

OEPA NOTIFICATION OF DEMOLITION AND RENOVATION

AN ASBESTOS SURVEY FOR THE LIC-79-1331 BRIDGE SCHEDULED FOR DEMOLITION WORK WAS CONDUCTED BY A CERTIFIED ASBESTOS HAZARD EVALUATION SPECIALIST. A COPY OF THE ASBESTOS SURVEY REPORT FOR THE BRIDGE HAS BEEN INCLUDED IN THE PLAN PACKAGE FOR THIS PROJECT. THE ASBESTOS SURVEY REPORT DID NOT IDENTIFY THE PRESENCE OF ANY ASBESTOS CONTAINING MATERIALS.

A COPY OF THE OHIO ENVIRONMENTAL PROTECTION AGENCY (OEPA) NOTIFICATION OF DEMOLITION AND RENOVATION FORM, PARTIALLY COMPLETED BY THE ASBESTOS HAZARD EVALUATION SPECIALIST, HAS BEEN INCLUDED AT THE END OF THE ASBESTOS SURVEY REPORT. THE CONTRACTOR SHALL COMPLETE THE NECESSARY SECTIONS OF THE FORM AND SUBMIT IT WITH A COPY OF THE ASBESTOS SURVEY REPORT AND APPLICABLE NOTIFICATION FEE TO:

ASBESTOS PROGRAM
OHIO EPA, DAPC
PO BOX 1049
COLUMBUS OH 43216-1049

AT LEAST 10 WORKING DAYS PRIOR TO THE START OF ANY DEMOLITION WORK.

NOTE: OHIO EPA'S ELECTRONIC NOTIFICATION SYSTEM IS NOW ONLINE AND WILL BE MADE THE REQUIRED METHOD OF NOTIFICATION SOMETIME IN 2018. ADDITIONAL INFORMATION CAN BE FOUND HERE:
<http://epa.ohio.gov/dapc/atu/asbestos.aspx#179575188-project-notification>

BASIS FOR PAYMENT: THE CONTRACTOR SHALL FURNISH ALL FEES, LABOR, AND MATERIAL NECESSARY TO COMPLETE AND SUBMIT THE OEPA NOTIFICATION FORM. PAYMENTS FOR THIS WORK SHALL BE INCIDENTAL TO THE ITEM 202 STRUCTURE REMOVAL ITEM(S) IN THE PLAN. A MINIMUM NESHAP NOTIFICATION FEE OF \$75.00 WILL BE REQUIRED.

INSPECTION FOR BATS AND NESTING BIRDS

PRIOR TO THE START OF DEMOLITION ACTIVITIES, THE CONTRACTOR SHALL INSPECT THE UNDERSIDE OF THE BRIDGES FOR THE PRESENCE OF BATS OR NESTING BIRDS. IF ANY BATS OR BIRD NESTS ARE OBSERVED, THE CONTRACTOR SHALL NOTIFY BRIAN TATMAN @ (740) 323-5191 (BRIAN.TATMAN@DOT.OHIO.GOV) OR NICOLE HAFER-LIPSTREU @ (740) 323-5103 (NICOLE.HAFERLIPSTREU@DOT.OHIO.GOV) IN THE DISTRICT 5 PLANNING DEPARTMENT PRIOR TO STARTING ANY DEMOLITION WORK.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN, ON SITE, FOR THE DURATION OF THE PROJECT. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIAL MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCE OF 650 FEET AND 475 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPR AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE SHOP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&M 614.03.

THE PROBABLE PCMS LOCATIONS AND WORK LIMITS FOR THOSE LOCATIONS ARE SHOWN ON SHEET(S) OF THE PLAN. PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED, FACING AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

THE CONTRACTOR SHALL IMPLEMENT A SYSTEM WHEREBY CHANGEABLE MESSAGES WILL BE IMPLEMENT WITHIN 2 HOURS FOLLOWING TELEPHONE NOTIFICATION FROM THE PROJECT ENGINEER TO A DESIGNATED PHONE.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&M 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE-DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE-DESCRIBED WORK.

A TOTAL OF 2 PCMS WILL BE REQUIRED FOR THIS PROJECT.

ITEM 614, MAINTAINING TRAFFIC

THROUGH TRAFFIC SHALL BE MAINTAINED AT ALL 7/59 THRU 17/59. ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH ITEM 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT, MATERIALS AND TOOLS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLANS.

ITEM 614, WORK ZONE IMPACT ATTENUATOR (UNIDIRECTIONAL OR BIDIRECTIONAL)

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING A NON-GATING IMPACT ATTENUATOR. FURNISH AN IMPACT ATTENUATOR FROM THE OFFICE OF ROADWAY ENGINEERING'S APPROVED LIST FOR WORK ZONE IMPACT ATTENUATORS, FROM THE ROADWAY STANDARDS APPROVED PRODUCTS WEB PAGE.

INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS IN ACCORDANCE WITH THE MANUFACTURER'S

THE CONTRACTOR SHALL REPAIR OR REPLACE A DAMAGED UNIT WITHIN 24 HOURS OF A DAMAGING IMPACT. SPECIFICATIONS.

WHEN BIDIRECTIONAL DESIGNS ARE SPECIFIED, THE CONTRACTOR SHALL SUPPLY APPROPRIATE TRANSITIONS.

WHEN GATING IMPACT ATTENUATORS ARE DESIRED, THE CONTRACTOR SHALL SUBMIT DOCUMENTATION TO THE ENGINEER FOR ACCEPTANCE.

THE COST FOR THE ADDITIONAL BARRIER REQUIRED FOR A GATING IMPACT ATTENUATOR SHALL BE INCLUDED IN THE COST OF THE GATING IMPACT ATTENUATOR.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT AND MAINTAIN A COMPLETE AND FUNCTIONAL IMPACT ATTENUATOR SYSTEM, INCLUDING ALL RELATED BACKUPS, TRANSITIONS, LEVELING PADS, HARDWARE AND GRADING, NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER.

COOPERATION BETWEEN CONTRACTORS

THE STATE OF OHIO HAS CONTRACTED PROJECTS LIC-16/79-VAR. PID 110414 AND D05-BP-FY2023 PID 102530, WHICH MAY BE CONSTRUCTED CONCURRENTLY WITH THIS PROJECT. IT IS IMPERATIVE THAT THE CONTRACTORS COOPERATE FULLY WITH EACH OTHER AS OUTLINED IN SECTION 105.08 OF THE CMS MANUAL. ALL MAINTENANCE OF TRAFFIC SHALL BE COORDINATED BETWEEN PROJECTS AND NOT CONFLICTING WITH ONE ANOTHER.

SIGNAL TIMING MODIFICATIONS

FOR ANY SIGNAL TIMING MODIFICATION DURING PHASED CONSTRUCTION YOU CAN CONTACT THE CITY OF NEWARK ENGINEERING OFFICE'S BJ VARNER AT 740-404-4696. IF ANY MODIFICATIONS ARE DONE TO THE SIGNAL TIMING THEY SHALL BE RETURNED TO THERE ORGINAL TIMING SEQUENCE.

FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHEILDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614 MAINTAINING TRAFFIC.

DESIGN AGENCY



DESIGNER
YEL

REVIEWER
TAG 7-10-22

PROJECT ID
98279

SHEET TOTAL
5 59

STANDARD DRAWINGS AND SUPPLEMENTAL SPECIFICATIONS

REFER TO THE FOLLOWING STANDARD BRIDGE DRAWING(S):

- AS-1-15 DATED (REVISED) 7/17/2015
- AS- 2-15 DATED (REVISED) 1/18/2019
- BD-1-11 DATED (REVISED) 7/20/2018
- ICD-2-18 DATED (REVISED) 1/21/2022
- PCB-91 DATED (REVISED) 7/17/2020
- PSBD-2-07 DATED (REVISED) 7/20/2018
- VPF-1-90 DATED (REVISED) 7/20/2018

AND TO THE FOLLOWING SUPPLEMENTAL SPECIFICATION(S):

- 800 DATED 10/15/2021
- 832 DATED 10/19/2018

REFERENCE

EXISTING BRIDGE PLANS MAY BE INSPECTED AND ARE PROVIDED WITH THIS PROJECT'S BIDDING DOCUMENTS.

EXISTING STRUCTURE VERIFICATION

DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURE HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURE AND FROM FIELD OBSERVATIONS AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURE AND THE PROPOSED WORK BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO C&MS, SECTIONS 102.05, 105.02, AND 513.04*. BASE CONTRACT BID PRICES UPON A RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PREBID EXAMINATION OF THE EXISTING STRUCTURE. HOWEVER, THE DEPARTMENT WILL PAY FOR ALL PROJECT WORK BASED UPON ACTUAL DETAILS AND DIMENSIONS THAT HAVE BEEN VERIFIED IN THE FIELD.

DESIGN SPECIFICATIONS

THIS STRUCTURE CONFORMS TO THE 9th EDITION OF THE "LRFD BRIDGE DESIGN SPECIFICATIONS" ADOPTED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS, 2020 AND THE ODOT BRIDGE DESIGN MANUAL, 2020.

