INCLUDED IN EACH REVIEW:
TRAFFIC CONTROL DEVICE CONDITIONS, PLACEMENT, VISIBILITY, TRAFFIC FLOW CONDITIONS, INCIDENTS, CONGESTION POINTS, DELAYS, INTERACTION OR WORK VEHICLES AND TRAFFIC, EVIDENCE OF ACCIDENTS, PROPER STORAGE OF MATERIALS AND EQUIPMENT, CONFORMANCE WITH THE TRAFFIC CONTROL PLAN, ADEQUACY OF THE TRAFFIC CONTROL PLAN, CONFLICTING OR NON-CONFORMING PAVEMENT MARKINGS. THE DESIGNATED INDIVIDUAL OR A QUALIFIED REPRESENTATIVE SHALL ALSO BE AVAILABLE ON AN AROUND THE CLOCK BASIS TO REPAIR AND/OR REPLACE DAMAGES OR MISSING TRAFFIC CONTROL DEVICES. THESE INDIVIDUALS NAMES AND PHONE NUMBERS SHALL BE GIVEN TO THE PROJECT ENGINEER AT THE PRE-CONSTRUCTION MEETING. THE TRAFFIC CONTROL INSPECTOR SHALL BE INCLUDED IN THE LUMP SUM BID PRICE FOR ITEM 614 MAINTAINING TRAFFIC, AS PER PLAN.

101 DEFINITION AND TERMS FOR
INCENTIVE/DISINCENTIVE PAYMENT

101.101 CONTRACT TIME
THE TOTAL TIME ESTABLISHED FOR THE COMPLETION OF ALL CONTRACT WORK.

101.151 CRITICAL WORK
COMPLETE ALL WORK ITEMS NECESSARY TO PERMIT ONE LANE OF TRAFFIC ON S.R. 16 WESTBOUND LANES AND TWO WAY TRAFFIC ON THE DAYTON ROAD NORTH APPROACH FOR SAFE, CONVENIENT USE BY THE TRAVELING PUBLIC. FOR DEFINITIONS OF PHASE 1 AND PHASE 2 WESTBOUND LANES AND NUMBER OF DAYS TO COMPLETE, SEE SEQUENCE OF OPERATIONS.

101.221 INCENTIVE/DISINCENTIVE CLAUSES FOR COMPLETION OF CRITICAL WORK
AN INCENTIVE CLAUSE IS A CONTRACT PROVISION WHICH COMPENSATES THE CONTRACTOR A FIXED AMOUNT OF MONEY FOR EACH CALENDAR DAY THE WORK DESIGNATED AS CRITICAL WORK COMPLETED AND UNRESTRICTED TRAFFIC RESTORED ON THE PROJECT BEFORE THE EXPIRATION OF THE INCENTIVE/DISINCENTIVE TIME ESTABLISHED IN THE CONTRACT.

A DISINCENTIVE CLAUSE IS A CONTRACT PROVISION WHICH ASSESSES A DEDUCTION FROM THE CONTRACT AMOUNT FOR EACH CALENDAR DAY THE CONTRACTOR OVERRUNS THE INCENTIVE/DISINCENTIVE TIME ESTABLISHED IN THE CONTRACT FOR THE COMPLETION OF THE WORK DESIGNATED AS CRITICAL WORK.

101.222 INCENTIVE/DISINCENTIVE DAILY AMOUNT
A FIXED AMOUNT OF MONEY AWARDED OR ASSESSED THE CONTRACTOR FOR EACH DAY ALL IDENTIFIED CRITICAL WORK DEFINED IN 101.151 IS COMPLETED PRIOR TO THE EXPIRATION OF INCENTIVE/DISINCENTIVE TIME DEFINED IN 101.223.

101.223 INCENTIVE/DISINCENTIVE TIME
ALL CRITICAL WORK AS DEFINED IN 101.151 SHALL BE COMPLETED BY 10(TEN) CONSECUTIVE CALENDAR DAYS.

101.251 LIQUIDATED DAMAGES
THE DAILY AMOUNT OF MONEY ESTABLISHED IN 108.07 TO BE DEDUCTED FROM THE CONTRACT AMOUNT FOR ADDITIONAL COSTS, OTHER THAN THE DISINCENTIVE DAILY AMOUNT, INCURRED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S FAILURE TO COMPLETE ALL CONTRACT WORK ON OR BEFORE THE COMPLETION DATE OF THE CONTRACT TIME ESTABLISHED IN THE CONTRACT OR EXTENDED CONTRACT TIME GRANTED UNDER 108.06.

108.04 LIMITATION OF OPERATIONS
ADD THE FOLLOWING:
THE CONTRACTOR IS ADVISED THAT TIME IS OF THE ESSENCE IN THE CONTRACT AND THAT HE WILL BE PERMITTED TO WORK AT NIGHT. SHOULD THE CONTRACTOR'S PROGRESS FALL BEHIND THE APPROVED PROGRESS SCHEDULE THE DEPARTMENT WILL ALLOW AND THE CONTRACTOR SHALL WORK ADDITIONAL CREWS AND/OR ADDITIONAL SHIFTS AS MAY BE NECESSARY IN ORDER TO MAINTAIN THE ACCEPTED PROGRESS SCHEDULE.

108.05 DATE OF COMPLETION
DELETE THE ENTIRE TEXT UNDER THIS ARTICLE OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS AND SUBSTITUTE THE FOLLOWING THEREFORE:

THE CONTRACTOR SHALL COMPLETE THE WORK ON OR BEFORE THE COMPLETION TIME SPECIFIED IN THE CONTRACT. OTHERWISE, THE DEPARTMENT SHALL PROCEED AS PROVIDED IN 108.07 OR 108.08. SHOULD THE CONTRACTOR COMPLETE THE CRITICAL WORK BEFORE OR AFTER THE SCHEDULED DATE OF COMPLETION THEREOF, OR BEFORE OR AFTER AN EXTENDED DATE DETERMINED AS SPECIFIED HEREIN, THE DEPARTMENT SHALL AWARD INCENTIVE PAYMENT OR ASSESS DISINCENTIVE PAYMENT AS PROVIDED IN 109.021.

IF THE CONTRACT IS REVISED IN ANY MATERIAL RESPECT AND IS DETERMINED THAT SAID REVISION WILL CAUSE DELAY IN THE COMPLETION OF THE WORK, THE DEPARTMENT MAY POSTPONE THE COMPLETION DATE FOR THE CONTRACT TIME DEFINED UNDER 101.101 BY THE NUMBER OF CALENDAR DAYS DETERMINED BY THE ENGINEER.

IF THE CONTRACTOR FINDS IT IMPOSSIBLE DUE TO ANY OF THE REASONS SET OUT BELOW TO COMPLETE THE WORK BY THE DATE SPECIFIED OR AS EXTENDED IN ACCORDANCE WITH THE PROVISIONS OF THIS SUBSECTION, THE CONTRACTOR MAY MAKE A WRITTEN REQUEST TO THE ENGINEER FOR AN EXTENSION OF TIME SETTING FORTH THEREIN ONE OR MORE OF THE BELOW LISTED REASONS WHICH THE CONTRACTOR BELIEVE WILL JUSTIFY THE GRANTING OF THEIR REQUEST. REQUESTS FOR EXTENSIONS OF TIME SHALL BE FILED IN WRITING BY THE CONTRACTOR WITH THE ENGINEER NOT LATER THAN TEN(10) DAYS FOLLOWING THE TERMINATION OF THE DELAY. THE CONTRACTOR'S PLEAS THAT INSUFFICIENT TIME WAS SPECIFIED IS NOT A VALID REASON FOR EXTENSION OF TIME.

IF THE ENGINEER FINDS THAT THE WORK WAS DELAYED DUE TO ONE OR MORE OF THE REASONS SET OUT BELOW, THEN THE ENGINEER MAY EXTEND THE TIME FOR COMPLETION OF THE CONTRACT AND, WHERE WARRANTED, THE CRITICAL WORK TIME IN SUCH AMOUNT AS THE CONDITIONS JUSTIFY.

EXTENSIONS OF TIME SHALL NOT BE GRANTED FOR DELAYS IN MATERIAL DELIVERY UNLESS SUCH DELAYS ARE INDUSTRY-WIDE, FOR LABOR STRIKES UNLESS SUCH STRIKES ARE AREA-WIDE AND FOR WEATHER EXCEPT IN CASES OF SITE SPECIFIC FLOODING OR SITE SPECIFIC WIND OR TORNADO DAMAGES.

THERE SHALL BE NO EXTENSION OF TIME GRANTED FOR OTHER WEATHERED-RELATED CONDITIONS, INCLUDING BUT NOT LIMITED TO, INCLEMENT WEATHER. FURTHER, EXTENSIONS OF TIME MAY BE GRANTED FOR INCREASES IN BID ITEM QUANTITIES WHICH EXCEED THE LIMITS SET IN SECTION 5525.14 OF THE OHIO REVISED CODE AND FOR WORK NOT ORIGINALLY CONTEMPLATED BY THE CONTRACT.

