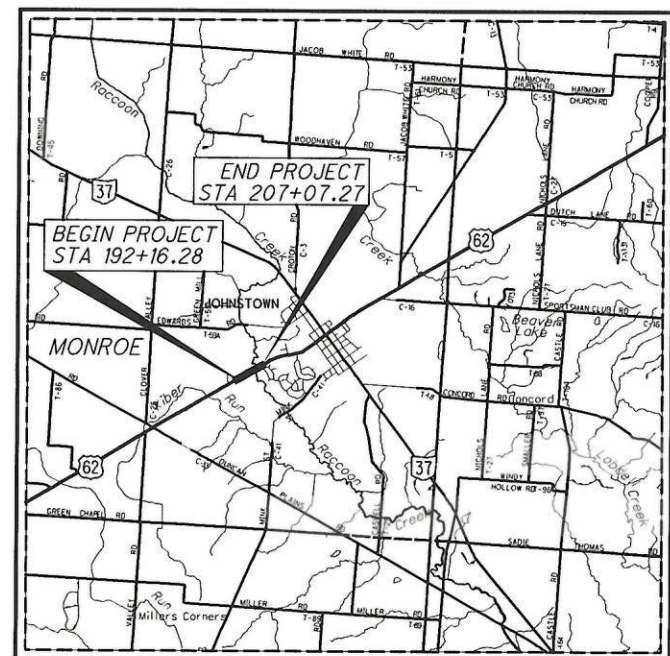


LIC - US 62-03.64  
 220243 PID - 110861  
 Dist 5 4/14/2022

Contract Proposal available @  
 www.contracts.dot.state.oh.us

693-29-C17



**LOCATION MAP**  
 LATITUDE: 40°08'52"N LONGITUDE: 82°42'01"W

PORTION TO BE IMPROVED	-----
INTERSTATE HIGHWAY	-----
FEDERAL ROUTES	-----
STATE ROUTES	-----
COUNTY & TOWNSHIP ROADS	-----
OTHER ROADS	-----

**DESIGN DESIGNATION**

CURRENT ADT (2020)	16,500
DESIGN YEAR ADT (2040)	18,000
DESIGN HOURLY VOLUME (2040)	1,800
DIRECTIONAL DISTRIBUTION	63%
TRUCKS (24 HOUR B&C)	6%
DESIGN SPEED	40 MPH
LEGAL SPEED	35 MPH
DESIGN FUNCTIONAL CLASSIFICATION:	
03 PRINCIPAL ARTERIAL (URBAN)	
NHS PROJECT	YES

**DESIGN EXCEPTIONS**

NONE

**ADA DESIGN WAIVERS**

NONE

**UNDERGROUND UTILITIES**  
 Contact Two Working Days Before You Dig

OHIO811. 8-1-1. or 1-800-362-2764  
 (Non members must be called directly)

PLAN PREPARED BY:  
 IBI GROUP  
 8101 North High Street  
 Columbus OH 43235  
 tel 614 818 4900 fax 614 818 4901

**ENGINEER'S SEAL:**  
 # ROADWAY

SIGNED: Jeffrey D. Hackenbracht  
 DATE: 01-05-22

**ENGINEER'S SEAL:**  
 TRAFFIC SIGNALS

SIGNED: Harry Fox III  
 DATE: 12-01-21

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# FOR ENTIRE PLAN, EXCEPT TRAFFIC SIGNALS AND RIGHT OF WAY

STANDARD CONSTRUCTION DRAWINGS						SUPPLEMENTAL SPECIFICATIONS		SPECIAL PROVISIONS	
BP-3.1	1/17/20	HW-2.1	7/20/18	HL-30.11	1/15/21	TC-81.22	7/16/21	800-2019	10/15/21
BP-4.1	7/19/13	HW-2.2	7/20/18	HL-30.22	1/15/21	TC-83.10	1/17/20	809	10/15/21
BP-5.1	7/16/21					TC-83.20	7/21/17	815	4/16/21
BP-7.1	7/17/20	MGS-1.1	7/16/21	TC-21.21	7/16/21	TC-85.10	4/17/20	816	10/18/19
		MGS-2.1	1/19/18	TC-41.20	10/18/13	TC-85.20	7/20/18	832	10/19/18
CB-2-2A-C	7/16/21	MGS-3.1	1/19/18	TC-41.30	10/18/13			836	1/19/18
CB-2-3,2-4	7/16/21	MGS-4.2	7/19/13	TC-41.40	10/18/13	MT-97.10	4/19/19	875	1/18/19
CB-3A	7/16/21			TC-41.41	7/19/19	MT-97.12	1/20/17	895	4/18/14
		MH-1	7/16/21	TC-42.20	10/18/13	MT-99.20	4/19/19	906	10/15/10
DM-1.1	7/17/20	MH-2	7/16/21			MT-101.90	7/17/20	907	10/18/19
DM-1.2	7/16/21	MH-3	7/16/21	TC-52.10	10/18/13	MT-105.10	1/17/20	909	10/15/21
DM-4.1	7/17/20			TC-52.20	1/15/21			995	7/17/15
DM-4.2	7/20/12	RM-2.1	7/19/13	TC-61.10	1/17/20				
DM-4.4	1/15/16	RM-4.5	7/21/17	TC-61.30	7/19/19				
		RM-4.6	7/19/13	TC-71.10	7/16/21				

**FEDERAL PROJECT NUMBER**  
 E191 (047)

**RAILROAD INVOLVEMENT**  
 NONE

**PROJECT DESCRIPTION**

IMPROVEMENT OF 0.3 MILES OF US 62 INCLUDING:  
 WIDENING OF US 62 FOR TWO-WAY LEFT TURN LANE BETWEEN VILLAGE SQUARE SHOPPING CENTER TO WESTVIEW DRIVE. ADDING CURB AND GUTTER ALONG WITH UPGRADING OF DRAINAGE SYSTEM AND THE SIGNAL AT VILLAGE SQUARE SHOPPING CENTER.

**EARTH DISTURBED AREAS**

PROJECT EARTH DISTURBED AREA:	1.57 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA:	0.25 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA:	4.9 ACRES

**2019 SPECIFICATIONS**

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

APPROVED: Jason Z. Stuger  
 DATE 1/6/2022 DISTRICT DEPUTY DIRECTOR

APPROVED: Jack Mordbanks  
 DATE 2-25-22 DIRECTOR, DEPARTMENT OF TRANSPORTATION

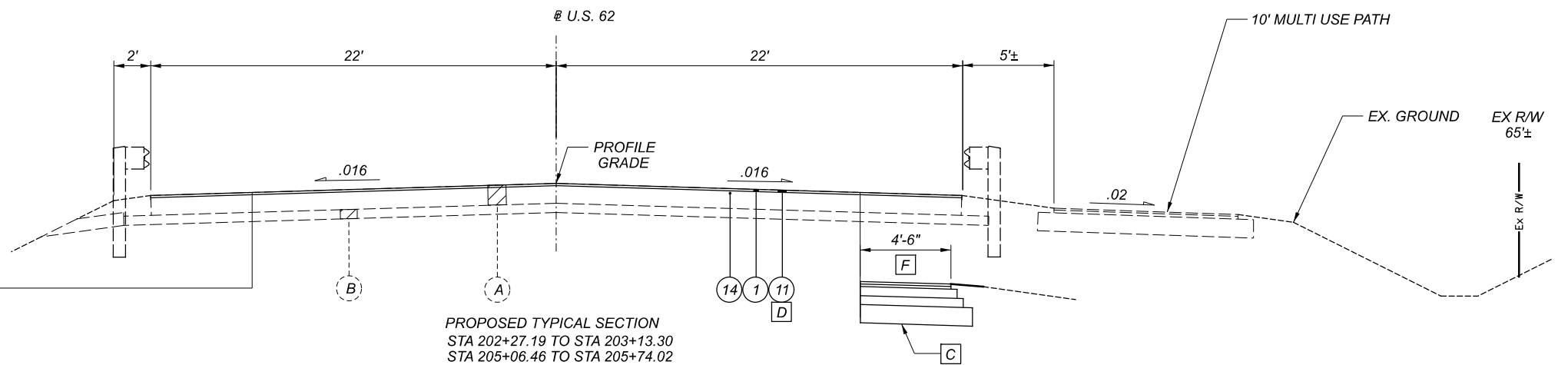
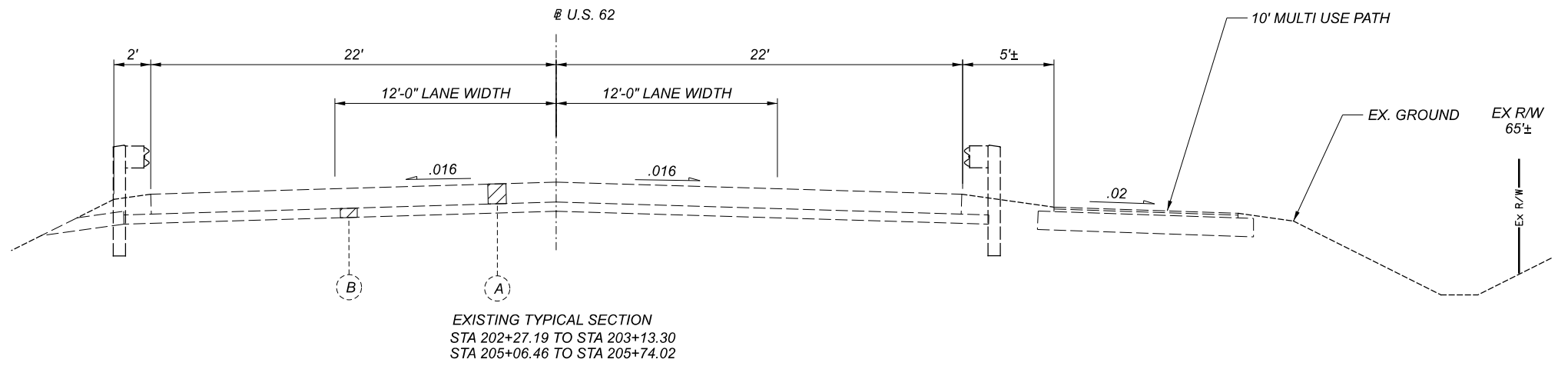
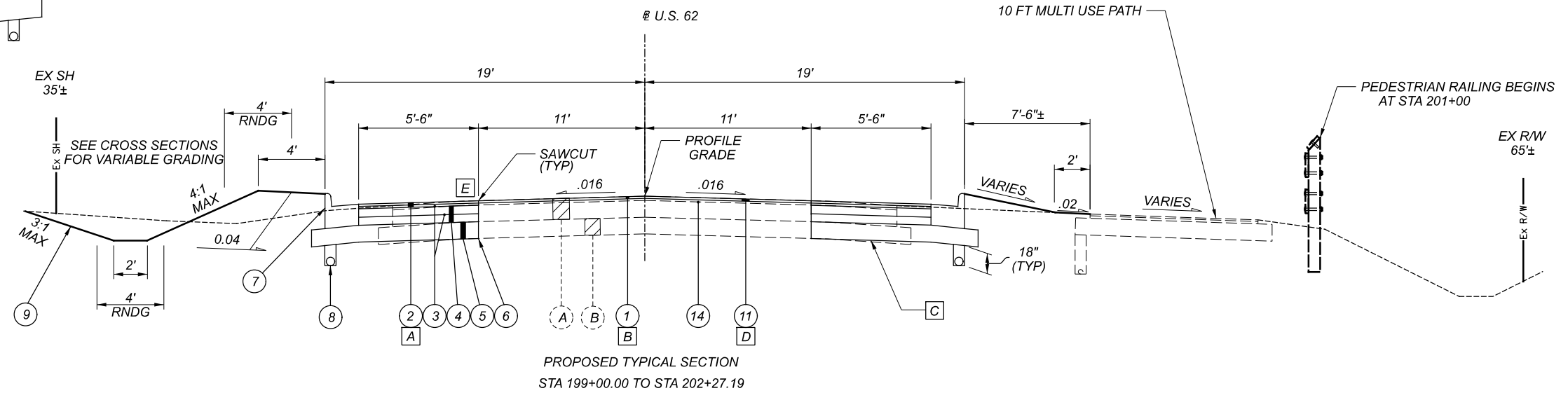
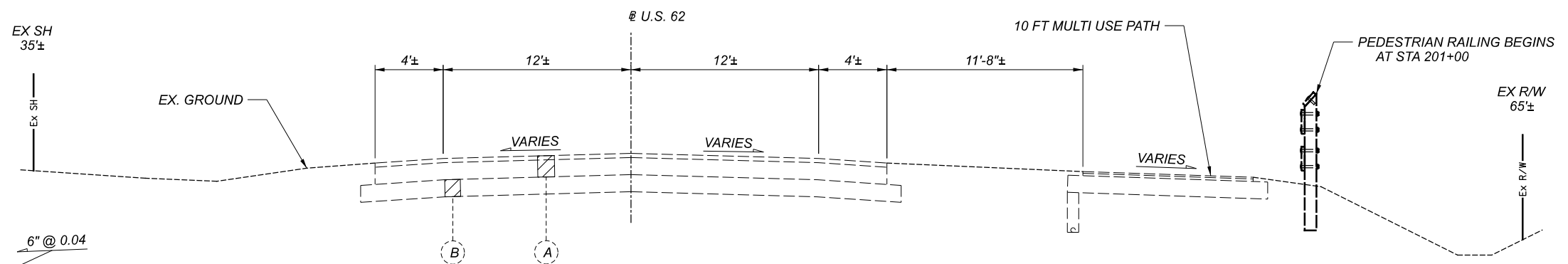
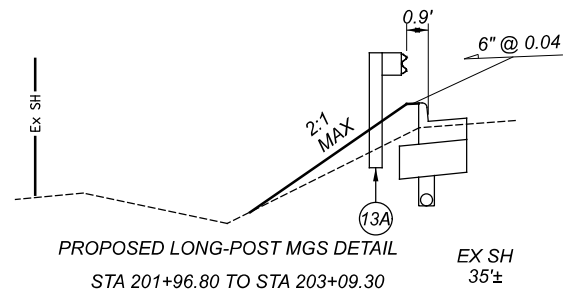
DESIGN AGENCY	IBI
DESIGNER	AJP
REVIEWER	JDH 12-20-21
PROJECT ID	110861
SHEET TOTAL	P.1   89

LIC-62-3.64 PID#110861





FOR LEGEND SEE SHEET P.3



- A** PLACE 3" OF INTERMEDIATE LAYER TO THE LEVEL OF THE EXISTING PAVEMENT
- B** AT END OF BOTH PHASES OF WIDENING, PLANE 1.5" AND RESURFACE ENTIRE PAVEMENT SURFACE BETWEEN CONCRETE GUTTERS. SEE MOT SEQUENCE OF CONSTRUCTION FOR MORE INFORMATION
- C** REFER TO OPPOSITE SIDE FOR PROPOSED PAVEMENT BUILDUP
- D** THE INTENT OF THIS PROJECT IS TO RE-ESTABLISH A UNIFORM CROSS SLOPE OF 0.016. THE CONTRACTOR SHALL ADJUST MILLING TO ACCOMPLISH THE INTENDED 0.016 CROSS SLOPE.
- E** SEE WIDENING DETAIL SHEET P.3.
- F** FROM STA 202+27.19 TO STA 202+58.00 LT.  
FROM STA 202+27.19 TO STA 202+38.00 RT.
- G** FROM STA 202+27.19 TO STA 203+12.51 LT.

DESIGN AGENCY



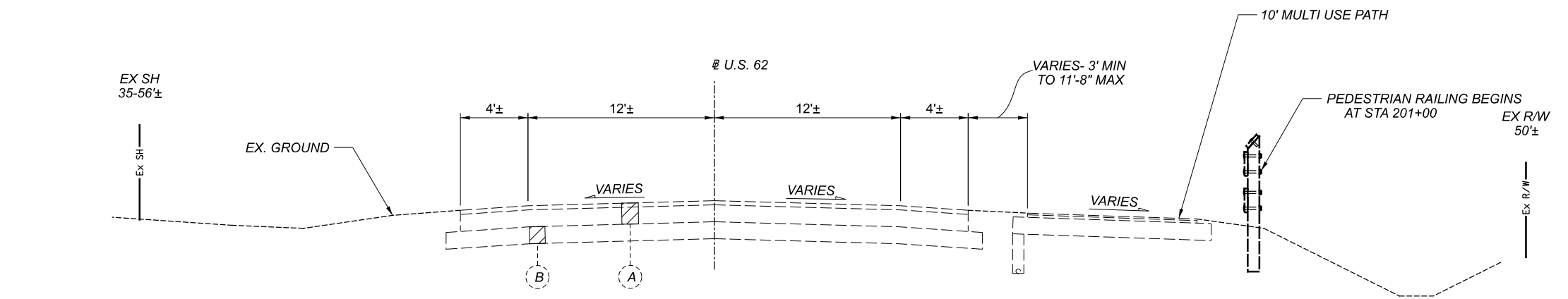
DESIGNER  
AJP

REVIEWER  
JDH 10-29-21

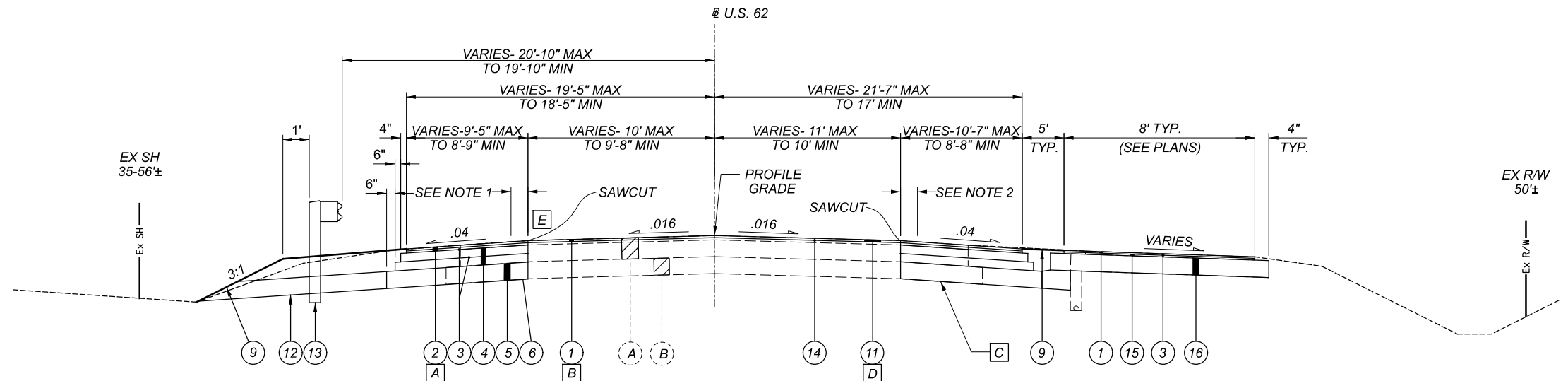
PROJECT ID  
110861

SHEET TOTAL  
P.4 89

FOR LEGEND SEE SHEET P.3



EXISTING TYPICAL SECTION U.S. 62  
STA 205+74.02 TO 207+07.27



PROPOSED TYPICAL SECTION U.S. 62  
STA 205+74.18 TO 207+07.27

- A** PLACE 3" OF INTERMEDIATE LAYER TO THE LEVEL OF THE EXISTING PAVEMENT.
- B** AT END OF BOTH PHASES OF WIDENING, PLANE 1.5" AND RESURFACE ENTIRE PAVEMENT SURFACE BETWEEN OUTSIDE EDGE OF SHOULDERS. SEE MOT SEQUENCE OF CONSTRUCTION FOR MORE INFORMATION.
- C** REFER TO OPPOSITE SIDE FOR PROPOSED PAVEMENT BUILDUP.
- D** THE INTENT OF THIS PROJECT IS TO RE-ESTABLISH A UNIFORM CROSS SLOPE OF 0.016. THE CONTRACTOR SHALL ADJUST MILLING TO ACCOMPLISH THE INTENDED 0.016 CROSS SLOPE.
- E** SEE WIDENING DETAIL SHEET P.3.

NOTES  
1. 1' MIN TO 3' MAX @ 0.016  
2. 1' MIN TO 3' MAX @ 0.016

LIC-62-3.64

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TYPICAL SECTIONS STA. 205+74.02 TO STA. 207+07.27

DESIGN AGENCY



DESIGNER	AJP
REVIEWER	JDH
PROJECT ID	110861
SHEET	TOTAL
P.5	89

ROUNDING

THE ROUNDING AT SLOPE BREAKPOINTS SHOWN ON THE TYPICAL SECTIONS APPLIES TO ALL CROSS-SECTIONS, EVEN THOUGH OTHERWISE SHOWN.

UTILITY COORDINATION SUMMARY

THE LOCATION OF THE UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE AS OBTAINED FROM THE OWNERS AS REQUIRED BY SECTION 153.64 O.R.C.

AMERICAN ELECTRIC POWER CO. (DISTRIBUTION)  
777 HOPEWELL DRIVE  
HEATH, OHIO 43056  
ATTN: PAUL PAXTON  
740-348-5322  
PTPAXTON@AEP.COM

AEP SOLUTION CENTER (SERVICES AND STREET LIGHTING)  
1-800-277-2177

CENTURY LINK TELEPHONE  
2025 AKRON ROAD  
WOOSTER, OHIO 44691  
ATTN: JEFF SCHOONOVER  
740-263-2819  
JEFFREY.L.SCHOONOVER@LUMEN.COM

COLUMBIA GAS OF OHIO  
3550 JOHNNY APPLESEED  
COLUMBUS, OHIO 43231  
ATTN: ERIN PFEIFER  
614-381-1458  
EPFEIFER@NISOURCE.COM

CITY OF JOHNSTOWN  
395 WEST JERSEY STREET  
JOHNSTOWN, OHIO 43031  
ATTN: JACK LIGGETT  
740-967-4746  
JLIGGETT@JOHNSTOWNOHIO.ORG

HORIZON NETWORK PARTNERS  
68 EAST MAIN STREET  
CHILLICOTHE, OH 45061  
ATTN: ROGER STEELE JR.  
740-804-7333  
ROGER.STEELEJR@HORIZONCONNECTS.COM  
CC: HNOC@HORIZONCONNECTS.COM  
ATTN: JIM LUMP  
JIM.LUMP@HORIZONCONNECTS.COM  
ATTN: GARY RAY  
W.RAY@HORIZONCONNECTS.COM

SPECTRUM CABLE TV  
PO BOX 2553  
COLUMBUS, OH 43015  
ATTN: JOSEPH VLOCK  
614-481-5066  
JOSEPH.VLOCK@CHARTER.COM

LICKING RURAL ELECTRIFICATION INC.  
11339 MT. VERNON ROAD  
P.O. BOX 455  
UTICA, OH 43080  
ATTN: JOHN STRATHMAN  
740-348-1149  
JSTRATHMAN@THEENERGYCOOP.COM

CENTURY LINK UTILITY COORDINATION

THE CONTRACTOR SHALL PROVIDE CENTURY LINK (SEE CONTACT INFORMATION ON THIS SHEET) AND THE PROJECT ENGINEER A MINIMUM OF 7-DAY NOTICE WHEN INSTALLING SIGNAL FOUNDATIONS (APPROX. STA. 191+62.3, 49.3' RT AND STA. 192+56.7, 42.2' RT) AND PROPOSED STORM SEWER CROSSINGS AT APPROX. STA. 193+03, STA. 195+98 AND STA. 199+01. THE MINIMUM 7-DAY NOTICE WILL ALLOW CENTURY LINK TO HAVE A CREW ON SITE DURING THIS WORK. IN ADDITION AND PRIOR TO CONSTRUCTING THE STORM IMPROVEMENTS BETWEEN STORM STRUCTURES D-10 AND D-11, THE CONTRACTOR SHALL USE EXTREME CAUTION AND HAND DIG TO LOCATE THE LINE AND CONFIRM THE ASSUMED DEPTH. THE COST TO PERFORM THE CENTURY LINK UTILITY COORDINATION SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE RESPECTIVE SIGNAL FOUNDATION AND STORM SEWER INSTALLATION.

COLUMBIA GAS UTILITY COORDINATION

THE CONTRACTOR SHALL PROVIDE COLUMBIA GAS (SEE CONTACT INFORMATION ON THIS SHEET) AND THE PROJECT ENGINEER A MINIMUM OF 7-DAY NOTICE WHEN INSTALLING PROPOSED STORM SEWER CROSSINGS AT APPROX. STA. 193+72.2, 23.9' LT, STA. 198+43.4, 42.4' RT AND STA. 198+43.4, 25.8' LT AND MANUFACTURED SYSTEM AT APPROX. STA. 201+51.1, 38.9' LT. THE MINIMUM 7-DAY NOTICE WILL ALLOW COLUMBIA GAS TO HAVE A CREW ON SITE DURING THIS WORK. PRIOR TO CONSTRUCTING STORM IMPROVEMENTS, THE CONTRACTOR SHALL USE EXTREME CAUTION AND HAND DIG TO LOCATE THE LINE AND CONFIRM THE ASSUMED DEPTH. THE COST TO PERFORM THE COLUMBIA GAS UTILITY COORDINATION SHALL BE CONSIDERED INCIDENTAL TO THE COST OF THE RESPECTIVE STORM SEWER INSTALLATION.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

EXISTING PLANS

EXISTING PLANS ENTITLED LIC-62-3.85 (2010) AND LIC-62-3.5 (2015), MAY BE INSPECTED IN THE ODOT DISTRICT 5 OFFICE IN JACKSONTOWN, OH.

CLEARING AND GRUBBING

THE DEPARTMENT HAS NOT MARKED INDIVIDUAL TREES AND STUMPS FOR REMOVAL. UNLESS SPECIFICALLY DESIGNATED AS "DO NOT DISTURB" IN THE PLANS, REMOVE ALL TREES AND STUMPS WITHIN THE CONSTRUCTION LIMITS UNDER THE LUMP SUM BID FOR ITEM 201 CLEARING AND GRUBBING.

SURVEYING PARAMETERS

PRIMARY PROJECT CONTROL MONUMENTS GOVERN ALL POSITIONING ON ODOT PROJECTS.

USE THE FOLLOWING PROJECT CONTROL, VERTICAL POSITIONING, AND HORIZONTAL POSITIONING PARAMETERS FOR ALL SURVEYING:

PROJECT CONTROL

POSITIONING METHOD: STATIC GNSS  
MONUMENT TYPE: (A)

VERTICAL POSITIONING

ORTHOMETRIC HEIGHT DATUM: NAVD88  
GEOID: GEOID12B

HORIZONTAL POSITIONING

REFERENCE FRAME: NAD83(2011)(EPOCH:2010.0000)  
ELLIPSOID: GRS80  
MAP PROJECTION: LAMBERT CONFORMAL CONIC  
COORDINATE SYSTEM: OHIO STATE PLANE (SOUTH) ZONE  
COMBINED SCALE FACTOR: 1.00002243  
ORIGIN OF COORDINATE SYSTEM: 0,0

USE THE POSITIONING METHODS AND MONUMENT TYPE USED IN THE ORIGINAL SURVEY TO RESTORE ALL MONUMENTS RELATED TO PRIMARY PROJECT CONTROL THAT ARE DAMAGED OR DESTROYED BY CONSTRUCTION ACTIVITIES. RESTORE THE DAMAGED OR DESTROYED MONUMENTS IN ACCORDANCE WITH C&MS 623.

UNITS ARE IN U.S. SURVEY FEET. USE THE FOLLOWING CONVERSION FACTOR: 1 METER = 3.280833333 U.S. SURVEY

ITEM 204 - PROOF ROLLING

THE FOLLOWING QUANTITY IS PROVIDED IN THE GENERAL SUMMARY TO ADDRESS LOCATIONS REQUIRING PROOF ROLLING.

ITEM 204 – PROOF ROLLING 2 HOUR

BENCHING OF FOUNDATION SLOPES

ALTHOUGH CROSS-SECTIONS INDICATE SPECIFIC DIMENSIONS FOR PROPOSED BENCHING OF THE EMBANKMENT FOUNDATIONS IN CERTAIN AREAS, NO WAIVER OF THE SPECIFICATIONS IS INTENDED. BENCH ALL OTHER SLOPED EMBANKMENT AREAS AS SET FORTH IN SECTION 203.05 OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS (C&MS). NO ADDITIONAL PAYMENT WILL BE MADE FOR BENCHING REQUIRED UNDER THE PROVISIONS OF SECTION 203.05.

CONNECTION BETWEEN EXISTING AND PROPOSED GUARDRAIL

WHEN IT IS NECESSARY TO SPLICE PROPOSED GUARDRAIL TO EXISTING GUARDRAIL, ONLY THE EXISTING GUARDRAIL SHALL BE CUT, DRILLED, OR PUNCHED. THE CONNECTION SHALL BE MADE USING A W-BEAM, BEAM SPLICE AS SHOWN IN AASHTO M 180-12, EXCEPT THE BEAM WASHERS ARE NOT TO BE USED. PAYMENT SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE RESPECTIVE GUARDRAIL ITEMS.

ITEM SPECIAL - MAILBOX SUPPORT

THIS WORK SHALL CONSIST OF FURNISHING AND ERECTING MAILBOX SUPPORTS AND ANY ASSOCIATED MOUNTING HARDWARE IN ACCORDANCE WITH PLAN DETAILS, AND ATTACHING AN OWNER-SUPPLIED MAILBOX AT LOCATIONS SPECIFIED IN THE PLAN, OR OTHERWISE ESTABLISHED BY THE ENGINEER.

WOOD POSTS SHALL BE NOMINAL 4 INCHES BY 4 INCHES SQUARE OR 4.5 INCHES DIAMETER ROUND, AND CONFORM TO 710.14.

STEEL POSTS SHALL BE NOMINAL PIPE SIZE 2 INCHES I.D., AND CONFORM TO AASHTO M 181.

ALL HARDWARE INCLUDING BUT NOT LIMITED TO PLATES, SCREWS, BOLTS, AND ETC. SHALL BE COMMERCIAL-GRADE GALVANIZED STEEL.

POSTS SHALL BE SET PER THE FIRST PARAGRAPH OF 606.03, AND SHALL IN NO INSTANCE BE ENCASED IN CONCRETE.

SUPPORT HARDWARE SHALL ACCOMMODATE EITHER A SINGLE OR A DOUBLE MAILBOX INSTALLATION, AND NO MORE THAN TWO BOXES MAY BE MOUNTED ON A SINGLE POST.

THE MAILBOX SHALL BE SECURELY AND NEATLY ATTACHED BY THE CONTRACTOR TO THE NEW SUPPORT. THE CONTRACTOR SHALL FURNISH ALL NECESSARY ATTACHMENT HARDWARE (NUTS, BOLTS, PLATES, SPACERS, AND WASHERS) AS NECESSARY TO ACCOMMODATE THE COMPLETE INSTALLATION.

IN THE ABSENCE OF A NEW BOX SUPPLIED BY THE OWNER, THE CONTRACTOR SHALL SALVAGE THE EXISTING BOX AND PLACE IT ON THE NEW SUPPORT. DUE CARE SHALL BE EXERCISED IN SUCH AN OPERATION, AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING OR REPLACING ANY BOX DAMAGED BY IMPROPER HANDLING ON HIS PART, AS JUDGED AND DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WITH THE LOCAL POST MASTER REGARDING THE TIMING OF THE MOVEMENT OF ANY MAILBOX TO A NEW LOCATION.

PAYMENT UNDER THIS ITEM SHALL BE LIMITED TO FINAL PERMANENT INSTALLATIONS, TEMPORARY INSTALLATIONS SHALL BE IN ACCORDANCE WITH 107.10. HOWEVER, THE SAME MATERIAL AND SIZE LIMITATIONS AS FOR PERMANENT INSTALLATIONS SHALL APPLY.

MAILBOX SUPPORTS, COMPLETE IN PLACE, WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH, FOR ITEM SPECIAL MAILBOX SUPPORT SYSTEM, (SINGLE) (DOUBLE).

SEEDING AND MULCHING

THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY TO PROMOTE GROWTH AND CARE OF PERMANENT SEEDED AREAS:

ITEM 659, SEEDING AND MULCHING, CLASS 2	3595 SY
ITEM 659, COMMERCIAL FERTILIZER	0.49 TON
ITEM 659, LIME	0.74 ACRE
ITEM 659, WATER	19 MGAL

SEEDING AND MULCHING SHALL BE APPLIED TO ALL AREAS OF EXPOSED SOIL BETWEEN THE RIGHT-OF-WAY LINES, AND WITHIN THE CONSTRUCTION LIMITS FOR AREAS OUTSIDE THE RIGHT-OF-WAY LINES COVERED BY WORK AGREEMENT OR SLOPE EASEMENT. QUANTITY CALCULATIONS FOR SEEDING AND MULCHING ARE BASED ON THESE LIMITS.

DRAINAGE

CROSSINGS AND CONNECTIONS TO EXISTING PIPES AND UTILITIES

WHERE PLANS PROVIDE FOR A PROPOSED CONDUIT TO BE CONNECTED TO, OR CROSS OVER OR UNDER AN EXISTING SEWER OR UNDERGROUND UTILITY, THE CONTRACTOR SHALL LOCATE THE EXISTING PIPES OR UTILITIES BOTH AS TO LINE AND GRADE BEFORE STARTING TO LAY THE PROPOSED CONDUIT.

IF IT IS DETERMINED THAT THE ELEVATION OF THE EXISTING CONDUIT, OR EXISTING APPURTENANCE TO BE CONNECTED, DIFFERS FROM THE PLAN ELEVATION OR RESULTS IN A CHANGE IN THE PLAN CONDUIT SLOPE, THE ENGINEER SHALL BE NOTIFIED BEFORE STARTING CONSTRUCTION OF ANY PORTION OF THE PROPOSED CONDUIT WHICH WILL BE AFFECTED BY THE VARIANCE IN THE EXISTING ELEVATIONS.

IF IT IS DETERMINED THAT THE PROPOSED CONDUIT WILL INTERSECT AN EXISTING SEWER OR UNDERGROUND UTILITY IF CONSTRUCTED AS SHOWN ON THE PLAN, THE ENGINEER SHALL BE NOTIFIED BEFORE STARTING CONSTRUCTION OF ANY PORTION OF THE PROPOSED CONDUIT WHICH WOULD BE AFFECTED BY THE INTERFERENCE WITH AN EXISTING FACILITY.

PAYMENT FOR ALL THE OPERATIONS DESCRIBED ABOVE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 611 CONDUIT ITEM.

REVIEW OF DRAINAGE FACILITIES

BEFORE ANY WORK IS STARTED ON THE PROJECT AND AGAIN BEFORE FINAL ACCEPTANCE BY THE STATE, REPRESENTATIVES OF THE STATE AND THE CONTRACTOR, ALONG WITH LOCAL REPRESENTATIVES, SHALL MAKE AN INSPECTION OF ALL EXISTING SEWERS WHICH ARE TO REMAIN IN SERVICE AND WHICH MAY BE AFFECTED BY THE WORK. THE CONDITION OF THE EXISTING CONDUITS AND THEIR APPURTENANCE SHALL BE DETERMINED FROM FIELD OBSERVATIONS. RECORDS OF THE INSPECTION SHALL BE KEPT IN WRITING BY THE STATE.

ALL NEW CONDUITS, INLETS, CATCH BASINS, AND MANHOLES CONSTRUCTED AS A PART OF THE PROJECT SHALL BE FREE OF ALL FOREIGN MATTER AND IN A CLEAN CONDITION BEFORE THE PROJECT WILL BE ACCEPTED BY THE STATE.

ALL EXISTING SEWERS INSPECTED INITIALLY BY THE ABOVE MENTIONED PARTIES SHALL BE MAINTAINED AND LEFT IN A CONDITION REASONABLY COMPARABLE TO THAT DETERMINED BY THE ORIGINAL INSPECTION. ANY CHANGE IN THE CONDITION RESULTING FROM THE CONTRACTOR'S OPERATIONS SHALL BE CORRECTED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER.

PAYMENT FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 611 CONDUIT ITEMS.

ITEM 605 - AGGREGATE DRAINS

AGGREGATE DRAINS SHALL BE PLACED AT 50 FOOT INTERVALS ON EACH SIDE OF NORMAL CROWNED SECTIONS, STAGGERED SO THAT EACH DRAIN IS 25 FEET FROM THE ADJACENT DRAIN ON THE OPPOSITE SIDE, AND AT 25 FOOT INTERVALS ON THE LOW SIDE ONLY OF SUPERELEVATED SECTIONS. AN AGGREGATE DRAIN SHALL BE PLACED AT THE LOW POINT OF EACH SAG VERTICAL CURVE.

POST CONSTRUCTION STORM WATER TREATMENT

THIS PLAN UTILIZES STRUCTURAL BEST MANAGEMENT PRACTICES (BMP'S) FOR POST CONSTRUCTION STORM WATER TREATMENT.

MANUFACTURED WATER QUALITY STRUCTURE

THIS PLAN UTILIZES MANUFACTURED WATER QUALITY STRUCTURES FOR WATER QUALITY TREATMENT. AREAS HAVE BEEN SHOWN IN THE PLANS FOR PLACEMENT OF AN OFF-LINE SYSTEM. PAYMENT FOR THESE DEVICES SHALL BE MADE AT THE CONTRACT UNIT PRICE FOR ITEM 895, MANUFACTURED WATER QUALITY STRUCTURE TYPE 4.

GENERAL NOTES

DESIGN AGENCY



DESIGNER

AJP

REVIEWER

JDH 12-01-21

PROJECT ID

110861

SHEET TOTAL

P.6 89

LIC-62-3.64

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ITEM 304 - AGGREGATE BASE, AS PER PLAN

THIS ITEM SHALL CONFORM TO ITEM 304 OF THE OHIO DEPARTMENT OF TRANSPORTATION CONSTRUCTION AND MATERIAL SPECIFICATIONS. THE QUANTITY PROVIDED IN THE PAVEMENT CALCULATIONS IN THE OFFICE CALCULATIONS, HAS BEEN ESTIMATED USING A 8" THICKNESS.

THE CONTRACTOR MUST FIRST FIELD VERIFY THE THICKNESS OF THE EXISTING AGGREGATE BASE AND DETERMINE THE DEPTH OF EXISTING SUBGRADE PRIOR TO PLACING OF THIS ITEM. IF THE EXISTING SUBGRADE IS FOUND TO BE AT A HIGHER ELEVATION THAN THE PROPOSED SUBGRADE (WHEN ASSUMING A PROPOSED 8" AGGREGATE BASE), THE CONTRACTOR SHALL PROVIDE THE RESULTS OF THE INVESTIGATION TO THE PROJECT ENGINEER. THE ENGINEER SHALL THEN DIRECT THE CONTRACTOR TO ADJUST THE THICKNESS OF THIS ITEM ACCORDINGLY.

IF THE ACTUAL QUANTITY USED IS LESS THAN THE AMOUNT BID, THE ENGINEER WILL DETERMINE THE CU. YDS. FOR NON-PAYMENT BY TAKING THE DIFFERENCE IN DEPTHS AND MULTIPLYING BY AREA OF PAVEMENT.

ITEM 202 - PAVEMENT REMOVED

PAYMENT FOR PAVEMENT REMOVED SHALL INCLUDE THE ENTIRE PAVEMENT BUILD UP (SEE TYPICAL SECTION FOR EXISTING PAVEMENT SECTION).

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER.

ITEM 202 - PAVEMENT REMOVED 200 SY

ITEM 253 - PAVEMENT REPAIR

THIS ITEM SHALL CONSIST OF FULL-DEPTH PAVEMENT REPAIR WITHIN THE PROJECT LIMITS THAT ARE DEEMED IN POOR CONDITION. PRIOR TO PAVEMENT MILLING AND AT THE DIRECTION OF THE ENGINEER, THE CONTRACTOR AND PROJECT ENGINEER SHALL WALK THE PROJECT SITE AND MARK LIMITS OF REPAIR. THE CONTRACTOR SHALL SAWCUT AND REMOVE MARKED PAVEMENT, PERFORM SUBGRADE COMPACTION AND PLACE ITEM 301, ASPHALT CONCRETE BASE, PG64-22, (449) IN TWO LIFTS TO THE TOP OF ADJACENT ASPHALT PAVEMENT. ITEM 407, TACK COAT SHALL BE PLACED BETWEEN LIFTS.

ALL REQUIREMENTS OF C&MS 253 SHALL STILL BE MET.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER.

ITEM 253 - PAVEMENT REPAIR 200 SY

ITEM 875 - LONGITUDINAL JOINT ADHESIVE

THE CONTRACTOR SHALL APPLY HOT ASPHALTIC JOINT ADHESIVE TO COLD LONGITUDINAL CONSTRUCTION JOINTS IN ASPHALT CONCRETE SURFACE PAVEMENT. SEE SUPPLEMENTAL SPECIFICATION 875 FOR STANDARD APPLICATION DETAILS.

PAVEMENT RESTORATION FOR PIPE INSTALLATIONS AND/OR REMOVALS

THE DRAINAGE SUBSUMMARY PROVIDES QUANTITIES FOR PAVEMENT RESTORATION FOLLOWING INSTALLATION OF PIPE CROSSING AT STA. 195+00 AND STA. 202+00.

THE QUANTITIES ARE BASED ON:

- A TRENCH WIDTH EQUAL TO FOUR TIMES THE PIPE DIAMETER
- A 301 THICKNESS OF 10 INCHES
- A 304 THICKNESS OF 8 INCHES

PROVIDE ANY MATERIALS USED OUTSIDE THE LIMITS STATED ABOVE AT NO ADDITIONAL COST.

ITEM 517 - RAILING, PIPE, AS PER PLAN

THE CONTRACTOR SHALL PROVIDE HANDRAIL IN ACCORDANCE WITH STANDARD CONSTRUCTION DRAWING RM-2.1 AND C&MS 517 WHERE DETAILED IN THE PLANS. A CONCRETE FOOTER WITH MINIMUM DIMENSIONS OF 6" WIDE BY 6" DEEP SHALL BE PROVIDED THE ENTIRE LENGTH OF THE HANDRAIL. THE TOP OF FOOTER SHALL BE FLUSH WITH THE TOP OF SIDEWALK. PEJF SHALL BE PROVIDED AT THE INTERFACE BETWEEN SIDEWALK AND FOOTER. IN ADDITION TO THE MATERIAL REQUIREMENTS OF THE SCD AND C&MS, THE HANDRAIL SHALL INCLUDE SHOP-FABRICATED OR FIELD-WELDED ANTI-SKATEBOARDING GUARDS AT A MAXIMUM SPACING OF FIVE FEET C/C. HANDRAIL SHOP DRAWINGS SHALL BE PROVIDED TO THE ENGINEER FOR APPROVAL PRIOR TO FABRICATION OR CONSTRUCTION.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID FOR ITEM 517, RAILING, PIPE, AS PER PLAN, LINEAR FOOT, AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT A COMPLETE AND FUNCTIONAL HANDRAIL SYSTEM, INCLUDING BUT NOT LIMITED TO HANDRAIL, POSTS, FOOTER, PEJF, ANTI-SKATEBOARD GUARDS AND ALL RELATED HARDWARE NOT SEPARATELY SPECIFIED.

PLAN NOTE - ELECTRONIC TICKETING

PURPOSE:  
PROVIDE ELECTRONIC MATERIAL TICKETS IN AN ELECTRONIC FORMAT DIRECTLY RECORDED FROM THE MATERIAL LOADING SOURCE.

PROVIDE ELECTRONIC MATERIAL TICKETS FOR THE FOLLOWING MATERIALS:

AGGREGATE  
ASPHALT CONCRETE  
PORTLAND CONCRETE  
(SELECT ONLY THOSE THAT APPLY TO THE PROJECT. DELETE THE MATERIALS WHICH ARE NOT USED)

THIS NOTE IN NO WAY SUPERSEDES ANY OTHER COMMERCIAL REGULATIONS OR ANY OTHER LEGAL REQUIREMENTS REGULATING THE TRANSPORTATION OF COMMERCIAL MATERIALS.

REQUIREMENTS:  
AT THE PRE-CONSTRUCTION MEETING, SUBMIT AN ELECTRONIC TICKETING PLAN TO THE ENGINEER DESCRIBING THE PROPOSED ELECTRONIC TICKET DELIVERY METHOD. THE ELECTRONIC MATERIAL TICKET SHALL CONTAIN INFORMATION AS REQUIRED PER THE APPLICABLE MATERIAL SPECIFICATION FOR WEIGHT MEASUREMENT AND OTHER MATERIAL CHARACTERISTICS: PROVIDE AN EXAMPLE(S) OR A "MOCK-UP" OF THE PROPOSED ELECTRONIC TICKET TO SHOW THE DETAILS ON WHAT IS TO BE TRANSMITTED TO THE DEPARTMENT. NAMING OF THE ELECTRONIC MATERIAL TICKET FILES SHALL BE DISTINCT SUCH THAT THE TICKET'S REPRESENTED MATERIAL IS EASILY DETERMINED; INCLUDE THE PROPOSED NAMING CONVENTION. DELIVERY MAY BE THROUGH A PRODUCER WEBSITE UPLOAD ACCESSIBLE TO THE ENGINEER, ODOT PROJECT SPECIFIC SHAREPOINT DOCUMENTATION SITE UPLOAD, OR ANOTHER SECURE ELECTRONIC TRANSMITTAL MEANS. EMAILING OF A TICKET TO AN ODOT CONTACT IS ACCEPTABLE BUT IS NOT PREFERRED. THE ELECTRONIC TICKETING PLAN SHALL IDENTIFY A CONTINGENCY METHOD FOR MANUALLY CAPTURING AND DELIVERING TICKET INFORMATION IF ELECTRONIC TRANSMISSION IS TEMPORARILY UNAVAILABLE. AN ELECTRONIC TICKETING PLAN WHICH INCLUDES SOLELY THE USE OF DIGITAL PHOTOS OF PAPER TICKETS IS NOT ACCEPTABLE.

THE DEPARTMENT RECOGNIZES THAT VARIOUS DIGITAL TICKETING SYSTEMS MAY BE COMMERCIALY AVAILABLE AND USED TO ACCOMMODATE INDIVIDUAL CONTRACTORS AND MATERIAL SUPPLIER CAPABILITIES. THE CONTRACTOR MAY PROVIDE A DIGITAL TICKETING SYSTEM GIVING SECURE ACCESS TO ORGANIZED DIGITAL DATA. IF UTILIZED, THE DIGITAL TICKETING SYSTEM MAY ALSO BE ACCESSIBLE BY REAL-TIME MONITORING WITH A MOBILE COMMUNICATION DEVICE SUCH AS A TABLET, SMARTPHONE, ETC. THROUGH MOBILE DEVICE APPLICATIONS ("MOBILE APP") IF ACCEPTABLE TO THE DEPARTMENT. IF A DIGITAL TICKETING SYSTEM REQUIRES A MOBILE APP, THE MOBILE APP SHALL BE AT NO COST TO THE DEPARTMENT. THE DIGITAL DATA MUST BE ABLE TO BE EXPORTED IN A FORMAT USABLE BY THE ENGINEER UPON REQUEST (I.E. MICROSOFT WORD, MICROSOFT EXCEL, PDF FORMATS).

DELIVER EACH ELECTRONIC MATERIAL TICKET TO THE ENGINEER PRIOR TO THE PLACEMENT OF MATERIAL, BUT NOT PRIOR TO THE LOADING OF MATERIAL AT THE SOURCE.

PROVIDE THE ENGINEER A DAILY MATERIAL SUMMARY REPORT BY THE END OF THE DAY'S HAULING ACTIVITIES, OR AT A TIME AS APPROVED BY THE ENGINEER. THE DAILY MATERIAL SUMMARY REPORT INCLUDES SUMMARY INFORMATION LISTED FOR EACH MATERIAL AS OUTLINED IN THE RESPECTIVE MATERIAL SPECIFICATION.

PAYMENT:  
COSTS FOR THE ELECTRONIC TICKETING SHALL BE INCIDENTAL TO THE PROJECT.

ENVIRONMENTAL COMMITMENTSNO IN-STREAM WORK PERMITTED

THE PROJECT INVOLVES CONSTRUCTION ACTIVITIES THAT WILL OCCUR ON THE BRIDGE CARRYING US-62 OVER RACCOON CREEK. NO WORK WITHIN THE STREAM WILL OCCUR, INCLUDING THE PLACEMENT OF TEMPORARY OR PERMANENT FILL, OR FORDING THE STREAM. NO DEBRIS MAY BE SWEEPED OR WASHED INTO THE STREAM.

CONSTRUCTION ACTIVITY NOTIFICATION

CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER A MINIMUM OF 21 DAYS PRIOR TO THE START OF CONSTRUCTION ACTIVITIES, LANE RESTRICTIONS, LANE CLOSURES, AND ROAD CLOSURES. THE PROJECT ENGINEER WILL FORWARD THIS INFORMATION TO THE DISTRICT 5 PUBLIC INFORMATION OFFICER (PIO), WHEREAS THE PIO WILL NOTIFY THE LOCAL EMERGENCY SERVICES, AFFECTED SCHOOLS AND BUSINESSES, AND ANY OTHER IMPACTED LOCAL PUBLIC AGENCIES OF ANY OF THE ABOVE-MENTIONED ITEMS VIA MEDIA SOURCES.

DESIGN AGENCY



DESIGNER

AJP

REVIEWER

JDH 12-20-21

PROJECT ID

110861

SHEET TOTAL

P.7 89

**MAINTENANCE OF TRAFFIC SEQUENCE OF CONSTRUCTION**

## PRE-PHASE

CONSTRUCT TEMPORARY ROADS ON RIGHT SIDE OF US 62.

## PHASE 1

CONSTRUCT LEFT SIDE OF US 62 (EXCEPT SURFACE COURSE OF NEW PAVEMENT). THE INTERMEDIATE COURSE SHALL BE PLACED LEVEL WITH THE EXISTING SURFACE COURSE.

## PHASE 2

CONSTRUCT LEFT SIDE OF US 62 (EXCEPT SURFACE COURSE OF NEW PAVEMENT). THE INTERMEDIATE COURSE SHALL BE PLACED LEVEL WITH THE EXISTING SURFACE COURSE

## PHASE 3

BETWEEN THE NEWLY CONSTRUCTED CURB LIMITS, MILL 1.5" OF EXISTING SURFACE COURSE, AS WELL AS 1.5" OF INTERMEDIATE COURSE PLACED WITHIN THE WIDENED AREAS DURING PHASE 1 AND PHASE 2 CONSTRUCTION. INSTALL SURFACE COURSE AND NEW PAVEMENT MARKINGS.

**ITEM 614 - MAINTAINING TRAFFIC**

A MINIMUM OF TWO LANES OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT, THE COMPLETED PAVEMENT, ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC, ITEM 615 ROADS FOR MAINTAINING TRAFFIC, AND TEMPORARY SURFACES USING ITEMS 410 AND 614.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DETERMINED BY THE ENGINEER FOR THE MAINTENANCE OF TRAFFIC.

ITEM 410 - TRAFFIC COMPACTED SURFACE, TYPE B	170 CY
ITEM 616 - WATER	50 MGAL

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

**PLACEMENT OF ASPHALT CONCRETE**

TWO-WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT THAT ONE-WAY TRAFFIC WILL BE PERMITTED FOR MINIMUM PERIODS OF TIME CONSISTENT WITH THE REQUIREMENTS OF THE SPECIFICATIONS FOR PROTECTION OF COMPLETED ASPHALT CONCRETE COURSES.

**DUST CONTROL**

THE CONTRACTOR SHALL FURNISH AND APPLY WATER FOR DUST CONTROL AS DIRECTED BY THE ENGINEER. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED FOR DUST CONTROL PURPOSES:

ITEM 616 - WATER	3 MGAL
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**EARTHWORK FOR MAINTAINING TRAFFIC**

THE FOLLOWING QUANTITIES HAVE BEEN INCLUDED IN THE PLAN FOR INFORMATION ONLY.

EXCAVATION FOR MAINTAINING TRAFFIC	65 CY
EMBANKMENT FOR MAINTAINING TRAFFIC	65 CY

WHEN UNDERCUTS ARE NECESSARY FOR MAINLINE PAVEMENT OR EMBANKMENT CONSTRUCTION, EVALUATE THE NEED FOR TEMPORARY ROAD UNDERCUTS IF WITHIN A CLOSE PROXIMITY TO THE MAINLINE UNDERCUTS. A GEOTECHNICAL EVALUATION SHOULD BE CONSIDERED TO DETERMINE IF THE EXISTING SOIL CONDITIONS ARE ADEQUATE TO SUPPORT THE TEMPORARY ROAD. ADDITIONAL SOIL BORINGS ALONG THE TEMPORARY ROAD ARE NOT NORMALLY REQUIRED.

**MAINTAINING DRIVEWAY ACCESS**

ACCESS FOR PEDESTRIAN AND VEHICULAR TRAFFIC TO ALL ADJOINING PROPERTIES SHALL BE MAINTAINED AT ALL TIMES. WHERE PROPERTIES ARE ACCESSED BY ONLY ONE DRIVEWAY, PARTIAL WIDTH CONSTRUCTION SHALL BE USED.

**COOPERATION BETWEEN CONTRACTORS**

THE STATE OF OHIO HAS CONTRACTED PROJECT LIC-62-00.49 PID 109329, WHICH MAY BE CONSTRUCTED CONCURRENTLY WITH THIS PROJECT. IT IS IMPERATIVE THAT THE CONTRACTORS COOPERATE FULLY WITH EACH OTHER AS OUTLINED IN SECTION 105.08 OF THE C&MS MANUAL. ALL MAINTENANCE OF TRAFFIC SHALL BE COORDINATED BETWEEN PROJECTS AND NOT CONFLICT WITH ONE ANOTHER.

**WORKING HOUR RESTRICTIONS**

WORK HOURS SHALL BE LIMITED TO 7 AM TO 7 PM. SHORT TERM CLOSURES USING FLAGGERS SHALL BE LIMITED TO 7 AM TO 4 PM.

**ITEM 613 - LOW STRENGTH MORTAR BACKFILL**

LOW STRENGTH MORTAR BACKFILL SHALL BE PLACED AS STRUCTURAL BACKFILL FOR STORM SEWER CROSSINGS PER C&MS ITEM 611 AND ITEM 613. THE CONTRACTOR SHALL PROVIDE AN INSTALLATION PLAN TO THE ENGINEER FOR APPROVAL PER ITEM 611. THE FOLLOWING QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER.

ITEM 613 - LOW STRENGTH MORTAR BACKFILL	20 CY
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**MAINTENANCE OF TRAFFIC SIGNAL/FLASHER INSTALLATION**

THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTAINING TRAFFIC SIGNAL/FLASHER INSTALLATIONS WITHIN THE PROJECT UNDER THE FOLLOWING CONDITIONS:

1. EXISTING SIGNAL/FLASHER INSTALLATIONS WHICH THE PLANS REQUIRE THE CONTRACTOR TO ADJUST, MODIFY, ADD ONTO OR REMOVE, OR WHICH THE CONTRACTOR ACTUALLY ADJUSTS, MODIFIES OR OTHERWISE DISTURBS. THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE ENTIRE INSTALLATION (AT AN INTERSECTION) FROM THE TIME HIS OPERATIONS FIRST DISTURB THE INSTALLATION UNTIL THE INSTALLATION HAS BEEN SUBSEQUENTLY REMOVED OR MODIFIED AND THE WORK IS ACCEPTED.

2. NEW OR REUSED SIGNAL/FLASHER INSTALLATIONS OR DEVICES, INSTALLED BY THE CONTRACTOR. THE CONTRACTOR SHALL BE RESPONSIBLE FOR MAINTENANCE OF THESE FROM THE TIME OF INSTALLATION UNTIL THE WORK IS ACCEPTED.

THE CONTRACTOR SHALL CORRECT AS QUICKLY AS POSSIBLE ALL OUTAGES OR MALFUNCTIONS. HE SHALL PROVIDE THE MAINTAINING AGENCY AND THE ENGINEER SUCH ADDRESSES AND PHONE NUMBERS WHERE HIS MAINTENANCE FORCES CAN BE CONTACTED. THE CONTRACTOR SHALL PROVIDE ONE OR MORE PERSONS TO RECEIVE ALL CALLS AND DISPATCH THE NECESSARY MAINTENANCE FORCES TO CORRECT OUTAGES. SUCH A PERSON OR PERSONS MAY BE USED TO PERFORM OTHER DUTIES AS LONG AS PROMPT ATTENTION IS GIVEN TO THESE CALLS AND A PERSON IS READILY AVAILABLE CONTINUOUSLY 24 HOURS A DAY, 7 DAYS A WEEK. ALL LAMP OUTAGES, CABLE OUTAGES, ELECTRICAL FAILURES, EQUIPMENT MALFUNCTIONS AND MISALIGNED SIGNAL HEADS SHALL BE CORRECTED TO THE SATISFACTION OF THE ENGINEER WITH THE SIGNAL BACK TO SERVICE WITHIN FOUR HOURS AFTER THE CONTRACTOR HAS BEEN NOTIFIED OF THE OUTAGE.

IN THE EVENT NEW SIGNALS ARE DAMAGED PRIOR TO ACCEPTANCE, ALL DAMAGED EQUIPMENT EXCEPT POLES AND CONTROL EQUIPMENT SHALL BE REPLACED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER WITH THE SIGNAL BACK IN SERVICE WITHIN 8 HOURS AFTER THE CONTRACTOR'S NOTIFICATION OF THE OUTAGE. THE CONTRACTOR SHALL ARRANGE FOR FULL TRAFFIC CONTROL UNTIL THE SIGNAL IS BACK IN OPERATION. IF POLES AND/OR CONTROL EQUIPMENT ARE DAMAGED AND MUST BE REPLACED, THE CONTRACTOR SHALL MAKE TEMPORARY REPAIRS AS NECESSARY TO BRING THE SIGNAL BACK INTO FULL OPERATION WITHIN THE ALLOWED 8-HOUR PERIOD, AND SHALL MAKE PERMANENT REPAIRS OR REPLACEMENT AS SOON THEREAFTER AS POSSIBLE.

NONE OF THE ABOVE SHALL BE CONSTRUED AS COLLECTIVE OR CONSECUTIVE OUTAGE TIME PERIODS AT ANY ONE LOCATION. THAT IS, WHERE MORE THAN ONE OUTAGE OCCURS AT ANY ONE LOCATION THEN THE ALLOTTED TIME LIMIT SHALL BE FOR THE WORST SINGLE OUTAGE.

WHERE OUTAGES ARE THE DIRECT RESULT OF A VEHICLE ACCIDENT THE RESPONSE OF THE CONTRACTOR SHALL BE AS OUTLINED ABOVE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR COLLECTION OF ANY COMPENSATION FOR THIS WORK FROM THOSE PARTIES RESPONSIBLE FOR THE DAMAGE.