SPECIAL DESIGN SPECIFICATIONS

THIS BRIDGE REQUIRED THE USE OF A THREE DIMENSIONAL MODEL USING THE APPROXIMATE METHOD TO ANALYZE THE STRUCTURE. THE COMPUTER PROGRAM USED FOR STRUCTURAL ANALYSIS WAS BENTLY LEAP BRIDGE CONCRETE. THE BRIDGE COMPONENTS DESIGNED BY THIS METHOD AND THE LIVE LOAD DISTRIBUTION FACTORS USED WERE:

DEAD LOAD DISTRIBUTION:
DISTRIBUTE DEAD LOAD EQUALLY TO ALL BEAMS

LIVE LOAD DISTRIBUTION FACTORS:

EXTERIOR MEMBERS -
0.25 FOR LANE LOAD MOMENTS.
& 0.56 FOR LANE LOAD SHEARS

INTERIOR MEMBERS -
0.25 FOR LANE LOAD MOMENTS.
& 0.56 FOR LANE LOAD SHEARS

DESIGN DATA

LRFD LOAD MODIFIERS

OPERATIONAL IMPORTANCE

A LOAD MODIFIER OF 1.0 HAS BEEN ASSUMED FOR THE DESIGN OF THIS STRUCTURE IN ACCORDANCE WITH THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, ARTICLE 1.3.5 AND THE ODOT BRIDGE DESIGN MANUAL.

DESIGN LOADING

DESIGN LOADING INCLUDES:

- VEHICULAR LIVE LOAD: HL-93*
- FUTURE WEARING SURFACE (FWS) OF 0.060 KIPS/SQ.FT
- PEDESTRIAN LIVE LOAD: 0.090 KIPS/SF.

DESIGN STRESSES

CONCRETE CLASS QC2:
COMPRESSIVE STRENGTH 4.5 KSI (SUPERSTRUCTURE)

CONCRETE CLASS QC1:
COMPRESSIVE STRENGTH 4.0 KSI (SUBSTRUCTURE)

REINFORCING STEEL MINIMUM YIELD STRENGTH 60 KSI

CONCRETE FOR PRESTRESSED BEAMS:
COMPRESSIVE STRENGTH (FINAL) - (8) KSI
COMPRESSIVE STRENGTH (RELEASE) - (6) KSI

PRESTRESSING STRAND:
AREA = (0.217) SQ.IN.
ULTIMATE STRENGTH = 270 KSI
INITIAL STRESS = 202.5 KSI
(LOW RELAXATION STRANDS)

MONOLITHIC WEARING SURFACE

MONOLITHIC WEARING SURFACE IS ASSUMED, FOR DESIGN PURPOSES, TO BE 1 INCH THICK.

ITEM 202, PORTIONS OF STRUCTURE REMOVED, OVER 20 FOOT SPAN, AS PER PLAN
ITEM 202, PORTIONS OF STRUCTURE REMOVED, AS PER PLAN (SUBSTRUCTURE)

THIS ITEM SHALL INCLUDE THE ELEMENTS INDICATED IN THE PLANS AND GENERAL NOTES AND THAT ARE NOT SEPARATELY LISTED FOR PAYMENT, EXCEPT FOR WEARING COURSE REMOVAL. ITEMS TO BE REMOVED INCLUDE ALL EXISTING MATERIALS BEING REPLACED BY NEW CONSTRUCTION AND MISCELLANEOUS ITEMS THAT ARE NOT SHOWN TO BE INCORPORATED INTO THE FINAL CONSTRUCTION AND ARE DIRECTED TO BE REMOVED BY THE ENGINEER. THE USE OF EXPLOSIVES, HEADACHE BALLS AND/OR HOE-RAMS WILL NOT BE PERMITTED. THE METHOD OF REMOVAL AND THE WEIGHT OF HAMMER SHALL BE APPROVED BY THE ENGINEER. PERFORM ALL WORK IN A MANNER THAT WILL NOT CUT, ELONGATE OR DAMAGE THE EXISTING REINFORCING STEEL TO BE PRESERVED. CHIPPING HAMMERS SHALL NOT BE HEAVIER THAN THE NOMINAL 90-POUND CLASS. PNEUMATIC HAMMERS SHALL NOT BE PLACED IN DIRECT CONTACT WITH REINFORCING STEEL THAT IS TO BE RETAINED IN THE REBUILT STRUCTURE. SUBMIT CONSTRUCTION PLANS ACCORDING TO C&MS 501.05.

ITEM 203 EMBANKMENT, AS PER PLAN

PLACE AND COMPACT EMBANKMENT MATERIAL IN 6 INCH LIFTS FOR THE CONSTRUCTION OF THE APPROACH EMBANKMENT.