EXTENSIONS OF DISINCENTIVE TIME WILL NOT BE GRANTED UNLESS, IN THE DETERMINATION OF THE ENGINEER, ONE OR MORE OF THE ABOVE-REFERENCED EXTRAORDINARY CIRCUMSTANCES OCCUR. THE CONTRACTOR SHALL FULLY JUSTIFY WHY ADDITIONAL MANPOWER AND EQUIPMENT, ADDITIONAL SHIFTS AND EXTENDED WORK DAYS CANNOT BE UTILIZED TO MAINTAIN THE ORIGINAL INCENTIVE/DISINCENTIVE TIME SCHEDULE, AT THE SOLE EXPENSE OF THE CONTRACTOR.

109 ACCEPTANCE, MEASUREMENT AND PAYMENT
ADD THE FOLLOWING:
109.021 INCENTIVE/DISINCENTIVE PAYMENT PLAN

THIS PROJECT CANNOT BE SAFELY AND EFFICIENTLY USED UNTIL THE CRITICAL WORK UNDER THIS CONTRACT, AS DEFINED IN 101.151, IS COMPLETED AND ACCEPTED BY THE DEPARTMENT.

ON THIS BASIS, THE CONTRACTOR SHALL BE ENTITLED TO AN INCENTIVE PAYMENT FOR THE EARLY COMPLETION OF ALL WORK DESIGNATED AS CRITICAL WORK, BEFORE THE EXPIRATION OF THE INCENTIVE/DISINCENTIVE TIME AS DEFINED AND SPECIFIED IN 101.223.

INCENTIVE PAYMENTS WILL BE AWARDED THE CONTRACTOR IN THE AMOUNT OF THE INCENTIVE/DISINCENTIVE DAILY AMOUNT MULTIPLIED BY THE TOTAL NUMBER OF FULL DAYS COMPLETION IS ACHIEVED PRIOR TO EXPIRATION OF THE INCENTIVE/DISINCENTIVE TIME AS DEFINED IN 101.223; HOWEVER THE TOTAL INCENTIVE PAYMENT SHALL NOT EXCEED \$100,000.00.

FAILURE OF THE CONTRACTOR TO COMPLETE WORK AS REQUIRED BY THE CONTRACT BEFORE EXPIRATION OF THE INCENTIVE/DISINCENTIVE TIME AS DEFINED IN 101.223, SHALL RESULT IN ASSESSMENT OF A DISINCENTIVE PAYMENT. DISINCENTIVE PAYMENT SHALL BE DEDUCTED FROM PAYMENTS DUE TO THE CONTRACTOR OR THEIR SURETY. THERE SHALL BE NO MAXIMUM DISINCENTIVE PAYMENT.

SHOULD IT BECOME APPARENT THAT DUE TO SIGNIFICANT DELAYS CAUSED BY THE CONTRACTOR, A DISINCENTIVE PAYMENT WILL BE ASSESSED. THE DEPARTMENT MAY DEVELOP AND IMPLEMENT A WITHHOLDING SCHEDULE TO RETAIN FUNDS FROM PARTIAL PAYMENTS DUE THE CONTRACTOR. IF THE TOTAL DISINCENTIVE PAYMENT EXCEEDS THE AMOUNT PAYABLE TO THE CONTRACTOR, THE EXCESS SHALL BE PAID TO THE STATE BY THE CONTRACTOR OR THEIR SURETY.

SUBSTANTIAL COMPLETION, AS DISCUSSED IN 109.071 AND 109.072, OF CRITICAL WORK SHALL NOT QUALIFY FOR COMPLETION OF CRITICAL WORK. ANY LANE CLOSURE (EASTBOUND OR WESTBOUND) REQUIRED TO COMPLETE ANY ITEM OF WORK AFTER REOPENING TO UNRESTRICTED HIGHWAY TRAFFIC, SHALL BE CAUSE TO REVISE THE COMPLETION DATE OF CRITICAL WORK AND REDUCE INCENTIVE PAYMENT, IF THE CLOSURE OCCURS AFTER THE END OF THE INCENTIVE/DISINCENTIVE, FOR EACH DAY A CLOSURE IS IN EFFECT.

THE INCENTIVE/DISINCENTIVE DAILY AMOUNT WILL BE TEN THOUSAND DOLLARS (\$10,000.00).

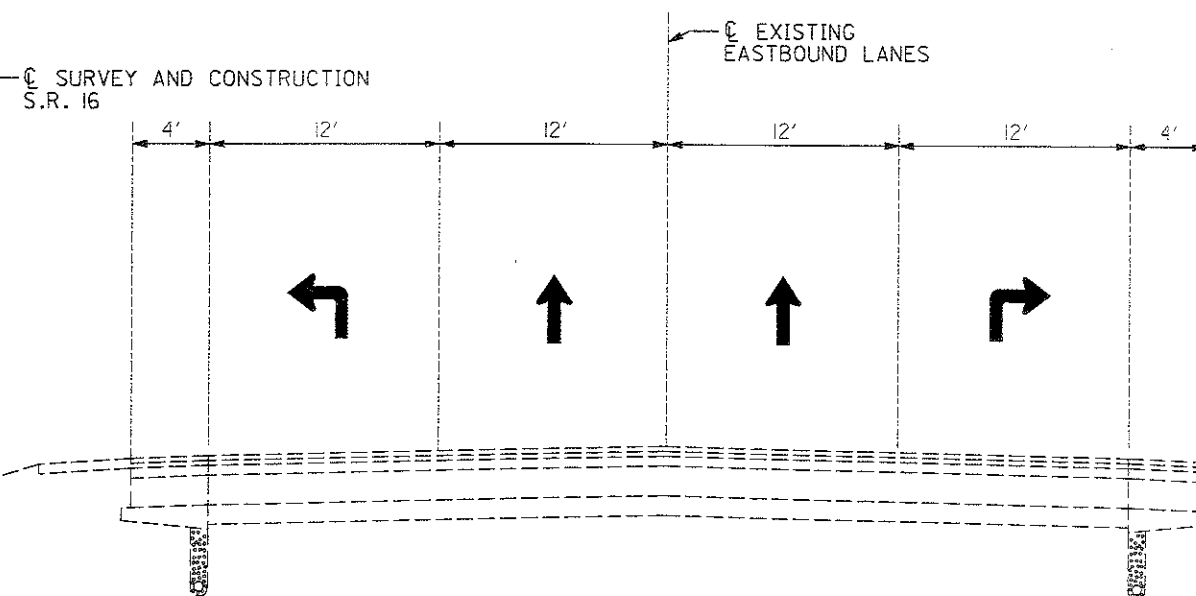
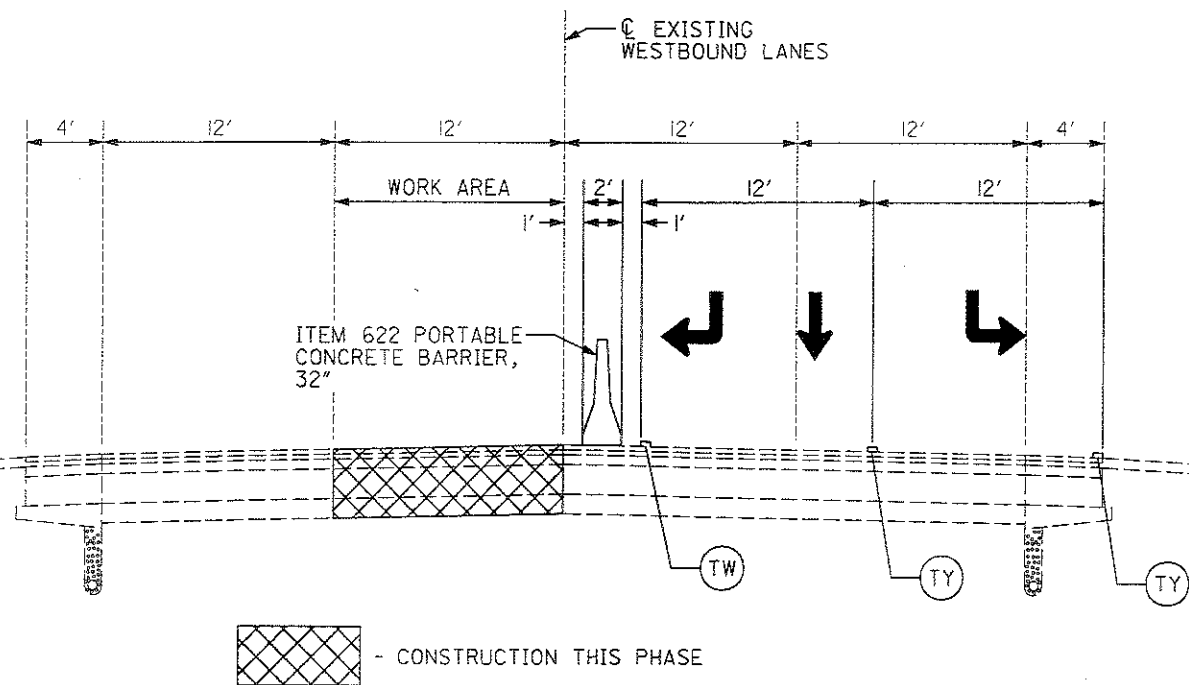
FOR THE PURPOSE OF CALCULATING INCENTIVE/DISINCENTIVE PAYMENTS, A DAY SHALL BE DEFINED AS EACH 24 HOUR PERIOD PRECEDING OR FOLLOWING 12:00 O'CLOCK MIDNIGHT.

THE INCENTIVE PAYMENT DUE OR THE DISINCENTIVE PAYMENT CHARGED THE CONTRACTOR, WILL BE PROCESSED BY CHANGE ORDER AFTER COMPLETION AND ACCEPTANCE OF THE CRITICAL WORK ON THE PROJECT.

