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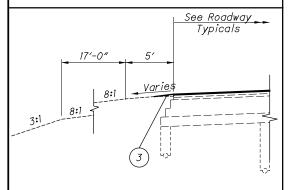
See Roadway Typicals DETAIL A

Applies From Sta. 300+50.18 to Sta. 301+70.00

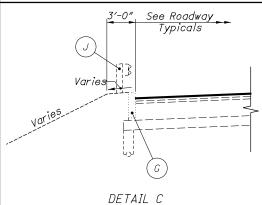
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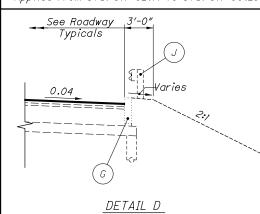
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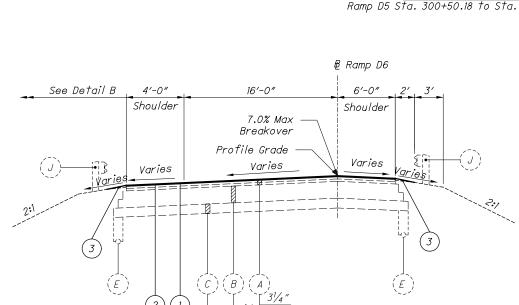
DETAIL B Applies From Sta. 313+04.25 to Sta. 315+63.37



Applies From Sta. 317+02.14 to Sta. 317+30.29

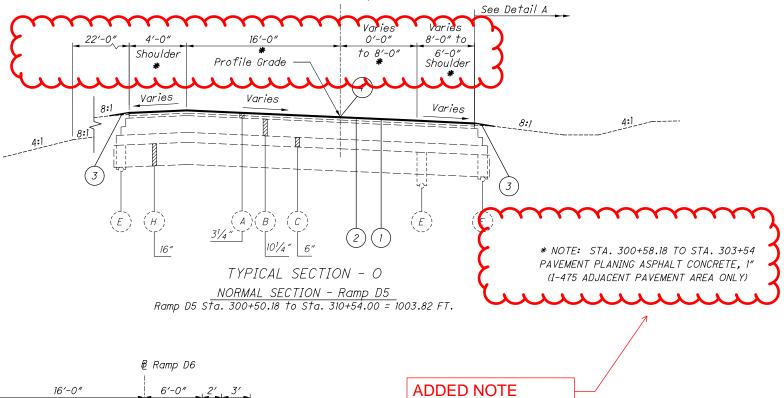


Applies From Sta. 317+05.11 to Sta. 317+30.29

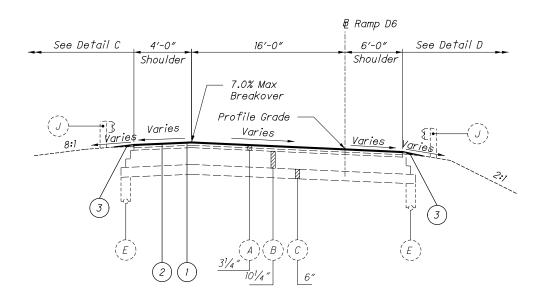


TYPICAL SECTION - P SUPERELEVATED SECTION - Ramp D6

Ramp D6 Sta. 311+19.00 to Sta. 315+63.37 = 444.37 FT.



₿ Ramp D5



TYPICAL SECTION - Q SUPERELEVATED SECTION - Ramp D6

Ramp D6 Sta. 315+63.37 to Sta. 317+30.29 = 166.92 FT. Sta. 318+90.43 to Sta. 321+31.65 = 241.22 FT.

LUC-75/

475-7.82/14.50

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ITEM 622 - CONCRETE BARRIER, TYPE D, AS PER PLAN 93 FT.

IT IS THE INTENT TO FOLLOW THE PLAN INSERT SHEET INCLUDED IN THIS PLAN FOR CONCRETE BARRIER WALL INSTALLATION. PLACEMENT OF MEDIAN BARRIER WALL SHALL IMMEDIATELY FOLLOW THE REMOVAL.

### AIRWAY/HIGHWAY CLEARANCE FOR AIRPORTS AND HELIPORTS

THIS PROJECT HAS BEEN IDENTIFIED AS BEING WITHIN THE INFLUENCE AREA OF A PUBLIC USE AIRPORT OR HELIPORT. NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT AT MAXIMUM OPERATING HEIGHT SHALL EXCEED A HEIGHT OF 145 FT. IF ANY TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT WILL EXCEED THIS HEIGHT, FURTHER COORDINATION WITH THE FEDERAL AVIATION ADMINISTRATION (FAA), AND THE ODOT OFFICE OF AVIATION. WILL BE NECESSARY PRIOR TO ERECTING SUCH TEMPORARY STRUCTURES OR OPERATING SUCH EQUIPMENT ON THE PROJECT. NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT SHALL EXCEED THE PERMISSIBLE HEIGHT, UNTIL COORDINATION IS MET AND DOCUMENTATION HAS BEEN FURNISHED. TO THE PROJECT ENGINEER. IF COORDINATION IS NOT OBTAINED, THEN THE PROJECT ENGINEER WILL HAVE THE AUTHORITY TO PROVIDE RESTRICTIONS AS REQUIRED.