WHERE THE CONTRACTOR HAS FAILED TO, OR CANNOT RESPOND TO, AN OUTAGE OR SIGNAL EQUIPMENT MALFUNCTION, AT THESE LOCATIONS WITHIN HIS RESPONSIBILITY, WITHIN PERIODS AS SPECIFIED ABOVE, THE ENGINEER MAY INVOKE THE PROVISIONS OF SECTION 105.15 AND ANY SUBSEQUENT BILLINGS TO THE STATE OR THE CITY OF JOHNSTOWN, OHIO FOR POLICE SERVICES AND MAINTENANCE SERVICES BY CITY FORCES SHALL BE DEDUCTED FROM MONIES DUE OR TO BECOME DUE THE CONTRACTOR IN ACCORDANCE WITH PROVISIONS OF SECTION 105.15.

THE CONTRACTOR SHALL PROVIDE THE MAINTENANCE SERVICE ENTIRELY WITH HIS FORCES OR HE MAY CHOOSE TO ENTER INTO A COOPERATIVE UNDERSTANDING WITH THE LOCAL MAINTAINING AGENCY TO PROVIDE THE MAINTENANCE. THE CONTRACTOR SHALL INFORM THE ENGINEER, IN WRITING, OF THE MAINTENANCE METHOD SELECTED.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR ANY DAMAGE TO ANY TRAFFIC SIGNAL COMPONENTS REQUIRED TO BE HANDLED DURING THE RELOCATION OF POLES AND REVISIONS TO THE SIGNAL SYSTEM. WHEN A TRAFFIC SIGNAL MUST BE TAKEN OUT OF SERVICE BY THE CONTRACTOR, DUE TO CONSTRUCTION PROCEDURES, THIS OUTAGE SHALL NOT EXCEED 4 HOURS AND SHALL NOT INCLUDE THE HOURS OF 6 AM TO 6 PM. ANY SIGNALIZED INTERSECTION, WHERE THE SIGNAL IS OUT OF SERVICE DUE TO CONSTRUCTION PROCEDURES, OR DUE TO AN OUTAGE OR MALFUNCTION OF EQUIPMENT AS DESCRIBED ABOVE, SHALL BE PROTECTED, BY THE CONTRACTOR, BY THE INSTALLATION OF TEMPORARY "STOP" SIGNS.

ANY VEHICULAR TRAFFIC SIGNAL HEAD, EITHER NEW OR EXISTING WHICH WILL BE OUT OF OPERATION SHALL BE COVERED IN THE MANNER DESCRIBED IN 632.25.

THE CONTRACTOR SHALL MAINTAIN COMPLETE RECORDS OF MALFUNCTIONS INCLUDING:

1. TIME OF NOTIFICATION OF MALFUNCTION;
2. TIME OF WORK CREWS ARRIVAL TO CORRECT THE MALFUNCTION;
3. ACTIONS TAKEN TO CORRECT THE MALFUNCTION, INCLUDING A LIST OF PARTS REPAIRED OR REPLACED;
4. A DIAGNOSIS OF REASON FOR THE MALFUNCTION AND PROBABILITY OF REOCCURRENCE;
5. TIME OF COMPLETION OF THE REPAIR AND SYSTEM RESTORED TO FULL SERVICE.

A COPY OF THESE RECORDS SHALL BE PROVIDED TO THE ENGINEER WITHIN THREE (3) WORKING DAYS FOLLOWING COMPLETION OF EACH REPAIR.

ALL COSTS RESULTING FROM THE ABOVE REQUIREMENTS SHALL BE CONSIDERED TO BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614, MAINTAINING TRAFFIC.

**TRENCH FOR WIDENING**

TRENCH EXCAVATION FOR BASE WIDENING SHALL BE ONLY ON ONE SIDE OF THE PAVEMENT AT A TIME. THE OPEN TRENCH SHALL BE ADEQUATELY MAINTAINED AND PROTECTED WITH DRUMS OR BARRICADES AT ALL TIMES. PLACEMENT OF PROPOSED SUBBASE AND BASE MATERIAL SHALL FOLLOW AS CLOSELY AS POSSIBLE BEHIND EXCAVATION OPERATIONS. THE LENGTH OF WIDENING TRENCH WHICH IS OPEN AT ANY ONE TIME SHALL BE HELD TO A MINIMUM AND SHALL AT ALL TIMES BE SUBJECT TO APPROVAL OF THE ENGINEER

**OVERNIGHT TRENCH CLOSING**

THE BASE WIDENING SHALL BE COMPLETED TO A DEPTH OF NO MORE THAN 12 INCHES BELOW THE EXISTING PAVEMENT BY THE END OF EACH WORK DAY. NO TRENCH SHALL BE LEFT OPEN OVERNIGHT EXCEPT FOR A SHORT LENGTH (25 FEET OR LESS) OF A WORK SECTION AT THE END OF THE TRENCH. IN CASE WORK MUST BE SUSPENDED BECAUSE OF INCLEMENT WEATHER OR OTHER REASONS, THE TRENCH FOR THE UNCOMPLETED BASE WIDENING SHALL BE BACKFILLED AT THE DIRECTION OF THE ENGINEER.

DESIGN AGENCY



DESIGNER

BSS

REVIEWER

JDH 12-01-21

PROJECT ID

110861

SHEET TOTAL

P.8 89



**ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS**

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

- DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC, OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

- FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION OR AT THE POINT OF ROAD CLOSURE, AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 60 HOUR

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

**NOTIFICATION OF TRAFFIC RESTRICTIONS**

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION OF TRAFFIC RESTRICTIONS TIME TABLE

ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS & PIO
RAMP & ROAD CLOSURES	≥ 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	≤ 12 HOURS	4 CALENDAR DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	≥ 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 CALENDAR DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO CLOSURE

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

**ITEM 614 - PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN**

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

THE CONTRACTOR SHALL IMPLEMENT A SYSTEM WHEREBY CHANGEABLE MESSAGES WILL BE IMPLEMENTED WITHIN 3 HOURS FOLLOWING TELEPHONE NOTIFICATION FROM THE PROJECT ENGINEER TO A DESIGNATED PHONE.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 14 SIGN MONTH (ASSUMING 2 PCMS SIGNS FOR 7 MONTHS)

**DELINEATION OF TEMPORARY AND PERMANENT GUARDRAIL**

BARRIER REFLECTORS SHALL BE INSTALLED ON ALL TEMPORARY GUARDRAIL USED FOR TRAFFIC CONTROL; AND, ON ALL PERMANENT GUARDRAIL LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE. BARRIER REFLECTORS SHALL CONFORM TO C&MS 626 AND THE SPACING SHALL BE APPROXIMATELY 50 FEET.

OBJECT MARKERS SHALL BE INSTALLED ON ALL TEMPORARY AND PERMANENT GUARDRAIL LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE. GUARDRAIL-MOUNTING OF OBJECT MARKERS SHALL BE MADE BY INSTALLING THE OBJECT MARKERS ON THE EXTENSION BLOCKS RATHER THAN DIRECTLY ONTO THE GUARDRAIL ITSELF. OBJECT MARKERS SHALL CONFORM TO C&MS 614.03 AND THE SPACING SHALL BE APPROXIMATELY 50 FEET WITH A 25 FOOT OFFSET FROM THE BARRIER REFLECTORS.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE PLANS AND CARRIED TO THE GENERAL SUMMARY:

- ITEM 614, BARRIER REFLECTOR, TYPE 2, ONE-WAY 11 EACH
- ITEM 614, OBJECT MARKER, ONE-WAY 11 EACH

PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIAL, LABOR, INCIDENTALS AND EQUIPMENT NECESSARY FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING THE ABOVE ITEMS.

DESIGN AGENCY



DESIGNER

BSS

REVIEWER

JDH 10-29-21

PROJECT ID

110861

SHEET TOTAL

P.9 89

SHEET NO.	REFERENCE NO.	LOCATION	STATION		SIDE	614			615	
			FROM	TO		WORK ZONE CENTER LINE, CLASS I, 642 PAINT	WORK ZONE EDGE LINE, CLASS I, 4", 642 PAINT	WORK ZONE STOP LINE, CLASS I, 642 PAINT	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B	ROADS FOR MAINTAINING TRAFFIC
						MILE	MILE	FT	SY	LS
		PHASE 1								
P.11-P.15	WCL-1	US 62	188+65	208+67		0.38				
P.11-P.12	WCL-2	US 62	189+96	191+65	RT	0.03				
P.12-P.15	WEW-1	US 62	188+65	208+67	LT		0.38			
P.13-P.15	WEW-2	US 62	197+00	208+67	RT		0.22			
P.13	TP-1	US 62	197+00	198+25	RT				56	
P.13-P.14	TP-2	US 62	199+10	202+35	RT				144	
P.15	TP-3	US 62	205+75	207+10	RT				60	
		PHASE 2								
P.16-P.20	WEW-3	US 62	186+27	208+67	RT		0.42			
P.16-P.20	WCL-3	US 62	188+64	208+67		0.38				
P.17	WSL	US 62						22		
P.19-P.20	WEW-4	US 62	203+30	208+67	LT					
TOTALS CARRIED TO GENERAL SUMMARY						0.79	1.02	22	260	LS

DESIGN AGENCY



DESIGNER

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REVIEWER

JDH 06-17-21

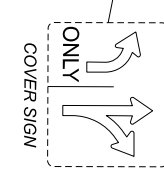
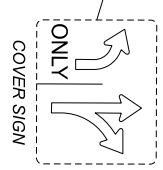
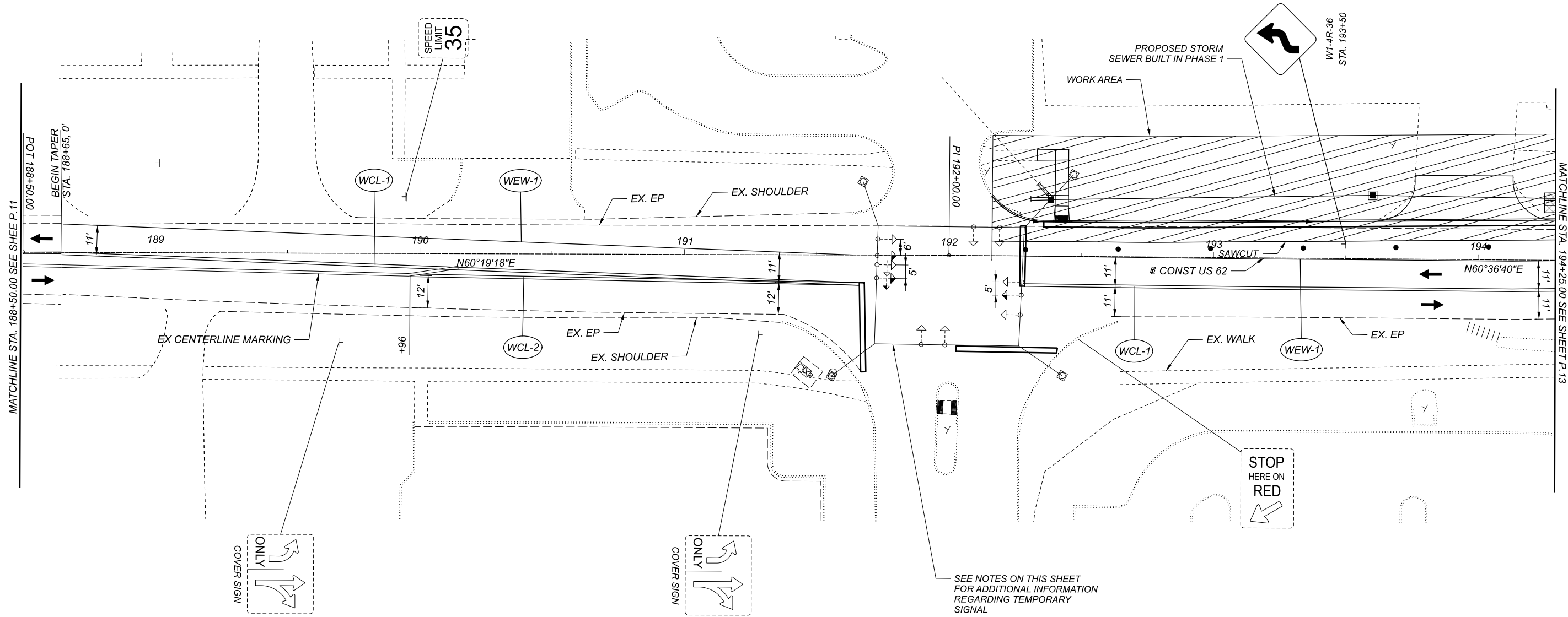
PROJECT ID

110861

SHEET TOTAL

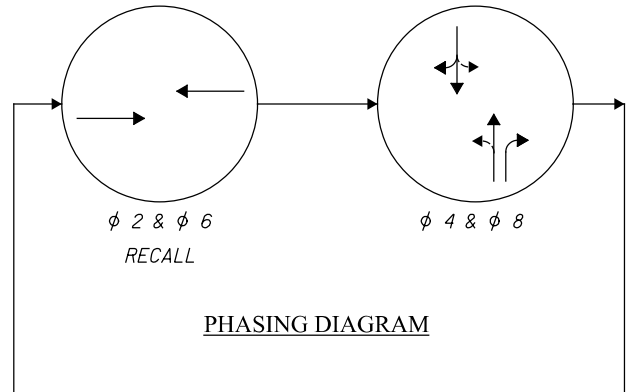
P.10 89





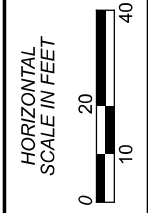
SEE NOTES ON THIS SHEET FOR ADDITIONAL INFORMATION REGARDING TEMPORARY SIGNAL

TEMPORARY TRAFFIC SIGNAL TIMING				
PHASE	2	4	6	8
DIRECTION	EB	NB	WB	SB
MIN GREEN	30	10	30	10
PASSAGE TIME	-	3	-	3
MAX GREEN	56	22	56	22
YELLOW CHANGE	4.1	4.4	4.1	4.4
RED CLEARANCE	1	1	1	1
WALK	7	7	-	-
PED CLEARANCE	17	15	-	-
RECALL	ON	OFF	ON	OFF



- LEGEND**
- EX. 3-SECTION SIGNAL HEAD
  - RELOCATED 3-SECTION SIGNAL HEAD
  - EX. 5-SECTION SIGNAL HEAD
  - RELOCATED 5-SECTION SIGNAL HEAD

- NOTES:**
1. FOR LEGEND SEE SHEET P.11
  2. REMOVE CONFLICTING MARKINGS
  3. REALIGN EXISTING SIGNAL HEADS FOR US 62 APPROACHES



**MAINTENANCE OF TRAFFIC PHASE 1**  
 STA. 188+50.00 TO STA. 194+25.00

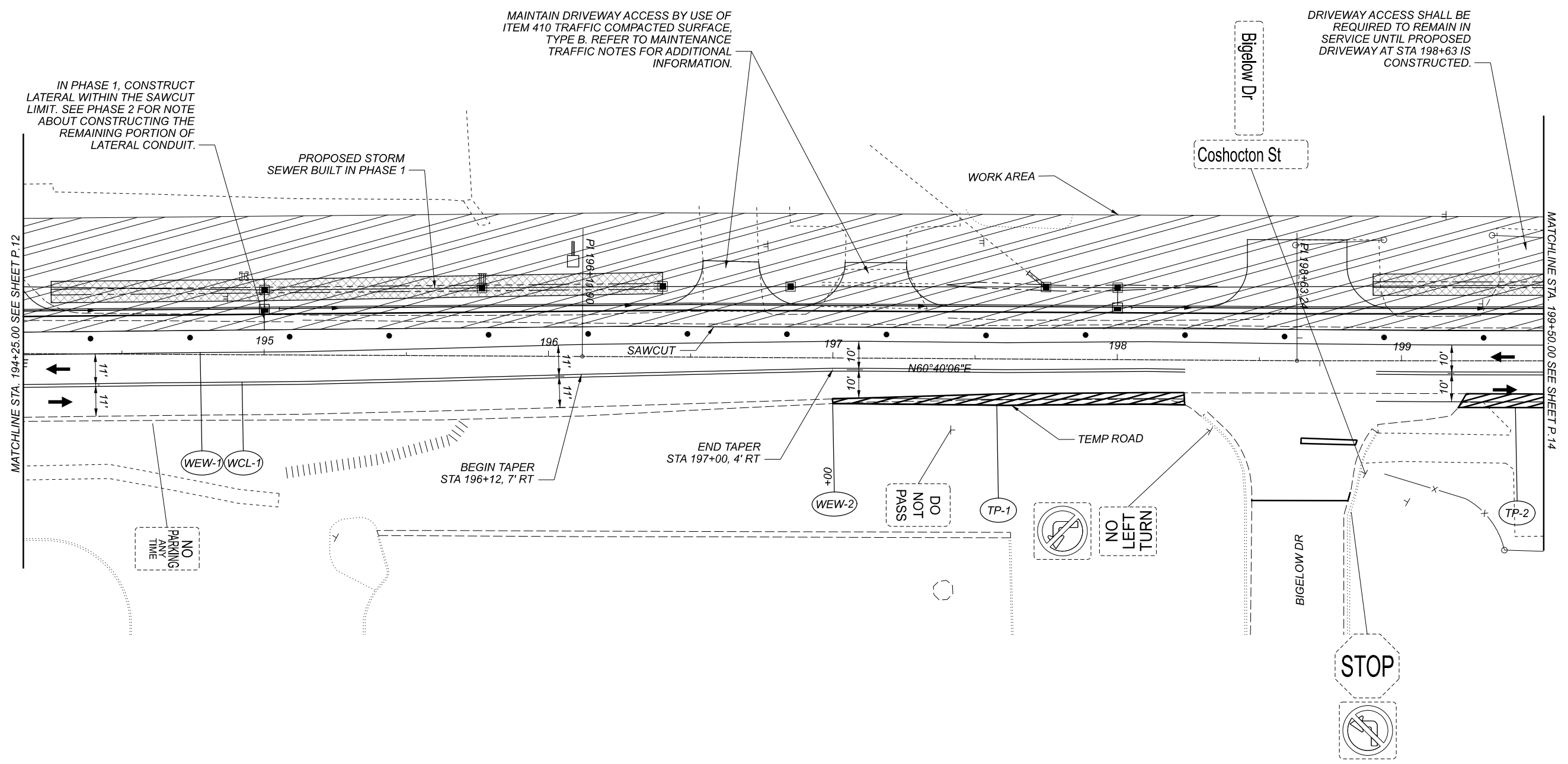
DESIGN AGENCY

DESIGNER  
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REVIEWER  
 JDH 10-29-21

PROJECT ID  
 110861

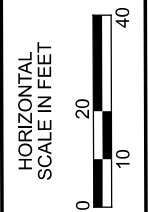
SHEET TOTAL  
 P.12 89



IN PHASE 1, CONSTRUCT LATERAL WITHIN THE SAWCUT LIMIT. SEE PHASE 2 FOR NOTE ABOUT CONSTRUCTING THE REMAINING PORTION OF LATERAL CONDUIT.

MAINTAIN DRIVEWAY ACCESS BY USE OF ITEM 410 TRAFFIC COMPACTED SURFACE, TYPE B. REFER TO MAINTENANCE TRAFFIC NOTES FOR ADDITIONAL INFORMATION.

DRIVEWAY ACCESS SHALL BE REQUIRED TO REMAIN IN SERVICE UNTIL PROPOSED DRIVEWAY AT STA 198+63 IS CONSTRUCTED.



MATCHLINE STA. 194+25.00 SEE SHEET P.12

MATCHLINE STA. 199+50.00 SEE SHEET P.14

**MAINTENANCE OF TRAFFIC PHASE 1**  
 STA. 194+25.00 TO STA. 199+50.00

NO PARKING ANY TIME

WEW-1 WCL-1

BEGIN TAPER STA 196+12, 7' RT

END TAPER STA 197+00, 4' RT

WEW-2

DO NOT PASS

TP-1

NO LEFT TURN

STOP

TP-2

- NOTES:  
 1. FOR LEGEND SEE SHEET P.11  
 2. REMOVE CONFLICTING MARKINGS

DESIGN AGENCY

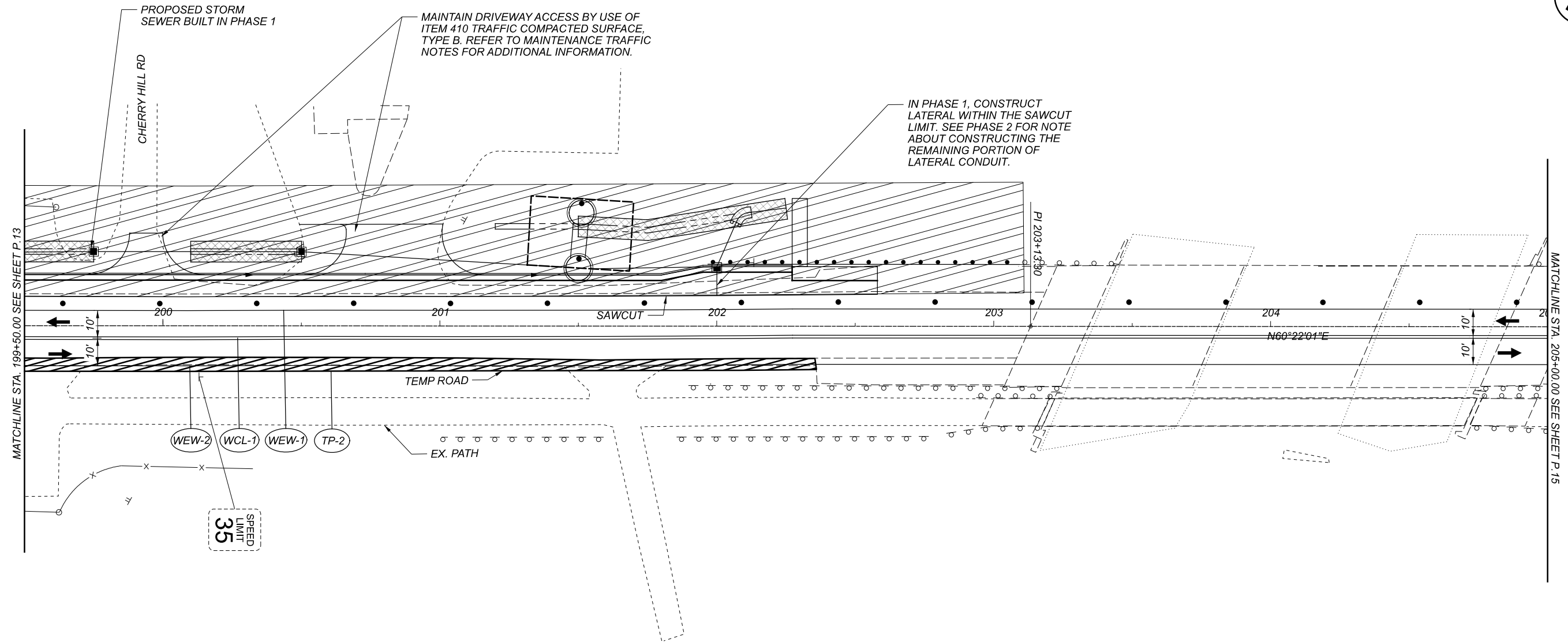


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REVIEWER  
 JDH 10-29-21

PROJECT ID  
 110861

SHEET	TOTAL
P.13	89



MAINTENANCE OF TRAFFIC PHASE 1  
 STA. 199+50.00 TO STA. 205+00.00

- NOTES:  
 1. FOR LEGEND SEE SHEET P.11  
 2. REMOVE CONFLICTING MARKINGS

DESIGN AGENCY

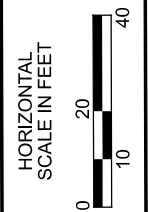
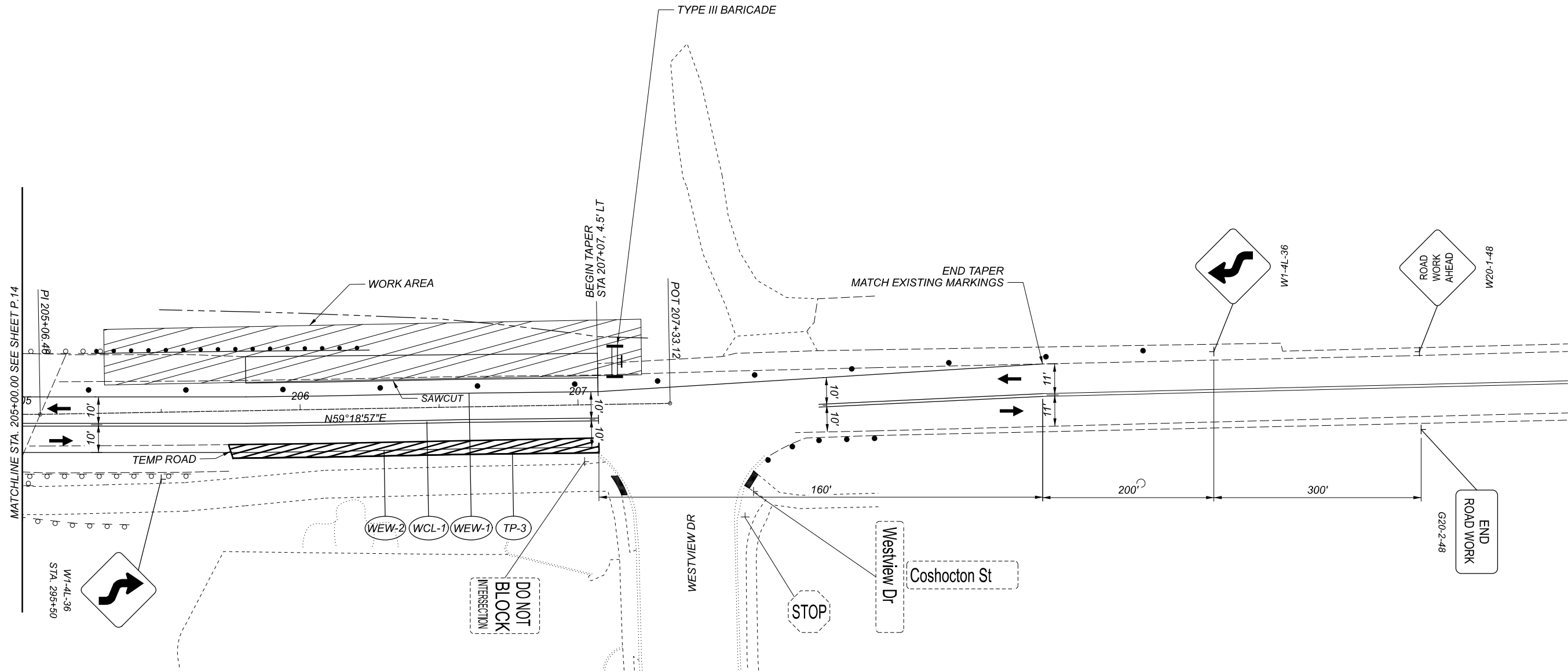


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PROJECT ID  
 110861

SHEET	TOTAL
P. 14	89



MAINTENANCE OF TRAFFIC PHASE 1  
 STA. 205+00.00 TO END

NOTES:  
 1. FOR LEGEND SEE SHEET P.11  
 2. REMOVE CONFLICTING MARKINGS

DESIGN AGENCY

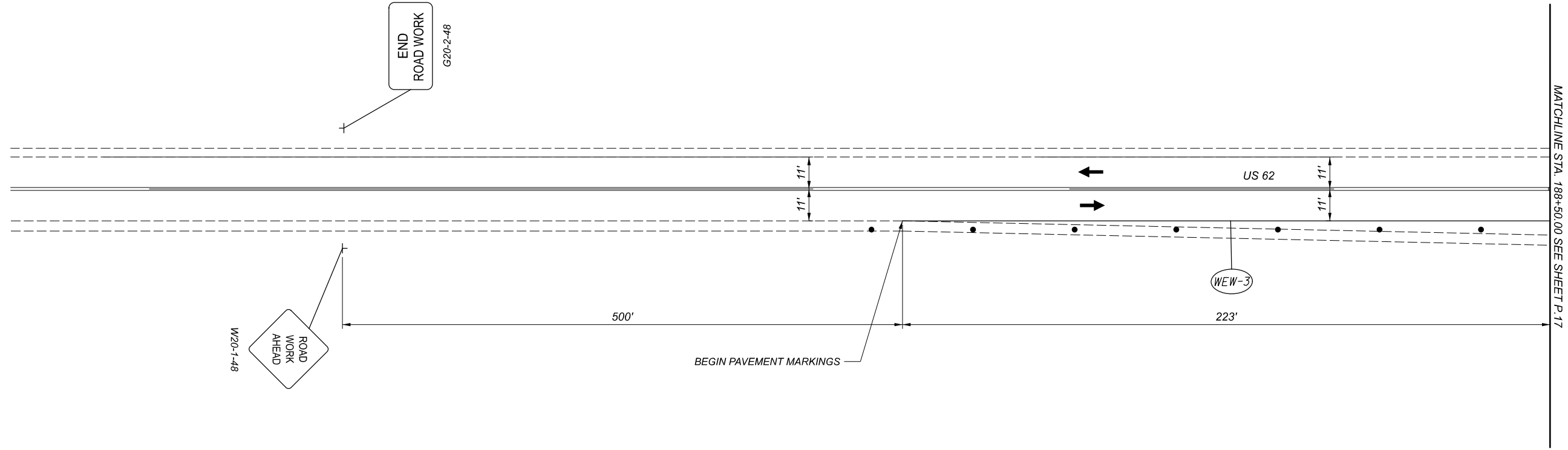


DESIGNER  
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 JDH 10-29-21

PROJECT ID  
 110861

SHEET	TOTAL
P. 15	89

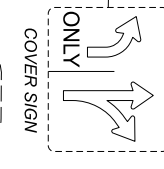
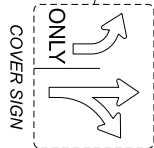
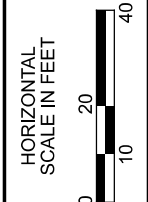
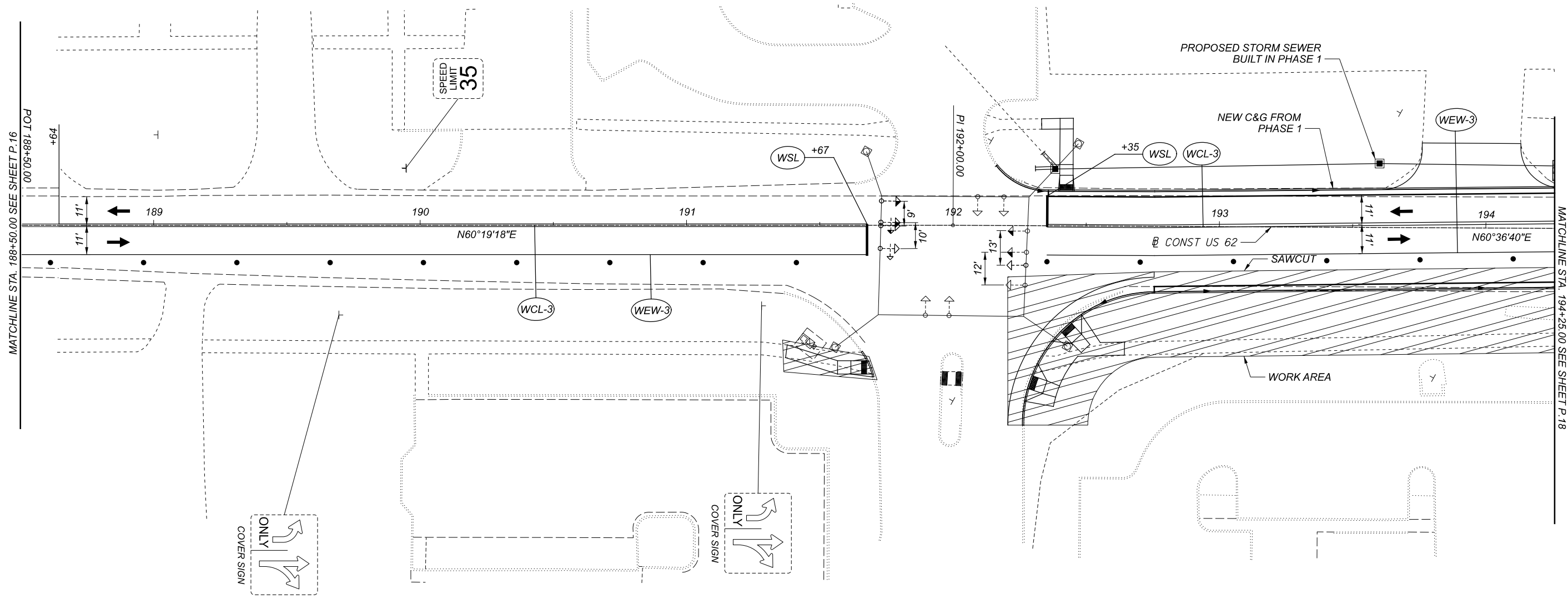


MAINTENANCE OF TRAFFIC PHASE 2  
 BEGINNING TO STA. 188+50.00

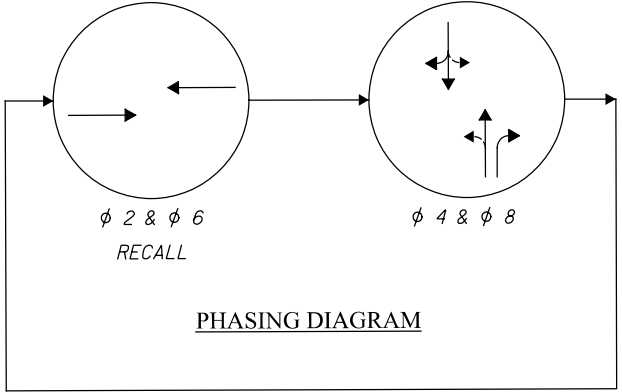
- NOTES:  
 1. FOR LEGEND SEE SHEET P.11  
 2. REMOVE CONFLICTING MARKINGS

DESIGN AGENCY	
<b>[BI]</b>	
DESIGNER	BSS
REVIEWER	JDH 10-29-21
PROJECT ID	110861
SHEET	TOTAL
P.16	89





TEMPORARY TRAFFIC SIGNAL TIMING				
PHASE	2	4	6	8
DIRECTION	EB	NB	WB	SB
MIN GREEN	30	10	30	10
PASSAGE TIME	-	3	-	3
MAX GREEN	56	22	56	22
YELLOW CHANGE	4.1	4.4	4.1	4.4
RED CLEARANCE	1	1	1	1
WALK	7	7	-	-
PED CLEARANCE	17	15	-	-
RECALL	ON	OFF	ON	OFF



- LEGEND**
- EX. 3-SECTION SIGNAL HEAD
  - RELOCATED 3-SECTION SIGNAL HEAD
  - EX. 5-SECTION SIGNAL HEAD
  - RELOCATED 5-SECTION SIGNAL HEAD
- NOTES:**
- FOR LEGEND SEE SHEET P.11
  - REMOVE CONFLICTING MARKINGS
  - REALIGN SIGNAL HEADS FOR US 62 APPROACHES FROM PHASE 1 (SHOWN AS EXISTING) OVER PHASE 2 LANES

**MAINTENANCE OF TRAFFIC PHASE 2**  
 STA. 188+50.00 TO STA. 194+25.00

DESIGN AGENCY

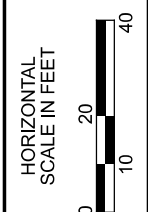
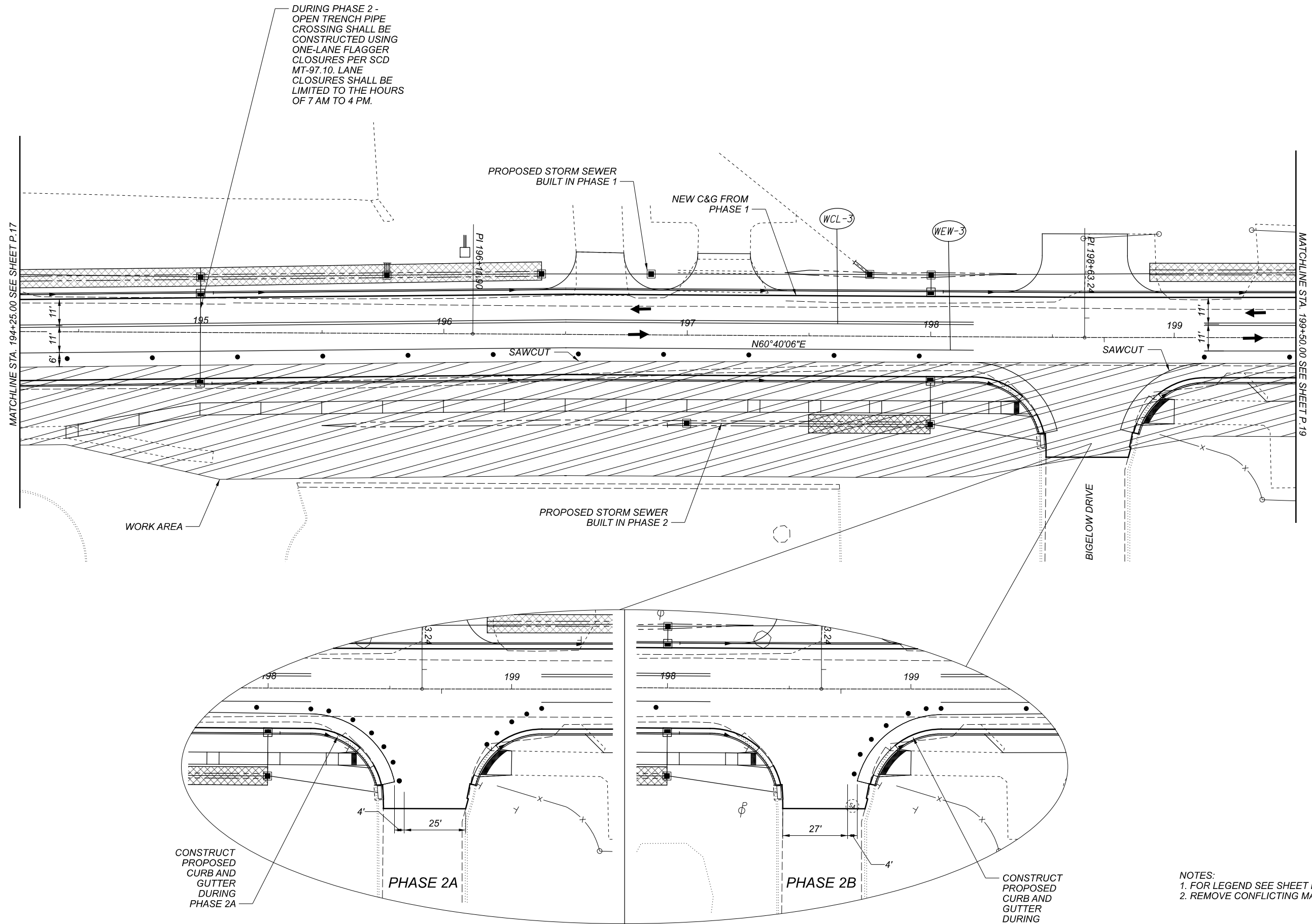


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JDH 10-29-21

PROJECT ID  
110861

SHEET TOTAL  
P.17 89



**MAINTENANCE OF TRAFFIC PHASE 2**  
 STA. 194+25.00 TO STA. 199+50.00

DESIGN AGENCY



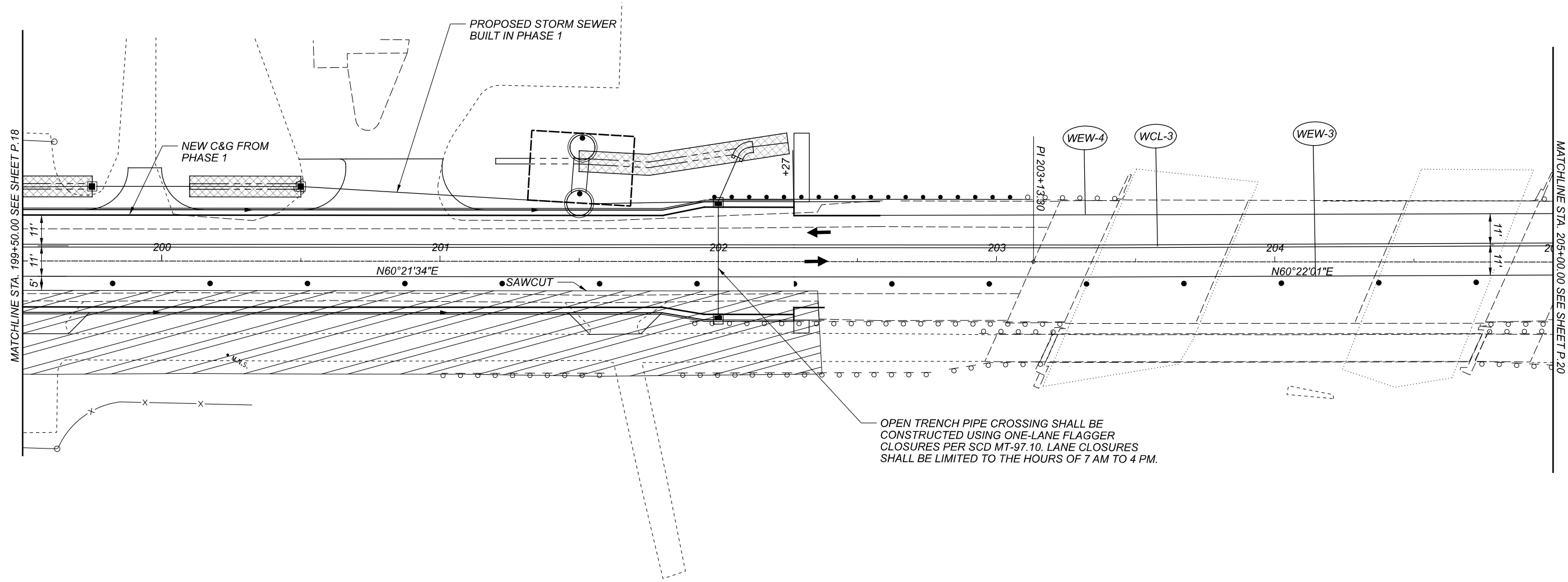
DESIGNER  
 JMB

REVIEWER  
 JDH 10-29-21

PROJECT ID  
 110861

SHEET TOTAL  
 P.18 89

- NOTES:  
 1. FOR LEGEND SEE SHEET P.11  
 2. REMOVE CONFLICTING MARKINGS



NOTES:  
 1. FOR LEGEND SEE SHEET P.11  
 2. REMOVE CONFLICTING MARKINGS

MAINTENANCE OF TRAFFIC PHASE 2  
 STA. 199+50.00 TO STA. 205+00.00

DESIGN AGENCY



DESIGNER  
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REVIEWER  
 JDH 10-29-21

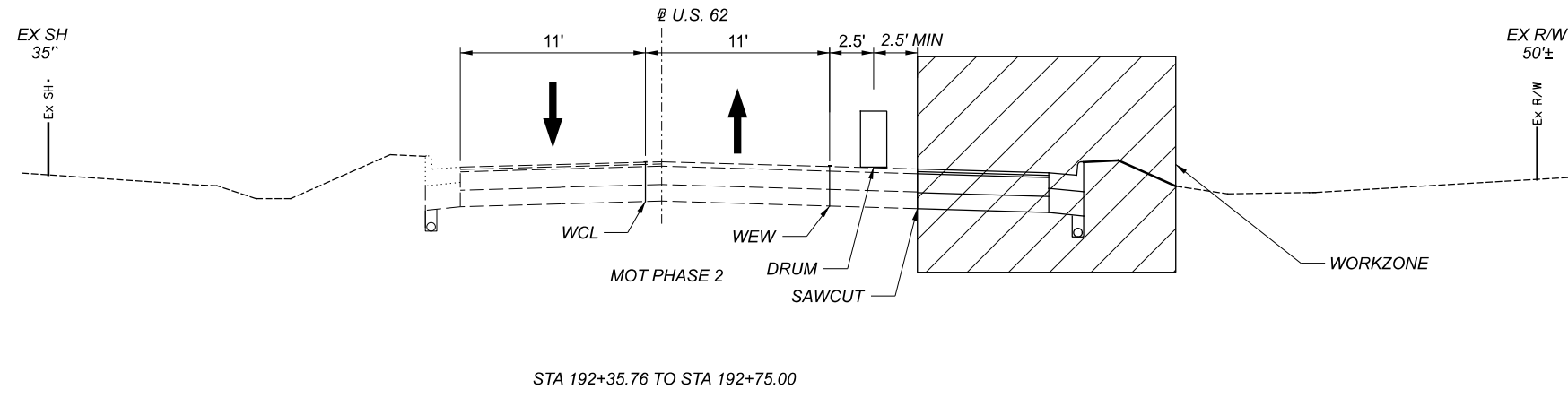
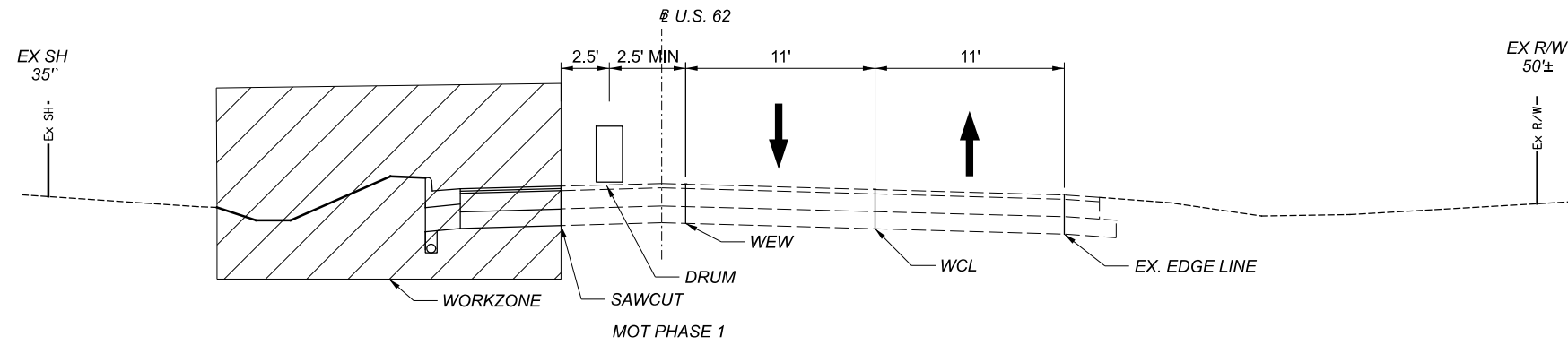
PROJECT ID  
 110861

SHEET	TOTAL
P.19	89

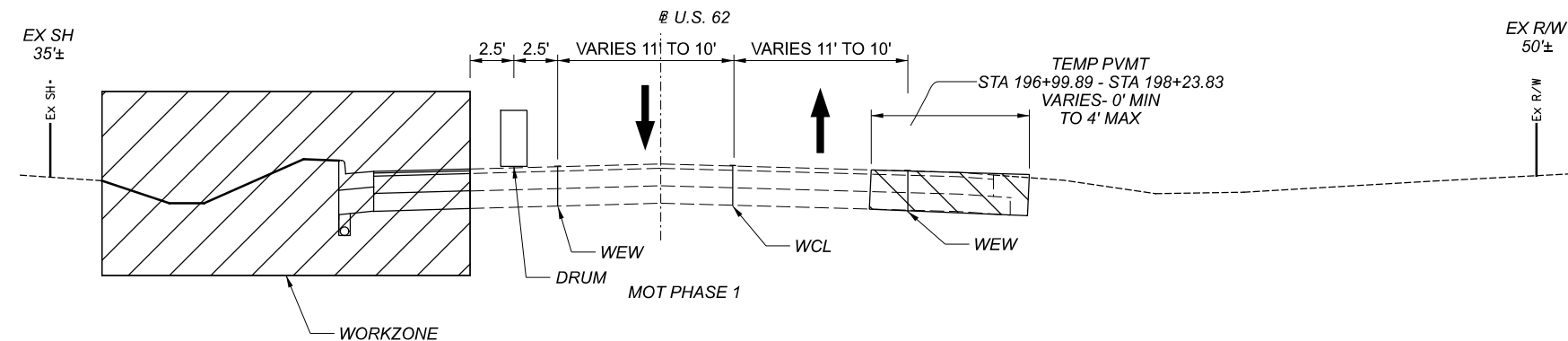


**WORK ZONE PAVEMENT MARKING LEGEND**

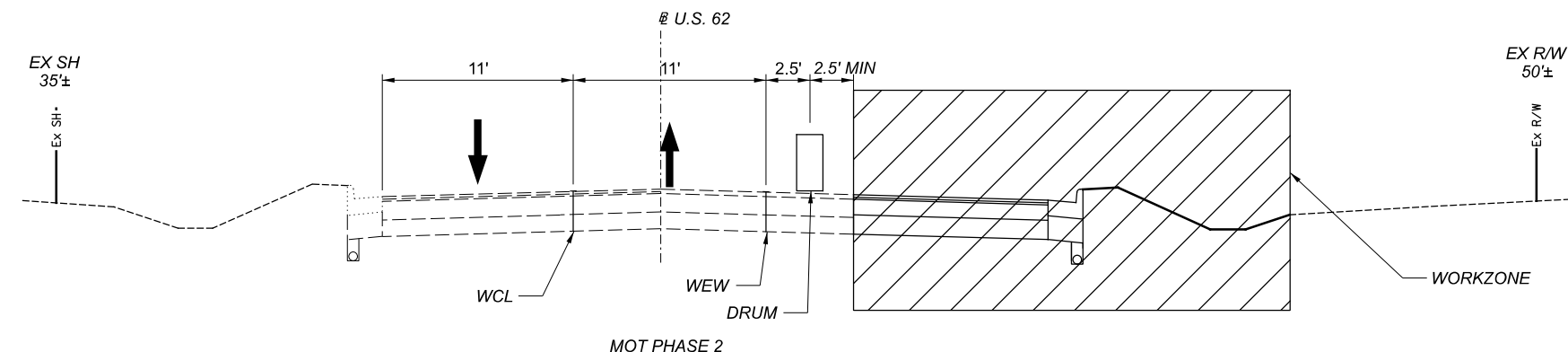
- WEW WORK ZONE EDGE LINE, WHITE
- WCL WORK ZONE CENTER LINE, DOUBLE SOLID



STA 192+35.76 TO STA 192+75.00



TEMP PVMT  
STA 196+99.89 - STA 198+23.83  
VARIES- 0' MIN  
TO 4' MAX



STA 192+75.00 TO STA 199+00.00

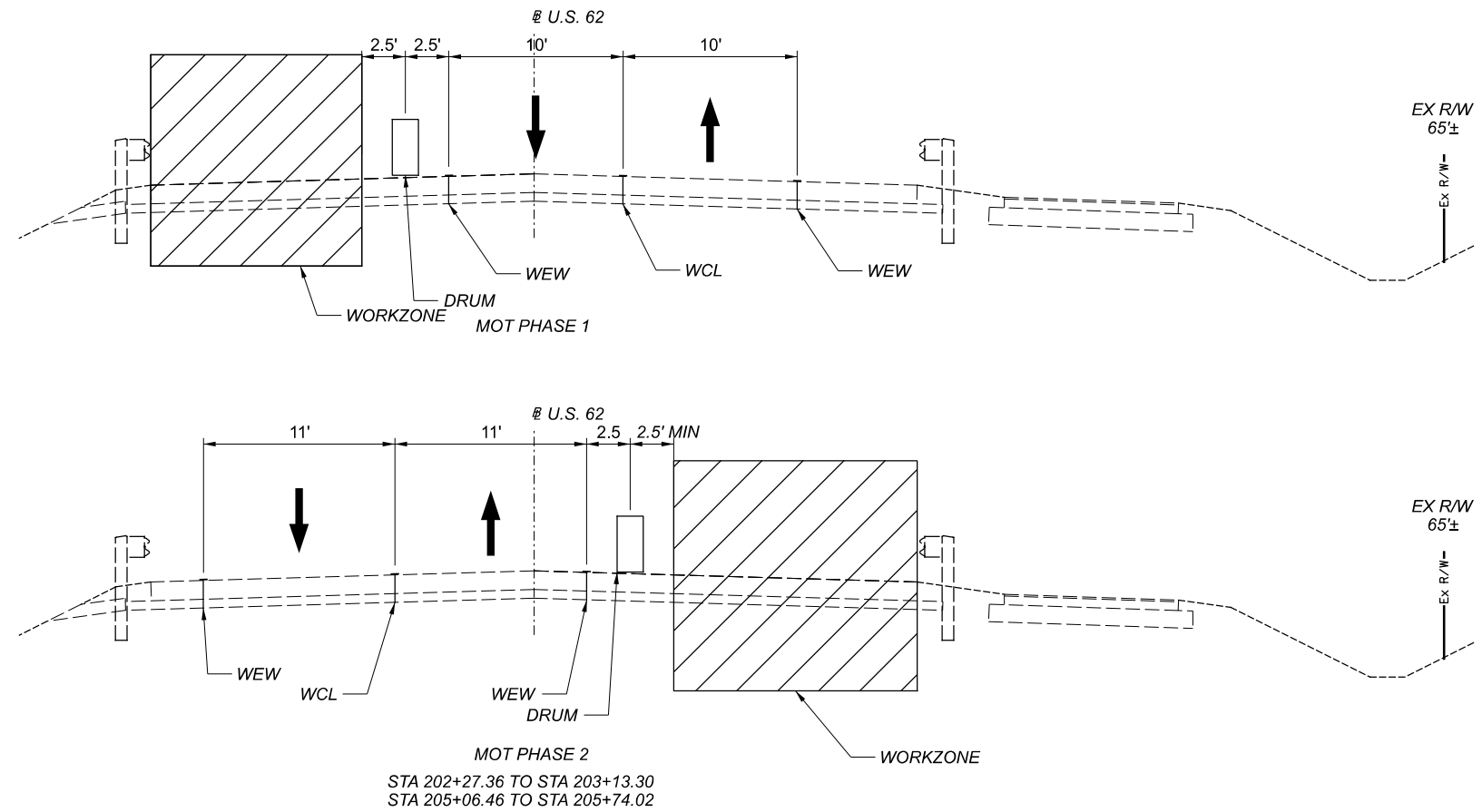
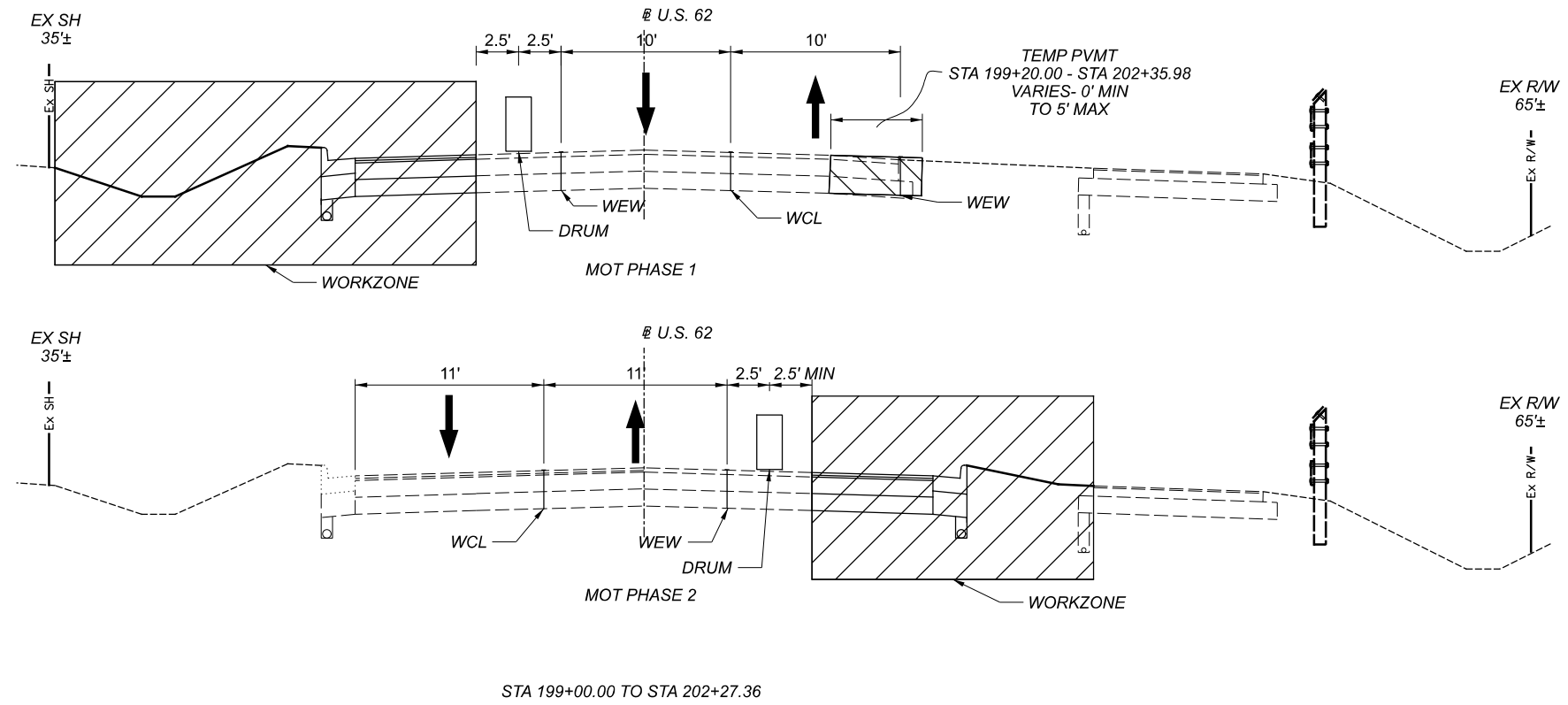
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TYPICAL SECTIONS STA. 192+35.76 TO STA. 199+00.00

DESIGN AGENCY	[B]
DESIGNER	BSS
REVIEWER	JDH
PROJECT ID	110861
SHEET	P.21
TOTAL	89

FOR LEGEND SEE SHEET P.21



LIC-62-3.64

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TYPICAL SECTIONS STA. 199+00.00 TO STA. 203+13.30 AND STA. 205+06.46 TO STA. 205+74.02