VANDAL PROTECTION FENCING

INSTALL FENCING FOR EACH CONSTRUCTION PHASE PRIOR TO OPENING THAT PHASE TO VEHICULAR AND/OR PEDESTRIAN TRAFFIC. USE BLACK PVC COATED FENCE.

ABUTMENT CONCRETE

DO NOT PLACE THE ABUTMENT CONCRETE ABOVE THE BRIDGE SEAT CONSTRUCTION JOINT UNTIL THE PRESTRESSED CONCRETE BOX BEAMS HAVE BEEN ERECTED.

ABUTMENT DIAPHRAGM CONCRETE, PHASED CONSTRUCTION

PLACE THE DIAPHRAGM CONCRETE ENCASING THE STRUCTURAL MEMBER ENDS OF AN INDIVIDUAL PHASE AFTER THE DECK PLACEMENT IN THE ADJACENT SPAN IS COMPLETE. PROCEDURES THAT PLACE THE ABUTMENT DIAPHRAGM WITH THE DECK CONCRETE MAY BE APPROVED BY THE ENGINEER IF THE PLACEMENT SUBMITTAL CAN ASSURE THAT THE DECK CONCRETE IN THE ADJACENT SPAN WILL BE PLACED BEFORE CONCRETE IN THE DIAPHRAGM HAS REACHED ITS INITIAL SET. PLACE CLOSURE POUR CONCRETE IN THE DIAPHRAGM AND DECK CONCURRENTLY.

DECK SLAB THICKNESS FOR CONCRETE QUANTITY

THE ESTIMATED QUANTITY OF DECK CONCRETE IS MEASURED ACCORDING TO C&MS 511. IN ADDITION TO THE DESIGN SLAB THICKNESS, THE QUANTITY INCLUDES A VARIABLE HAUNCH THICKNESS THAT PROVIDES AN ALLOWANCE FOR: VERTICAL GRADE ADJUSTMENT AND BEAM CAMBER.

SCREED ELEVATIONS

SCREED ELEVATIONS SHOWN REPRESENT THE THEORETICAL DECK SURFACE LOCATION PRIOR TO DEFLECTIONS CAUSED BY DECK PLACEMENT AND OTHER ANTICIPATED DEAD LOADS.

ELASTOMERIC BEARINGS

THE ELASTOMER SHALL HAVE A HARDNESS OF 50 DUROMETER. THE BEARINGS WERE DESIGNED IN ACCORDANCE WITH SECTION 14.7.6 (METHOD A) OF THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS. THE LONG-TERM COMPRESSION PROOF LOAD TEST (AASHTO STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES, DIVISION II, SECTION 18.7.2.6) IS NOT REQUIRED.

CUT LINE CONSTRUCTION JOINT PREPARATION

SAW CUT BOUNDARIES OF PROPOSED CONCRETE REMOVALS 1 INCH DEEP. REMOVE CONCRETE TO A ROUGH SURFACE. LEAVE THE EXISTING REINFORCING STEEL, IF REQUIRED IN THE PLANS, IN PLACE. INSTALL DOWEL BARS IF SPECIFIED. PRIOR TO CONCRETE PLACEMENT ABRASIVELY CLEAN JOINT SURFACES AND EXISTING EXPOSED REINFORCEMENT TO REMOVE LOOSE AND DISINTEGRATED CONCRETE AND LOOSE RUST. THOROUGHLY CLEAN THE JOINT SURFACE AND EXPOSED REINFORCEMENT OF ALL DIRT, DUST, RUST OR OTHER FOREIGN MATERIAL BY THE USE OF WATER, AIR UNDER PRESSURE, OR OTHER METHODS THAT PRODUCE SATISFACTORY RESULTS. EXISTING REINFORCING STEEL DOES NOT HAVE TO HAVE A BRIGHT STEEL FINISH BUT REMOVE ALL PACK AND LOOSE RUST. THOROUGHLY DRENCH EXISTING CONCRETE SURFACES WITH CLEAN WATER AND ALLOW TO DRY TO A DAMP CONDITION BEFORE PLACING CONCRETE.

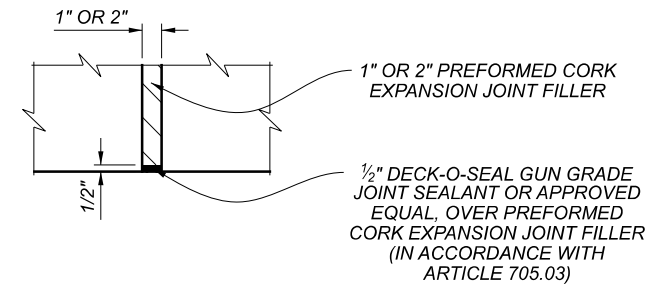
ITEM 516 - 2" DEEP JOINT SEALER, AS PER PLAN

UPON COMPLETION OF THE PROPOSED BRIDGE DECK, APPROACH SLAB, AND ASPHALT RESURFACING, THE CONTRACTOR SHALL SAW CUT ALONG THE END OF THE BRIDGE DECK ENDS (WITHOUT CUTTING THE DECK) AN AREA 1" WIDE BY 2" DEEP AND FILL THIS AREA WITH HOT APPLIED JOINT SEALER 705.04.