SHOULD DELAYS CAUSE THE CRITICAL WORK TO BE COMPLETED AFTER THE COMPLETION DATE AS SET OUT IN THE CONTRACT OR AS EXTENDED PER 108.05, ONLY DISINCENTIVE PAYMENTS WILL BE ASSESSED UNTIL COMPLETION OF CRITICAL WORK. FROM THAT TIME UNTIL COMPLETION OF THE TOTAL PROJECT, LIQUIDATED DAMAGES, AS DEFINED IN 108.07, WILL BE ASSESSED. THE DEPARTMENT WILL NOT ASSESS DISINCENTIVE PAYMENT AND LIQUIDATED DAMAGES CONCURRENTLY.

109.071 ACCEPTANCE - TRAFFIC DETOURED
ADD THE FOLLOWING:

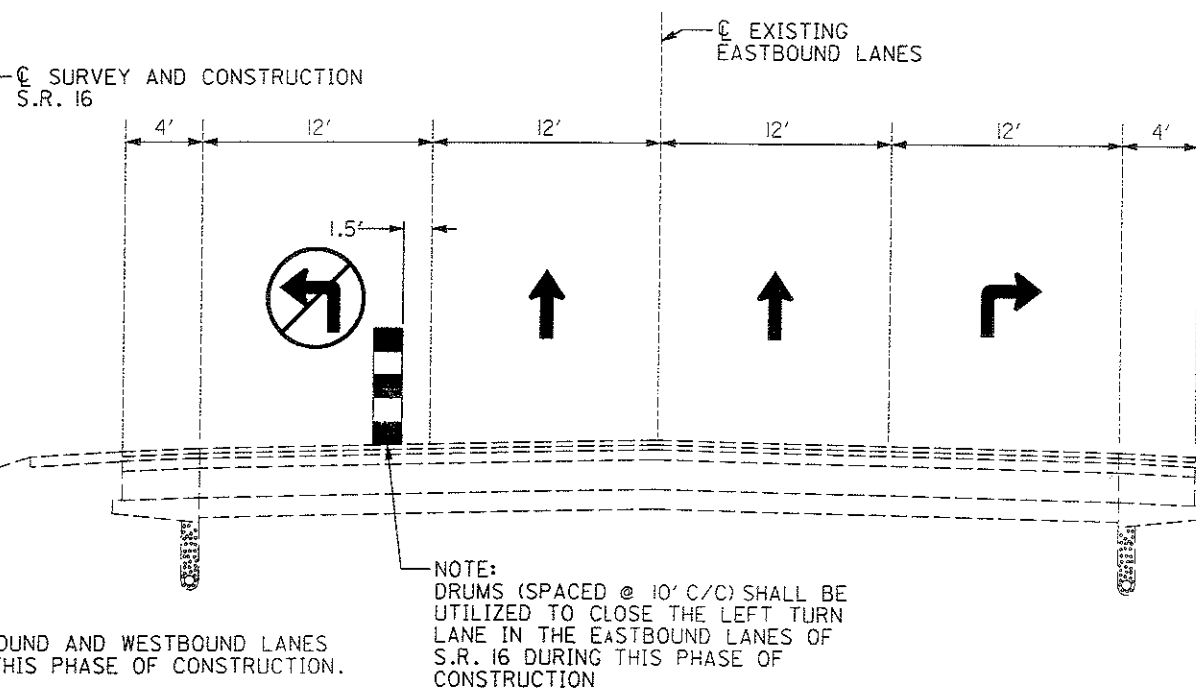
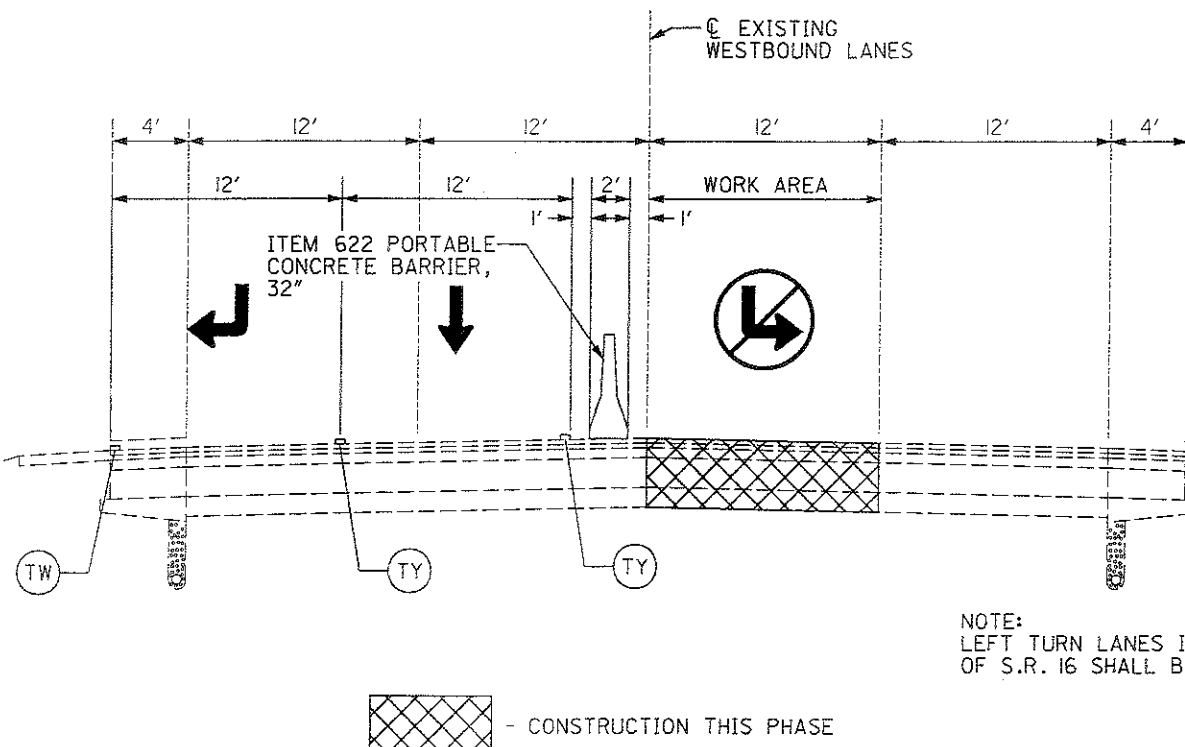
(D) ACCEPTANCE OF CRITICAL WORK
WHEN THE CRITICAL WORK IS COMPLETED TO THE SATISFACTION OF THE ENGINEER AND THE PROJECT IS UNRESTRICTED TRAFFIC, THE DEPARTMENT WILL MAKE A PARTIAL ACCEPTANCE OF THE CRITICAL WORK. THIS WILL RELIEVE THE CONTRACTOR OF THE RESPONSIBILITY FOR MAINTENANCE OF THE COMPLETED WORK, BUT THEY WILL BE REQUIRED TO REPAIR ANY DAMAGE CAUSED BY HIS OPERATIONS, DEFECTIVE WORK, OR NONCOMPLIANCE WITH THE PLANS, SPECIFICATIONS AND CONTRACT, UNTIL THE FINAL ESTIMATE HAS BEEN APPROVED BY THE DEPARTMENT. FINAL ACCEPTANCE OF THE TOTAL PROJECT WILL BE IN ACCORDANCE WITH 109.073.



PHASE 1, STEPS 1 AND 2

(TY) - ITEM 614 TEMPORARY EDGE LINE, CLASS 1, 642 PAINT (YELLOW)