DESIGN AGENCY



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REVIEWER

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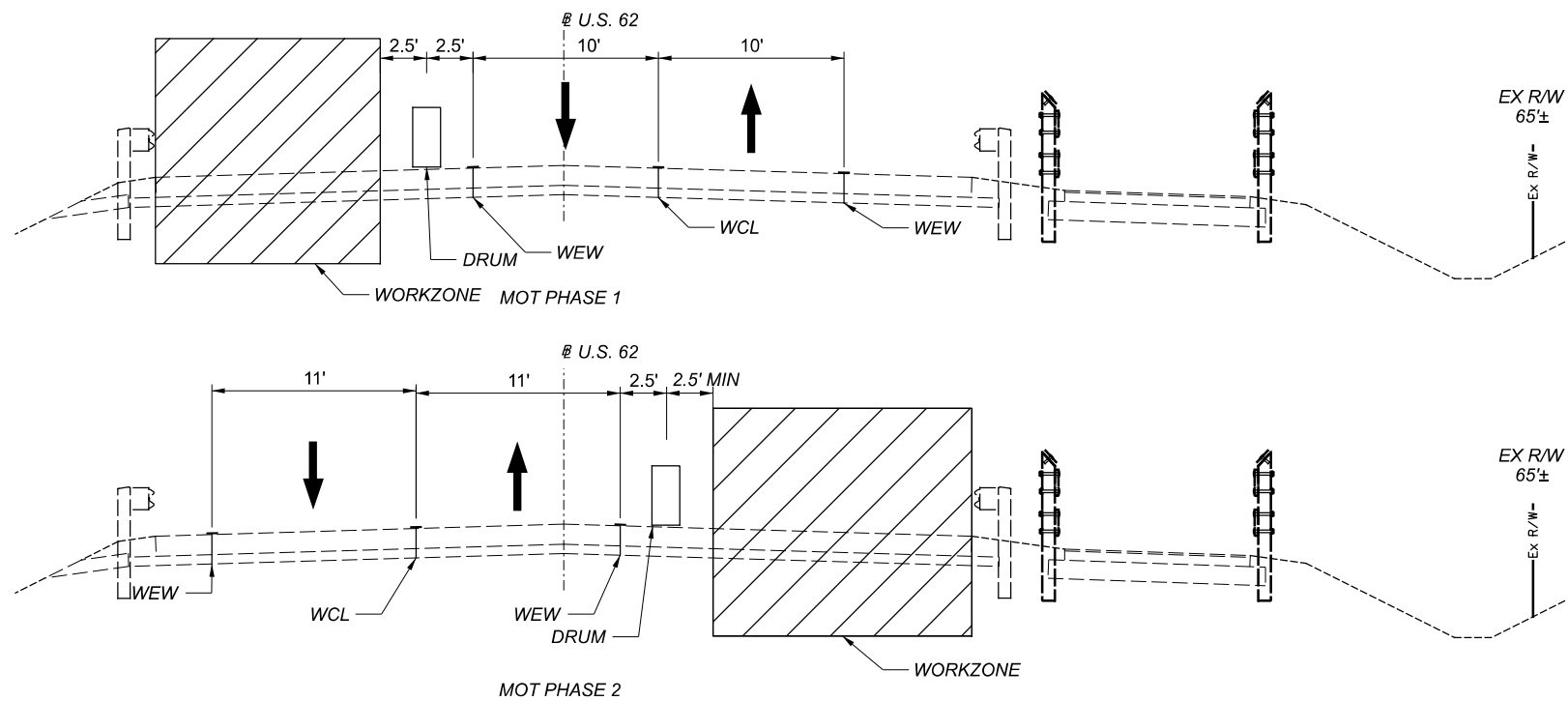
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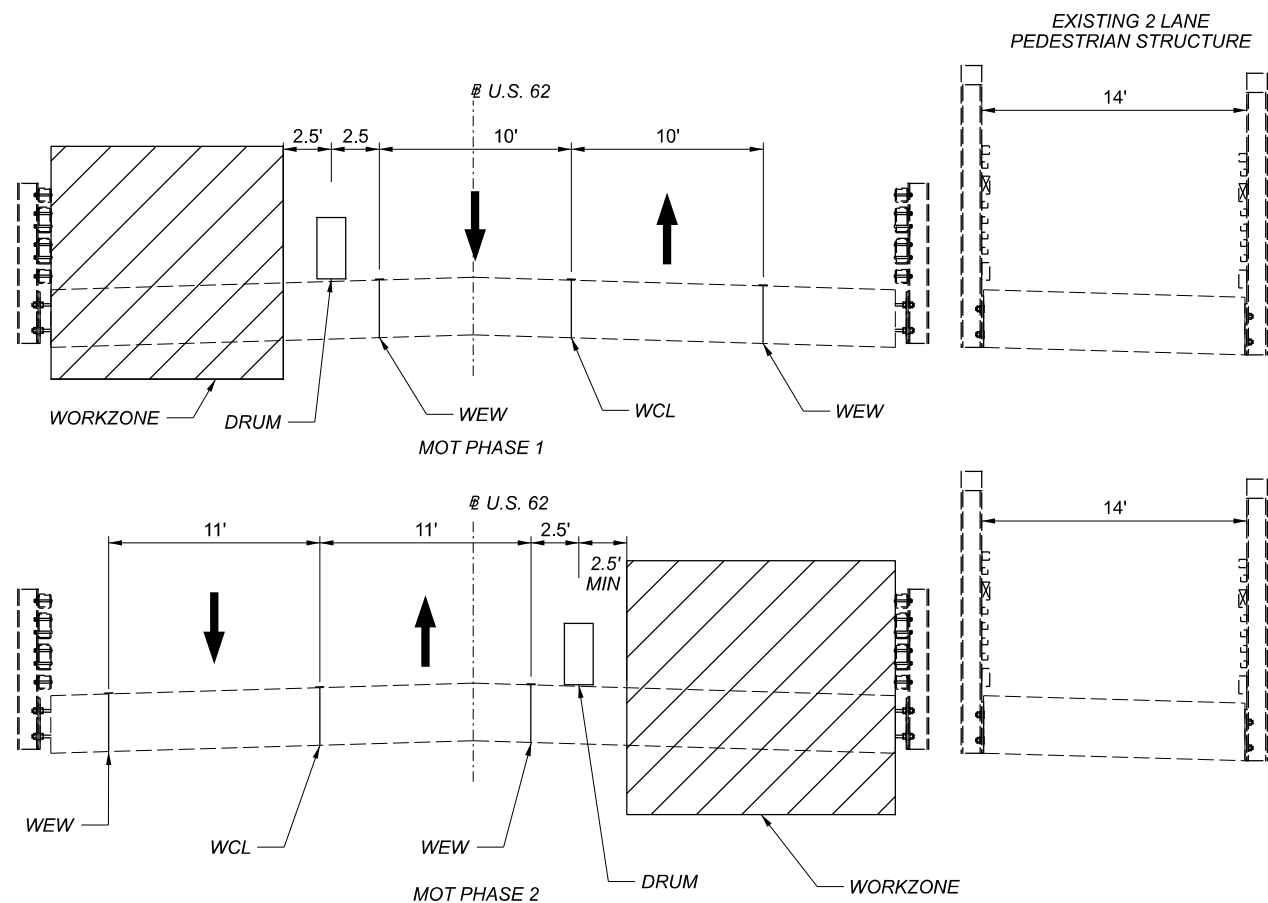
SHEET TOTAL

P.22 89

FOR LEGEND SEE SHEET P.21



PROPOSED RESTRIPING APPROACH SLAB SECTION  
 STA 203+13.30 TO STA 203+33.30  
 STA 204+86.46 TO STA 205+06.46



PROPOSED RESTRIPING BRIDGE SECTION U.S. 62  
 STA 203+33.30 TO STA 204+86.46

LIC-62-3.64

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TYPICAL SECTIONS STA. 203+13.30 TO STA. 205+06.46

DESIGN AGENCY



DESIGNER

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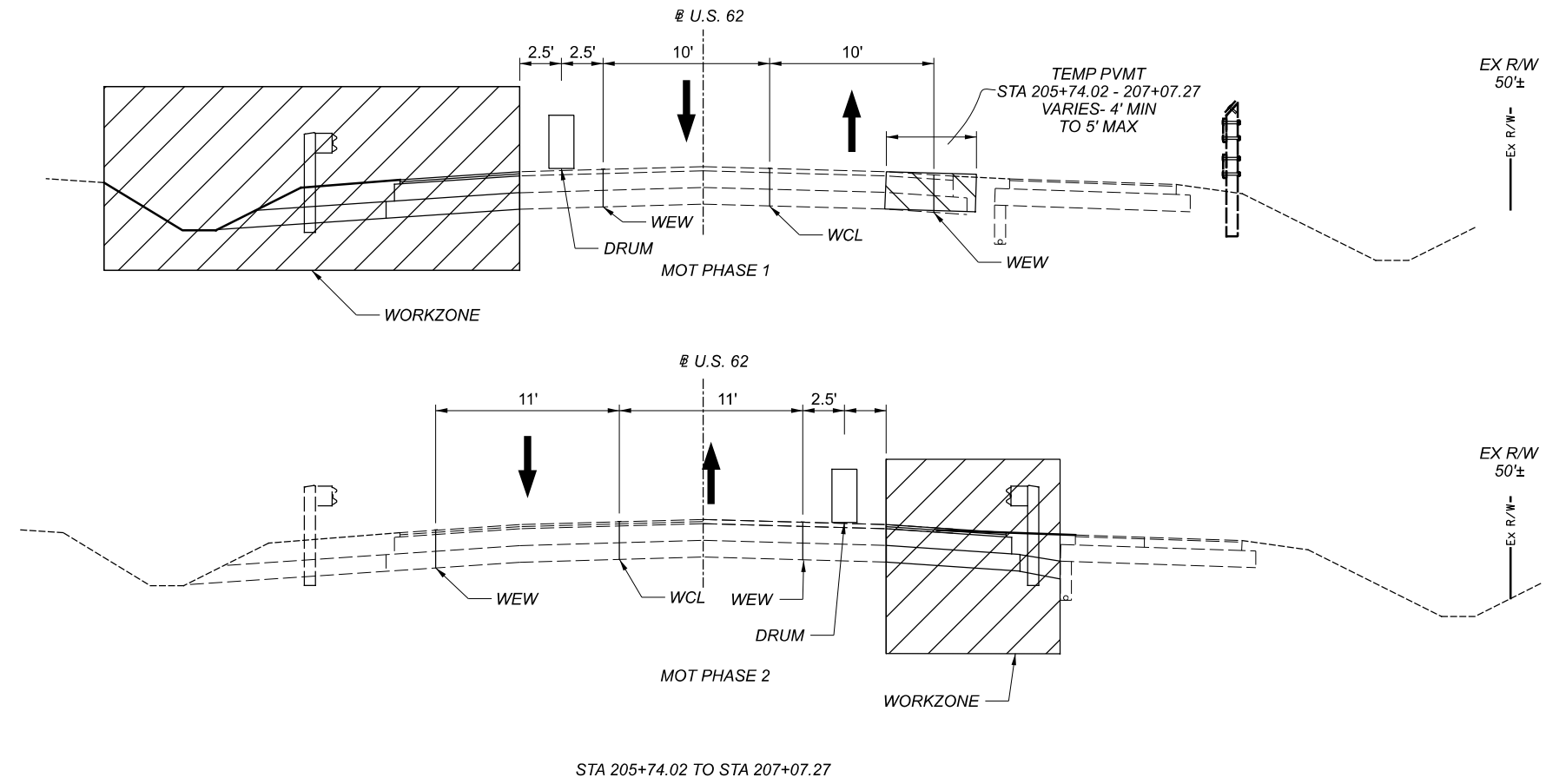
PROJECT ID

110861

SHEET TOTAL

P.23 89

FOR LEGEND SEE SHEET P.21



LIC-62-3.64

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DESIGN AGENCY



DESIGNER

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REVIEWER

JDH 10-29-21

PROJECT ID

110861

SHEET TOTAL


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TYPICAL SECTIONS STA. 205+74.02 TO STA. 207+07.27



SHEET NUM.											PART.	ITEM	ITEM	GRAND	UNIT	DESCRIPTION	SEE SHEET NO.	
OFFICE CALCS	P.6	P.7	P.28	P.29	P.30	P.55	P.75				01/SAF/PV	EXT	TOTAL					
	LS										LS	201	11000	LS		ROADWAY		
1,439		200			20	628					2,287	202	23000	2,287	SY	CLEARING AND GRUBBING		
			743								743	202	30000	743	SF	PAVEMENT REMOVED		
			91								91	202	32000	91	FT	WALK REMOVED		
			43								43	202	32500	43	FT	CURB REMOVED		
					1,101						1,101	202	35100	1,101	FT	CURB AND GUTTER REMOVED		
			2								2	202	42010	2	EACH	PIPE REMOVED, 24" AND UNDER		
			9								9	202	53100	9	EACH	ANCHOR ASSEMBLY REMOVED, TYPE E		
					1						1	202	58000	1	EACH	MAILBOX REMOVED		
					11						11	202	58100	11	EACH	MANHOLE REMOVED		
			2								2	202	75250	2	EACH	CATCH BASIN REMOVED		
			9								9	202	98100	9	EACH	GATE REMOVED		
			1								1	202	98100	1	EACH	REMOVAL MISC.: WOOD POST		
			1								1	202	98100	1	EACH	REMOVAL MISC.: ROCK		
				582							582	203	10000	582	CY	REMOVAL MISC.: PRV. SIGN & 1' HIGH LANDSCAPE TIMBERS		
					404						404	203	20000	404	CY	EXCAVATION		
2,430						419					2,849	204	10000	2,849	SY	EMBANKMENT		
	2										2	204	45000	2	HOUR	SUBGRADE COMPACTION		
			770								770	517	73501	770	FT	PROOF ROLLING	P.7	
			50								50	606	15050	50	FT	RAILING, PIPE, AS PER PLAN		
											100	606	15100	100	FT	GUARDRAIL, TYPE MGS		
			1								1	606	26150	1	EACH	GUARDRAIL, TYPE MGS WITH LONG POSTS		
			1								1	606	26550	1	EACH	ANCHOR ASSEMBLY, MGS TYPE E, MASH 2016		
			2,400								2,400	608	10000	2,400	SF	ANCHOR ASSEMBLY, MGS TYPE T		
			533								533	608	52000	533	SF	4" CONCRETE WALK		
											59	608	53020	59	SF	CURB RAMP		
											1	623	38500	1	EACH	DETECTABLE WARNING		
											8	623	40500	8	EACH	MONUMENT ASSEMBLY		
											5	623	40520	5	EACH	REFERENCE MONUMENT		
											5	SPECIAL	69050100	5	EACH	RIGHT-OF-WAY MONUMENT	P.6	
											2	SPECIAL	69050200	2	EACH	MAILBOX SUPPORT SYSTEM, SINGLE		
											2	SPECIAL	69050200	2	EACH	MAILBOX SUPPORT SYSTEM, DOUBLE	P.6	
					2						2	601	21050	2	SY	EROSION CONTROL		
					18						18	601	21060	18	SY	TIED CONCRETE BLOCK MAT WITH TYPE 1 UNDERLAYMENT		
					2						2	601	32204	2	CY	TIED CONCRETE BLOCK MAT WITH TYPE 2 UNDERLAYMENT		
	3,595										3,595	659	00510	3,595	SY	ROCK CHANNEL PROTECTION, TYPE C WITH GEOTEXTILE FABRIC		
	0.49										0.49	659	20000	0.49	TON	SEEDING AND MULCHING, CLASS 2		
											0.74	659	31000	0.74	ACRE	COMMERCIAL FERTILIZER		
											19	659	35000	19	MGAL	LIME		
											325	670	00710	325	SY	WATER		
											LS	832	15000	LS		DITCH EROSION PROTECTION MAT, TYPE A		
											LS	832	15002	LS		STORM WATER POLLUTION PREVENTION PLAN		
											LS	832	15010	LS		STORM WATER POLLUTION PREVENTION INSPECTIONS		
											23,454	832	30000	23,454	EACH	STORM WATER POLLUTION PREVENTION INSPECTION SOFTWARE		
					63						63	836	10000	63	SY	EROSION CONTROL		
																SEEDING AND EROSION CONTROL WITH TURF REINFORCING MAT, TYPE 1		
					0.5						0.5	602	20000	0.5	CY	DRAINAGE		
					1,864						1,864	605	14020	1,864	FT	CONCRETE MASONRY		
59											59	605	31100	59	FT	6" BASE PIPE UNDERDRAINS WITH GEOTEXTILE FABRIC		
											70	611	00510	70	FT	AGGREGATE DRAINS		
											100	611	02000	100	FT	6" CONDUIT, TYPE F FOR UNDERDRAIN OUTLETS		
											10	611	02000	10	FT	8" CONDUIT, TYPE C		
											10	611	02000	10	FT	8" CONDUIT, TYPE C, 707.33		
											15	611	02500	15	FT	8" CONDUIT, TYPE C, 707.45		
											158	611	04400	158	FT	8" CONDUIT, TYPE E		
											219	611	04600	219	FT	12" CONDUIT, TYPE B		
																12" CONDUIT, TYPE C		

GENERAL SUMMARY

DESIGN AGENCY  
  
 DESIGNER  
 AJP  
 REVIEWER  
 JDH 10-29-21  
 PROJECT ID  
 110861  
 SHEET TOTAL  
 P.25 89



SHEET NUM.										PART.	ALT	ITEM	ITEM	GRAND	UNIT	DESCRIPTION	SEE
P.8	P.9	P.10	P.73							01/ SAF/PV	(X)	EXT	TOTAL				SHEET NO.
<b>TRAFFIC SIGNALS CONTINUED</b>																	
			6							6		632	05006	6	EACH	VEHICULAR SIGNAL HEAD, (LED), 3-SECTION, 12" LENS, 1-WAY, POLYCARBONATE, BLACK	
			2							2		632	05086	2	EACH	VEHICULAR SIGNAL HEAD, (LED), 5-SECTION, 12" LENS, 1-WAY, POLYCARBONATE, BLACK	
			4							4		632	20731	4	EACH	PEDESTRIAN SIGNAL HEAD (LED), TYPE D2, COUNTDOWN, AS PER PLAN	P.65
			8							8		632	25000	8	EACH	COVERING OF VEHICULAR SIGNAL HEAD	
			4							4		632	25010	4	EACH	COVERING OF PEDESTRIAN SIGNAL HEAD	
			2							2		632	26000	2	EACH	PEDESTRIAN PUSHBUTTON	
			890							890		632	40500	890	FT	SIGNAL CABLE, 5 CONDUCTOR, NO. 14 AWG	
			645							645		632	40700	645	FT	SIGNAL CABLE, 7 CONDUCTOR, NO. 14 AWG	
			4							4		632	64010	4	EACH	SIGNAL SUPPORT FOUNDATION	P.67
			4							4		632	64950	4	EACH	TEST HOLE PERFORMED	P.67
			50							50		632	68200	50	FT	POWER CABLE, 2 CONDUCTOR, NO. 6 AWG	
			120							120		632	69800	120	FT	SERVICE CABLE, 3 CONDUCTOR, NO. 6 AWG	
			1							1		632	70000	1	EACH	POWER SERVICE	P.65
			1							1		632	70400	1	EACH	CONDUIT RISER, 2" DIAMETER	
			2							2		632	72101	2	EACH	SIGNAL SUPPORT, TYPE TC-81.22, DESIGN 2, AS PER PLAN	P.66
			2							2		632	72111	2	EACH	SIGNAL SUPPORT, TYPE TC-81.22, DESIGN 4, AS PER PLAN	P.66
			1							1		632	90100	1	EACH	REMOVAL OF TRAFFIC SIGNAL INSTALLATION	P.65
			1							1		633	65511	1	EACH	CABINET, TYPE TS-2, AS PER PLAN	P.65
			1							1		633	67100	1	EACH	CABINET FOUNDATION	
			1							1		633	75001	1	EACH	UNINTERRUPTIBLE POWER SUPPLY (UPS), 1000 WATT, AS PER PLAN	P.65
			1							1		809	69123	1	EACH	ATC CONTROLLER, AS PER PLAN	P.65
			1							1		815	30001	1	EACH	SPREAD SPECTRUM RADIO, AS PER PLAN	P.66
			1							1		816	30000	1	EACH	VIDEO DETECTION SYSTEM	
<b>TRAFFIC SIGNALS ALTERNATES</b>																	
			2							2	X	632	72101	2	EACH	SIGNAL SUPPORT, TYPE TC-81.22, DESIGN 2, AS PER PLAN	P.67
			2							2	X	632	72111	2	EACH	SIGNAL SUPPORT, TYPE TC-81.22, DESIGN 4, AS PER PLAN	P.67
<b>MAINTENANCE OF TRAFFIC</b>																	
170										170		410	11000	170	CY	TRAFFIC COMPACTED SURFACE, TYPE B	
	60									60		614	11110	60	HOUR	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	
	11									11		614	13312	11	EACH	BARRIER REFLECTOR, TYPE 2, ONE-WAY	
	11									11		614	13350	11	EACH	OBJECT MARKER, ONE WAY	
	14									14		614	18601	14	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN	P.9
		0.79								0.79		614	21100	0.79	MILE	WORK ZONE CENTER LINE, CLASS I, 642 PAINT	
		1.02								1.02		614	22100	1.02	MILE	WORK ZONE EDGE LINE, CLASS I, 4", 642 PAINT	
		22								22		614	26200	22	FT	WORK ZONE STOP LINE, CLASS I, 642 PAINT	
		LS								LS		615	10000	LS		ROADS FOR MAINTAINING TRAFFIC	
		260								260		615	25000	260	SY	PAVEMENT FOR MAINTAINING TRAFFIC, CLASS B	
53										53		616	10000	53	MGAL	WATER	
<b>INCIDENTALS</b>																	
										LS		614	11000	LS		MAINTAINING TRAFFIC	
										5		619	16010	5	MNTH	FIELD OFFICE, TYPE B	
										LS		623	10000	LS		CONSTRUCTION LAYOUT STAKES AND SURVEYING	
										LS		624	10000	LS		MOBILIZATION	

GENERAL SUMMARY

DESIGN AGENCY



DESIGNER

AJP

REVIEWER

JDH 10-29-21

PROJECT ID

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SHEET TOTAL

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SHEET NO.	REF NO.	STATION		ROUTE	SIDE	202	202	202	202	202	202	202	202	202	517	606	606	606	606	608	608	608	609	609	SPECIAL	SPECIAL
						WALK REMOVED	CURB REMOVED	CURB AND GUTTER REMOVED	ANCHOR ASSEMBLY REMOVED, TYPE E	MAILBOX REMOVED	GATE REMOVED	REMOVAL MISC.: WOOD POST	REMOVAL MISC.: ROCK	REMOVAL MISC.: PRV. SIGN & 1' HIGH LANDSCAPE TIMBERS	RAILING, PIPE, AS PER PLAN	GUARDRAIL, TYPE MGS	GUARDRAIL, TYPE MGS WITH LONG POSTS	ANCHOR ASSEMBLY, MGS TYPE E, MASH 2016	ANCHOR ASSEMBLY, MGS TYPE T	4" CONCRETE WALK	CURB RAMP	DETECTABLE WARNING	COMBINATION CURB AND GUTTER, TYPE 2	CURB, TYPE 6	MAILBOX SUPPORT SYSTEM, SINGLE	MAILBOX SUPPORT SYSTEM, DOUBLE
		SF	FT			FT	EACH	EACH	EACH	EACH	EACH	EACH	FT	FT	FT	EACH	EACH	SF	SF	SF	FT	FT	EACH	EACH		
P.32	R-1	191+37	191+69	US 62	R	170.24	10.57																			
P.32	R-2	192+16	192+27	US 62	L		12.83																			
P.32	R-3	192+26	192+65	US 62	R	162.10	67.81																			
P.32	R-4	192+50		US 62	L																			3		
P.33	R-5	194+45	195+05	US 62	R	299.02																			3	
P.33	R-6	196+29		US 62	L																				2	
P.33	R-7	198+32	198+47	US 62	R			26.56																	2	
P.33	R-8	198+39		US 62	L									1												
P.34	R-9	198+78	199+47	US 62	L					2																
P.34	R-10	198+83	198+90	US 62	R			16.69																		
P.34	R-11	198+89	199+00	US 62	R	111.90																				
P.34	R-12	200+07		US 62	L					4															2	
P.34	R-13	200+50		US 62	L								9													
P.34	R-14	201+10		US 62	L									1												
P.34	GR-1	201+96.80	203+09.30	US 62	L																					
	GR-2	NOT USED																								
P.35	GR-3	205+25.26	206+25.25	US 62	L																					
P.32	DW-1	191+97.00		US 62	R																				10.30	
P.32	DW-2	192+02.00		US 62	R																				10.07	
P.35	DW-3	207+13.00		US 62	R																				26.24	
P.35	DW-4	207+62.00		US 62	R																				12.25	
P.32	SW-1	191+37.48	191+56.70	US 62	R																				95.77	
P.32	SW-2	192+32.65	192+64.57	US 62	R																				294.23	
P.32	SW-3	192+33.10	192+42.50	US 62	L																				114.16	
P.33	SW-4	194+45.00	198+24.03	US 62	R										769.8										1895.93	
P.32	CR-1	191+63.00		US 62	R																				69.97	
P.32	CR-2	192+33.00		US 62	R																				83.97	
P.32	CR-3	192+43.00		US 62	L																				53.00	
P.32	CR-4	192+45.00		US 62	R																				83.88	
P.33	CR-5	198+34.00		US 62	R																				104.31	
P.34	CR-6	198+93.00		US 62	R																				138.08	
P.32-34	CG-1	192+35.76	202+27.19	US 62	L																				991.70	
P.32-33	CG-2	192+75.64	198+37.85	US 62	R																				564.22	
P.33	CG-3	198+43.65	198+46.76	US 62	R																				8.49	
P.34	CG-4	198+83.36	198+84.60	US 62	R																				4.00	
P.34	CG-5	198+93.57	202+27.19	US 62	R																				335.36	
P.32	C-1	192+16.18	192+35.76	US 62	L																				22.49	
P.32	C-2	192+26.48	192+27.23	US 62	R																				8.98	
P.32	C-3	192+31.99	192+37.40	US 62	R																				9.97	
P.32	C-4	192+48.36	192+75.67	US 62	R																				29.05	
SUBTOTALS						743.26	91.21	43.25	2	9	2	9	1	1	769.79	50.0	100.0	1	1	2400.09	533.21	58.86	1903.77	73.52	5	2
TOTALS CARRIED TO GENERAL SUMMARY						743	91	43	2	9	2	9	1	1	770	50	100	1	1	2400	533	59	1904	74	5	2

ROADWAY SUBSUMMARY

DESIGN AGENCY



DESIGNER

MEP

REVIEWER

JDH 10-29-21

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110861

SHEET TOTAL

P.28 89

**EARTHWORK SUBSUMMARY**

PLAN SHEET NO.	STATION		203	203	659	
			EXCAVATION	EMBANKMENT	SEEDING AND MULCHING, CLASS 2	
	FROM	TO	CY	CY	SY	
	US 62					
P.36	192+76.00	193+00.00	26	4	SEE GENERAL NOTES FOR CADD AREA	
P.37	193+25.00	193+75.00	81	13		
P.38	194+00.00	194+50.00	86	21		
P.39	194+75.00	195+25.00	95	28		
P.40	195+50.00	196+00.00	81	27		
P.41	196+25.00	196+75.00	116	15		
P.42	197+00.00	197+50.00	134	26		
P.43	197+75.00	199+25.00	220	59		
P.44	199+50.00	200+00.00	94	3		
P.45	200+25.00	200+75.00	84	5		
P.46	201+00.00	201+25.00	61	4		
P.47	201+50.00	201+75.00	56	21		
P.48	202+00.00	202+25.00	115 (A)	101 (A)		
P.49	206+00.00	206+50.00	82 (A)	56 (A)		
P.50	206+75.00	207+07.11	58	21		
	ADJUSTMENT FOR PAVEMENT REMOVED		-480			
	ADJUSTMENT FOR PAVEMENT PLANING		-327			
<b>TOTALS CARRIED TO GENERAL SUMMARY</b>			<b>582</b>	<b>404</b>		--

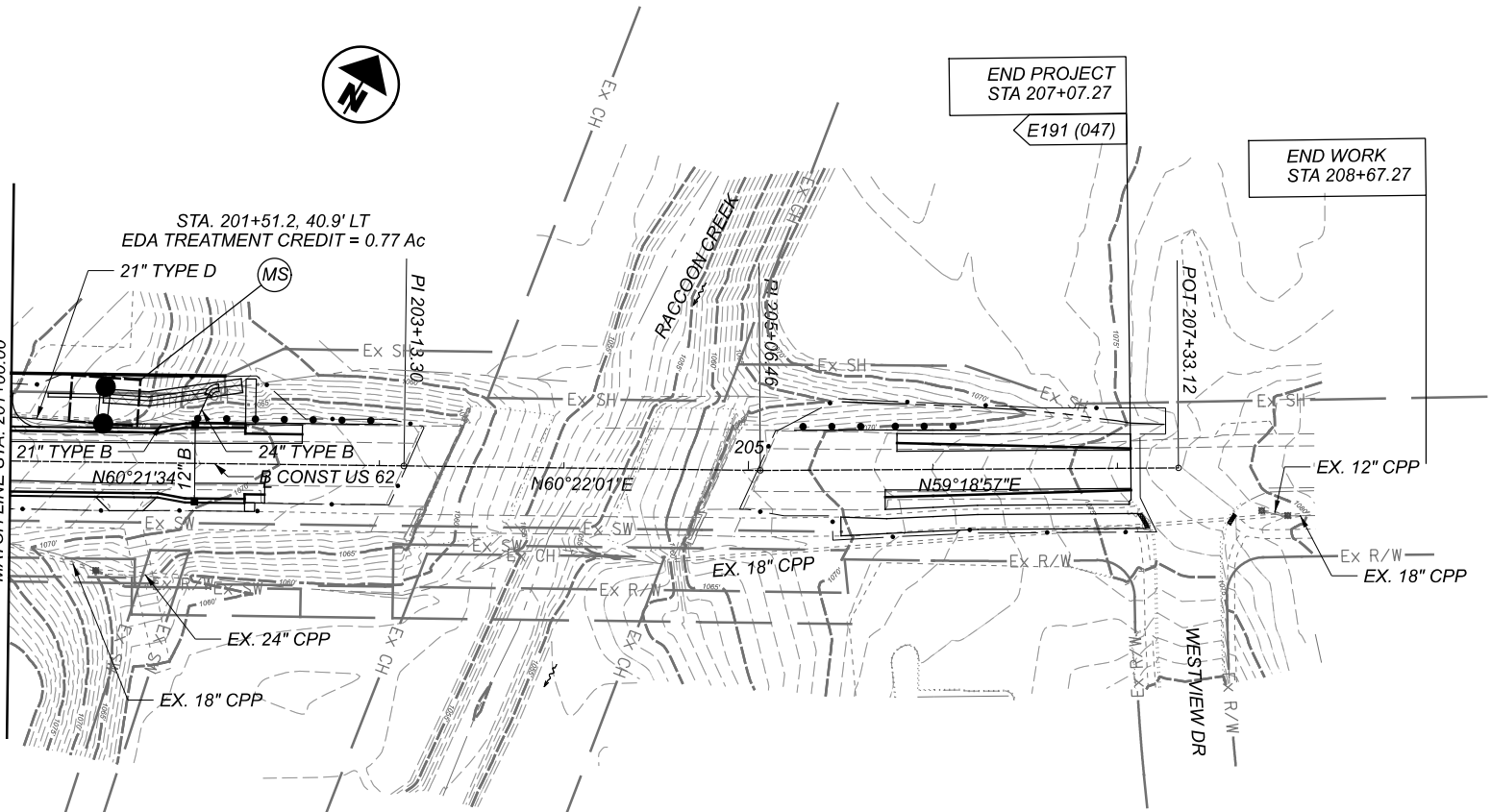
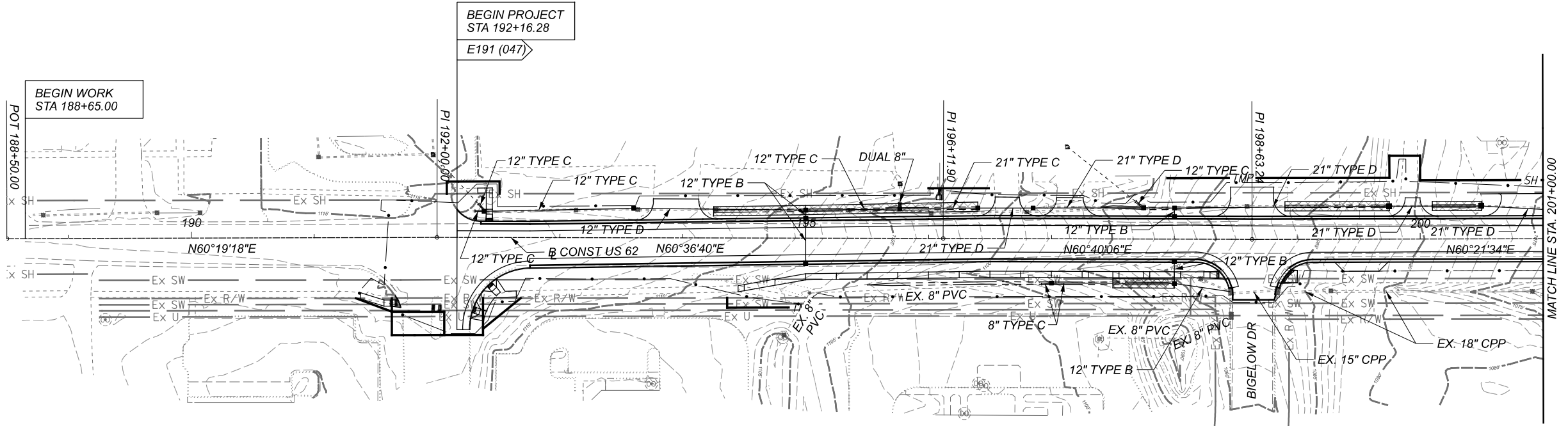
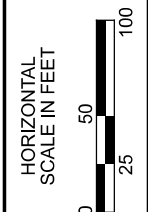
(A) VOLUME INCLUDES SPECIAL BENCHING

**WATER WORK SUBSUMMARY**

SHEET NO.	REFERENCE NO.	STATION		SIDE	638		
					FIRE HYDRANT ADJUSTED TO GRADE	VALVE BOX ADJUSTED TO GRADE	SERVICE BOX ADJUSTED TO GRADE
		FROM	TO		EACH	EACH	EACH
P.33	W-1	194+00		RT	1	1	
P.33	W-2	196+70		RT	1	1	
P.33	W-3	197+00		RT		1	
P.33	W-4	198+38		RT		1	
P.33	W-5	198+49		LT			1
P.34	W-6	200+25		LT			1
P.34	W-7	200+35		LT			1
<b>TOTALS CARRIED TO GENERAL SUMMARY</b>					<b>2</b>	<b>4</b>	<b>3</b>







- LEGEND**
- CATCH BASIN, NO. 2-2B, 3A
  - EX. CATCH BASIN
  - MANHOLE, NO. 3
  - (MS) MANUFACTURED WATER QUALITY STRUCTURE, TYPE 4

**PROJECT DESCRIPTION**  
 IMPROVEMENT OF 0.3 MILES OF US 62 INCLUDING:  
 WIDENING OF US 62 FOR TWO-WAY LEFT TURN LANE  
 BETWEEN VILLAGE SQUARE SHOPPING CENTER TO  
 WESTVIEW DRIVE. ADDING CURB AND GUTTER ALONG  
 WITH UPGRADING OF DRAINAGE SYSTEM AND THE  
 SIGNAL AT VILLAGE SQUARE SHOPPING CENTER.

LATITUDE: 40°08'52"N LONGITUDE: 82°42'01"W  
 USGS QUADRANT MAPS:  
 JOHNSTOWN, OH 2019

PROJECT DATA			
TOTAL AREA (RIGHT-OF-WAY):	3.89 ACRES	RUNOFF COEFFICIENT FOR PRE-CONSTRUCTION SITE:	0.4 TO 0.9
PROJECT EARTH DISTURBED AREA:	1.57 ACRES	RUNOFF COEFFICIENT FOR POST-CONSTRUCTION SITE:	0.7
ESTIMATED CONTRACTOR EARTH DISTURBED AREA:	0.25 ACRES	POST CONSTRUCTION BMP: MANUFACTURED WATER QUALITY STRUCTURE, TYPE 4	
NOTICE OF INTENT EARTH DISTURBED AREA:	4.9 ACRES	IMMEDIATE RECEIVING WATERS:	RACCOON CREEK
IMPERVIOUS (PAVED) AREA FOR PRE-CONSTRUCTION SITE:	1.81 ACRES	SUBSEQUENT RECEIVING WATERS:	SOUTH FORK LICKING RIVER
IMPERVIOUS (PAVED) AREA FOR POST-CONSTRUCTION SITE:	1.95 ACRES		

PROJECT SITE PLAN

DESIGN AGENCY

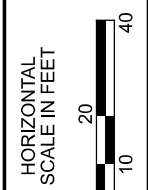
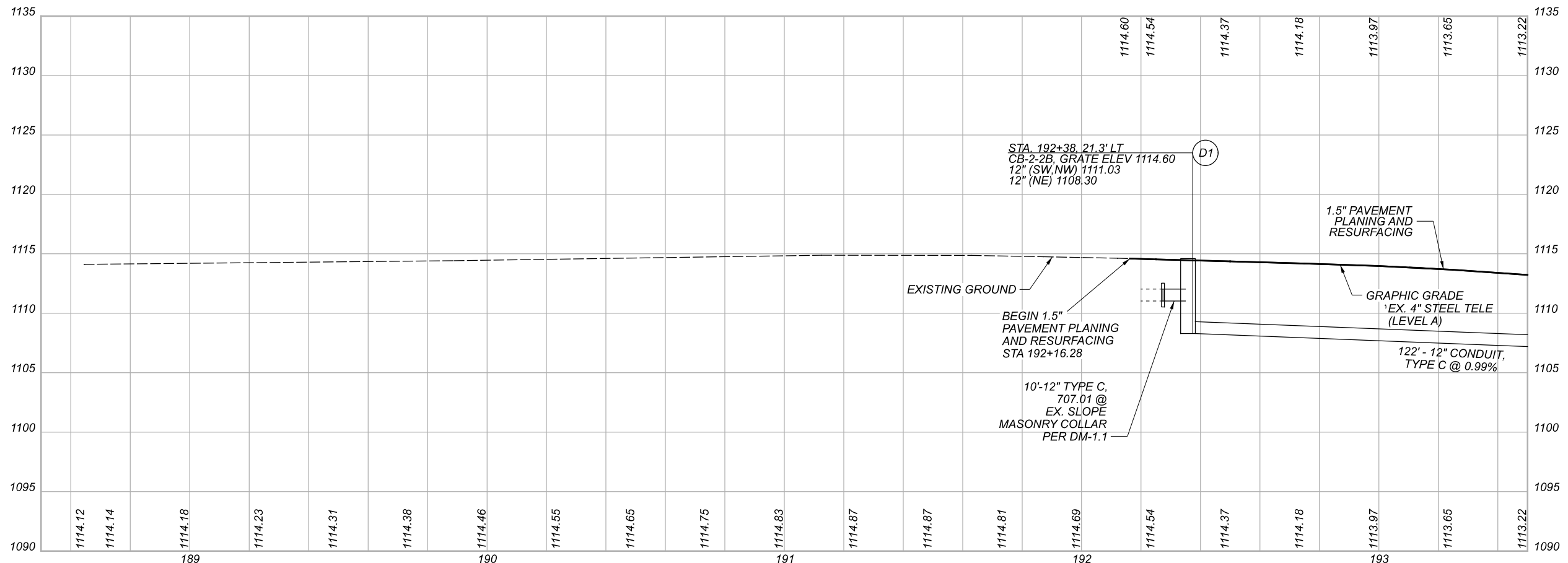
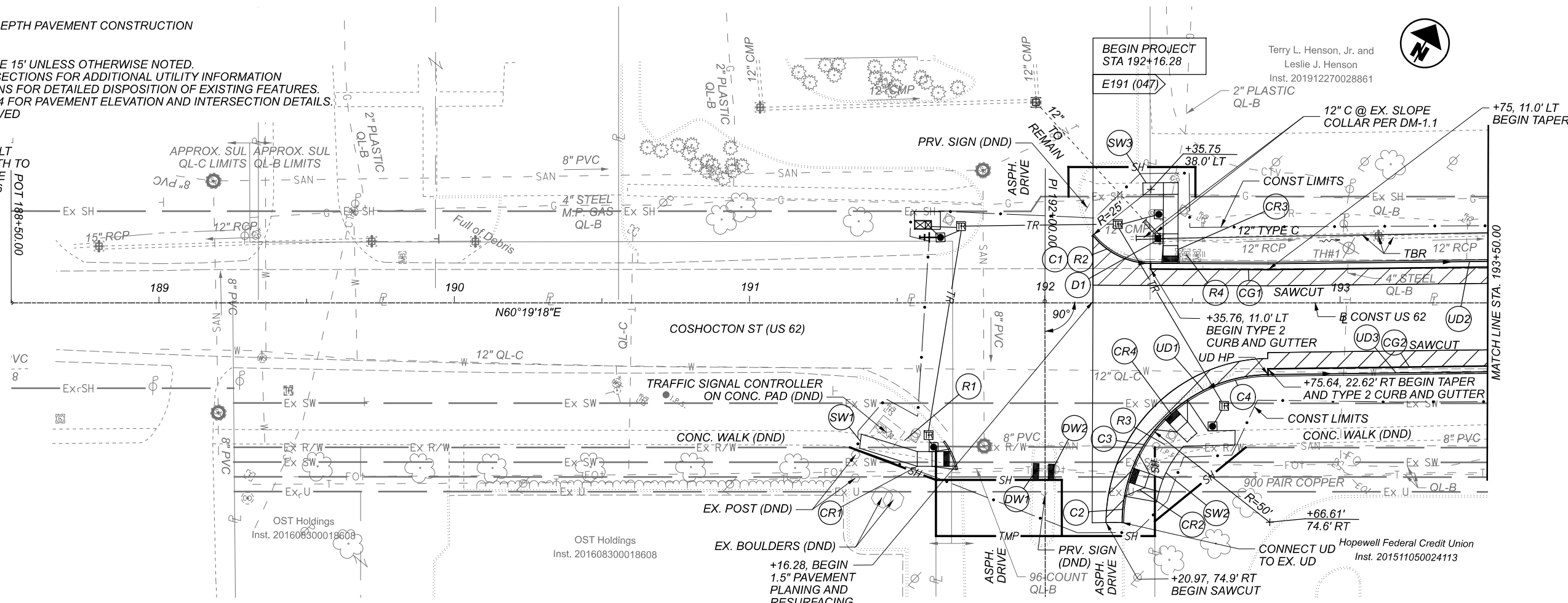


DESIGNER  
 MEP  
 REVIEWER  
 JDH 10-29-21  
 PROJECT ID  
 110861  
 SHEET TOTAL  
 P.31 89

 FULL DEPTH PAVEMENT CONSTRUCTION

- NOTES:  
 1. ALL DRIVE RADII ARE 15' UNLESS OTHERWISE NOTED.  
 2. REFER TO CROSS SECTIONS FOR ADDITIONAL UTILITY INFORMATION  
 3. REFER TO RW PLANS FOR DETAILED DISPOSITION OF EXISTING FEATURES.  
 4. SEE SHEETS P.51-54 FOR PAVEMENT ELEVATION AND INTERSECTION DETAILS.  
 5. TBR = TO BE REMOVED

TH#1  
 STA. 193+02.88, 18.41' LT  
 3.15' MEASURED DEPTH TO  
 TOP EX. 4" STEEL TELE  
 GROUND ELEV 1114.76  
 TOP ELEV 1111.61



PLAN AND PROFILE - US 62  
 STA. 188+50.00 TO STA. 193+50.00

DESIGN AGENCY



DESIGNER  
AJP

REVIEWER  
JDH 10-29-21

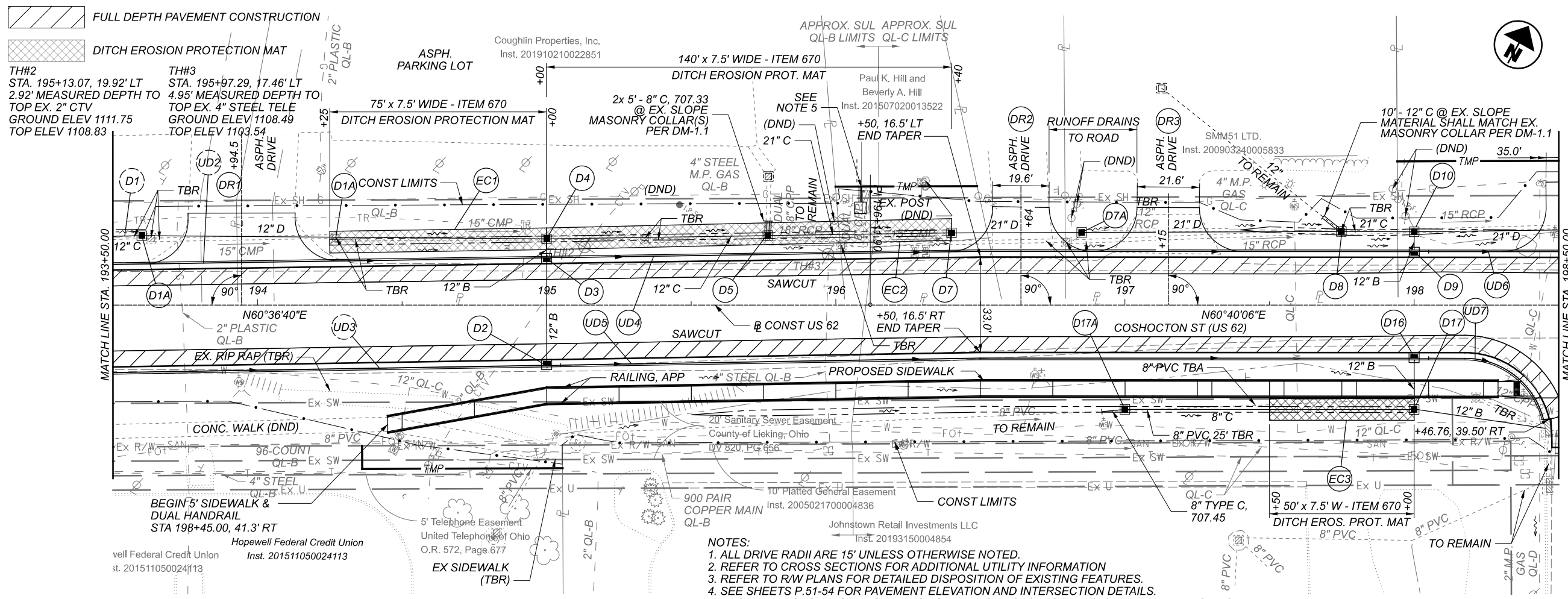
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110861

SHEET TOTAL  
P.32 89

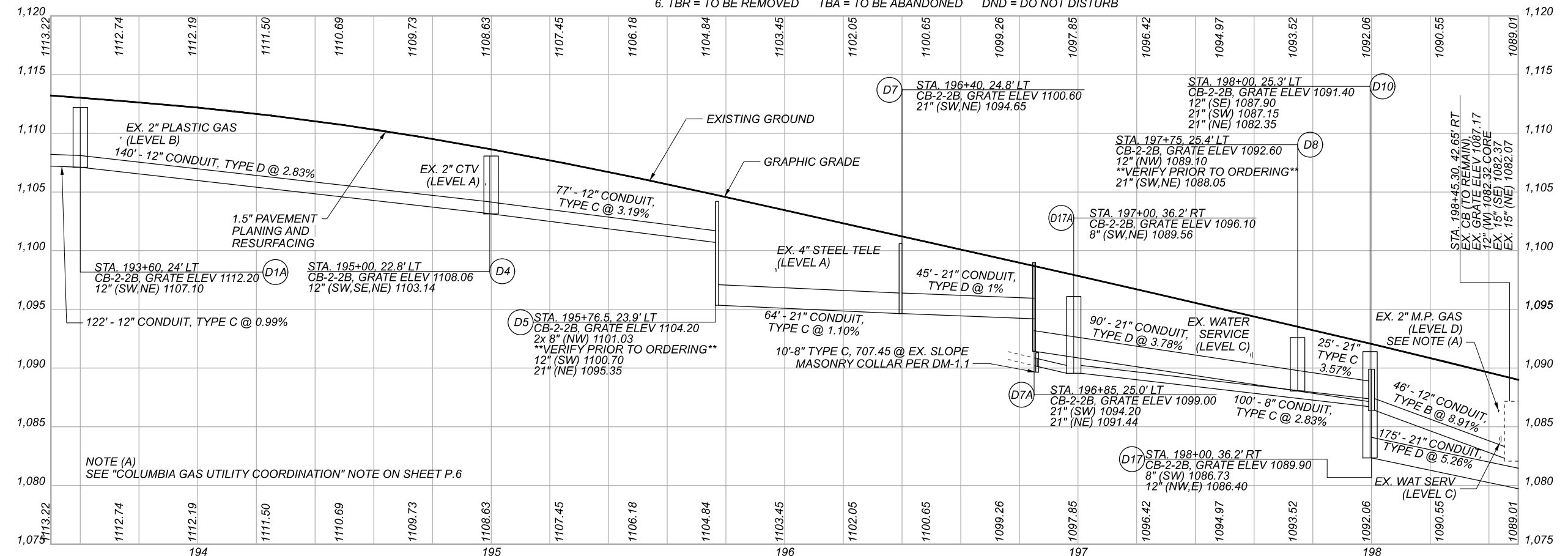
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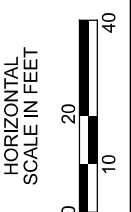




- NOTES:**
1. ALL DRIVE RADII ARE 15' UNLESS OTHERWISE NOTED.
  2. REFER TO CROSS SECTIONS FOR ADDITIONAL UTILITY INFORMATION
  3. REFER TO RW PLANS FOR DETAILED DISPOSITION OF EXISTING FEATURES.
  4. SEE SHEETS P.51-54 FOR PAVEMENT ELEVATION AND INTERSECTION DETAILS.
  5. PROVIDE UNOBSTRUCTED OUTLETS FOR EX. DOWNSPOUTS (3 EACH) VIA 5' MIN. LENGTH OF TYPE E CONDUIT (SIZE AND MATERIAL SHALL MATCH EX.) AND 4'x4' ITEM 601 - TIED CONCRETE BLOCK MAT WITH TYPE 1 UNDERLAYMENT. REMOVE EX. CB AND EXCESS CONDUIT.
  6. TBR = TO BE REMOVED TBA = TO BE ABANDONED DND = DO NOT DISTURB



NOTE (A)  
 SEE "COLUMBIA GAS UTILITY COORDINATION" NOTE ON SHEET P.6



PLAN AND PROFILE - US 62  
 STA. 193+50.00 TO STA. 198+50.00

DESIGN AGENCY



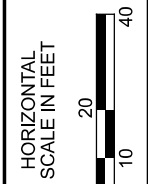
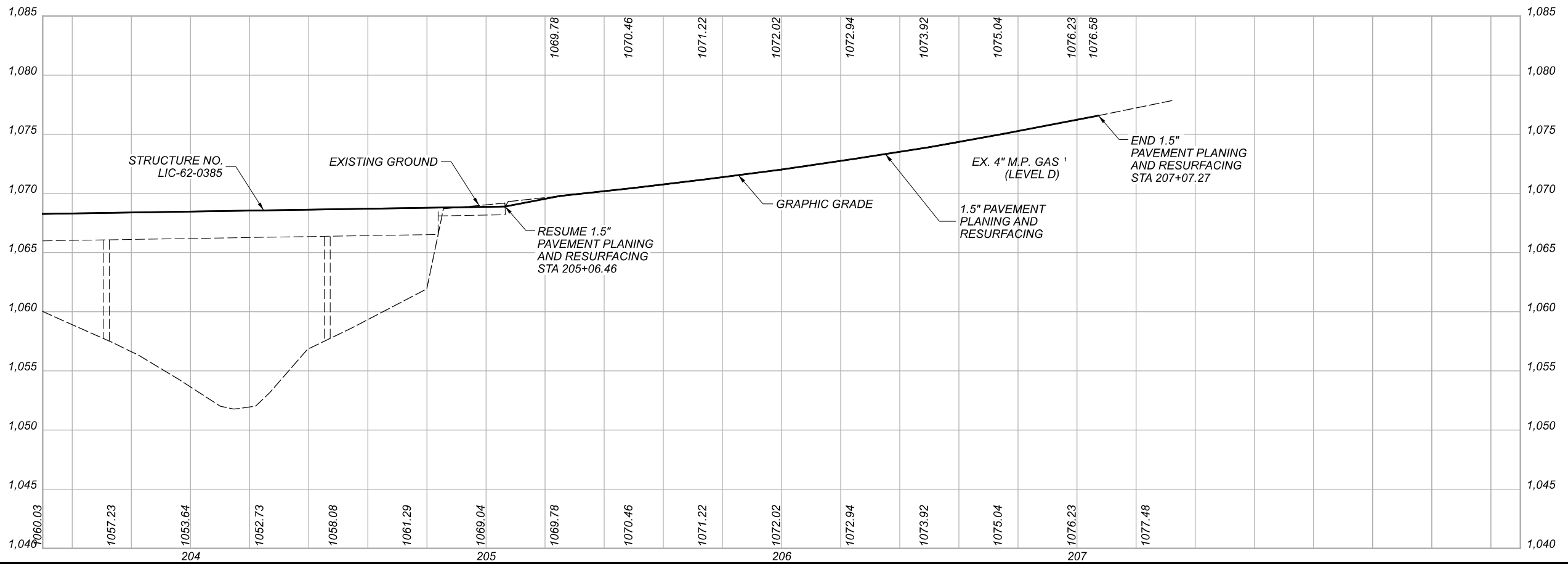
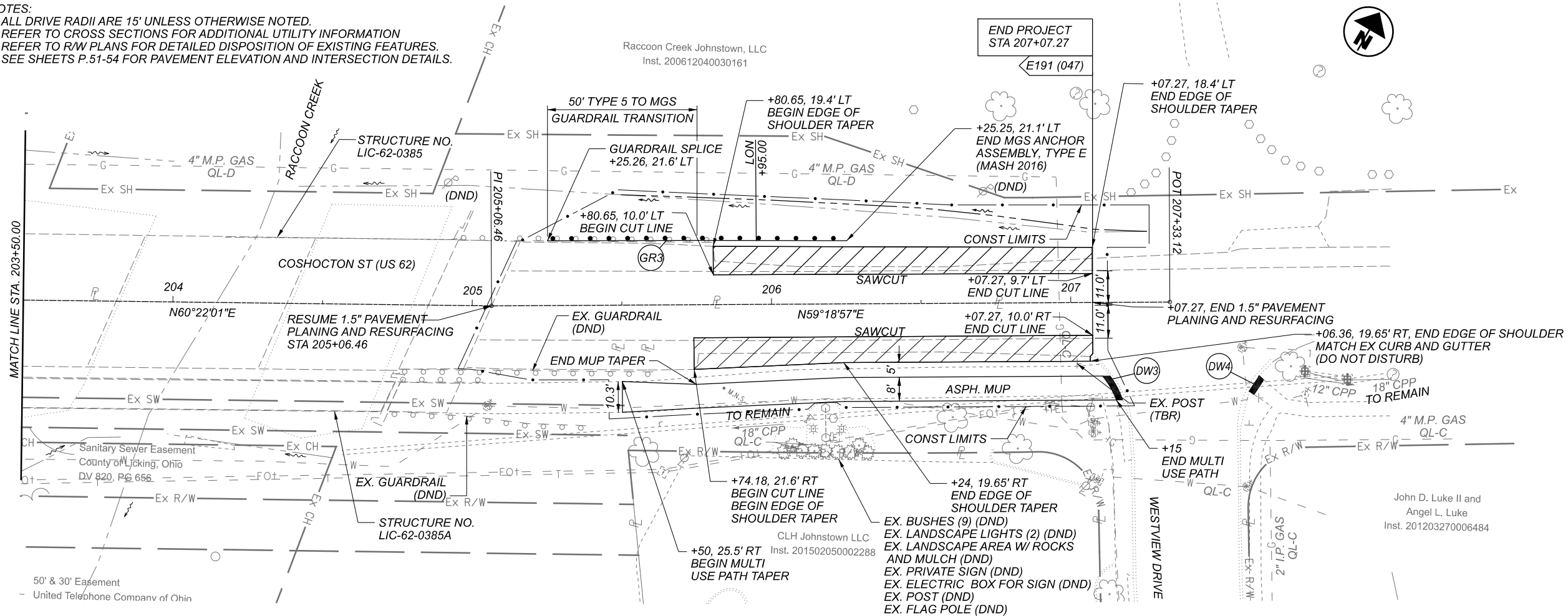
DESIGNER	AJP
REVIEWER	JDH 10-29-21
PROJECT ID	110861
SHEET	TOTAL
P.33	89



 FULL DEPTH PAVEMENT CONSTRUCTION

NOTES:

1. ALL DRIVE RADII ARE 15' UNLESS OTHERWISE NOTED.
2. REFER TO CROSS SECTIONS FOR ADDITIONAL UTILITY INFORMATION
3. REFER TO R/W PLANS FOR DETAILED DISPOSITION OF EXISTING FEATURES.
4. SEE SHEETS P.51-54 FOR PAVEMENT ELEVATION AND INTERSECTION DETAILS.