ITEM 516 - 1" OR 2" PREFORMED EXPANSION JOINT FILLER, AS PER PLAN

ALL 1" P.E.J.F. AND 2" P.E.J.F. CALLED FOR IN THE PLANS SHALL BE PREFORMED CORK JOINT FILLER (IN ACCORDANCE WITH ARTICLE 705.03). RECESS JOINT FILLER 1/2" FOR ALL JOINTS (SEE DETAIL). SEAL ALL JOINTS WITH DECK-O-SEAL GUN GRADE-JOINT SEALANT OR AN APPROVED EQUAL. THE COLOR SHALL BE STONE GRAY. APPROVED MANUFACTURER'S APPLICATION METHODS SHALL BE FOLLOWED DURING SURFACE PREPARATION AND APPLICATION FOR MAXIMUM EFFECTIVENESS.

DECK-O-SEAL
P.O. BOX 397
HAMPSHIRE, IL 60140
PHONE: 800-542-7665



PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID FOR ITEM 516 - 1" PEJF, APP, SQ. FT. AND 2" PEJF, APP, SQ. FT., AND SHALL INCLUDE ALL LABOR, EQUIPMENT, AND INCIDENTALS REQUIRE TO COMPLETE THE WORK DESCRIBED.

ITEM 526 - REINFORCED CONCRETE APPROACH SLABS (T=15"), AS PER PLAN

FURNISH APPROACH SLABS CONFORMING TO CMS 526. THE ACCEPTED QUANTITIES SHALL INCLUDE: CONCRETE, REINFORCING STEEL, SIDEWALK, JOINT FILLERS, JOINT SEALERS, JOINT SEALS, P.E.J.F., A.P.P., WATERPROOFING, AND ANY OTHER INCIDENTALS SHOWN ON THE APPROACHSLAB DETAIL SHEETS UNLESS OTHERWISE NOTED IN THE PLAN. THE DEPARTMENT WILL MEASURE APPROACH SLABS BY THE NUMBER OF SQUARE YARDS.

LIC-79-13.30

MODEL: Sheet PAPER: 17x11 (in.) DATE: 10/28/2022 TIME: 3:25:40 PM USER: lgreenwa pwc:\hobolod-pw-bentley.com\shahdo\pww-02\Documents\01 Active Projects\Distict 05\Lic79-13\00-Engineering\Structures\SFN_4504941_Sheets\98279_SFN_4504941_SN001.dgn

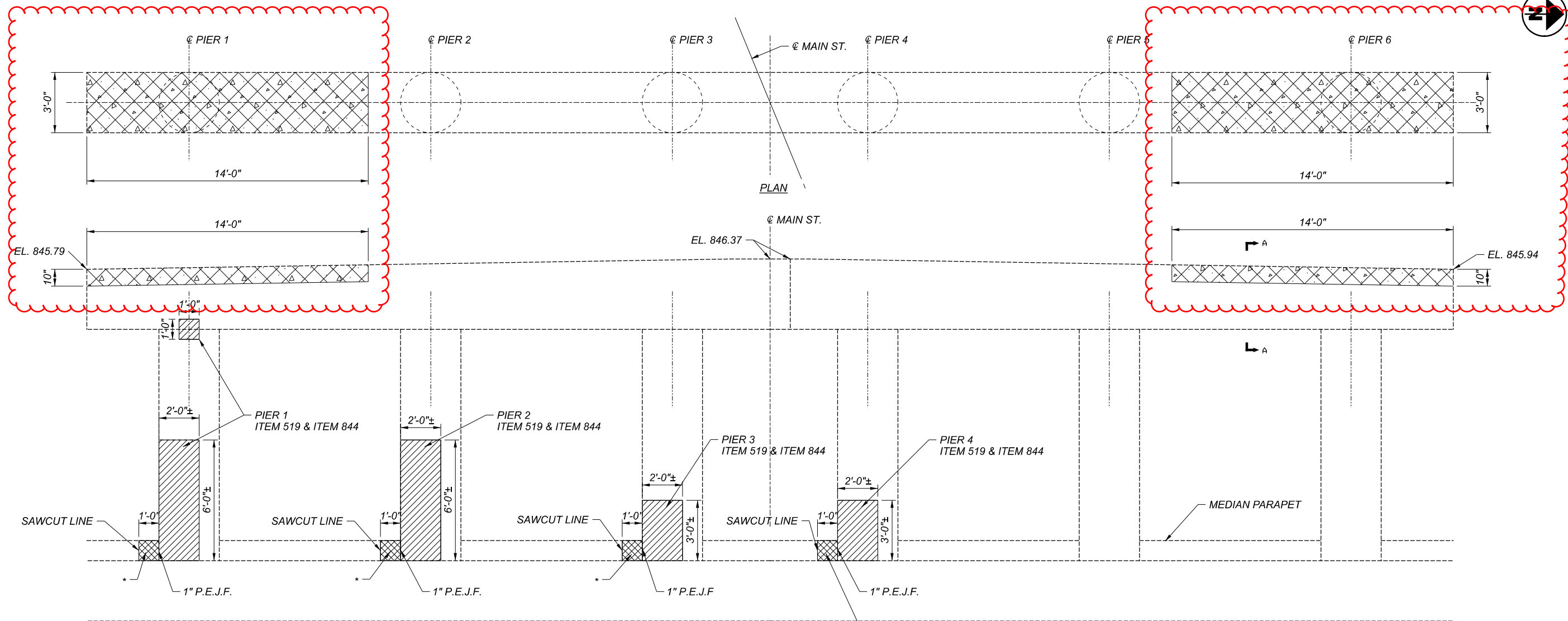
BRIDGE NOTES
BRIDGE NO. LIC-79-1332
OVER S.R. 79