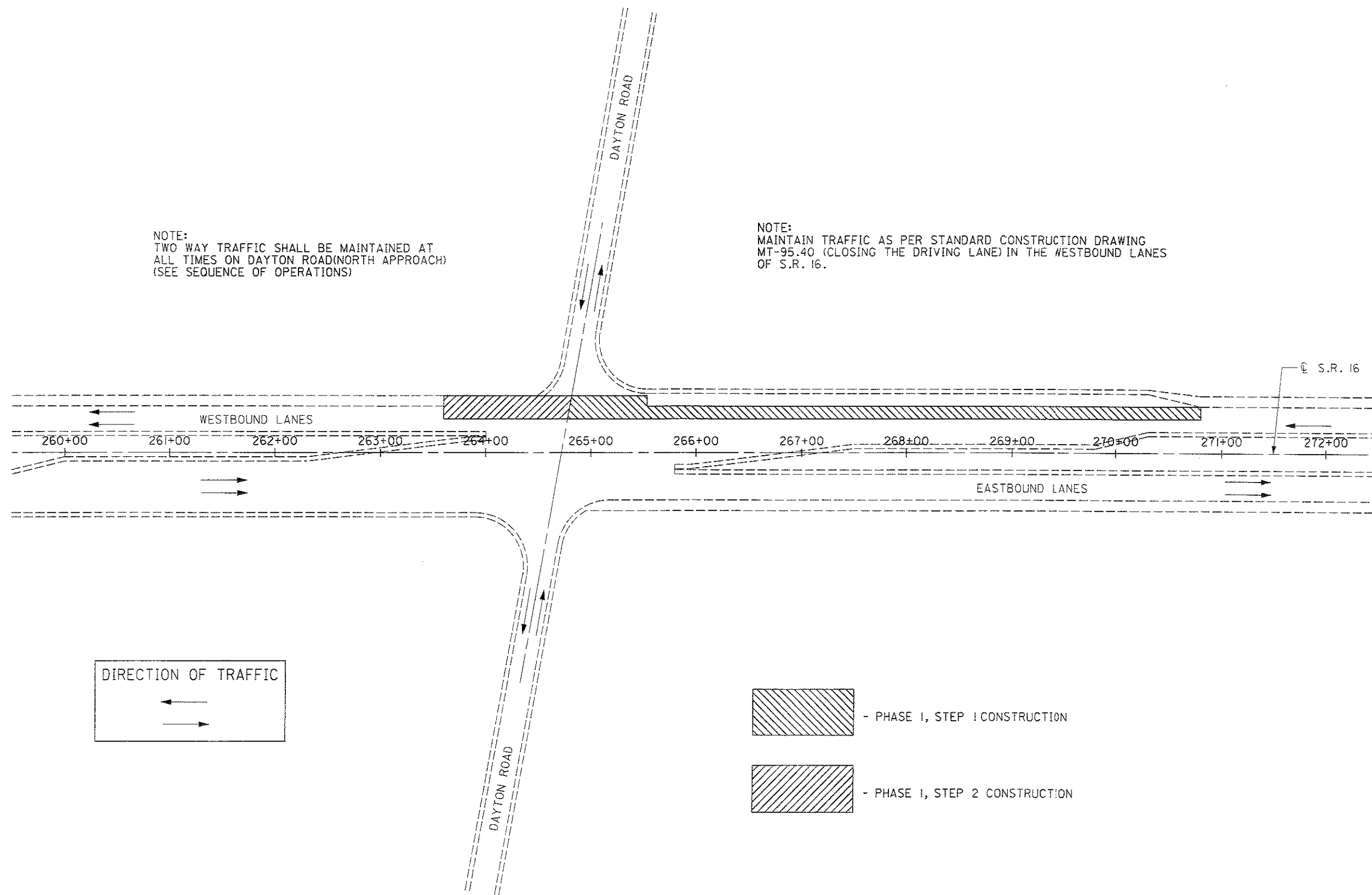
(TW) - ITEM 614 TEMPORARY EDGE LINE, CLASS 1, 642 PAINT (WHITE)

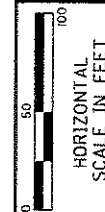
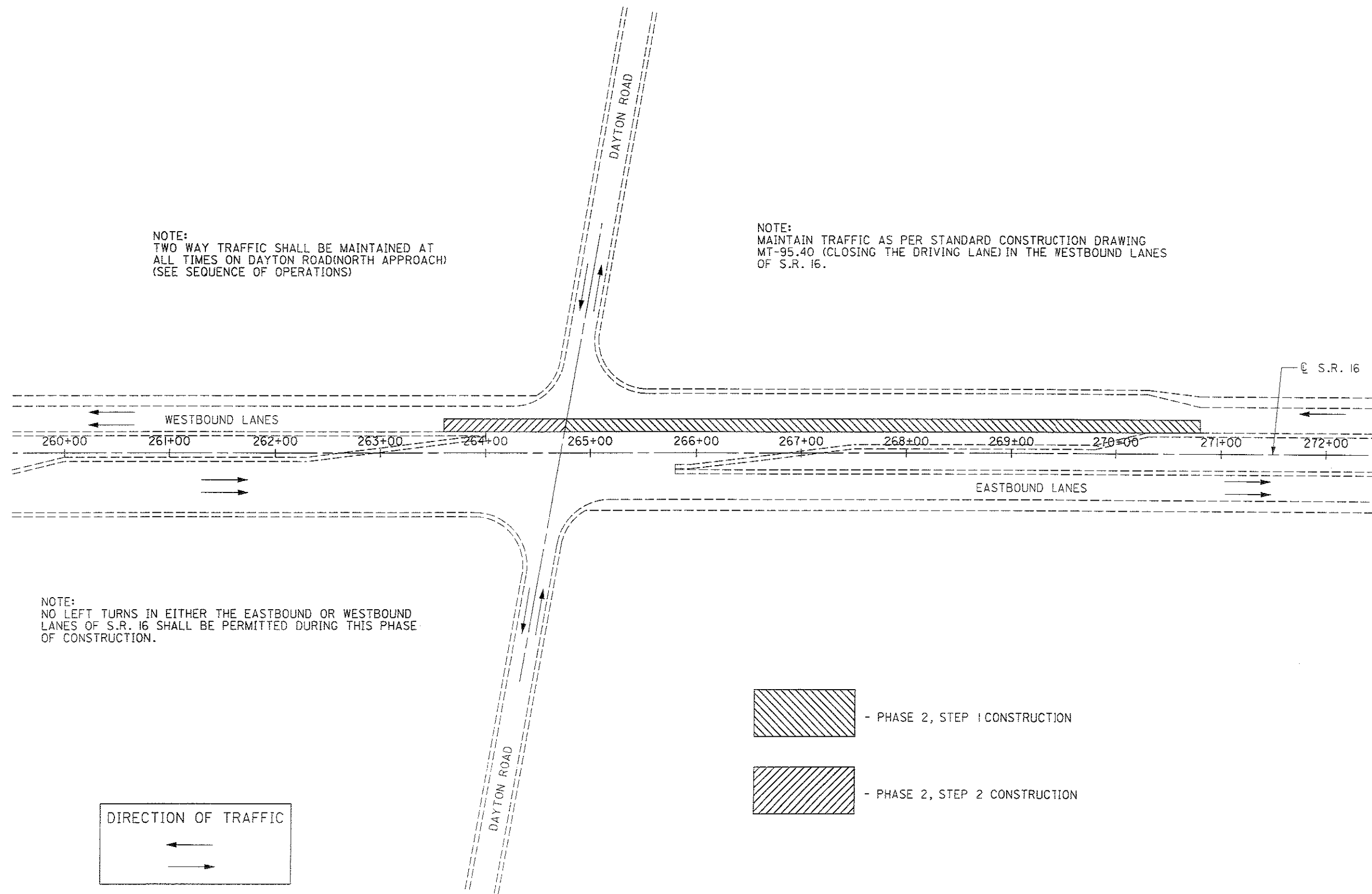


NOTE:
LEFT TURN LANES IN BOTH THE EASTBOUND AND WESTBOUND LANES OF S.R. 16 SHALL BE CLOSED DURING THIS PHASE OF CONSTRUCTION.

NOTE:
DRUMS (SPACED @ 10' C/C) SHALL BE UTILIZED TO CLOSE THE LEFT TURN LANE IN THE EASTBOUND LANES OF S.R. 16 DURING THIS PHASE OF CONSTRUCTION

PHASE 2, STEPS 1 AND 2



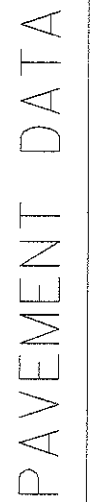


CALCULATED
CHECKED

MAINTENANCE OF TRAFFIC DETAIL SHEET

LIC-16-23.63

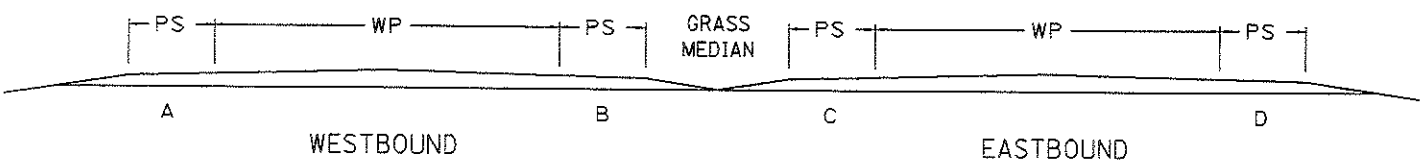
CALCULATED	SAB
CHECKED	LME

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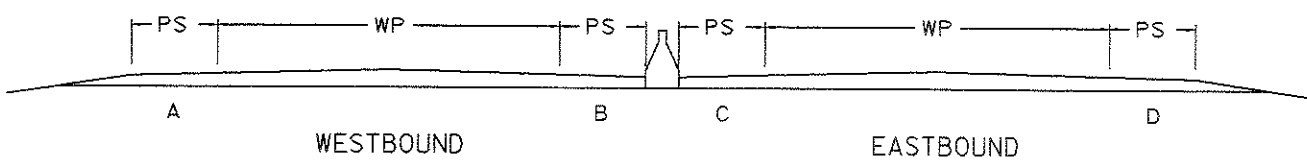
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SHOULDER DATA

TYPICAL 1



TYPICAL 2



* AVERAGE SHOULDER WIDTH THRU SECTION

FIELD MEASURED

NOTE:
IN AREAS OF RIGHT TURN LANES THE PAVED SHOULDER IS 4 FEET WIDE. THE QUANTITIY FOR SHOULDER CORRECTION IS AN ADJUSTMENT FOR NARROWING OF THE SHOULDER. APPROXIMATE TURN LANE LENGTH USED FOR CALCULATIONS ARE: EASTBOUND 489 FEET, WESTBOUND 539 FEET.

DEDUCT FOR BRIDGES
(BRIDGE LENGTH X SHOULDER WIDTH)
LIC-16-2591
79.58' X 26' / 9 = 230 SQ.YD.