PLAN AND PROFILE - US 62  
STA. 203+50.00 TO STA. 207+33.12

DESIGN AGENCY



DESIGNER  
AJP

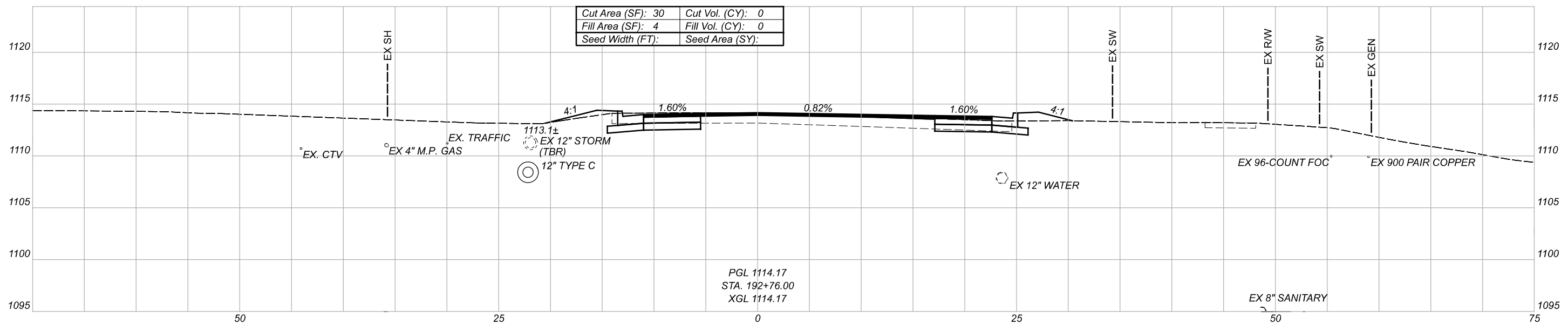
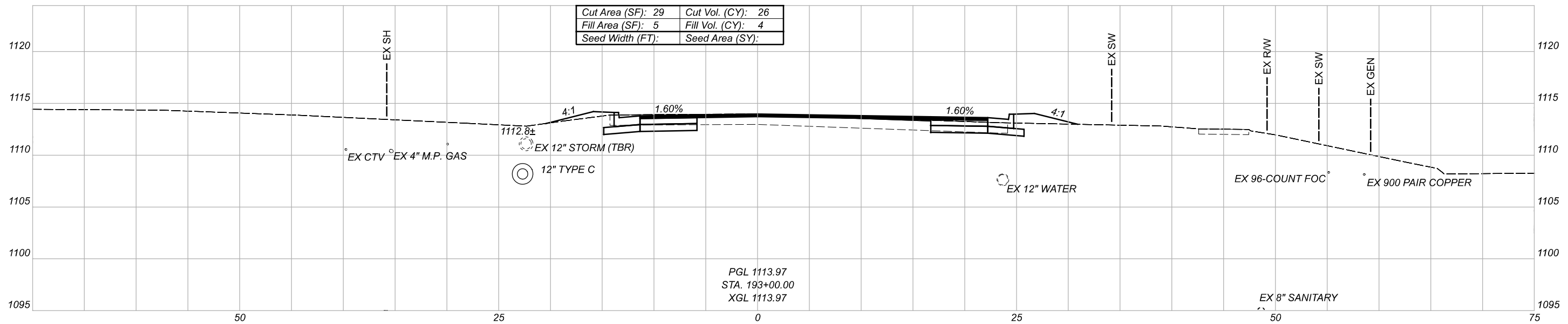
REVIEWER  
JDH 10-29-21

PROJECT ID  
110861

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P.35 89

CTY-RTE-SECTION

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CROSS SECTIONS  
 STA. 192+76.00 TO STA. 193+00.00

DESIGN AGENCY



DESIGNER

BSB

REVIEWER

JDH 10-29-21

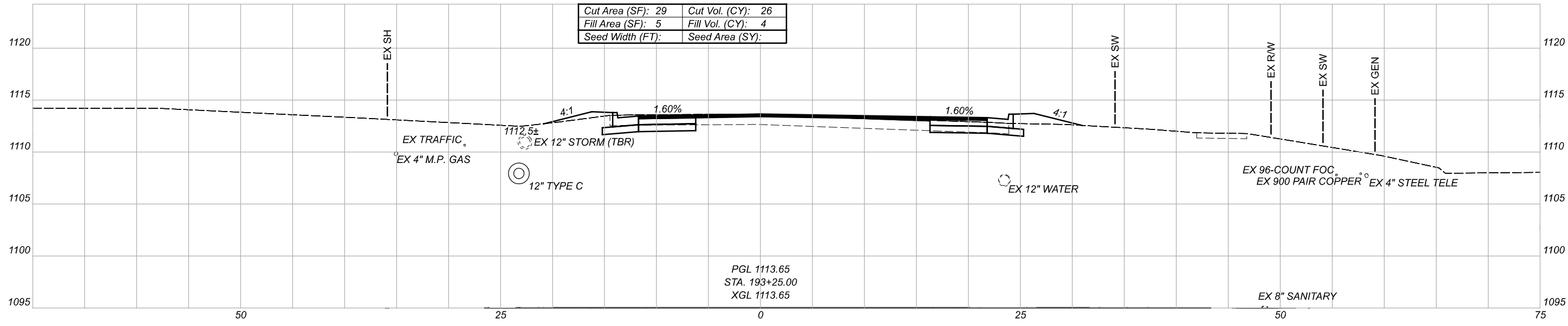
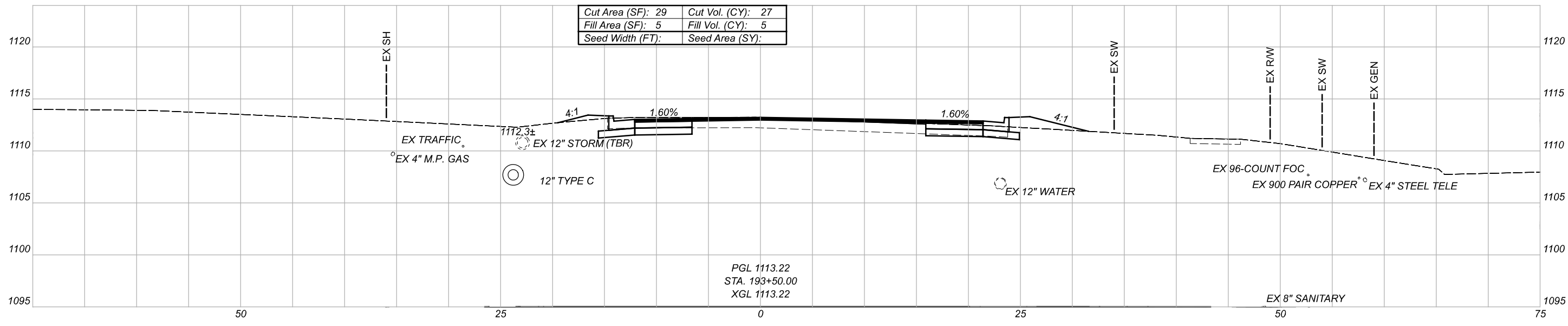
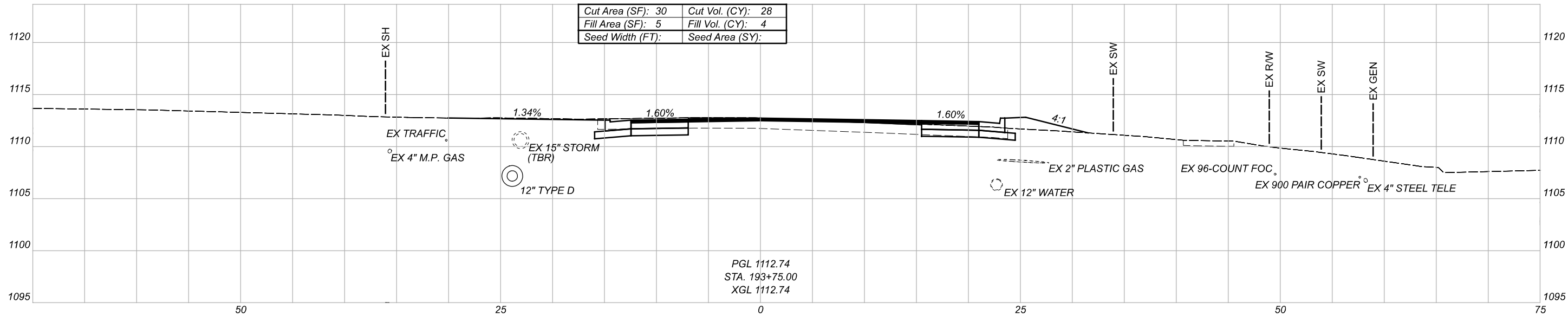
PROJECT ID

110861


Sheet Totals		
Seeding	Cut	Fill
.	26	4

FOR SEEDING QUANTITIES, SEE SHEET P.6

SHEET	TOTAL
P.36	89

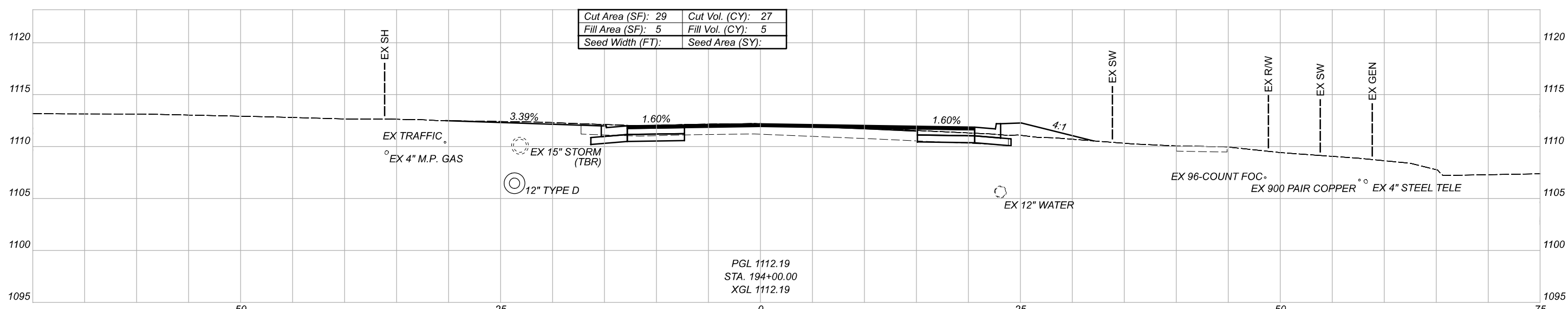
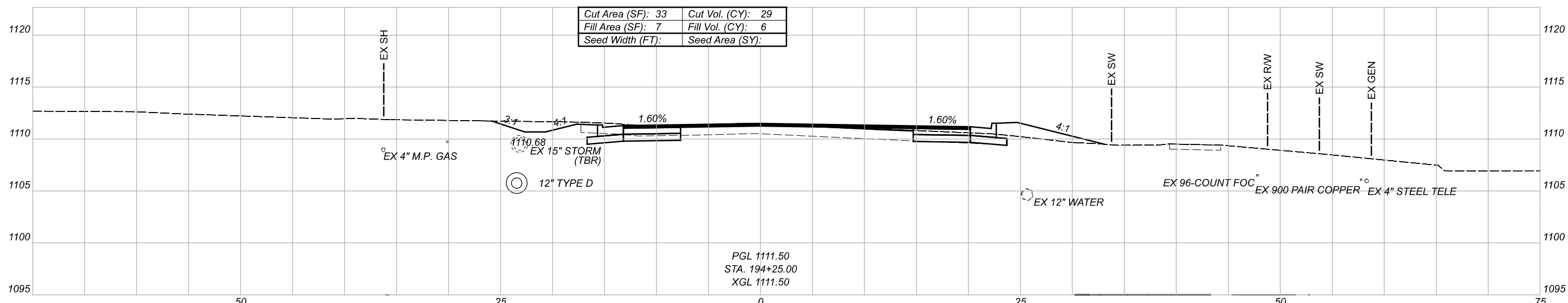
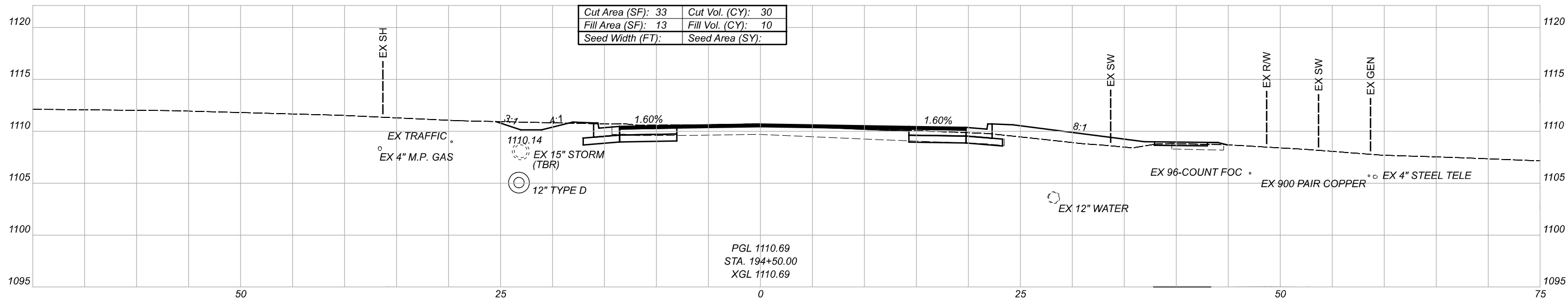


CROSS SECTIONS  
 STA. 193+25.00 TO STA. 193+75.00

DESIGN AGENCY  
  
 DESIGNER  
 BSB  
 REVIEWER  
 JDH 10-29-21  
 PROJECT ID  
 110861

Sheet Totals			110861	
Seeding	Cut	Fill	SHEET	TOTAL
.	81	13	P.37	89

FOR SEEDING QUANTITIES, SEE SHEET P.6

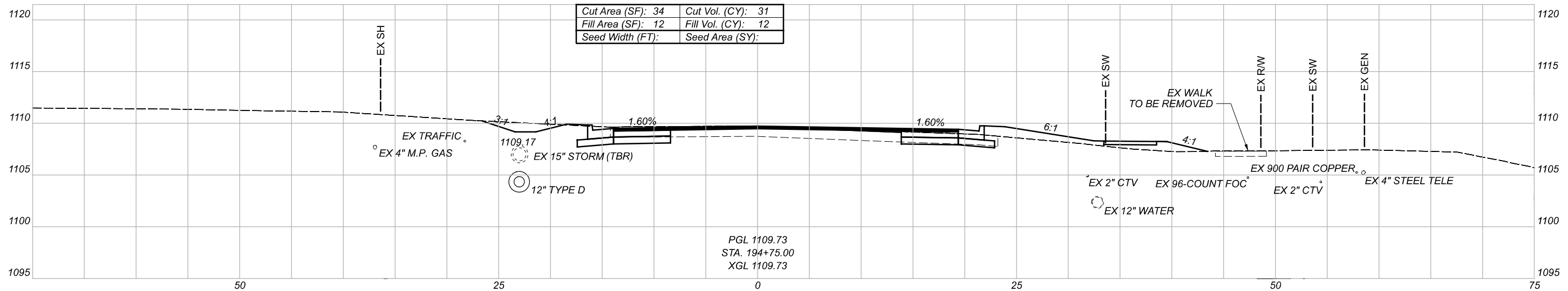
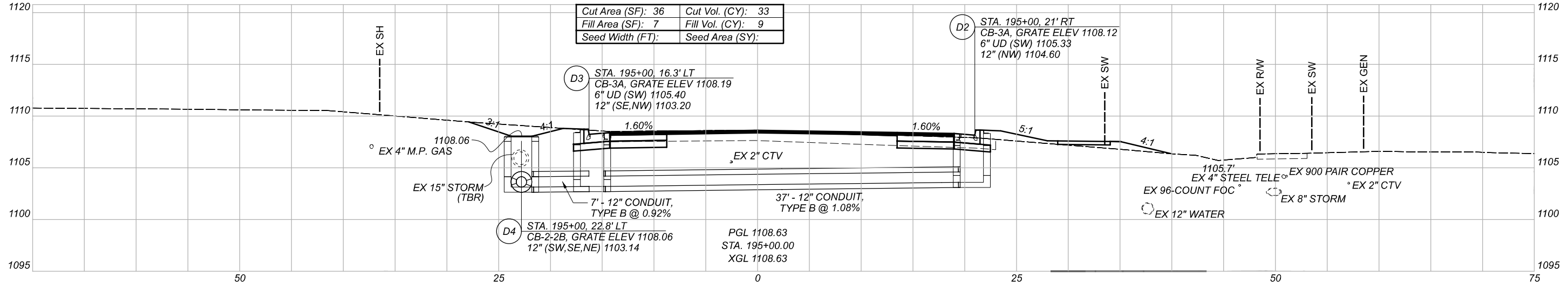
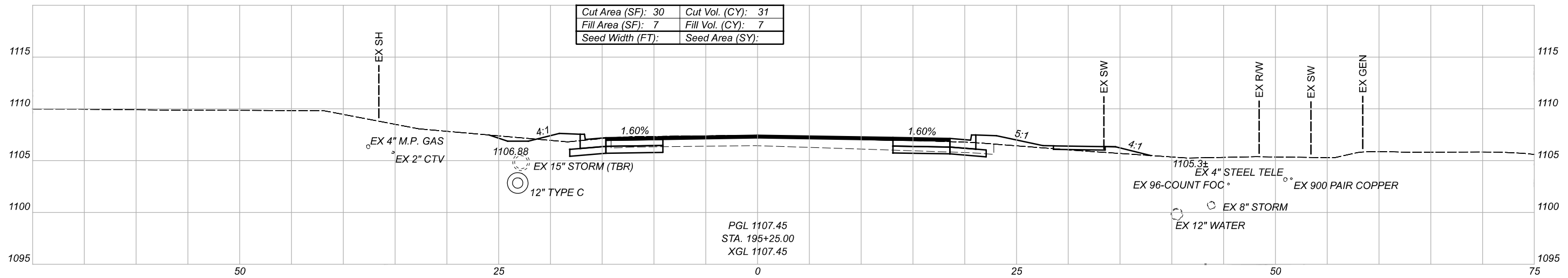


CROSS SECTIONS  
 STA. 194+00.00 TO STA. 194+50.00

DESIGN AGENCY	[ IBI ]	
DESIGNER	BSB	
REVIEWER	JDH 10-29-21	
PROJECT ID	110861	
SHEET	P.38	TOTAL 89

Sheet Totals		
Seeding	Cut	Fill
.	86	21

FOR SEEDING QUANTITIES, SEE SHEET P.6

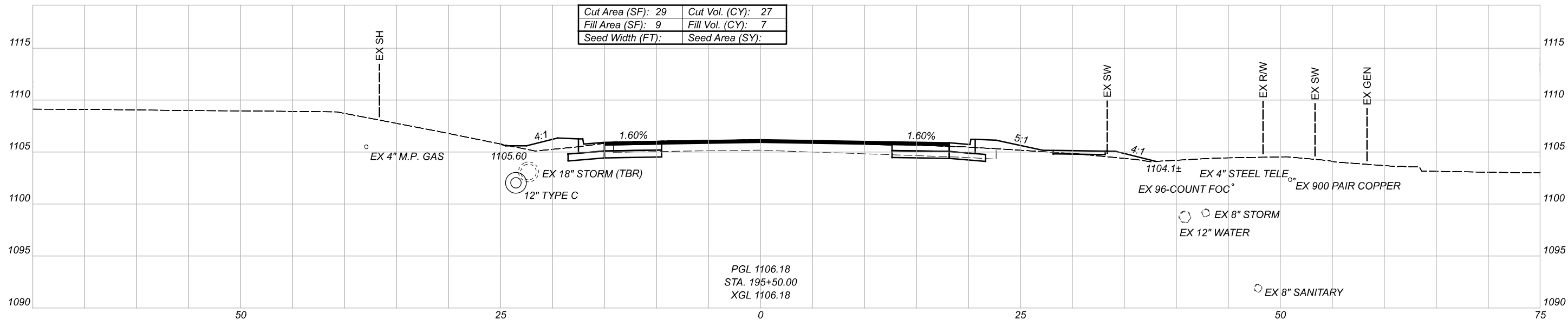
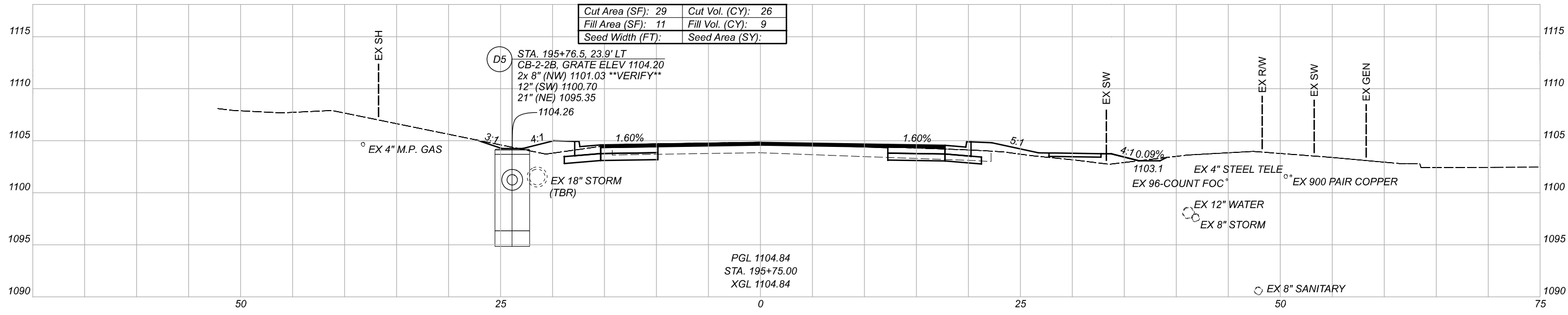
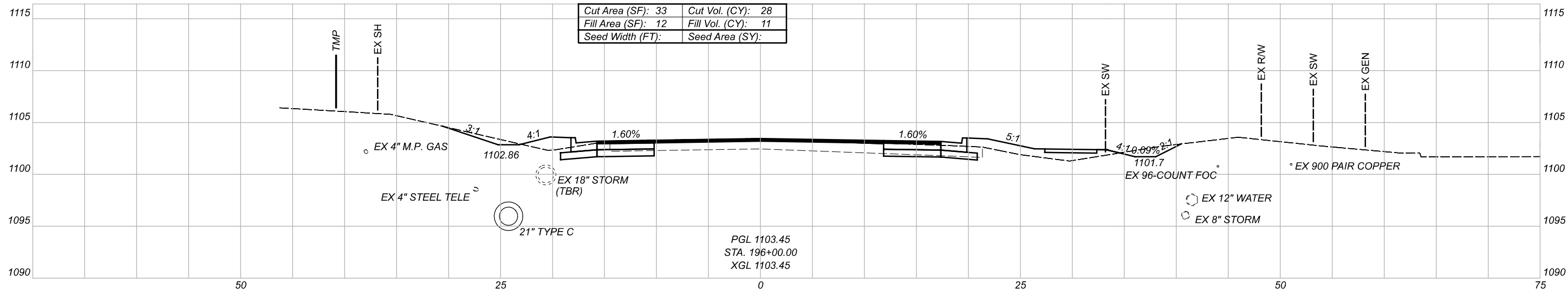


CROSS SECTIONS  
 STA. 194+75.00 TO STA. 195+25.00


DESIGN AGENCY	[BI]
DESIGNER	BSB
REVIEWER	JDH 10-29-21
PROJECT ID	110861

Sheet Totals			110861
Seeding	Cut	Fill	SHEET TOTAL
.	95	28	P.39 89

FOR SEEDING QUANTITIES, SEE SHEET P.6



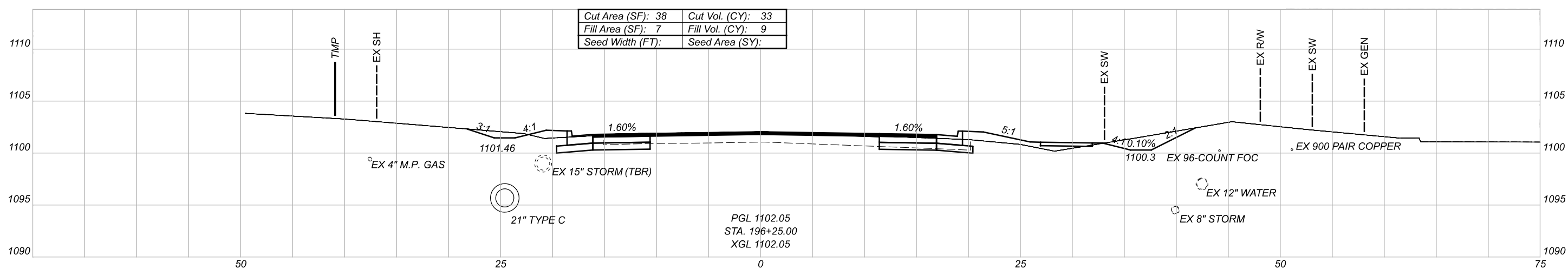
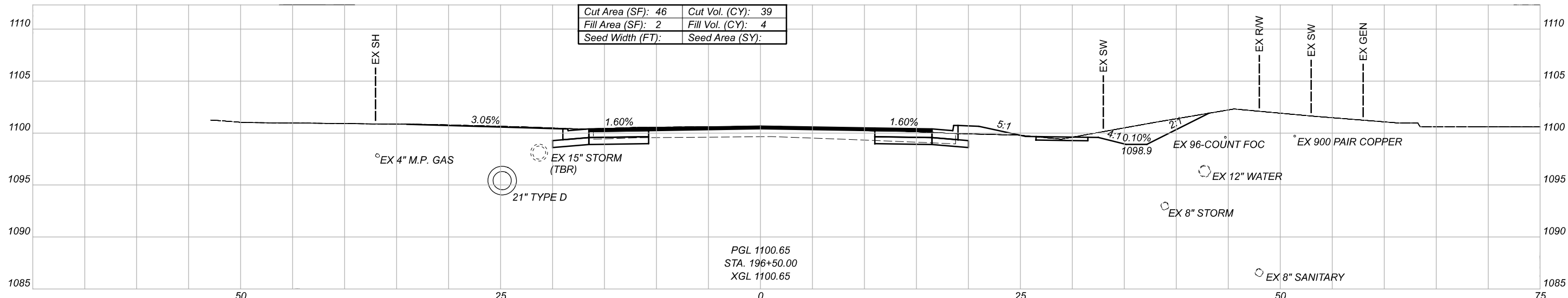
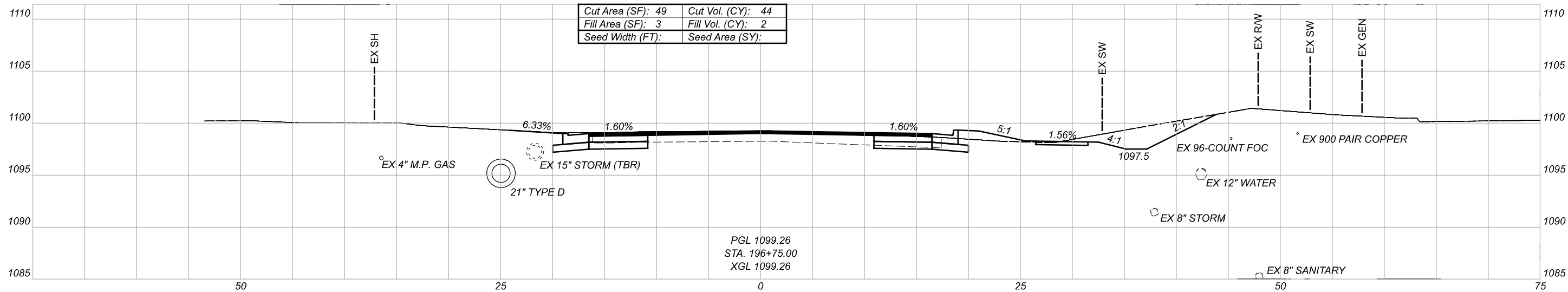
CROSS SECTIONS  
 STA. 195+50.00 TO STA. 196+00.00

DESIGN AGENCY  
  
 DESIGNER  
 BSB  
 REVIEWER  
 JDH 10-29-21  
 PROJECT ID  
 110861

Sheet Totals			SHEET TOTAL	
Seeding	Cut	Fill	P.40	89
.	81	27		

FOR SEEDING QUANTITIES, SEE SHEET P.6





CROSS SECTIONS  
 STA. 196+25.00 TO STA. 196+75.00

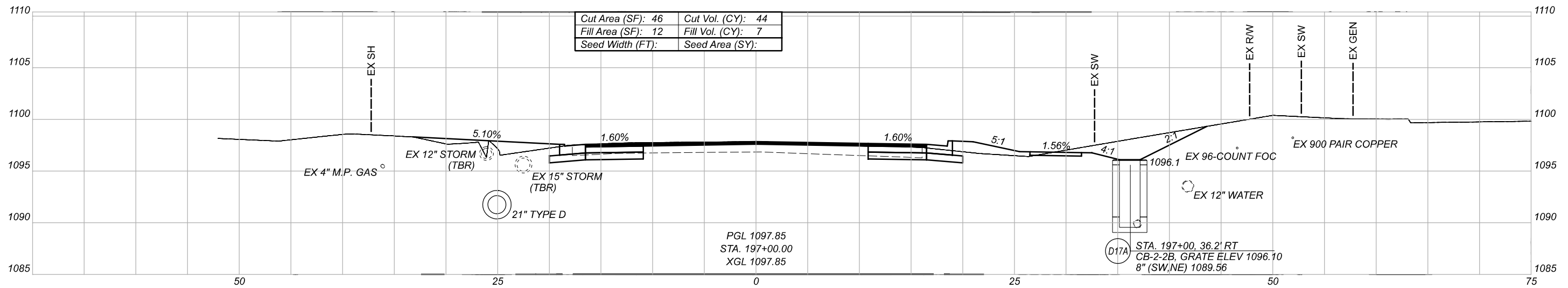
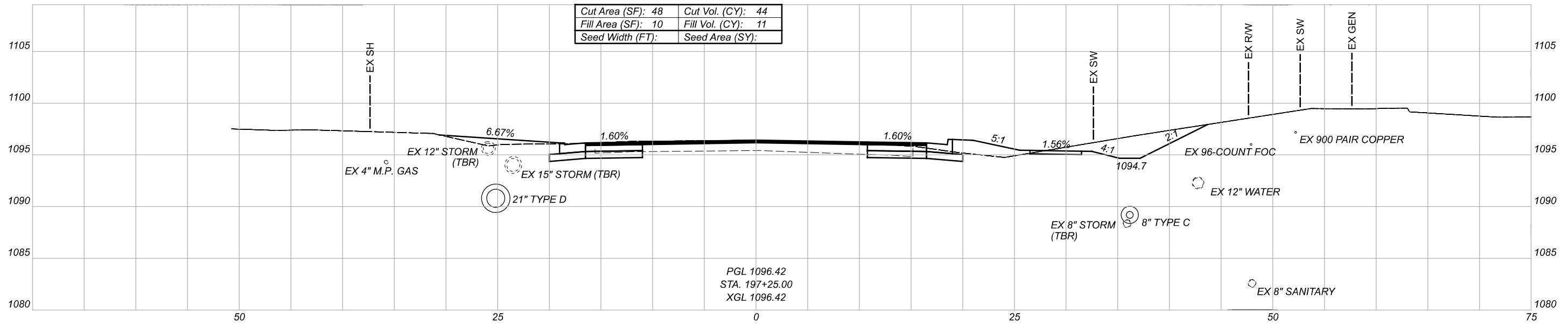
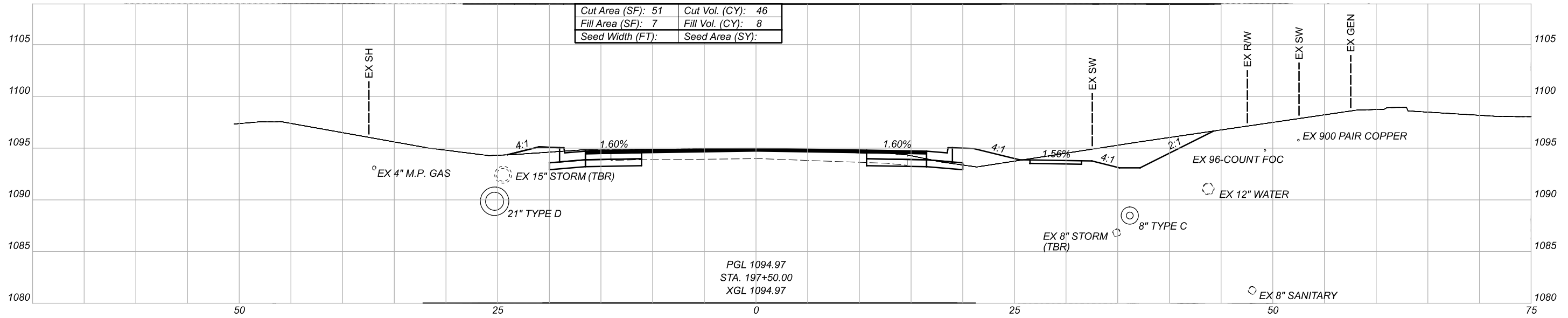
DESIGN AGENCY	<b>[BI]</b>
DESIGNER	
REVIEWER	
PROJECT ID	

DESIGNER	BSB
REVIEWER	JDH 10-29-21
PROJECT ID	110861

Sheet Totals		
Seeding	Cut	Fill
.	116	15

FOR SEEDING QUANTITIES, SEE SHEET P.6

SHEET	TOTAL
P.41	89



CROSS SECTIONS  
STA. 197+00.00 TO STA. 197+50.00

DESIGN AGENCY



DESIGNER

BSB

REVIEWER

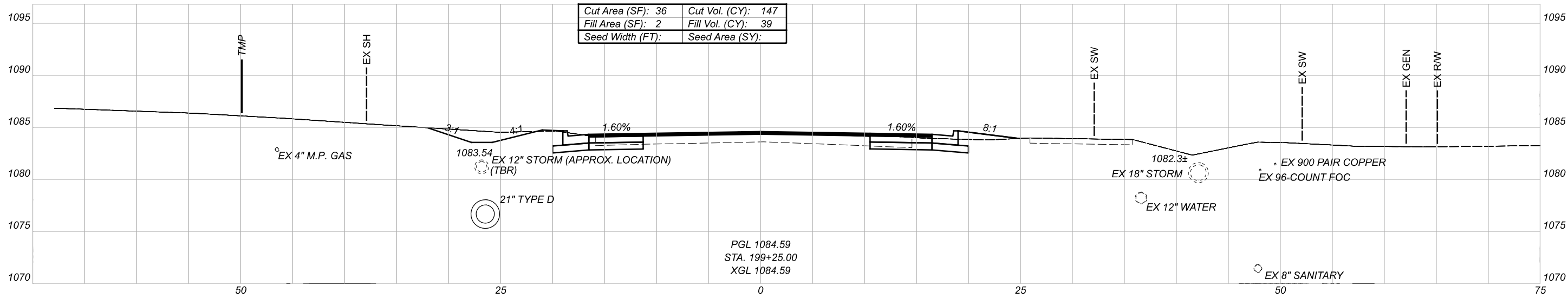
JDH 10-29-21

PROJECT ID

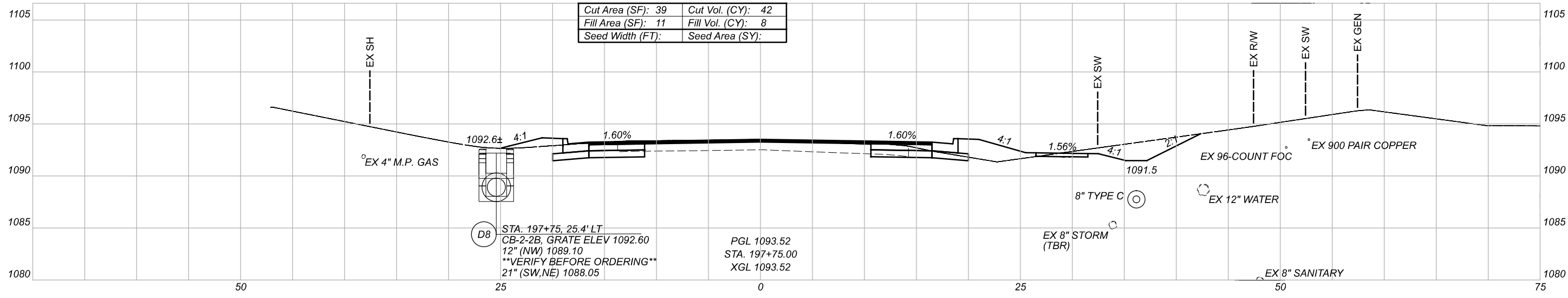
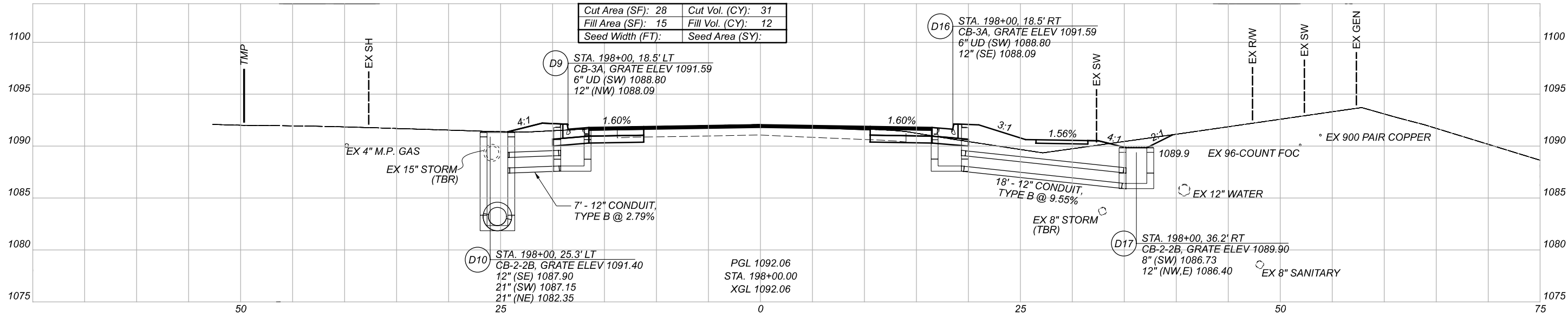
110861

Sheet Totals			SHEET TOTAL	
Seeding	Cut	Fill	P.42	89
.	134	26		


FOR SEEDING QUANTITIES, SEE SHEET P.6



NOTE: REFER TO SHEET P.57 FOR ADDITIONAL CROSS SECTION INFORMATION BETWEEN STA. 198+00 AND STA. 199+25

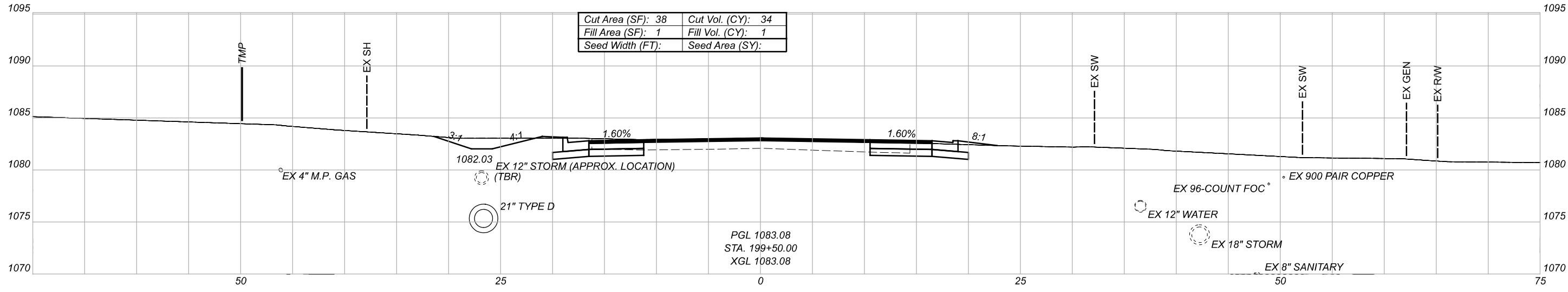
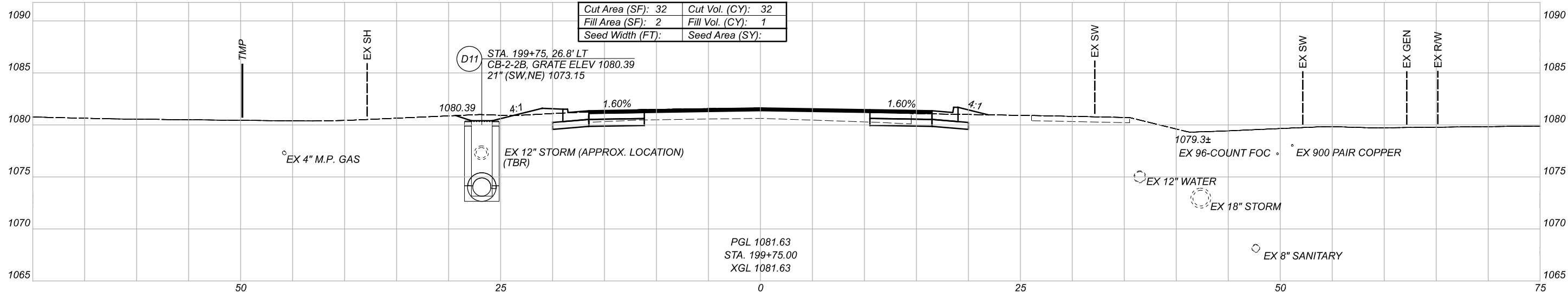
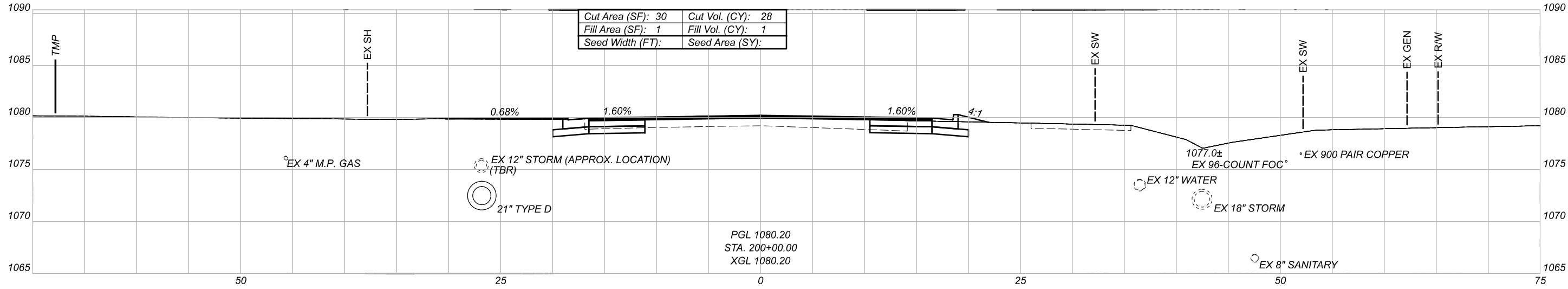


CROSS SECTIONS  
STA. 197+75.00 TO STA. 199+25.00


DESIGN AGENCY  
  
 DESIGNER  
BSB  
 REVIEWER  
JDH 10-29-21  
 PROJECT ID  
110861

Sheet Totals			110861	
Seeding	Cut	Fill	SHEET	TOTAL
.	220	59	P.43	89

FOR SEEDING QUANTITIES, SEE SHEET P.6

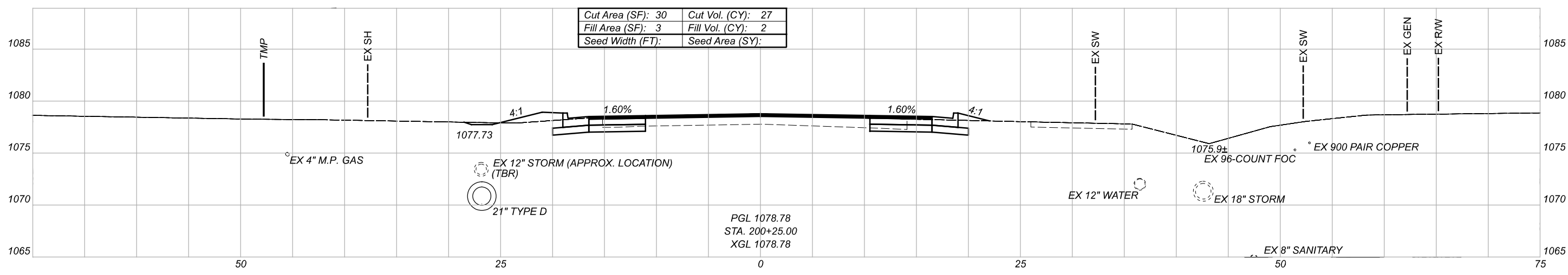
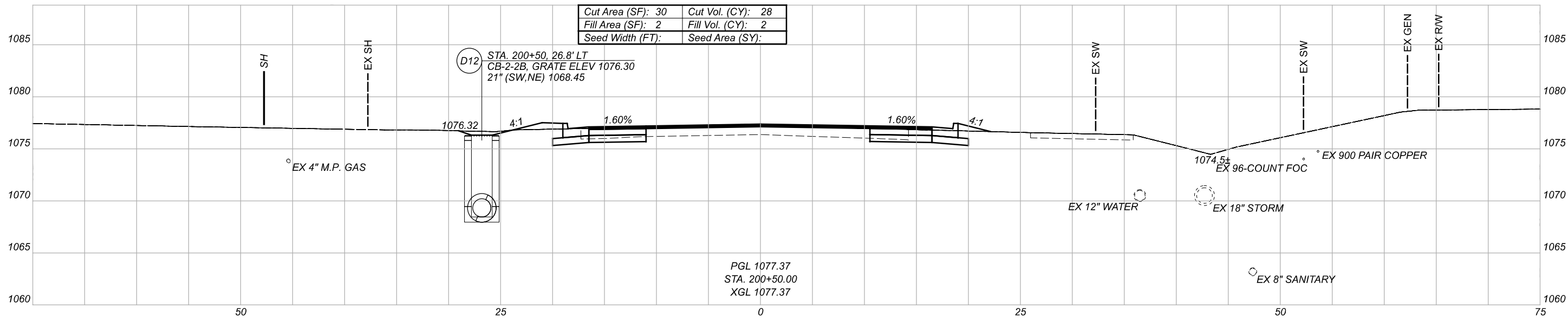
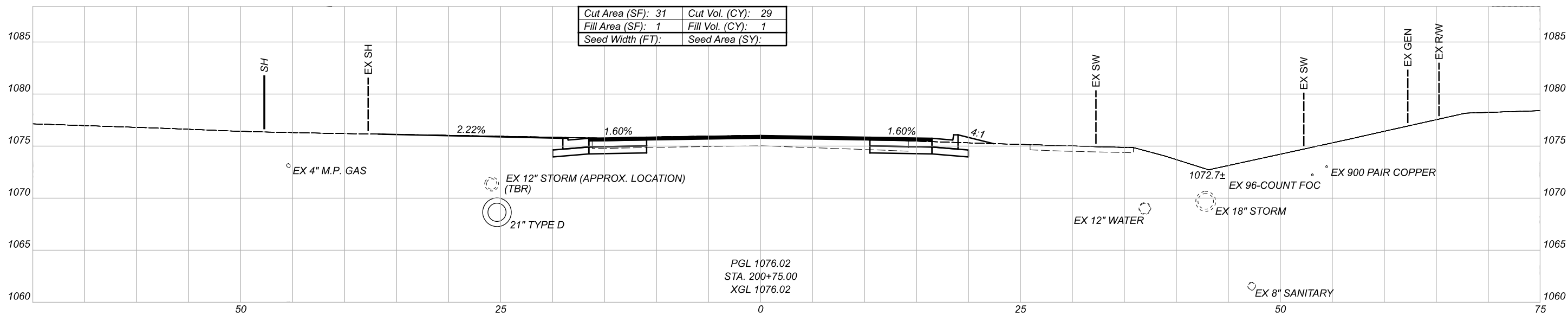


CROSS SECTIONS  
 STA. 199+50.00 TO STA. 200+00.00

DESIGN AGENCY  
  
 DESIGNER  
 BSB  
 REVIEWER  
 JDH 10-29-21  
 PROJECT ID  
 110861

Sheet Totals			SHEET TOTAL	
Seeding	Cut	Fill	P.44	89
.	94	3		

FOR SEEDING QUANTITIES, SEE SHEET P.6

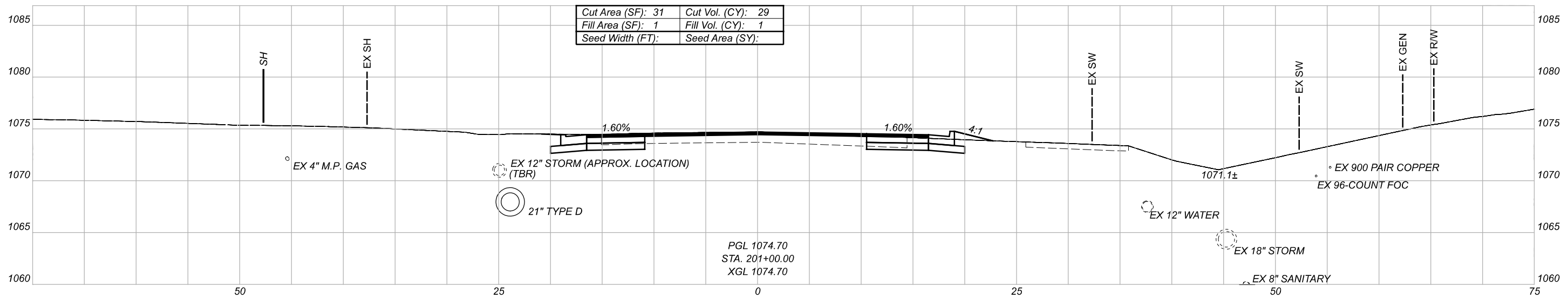
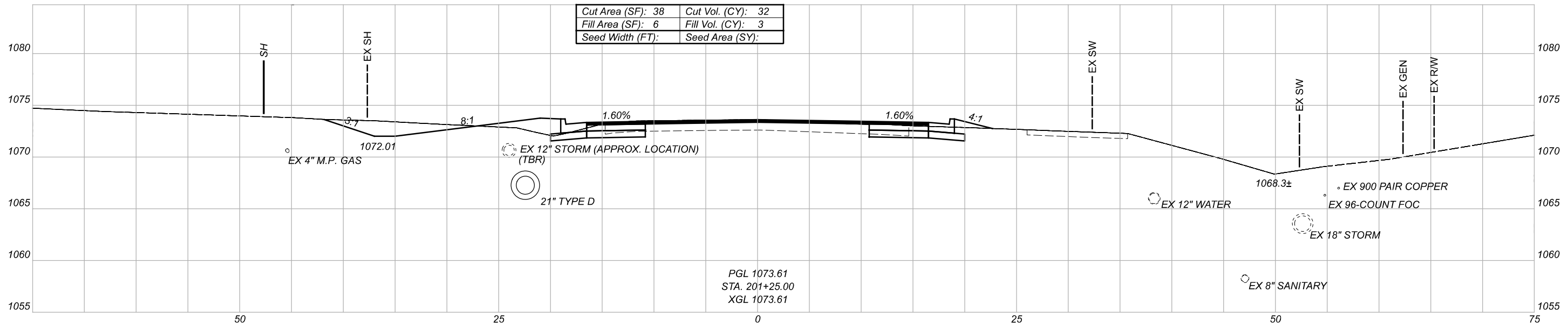


CROSS SECTIONS  
 STA. 200+25.00 TO STA. 200+75.00

DESIGN AGENCY	[BI]
DESIGNER	BSB
REVIEWER	JDH 10-29-21
PROJECT ID	110861

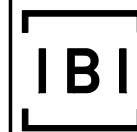
Sheet Totals			110861
Seeding	Cut	Fill	SHEET TOTAL
.	84	5	P.45 89

FOR SEEDING QUANTITIES, SEE SHEET P.6



CROSS SECTIONS  
 STA. 201+00.00 TO STA. 201+25.00

DESIGN AGENCY



DESIGNER

BSB

REVIEWER

JDH 10-29-21

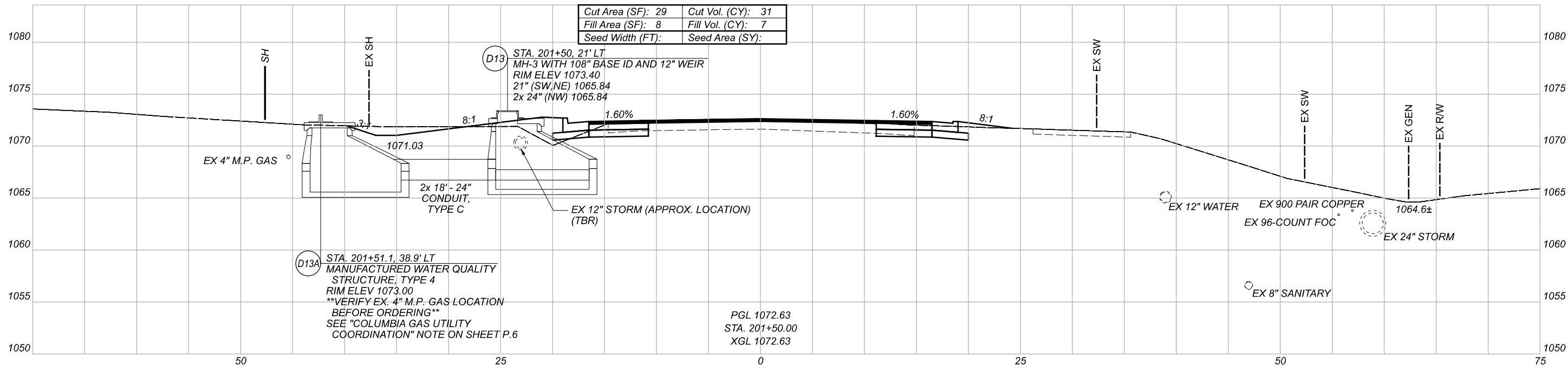
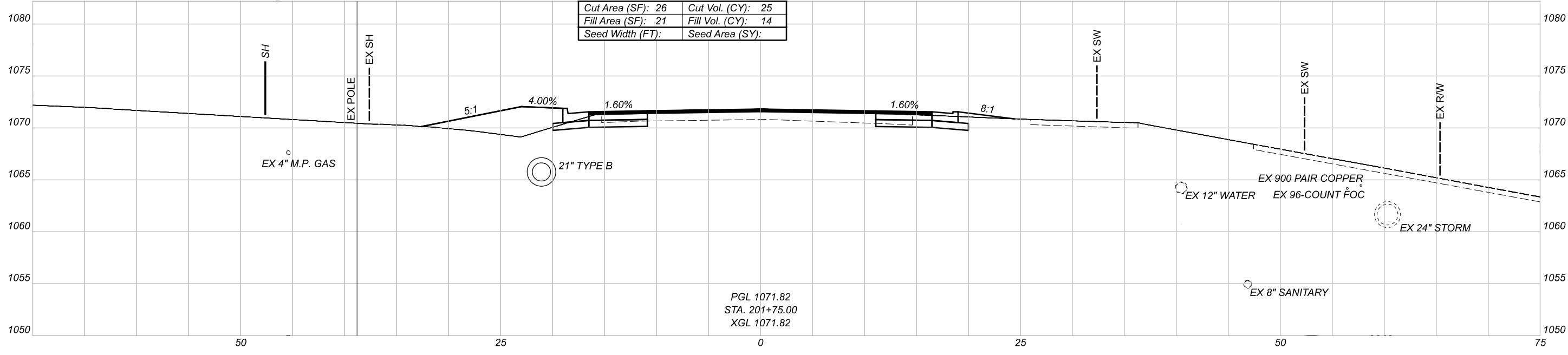
PROJECT ID

110861

Sheet Totals		
Seeding	Cut	Fill
.	61	4

FOR SEEDING QUANTITIES, SEE SHEET P.6

SHEET	TOTAL
P.46	89



CROSS SECTIONS  
STA. 201+50.00 TO STA. 201+75.00

DESIGN AGENCY



DESIGNER

BSB

REVIEWER

JDH 10-29-21

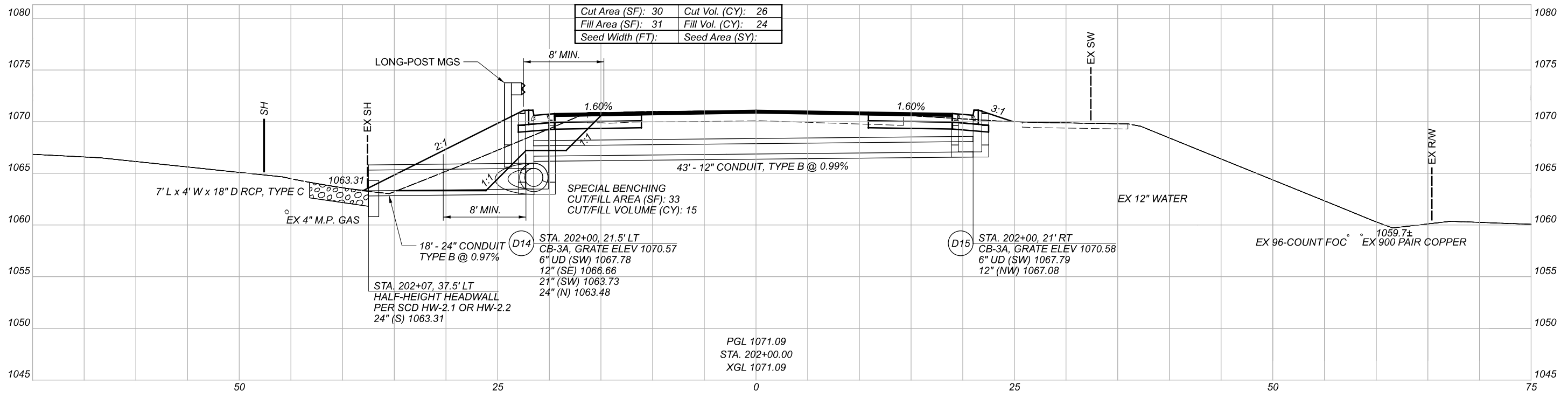
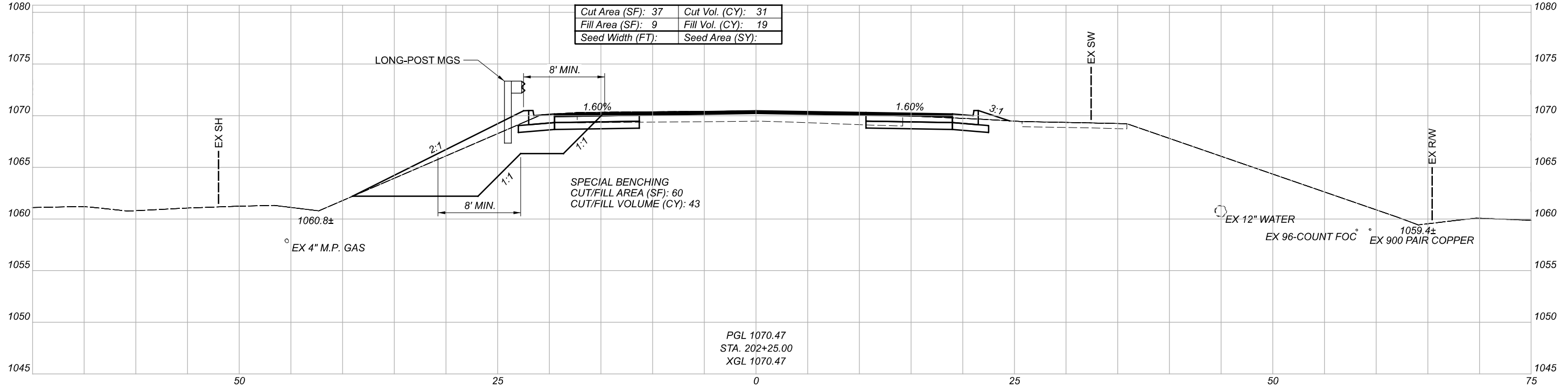
PROJECT ID