SFN	
4504941	
DESIGN AGENCY	
DESIGNER	CHECKER
YEL	TAG
REVIEWER	
TAG 07/10/22	
PROJECT ID	
98279	
SUBSET	TOTAL
2	30
SHEET	TOTAL
31	59

SHEET NUM.										PART.	ITEM	ITEM	GRAND	UNIT	DESCRIPTION	SEE SHEET NO.	
										01/NHS/14/NEWA	ITEM	EXT	TOTAL				
STRUCTURE OVER 20 FOOT SPAN (LIC-79-13.31 or SFN 4504941)																	
										LS	202	11203	LS			PORTIONS OF STRUCTURE REMOVED, OVER 20 FOOT SPAN, AS PER PLAN	32
										3	202	11301	3	CY	PORTIONS OF STRUCTURE REMOVED, AS PER PLAN, (SUBSTRUCTURE)	32	
										294	202	22906	294	SY	APPROACH SLAB REMOVED		
										1,208	202	23500	1,208	SY	WEARING COURSE REMOVED		
										315	202	38500	315	FT	BRIDGE RAILING REMOVED		
										LS	503	11100	LS		COFFERDAMS AND EXCAVATION BRACING		
										LS	503	21301	LS		UNCLASSIFIED EXCAVATION, AS PER PLAN	30	
										82,420	509	10000	82,420	LB	EPOXY COATED REINFORCING STEEL		
										227	511	31612	227	CY	CLASS QC2 CONCRETE WITH QC/QA, SUPERSTRUCTURE		
										92	511	34463	92	CY	CLASS QC SCC CONCRETE WITH QC/QA, BRIDGE DECK (PARAPET), AS PER PLAN	20	
										3	511	43210	3	CY	CLASS QC1 CONCRETE, PIER		
										205	511	43542	205	CY	CLASS QC1 CONCRETE WITH QC/QA, ABUTMENT INCLUDING FOOTING		
										64	511	51512	64	CY	CLASS QC2 CONCRETE WITH QC/QA, SIDEWALK		
										873	512	10050	873	SY	SEALING OF CONCRETE SURFACES (NON-EPOXY)	32	
										4	515	12060	4	EACH	PRESTRESSED CONCRETE COMPOSITE BOX BEAM BRIDGE MEMBERS, LEVEL 1, CB27-36 (79'-2.5" L)		
										30	515	12070	30	EACH	PRESTRESSED CONCRETE COMPOSITE BOX BEAM BRIDGE MEMBERS, LEVEL 1, CB27-48 (79'-2.5" L)		
										2	516	13201	2	SF	1/2" PREFORMED EXPANSION JOINT FILLER, AS PER PLAN	32	
										221	516	13600	221	SF	1" PREFORMED EXPANSION JOINT FILLER		
										32	516	13901	32	SF	2" PREFORMED EXPANSION JOINT FILLER, AS PER PLAN	32	
										159	516	14014	159	FT	INTEGRAL ABUTMENT EXPANSION JOINT SEAL		
										145	516	14600	145	FT	STRUCTURAL JOINT OR JOINT SEALER, MISC.: EMSEAL WITH SLEEPER SLAB	30	
										225	516	31011	225	FT	2" DEEP JOINT SEALER, AS PER PLAN	32	
										102	516	41100	102	EACH	1/8" PREFORMED BEARING PAD		
										8	516	43200	8	EACH	ELASTOMERIC BEARING WITH INTERNAL LAMINATES ONLY (NEOPRENE) (6"x12"x2.0")	32	
										60	516	43200	60	EACH	ELASTOMERIC BEARING WITH INTERNAL LAMINATES ONLY (NEOPRENE) (6"x16"x2.0")	32	
										4	516	43200	4	EACH	ELASTOMERIC BEARING WITH INTERNAL LAMINATES ONLY (NEOPRENE) (6"x30"x2.0")	32	
										30	516	43200	30	EACH	ELASTOMERIC BEARING WITH INTERNAL LAMINATES ONLY (NEOPRENE) (6"x42"x2.0")	32	
										LS	518	21230	LS		POROUS BACKFILL WITH GEOTEXTILE FABRIC		
										200	518	40000	200	FT	6" PERFORATED CORRUGATED PLASTIC PIPE		
										60	518	40010	60	FT	6" NON-PERFORATED CORRUGATED PLASTIC PIPE, INCLUDING SPECIALS		
										372	526	25001	372	SY	REINFORCED CONCRETE APPROACH SLABS (T=15"), AS PER PLAN	32	
										2,189	SPECIAL	53000600	2,189	SF	STRUCTURES : AESTHETIC TREATMENT, (CONCRETE FORMLINER / STAIN)	33	
										370	607	39901	370	FT	VANDAL PROTECTION FENCE, 6' STRAIGHT, COATED FABRIC, AS PER PLAN	32	
										74	844	10000	74	SF	CONCRETE PATCHING WITH GALVANIC ANODE PROTECTION	32	