(I) BRIDGE LENGTH X SHOULDER WIDTH											PAVED SHOULDER DATA												
L O C A T I O N	ROUTE	LOG POINT TO LOG POINT	LENGTH		T Y P I C A L	PROPOSED WIDTH (FT.)				SHOULDER AREA SQ.YDS.	407	442 ASPHALT CONCRETE			202	254	304		451	617			* N O T E S
			MILES	LIN.FT.		A	B	C	D		TACK COAT @ 0.075 gal./s.y.	THICK	SURFACE COURSE, 12.5MM, TYPE A (446)	PAVEMENT REMOVED, AS PER PLAN	PAVEMENT PLANING, ASPHALT CONCRETE (1.75")	THICK	AGGREGATE BASE	12" REINFORCED CONCRETE FAVEMENT	COMPACTED AGGREGATE, AS PER PLAN 2' X 2.0" AVG. THICKNESS TO BACK UP PAVED BERM				
																				GALLON			
I	SR 16	EASTBOUND																					
		23.76-25.08	1.32	6970	1			4	10	10842	813	1.75	527.0			10842				301			
		25.08-25.66	0.58	3062	2			*6.8	10	5716	429	1.75	277.9			5716				66			
		25.66-28.07	2.41	12725	1			4	10	19794	1484	1.75	962.2			19794				550			
I	SR 16	WESTBOUND																					
		23.63-23.72	0.09	475	1	8	4			633	47	1.75	30.8			633				21			
		23.72-23.75	0.03	158	1	8				140					140		6	24	140				
		23.84-23.86	0.02	#80	1		4			36					36		6	6	36				
		23.86-25.08	1.22	6442	1	8	4			8589	644	1.75	417.5			8589				278			
		25.08-25.66	0.58	3062	2	8	*7			5103	383	1.75	248.1			5103				66			
		25.66-28.07	2.41	12725	1	8	4			16967	1273	1.75	824.8			16967				550			
		DEDUCT FOR BRIDGES (I)								(230)	(17)		(11.2)			(230)				(7)			
		SHOULDER CORRECTION								(566)	(42)		(27.5)			(566)							
		TOTALS									5014		3250		176	66848		30	176	1825			

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SHOULDER DATA

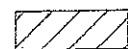
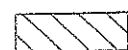
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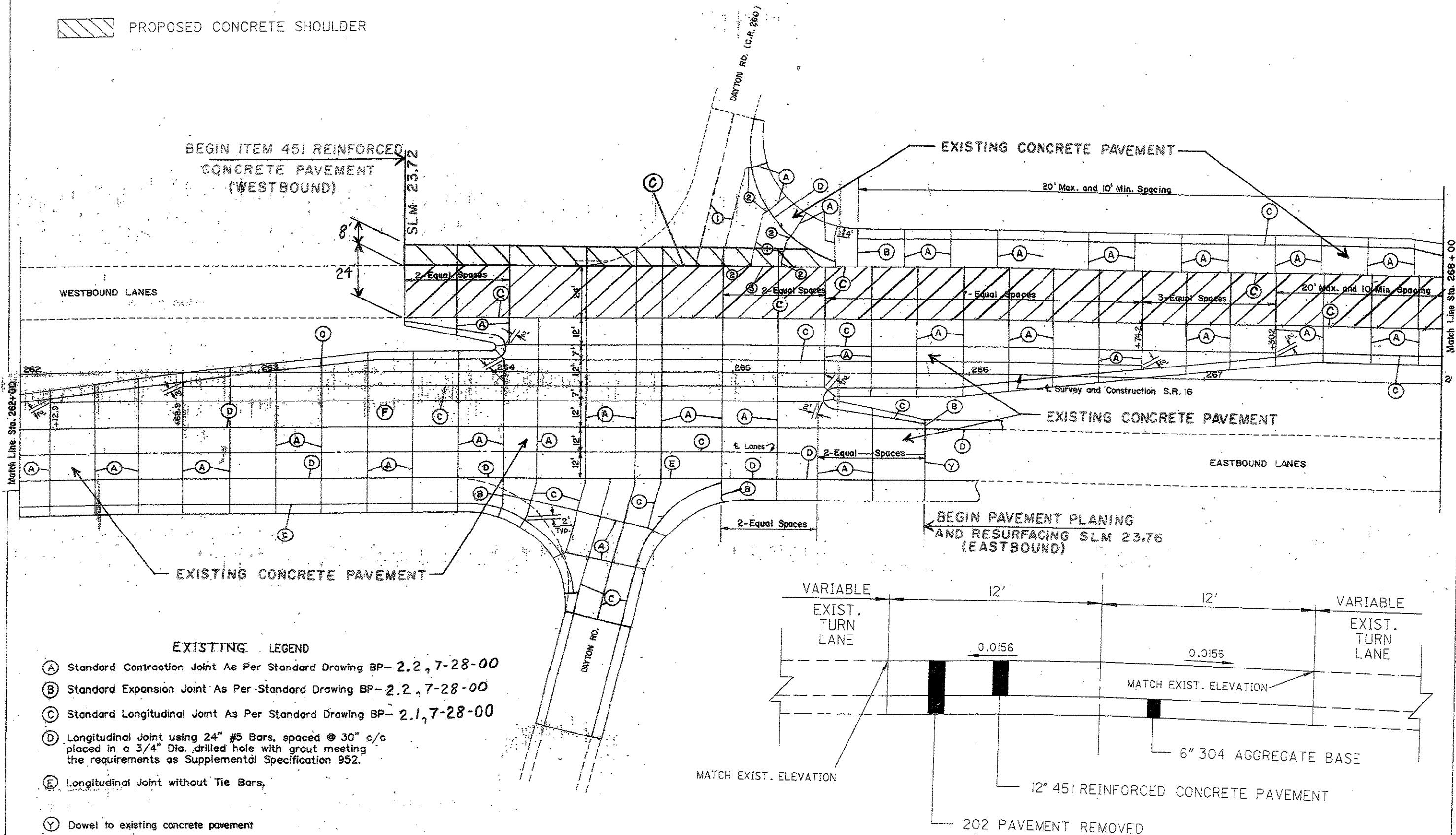


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CALCULATED	CHECKED
SAB	LINE

317	LIC-16-23.63	EXTRA AREAS
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-  PROPOSED CONCRETE MAINLINE
 PROPOSED CONCRETE SHOULDER

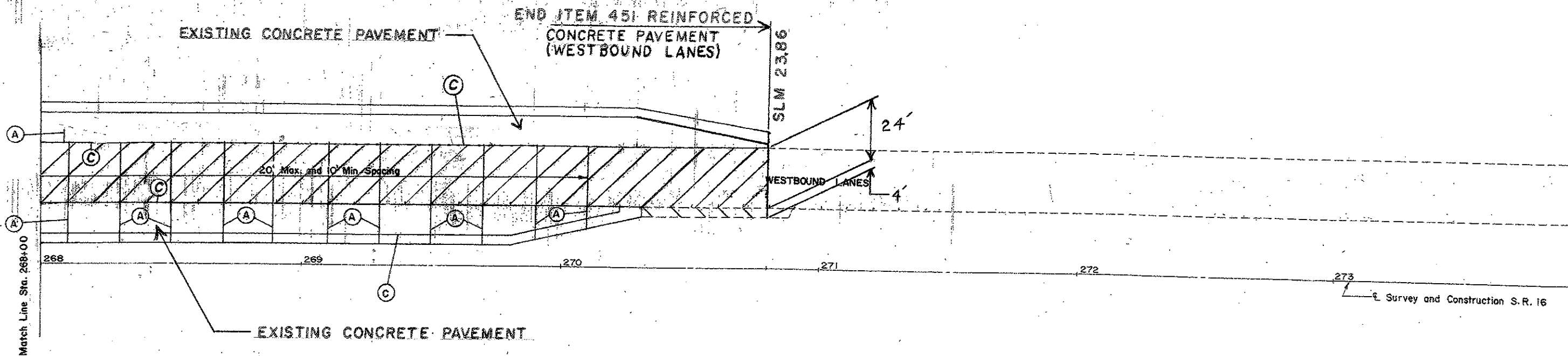


PAVEMENT DETAIL

LIC-16-23.63

PROPOSED CONCRETE MAINLINE

PROPOSED CONCRETE SHOULDER



EXISTING LEGEND

- (A) Standard Contraction Joint As Per Standard Drawing BP-2.2, 7-28-00
- (B) Standard Expansion Joint As Per Standard Drawing BP-2.2, 7-28-00
- (C) Standard Longitudinal Joint As Per Standard Drawing BP-2.1, 7-28-00
- (D) Longitudinal Joint using 24" #5 Bars, spaced @ 30" c/c placed in a 3/4" Dia. drilled hole with grout meeting the requirements as Supplemental Specification 952.
- (E) Longitudinal Joint without Tie Bars
- (Y) Dowel to existing concrete pavement



HORIZONTAL
SCALE IN FEET

CALCULATED
CHECKED

PAVEMENT DETAIL

LIC-16-23.63

19
31

ITEM 646 EPOXY PAVEMENT MARKING

THIS ITEM SHALL BE USED TO REPLACE PAVEMENT MARKINGS ON CONCRETE PAVEMENT AT DAYTON RD. INTERSECTION. EXISTING MARKINGS SHALL BE REMOVED BY AN APPROVED METHOD BEFORE PLACING NEW EPOXY MARKINGS. THE FOLLOWING QUANTITIES ARE CARRIED TO THE GENERAL SUMMARY FOR THE PEURPOSE DESCRIBED ABOVE.