110861

Sheet Totals			SHEET TOTAL	
Seeding	Cut	Fill	P.47	TOTAL
.	56	21	P.47	89

FOR SEEDING QUANTITIES, SEE SHEET P.6

STA. 202+27.19  
 END WIDENING WITH CURB AND GUTTER



CROSS SECTIONS  
 STA. 202+00.00 TO STA. 202+25.00

DESIGN AGENCY



DESIGNER

BSB

REVIEWER

JDH 10-29-21

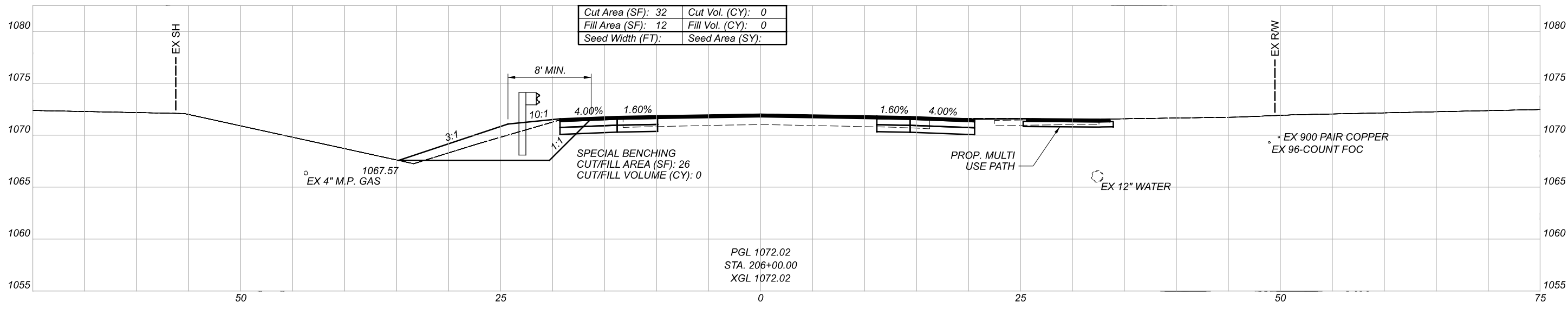
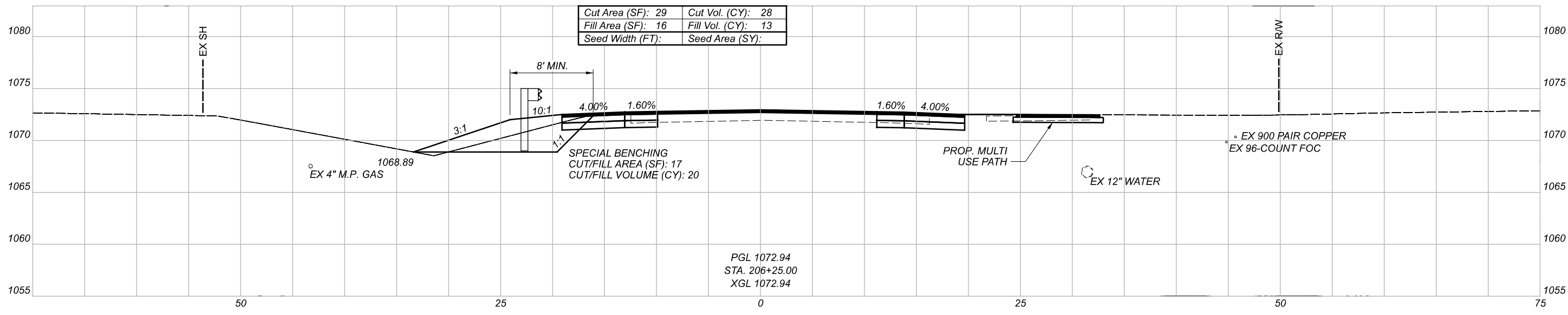
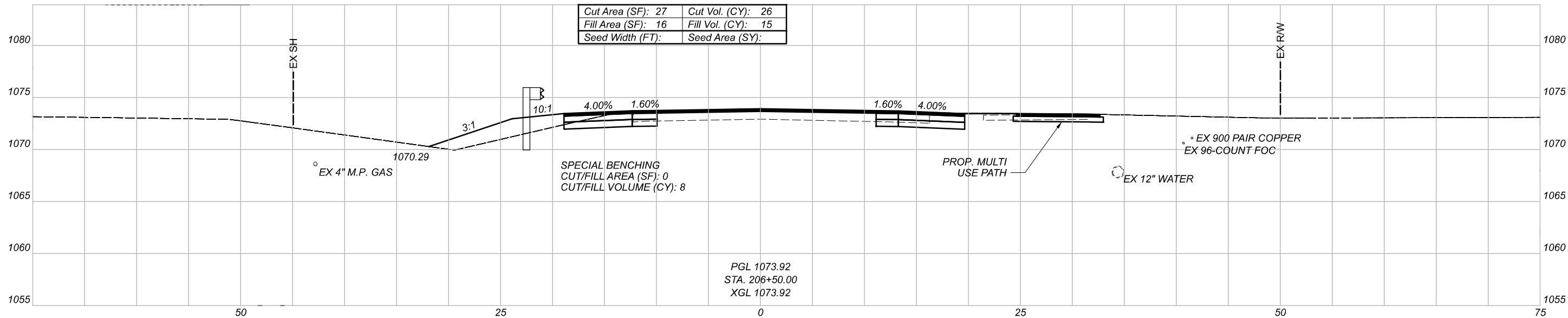
PROJECT ID

110861

Benching			Sheet Totals		
Cut	Fill	Seeding	Cut	Fill	
58	58	.	57	43	


FOR SEEDING QUANTITIES, SEE SHEET P.6





CROSS SECTIONS  
STA. 206+00.00 TO STA. 206+50.00

DESIGN AGENCY



DESIGNER  
BSB

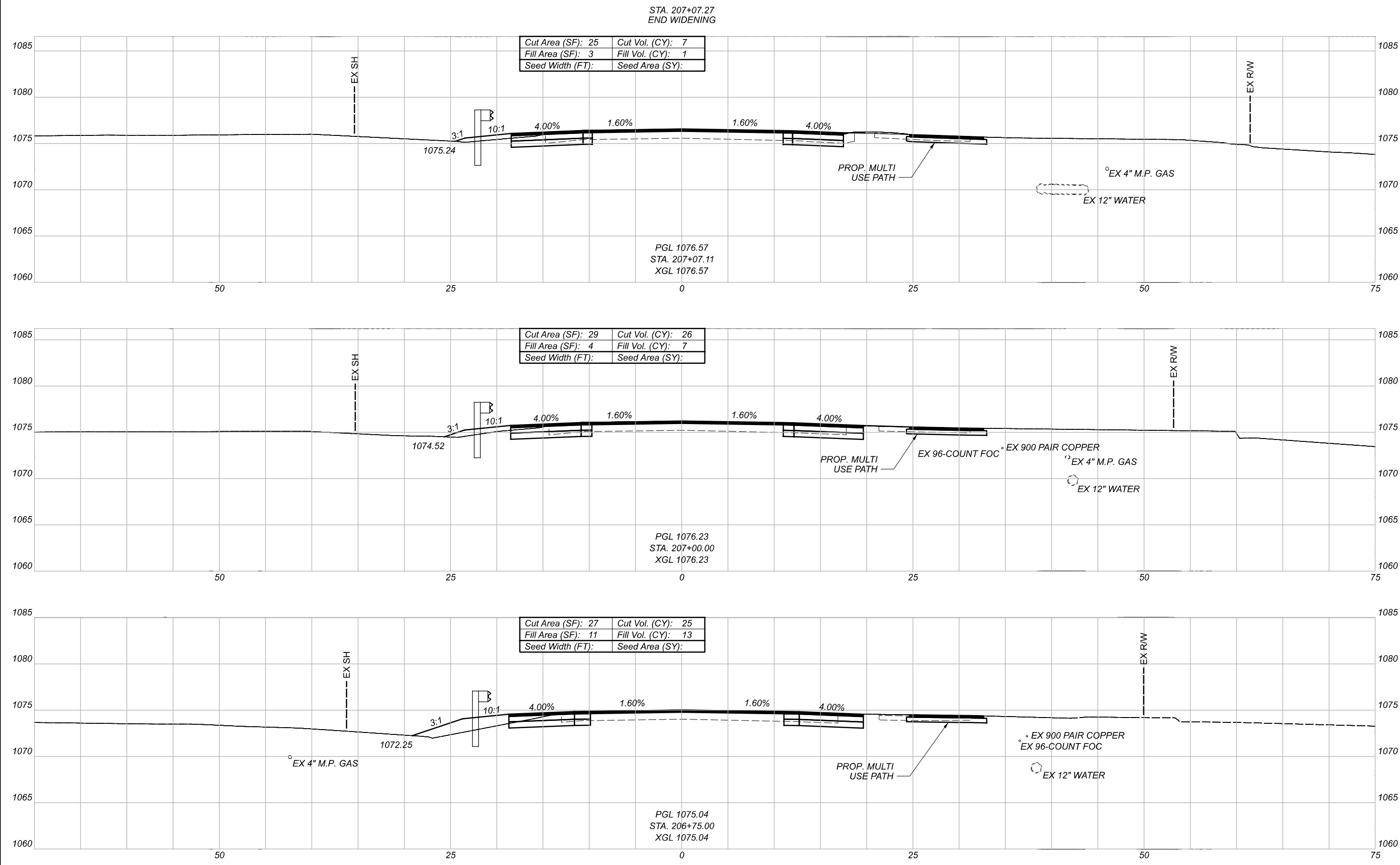
REVIEWER  
JDH 10-29-21

PROJECT ID  
110861


SHEET	TOTAL
P.49	89

Benching		Sheet Totals	
Cut	Fill	Seeding	Seed Area
28	28	·	54

FOR SEEDING QUANTITIES, SEE SHEET P.6

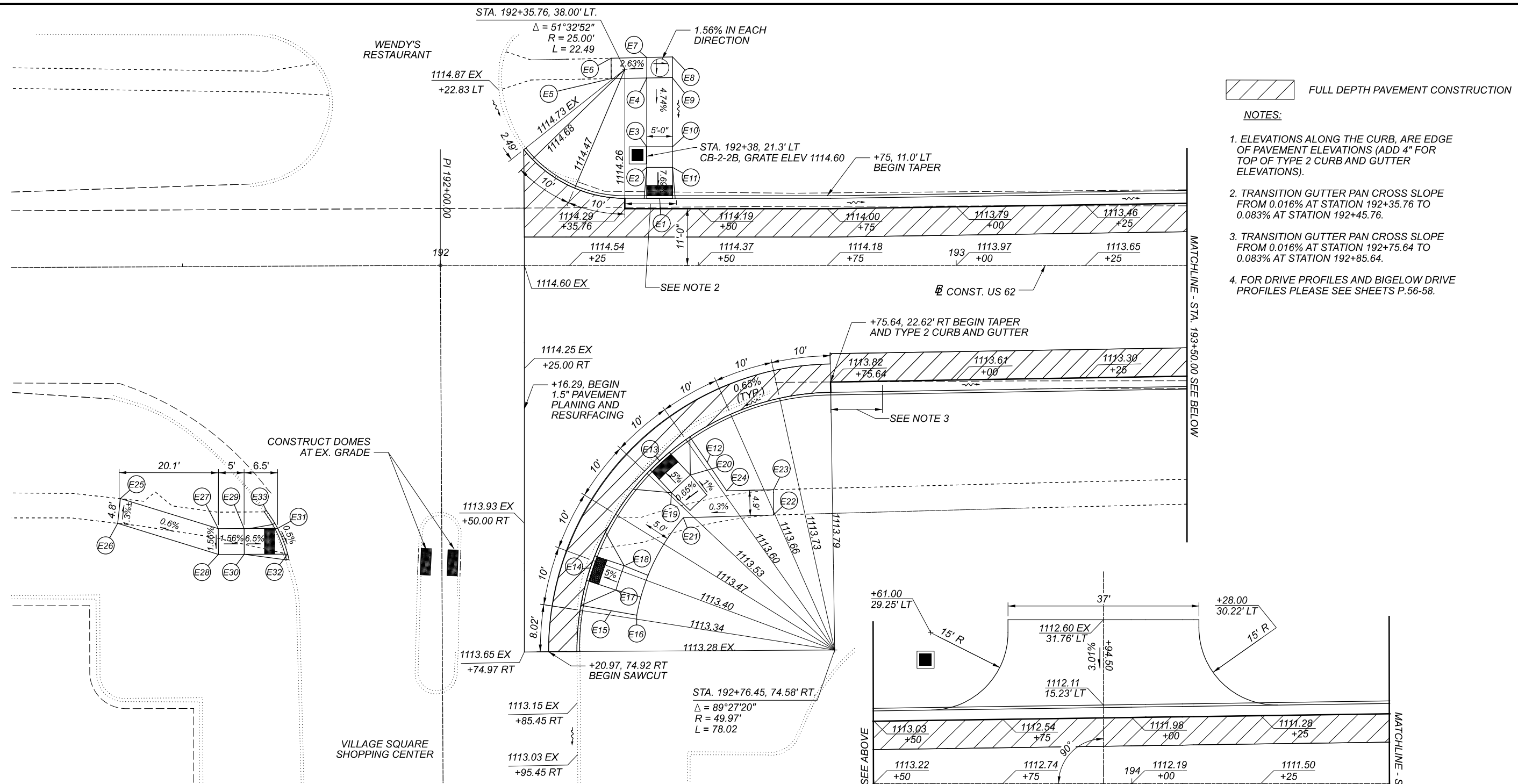


CROSS SECTIONS  
STA. 206+75.00 TO STA. 207+07.11

DESIGN AGENCY  
  
 DESIGNER  
 BSB  
 REVIEWER  
 JDH 10-29-21  
 PROJECT ID  
 110861

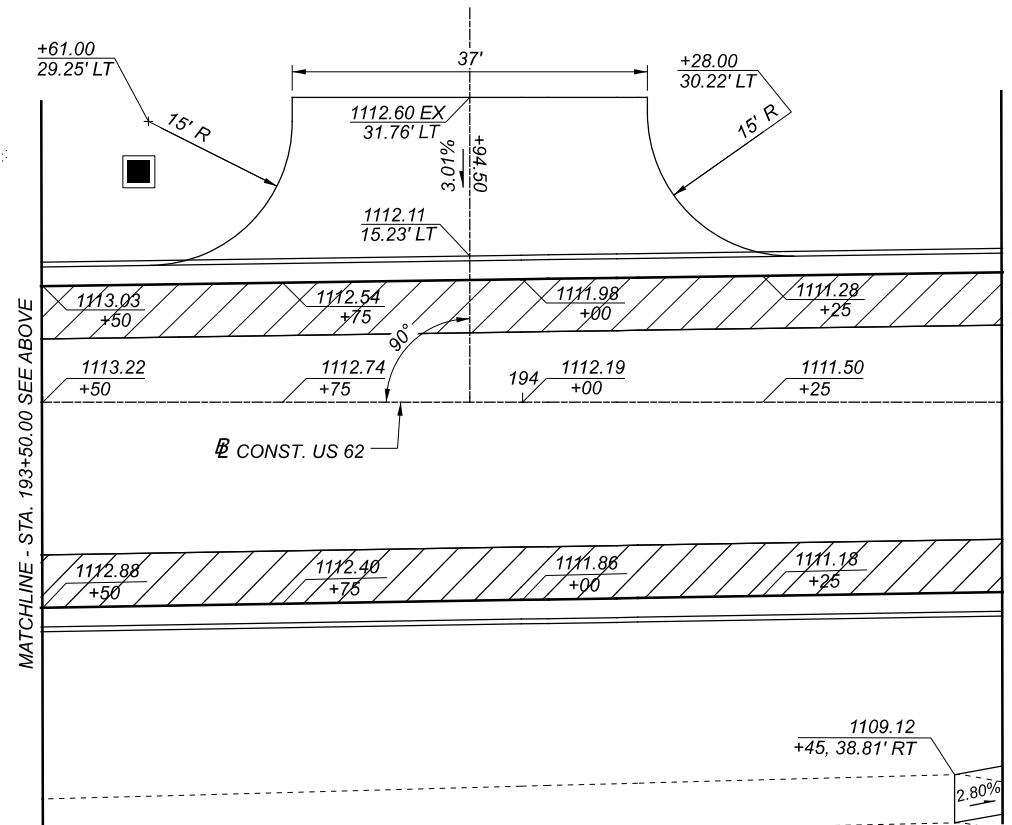
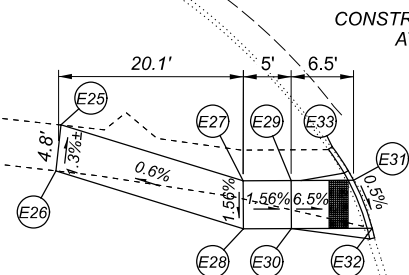
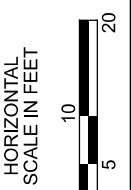
Sheet Totals			SHEET TOTAL	
Seeding	Cut	Fill	P.50	89
.	58	21		

FOR SEEDING QUANTITIES, SEE SHEET P.6



FULL DEPTH PAVEMENT CONSTRUCTION

- NOTES:
- ELEVATIONS ALONG THE CURB, ARE EDGE OF PAVEMENT ELEVATIONS (ADD 4" FOR TOP OF TYPE 2 CURB AND GUTTER ELEVATIONS).
  - TRANSITION GUTTER PAN CROSS SLOPE FROM 0.016% AT STATION 192+35.76 TO 0.083% AT STATION 192+45.76.
  - TRANSITION GUTTER PAN CROSS SLOPE FROM 0.016% AT STATION 192+75.64 TO 0.083% AT STATION 192+85.64.
  - FOR DRIVE PROFILES AND BIGELOW DRIVE PROFILES PLEASE SEE SHEETS P.56-58.



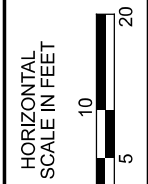
LOCATION	ELEVATION	STATION	OFFSET	LOCATION	ELEVATION	STATION	OFFSET	LOCATION	ELEVATION	STATION	OFFSET
E1	1114.08	+42.50	13.00' LT	E12	1113.46	+51.71	38.22' RT	E25	1114.44	+37.76	44.92' RT
E2	1114.58	+40.00	19.00' LT	E13	1113.39	+42.50	37.92' RT	E26	1114.50	+37.20	49.71' RT
E3	1114.64	+40.00	23.00' LT	E14	1113.23	+29.10	58.69' RT	E27	1114.63	+56.70	50.83' RT
E4	1115.24	+40.00	36.33' LT	E15	1113.46	+33.14	66.99' RT	E28	1114.56	+56.69	55.83' RT
E5	1115.45 EX	+33.12	36.21' LT	E16	1113.51	+38.06	67.85' RT	E29	1114.56	+61.70	50.84' RT
E6	1115.48 EX	+33.08	40.16' LT	E17	1113.49	+34.03	62.99' RT	E30	1114.48	+61.69	55.84' RT
E7	1115.32	+40.00	40.31' LT	E18	1113.76	+35.63	58.25' RT	E31	1114.06	+68.20	50.85' RT
E8	1115.24	+45.00	40.42' LT	E19	1113.39	+44.79	44.07' RT	E32	1114.03	+70.11	55.85' RT
E9	1115.16	+45.00	36.42' LT	E20	1113.44	+48.46	40.67' RT	E33	1114.08	+66.00	47.31' RT
E10	1114.56	+45.00	23.00' LT	E21	1113.46	+47.10	48.94' RT				
E11	1114.50	+45.00	19.00' LT	E22	1113.51 EX	+64.54	48.41' RT				
				E23	1113.55 EX	+64.59	43.50' RT				
				E24	1113.53	+55.48	43.73' RT				

PAVEMENT DETAILS  
 STA 192+25.00 TO STA 194+50.00

 FULL DEPTH PAVEMENT CONSTRUCTION

**NOTES:**

- ELEVATIONS ALONG THE CURB, ARE EDGE OF PAVEMENT ELEVATIONS (ADD 4" FOR TOP OF TYPE 2 CURB AND GUTTER ELEVATIONS).
- FOR DRIVE PROFILES AND BIGELOW DRIVE PROFILES PLEASE SEE SHEETS P.56-58.



**PAVEMENT DETAILS**  
STA 194+50.00 TO STA 199+25.00

DESIGN AGENCY

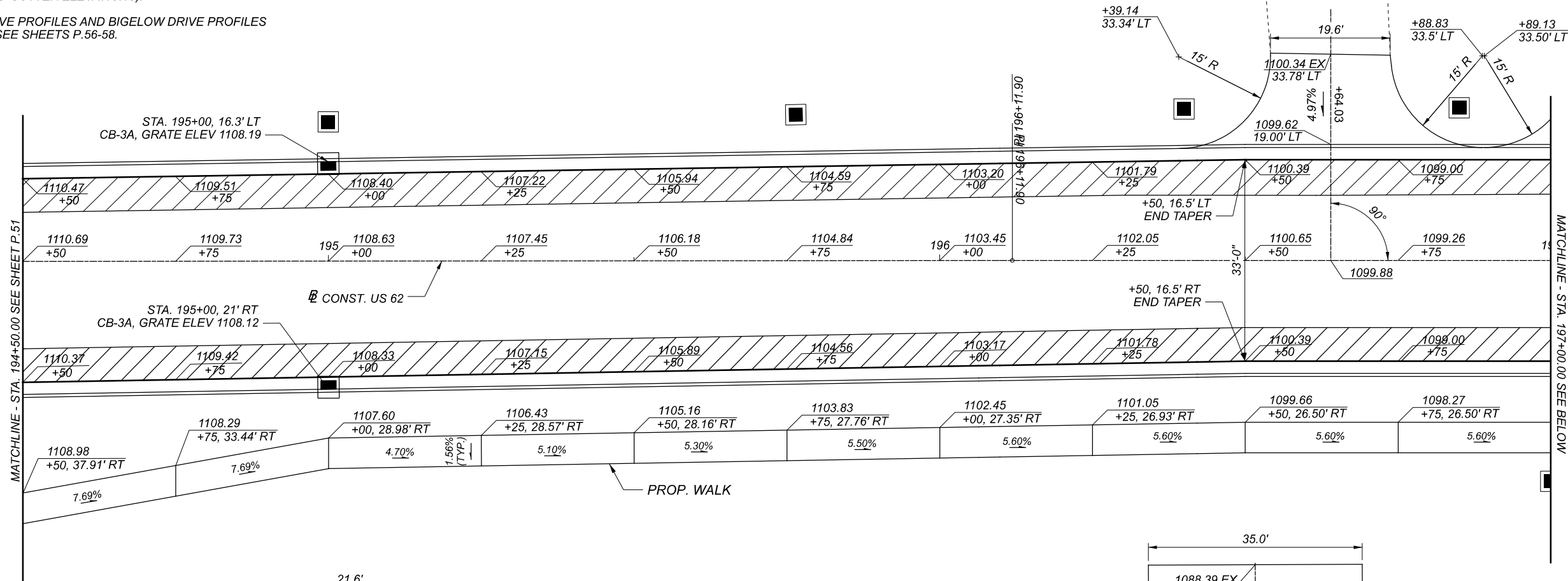


DESIGNER  
AJP

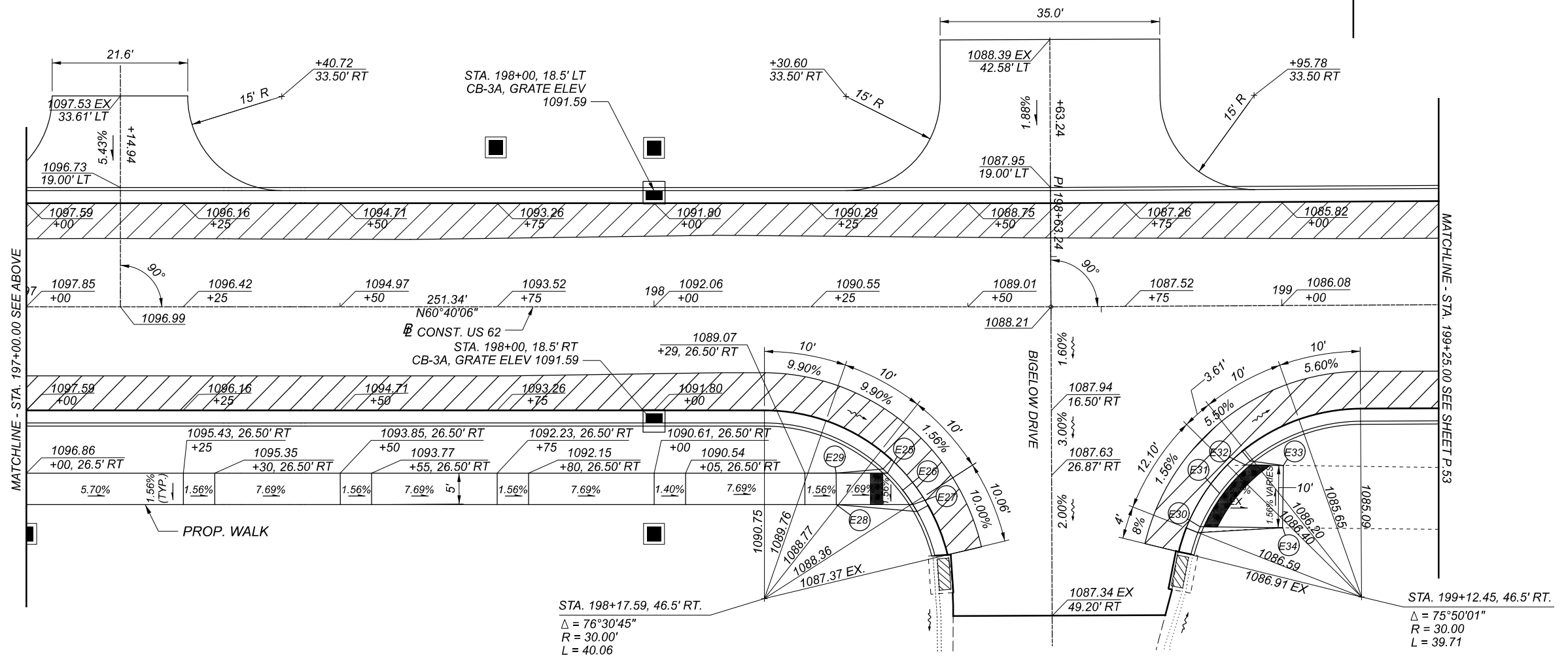
REVIEWER  
JDH 10-29-21

PROJECT ID  
110861

SHEET TOTAL  
P.52 89



LOCATION	ELEVATION	STATION	OFFSET
E25	1088.37	+37.19	26.50' RT
E26	1088.32	+38.70	28.88' RT
E27	1088.26	+41.24	31.50' RT
E28	1088.92	+29.03	31.50' RT
E29	1089.00	+29.03	26.50' RT
E30	1086.49	+86.84	35.18' RT
E31	1086.39	+89.92	29.88' RT
E32	1086.18	+94.08	25.37' RT
E33	1085.77 EX	+00.11	25.47' RT
E34	1085.65 EX	+99.94	35.40' RT



STA. 198+17.59, 46.5' RT.  
Δ = 76°30'45"  
R = 30.00'  
L = 40.06

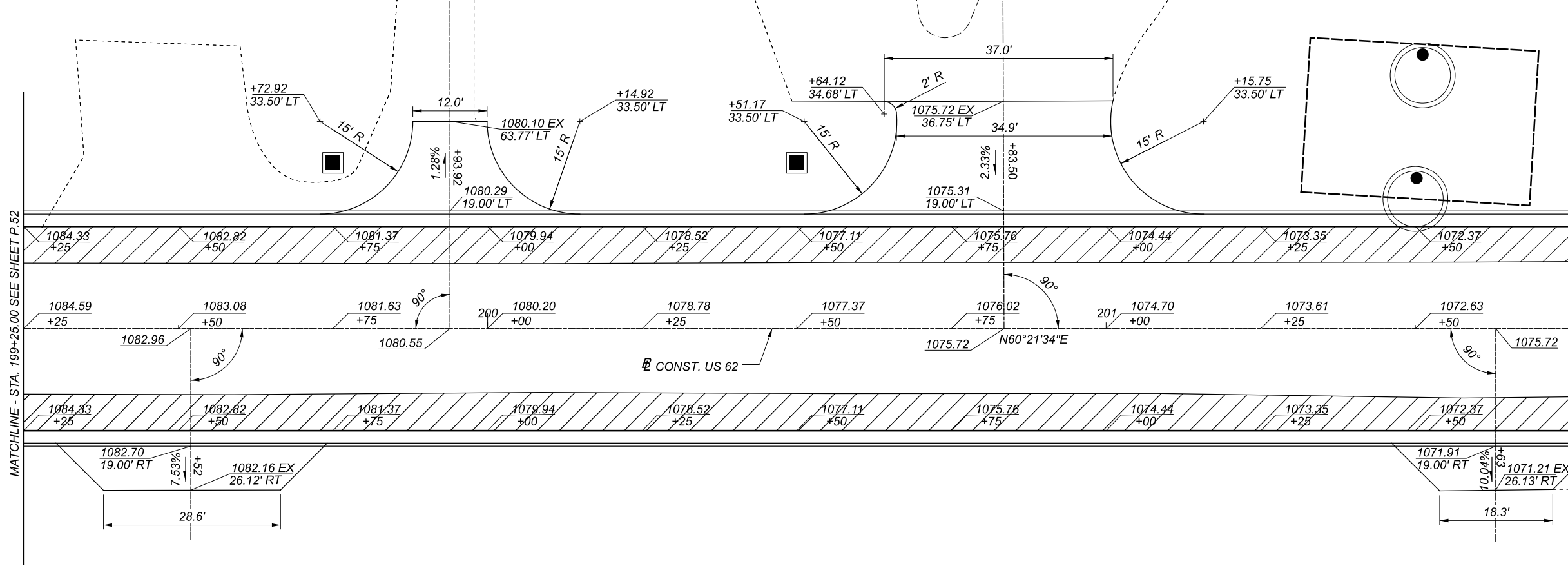
STA. 199+12.45, 46.5' RT.  
Δ = 75°50'01"  
R = 30.00'  
L = 39.71

LIC-62-3.64

MODEL: Sheet PAPER: 11x17 (in.) DATE: 2021-11-18 TIME: 10:29:18 AM USER: mott+phillips  
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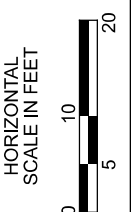
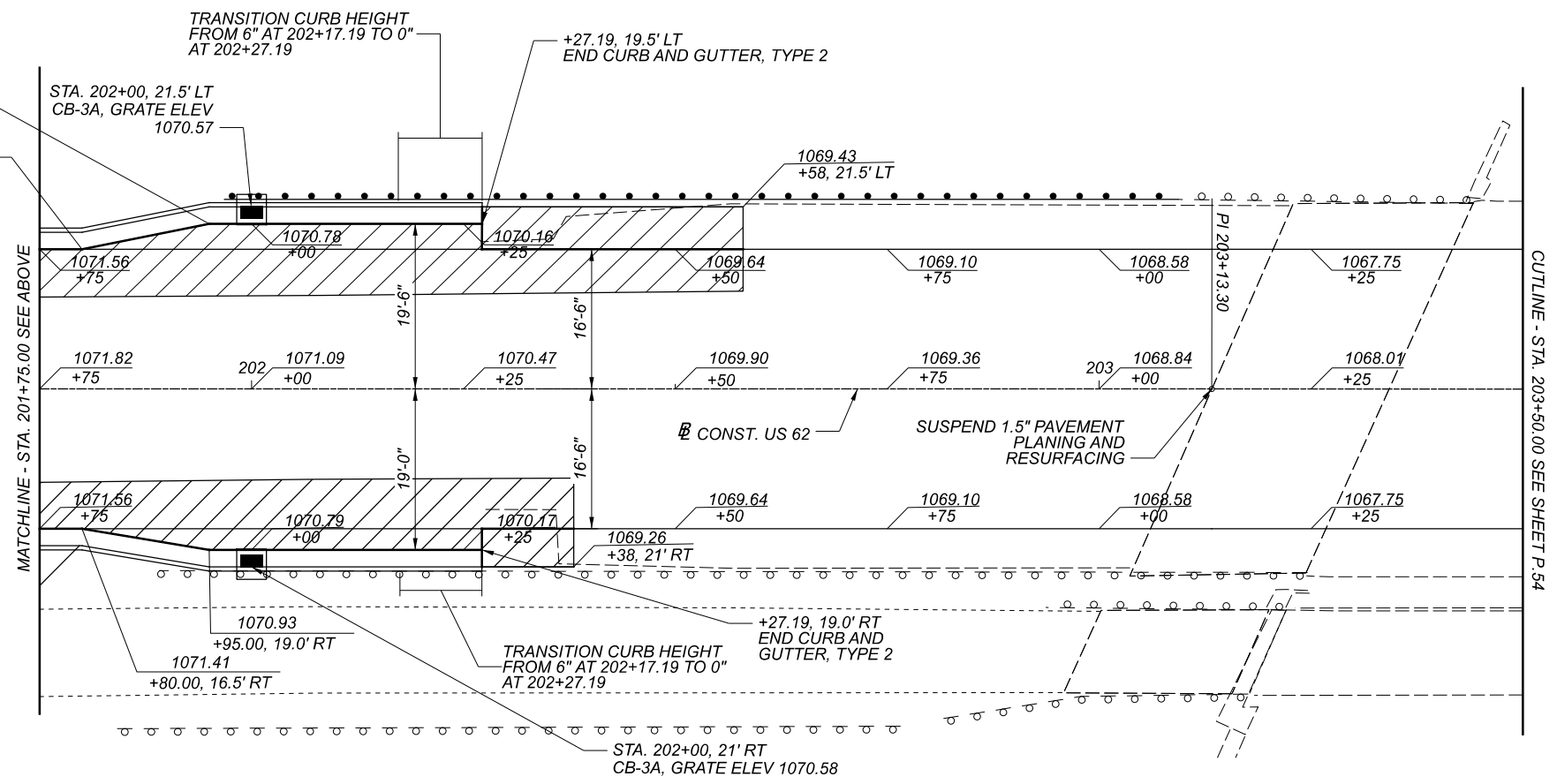
MATCHLINE - STA. 199+25.00 SEE SHEET P. 52

MATCHLINE - STA. 201+75.00 SEE BELOW



FULL DEPTH PAVEMENT CONSTRUCTION

- NOTES:
- ELEVATIONS ALONG THE CURB, ARE EDGE OF PAVEMENT ELEVATIONS (ADD 4" FOR TOP OF TYPE 2 CURB AND GUTTER ELEVATIONS).
  - FOR DRIVE PROFILES AND BIGELOW DRIVE PROFILES PLEASE SEE SHEETS P.56-58.



PAVEMENT DETAILS  
STA 199+25.00 TO STA 203+50.00

DESIGN AGENCY

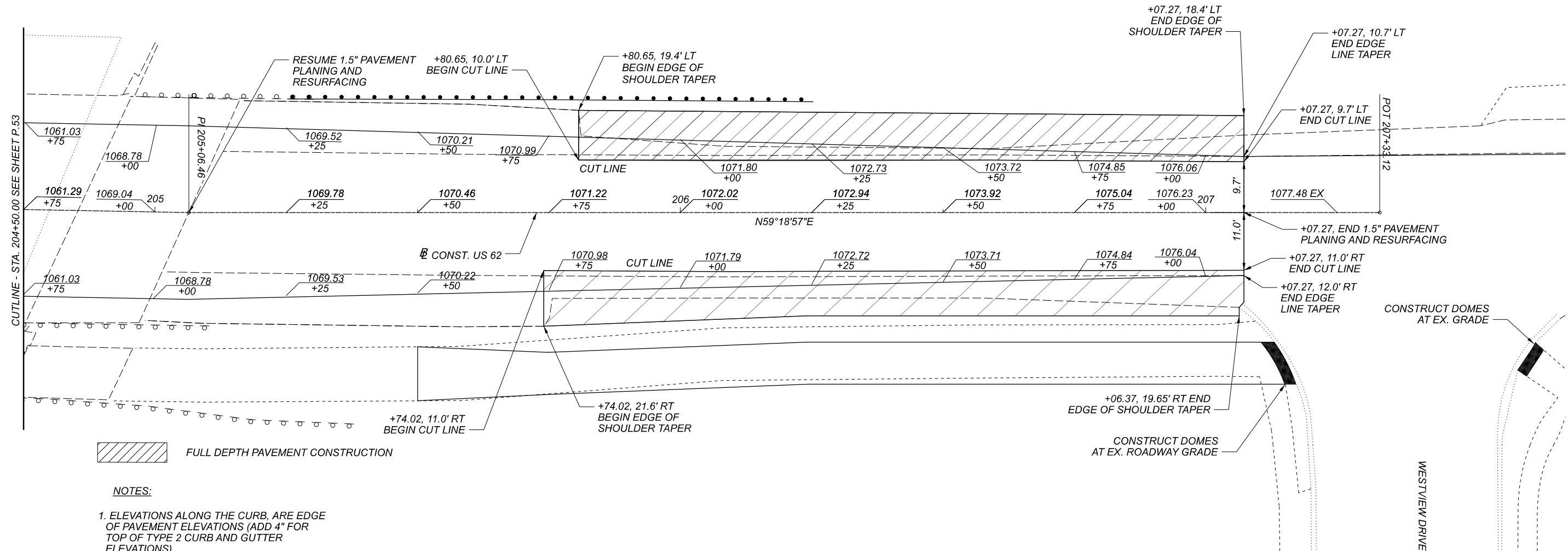


DESIGNER  
AJP

REVIEWER  
JDH 10-29-21

PROJECT ID  
110861

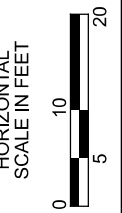
SHEET	TOTAL
P.53	89



 FULL DEPTH PAVEMENT CONSTRUCTION

**NOTES:**

1. ELEVATIONS ALONG THE CURB, ARE EDGE OF PAVEMENT ELEVATIONS (ADD 4" FOR TOP OF TYPE 2 CURB AND GUTTER ELEVATIONS).
2. FOR DRIVE PROFILES AND BIGELOW DRIVE PROFILES PLEASE SEE SHEETS P.56-58.



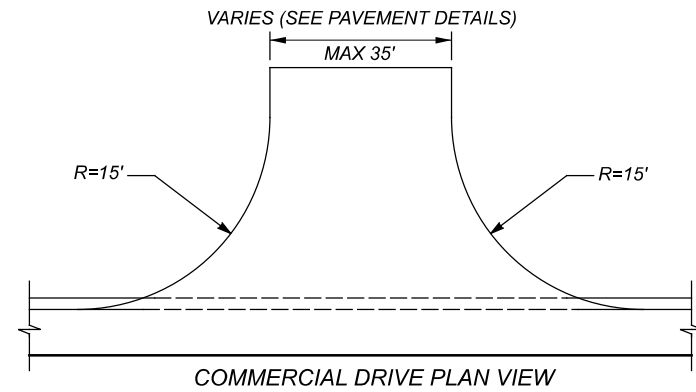
**PAVEMENT DETAILS  
STA 204+50.00 TO STA 207+7.27**

DESIGN AGENCY



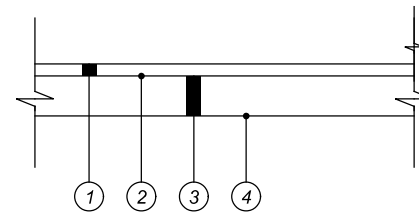
DESIGNER	AJP
REVIEWER	JDH
PROJECT ID	110861
SHEET	P.54
TOTAL	89

- NOTES:
- FOR DRIVE PROFILES  
SEE SHEETS P.56 TO P.58
  - FOR ADDITIONAL DRIVE  
DETAILS SEE PAVEMENT  
DETAIL SHEETS P.51 TO P.54

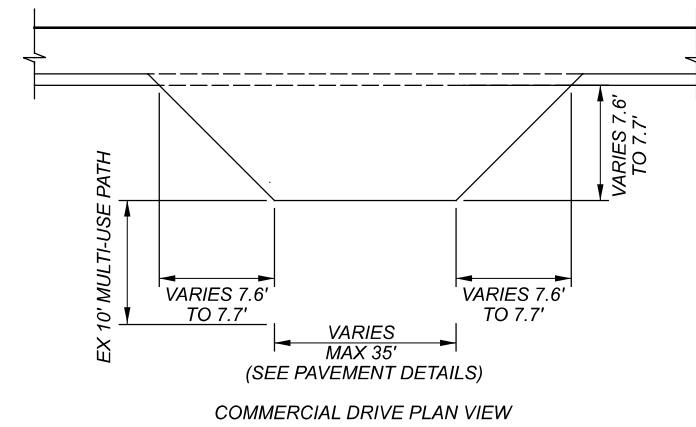


- ITEM 441 - 1.5" AC SURFACE COURSE, TYPE 1, (449), PG64-22
- ITEM 407 - NON-TRACKING TACK COAT
- ITEM 301 - 5" ASPHALT CONCRETE BASE, PG64-22, (449)
- ITEM 204 - SUBGRADE COMPACTION

STA 193+94.50, STA 196+64.03, STA 197+14.94  
STA 198+63.24, STA 199+93.92, STA 200+83.50



COMMERCIAL ASPHALT  
DRIVEWAY SECTION



- ITEM 441 - 1.5" AC SURFACE COURSE, TYPE 1, (449), PG64-22
- ITEM 407 - NON-TRACKING TACK COAT
- ITEM 301 - 5" ASPHALT CONCRETE BASE, PG64-22, (449)
- ITEM 204 - SUBGRADE COMPACTION

STA 199+52.11, STA 201+63.00

DRIVEWAY SUBSUMMARY

REFERENCE NO.	SHEET NO.	ROUTE	STATION	SIDE	CADD AREA (SF)	DRIVE TYPE	202	204	301	407	441
							PAVEMENT REMOVED	SUBGRADE COMPACTION	5" ASPHALT CONCRETE BASE, PG64-22, (449)	NON-TRACKING TACK COAT	1.5" AC SURFACE COURSE, TYPE 1, (449), PG64-22
							SY	SY	CY	GAL	CY
DR-1	P.33	US 62	193+94.50	LT	695.80	COMM.	90.59	77.31	10.74	4.25	3.22
DR-2	P.33	US 62	196+64.03	LT	373.68	COMM.	40.39	41.52	5.77	2.28	1.73
DR-3	P.33	US 62	197+14.94	LT	399.48	COMM.	49.73	44.39	6.16	2.44	1.85
DR-4	P.33-34	US 62	198+63.24	LT	909.16	COMM.	91.98	101.02	14.03	5.56	4.21
DR-5	P.34	US 62	199+52.11	RT	254.40	COMM.	43.61	28.27	3.93	1.55	1.18
DR-6	P.34	US 62	199+93.92	LT	258.15	COMM.	151.24	28.68	3.98	1.58	1.20
DR-7	P.34	US 62	200+83.50	LT	699.24	COMM.	130.19	77.69	10.79	4.27	3.24
DR-8	P.34	US 62	201+63.00	RT	181.12	COMM.	30.17	20.12	2.80	1.11	0.84
SUBTOTALS							627.90	419.00	58.20	23.04	17.47
TOTALS CARRIED TO GENERAL SUMMARY							628	419	58	23	17

DRIVE DETAILS

DESIGN AGENCY



DESIGNER

AJP

REVIEWER

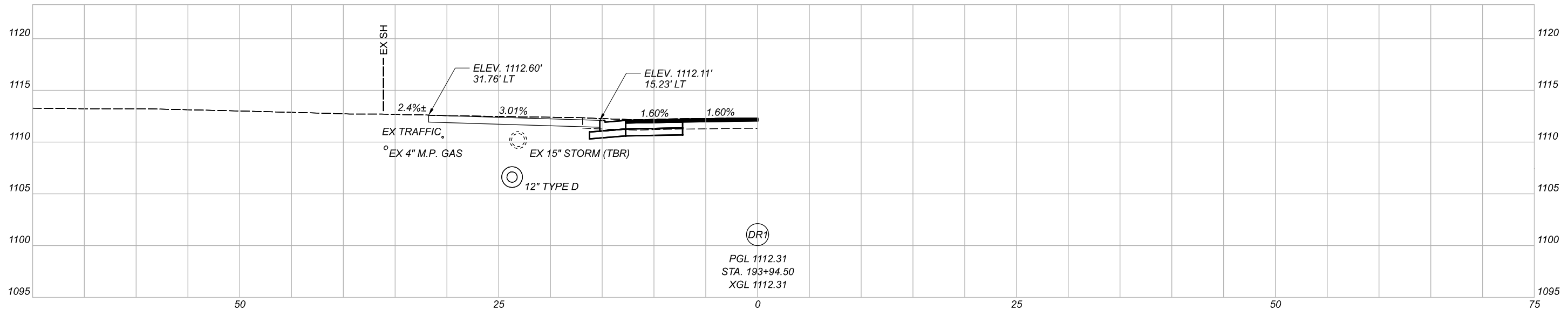
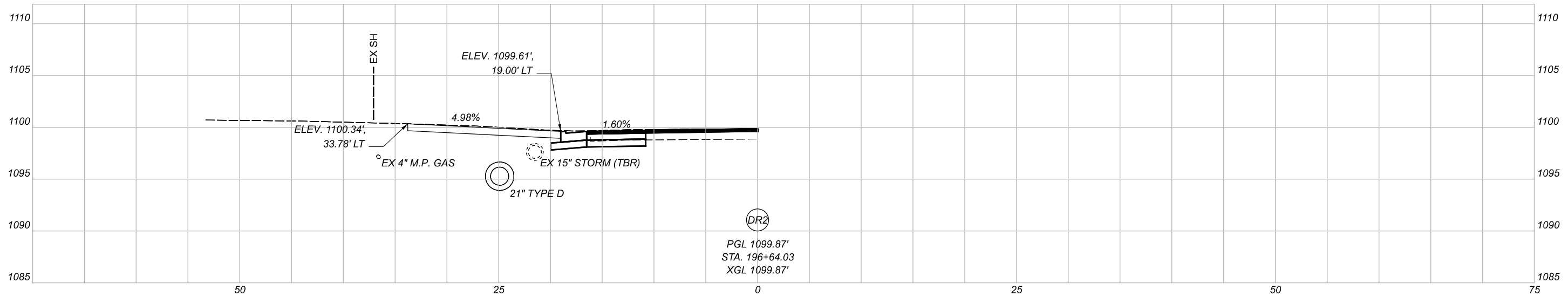
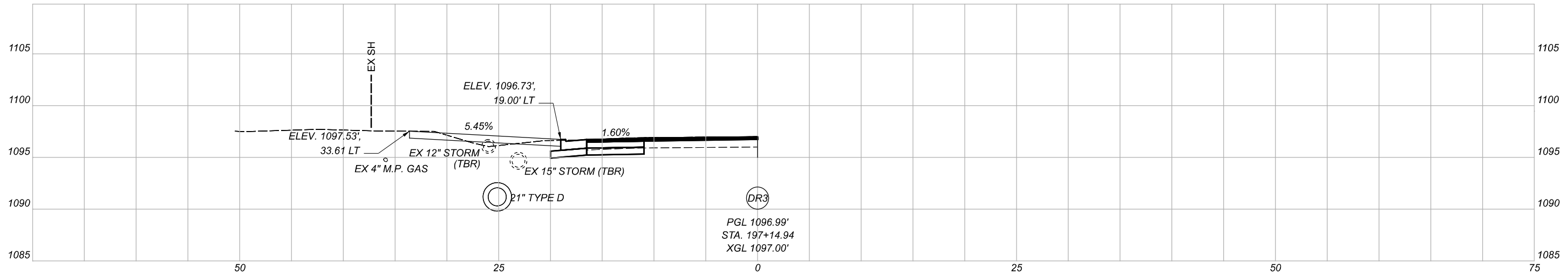
JDH 12-20-21

PROJECT ID

110861

SHEET TOTAL

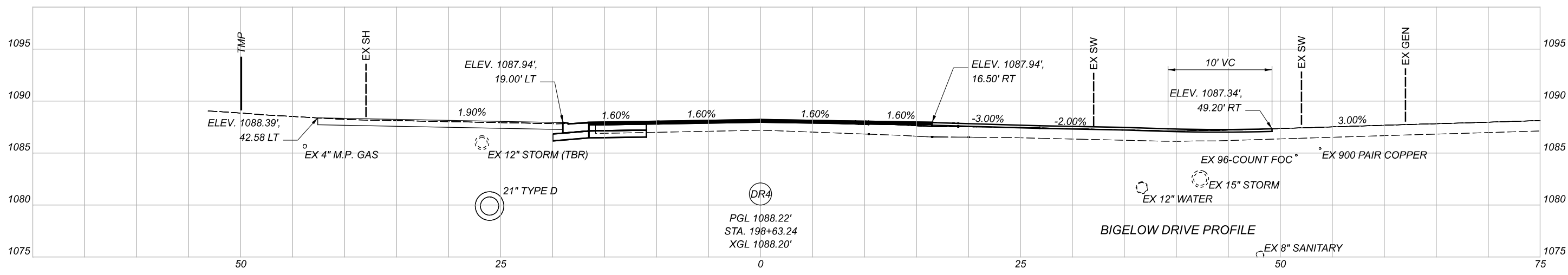
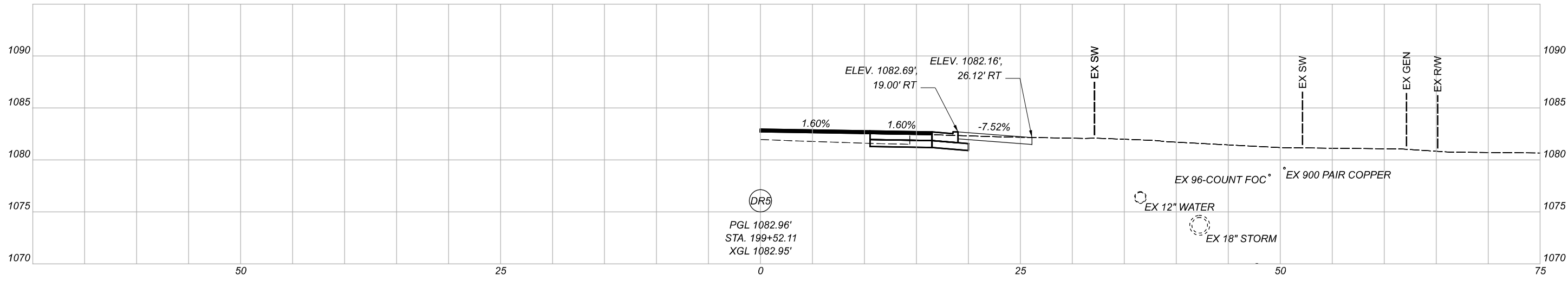
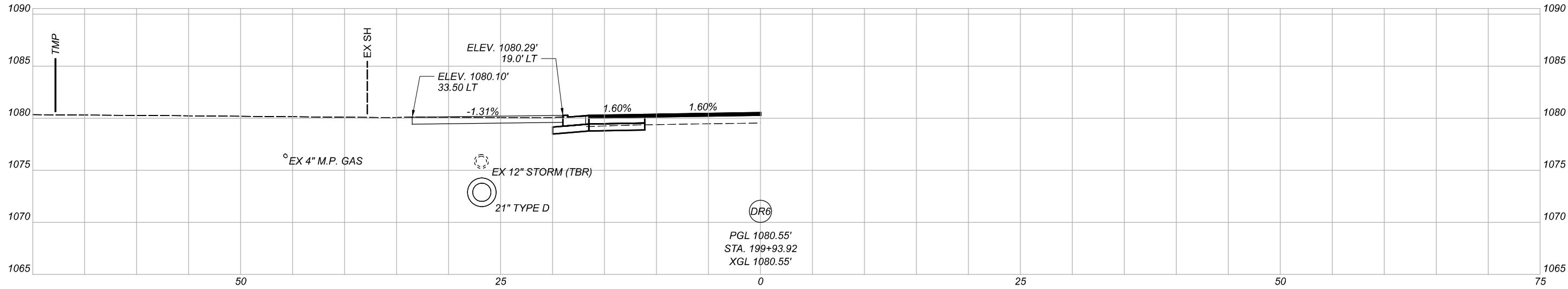
P.55 89



DRIVE PROFILES  
 STA. 193+94.50 TO STA. 197+14.94

DESIGN AGENCY	
<b>[ IBI ]</b>	
DESIGNER	BSB
REVIEWER	JDH
PROJECT ID	110861
SHEET	TOTAL
P.56	89





BIGELOW DRIVE PROFILE

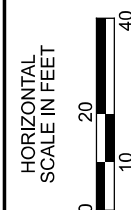
DRIVE PROFILES  
STA. 198+63.24 TO STA. 199+93.92

DESIGN AGENCY	[IBI]
DESIGNER	
REVIEWER	JDH
PROJECT ID	110861
SHEET	P.57
TOTAL	89



SHEET NO.	REF. NO.	LOCATION	STATION	SIDE	CODE	SIZE (INCHES)	626		630					
							BARRIER REFLECTOR, TYPE 2 (BIDIRECTIONAL)	GROUND MOUNTED SUPPORT, NO. 3 POST	SIGN POST REFLECTOR	SIGN, FLAT SHEET	REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL	REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL		
							EACH	FT	EACH	SF	EACH	EACH		
P.61	R-1	US 62	192+50	RT						1	1			
P.62	R-2	US 62	194+50	RT						1	1			
P.62	R-3	US 62	194+89	LT						2	2			
P.62	R-4	US 62	196+89	LT						1	1			
P.62	R-5	US 62	197+50	RT						1	1			
P.62	R-6	US 62	198+35	RT						2	1			
P.62	R-7	US 62	198+75	RT						2	1			
P.62	R-8	US 62	198+89	LT						2	1			
P.62	R-9	US 62	199+25	LT						1	1			
P.62	S-1	US 62	194+50	LT	R3-H8BH-36	36 x 30	14.0		7.5					
P.62	S-2	US 62	195+00	RT	R3-9CP-24	24 x 6	14.0		1.0					
P.62	S-2	US 62	195+00	RT	R3-9B-24	24 x 36			6.0					
P.62	S-3	US 62	197+50	LT	W3-3-30	30 x 30	14.0		6.3					
P.62	S-4	US 62	198+89	RT	R1-1-30	30 x 30	14.0	1	6.3					
P.62	S-5	US 62	199+00	RT	D3-1-12	12 x 48	15.0		4.0					
P.63	R-10	US 62	200+40	RT						1	1			
P.63	BR-1	US 62	201+90	RT			5							
P.63	BR-2	US 62	201+97	LT			7							
P.63	R-11	US 62	202+15	LT						1	1			
P.63	S-6	US 62	200+50	RT	R2-1-24	24 x 36	14.0		6.0					
P.63	S-7	US 62	205+00	LT	R3-9CP-24	24 x 6	14.0		1.0					
P.63	S-7	US 62	205+00	LT	R3-9B-24	24 x 36			6.0					
P.63	S-8	US 62	205+00	RT	R3-9DP-24	24 x 6	14.0		1.0					
P.63	S-8	US 62	205+00	RT	R3-9B-24	24 x 36			6.0					
P.64	R-12	US 62	207+05	RT						1	1			
TOTALS CARRIED TO GENERAL SUMMARY							12	113.0	1	51.1	16	13		

SHEET NO.	REFERENCE NO.	LOCATION	STATION		SIDE	644							646			
			FROM	TO		EDGE LINE, 4" (WHITE)	CENTER LINE	CHANNELIZING LINE, 8"	STOP LINE	CROSSWALK LINE	TRANSVERSE/DIAGONAL LINE (YELLOW)	LANE ARROW	EDGE LINE, 4" (WHITE)	CENTER LINE		
			MILE	MILE		FT	FT	FT	FT	FT	EACH	MILE	MILE			
P.60-P.61	EW-1	US 62	186+30	192+75	LT	0.12										
P.60-P.61	EW-2	US 62	186+30	192+75	RT	0.12										
P.60-P.62	CL-1	US 62	186+30	194+53	LT		0.16									
P.60-P.61	CL-2	US 62	186+30	190+50	RT		0.08									
P.60-P.61	TY-1	US 62	186+30	190+50	RT					420						
P.61	CH-1	US 62	190+70	191+66	RT			96								
P.61	A-1	US 62		190+90	RT									1		
P.61	A-2	US 62		191+56	RT									1		
P.61	SL-1	US 62		191+66	RT				33							
P.61	CW-1	US 62	191+66	192+30	RT					24		128				
P.61	SL-2		DRIVEWAY							24						
P.61	A-3		DRIVEWAY											2		
P.61	CH-2		DRIVEWAY					50								
P.61	CW-2	US 62	192+38	192+48	RT/LT						96					
P.61	SL-3	US 62		192+53	RT/LT				22							
P.61	A-4	US 62		192+63	RT									1		
P.61	A-5	US 62		193+29	RT									1		
P.61	A-6	US 62		192+95	RT									1		
P.61-P.62	CH-3	US 62	192+53	194+03	LT			150								
P.62-P.64	CSD-1	US 62	194+53	205+06	LT		0.16									0.04
P.62-P.64	CSD-2	US 62	194+53	205+06	RT		0.16									0.04
P.62	A-7	US 62		196+50	RT									2		
P.62	CW-3		BIGELOW DRIVE													
P.62	SL-4		BIGELOW DRIVE							25						
P.63	A-8	US 62		199+50	RT									2		
P.63-P.64	EW-3	US 62	202+25	208+68	LT	0.08										0.04
P.63-P.64	EW-4	US 62	202+25	208+68	RT	0.08										0.04
P.63	A-9	US 62		202+50	LT									2		
P.64	CL-3	US 62	205+06	208+68	LT		0.07									
P.64	TY-2	US 62	205+06	207+07	RT							201				
P.64	CL-4	US 62	205+06	207+07	RT		0.04									
TOTALS CARRIED TO GENERAL SUMMARY						0.40	0.67	296	104	224	621	13	0.08	0.08		












PAVEMENT MARKING AND SIGNAGE SUBSUMMARY

DESIGN AGENCY

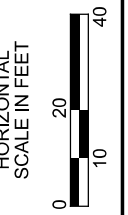
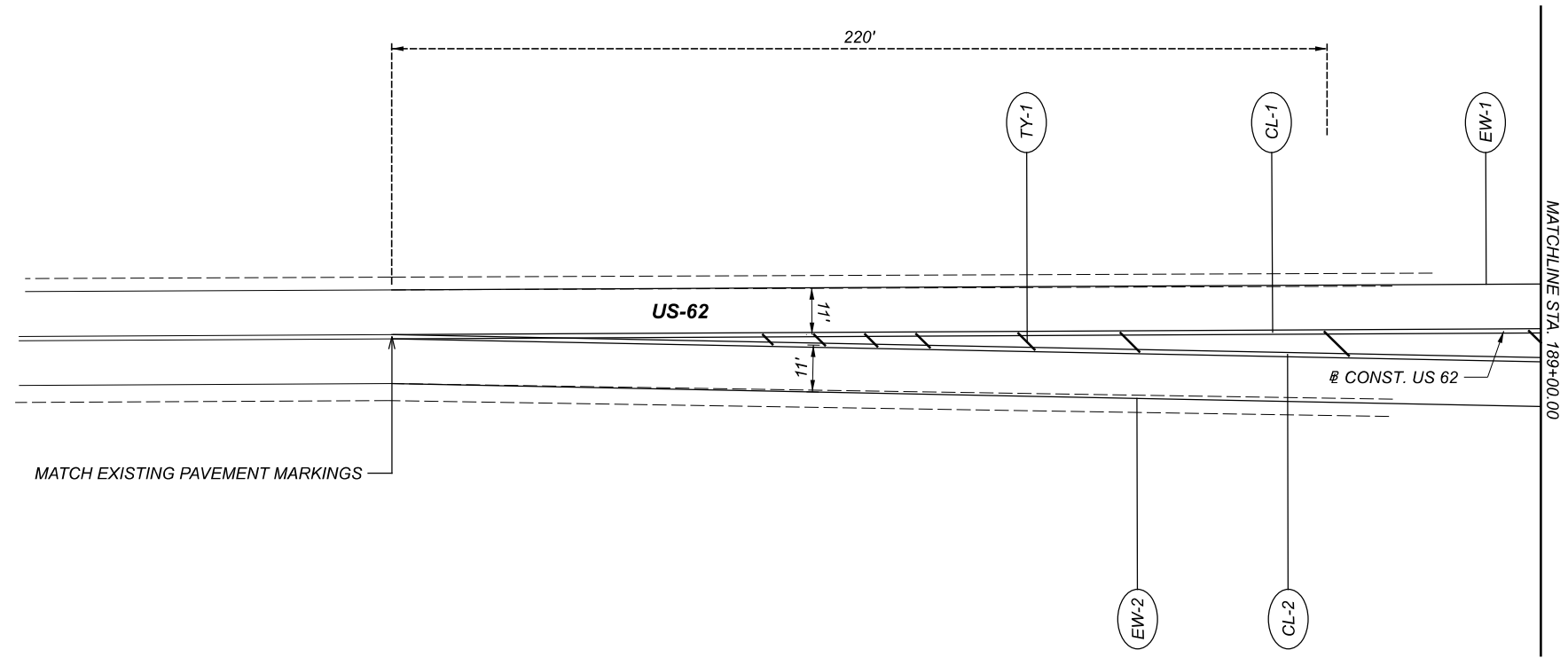
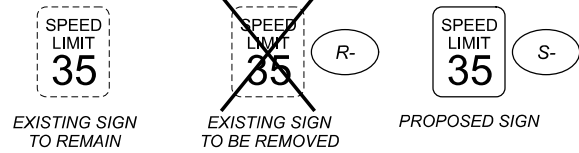


DESIGNER  
BSS  
 REVIEWER  
JDH 06-17-21  
 PROJECT ID  
110861  
 SHEET TOTAL  
P.59 89

**SIGN AND PAVEMENT MARKING LEGEND**

-  **CW-X** CROSSWALK LINE
-  **SL-X** STOP LINE
-  **EW-X** EDGE LINE, 4" (WHITE)
-  **CL-X** CENTER LINE, SOLID DOUBLE
-  **CSD-X** CENTERLINE, SOLID & DASHED
-  **CH-X** CHANNELIZING LINE, 8"
-  **A-X** LANE ARROW
-  **TY-X** TRANSVERSE/DIAGONAL LINE (YELLOW)
-  **BR-X** BARRIER REFLECTOR

MARKINGS ON ASPHALT SURFACES SHALL BE ITEM 644.  
 MARKINGS ON CONCRETE SURFACES SHALL BE ITEM 646.