ESTIMATED QUANTITIES
BRIDGE NO. LIC-79-1332
OVER S.R. 79

SFN	
4504941	
DESIGN AGENCY	
DESIGNER	CHECKER
YEL	TAG
REVIEWER	
TAG 7-10-22	
PROJECT ID	
98279	
SUBSET	TOTAL
4	30
SHEET	TOTAL
33	59

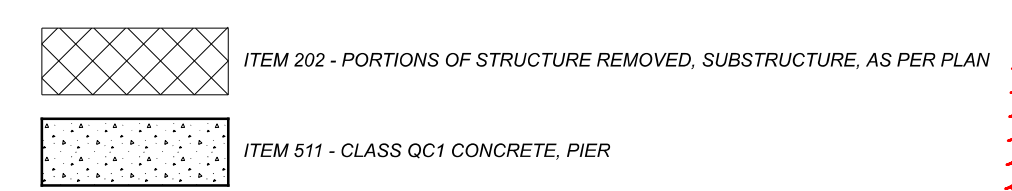
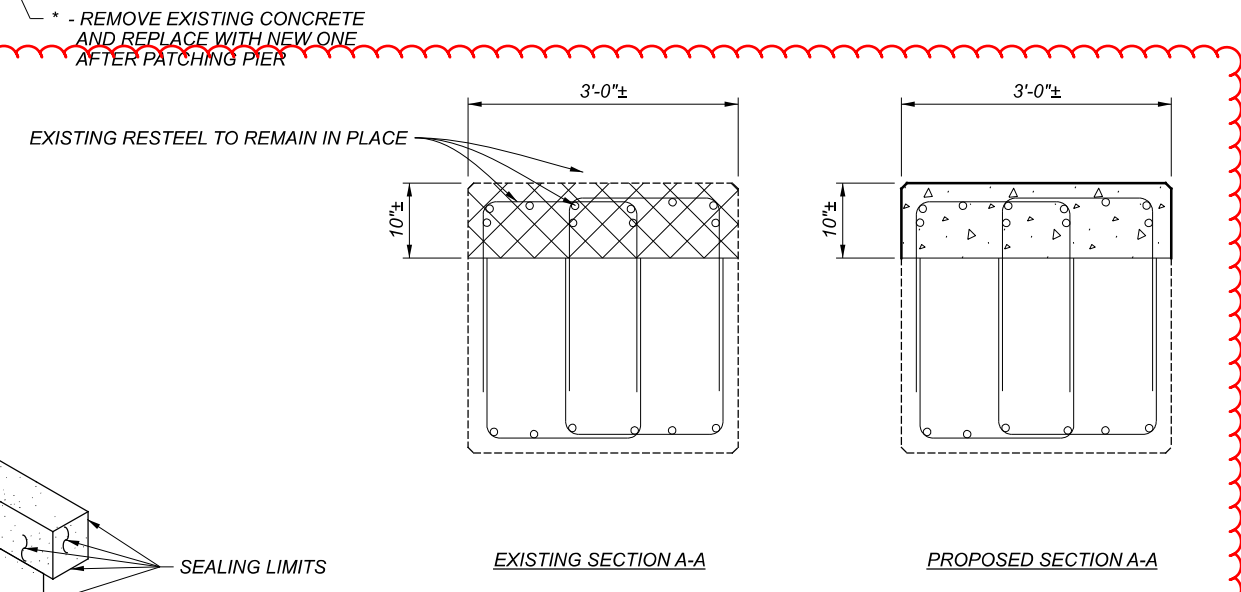
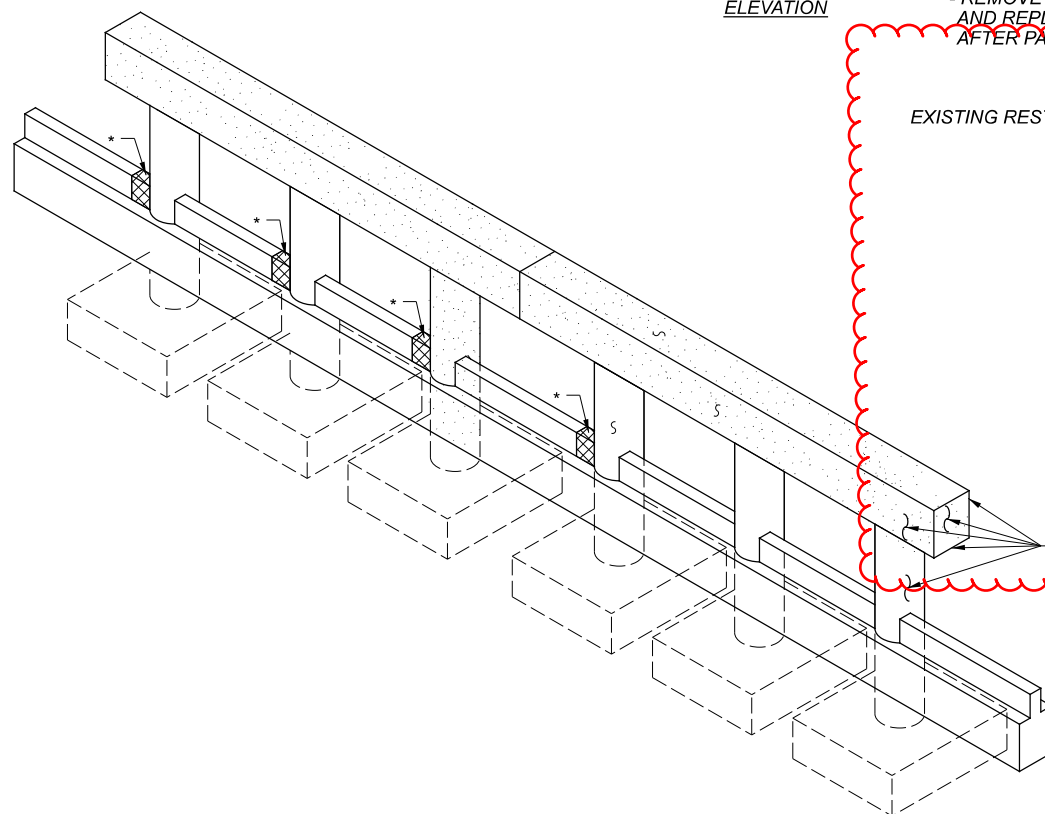


ITEM 519 - PATCHING CONCRTE STRUCTURE, AS PER PLAN

EXACT DIMENSIONS AND LOCATIONS OF PATCHES SHALL BE DETERMINED BY THE ENGINEER IN THE FIELD.

ESTIMATED PATCHING QUANTITIES (S.F.)		
LOCATION	MEASURED QUANTITIES	ESTIMATED QUANTITIES
PIER 1	13	26
PIER 2	12	24
PIER 3	6	12
PIER 4	6	12

ESTIMATED QUANTITES HAVE BEEN INCREASED BY 50% OVER MEASURED QUANTITIES TO ACCOUNT FOR ADDITIONAL DETERIORATION.



PIER DETAILS
BRIDGE NO.: LIC-79-1332
OVER S.R. 79

SFN	4504941
DESIGN AGENCY	
DESIGNER	
CHECKER	
REVIEWER	
TAG	07-10-22
PROJECT ID	98279
SUBSET	TOTAL
11	30
SHEET	TOTAL
40	59