EASTBOUND: SLM 23.63-23.76 = 0.13 MILE
WESTBOUND: SLM 23.72-23.86 = 0.14 MILE

ITEM 646 EDGE LINE - 0.54 MILE
ITEM 646 LANE LINE - 0.27 MILE
ITEM 644 REMOVAL OF PAVEMENT MARKING - 2112 FEET

CALCULATED
LME
CHECKED
TJD

EDGE/LANE LINE SUB-SUMMARY

LIC-16-23.63

EDGE LINE SUB-SUMMARY ITEM 644 THERMOPLASTIC

LOCATION	COUNTY	ROUTE	S.L.M.		WHITE EDGE LINE QU.			YELLOW EDGE LINE QU.			PARTICIPATION TYPE				EDGE LINE MILE	REMARKS
			FROM	TO	MILE	HIGHWAY	RAMP	MILE	HIGHWAY	RAMP	IRG	FG	RSG	NON FED STATE		
I	LIC	SR 16	EASTBOUND													
			23.76	28.07	4.31	4.31		4.31	4.31						8.62	
			WESTBOUND													
			23.63	23.72	0.09	0.09		0.09	0.09						0.18	
			23.86	28.07	4.21	4.21		4.21	4.21						8.42	
			TOTALS												17.22	

LANE LINE SUB-SUMMARY ITEM 644 THERMOPLASTIC

LOCATION	COUNTY	ROUTE	S.L.M.		LANE LINE QUANTITIES									LANE LINE MILE	REMARKS
			FROM	TO	TOTAL MILE	4 INCH LANE LINE									
I	LIC	SR 16	EASTBOUND												
			23.76	28.07	4.31	4.31							4.31		
			WESTBOUND												
			23.63	23.72	0.09	0.09							0.09		
			23.86	28.07	4.21	4.21							4.21		
			TOTALS										8.61		

CALCULATED SAB	CHECKED LME
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PAVEMENT MARKING

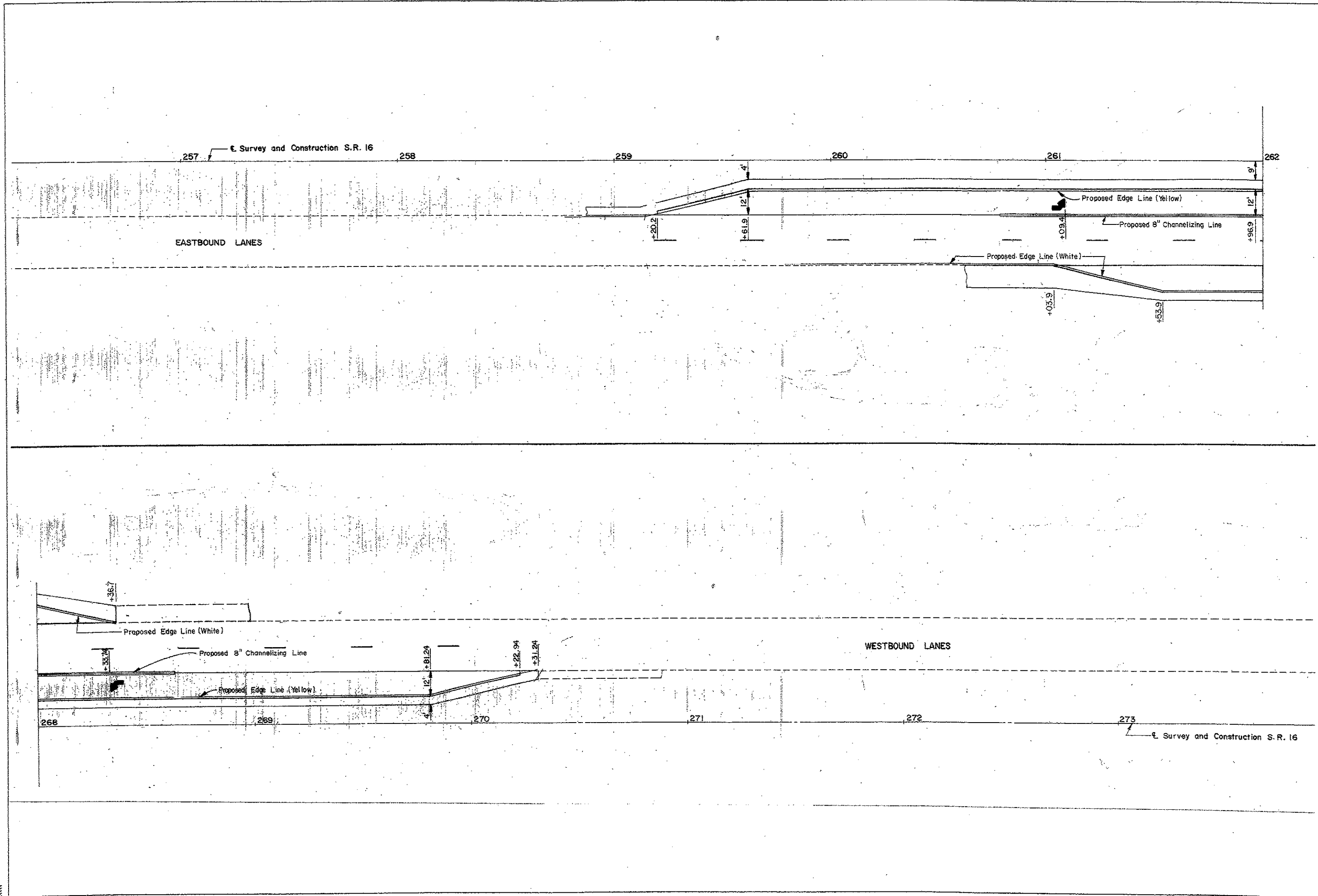
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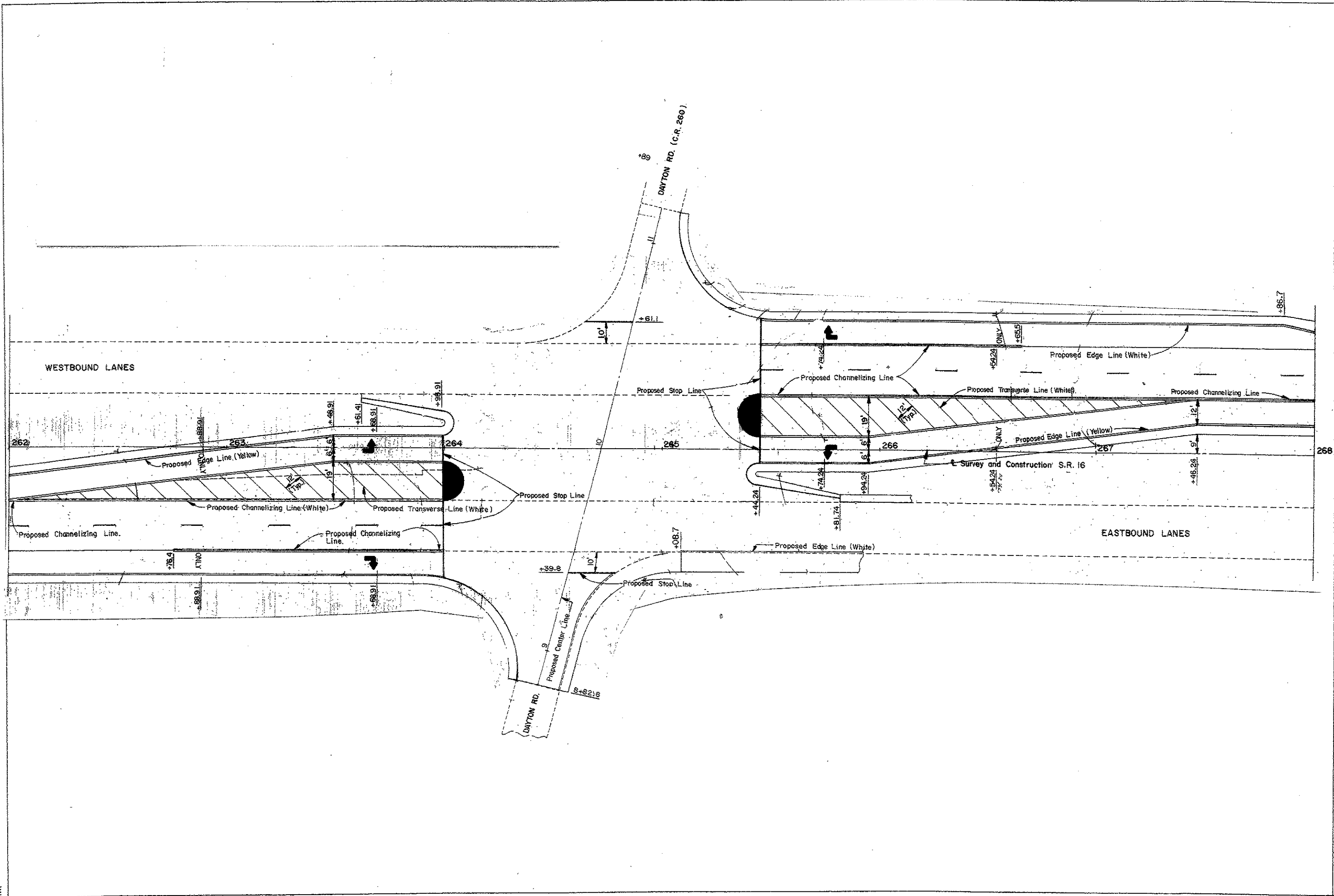
CALCULATED SAB	CHECKED LME
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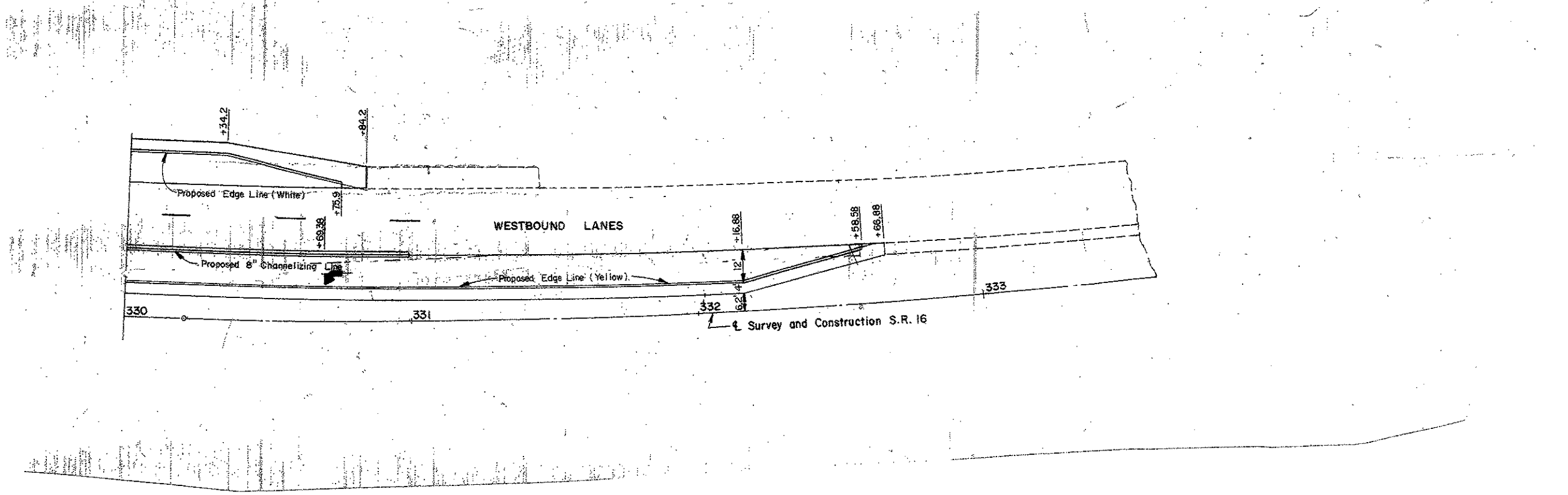
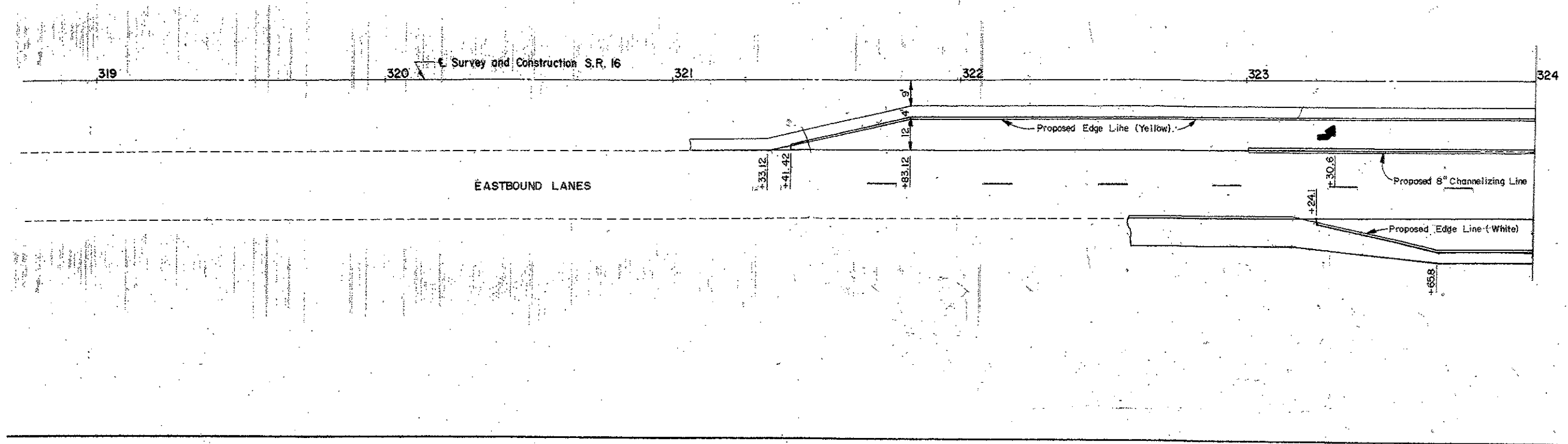
PAVEMENT MARKING