**TRAFFIC CONTROL PLAN  
 BEGINNING TO STA. 189+00.00**

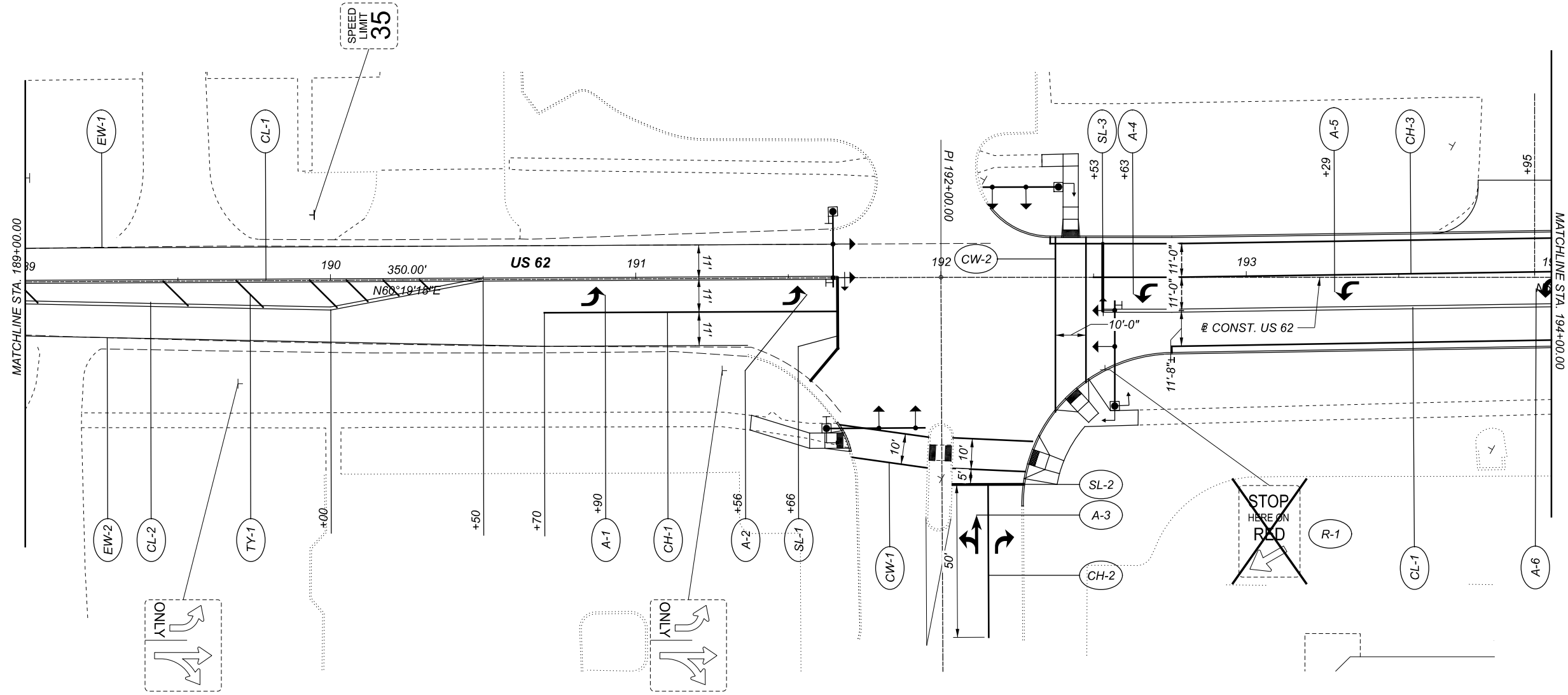
DESIGN AGENCY



DESIGNER	BSS
REVIEWER	JDH 06-17-21
PROJECT ID	110861
SHEET	TOTAL
P.60	89

**NOTES:**

1. FOR SIGN AND PAVEMENT MARKING PLAN LEGEND SEE SHEET P.60



**TRAFFIC CONTROL PLAN**  
 STA. 189+00.00 TO STA. 194+00.00

DESIGN AGENCY

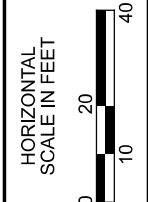


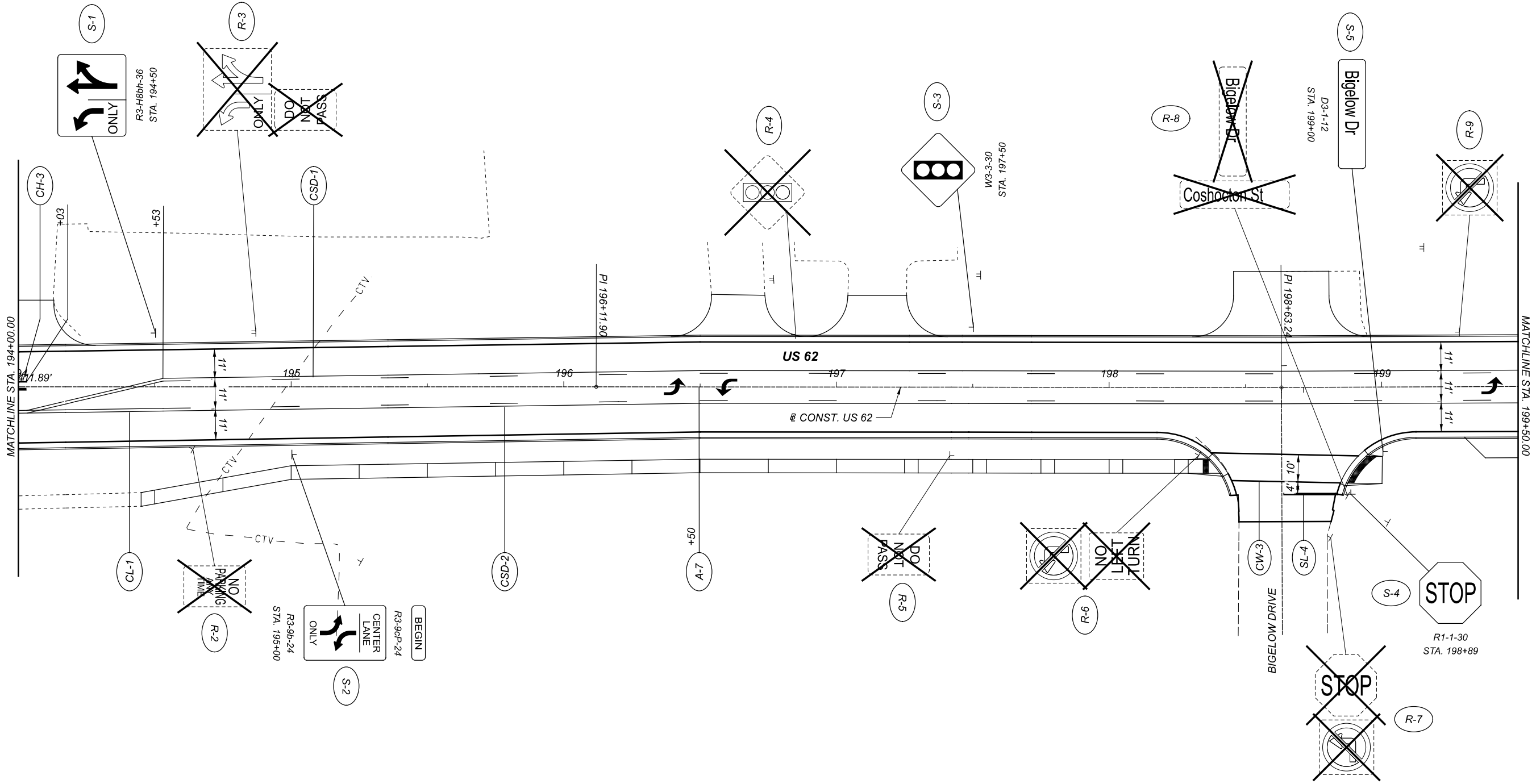
DESIGNER  
 BSS

REVIEWER  
 JDH 06-17-21

PROJECT ID  
 110861

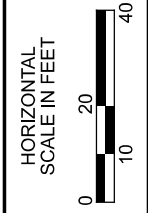
SHEET	TOTAL
P.61	89





**NOTES:**

1. FOR SIGN AND PAVEMENT MARKING PLAN LEGEND SEE SHEET P.60



**TRAFFIC CONTROL PLAN**  
 STA. 194+00.00 TO STA. 199+50.00

DESIGN AGENCY



DESIGNER

BSS

REVIEWER

JDH 06-17-21

PROJECT ID

110861

SHEET TOTAL

P.62 89







**POWER SUPPLY FOR TRAFFIC SIGNALS**

ELECTRIC POWER SHALL BE OBTAINED FROM THE AEP AT THE LOCATION INDICATED ON THE PLANS. POWER SUPPLIED SHALL BE 120/240 VOLTS.

THE CONTRACTOR SHALL CONTACT THE AEP SOLUTION CENTER TO PLACE AN ORDER ONLINE AT "AEP.OHIO.COM" OR CALL 1-800-277-2177.

**UTILITIES**

SEE SHEET 6 FOR A LIST OF UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS.

THE LOCATION OF THE UNDERGROUND UTILITIES ARE AS OBTAINED FROM THE OWNERS AS REQUIRED BY SECTION 153.64 O.R.C.

**SIGNAL ACTIVATION**

PRIOR TO ACTIVATING THE NEW SIGNAL TO STOP-AND-GO MODE AND/OR REMOVING THE EXISTING TRAFFIC SIGNAL FROM SERVICE, ALL ITEMS IN THE PROPOSED SIGNAL PLAN SHALL BE FULLY COMPLETED, (I.E., VEHICLE DETECTION, PEDESTRIAN SIGNAL HEADS, ETC.). IF THERE ARE CONSTRUCTABILITY ISSUES (I.E., ROADWAY WIDENING, ETC.) THAT PREVENT THE SIGNAL FROM BEING COMPLETED PRIOR TO ACTIVATION, IT SHALL BE BROUGHT TO THE ATTENTION OF THE PROJECT ENGINEER AND DISTRICT TRAFFIC ENGINEER. THE DISTRICT TRAFFIC ENGINEER WILL THEN REVIEW, APPROVE OR REJECT PROPOSALS TO ACTIVATE THE TRAFFIC SIGNAL PRIOR TO COMPLETION.

THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER AND DISTRICT TRAFFIC ENGINEER AT LEAST 10 WORKING DAYS PRIOR TO SCHEDULING THE FINAL INSPECTION OF THE SIGNAL INSTALLATION. FINAL INSPECTION IS NOT CONSIDERED COMPLETE UNTIL DESIGNATED DISTRICT TRAFFIC PERSONNEL INSPECT THE TRAFFIC SIGNAL AND ISSUE WRITTEN APPROVAL. IF ISSUES ARE FOUND DURING THE FINAL INSPECTION THAT AFFECT THE SAFETY OF THE TRAVELING PUBLIC AND/OR THE EFFICIENCY OF THE INTERSECTION, THE SIGNAL SHALL NOT BE ACTIVATED ON THE PROPOSED DATE. ANY PUNCH LIST ITEMS THAT ARE FOUND SHALL BE CORRECTED AND REINSPECTED BY DISTRICT TRAFFIC PERSONNEL PRIOR TO FINAL ACCEPTANCE. ODOT FORCES SHALL ONLY ASSUME DAY TO DAY MAINTENANCE OF THE TRAFFIC SIGNAL AFTER FINAL WRITTEN ACCEPTANCE HAS BEEN ISSUED.

**ITEM 632 REMOVAL OF TRAFFIC INSTALLATION**

TRAFFIC SIGNAL INSTALLATIONS, INCLUDING SIGNAL HEADS, CABLE, MESSENGER WIRE, STRAIN POLES, CABINET, PULL BOXES, CONTROLLER, ETC. SHALL BE REMOVED IN ACCORDANCE WITH C&MS 632.26 AND AS INDICATED ON THE PLANS. REMOVED ITEMS SHALL BE REUSED AS PART OF A NEW INSTALLATION ON THE PROJECT OR STORED ON THE PROJECT FOR SALVAGE BY ODOT DISTRICT 5 IN ACCORDANCE WITH THE LISTING GIVEN HEREIN.

ITEMS TO BE STORED:

- SIGNAL HEADS
- CABINET & CONTROLLER
- UPS CABINET & HARDWARE
- STRAIN POLES

REMOVED ITEMS SHALL BE DELIVERED TO THE NEAREST ODOT FACILITY WHOSE ADDRESS IS LISTED BELOW:

9659 JACKSONTOWN RD, THORNVILLE, OH 43076

**ITEM 632 REMOVAL OF TRAFFIC INSTALLATION (CONTINUED)**

IN THE EVENT THE ITEMS STORED ON THE PROJECT FOR SALVAGE BY THE LOCAL AGENCY ARE NOT REMOVED, THE CONTRACTOR SHALL, WHEN DIRECTED BY THE ENGINEER IN WRITING, REMOVE AND DISPOSE OF THE ITEMS AT NO ADDITIONAL COST TO THE PROJECT.

**DETECTION MAINTENANCE**

IF VEHICLE DETECTION BECOMES UNEXPECTEDLY DISABLED, REQUIRES MODIFICATION, OR IS SCHEDULED TO BE TEMPORARILY REMOVED DURING THE CONSTRUCTION PROJECT, THE CONTRACTOR SHALL IMMEDIATELY NOTIFY THE PROJECT ENGINEER AND DISTRICT TRAFFIC ENGINEER.

IF THE LOSS OF VEHICLE DETECTION IS KNOWN PRIOR TO THE START OF CONSTRUCTION, IT SHALL BE DISCUSSED AT THE PRECONSTRUCTION MEETING. AT SUCH TIME, THE DISTRICT TRAFFIC ENGINEER SHALL ADVISE THE PROJECT ENGINEER AND CONTRACTOR ON THE APPROPRIATE ACTION TO RECTIFY ANY LOSS OF VEHICLE DETECTION. THIS MAY INCLUDE PLACING THE TRAFFIC SIGNAL ON MINIMUM OR MAXIMUM RECALL, MODIFYING THE MINIMUM GREEN TIMES, AND REMOVING THE MALFUNCTIONING DETECTION FROM SERVICE. WHERE NON-INTRUSIVE DETECTION (I.E. VIDEO, RADAR) ALREADY EXISTS, THE CONTRACTOR SHALL INSURE THAT DETECTION IS OPERATING AND MAINTAINED BY RECONFIGURING THE DETECTION UNITS ACCORDINGLY DURING ALL CONSTRUCTION PHASES. THIS IS TO AVOID THE SIGNAL FROM MAXING OUT THE EFFECTED SIGNAL PHASE AND CREATING UNNECESSARY DELAYS.

LOCATIONS WHERE NON-INTRUSIVE DETECTION IS PROPOSED AND THE EXISTING VEHICLE DETECTION IS TO BE ABANDONED, THE NON-INTRUSIVE VEHICLE DETECTION SHALL BE INSTALLED, CONFIGURED AND MADE FULLY FUNCTIONAL PRIOR TO THE EXISTING DETECTION BEING DISABLED. THE CONTRACTOR SHALL CONTINUE TO MAINTAIN AND MODIFY THE DETECTION UNTIL FINAL ACCEPTANCE OF THE TRAFFIC SIGNAL. THIS IS TO ENSURE VEHICLE DETECTION REMAINS FULLY FUNCTIONAL THROUGHOUT CONSTRUCTION.

**WORK INSPECTION**

THE CONTRACTOR SHALL PROVIDE THE PROJECT ENGINEER AND DISTRICT TRAFFIC ENGINEER WITH 72 HOUR NOTICE OF ANY SIGNAL WORK TO BE PERFORMED AT THE INTERSECTION SITE SO THAT INSPECTION SERVICES CAN BE SUPPLIED.

**GUARANTEE**

THE CONTRACTOR SHALL GUARANTEE THAT THE TRAFFIC CONTROL SYSTEM INSTALLED AS PART OF THIS CONTRACT SHALL OPERATE SATISFACTORILY FOR A PERIOD OF ONE YEAR FOLLOWING COMPLETION OF THE 10-DAY PERFORMANCE TEST. IN THE EVENT OF UNSATISFACTORY OPERATION THE CONTRACTOR SHALL CORRECT FAULTY INSTALLATIONS, MAKE REPAIRS AND REPLACE DEFECTIVE PARTS WITH NEW PARTS OF EQUAL OR BETTER QUALITY. EQUIPMENT, MATERIAL AND LABOR COSTS INCURRED IN CORRECTING AN UNSATISFACTORY OPERATION SHALL BE BORNE BY THE CONTRACTOR.

THE GUARANTEE SHALL COVER THE FOLLOWING ITEMS OF THE TRAFFIC CONTROL SYSTEM: CONTROLLERS AND ASSOCIATED EQUIPMENT, DETECTOR UNITS, INTERCONNECTION ITEMS AND MASTER CONTROL EQUIPMENT.

CUSTOMARY MANUFACTURER'S GUARANTEES FOR THE FOREGOING ITEMS SHALL BE TURNED OVER TO THE STATE OR THE MAINTAINING AGENCY FOLLOWING ACCEPTANCE OF THE EQUIPMENT.

THE COST OF GUARANTEEING THE TRAFFIC CONTROL SYSTEM WILL BE INCIDENTAL TO AND INCLUDED IN THE CONTRACT UNIT PRICE OF THE VARIOUS ITEMS MAKING UP THE SYSTEM.

**632 PEDESTRIAN SIGNAL HEAD (LED), (COUNTDOWN), TYPE D2, AS PER PLAN**

IN ADDITION TO THE REQUIREMENTS OF C&MS 632 AND 732 THE FOLLOWING SHALL APPLY:

1. SIGNAL HEADS AND VISORS SHALL BE CONSTRUCTED OF BLACK POLYCARBONATE PLASTIC AND MEET ITE SPECIFICATIONS.
2. PROPER EXTERIOR COLORS SHALL BE OBTAINED BY USE OF COLORED PLASTIC MATERIAL RATHER THAN PAINTING.
3. PIPE, SPACERS AND FITTINGS CONSTRUCTED OF POLYCARBONATE PLASTIC MAY BE USED IN LIEU OF GALVANIZED STEEL OR ALUMINUM.
4. THE PEDESTRIAN SIGNAL HEAD SHALL BE OF THE LED COUNTDOWN TYPE.
5. NEW ATTACHMENT HARDWARE AND FITTINGS SHALL BE USED.
6. THE LIGHT EMITTING DIODE (LED) MODULES SHALL MEET THE REQUIREMENTS OF C&MS 732.04-C. THE CONTRACTOR SHALL PROVIDE ODOT, IN WRITING, WITH THE LED MANUFACTURER NAME, SERIAL NUMBER, PART NUMBER, DESCRIPTION OF LAMP, AND DATE OF MANUFACTURE FOR ALL LED UNITS THAT ARE TO BE USED IN THE SIGNAL HEAD PRIOR TO INSTALLATION, FOR ACCEPTANCE AND WARRANTY PURPOSES.

PAYMENT FOR ITEM 632 PEDESTRIAN SIGNAL HEAD (LED), (COUNTDOWN), TYPE D2, AS PER PLAN SHALL BE MADE FOR THE NUMBER OF COMPLETE SIGNAL HEAD FURNISHED AND INSTALLED, INCLUDING ALL LABOR, EQUIPMENT, MATERIALS AND NEW ATTACHMENT HARDWARE.

**633 CABINET, TYPE TS-2, AS PER PLAN**

THE CABINET SHALL BE FURNISHED AND INSTALLED ACCORDING TO CMS 633 AND 733 AND BE LISTED ON THE TRAFFIC AUTHORIZED PRODUCTS LIST (TAP).

THE GROUND-MOUNTED CABINET SHALL BE A NEMA TS-2, TYPE 1, CABINET SIZE 7 WITH 16 LOAD SWITCH BAYS, LED UNDER-SHELF LIGHTING, POWER HARNESSSES FOR BOTH TS2 TYPE 1 AND TYPE 2 CONTROLLERS AND SHALL HAVE A MINIMUM OF THREE SHELVES.

EACH CABINET SHALL COME EQUIPPED WITH TWO 16-CHANNEL CABINET DETECTOR RACKS (CDR) INCLUDING BUS INTERFACE UNITS (BIU). THE LOOP DETECTOR TERMINATION PANEL FOR THE SECOND DETECTOR RACK SHALL BE OMITTED.

THE CABINET SHALL BE FURNISHED WITH AN EDI MMU AS ALLOWED ON THE TAP/APPROVED PRODUCTS LIST.

THE EXTERIOR OF THE CONTROLLER CABINET SHALL BE FINISHED TO MATCH THE SIGNAL SUPPORT FINISH COLOR. ALL PAINTING SHALL BE PERFORMED UNDER CONTROLLED ENVIRONMENT CONDITIONS AND IN ACCORDANCE WITH ALL MANUFACTURERS RECOMMENDATIONS PERTAINING TO SURFACE PREPARATION, MATERIAL HANDLING, AND APPLICATION. THE TOP FINISH COAT OF PAINT SHALL BE SIMILAR TO FEDERAL STANDARD PAINT #17038 (BLACK). THE CONTRACTOR SHALL PROVIDE A PAINT SAMPLE CHIP TO BE SUBMITTED WITH THE CABINET SHOP DRAWINGS FOR REVIEW AND APPROVAL. THE APPLICATION PROCEDURE SHALL GUARANTEE A FINISH THAT WILL NOT SCALE, FLAKE, OR PEEL.

PAYMENT FOR ITEM 633 CABINET, TYPE TS-2, AS PER PLAN WILL BE AT THE CONTRACT BID PRICE PER EACH COMPLETE AND IN PLACE INCLUDING ALL CONNECTIONS TESTED AND ACCEPTED.

**809 ATC V6.24 CONTROLLER, AS PER PLAN**

THE CONTROLLER UNIT SHALL BE FURNISHED AND INSTALLED PER SS 809 AND BE LISTED ON THE TRAFFIC AUTHORIZED PRODUCTS (TAP) LIST.

THE CONTROLLER SHALL BE A SIEMENS M60 SERIES AND COMPATIBLE WITH THE CABINET TYPE BEING INSTALLED.

**633 UNINTERRUPTIBLE POWER SUPPLY (UPS), 1000 WATT, AS PER PLAN**

IN ADDITION TO THE REQUIREMENTS OF C&MS 633 AND 733, POLE ATTACHMENT HARDWARE WILL BE INCLUDED FOR POLE-MOUNTED CABINETS, AND A CABINET RISER (8-INCH MINIMUM) AND ANCHOR BOLTS WILL BE PROVIDED FOR BASE-MOUNTED CABINETS. BEFORE PERFORMING THE WORK, THE CONTRACTOR, THE DISTRICT TRAFFIC ENGINEER AND THE PROJECT ENGINEER WILL PERFORM A SITE INSPECTION TO ESTABLISH THE LOCATION OF THE UPS CABINET AND FOUNDATION.

THE UPS CABINET SHALL INCLUDE A GENERATOR POWER PANEL WITH A HEAVY-DUTY POWER RELAY VERSUS THE LINE VOLTAGE GENERATOR SWITCH. THE GENERATOR INLET SHALL BE A RECESSED PANEL WITH A DOOR THAT IS FLUSH WITH THE EXTERNAL SIDE OF THE UPS CABINET. IT SHALL INCLUDE A RECESSED PLUG, AUTOMATIC TRANSFER SWITCH AND A DOOR THAT SECURELY CLOSES OVER THE POWER CORD.

THE CABINET SHALL HAVE A DOOR STOP MECHANISM AND THERMOSTATICALLY CONTROLLED FAN.

THE CABINET SHALL INCLUDE A BATTERY BALANCING DEVICE THAT REGULATES THE BATTERIES AND OPTIMIZES PERFORMANCE.

AFTER FOUR (4) HOURS OF BATTERY RUNTIME, THE SYSTEM SHALL BE PROGRAMMED TO SWITCH THE INTERSECTION FROM FULL OPERATION TO CONTROLLER AUTOMATIC FLASH OPERATION THROUGH THE MONITOR. THE CONTROLLER SHALL BE PROGRAMMED SO THAT FLASH OPERATION SHALL BEGIN ONCE THE INTERSECTION RUNS MINOR STREET GREEN (TYP. PH. 4 & 8), ALL-RED CLEARANCE, AND THEN FLASH OPERATION.

THE UPS OUTPUT NOTIFICATIONS FOR ON BATTERY, BATTERY 2-HOUR TIMER, AND LOW BATTERY SHALL BE WIRED INTO THE TRAFFIC SIGNAL CABINET BACK PANEL OR THROUGH THE CONTROLLER WITH A CII TO PROVIDE SPECIAL STATUS ALARMS FOR EACH OUTPUT INTO THE SIGNAL CONTROLLER.

THIS ITEM SHALL INCLUDE A RED LED STATUS INDICATOR LAMP TO ALLOW MAINTENANCE PERSONNEL AND LAW ENFORCEMENT TO QUICKLY ASSESS WHETHER A TRAFFIC SIGNAL CABINET IS BEING POWERED BY A UPS. THE LED HOUSING SHALL BE NEMA 4X, IP65 OR IP66, RATED FOR OUTDOOR USE AND BE TAMPER/SHATTER RESISTANT. IT SHALL BE A DOMED ENCLOSURE CONTAINING A RED LENS WITH LED THAT IS VISIBLE FROM 100 FOOT MINIMUM. THE ENCLOSURE AND LED MODULE SHOULD BE PLACED ON THE SIDE OF THE UPS CABINET FACING TOWARDS THE MAINLINE ROADWAY AND SEALED FROM WATER INTRUSION. IT SHOULD BE WIRED USING MINIMUM 20GA STRANDED, INSULATED HOOKUP WIRE TO THE STATUS RELAY OUTPUTS OF THE UPS. THE WIRES SHALL BE TERMINATED BY LUGS AT THE DISPLAY END AND PERMANENTLY LABELED "BACKUP POWER STATUS DISPLAY," WITH WIRE POLARITY INDICATED. THE RED LED SHALL ONLY ILLUMINATE TO INDICATE THE CABINET IS OPERATING UNDER UPS BACKUP POWER (THE "BACKUP" OPERATING CONDITION). THIS ITEM INCLUDES PROGRAMMING THE UPS STATUS RELAY OUTPUTS TO PRODUCE THE LAMP STATUS DISPLAYS. THESE STATUS DISPLAYS WILL BE SOLID 100% DUTY CYCLE (NOT FLASHING) DISPLAYS. THE OPERATING VOLTAGE OF THE LED LAMP SHALL BE 120V AC UNLESS OTHERWISE INDICATED.

**632 SIGNAL SUPPORT, (BY TYPE), AS PER PLAN**

IN ADDITION TO PROVISIONS OF THE ODOT C&MS, FURNISH AND INSTALL SIGNAL POLES AS SPECIFIED IN THE PLANS.

ALL SIGNAL AND POWER CABLES SHALL BE RUN INSIDE THE SIGNAL SUPPORTS. THE USE OF EXTERNAL CONDUIT RISERS FOR SIGNALS, LIGHTING OR POWER CABLING, OR ANY OTHER USE, AS SHOWN IN STANDARD DRAWING TC-83.10 SHALL BE PROHIBITED.

THE CONTRACTOR SHALL PROVIDE ALL NECESSARY ATTACHMENTS OR CONNECTIONS TO EACH SIGNAL SUPPORT POLE. ALL HOLES IN THE POLE NECESSARY TO ATTACH WIRE OR OTHERWISE USE EQUIPMENT SPECIFIED IN THESE PLANS AND QUANTITIES SHALL BE INSTALLED DURING THE MANUFACTURE OF THE POLE OR SHALL BE DRILLED, REAMED, OR HOLE SAWED BY THE CONTRACTOR. FLAME CUTTING (OXYACETYLENE OR ELECTRICAL ARC) WILL NOT BE ACCEPTED. ALL CUT EDGES OR OTHER DEFECTS IN THE ZINC COATING SHALL BE CLEANED AND COVERED WITH TWO COATS OF ZINC RICH REPAIR PAINT MATCHING THE FACTORY FINISH. PEDESTRIAN SIGNAL HEADS SHALL BE RIGIDLY MOUNTED TO THE SIGNAL SUPPORT POLES. THE CONTRACTOR SHALL NOT USE POLE CLAMPS OR BANDING TO MOUNT THE PEDESTRIAN SIGNAL HEADS. STAINLESS STEEL BANDING MAY BE USED ONLY WHERE SPECIFICALLY AUTHORIZED BY THESE PLANS AND SPECIFICATIONS OR AS DIRECTED BY THE ENGINEER. ALL BANDING, WHERE USED, SHALL BE FACTORY PAINTED TO MATCH THE SIGNAL SUPPORTS.

POLES AND ARMS SHALL BE HOT-DIPPED GALVANIZED AND COATED USING AN APPROVED THERMOSET POWDER MATERIAL PROCESS. ALL VISIBLE ELEMENTS OF THE SIGNAL MOUNTING HARDWARE, SHALL BE PROPERLY PREPARED PRIMED AND PAINTED ACCORDING TO THESE SPECIFICATIONS. ALL COATINGS SHALL BE PERFORMED UNDER CONTROLLED ENVIRONMENTAL CONDITIONS AND IN ACCORDANCE WITH ALL MANUFACTURERS RECOMMENDATIONS PERTAINING TO SURFACE PREPARATION, MATERIAL HANDLING, AND APPLICATION. THE COATING SHALL BE BLACK THERMOSET POLYESTER POWDER COAT FINISH PER FEDERAL STANDARD PAINT #17038.

THE CONTRACTOR SHALL SUBMIT DETAILED SHOP DRAWINGS TO THE ENGINEER THAT ILLUSTRATE THE HARDWARE TO BE FURNISHED AND THE CURVATURE OF THE MAST ARM TO ILLUSTRATE THE COSMETIC APPEARANCE OF THE SUPPORTS, ARMS, AND HARDWARE TO BE FURNISHED. THE SUPPORT SUPPLIER SHALL PROVIDE STRUCTURAL DESIGN DATA SIGNED AND SEALED BY AN OHIO LICENSED PROFESSIONAL ENGINEER DEMONSTRATING THAT EACH SUPPORT PROVIDED CONFORMS TO ALL APPLICABLE OHIO DEPARTMENT OF TRANSPORTATION DESIGN CRITERIA. THE CONTRACTOR SHALL ALSO SUBMIT COLOR CHIPS TO THE ENGINEER TO CONFIRM THE COLOR OF THE FINISH TO THE SIGNAL SUPPORTS. THE ENGINEER SHALL APPROVE THE SHOP DRAWINGS AND THE COLOR ON THE CHIPS BEFORE SUPPORTS ARE ORDERED.

PAYMENT FOR ITEM 632 "SIGNAL SUPPORT, (BY TYPE), AS PER PLAN" SHALL BE MADE AT THE CONTRACT UNIT PRICE PER EACH COMPLETE AND IN PLACE, AND SHALL INCLUDE ALL SIGNAL SUPPORT DESIGN, LABOR, MATERIALS, AND EQUIPMENT NECESSARY TO COMPLETE THE WORK.

**GROUNDING AND BONDING**

THE REQUIREMENTS OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS (C&MS) AND THE TC SERIES OF STANDARD CONSTRUCTION DRAWINGS ARE MODIFIED AS FOLLOWS:

1. ALL METALLIC PARTS CONTAINING ELECTRICAL CONDUCTORS SHALL BE PERMANENTLY JOINED TO FORM AN EFFECTIVE GROUND FAULT CURRENT PATH BACK TO THE GROUNDED CONDUCTOR IN THE POWER SERVICE DISCONNECT SWITCH.
  - A. PROVIDE AN EQUIPMENT GROUNDING CONDUCTOR IN METALLIC CONDUITS (725.04) IN ADDITION TO THE CONDUCTORS SPECIFIED AND BOND THE CONDUIT TO THIS GROUNDING CONDUCTOR.
  - B. WHEN AN EQUIPMENT GROUNDING CONDUCTOR IS REQUIRED IN PLASTIC CONDUIT (725.05), THE INSTALLATION SHALL INCLUDE A SEPARATE EQUIPMENT GROUNDING CONDUCTOR IN ADDITION TO THE CONDUCTORS SPECIFIED.
  - C. METALLIC CONDUIT CARRYING THE LOOP WIRES FROM IN THE PAVEMENT TO THE PULL BOX SPLICE LOCATION WILL ONLY BE BONDED AT THE PULL BOX END, AND WILL NOT CONTAIN AN EQUIPMENT GROUNDING CONDUCTOR.
  - D. IF MULTIPLE CONDUIT RUNS BEGIN AND END AT THE SAME POINTS, ONLY ONE EQUIPMENT GROUNDING CONDUCTOR IS REQUIRED.
  - E. IF AN EQUIPMENT GROUNDING CONDUCTOR IS NEEDED IN CONDUIT BETWEEN SIGNALIZED INTERSECTIONS FOR UNDERGROUND INTERCONNECT CABLE, THE GROUNDING SYSTEM FOR EACH SIGNALIZED INTERSECTION WILL BE SEPARATED ABOUT MIDWAY BETWEEN THE INTERSECTIONS.
  - F. THE MESSENGER WIRE AT SIGNALIZED INTERSECTIONS WILL BE USED AS THE CONDUCTIVE PATH FROM CORNER TO CORNER IF CONDUIT IS NOT PROVIDED UNDER THE ROADWAY. WHEN CONDUIT CONNECTS THE CORNERS OF AN INTERSECTION, AN EQUIPMENT GROUNDING CONDUCTOR SHALL BE USED IN THE CONDUIT.
2. CONDUITS.
  - A. THE 725.04 CONDUIT SHALL HAVE GROUNDING BUSHINGS INSTALLED AT ALL TERMINATION POINTS. THE BUSHING MATERIAL SHALL BE COMPATIBLE WITH GALVANIZED STEEL CONDUIT AND THE GROUNDING LUG MATERIAL SHALL BE COMPATIBLE FOR USE WITH COPPER WIRE. THREADED OR COMPRESSION TYPE BUSHINGS MAY BE USED.
  - B. THE 725.05 CONDUIT SHALL HAVE THE INSIDE AND OUTSIDE DIAMETERS OF THE CONDUIT DEBURRED AT ALL TERMINATION POINTS.
  - C. BOTH ENDS OF METALLIC CONDUIT SHALL BE BONDED TO THE EQUIPMENT GROUNDING CONDUCTOR.
  - D. METALLIC CONDUIT MAY BE BONDED TO METALLIC BOXES THROUGH THE USE OF CONDUIT FITTINGS UL APPROVED FOR THIS TYPE OF CONNECTION, WITH THE BOX BONDED TO THE EQUIPMENT GROUNDING CONDUCTOR.

**GROUNDING AND BONDING (CONTINUED)**

3. WIRE FOR GROUNDING AND BONDING.

- A. USE INSULATED, COPPER WIRE FOR THE EQUIPMENT GROUNDING CONDUCTOR. BONDING JUMPERS IN BOXES AND ENCLOSURES MAY BE BARE OR INSULATED COPPER WIRE. WIRE SIZE SHALL BE AS FOLLOWS:
    - I. USE 4 AWG BETWEEN THE POWER SERVICE AND SUPPORTS, POLES, PEDESTALS, CONTROLLER OR FLASHER CABINETS.
    - II. USE A MINIMUM 8 AWG BETWEEN LOOP DETECTOR PULL BOXES AND THE FIRST CONDUIT THAT REQUIRES A LARGER SIZE AS SPECIFIED IN 3.A.I ABOVE.
    - III. USE A MINIMUM 8 AWG BETWEEN THE "PREPARE TO STOP WHEN FLASHING" INSTALLATION (INCLUDING SUPPORT) AND THE FIRST CONDUIT THAT REQUIRES A LARGER SIZE AS SPECIFIED IN 3.A.I ABOVE.
    - IV. THE INSULATION SHALL BE GREEN OR GREEN WITH YELLOW STRIPE(S). FOR 4 AWG OR LARGER, INSULATION MAY ALSO BE BLACK WITH GREEN TAPE/LABELS INSTALLED AT ALL ACCESS POINTS.
  - B. IN A HIGHWAY LIGHTING SYSTEM, THE EQUIPMENT GROUNDING CONDUCTOR SHALL BE THE SAME WIRE SIZE AS THE DUCT CABLE OR DISTRIBUTION CABLE CIRCUIT CONDUCTORS, WITH THE MINIMUM CONDUCTOR SIZE OF 4 AWG. BONDING JUMPERS WILL BE MINIMUM SIZE 4 AWG.
4. GROUND ROD.
- A. A 3/4-INCH SCHEDULE 40 PVC CONDUIT WILL BE USED IN FOUNDATIONS AND CONCRETE WALLS FOR THE GROUNDING CONDUCTOR (GROUND WIRE) RACEWAY TO THE GROUND ROD. SHOULD METALLIC CONDUIT BE USED, BOTH ENDS OF THE CONDUIT SHALL BE BONDED TO THE GROUNDING CONDUCTOR.
  - B. THE TYPICAL GROUNDING CONDUCTOR (GROUND WIRE) SHALL BE 4 AWG INSULATED, COPPER.

5. THE GREEN CONDUCTOR IN SIGNAL CABLES (CONDUCTOR #4) SHALL NOT BE USED TO SUPPLY POWER TO A SIGNAL INDICATION. IT WILL BE CONNECTED TO THE SIGNAL BODY AS AN EQUIPMENT GROUND IN ALUMINUM HEADS AND IT WILL BE UNUSED IN PLASTIC HEADS. UNUSED CONDUCTORS SHALL BE GROUNDED IN THE CABINET. TYPICAL USE OF CONDUCTORS IS AS FOLLOWS:

COND. NO.	COLOR	VEHICLE SIGNAL	PEDESTRIAN SIGNAL
1	BLACK	GREEN BALL	#1 WALK
2	WHITE	AC NEUTRAL	AC NEUTRAL
3	RED	RED BALL	#1 DW/FDW
4	GREEN	EQUIPMENT GROUND	EQUIPMENT GROUND
5	ORANGE	YELLOW BALL	#2 DW/FDW
6	BLUE	GREEN ARROW	#2 WALK
7	WHITE/BLACK	STRIPE YELLOW ARROW	NOT USED

**GROUNDING AND BONDING (CONTINUED)**

6. POWER SERVICE AND DISCONNECT SWITCH.

- A. AT THE POWER SERVICE LOCATION, THE GROUNDING CONDUCTOR (GROUND WIRE) FROM THE DISCONNECT SWITCH NEUTRAL (AC-) BAR TO THE GROUND ROD SHALL BE A CONTINUOUS, UNSPLICED CONDUCTOR. IF SPLICED, IT SHALL BE AN EXOTHERMIC WELD BUTT SPLICE.
  - B. THE SERVICE NEUTRAL (AC-) SHALL ONLY BE CONNECTED TO GROUND AT THE PRIMARY POWER SERVICE DISCONNECT SWITCH.
    - I. NEMA CONTROLLER CABINETS: IF A POWER SERVICE DISCONNECT SWITCH IS LOCATED BEFORE THE CONTROLLER CABINET, THE NEUTRAL (AC-) AND THE GROUNDING BARS IN THE CONTROLLER CABINET SHALL NOT BE CONNECTED TOGETHER AS SHOWN IN NEMA TS-2, FIGURE 5-4.
    - II. IF SECONDARY DISCONNECT SWITCHES ARE CONNECTED AFTER THE PRIMARY DISCONNECT SWITCH, THE NEUTRAL (AC-) SHALL ONLY BE GROUNDED AT THE PRIMARY SWITCH. EQUIPMENT GROUNDING CONDUCTORS SHALL BE BROUGHT TO THE PRIMARY SWITCH, BUT SHALL BE GROUNDED AT BOTH SECONDARY AND PRIMARY SWITCHES.
7. PAYMENT - ALL MATERIALS AND WORK REQUIRED TO COMPLETE THE EFFECTIVE GROUND FAULT CURRENT PATH SYSTEM ARE INCIDENTAL TO THE CONDUCTORS INSTALLED BY CONTRACT.

**815 SPREAD SPECTRUM RADIO, AS PER PLAN**

IN ADDITION TO THE REQUIREMENTS OF SUPPLEMENTAL SPECIFICATION 815, THIS ITEM SHALL INCLUDE INSTALLING AN ANTENNA ON THE POLE AT THE SOUTHWEST CORNER OF THE INTERSECTION OF US 62 AND VILLAGE SQUARE DRIVE/WENDY'S, PROVIDING CABLE FROM THE ANTENNA TO THE PROPOSED CONTROLLER AT THAT INTERSECTION, AND ALL EQUIPMENT AND CONNECTIONS NECESSARY TO TIE THE SIGNAL FROM THIS PROJECT TO THE EXISTING SIGNAL AT US 62 AND CLARK DRIVE/KROGER.

THIS ITEM OF WORK SHALL ALSO INCLUDE INSTALLING A RADIO ANTENNA ON THE POLE AT THE SOUTHWEST CORNER OF THE INTERSECTION OF US 62 AND CLARK DRIVE/KROGER, AND ALL WIRING AND EQUIPMENT NECESSARY TO TIE INTO THE EXISTING CONTROLLER.

THE CLARK DRIVE/KROGER CONTROLLER SHALL BE PROGRAMMED AS THE MASTER AND THE VILLAGE SQUARE DRIVE/WENDY'S CONTROLLER SHALL BE PROGRAMMED AS THE SLAVE, USING THE COORDINATION TIMING PLANS PROVIDED ON SHEET 70.

PRIOR TO BEGINNING CONSTRUCTION THE CONTRACTOR SHALL PERFORM A SITE REVIEW WITH THE ENGINEER AND RADIO/CONTROLLER MANUFACTURER'S REPRESENTATIVE TO DISCUSS AND CONFIRM ANTENNA LOCATION(S), THE NEED FOR REPEATER UNITS, AND INSTALLATION REQUIREMENTS.

ALL MATERIAL, EQUIPMENT, TOOLS, WIRE, AND INCIDENTALS SHALL BE INCLUDED IN THE CONTRACT BID PRICE PER EACH ITEM 815 SPREAD SPECTRUM RADIO, AS PER PLAN, TESTED AND ACCEPTED.

TRAFFIC SIGNAL GENERAL NOTES  
US 62 & WENDY'S/VILLAGE SQUARE

LIC-62-3.64

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DESIGN AGENCY



DESIGNER	HCF
REVIEWER	JMB
PROJECT ID	110861
SHEET	TOTAL
P.66	89

**TEST HOLE PERFORMED**

IT IS ANTICIPATED THAT THE CONTRACTOR WILL ENCOUNTER UNDERGROUND UTILITIES WHILE EXCAVATING FOR SIGNAL SUPPORT FOUNDATIONS OR SIMILAR FOUNDATIONS. AFTER ACCURATELY IDENTIFYING THE PROPOSED LOCATION OF THE FOUNDATION, AS SHOWN IN THE PLANS AND AFTER MODIFYING THAT LOCATION, IF NECESSARY, BASED ON THE FIELD MARKING OF UNDERGROUND UTILITY LOCATION, THE CONTRACTOR DISCOVERS A UTILITY CONFLICT DURING THE EXCAVATION OPERATION, THE CONTRACTOR WILL BE COMPENSATED FOR EACH PARTIAL FOUNDATION EXCAVATION ACCORDING TO THE BID PRICE.

BEFORE THE CONTRACTOR BEGINS THE EXCAVATION AT THE MODIFIED LOCATION, THE CONTRACTOR SHALL VERIFY THAT THERE WILL BE NO OVERHEAD UTILITY CONFLICTS RESULTING FROM THE NEW SIGNAL SUPPORT LOCATION. NEW SUPPORT LOCATIONS ARE TO BE APPROVED BY THE ENGINEER.

THE WORK WILL INCLUDE BACKFILLING, COMPACTING, AND RESTORATION OF THE EXCAVATION TO THE SITE'S ORIGINAL CONDITION.

EXCAVATIONS SHALL NOT BE LEFT OPEN OVERNIGHT.

PAYMENT FOR THIS ITEM SHALL BE AT THE UNIT PRICE BID PER EACH ITEM 632 TEST HOLE PERFORMED TO BE USED AT THE DIRECTION OF THE ENGINEER.

**632 SIGNAL SUPPORT FOUNDATION**

PRIOR TO ORDERING THE SIGNAL SUPPORTS, THE CONTRACTOR SHALL CONTACT OUPS TO HAVE ALL THE UTILITIES LOCATED IN THE FIELD THEN MEET WITH THE PROJECT ENGINEER TO LOCATE THE PROPOSED SUPPORT LOCATIONS TO INSURE THERE ARE NO CONFLICTS WITH UTILITIES. IF THERE ARE ISSUES, THE PROJECT ENGINEER SHALL PROVIDE GUIDANCE AS TO THE RELOCATION OF THE SUPPORT POLES.

PAYMENT WILL BE AT THE CONTRACT UNIT PRICE AND WILL BE FULL COMPENSATION FOR ALL LABOR, MATERIALS, TOOLS, EQUIPMENT AND OTHER INCIDENTALS NECESSARY FOR EACH SUPPORT FURNISHED, IN PLACE, COMPLETE AND ACCEPTED.

**ITEM 632 SIGNAL SUPPORT. (BY TYPE), AS PER PLAN, ALTERNATE BID**

THE SIGNAL SUPPORT POLES AND MAST ARMS SHALL BE FLUTED USING 16-SHARP FLUTE STYLE AND SHALL INCORPORATE ALL OF THE DESIGN FEATURES, ACCESSORIES, OR REQUIREMENTS OF THE STANDARD BID ITEM. THE MAST ARMS SHALL HAVE A PRONOUNCED UPSWEEP BEGINNING AT THE ATTACHMENT END AND EXTENDING A UNIFORM DISTANCE TOWARD THE UNSUPPORTED END OF THE ARM. THE REMAINING LENGTH OF THE ARM SHALL BE STRAIGHT AND SHALL HAVE A NORMAL RISE PER ODOT'S STANDARD DRAWING TC-81.21.

THE TOP OF THE SIGNAL POLE SHALL BE CAPPED WITH AN ORNAMENTAL TOP. THE ORNAMENTAL TOP SHALL BE THE "RPBP" STYLE AS MANUFACTURED BY STERNBERG LIGHTING, 555 LAWRENCE AVE., ROSEVILLE, ILLINOIS 60172, THE "ALHAMBRA" STYLE AS MANUFACTURED BY VALMONT INDUSTRIES, INC. 58027 CHARLOTTE AVENUE, ELKHART, INDIANA 46516, OR AN APPROVED EQUAL.

THE BASE OF THE SUPPORTS SHALL BE COVERED WITH A DECORATIVE SPLIT PEDESTAL BASE HAVING INTERNAL CORROSION RESISTANT HARDWARE. THE SPLIT PEDESTAL SHALL BE THE "OXFORD 920ISS" STYLE AS MANUFACTURED BY STERNBERG LIGHTING, 555 LAWRENCE AVE., ROSEVILLE, ILLINOIS 60172, THE "HUNTINGTON" STYLE AS MANUFACTURED BY VALMONT INDUSTRIES, INC. 58027 CHARLOTTE AVENUE, ELKHART, INDIANA 46516, OR AN APPROVED EQUAL.

DESIGN AGENCY



DESIGNER  
HCF

REVIEWER  
JMB 12-01-21

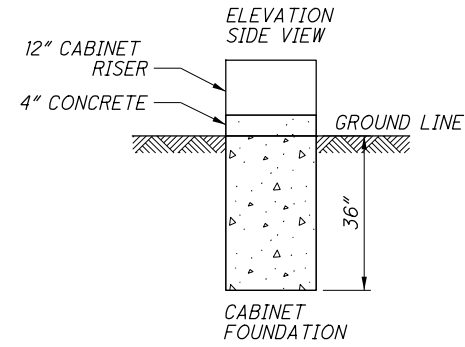
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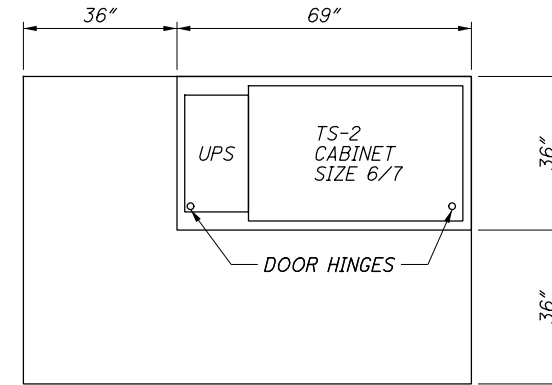
TRAFFIC SIGNAL GENERAL NOTES  
US 62 & WENDY'S/VILLAGE SQUARE

TS-2 SIZE 6/7 CABINET DETAIL

CABINET FOUNDATION DETAIL



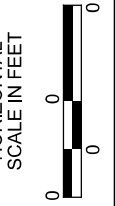
CABINET & WORK PAD DETAIL



PLAN VIEW

**NOTES:**

- 1) THE SIZE OF THE UPS FOUNDATION MAY VARY BASED ON THE CABINET SIZE PROVIDED.
- 2) UPS FOUNDATION ELEVATION SHOULD MATCH CABINET FOUNDATION ELEVATION.
- 3) THE UPS CABINET SHALL BE MOUNTED FLUSH UP AGAINST THE SIGNAL CABINET AND SEALED.
- 4) CONDUIT AND WIRING FROM THE SIGNAL CABINET TO THE UPS SHALL BE INSTALLED THROUGH THE CABINET RISER.