LIC-16-23.63

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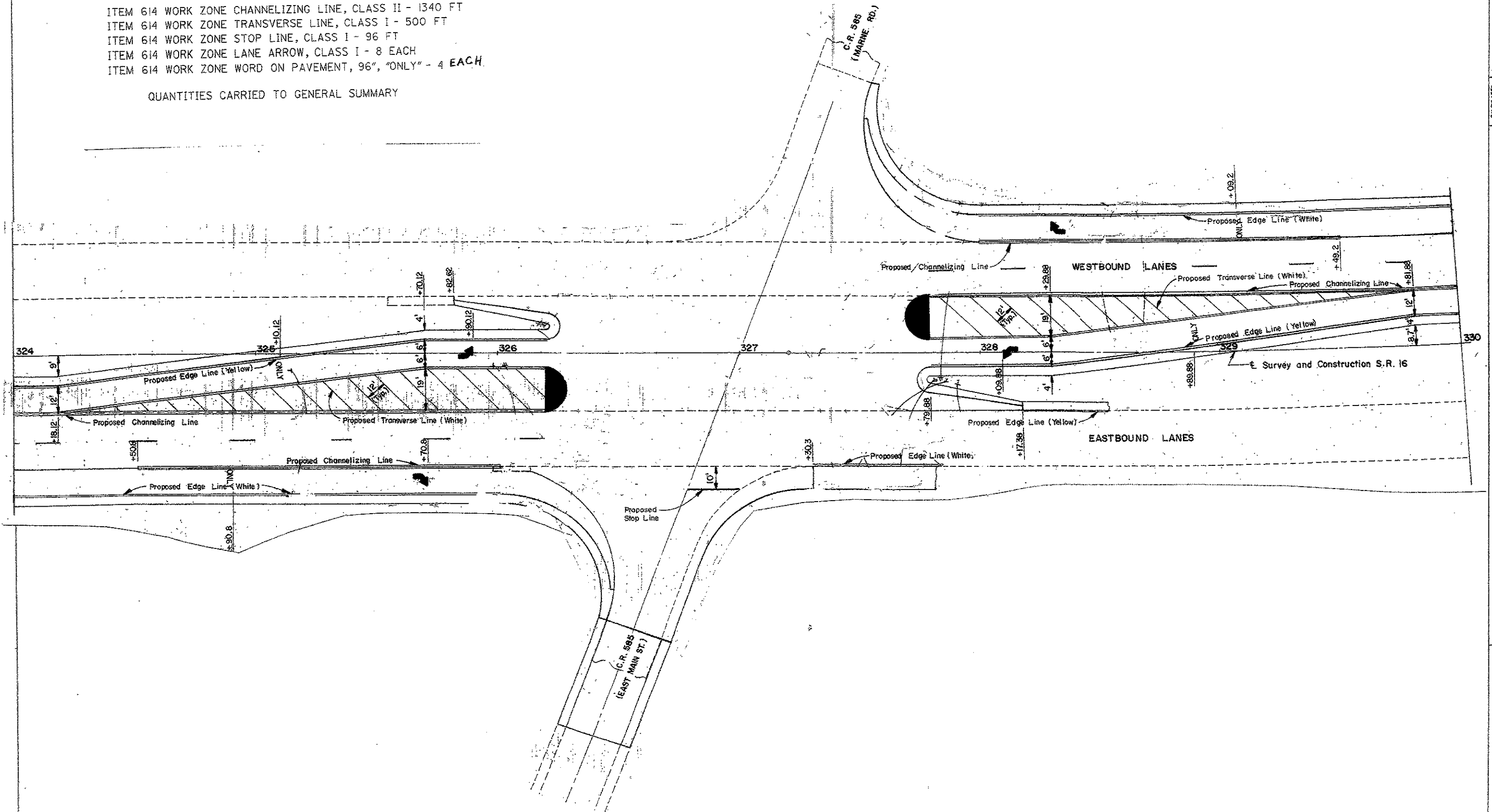




WORK ZONE PAVEMENT MARKING

- ITEM 614 WORK ZONE CHANNELIZING LINE, CLASS II - 1340 FT
- ITEM 614 WORK ZONE TRANSVERSE LINE, CLASS I - 500 FT
- ITEM 614 WORK ZONE STOP LINE, CLASS I - 96 FT
- ITEM 614 WORK ZONE LANE ARROW, CLASS I - 8 EACH
- ITEM 614 WORK ZONE WORD ON PAVEMENT, 96", "ONLY" - 4 EACH

QUANTITIES CARRIED TO GENERAL SUMMARY

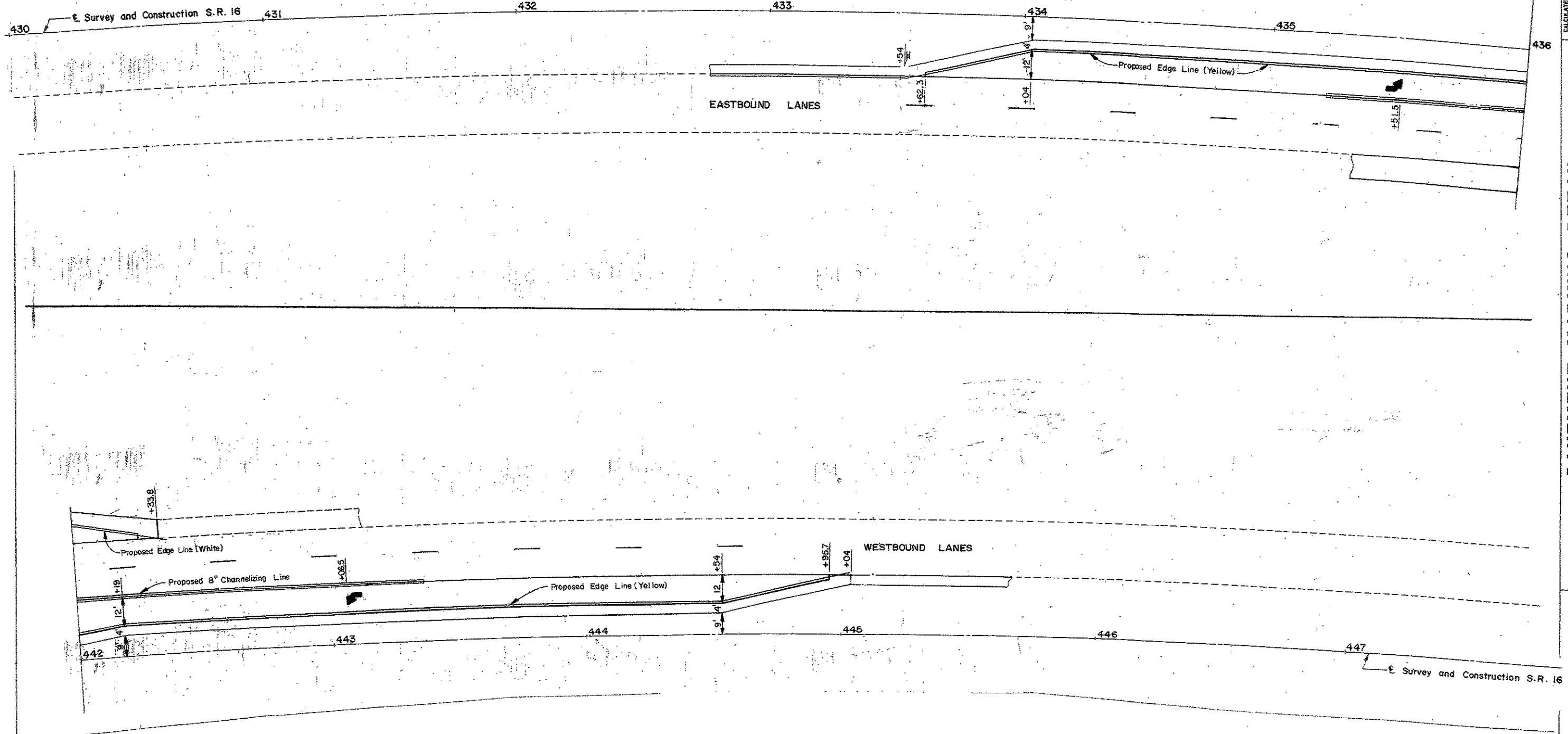


HORIZONTAL
SCALE IN FEET

CALCULATED
CHECKED

PAVEMENT MARKING DETAIL

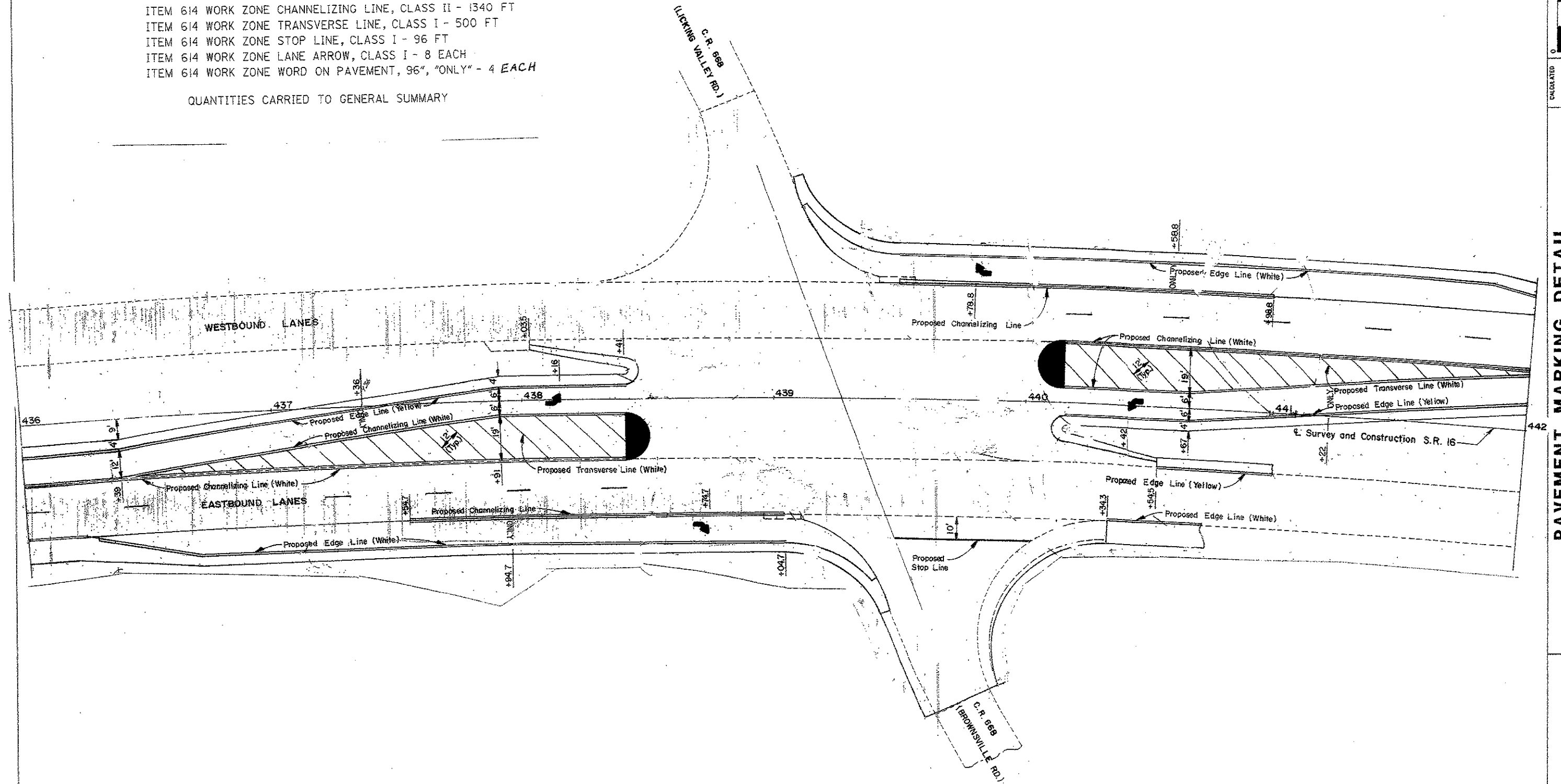
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WORK ZONE PAVEMENT MARKING

- ITEM 614 WORK ZONE CHANNELIZING LINE, CLASS II - 1340 FT
- ITEM 614 WORK ZONE TRANSVERSE LINE, CLASS I - 500 FT
- ITEM 614 WORK ZONE STOP LINE, CLASS I - 96 FT
- ITEM 614 WORK ZONE LANE ARROW, CLASS I - 8 EACH
- ITEM 614 WORK ZONE WORD ON PAVEMENT, 96", "ONLY" - 4 EACH

QUANTITIES CARRIED TO GENERAL SUMMARY



PAVEMENT MARKING DETAIL

LIC-16-23.63

28
31

CHKD. BY _____
DATE _____

CALCULATED	CHECKED
SAB	LNE

DETAIL	
4	4 LANE DIVIDED TO 2 LANE TRANSITION
5	4 LANE UNDIVIDED TO 2 LANE TRANSITION
6	ONE LANE BRIDGE
7	STOP APPROACH
8	THRU APPROACH
9	TWO WAY LEFT TURN LANE

DETAIL	
10	APPROACH W/LT. TURN LANE
11	HORIZONTAL CURVE 40' (NOTE 2)
12	HORIZONTAL CURVE ALT. (NOTE 3)
GAP	CENTERLINE AT 80' TYP.

[illegible]

RPM LOCATION SUB-SUMMARY

LIC-16-23.63

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[illegible]