TRAFFIC SIGNAL GENERAL DETAILS  
US 62 & WENDY'S/VILLAGE SQUARE

DESIGN AGENCY



DESIGNER  
HCF

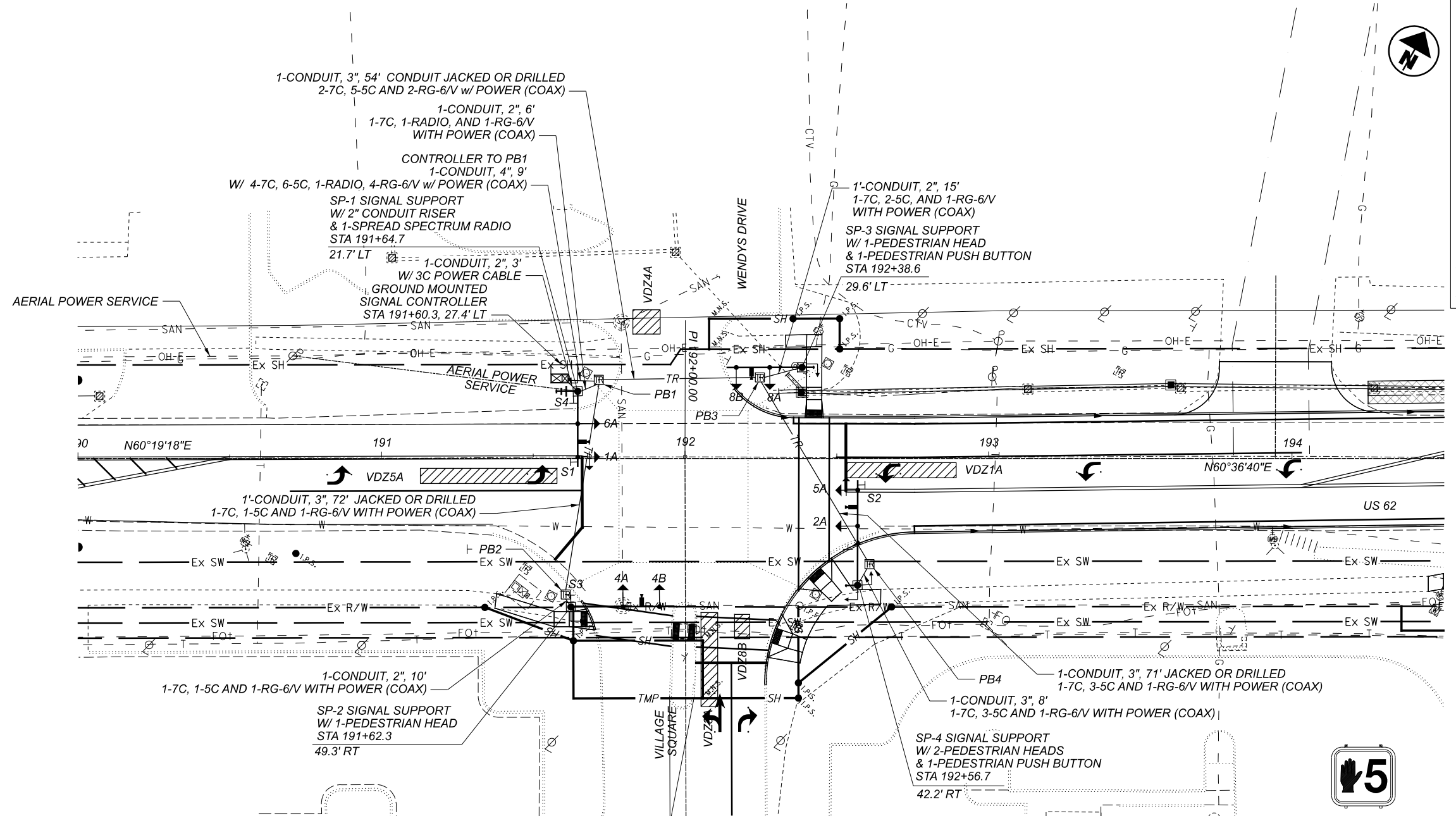
REVIEWER  
JMB 12-01-21

PROJECT ID  
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SHEET	TOTAL
P.68	89

LIC-62-3.64

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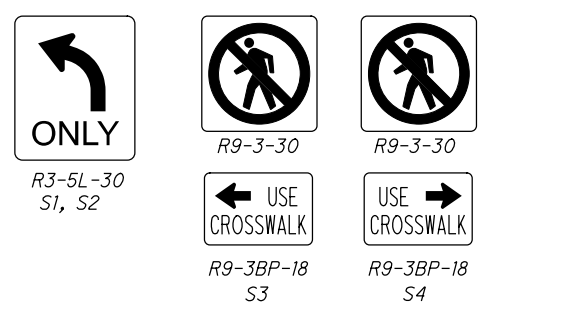
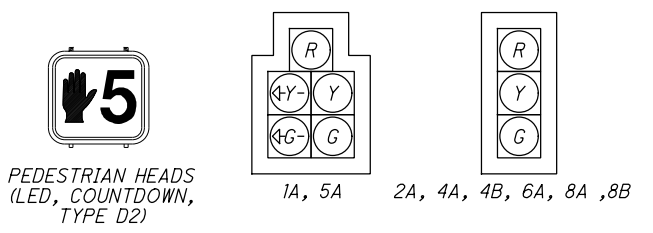


**LEGEND**

	PROP	EXIST
TRAFFIC SIGNAL, 2 UNIT, 3 UNIT, OR PHB HEAD, 12"		
TRAFFIC SIGNAL, 4 OR 5 UNIT HEAD, 12"		
SIGNAL SUPPORT POLE		
PEDESTRIAN HEAD		
PEDESTRIAN PUSH BUTTON		
CONTROLLER CABINET AND WORK PAD (TS-2)		
TRAFFIC PULL BOX		
VIDEO DETECTION CAMERA		
ETHERNET RADIO		
DETECTION ZONE		



**SIGNAL HEADS**



PULL BOX #	STATION BL CONST	SIDE	OFFSET	SIZE (IN.)
PB-1	191+71.7	Lt	25.5	24
PB-2	191+60.7	Rt	45.3	18
PB-3	192+24.7	Lt	26.2	18
PB-4	192+60.9	Rt	35.2	18

TRAFFIC SIGNAL GENERAL NOTES  
 US 62 & WENDY'S VILLAGE SQUARE

DESIGN AGENCY

DESIGNER  
 HCF

REVIEWER  
 JMB 12-01-21

PROJECT ID  
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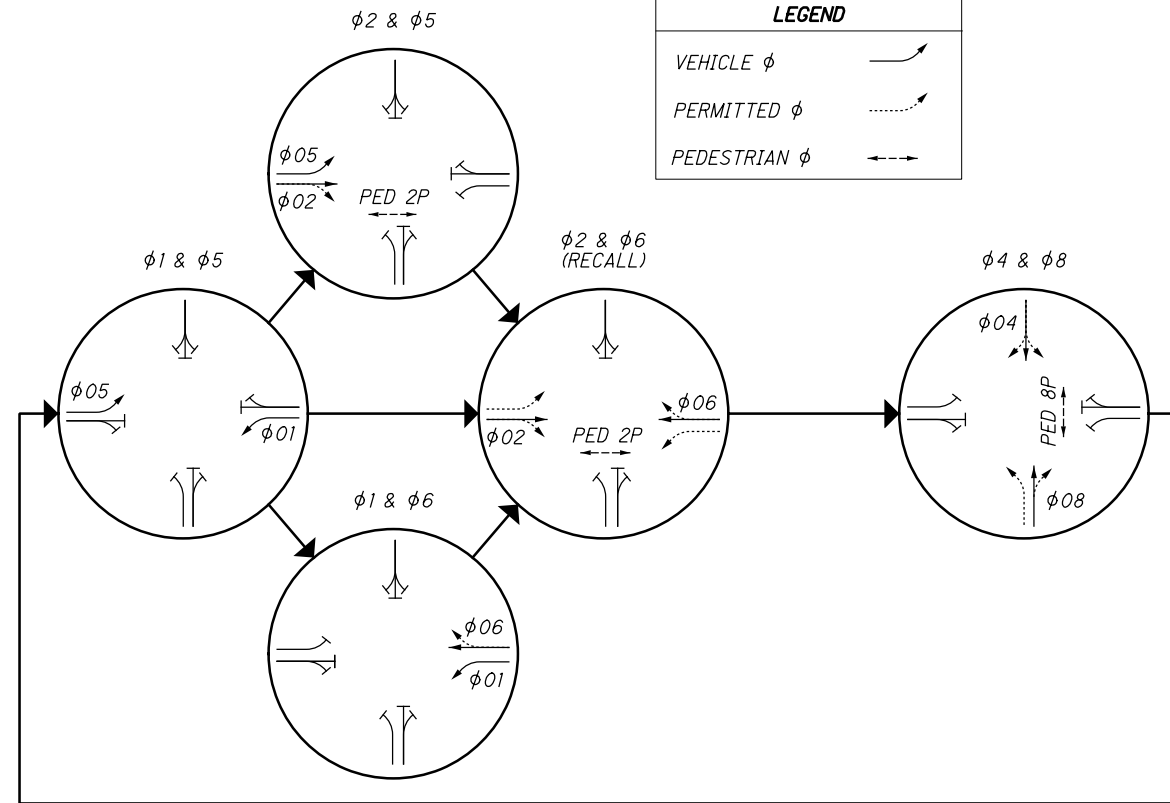
SHEET TOTAL  
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**SIGNAL TIMING CHART**

INTERSECTION: US 62 & WENDY'S/VILLAGE PARK									
MAINTAINING AGENCY: VILLAGE OF JOHNSTOWN									
START UP		DUAL ENTRY: YES		PHASES: 2+6, 4+8					
REST IN RED:		RING 1 -		RING 2 -					
OVERLAP		A		B		C		D	
PHASES		-		-		-		-	
START IN:	ALL-RED FLASH								
TIME FOR: FLASH, ALL RED (SEC.):	9, 6								
FIRST PHASE(S):	2+6								
COLOR DISPLAYED:	GREEN								
INTERVAL OR FEATURE		CONTROLLER MOVEMENT NO.							
INTERSECTION MOVEMENT (PHASE)		1	2	3	4	5	6	7	8
DIRECTION		WBLT	EB	NB	EBLT	WB	-	-	SB
MINIMUM GREEN (INITIAL) (SEC.)		7	10	-	10	7	10	-	10
ADDED INITIAL *(SEC./ACTUATION)		-	-	-	-	-	-	-	-
MAXIMUM INITIAL *(SEC.)		-	-	-	-	-	-	-	-
PASSAGE TIME (PRESET GAP) (SEC.)		3	3	-	3	3	3	-	3
TIME BEFORE REDUCTION *(SEC.)		-	-	-	-	-	-	-	-
MINIMUM GAP *(SEC.)		-	-	-	-	-	-	-	-
TIME TO REDUCE *(SEC.)		-	-	-	-	-	-	-	-
MAXIMUM GREEN I (SEC.)		15	40	-	22	15	40	-	22
MAXIMUM GREEN II (SEC.)		-	-	-	-	-	-	-	-
YELLOW CHANGE (SEC.)		3.2	4.1	-	4.4	3.2	4.1	-	4.4
ALL RED CLEARANCE (SEC.)		1.7	1	-	1	1.7	1	-	1
DELAYED GREEN (LPI) # (SEC.)		-	-	-	-	-	-	-	-
FLASHING YELLOW ARROW DELAY^ (SEC.)		-	-	-	-	-	-	-	-
WALK (SEC.)		-	7	-	7	-	-	-	-
PEDESTRIAN CLEARANCE (SEC.)		-	17	-	15	-	-	-	-
RECALL	MAXIMUM (ON/OFF)	NO	NO	-	NO	NO	NO	-	NO
	MINIMUM (ON/OFF)	NO	YES	-	NO	NO	YES	-	NO
	PEDESTRIAN (ON/OFF)	NO	YES	-	NO	NO	YES	-	NO
MEMORY (ON/OFF)	NO	NO	-	NO	NO	NO	-	NO	

\*VOLUME DENSITY CONTROLS  
 # FOR CROSSINGS WITH PEDESTRIAN PUSHBUTTONS, LPI'S (LEADING PEDESTRIAN INTERVALS) MAY BE IMPLEMENTED (3-6 SEC.) IN ACCORDANCE WITH LPI DURATION TIME PER THE ODOT SIGNAL CALCULATIONS - CLEARANCE INTERVALS SPREADSHEET  
 ^ WHEN IMPLEMENTING FYA, A MINIMUM 3 SEC. DELAY SHALL BE PROGRAMMED PER FYA PHASE.

**PHASING DIAGRAM (TYPICAL)**



**COORDINATION TIMING PLANS**

**General Notes:**

- All offsets are measured from the referenced phase END OF GREEN / BEGINNING OF YELLOW
- All offsets are measured from φ 2 & φ 6
- Shared schedules of the Week listed (ex. Mon-Fri)
- Plan Name shall denote AM Plan, PM Plan, Midday, etc.
- Hours shall be listed in military time (ex. 0600-01300)
- Plan No. or C/S/O shall be listed programmed in Controller (follow template if provided)
- Plans should be provided for each weekday for the following time periods: AM, Midday, PM, and Off-peak. If authorized, additional plans shall be developed for Weekend Peak/Off-Peaks or High Volume

DAY(S) OF WEEK	PLAN NAME	HOURS	PLAN NO. OR CYCLE/SPLIT/OFFSET	CYCLE LENGTH (SEC)
M-F	FREE	0000-0630	1	-
M-F	AM	0630-0930	2	75
M-F	FREE	0930-1530	1	-
M-F	PM	1530-1830	3	70
M-F	FREE	1830-0000	1	-
SA-SU	FREE	0000-0000	1	-
-	-	-	-	-
-	-	-	-	-
-	-	-	-	-

**COORDINATION TIMING CHART**

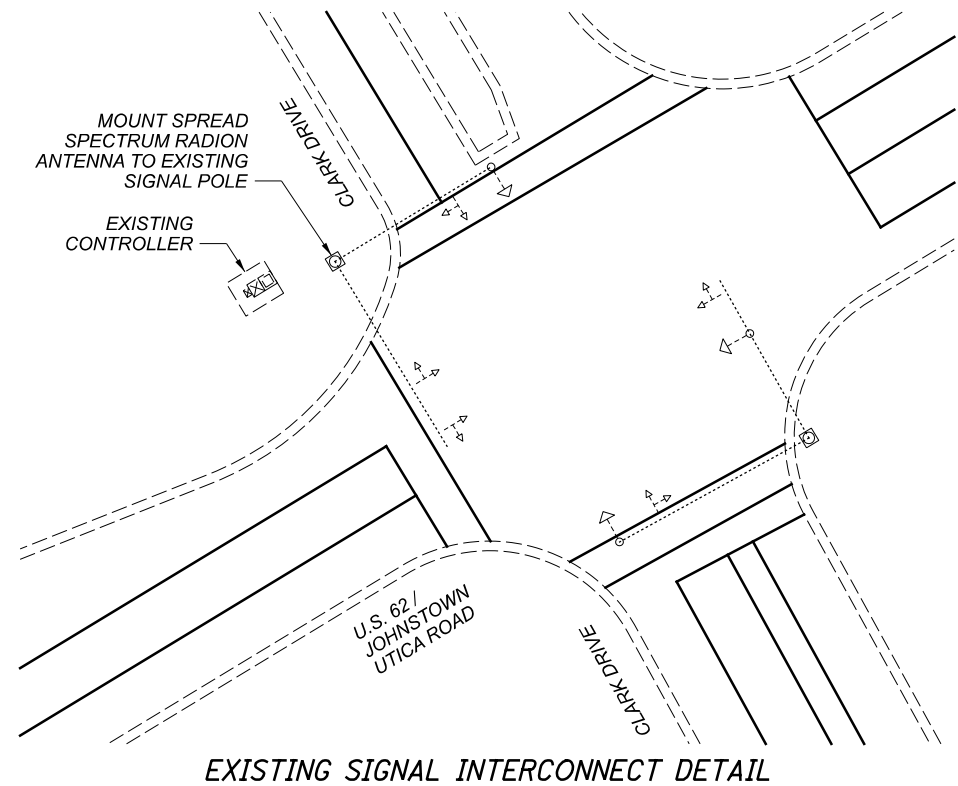
US 62 / KROGER / CLARK										
PHASE	1	2	3	4	5	6	7	8	OFFSET 1 (SEC)	OFFSET 2 (SEC)
DIRECTION	WBLT	EB	SBLT	NB	EBLT	WB	NBLT	SB		
PLAN NO. OR C/S/O	SPLITS (G+Y+AR) IN SECONDS									
2	12	27	12	24	12	27	12	24	0	-
3	12	23	12	23	12	23	12	23	0	-
-	-	-	-	-	-	-	-	-	-	-
-	-	-	-	-	-	-	-	-	-	-

US 62 / WENDY'S / VILLAGE PARK										
PHASE	1	2	3	4	5	6	7	8	OFFSET 1 (SEC)	OFFSET 2 (SEC)
DIRECTION	WBLT	EB	NB	EBLT	WB	-	-	SB		
PLAN NO. OR C/S/O	SPLITS (G+Y+AR) IN SECONDS									
2	12	40	-	23	12	40	-	23	49	-
3	12	35	-	23	12	35	-	23	44	-
-	-	-	-	-	-	-	-	-	-	-
-	-	-	-	-	-	-	-	-	-	-

**VIDEO DETECTION CHART**

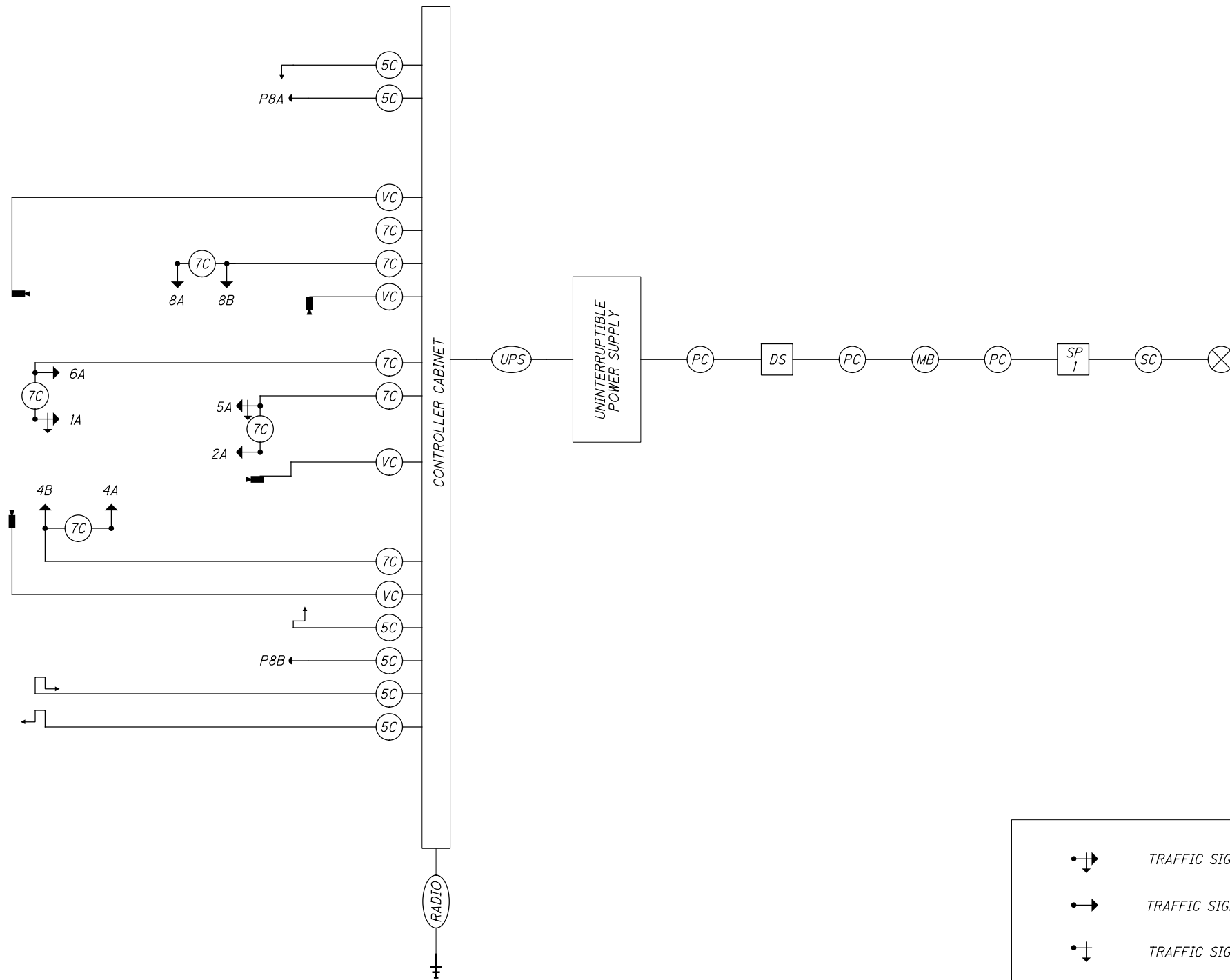
DETECTION ZONE	MOVEMENT	PULSE OR PRESENCE	ASSOCIATED PHASE	DELAY PROGRAMMED IN CONTROLLER (SEC.)	EXTENSION PROGRAMMED IN CONTROLLER (SEC.)	DELAY INHIBITED PHASE	PURPOSE	DETECTION ZONE LENGTH (FT)
VDZ2B	EB LT	PRESENCE	5	-	1.5	-	STOP LINE	20
VDZ4A	SB	PRESENCE	4	8	3	4	STOP LINE	20
VDZ1A	WB LT	PRESENCE	1	-	3	-	STOP LINE	20
VDZ3A	NB LT	PRESENCE	3	8	3	8	STOP LINE	20
VDZ8B	NB	PRESENCE	8	8	3	8	STOP LINE	20

Note: Advance/Dilemma Zone Speed Threshold: 30 MPH  
 Purpose: Stop-Line or Advance Detection



**EXISTING SIGNAL INTERCONNECT DETAIL**

**WIRING DIAGRAM (TYPICAL)**

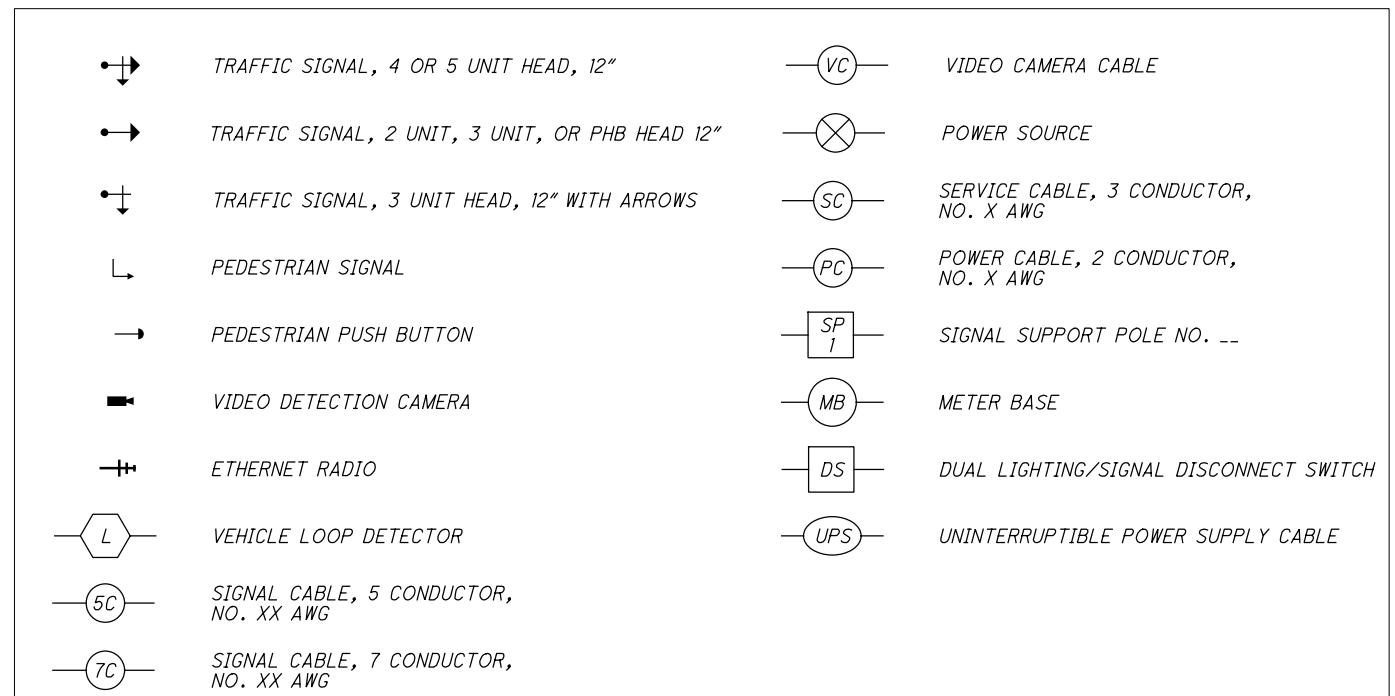


**FIELD WIRING HOOKUP DIAGRAM**

SIGNAL HEAD	INDICATION	FIELD TERMINAL	FLASH	SIGNAL HEAD	INDICATION	FIELD TERMINAL	FLASH
1A (WB LT)	R	φ 6 R	R	8A,8B (NB)	R	φ 8 R	R
	Y	φ 6 Y			Y	φ 8 Y	
	G	φ 6 G			G	φ 8 G	
	<-Y--	φ 1 Y			-	-	
	<-G--	φ 1 G					
2A (EB)	R	φ 2 R	R	-	-	-	-
	Y	φ 2 Y		-	-	-	-
	G	φ 2 G		-	-	-	-
	-	-		-	-	-	-
<b>PEDESTRIAN MOVEMENTS</b>							
4A, 4B (SB)	R	φ 4 R	R	PED A	W	φ 2 PED/ LS 2P G	OUT
	Y	φ 4 Y		-	DW	φ 2 PED/ LS 2P R	
	G	φ 4 G		-	-	-	
	-	-		-	-	-	
5A (EB LT)	R	φ 2 R	R	PED D	W	φ 8 PED/ LS 8P G	OUT
	Y	φ 2 Y		-	DW	φ 8 PED/ LS 8P R	
	G	φ 2 G		-	-	-	
	<-Y--	φ 5 Y		-	-	-	
	<-G--	φ 5 G					
<b>OVERLAPS</b>							
6A (WB)	R	φ 6 R	R	-	-	-	-
	Y	φ 6 Y		-	-	-	
	G	φ 6 G		-	-	-	

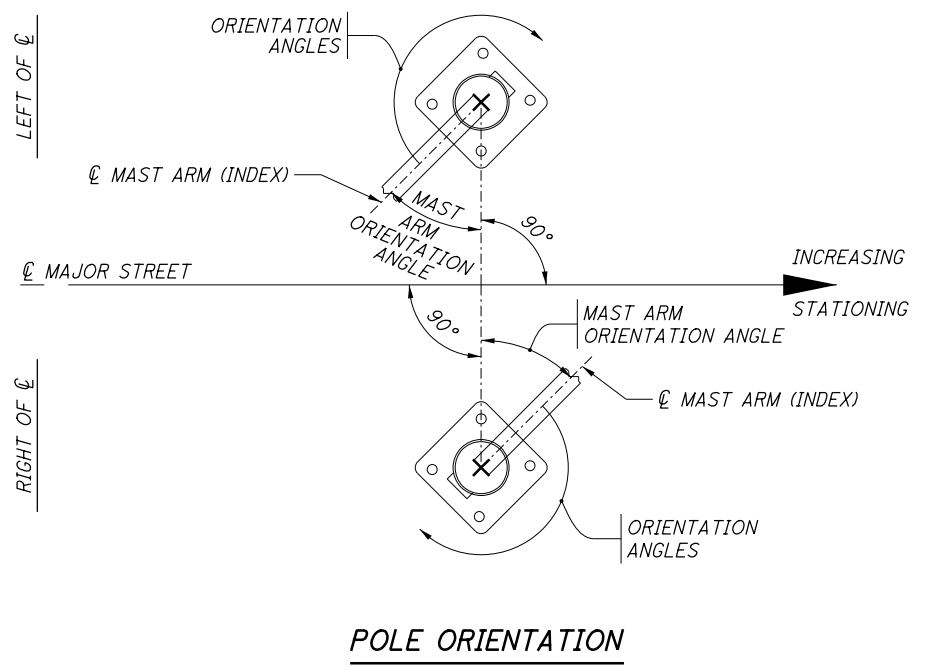
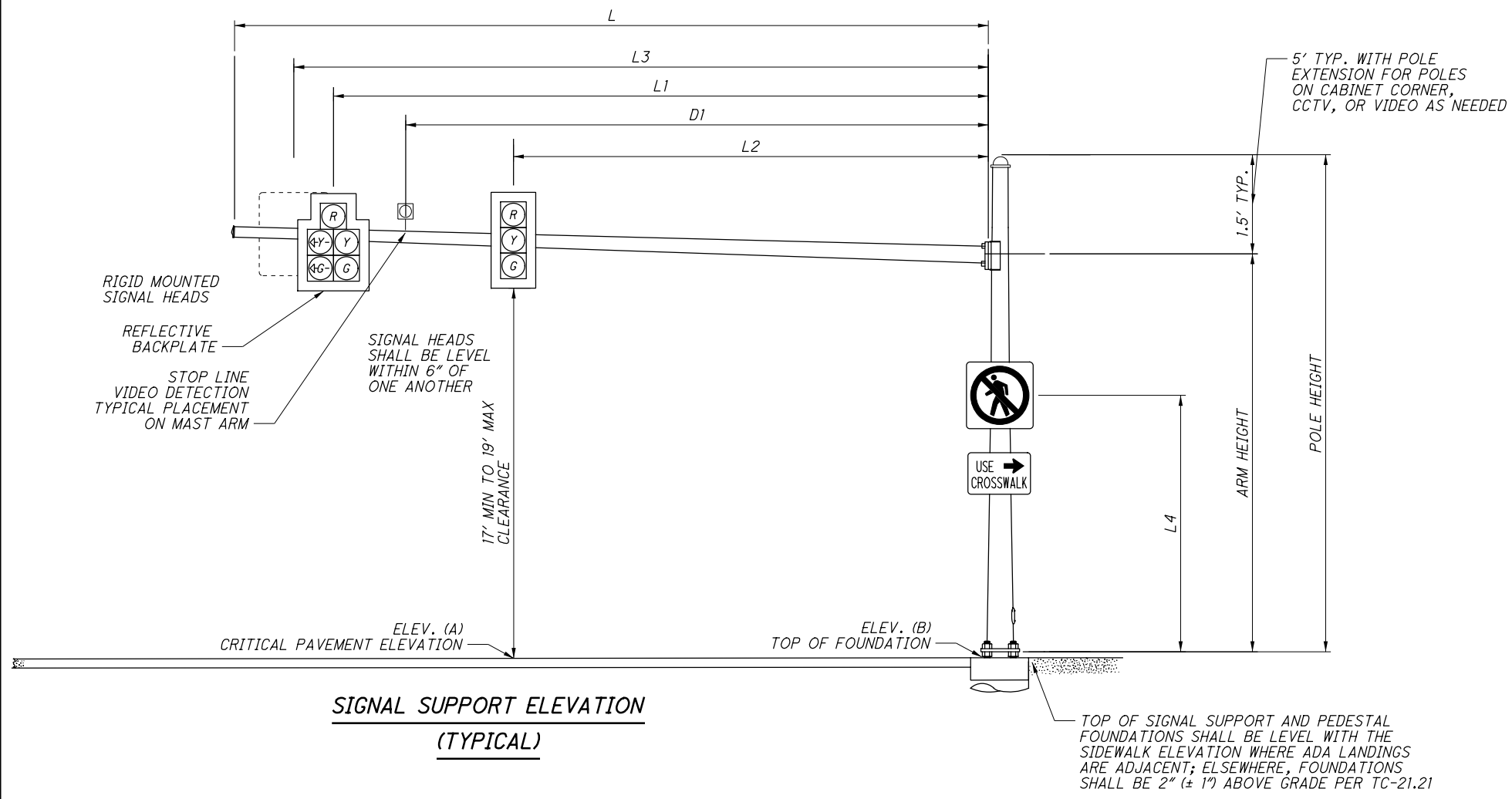
LS = LOAD SWITCH

**LEGEND**



**NOTES:**

- FOR LOCATIONS WITH LEFT TURN LANES RUN 7C FOR POTENTIAL PT/PM LT PHASE IF INITIAL DESIGN IS FOR PERMITTED ONLY.
- OVERLAPS SHALL BE WIRED TO THE APPROPRIATE LOAD SWITCHES AS PER THE FIELD HOOKUP CHART AND CONFIGURED IN THE CONTROLLER SOFTWARE PER THE SIGNAL TIMING CHART.



MAST ARM TABLE

SUPPORT NO.	STATION	OFFSET	Plan Details for Signal Supports - Arm Lengths											Plan Details for Signal Supports - Mast Arm Orientation									
			ELEVATION		SIGNAL SUPPORT DETAILS									ORIENTATION ANGLES FROM MAST ARM									
			A (Pavt. Elev.)	B (Top of Found.)	DESIGN TYPE	DESIGN NO.	POLE HEIGHT	ARM HEIGHT	L	L1	L2	L3	D1	L4	MAST ARM A ANGLE	MAST ARM B ANGLE	PEDESTRIAN SIGNAL	PEDESTRIAN PUSHBUTTON	POWER SERVICE	SIGNAL CABINET	BRACKET ARM	HANDHOLE	CABLE ENTRANCE 12" FROM TOP
FT	FT	FT	FT	FT	FT	FT	FT	FT	FT	FT	FT	DEG	DEG	DEG	DEG	DEG	DEG	DEG	DEG	DEG			
SP-1	191+64.7	21.7 LT	1114.86	1115.44	TC-81.22	2	21.00	19.5	25	21	9	23	16	10	0	-	-	90	-	-	-	180	90
SP-2	191+62.3	49.3 RT	1114.30	1114.43	TC-81.22	4	21.00	19.5	33	30	18	-	24	10	90	-	90	-	-	-	-	180	-
SP-3	192+38.6	29.6 LT	1115.25	1114.94	TC-81.22	2	21.50	20.0	25	22	11	-	16	-	90	-	180	180	-	-	-	180	-
SP-4	192+56.7	42.2 RT	1114.15	1113.53	TC-81.22	4	22.00	20.5	35	31	19	33	26	-	0	-	90/180	270	-	-	-	180	-





# RIGHT OF WAY LEGEND SHEET IMPROVEMENTS OF LIC-62-3.64

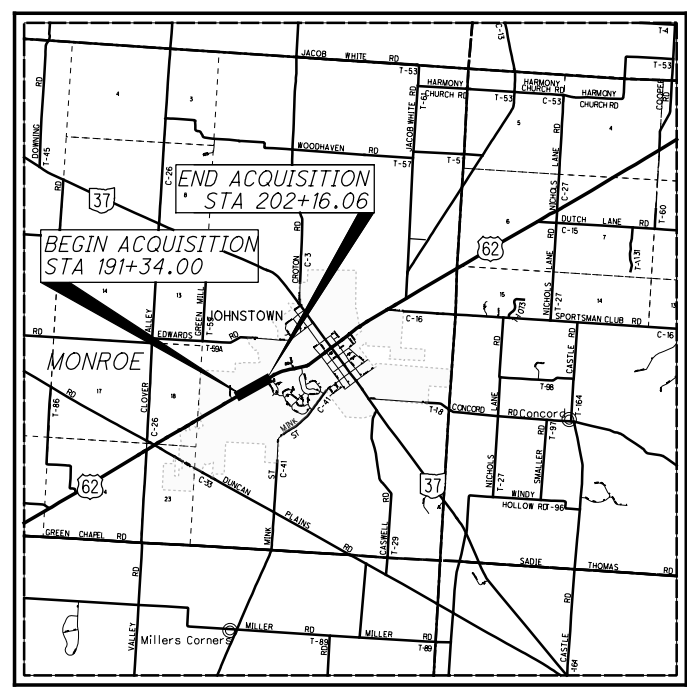
LICKING COUNTY  
MONROE TOWNSHIP  
LOT 3, QUARTER TOWNSHIP 4, T. 3N, R. 15W,  
UNITED STATES MILITARY LANDS  
CITY OF JOHNSTOWN

## PROJECT DESCRIPTION

IMPROVEMENT OF 0.3 MILES OF US 62 INCLUDING:  
WIDENING OF US 62 FOR TWO-WAY LEFT TURN LANE  
BETWEEN VILLAGE SQUARE SHOPPING CENTER TO  
WESTVIEW DRIVE. ADDING CURB AND GUTTER ALONG  
WITH UPGRADING OF DRAINAGE SYSTEM. UPGRADE OF  
SIGNAL AT VILLAGE SQUARE SHOPPING CENTER.

## PLANS PREPARED BY:

FIRM NAME: SJCA INC.  
R/W DESIGNER: GREGORY QUEBE, P.S.  
R/W REVIEWER: DAVID L. CHIESA, P.S.  
FIELD REVIEWER: GREGORY QUEBE, P.S.  
PRELIMINARY FIELD REVIEW DATE: 09-15-20  
TRACINGS FIELD REVIEW DATE: 06-17-21  
OWNERSHIP UPDATED BY: GREGORY QUEBE, P.S.  
DATE COMPLETED: 11-23-21  
PLAN COMPLETION DATE: 11-23-21



LOCATION MAP

LATITUDE: 40°08'52"N LONGITUDE: 82°42'01"W



## INDEX OF SHEETS:

LEGEND SHEET	1
CENTERLINE PLAT	2
PROPERTY MAP	3-4
SUMMARY OF ADDITIONAL R/W	5
R/W TOPOGRAPHY SHEETS	6-12 (EVEN)
R/W BOUNDARY SHEETS	7-13 (ODD)

## UTILITIES

<b>AMERICAN ELECTRIC POWER CO. (DISTRIBUTION)</b> 777 HOPEWELL DRIVE HEATH, OHIO 43056 ATTN: PAUL PAXTON 740-348-5322 PTPAXTON@AEP.COM	<b>HORIZON NETWORK PARTNERS</b> 68 EAST MAIN STREET CHILICOTHE, OH 45061 ATTN: ROGER STEELE JR 740-804-7333 ROGER.STEELEJR@HORIZONCONNECTS.COM CC: HNO@HORIZONCONNECTS.COM ATTN: JIM LUMP JIM.LUMP@HORIZONCONNECTS.COM ATTN: GARY RAY W.RAY@HORIZONCONNECTS.COM
<b>AEP SOLUTION CENTER (SERVICES AND STREET LIGHTING)</b> 1-800-277-2177	<b>SPECTRUM CABLE TV</b> PO BOX 2553 COLUMBUS, OH 43015 ATTN: JOSEPH VLOCK 614-481-5066 JOSEPH.VLOCK@CHARTER.COM
<b>CENTURY LINE TELEPHONE</b> 2025 AKRON ROAD WOOSTER, OHIO 44691 ATTN: JEFF SCHOONOVER 740-263-2819 JEFFREY.L.SCHOONOVER@LUMEN.COM	<b>LICKING RURAL ELECTRIFICATION INC.</b> 11339 MT. VERNON ROAD P.O. BOX 455 UTICA, OH 43080 ATTN: JOHN STRATHMAN 740-348-1149 JSTRATHMAN@THEENERGYCOOP.COM
<b>COLUMBIA GAS OF OHIO</b> 3550 JOHNNY APPLESEED COLUMBUS, OHIO 43231 ATTN: ERIN PFEIFER 614-381-1458 EPFEIFER@NISOURCE.COM	<b>CITY OF JOHNSTOWN</b> 395 WEST JERSEY STREET JOHNSTOWN, OHIO 43031 ATTN: JACK LIGGETT 740-967-4746 JLIGGETT@JOHNSTOWNOHIO.ORG

**NOTE:**  
THE LOCATION OF THE UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE OBTAINED FROM THE OWNER OF THE UTILITIES AS REQUIRED BY SECTION 153.64 O.R.C.

## CONVENTIONAL SYMBOLS

County Line	-----
Township Line	-----
Section Line	-----
Corporation Line	----- or -----
Fence Line (Ex)	----- (Pr) -----
Center Line	-----
Right of Way (Ex)	----- Ex R/W -----
Right of Way (Pr)	----- R/W -----
Standard Highway Ease.(Ex)	----- Ex SH -----
Standard Highway Ease.(Pr)	----- SH -----
Temporary Right of Way	----- TMP -----
Channel Ease. (Pr)	----- CH -----
Utility Ease. (Ex)	----- Ex U -----
Railroad	----- or -----
Guardrail (Ex)	----- (Pr) -----
Construction Limits	-----
Edge of Pavement (Ex)	-----
Edge of Pavement (Pr)	-----
Edge of Shoulder (Ex)	-----
Edge of Shoulder (Pr)	-----
Ditch / Creek (Ex)	-----
Ditch / Creek (Pr)	-----
Tree Line (Ex)	-----
Ownership Hook Symbol	Example -----
Property Line Symbol	Example -----
Break Line Symbol	Example -----
Tree (Pr)	-----, Shrub (Ex)
Tree (Remove)	-----, Shrub (Remove)
Evergreen (Ex)	-----, Stump
Evergreen (Remove)	-----, Stump (Remove)
Wetland (Pr)	-----, Grass (Pr)
Post (Ex)	-----, Mailbox (Pr)
Light (Ex)	-----, Telephone Marker (Ex)
Fire Hydrant (Ex)	-----, Water Meter (Ex)
Water Valve (Ex)	-----, Utility Valve Unknown (Ex.)
Telephone Pole (Ex)	-----, Power Pole (Ex)
Light Pole (Ex)	-----

## STRUCTURE KEY

	RESIDENTIAL
	COMMERCIAL
	OUT-BUILDING

- \* DENOTES RIGHT OF WAY ENCROACHMENT (SEE R/W TOPO SHEETS FOR DISPOSITION)
- X DENOTES REMOVAL ITEMS FOR TREES, SHRUBS, OR STRUCTURES (SEE R/W TOPO SHEETS FOR LOCATION AND DESCRIPTION)

TYPES OF TITLE LEGEND:  
SHV = STANDARD HIGHWAY EASEMENT  
TV = TEMPORARY EASEMENT

DISPOSITION LEGEND:  
(DND) = DO NOT DISTURB  
(TBR) = TO BE REMOVED  
(REL) = TO BE RELOCATED  
(TBA) = TO BE ADJUSTED  
(ATG) = ADJUST TO GRADE  
(RTG) = RECONSTRUCT TO GRADE

SEE ROADWAY PLANS FOR UTILITY DISPOSITIONS.

I, Gregory M. Quebe, P.S. have conducted a survey of the existing conditions for the Ohio Department of Transportation in February of 2020. The results of that survey are contained herein. The horizontal coordinates expressed herein are based on the Ohio State Plane Coordinates System, South Zone and NAD 83 (2011) datum. The Project Coordinates (US Survey Feet) are relative to State Plane Grid Coordinates (US Survey Feet) by a Project Adjustment Factor of 1.00002243. As a part of this project I have reestablished the locations of the existing property lines and centerline of existing Right of Way for property takes contained herein. All of my work contained herein was conducted in accordance with Ohio Administrative Code 4733-37 Standards for Boundary Surveys unless so noted. The words I and my as used herein are to mean that either myself or someone working under my direct supervision.

I, David L. Chiesa, P.S. have established the proposed property lines, calculated the Gross Take, present roadway occupied (PRO), Net Take and Net Residue; as well as prepared the legal descriptions necessary to acquire these parcels as shown herein. As a part of this work I oversee the setting of right of way monuments at property corners, property line intersections, points along the right of way and/or angle points on the right of way. Section Corners and other points as shown herein. All of my work contained herein was conducted in accordance with Ohio Administrative Code 4733-37 Standards for Boundary Surveys unless so noted. The words I and my as used herein are to mean that either myself or someone working under my direct supervision.

**UNDERGROUND UTILITIES**  
Contact Two Working Days Before You Dig

OHIO811, 8-1-1, or 1-800-362-2764 (Non members must be called directly)

## SURVEYORS SEAL

SIGNED: *Gregory M. Quebe*  
DATE: 11-23-21

## SURVEYORS SEAL

SIGNED: *David L. Chiesa*  
DATE: 11-23-21

RIGHT OF WAY LEGEND SHEET  
LIC-62-3.64 PID #110861

DESIGN AGENCY  
**SJCA**

DESIGNER	GMQ
REVIEWER	DLC
PROJECT ID	110861
SUBSET	1
TOTAL	13
SHEET	P.74
TOTAL	89

LIC-62-3.64

MODEL: Sheet PAPER: 11/19/2021 TIME: 3:54:45 PM USER: khorrison  
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*Gregory M. Quebe*  
Gregory M. Quebe, Professional Land Surveyor No. PS. 8747

11-23-21  
Date:

*David L. Chiesa*  
David L. Chiesa, Professional Land Surveyor No. PS. 7740

11-23-21  
Date:

MODEL: \\00861\RC0001\Sheet1 PAPER SIZE: 11x17 (in.) DATE: 11/19/2021 TIME: 3:56:48 PM USER: kharrison  
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RECEIVED \_\_\_\_\_, 20 \_\_\_\_  
RECORDED \_\_\_\_\_, 20 \_\_\_\_  
BOOK \_\_\_\_\_ PAGE \_\_\_\_\_  
COUNTY RECORDER

**MONUMENT LEGEND**

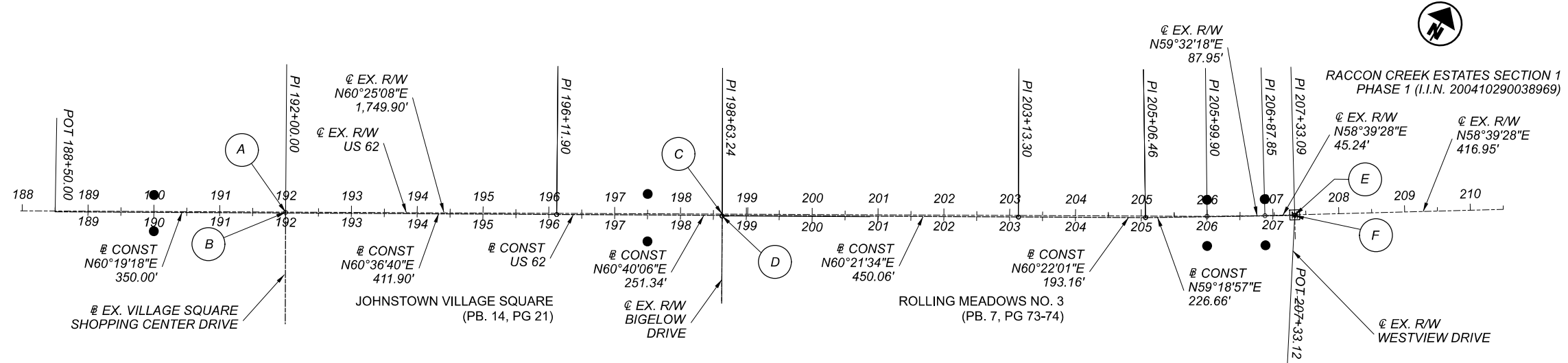
- ☒ EXISTING R/W MONUMENT BOX
- ☒ PROPOSED R/W MONUMENT BOX
- ⊙ EXISTING CONCRETE MONUMENT
- PROPOSED CONCRETE MONUMENT
- ⚡ RAILROAD SPIKE FOUND
- ⚡ RAILROAD SPIKE SET
- I.P.F. IRON PIN FOUND
- ⊙ I.P.F. IRON PIN FOUND W/ ID CAP
- I.P.S. IRON PIN SET W/ ID CAP
- ⊙ I.P.F. IRON PIPE FOUND
- ⊙ I.P.S. IRON PIPE SET
- ⊙ R.K.F. P.K. NAIL FOUND
- ⊙ R.K.S. P.K. NAIL SET
- M.N.S. MAG NAIL SET

# IMPROVEMENTS OF LIC-62-3.64

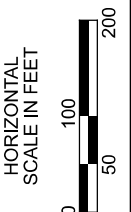
LICKING COUNTY  
MONROE TOWNSHIP  
LOT 3, QUARTER TOWNSHIP 4, T. 3N, R. 15W,  
UNITED STATES MILITARY LANDS  
CITY OF JOHNSTOWN

**PROJECT CONTROL**  
STATE PLANE GRID OHIO SOUTH ZONE, NAD 83 (2011)  
EPOCH 2010.0, TRANSLATED TO GROUND COORDINATE  
VALUES USING THE PROJECT ADJUSTMENT FACTOR  
1.00002243. ELEVATIONS ARE NAVD 1988 COMPUTED  
USING GEOID 2009.

**BASIS OF BEARINGS**  
ALL BEARINGS SHOWN ARE FOR PROJECT USE ONLY.  
BEARINGS ARE BASED ON TRUE NORTH DERIVED  
FROM GPS OBSERVATIONS USING ODOT CORS  
NETWORK STATIONS, CONVERTED TO THE  
OHIO STATE PLANE SOUTH ZONE, HORIZONTAL  
DATUM: NAD83 (2011) EPOCH 2010.0



- (A) CENTERLINE EX. R/W INTERSECTIONS:  
STA 192+00.00 @ EX. R/W COSHOCTON STREET (US 62) =  
INTERSECTION WITH @ EX. VILLAGE SQUARE SHOPPING CENTER DRIVE
- (B) STA 192+00.00 @ CONST. COSHOCTON STREET (US 62) =  
INTERSECTION WITH @ EX. VILLAGE SQUARE SHOPPING CENTER DRIVE
- (C) STA 198+63.24 @ EX. R/W COSHOCTON STREET (US 62) =  
INTERSECTION WITH @ EX. R/W BIGELOW DRIVE
- (D) STA 198+63.24 @ CONST. R/W COSHOCTON STREET (US 62) =  
INTERSECTION WITH @ EX. R/W BIGELOW DRIVE
- (E) STA 207+33.09 @ EX. R/W COSHOCTON STREET (US 62) =  
INTERSECTION WITH @ EX. R/W WESTVIEW DRIVE
- (F) STA 207+33.12 @ CONST. COSHOCTON STREET (US 62) =  
INTERSECTION WITH @ EX. R/W WESTVIEW DRIVE



**BASIS OF @ EX. R/W AND RIGHT OF WAY WIDTH:**  
THE EXISTING @ EX. R/W AND RIGHT OF WAY WIDTH WERE  
DETERMINED USING EXISTING MONUMENTATION FOUND ALONG  
THE RIGHT OF WAY AND EXISTING PLANS FOR  
LICKING\_US0062\_1.62, LICKING\_US0062\_3.27\_1988 AND  
LICKING\_US0062\_3.85\_2009 AND EXISTING DOCUMENTATION  
ON FILE FROM LICKING TOWNSHIP (SURVEYS, DEEDS, SUBDIVISION  
PLATS FOR JOHNSTOWN VILLAGE SQUARE AND ROLLING  
MEADOWS SECTION 3).

THE PROPOSED RIGHT OF WAY SHALL BE REFERENCED FROM  
THE CENTERLINE OF EXISTING RIGHT OF WAY (@ EX. R/W)

**CENTERLINE MONUMENT NOTE:**  
SETTING OF ALL MONUMENTS SHALL BE PERFORMED BY A SURVEYOR REGISTERED  
IN THE STATE OF OHIO. THE MONUMENT ASSEMBLIES AND REFERENCE MONUMENTS  
WILL BE INSTALLED BY THE CONTRACTOR AT THE TIME OF CONSTRUCTION.  
THE IRON PIN AND CAP (WHEN REQUIRED) ARE TO BE INSTALLED BY THE  
CONTRACTOR'S SURVEYOR.

CHANGES OR ALTERATIONS TO THE LOCATION OF ANY MONUMENTS SHOWN  
IN THIS TABLE, REQUIRE PRIOR APPROVAL FROM THE DISTRICT REAL ESTATE  
ADMINISTRATOR OF THE OHIO DEPARTMENT OF TRANSPORTATION. IN THE EVENT  
THAT CHANGES OR ALTERATIONS ARE APPROVED, A REVISED CENTERLINE PLAT  
WITH THE NEW LOCATIONS SHALL BE RECORDED IN THE APPLICABLE COUNTY  
RECORDS AND THE OHIO DEPARTMENT OF TRANSPORTATION. SPECIFICATIONS  
FOR MONUMENT ASSEMBLIES, REFERENCE MONUMENTS AND RIGHT OF WAY  
MONUMENTS ARE SHOWN ON STANDARD CONSTRUCTION DRAWING RM-1.1.

BASELINE CONST. US 62 MAINLINE CORRELATION WITH CENTERLINE EX. R/W US 62				
STATION (ON BASELINE CONST.)	STATION (ON CENTERLINE EX. R/W)	OFFSET (FROM CENTERLINE EX. R/W)	GROUND COORDINATES (ON BASELINE CONST.)	
			NORTH (Y)	EAST (X)
POT 188+50.00	POT 188+50.00	1.07' RT	781,852.535	1,911,617.980
POT 190+00.00	POT 190+00.00	0.81' RT	781,926.805	1,911,748.303
PI 192+00.00	POT 192+00.00	0.47' RT	782,025.834	1,911,922.071
PI 196+11.90	POT 196+11.90	1.86' RT	782,227.964	1,912,280.957
POT 197+49.99	POT 197+50.00	2.46' RT	782,295.612	1,912,401.348
PI 198+63.24	POT 198+63.23	2.95' RT	782,351.085	1,912,500.072
PI 203+13.30	POT 203+13.29	2.48' RT	782,573.667	1,912,891.242
POT 205+06.46	POT 205+06.46	2.31' RT	782,669.175	1,913,059.142
POT 205+99.93	PI 205+99.90	0.51' RT	782,716.873	1,913,139.524
POT 206+87.88	PI 206+87.85	0.17' RT	782,761.754	1,913,215.160
POT 207+33.12	PI 207+33.09	0.69' RT	782,784.840	1,913,254.066

CENTERLINE OF EXISTING RIGHT OF WAY COSHOCTON STREET (US 62)	GROUND COORDINATES	CENTERLINE MONUMENTS TO BE SET DURING CONSTRUCTION			F/W MONUMENTS TO BE SET OR EXPECTED TO BE DISTURBED	DESCRIPTION	
		ITEM 623E38500 MON. ASSY.	ITEM 623E40500 REF. MON.	ITEM 623E40520 R/W MON.			
190+00.00	25.00' LT	781,949.25	1,911,735.56	1		TYPE A RM 1.1	
190+00.00	30.00' RT	781,901.42	1,911,762.71		1	TYPE A RM 1.1	
197+50.00	30.00' LT	782,323.84	1,912,385.33		1	TYPE A RM 1.1	
197+50.00	42.00' RT	782,261.23	1,912,420.88		1	TYPE A RM 1.1	
205+99.71 BK = 206+00.09 AH	25.00' LT	782,738.96	1,913,126.76		1	TYPE A RM 1.1	
206+00.25 BK = 205+99.55 AH	45.00' RT	782,678.35	1,913,161.78		1	TYPE A RM 1.1	
206+87.66 BK = 206+88.04 AH	25.00' LT	782,783.35	1,913,202.24		1	TYPE A RM 1.1	
206+88.20 BK = 206+87.50 AH	45.00' RT	782,723.28	1,913,238.19		1	TYPE A RM 1.1	
207+33.09	0.00' RT	782,785.43	1,913,253.72	1		TYPE C RM 1.1	
192+05.79	61.01' RT	781,976.04	1,911,956.98		1	MAG NAIL SET	
192+36.37	29.43' LT	782,069.79	1,911,938.93		1	IRON PIPE FOUND	
196+00.30	35.49' LT	782,254.71	1,912,252.44		1	IRON PIN FOUND	
198+33.48	48.55' RT	782,296.74	1,912,496.72		1	IRON PIPE FOUND	
200+31.53	35.00' LT	782,467.17	1,912,627.70		1	IRON PIN SET	
TOTAL CARRIED TO GENERAL SUMMARY					1	8	5

I, Gregory M. Quebe, P.S. have conducted a survey of the existing conditions for the Ohio Department of Transportation in February of 2020. The results of that survey are contained herein. The horizontal coordinates expressed herein are based on the Ohio State Plane Coordinates System, South Zone and NAD 83 (2011) datum. The Project Coordinates (US Survey Feet) are relative to State Plane Grid Coordinates (US Survey Feet) by a Project Adjustment Factor of 1.00002243. As a part of this project I have reestablished the locations of the existing property lines and centerline of existing Right of Way for property takes contained herein. All of my work contained herein was conducted in accordance with Ohio Administrative Code 4733-37 Standards for Boundary Surveys unless so noted. The words I and my as used herein are to mean that either myself or someone working under my direct supervision.

Gregory M. Quebe, Professional Land Surveyor No. PS. 8747

11-23-21  
Date:

I, David L. Chiesa, P.S. have established the proposed property lines, calculated the Gross Take, present roadway occupied (PRO), Net Take and Net Residue; as well as prepared the legal descriptions necessary to acquire these parcels as shown herein. As a part of this work I oversaw the setting of right of way monuments at property corners, property line intersections, points along the right of way and/or angle points on the right of way. Section Corners and other points as shown herein. All of my work contained herein was conducted in accordance with Ohio Administrative Code 4733-37 Standards for Boundary Surveys unless so noted. The words I and my as used herein are to mean that either myself or someone working under my direct supervision.

David L. Chiesa, Professional Land Surveyor No. PS. 7740

11-23-21  
Date:

SURVEYORS SEAL:



SIGNED:   
DATE: 11-23-21

SURVEYORS SEAL:



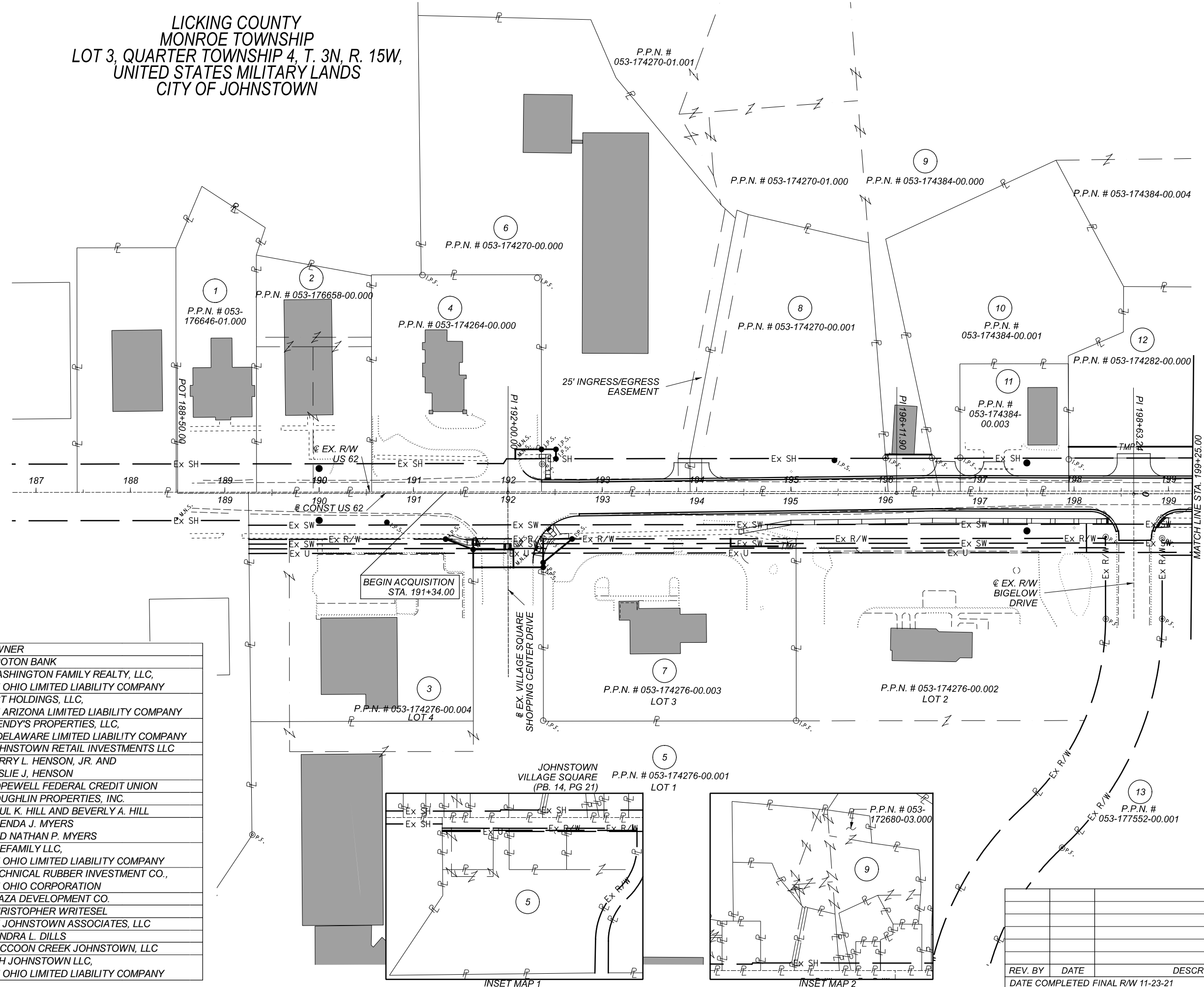
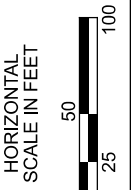
SIGNED:   
DATE: 11-23-21

CENTERLINE PLAT  
LIC-62-3.64 PID #110861

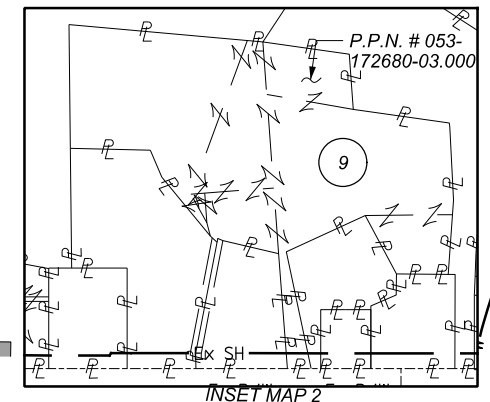
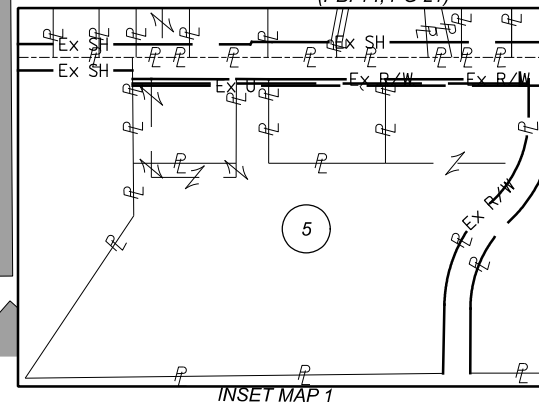
DESIGN AGENCY

DESIGNER	GMQ
REVIEWER	DLC 11-23-21
PROJECT ID	110861
SUBSET	TOTAL
2	13
SHEET	TOTAL
P.75	89

LICKING COUNTY  
 MONROE TOWNSHIP  
 LOT 3, QUARTER TOWNSHIP 4, T. 3N, R. 15W,  
 UNITED STATES MILITARY LANDS  
 CITY OF JOHNSTOWN



PARCEL NO.	OWNER
1	CROTON BANK
2	WASHINGTON FAMILY REALTY, LLC, AN OHIO LIMITED LIABILITY COMPANY
3	OST HOLDINGS, LLC, AN ARIZONA LIMITED LIABILITY COMPANY
4	WENDY'S PROPERTIES, LLC, A DELAWARE LIMITED LIABILITY COMPANY
5	JOHNSTOWN RETAIL INVESTMENTS LLC
6	TERRY L. HENSON, JR. AND LESLIE J. HENSON
7	HOPEWELL FEDERAL CREDIT UNION
8	COUGHLIN PROPERTIES, INC.
9	PAUL K. HILL AND BEVERLY A. HILL
10	BRENDA J. MYERS AND NATHAN P. MYERS
11	ACEFAMILY LLC, AN OHIO LIMITED LIABILITY COMPANY
12	TECHNICAL RUBBER INVESTMENT CO., AN OHIO CORPORATION
13	PLAZA DEVELOPMENT CO.
14	CHRISTOPHER WRITESEL
15	DK JOHNSTOWN ASSOCIATES, LLC
16	SANDRA L. DILLS
17	RACCOON CREEK JOHNSTOWN, LLC
18	CLH JOHNSTOWN LLC, AN OHIO LIMITED LIABILITY COMPANY



REV. BY	DATE	DESCRIPTION
DATE COMPLETED FINAL RW 11-23-21		

PROPERTY MAP  
 STA 187+00.00 TO STA 199+25.00

DESIGN AGENCY	SJCA
DESIGNER	GMQ
REVIEWER	DLC 11-23-21
PROJECT ID	110861
SUBSET	3
TOTAL	13
SHEET	P.76
TOTAL	89

LIC-62-3.64

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**TOTAL NUMBER OF :**

9 OWNERSHIPS 0 TOTAL TAKES  
 11 PARCELS 0 OWNERSHIPS W/ STRUCTURES INVOLVED

NET RESIDUE = RECORD AREA - TOTAL PRO - NET TAKE  
 NET TAKE = GROSS TAKE - PRO IN TAKE

**GRANTEE :**

ALL RIGHT OF WAY ACQUIRED IN THE NAME OF  
 THE CITY OF JOHNSTOWN, LICKING COUNTY, OHIO  
 UNLESS OTHERWISE SHOWN.

**ALL AREAS IN ACRES**

PARCEL NO.	OWNER	SHEET NO.	OWNERS RECORD	AUDITOR'S PARCEL	RECORD AREA	TOTAL P.R.O.	GROSS TAKE	P.R.O. IN TAKE	NET TAKE	STRUC-TURE	NET RESIDUE LEFT	NET RESIDUE RIGHT	TYPE FUND	REMARKS	AS ACQUIRED BOOK	PAGE
1	Croton Bank	6,7	UNKNOWN	053-176646-01.000	0.630	0.058							STATE	DEED REFERENCE UNKNOWN, NO TAKE		
2	Washington Family Realty, LLC, an Ohio Limited Liability Company	6,7	I.I.N. 201707210015258	053-176658-00.000	0.620	0.084								NO TAKE		
3-SHV	OST Holdings, LLC, an Arizona limited liability company	6,7	I.I.N. 201608300018608	053-174276-00.004	1.050	0.000	0.004	0.000	0.004			1.046		**LOT 4, 0.003 AC SANITARY SEWER EASEMENT OVERLAP (D.V. 820, PG. 656) 0.004 AC PLATTED EASMENT OVERLAP (INST. 2005021700004836) 1.359 SF TELEPHONE EASEMENT OVERLAP (D.R. 572, PG. 677)		
4-SHV	Wendy's Properties, LLC, a Delaware Limited Liability Company	6,7	I.I.N. 202110210032251	053-174264-00.000	0.950	0.128	0.006	0.000	0.006			0.816		*PRV SIGN (DND)		
5-SHV	Johnstown Retail Investments LLC	6,7	I.I.N. 201903150004854	053-174276-00.002 053-174276-00.001	1.440 9.650	0.376 0.000	0.000 0.032	0.000 0.000	0.000 0.032					**LOT 2, 0.009 AC SANITARY SEWER EASEMENT OVERLAP (D.V. 820, PG. 656) **LOT 1, 0.018 AC PLATTED EASMENT OVERLAP (INST. 2005021700004836) 0.005 AC TELEPHONE EASEMENT OVERLAP (D.R. 572, PG. 677)		
				TOTAL:	11.090	0.376	0.032	0.000	0.032			10.682				
5-TV		6,7		053-174276-00.001			0.019	0.000	0.019	S				TO CONSTRUCT TRAFFIC CONTROL EQUIPMENT *PRV SIGN (DND) 0.004 AC TELEPHONE EASEMENT OVERLAP (D.R. 572, PG. 677)		
6-SHV	Terry L. Henson, Jr. and Leslie J. Henson	6-9	I.I.N. 201912270028861	053-174270-00.000	2.569	0.126	0.004	0.000	0.004			2.439				
7-SHV	Hopewell Federal Credit Union	6-9	I.I.N. 201511050024113	053-174276-00.003	1.190	0.000	0.009	0.000	0.009			1.181		**LOT 3, 0.003 AC SANITARY SEWER EASEMENT OVERLAP (D.V. 820, PG. 656) 0.005 AC PLATTED EASMENT OVERLAP (INST. 2005021700004836) 0.002 AC TELEPHONE EASEMENT OVERLAP (D.R. 572, PG. 677)		
7-TV		6-9					0.013	0.000	0.013					TO GRADE, SEED AND REMOVE EXISTING SIDEWALK 0.008 AC SANITARY SEWER EASEMENT OVERLAP (D.V. 820, PG. 656) 0.013 AC PLATTED EASMENT OVERLAP (INST. 2005021700004836)		
8	Coughlin Properties, Inc.	8-9	I.I.N. 201910210022851	053-174270-00.001	1.150	0.168								NO TAKE		
9-TV	Paul K. Hill and Beverly A. Hill	8-9	I.I.N. 201507020013522	053-174384-00.000 053-174384-00.004 053-174270-01.001 053-174270-01.000 053-172680-03.000 TOTAL:	2.728 0.500 2.450 1.330 0.570 7.578	0.042 0.000 0.000 0.000 0.000 0.042	0.005 0.000 0.000 0.000 0.000 0.005	0.000 0.000 0.000 0.000 0.000 0.000	0.005 0.000 0.000 0.000 0.000 0.005					TO GRADE AND SEED		
10	Brenda J. Myers and Nathan P. Myers	8-9	I.I.N. 202008240021646	053-174384-00.001	1.010	0.021								NO TAKE		
11	Acefamily LLC, an Ohio Limited Liability Company	8-9	I.I.N. 202011190031592	053-174384-00.003	0.350	0.092								NO TAKE		
12-TV	Technical Rubber Investment Co., an Ohio Corporation	8-11	I.I.N. 201407070012330	053-174282-00.000	0.876	0.155	0.058	0.000	0.058					TO GRADE, SEED AND CONSTRUCT DRIVEWAY AND REMOVE DRIVEWAY *ROCK (TBR), EX. GATE (2) (TBR)		
13	Plaza Development Co.	10-13	I.I.N. 2002212240049408	053-177552-00.001	12.190	0.215								*PRV. SIGN (2)(DND), *FENCE (WOVEN WIRE w/WOOD POST) (DND) (123 L.F. TOTAL ENCROACHING) (DND), NO TAKE		
14-TV	Christopher Writesel	10-13	I.I.N. 202001090000629	053-172680-00.000	5.872	0.036	0.016	0.000	0.016					TO GRADE, SEED AND CONSTRUCT DRIVEWAY		
15-SHV	DK Johnstown Associates, LLC	10-13	I.I.N. 200612140035753	053-174240-00.000	0.810	0.149	0.042	0.000	0.042			0.619		*5 ROUND WOOD POST 2' HIGH POST (ENCROACHING), 9 ROUND WOOD POST 2' HIGH (TBR) *PRV. SIGN AND 1' HIGH LANDSCAPE TIMBERS (TBR)		
16	Sandra L. Dills	10-13	I.I.N. 201703290006402	053-174240-00.001 053-174240-00.002 TOTAL:	0.820 0.560 1.380	0.000 0.145 0.145								NO TAKE		
17	Raccoon Creek Johnstown, LLC	12-13	I.I.N. 200612040034840	053-173052-00.000	10.660	0.739								NO TAKE, ***PT LOTS 6,7,8 & 9		
18	CLH Johnstown LLC, an Ohio Limited Liability Company	12-13	I.I.N. 201502050002288	053-177840-00.000	0.500	0.000							STATE	NO TAKE, ****RESERVE A, * EX. FLAG POLE (DND), *EX. POST (DND), *EX. PRIVATE SIGN (DND) *EX. LANDSCAPE AREA W/ ROCKS AND MULCH (DND), *EX. LANDSCAPE LIGHT (DND) *EX. ELECTRIC BOX FOR SIGN (DND), *EX. LANDSCAPE LIGHT (DND), *EX. BUSHES (9) (DND)		

NOTE: UNDER NO CIRCUMSTANCES ARE TEMPORARY EASEMENTS TO BE USED FOR STORAGE OF MATERIAL OR EQUIPMENT BY THE CONTRACTOR UNLESS NOTED OTHERWISE.

ALL TEMPORARY PARCELS TO BE OF 12 MONTH DURATION.

(c) = CALCULATED AREA

\* DENOTES RIGHT OF WAY ENCROACHMENT

\*\* JOHNSTOWN VILLAGE SQUARE (PG 14, PG 21)  
 \*\*\* RACCOON CREEK ESTATES SECTION 1 PHASE 1 (I.I.N. 200410290008969)  
 \*\*\*\* ROLLING MEDOWS NO. 3 (PG 7, PG 73-74)

TYPES OF TITLE LEGEND:  
 SHV = STANDARD HIGHWAY EASEMENT  
 TV = TEMPORARY EASEMENT

SUMMARY OF ADDITIONAL RIGHT OF WAY

DESIGN AGENCY



DESIGNER

GMQ

REVIEWER

DLC 11-23-21

PROJECT ID

110861

SUBSET TOTAL

5 13

SHEET TOTAL

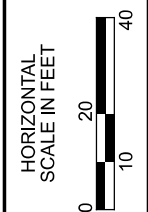
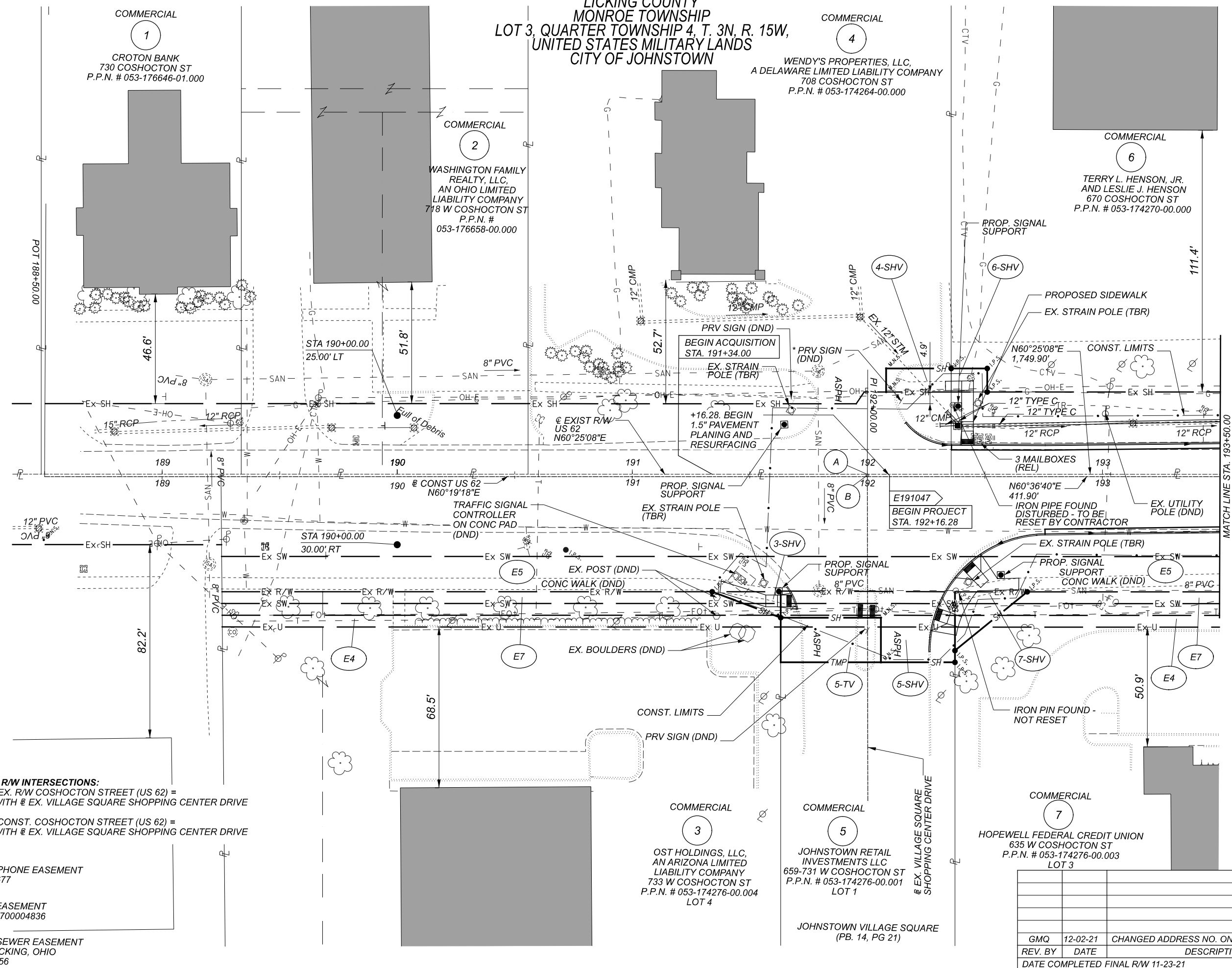
P.78 89

REV. BY	DATE	DESCRIPTION
GMQ	12-09-21	CHANGED RECORD AREA FOR PARCEL 9-TV
GMQ	12-02-21	CHANGED NET RESIDUE FOR PARCEL 15-SHV
GMQ	12-02-21	CHANGED RECORD AREA FOR PARCELS 9-TV, 12-TV, 14-TV, & 15-SHV
		FIELD REVIEW BY GREG QUEBE DATE: 06-17-21
		OWNERSHIP VERIFIED BY GREG QUEBE DATE: 11-23-21
		DATE COMPLETED FINAL R/W 11-23-21

LIC-62-3.64

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LICKING COUNTY  
 MONROE TOWNSHIP  
 LOT 3, QUARTER TOWNSHIP 4, T. 3N, R. 15W,  
 UNITED STATES MILITARY LANDS  
 CITY OF JOHNSTOWN

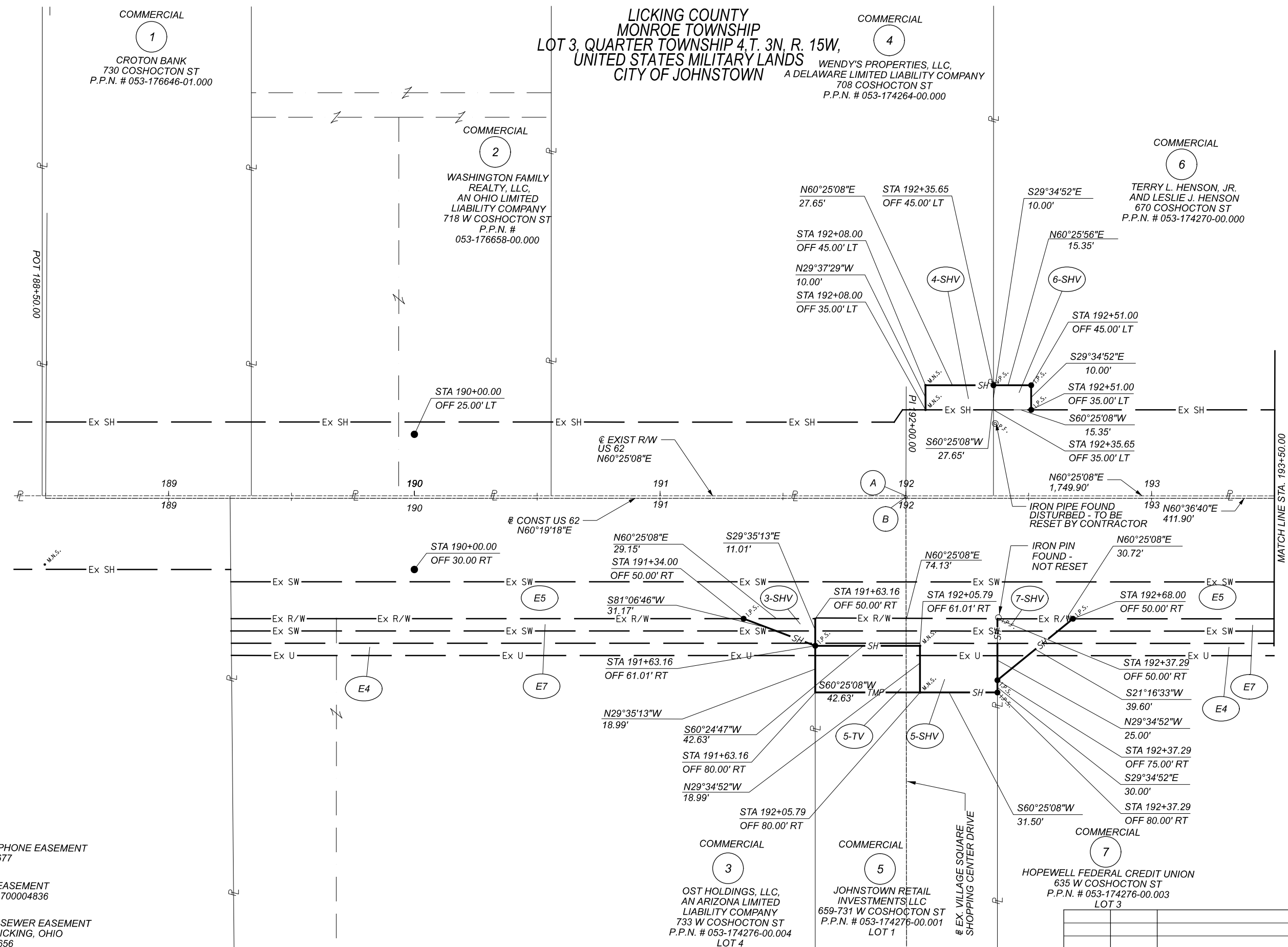


- CENTERLINE EX. R/W INTERSECTIONS:**
- (A) STA 192+00.00 @ EX. R/W COSHOCTON STREET (US 62) = INTERSECTION WITH @ EX. VILLAGE SQUARE SHOPPING CENTER DRIVE
  - (B) STA 192+00.00 @ CONST. COSHOCTON STREET (US 62) = INTERSECTION WITH @ EX. VILLAGE SQUARE SHOPPING CENTER DRIVE
  - (E4) 5' WIDE TELEPHONE EASEMENT  
O.R. 572, PG 677
  - (E7) 10' PLATTED EASEMENT  
INST. 2005021700004836
  - (E5) 20' SANITARY SEWER EASEMENT  
COUNTY OF LICKING, OHIO  
D.V. 820, PG. 656

RIGHT OF WAY TOPOGRAPHY SHEET  
 STA 188+50.00 TO STA 193+50.00

DESIGN AGENCY		SJCA	
DESIGNER		GMQ	
REVIEWER		DLC 11-23-21	
PROJECT ID		110861	
SUBSET		TOTAL	6 13
GMQ	12-02-21	CHANGED ADDRESS NO. ON PARCELS 3, 4, & 5	6 13
REV. BY	DATE	DESCRIPTION	SHEET TOTAL
			P.79 89
DATE COMPLETED FINAL RW 11-23-21			

LICKING COUNTY  
 MONROE TOWNSHIP  
 LOT 3, QUARTER TOWNSHIP 4, T. 3N, R. 15W,  
 UNITED STATES MILITARY LANDS  
 CITY OF JOHNSTOWN



- (E4) 5' WIDE TELEPHONE EASEMENT  
O.R. 572, PG 677
- (E7) 10' PLATTED EASEMENT  
INST. 2005021700004836
- (E5) 20' SANITARY SEWER EASEMENT  
COUNTY OF LICKING, OHIO  
D.V. 820, PG. 656

NOTE:  
 ALL STATIONS AND OFFSETS ARE FROM THE  $\phi$   
 OF EXIST R/W OF U.S. 62 UNLESS OTHERWISE NOTED

- CENTERLINE EX. R/W INTERSECTIONS:**
- (A) STA 192+00.00  $\phi$  EX. R/W COSHOCTON STREET (US 62) =  
INTERSECTION WITH  $\phi$  EX. VILLAGE SQUARE SHOPPING CENTER DRIVE
  - (B) STA 192+00.00  $\phi$  CONST. COSHOCTON STREET (US 62) =  
INTERSECTION WITH  $\phi$  EX. VILLAGE SQUARE SHOPPING CENTER DRIVE

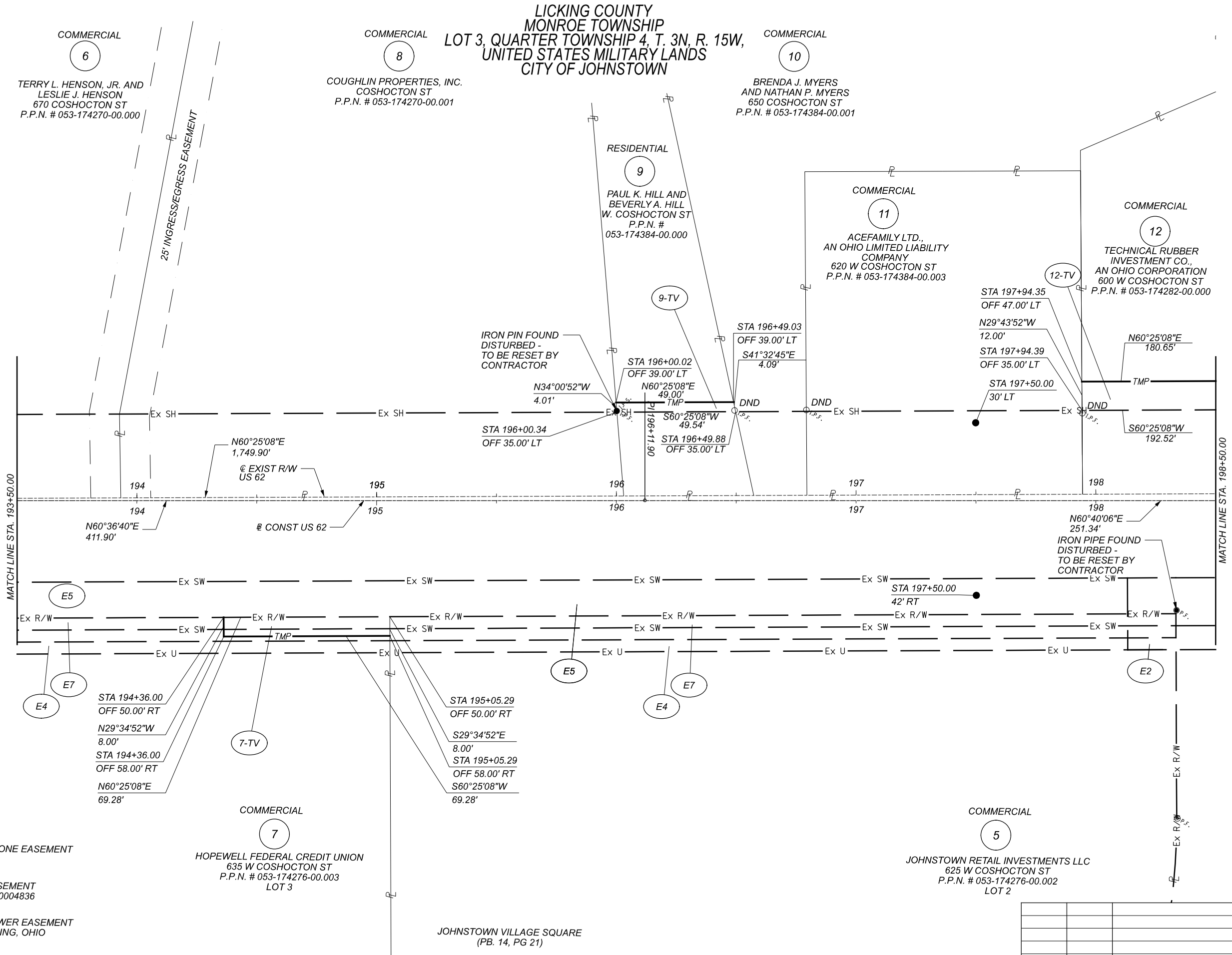
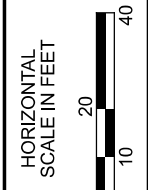
DESIGN AGENCY	SJCA	
DESIGNER	GMQ	
REVIEWER	DLC 11-23-21	
PROJECT ID	110861	
SUBSET	7	TOTAL 13
REV. BY	DATE	DESCRIPTION
GMQ	12-02-21	CHANGED ADDRESS NO. ON PARCELS 3, 4, & 5
REV. BY	DATE	DESCRIPTION
DATE COMPLETED	FINAL R/W 11-23-21	
SHEET	P.80	TOTAL 89

RIGHT OF WAY BOUNDARY SHEET  
 STA. 188+50.00 TO STA. 193+50.00





LICKING COUNTY  
 MONROE TOWNSHIP  
 LOT 3, QUARTER TOWNSHIP 4, T. 3N, R. 15W,  
 UNITED STATES MILITARY LANDS  
 CITY OF JOHNSTOWN



- E4** 5' WIDE TELEPHONE EASEMENT  
O.R. 572, PG 677
- E7** 10' PLATTED EASEMENT  
INST. 2005021700004836
- E5** 20' SANITARY SEWER EASEMENT  
COUNTY OF LICKING, OHIO  
D.V. 820, PG. 656

NOTE:  
 ALL STATIONS AND OFFSETS ARE FROM THE  $\phi$   
 OF EXIST R/W OF U.S. 62 UNLESS OTHERWISE NOTED

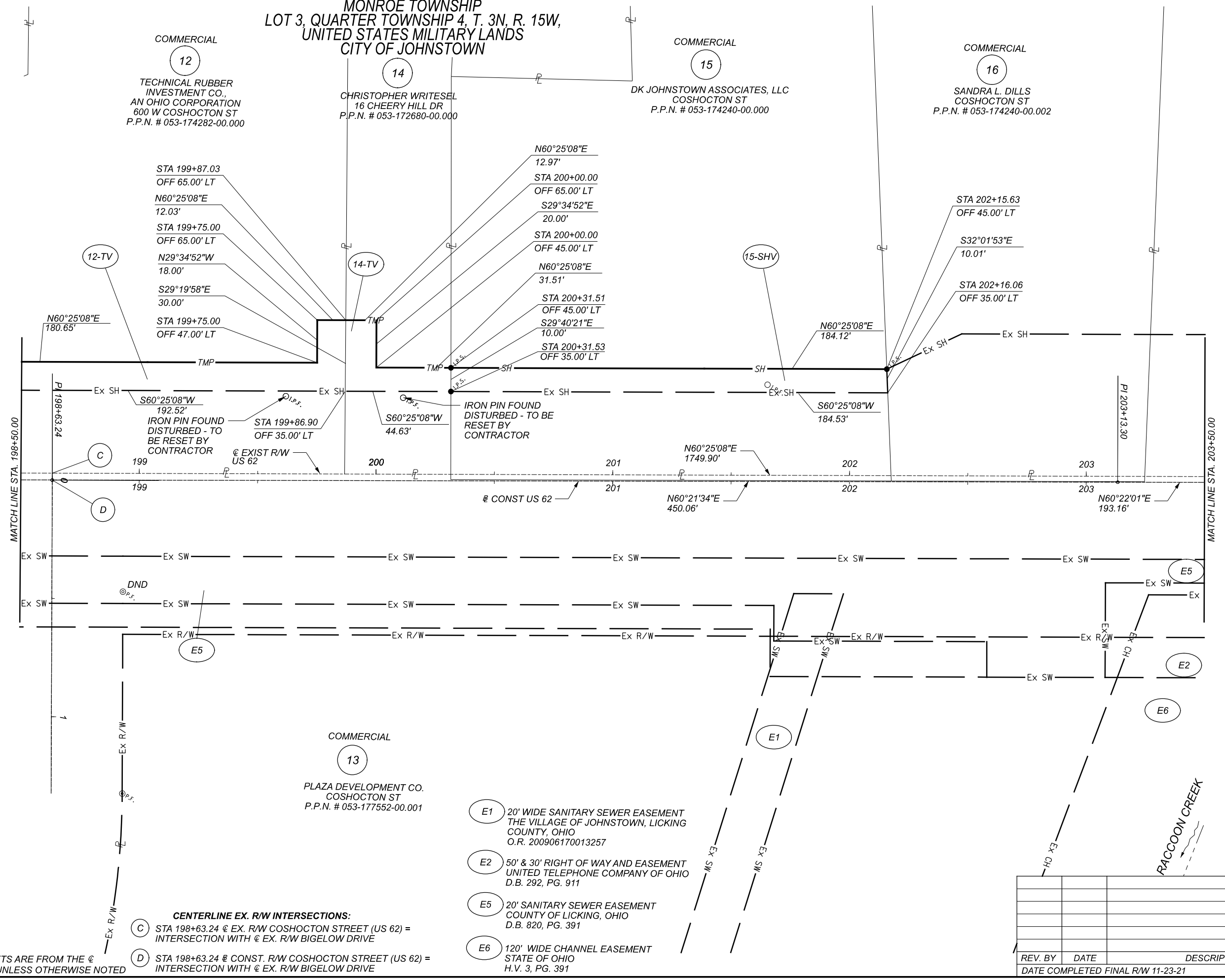
RIGHT OF WAY BOUNDARY SHEET  
 STA. 193+50.00 TO STA. 198+50.00

DESIGN AGENCY		<b>SJCA</b>	
DESIGNER		GMQ	
REVIEWER		DLC 11-23-21	
PROJECT ID		110861	
SUBSET	TOTAL	9	13
SHEET	TOTAL	P.82	89

REV. BY	DATE	DESCRIPTION
DATE COMPLETED FINAL R/W 11-23-21		



LICKING COUNTY  
MONROE TOWNSHIP  
LOT 3, QUARTER TOWNSHIP 4, T. 3N, R. 15W,  
UNITED STATES MILITARY LANDS  
CITY OF JOHNSTOWN



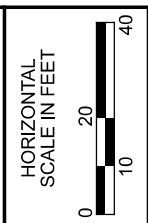
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NOTE:  
ALL STATIONS AND OFFSETS ARE FROM THE @  
OF EXIST R/W OF U.S. 62 UNLESS OTHERWISE NOTED

**CENTERLINE EX. R/W INTERSECTIONS:**  
**(C)** STA 198+63.24 @ EX. R/W COSHOCTON STREET (US 62) =  
INTERSECTION WITH @ EX. R/W BIGELOW DRIVE  
**(D)** STA 198+63.24 @ CONST. R/W COSHOCTON STREET (US 62) =  
INTERSECTION WITH @ EX. R/W BIGELOW DRIVE

- (E1)** 20' WIDE SANITARY SEWER EASEMENT  
THE VILLAGE OF JOHNSTOWN, LICKING  
COUNTY, OHIO  
O.R. 200906170013257
- (E2)** 50' & 30' RIGHT OF WAY AND EASEMENT  
UNITED TELEPHONE COMPANY OF OHIO  
D.B. 292, PG. 911
- (E5)** 20' SANITARY SEWER EASEMENT  
COUNTY OF LICKING, OHIO  
D.B. 820, PG. 391
- (E6)** 120' WIDE CHANNEL EASEMENT  
STATE OF OHIO  
H.V. 3, PG. 391



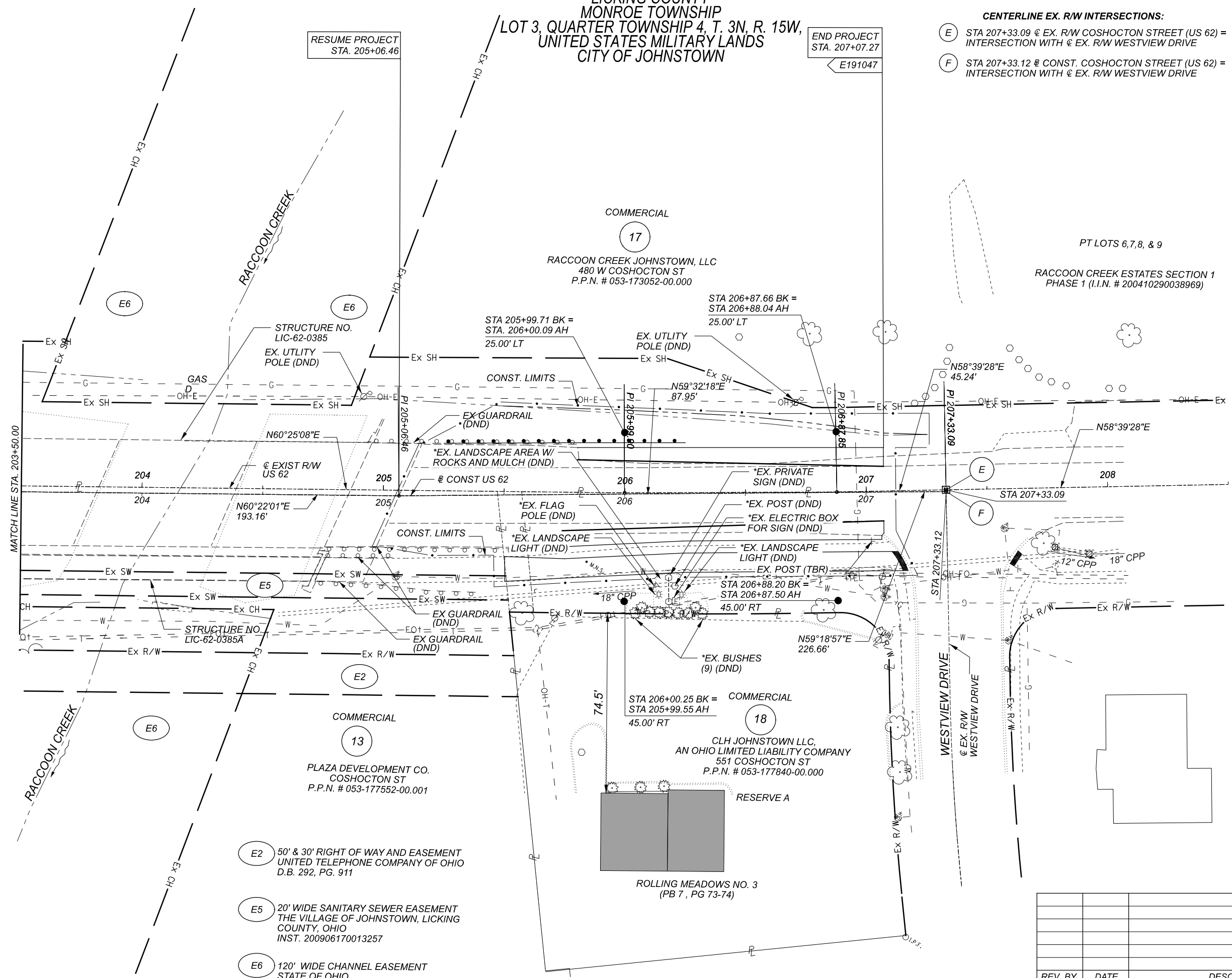
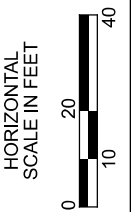
RIGHT OF WAY BOUNDARY SHEET  
STA. 198+50.00 TO STA. 203+50.00

DESIGN AGENCY		SJCA	
DESIGNER		GMQ	
REVIEWER		DLC 11-23-21	
PROJECT ID		110861	
SUBSET	TOTAL	11	13
SHEET	TOTAL	P.84	89
REV. BY	DATE	DESCRIPTION	
DATE COMPLETED FINAL R/W 11-23-21			

LICKING COUNTY  
 MONROE TOWNSHIP  
 LOT 3, QUARTER TOWNSHIP 4, T. 3N, R. 15W,  
 UNITED STATES MILITARY LANDS  
 CITY OF JOHNSTOWN

CENTERLINE EX. R/W INTERSECTIONS:

- (E) STA 207+33.09 @ EX. R/W COSHOCTON STREET (US 62) = INTERSECTION WITH @ EX. R/W WESTVIEW DRIVE
- (F) STA 207+33.12 @ CONST. COSHOCTON STREET (US 62) = INTERSECTION WITH @ EX. R/W WESTVIEW DRIVE



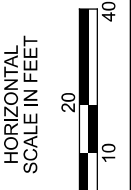
- (E2) 50' & 30' RIGHT OF WAY AND EASEMENT UNITED TELEPHONE COMPANY OF OHIO D.B. 292, PG. 911
- (E5) 20' WIDE SANITARY SEWER EASEMENT THE VILLAGE OF JOHNSTOWN, LICKING COUNTY, OHIO INST. 200906170013257
- (E6) 120' WIDE CHANNEL EASEMENT STATE OF OHIO H.V. 3, PG. 391

RIGHT OF WAY TOPOGRAPHY SHEET  
 STA 203+50.00 TO STA 208+50.00

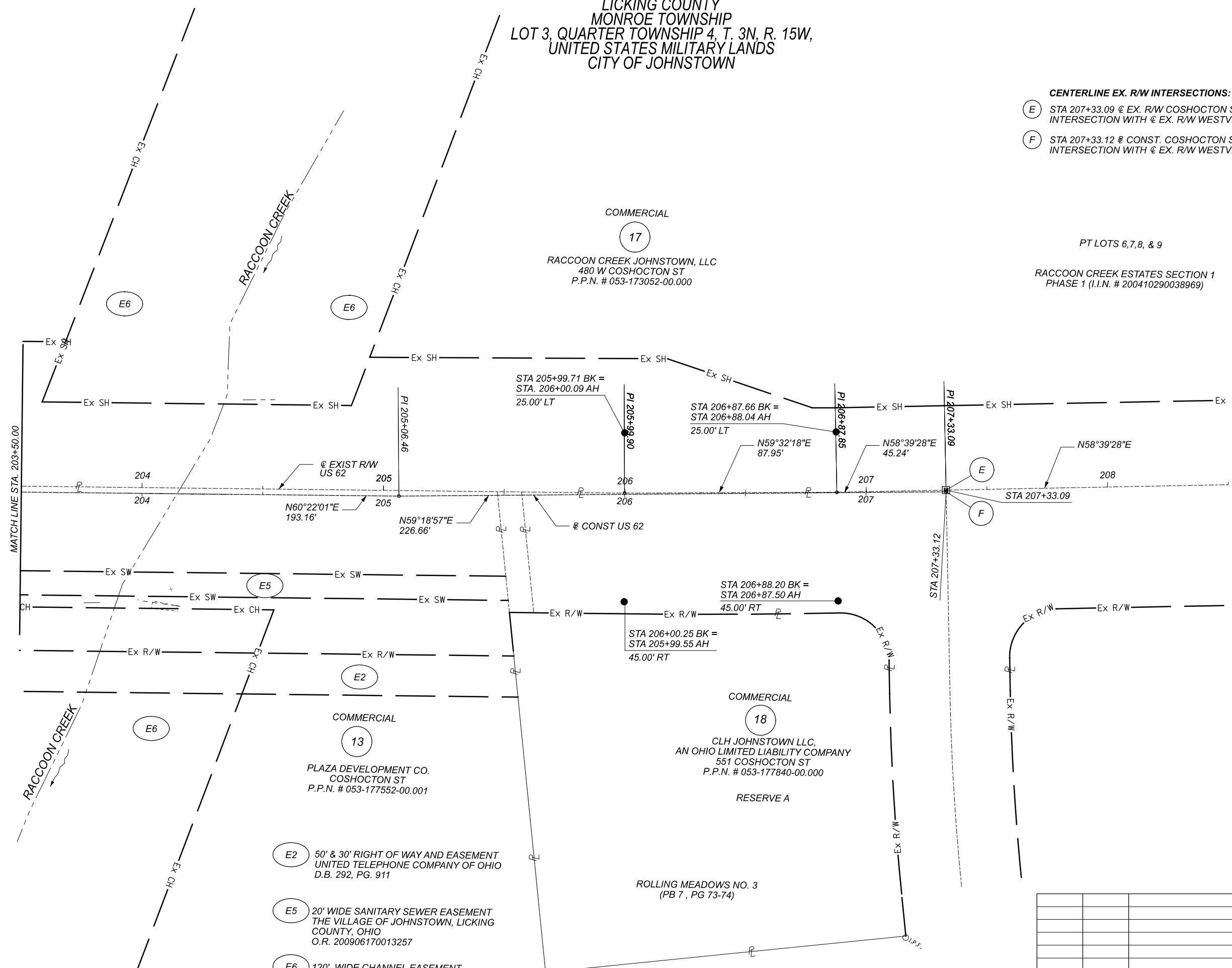
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DESIGNER		GMQ	
REVIEWER		DLC 11-23-21	
PROJECT ID		110861	
SUBSET	TOTAL	12	13
SHEET	TOTAL	P.85	89

REV. BY	DATE	DESCRIPTION
DATE COMPLETED FINAL RW 11-23-21		

LICKING COUNTY  
 MONROE TOWNSHIP  
 LOT 3, QUARTER TOWNSHIP 4, T. 3N, R. 15W,  
 UNITED STATES MILITARY LANDS  
 CITY OF JOHNSTOWN



- CENTERLINE EX. R/W INTERSECTIONS:**
- (E) STA 207+33.09 @ EX. R/W COSHOCTON STREET (US 62) = INTERSECTION WITH @ EX. R/W WESTVIEW DRIVE
  - (F) STA 207+33.12 @ CONST. COSHOCTON STREET (US 62) = INTERSECTION WITH @ EX. R/W WESTVIEW DRIVE



NOTE:  
 ALL STATIONS AND OFFSETS ARE FROM THE @  
 OF EXIST R/W OF U.S. 62 UNLESS OTHERWISE NOTED

- (E2) 50' & 30' RIGHT OF WAY AND EASEMENT UNITED TELEPHONE COMPANY OF OHIO D.B. 292, PG. 911
- (E5) 20' WIDE SANITARY SEWER EASEMENT THE VILLAGE OF JOHNSTOWN, LICKING COUNTY, OHIO O.R. 200906170013257
- (E6) 120' WIDE CHANNEL EASEMENT STATE OF OHIO H.V. 3, PG. 391

RIGHT OF WAY BOUNDARY SHEET  
 STA. 203+50.00 TO STA. 208+50.00

DESIGN AGENCY		SJCA	
DESIGNER		GMQ	
REVIEWER		DLC 11-23-21	
PROJECT ID		110861	
SUBSET	TOTAL	13	13
SHEET	TOTAL	P.86	89

REV. BY	DATE	DESCRIPTION
DATE COMPLETED FINAL R/W 11-23-21		

**PROJECT DESCRIPTION**

IT IS PROPOSED TO WIDEN US 62 FROM THE ENTRANCE DRIVE TO THE VILLAGE SQUARE SHOPPING CENTER TO JUST WEST OF THE BRIDGE CARRYING US 62 OVER RACCOON CREEK TO ACCOMMODATE A TWO-WAY LEFT TURN LANE. THE WIDENING IS TO OCCUR ALONG BOTH OUTSIDE EDGES OF THE EXISTING PAVEMENT, WITH THE EXISTING PAVEMENT BEING LEFT IN PLACE AND OVERLAIN.

**HISTORIC RECORDS**

FROM THE ODOT GEOTECHNICAL DOCUMENT MANAGEMENT SYSTEM, THE LOG OF AN EMBANKMENT BORING PERFORMED WITHIN THE CURRENT PROJECT LIMITS AS PART OF A 2008 STRUCTURE EXPLORATION AT THE BRIDGE OVER RACCOON CREEK WAS LOCATED. THIS BORING WAS LOCATED ROUGHLY 40 FEET WEST OF THE BRIDGE AND ENCOUNTERED EXISTING PAVEMENT CONSISTING OF 12 INCHES OF ASPHALT AND 6 INCHES OF GRANULAR BASE.

**GEOLOGY**

THIS SITE LIES WITHIN THE GLACIATED PORTION OF OHIO KNOWN AS THE GALION GLACIATED LOW PLATEAU PHYSIOGRAPHIC REGION. THE GALION GLACIATED LOW PLATEAU IS A ROLLING UPLAND TRANSITIONAL PLATEAU BETWEEN THE GENTLEY ROLLING TILL PLAIN AND THE HILLY GLACIATED ALLEGHENY PLATEAU, MANTLED WITH THIN TO THICK DRIFT AND CONTAINS MEDIUM- TO LOW-LIME WISCONSIAN-AGE TILL OVERLYING MISSISSIPPIAN-AGE SHALES AND SANDSTONES. ODNr BEDROCK MAPPING OF THIS SITE INDICATES BEDROCK IS TYPICALLY PRESENT 400 FEET BELOW THE GROUND SURFACE FOR THE LENGTH OF THIS PROJECT. A REVIEW OF AVAILABLE ODNr MAPPING INDICATES THIS SITE IS NOT LOCATED OVER OR NEAR ANY KNOWN UNDERGROUND MINES, IS NOT IN AN AREA OF KNOWN KARST FEATURES, OR WITHIN A PORTION OF THE STATE THAT IS SUBJECT TO SEVERE SLOPE FAILURES.

**RECONNAISSANCE**

S&ME PERSONNEL CONDUCTED RECONNAISSANCE ON MARCH 26, 2020, TO OBSERVE THE PROJECT SITE AND FIELD MARK THE PLANNED ROADWAY BORING LOCATIONS. THE EXISTING ROADWAY PAVEMENT WAS OBSERVED TO BE IN GOOD CONDITION WITH SOME LONGITUDINAL AND TRANSVERSE CRACKING NOTED. EXISTING UNDERGROUND UTILITY MARKERS WERE NOTED ON BOTH SIDES OF THE ROADWAY, AND ABOVE-GROUND UTILITIES WERE LOCATED ALONG THE NORTH/WEST SIDE OF THE ROAD. NO VISIBLE ROCK WAS EXPOSED. NO OBVIOUS VISUAL SIGNS OF SLOPE INSTABILITY ON EXISTING EMBANKMENTS WERE NOTED DURING THE VISIT.

**SUBSURFACE EXPLORATION**

ON APRIL 3, 2020, A TOTAL OF THREE (3) BORINGS WERE PERFORMED WITH AN ATV-MOUNTED DRILLING RIG USING A 4-1/2-INCH O.D. CONTINUOUS-FLIGHT AUGER TO ADVANCE BORINGS BETWEEN SAMPLING ATTEMPTS. DISTURBED BUT REPRESENTATIVE SOIL SAMPLES WERE OBTAINED BY LOWERING A 2-INCH O.D. SPLIT-BARREL SAMPLER TO THE BOTTOM OF THE BORING AND THEN DRIVING THE SAMPLER INTO THE SOIL WITH BLOWS FROM A 140-POUND HAMMER FREELY FALLING 30 INCHES (ASTM D 1586 - STANDARD PENETRATION TEST). SIX (6) FEET OF CONTINUOUS SPT SAMPLING WAS ATTEMPTED BENEATH THE APPROXIMATE PAVEMENT SUBGRADE. THE SPT HAMMER SYSTEM ON THE DRILL RIG WAS CALIBRATED ON JANUARY 21, 2019, IN ACCORDANCE WITH ASTM D 4633 TO DETERMINE THE DRILL ROD ENERGY RATIO (91.4%). AT THE COMPLETION OF DRILLING, GROUNDWATER OBSERVATIONS WERE RECORDED AND THE BORINGS WERE BACKFILLED WITH CUTTINGS MIXED WITH BENTONITE CHIPS. THE SURFACE OF THE EXISTING PAVEMENT WAS REPAIRED WITH COLD PATCH ASPHALT.

**EXPLORATION FINDINGS**

PAVEMENT CORE LOCATIONS ENCOUNTERED 10-1/4 TO 14 INCHES OF ASPHALT PAVEMENT OVER 6 TO 10 INCHES OF GRANULAR BASE. BORING B-002, DRILLED OUTSIDE THE EXISTING PAVEMENT, ENCOUNTERED 6 INCHES OF ROOTMAT AT THE GROUND SURFACE.

BENEATH THE EXISTING PAVEMENT AND SURFICIAL MATERIALS, ALL THREE BORINGS ENCOUNTERED PRIMARILY STIFF TO HARD BROWN AND BROWN MOTTLED WITH GRAY SANDY SILT (A-4a). IN BORING B-002, THE SOIL WAS DESCRIBED AS BEING GRAY OR DARK-GRAY. IN BORING B-003, THE UPPERMOST 19 INCHES OF MATERIAL WAS DESCRIBED AS POSSIBLE FILL AND DENSE, AND A FEW ZONES OF MEDIUM-STIFF SOIL WERE ENCOUNTERED BELOW 5-1/2 FEET.

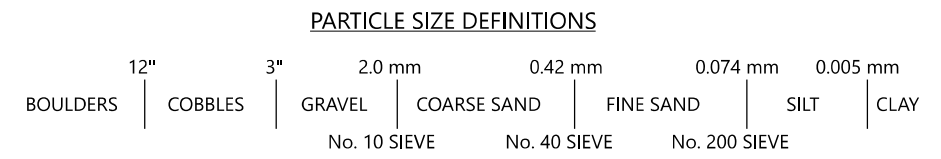
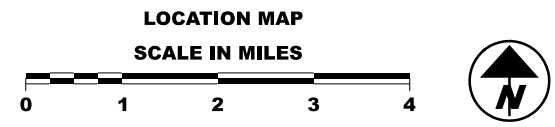
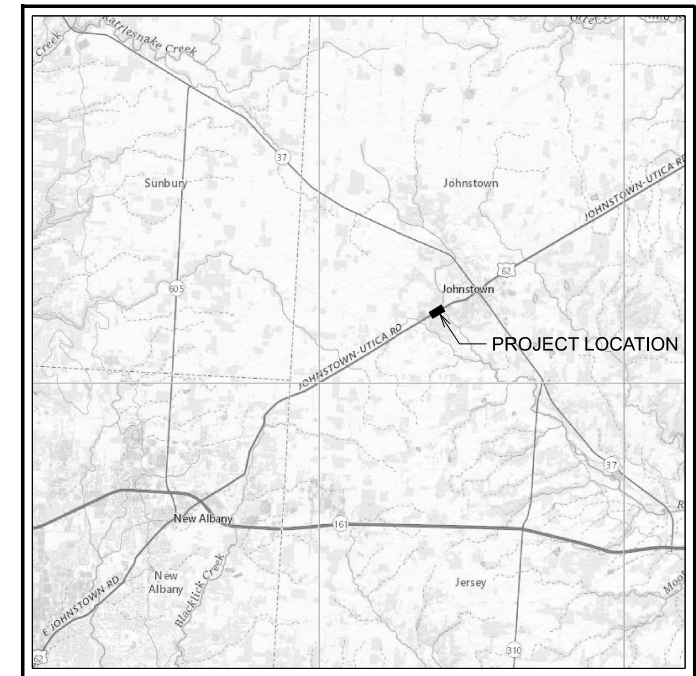
**SPECIFICATIONS**

THIS GEOTECHNICAL EXPLORATION HAS BEEN PERFORMED IN GENERAL ACCORDANCE WITH THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, OFFICE OF GEOTECHNICAL ENGINEERING, SPECIFICATIONS FOR GEOTECHNICAL EXPLORATIONS DATED JANUARY 2020.

**ADDITIONAL INFORMATION**

ALL AVAILABLE SOIL AND BEDROCK INFORMATION THAT CAN BE CONVENIENTLY SHOWN ON THESE GEOTECHNICAL EXPLORATION SHEETS HAS BEEN SO REPORTED. ADDITIONAL EXPLORATIONS MAY HAVE BEEN MADE TO STUDY SOME SPECIAL ASPECT OF THE PROJECT. COPIES OF THIS DATA, IF ANY, MAY BE INSPECTED IN THE DISTRICT DEPUTY DIRECTOR'S OFFICE OR THE OFFICE OF GEOTECHNICAL ENGINEERING AT 1980 WEST BROAD STREET, COLUMBUS, OHIO.

LEGEND		ODOT CLASS	CLASSIFIED MECH./VISUAL	
	SANDY SILT	A-4a	6	6
		TOTAL	6	6
	SOD AND TOPSOIL = X = APPROXIMATE THICKNESS	VISUAL		
	PAVEMENT OR BASE = X = APPROXIMATE THICKNESS	VISUAL		
	BORING LOCATION - PLAN VIEW.			
	DRIVE SAMPLE AND/OR ROCK CORE BORING PLOTTED TO VERTICAL SCALE ONLY. HORIZONTAL BAR INDICATES A CHANGE IN STRATIGRAPHY.			
WC	INDICATES WATER CONTENT IN PERCENT.			
N <sub>60</sub>	INDICATES STANDARD PENETRATION RESISTANCE NORMALIZED TO 60% DRILL ROD ENERGY RATIO.			
SS	INDICATES A SPLIT SPOON SAMPLE			



**INDEX OF SHEETS**

LOCATION FROM STA. TO STA.	PLAN VIEW SHEET	PROFILE SHEET	CROSS-SECTION SHEET	CUT MAX.	CUT MIN.
192+16.28 US 62 205+00.00	3	3	-	<1 FT	<1 FT

- RECON - PT (03-26-2020)
- DRILLING - S&ME (04-02-2020)
- DRAWN - DWM (09-14-2020)
- REVIEWED - RSW (09-15-2020)

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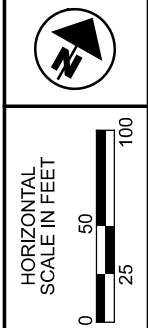
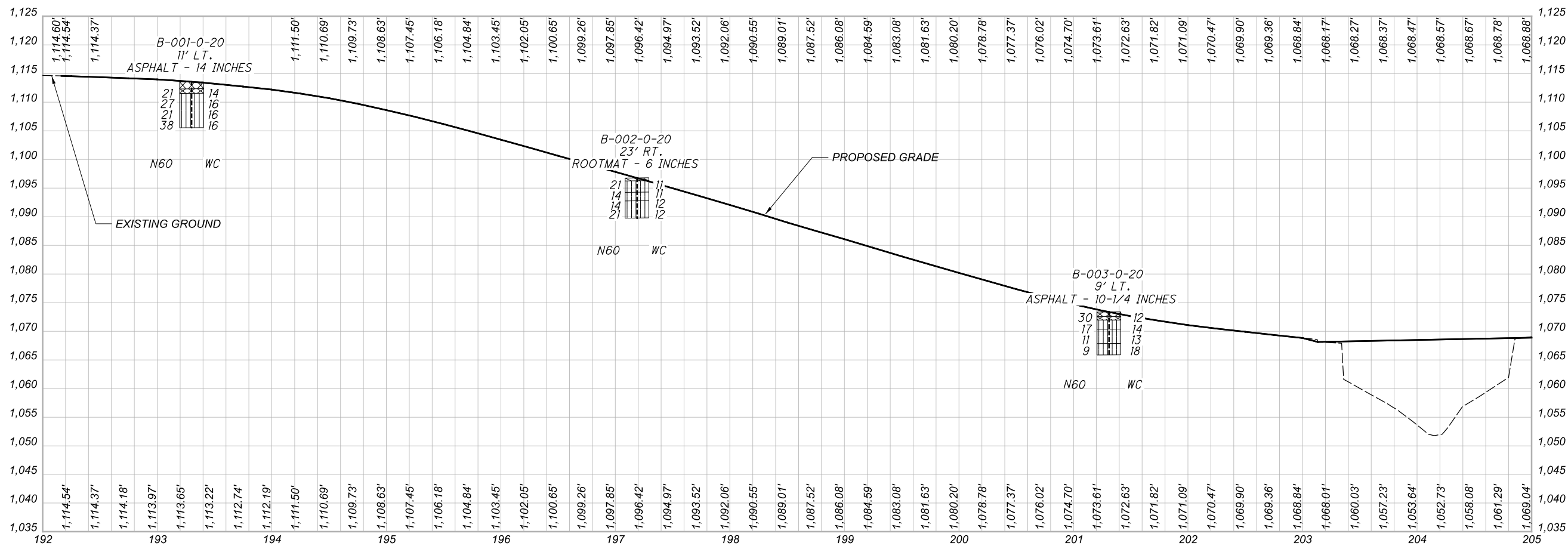
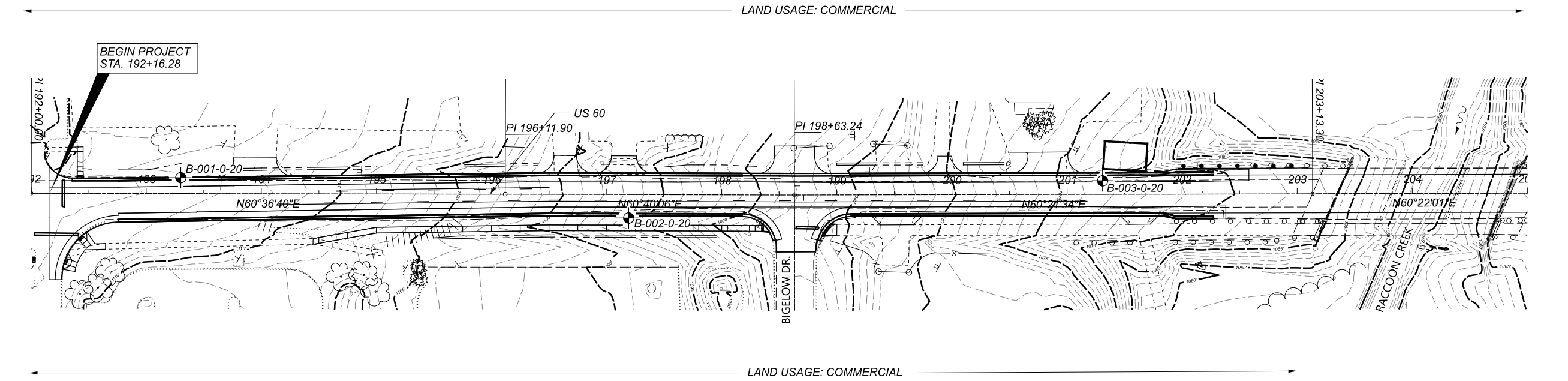
LIC-62-03.64

SOIL PROFILE LIC-62-03.64


DESIGN AGENCY	
DESIGNER	
DWM	
REVIEWER	
RSW 09-16-20	
PROJECT ID	
101861	
SUBSET	TOTAL
1	3
SHEET	TOTAL
P.87	89







PLAN AND PROFILE  
 US 62 - STA. 192+16.28 TO STA. 205+00.00

DESIGN AGENCY  
  
 DESIGNER  
 DWM  
 REVIEWER  
 RSW 09/16/20  
 PROJECT ID  
 101861  
 SHEET TOTAL  
 P.89 89