THE TOLEDO HOSPITAL PROMEDICA HEALTH SYSTEM 2142 N. COVE BLVD. TOLEDO, OHIO 43606 419.291.4000

# ITEM 442 ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A (447), AS PER PLAN

ITEM 442 ASPHALT CONCRETE SURFACE COURSE, 12.5MM TYPE A (447), AS PER PLAN SHALL FOLLOW THE SPECIFICATIONS FOR THE 442 ITEM EXCEPT FOR SECTION 442.04 ASPHALT BINDER, THE BINDER SHALL BE PG76-22M FOR THE SURFACE COURSE AND A MAXIMUM OF 10% OF RAP BY DRY WEIGHT OF MIX CAN BE USED. ADDITIONALLY, THE COARSE AGGREGATE FOR THIS ITEM SHALL CONTAIN A MINIMUM OF 50% AIR COOLED BLAST FURNACE SLAG, STEEL SLAG, OR CRUSHED IGNEOUS ROCK.

## ITEM 442, ASPHALT CONCRETE INTERMEDIATE COURSE, AS PER PLAN

ITEM 442, ASPHALT CONCRETE INTERMEDIATE COURSE, 19MM, TYPE A (446), AS PER PLAN SHALL FOLLOW THE SPECIFICATIONS FOR THE 442 ITEM EXCEPT FOR SECTION 442.04 ASPHALT BINDER. THE BINDER SHALL BE PG76-22M FOR THE INTERMEDIATE COURSE AND A MAXIMUM OF 20% OF RAP BY DRY WEIGHT OF MIX CAN BE USED.

## ITEM 424 - FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN

PER CMS 424.08. 448 DENSITY APPLIES TO THIS PROJECT. DENSITY WILL BE TESTED ACCORDING TO SUPPLEMENT 1055 PER CMS 448.02. THE DISINCENTIVE PORTION OF S-1055 (TABLE 1055.01-1 AND TABLE 1055.04) WILL BE WAIVED PROVIDING THAT THE CONTRACTOR MAKES EVERY EFFORT TO OBTAIN DENSITY AND DOES NOT USE VIBRATORY ROLLERS.

AN ESTIMATED QUANTITY OF 100 CY HAS BEEN CARRIED TO THE GENERAL SUMMARY TO ACCOUNT FOR SURFACE IRREGULARITIES.

#### PLANED SURFACES

NO PLANED SURFACES SHALL BE OPEN TO THE PUBLIC FOR MORE THAN 5 DAYS. IF THE PLANED SURFACE IS OPEN FOR MORE THAN 5 DAYS. THEN IT IS THE CONTRACTOR'S RESPONSIBILITY TO REPAIR THE PAVEMENT FAILURES THAT OCCURRED AFTER THE 5

## RUMBLE STRIPE REMOVAL BEFORE PAVING

RUMBLE STRIPS WILL BE PLANED WITH ITEM 441, FOR THE I-475 LOCATION. THE QUANTITIES FOR PLANING AND PAVING THE RUMBLE STRIPS ARE PROVIDED BELOW. QUANTITIES ARE BASED ON 2' WIDE MILL. QUANTITIES TO BE CARRIED TO THE GENERAL SUMMARY.

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I 475 EB EXISTING LENGTH OF RUMBLE STRIP: LENGTH = 1346 FT (x2) = 2692 FT

I 475 WB EXISTING LENGTH OF RUMBLE STRIP: LENGTH = 7192 FT (x2) = 14384 FT

RAMP S-W TO I 475 WB EXISTING LENGTH OF RUMBLE STRIP: LENGTH =900 FT (x2) = 1800 FT

RAMP N-W TO I 475 WB EXISTING LENGTH OF RUMBLE STRIP: LENGTH = 775 FT (x2) = 1550 FT

TOTAL

22026 FT

ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, 1 1/2 " I 475 EB 598 SY 3197 SY I 475 WB RAMP S-W TO I 475 WB 400 SY

RAMP N-W TO I 475 WB 345 SY ITEM 254 TOTAL = 4540 SY

ITEM 407 - NON TRACKING TACK COAT

I 475 EB 51 GAL I 475 WB 272 GAL

RAMP S-W TO I 475 WB 34 GAI

RAMP N-W TO I 475 WB 30 GAL ITEM 407 TOTAL = 387 GAL

ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22, 1 1/2"

I 475 EB 25 CY

I 475 WB 133 CY RAMP S-W TO I 475 WB 17 CY

RAMP N-W TO I 475 WB 15 CY

ITEM 441 TOTAL = 190 CY

MODIFIED QUANTITIES

## ITEM 255, FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, MISC .: CLASS QC 3

MATERIALS: PROVIDE MATERIALS CONFORMING TO THE FOLLOWING REQUIREMENTS:

PORTLAND CEMENT CONCRETE

499.03, CLASS QC 3, W/MACRO-FIBERS\* FLEXURAL STRENGTH: 300 PSI IN 12 HOURS

PERMEABILITY: 2000 COULOMBS

MACROFIBERS: MINIMUM 4.0 LB/CY

COARSE AGGREGATE (NO. 57 & NO.8) 703.02 & 703.13 FINE AGGREGATE (NATURAL SAND) 703.02 PORTLAND CEMENT, TYPE I 701.04 FLY ASH OR NATURAL POZZOLAN 701.13 SLAG CEMENT WATER 499.02 CHEMICAL ADMIXTURE 705.12 AIR-ENTRAINING ADMIXTURE MACRO-FIBERS FOR CONCRETE 705.10 705.29 LIQUID MEMBRANE-FORMING COMPOUNDS FOR CONCRETE CURING

\*USE A MINIMUM DOSAGE RATE OF FIBERS OF 4.0 LB/YD3 OF CONCRETE. ENSURE THE FINAL PROPOSED MIX IS WORKABLE AND ABLE TO BE PRODUCED SUCH THAT BALLING OR CLUMPING OF THE FIBERS IS NOT A PROBLEM AS DETERMINED BY THE ENGINEER. A DEMONSTRATION OF THE MIX PRODUCTION, OR TRIAL MIX, MAY BE REQUIRED BY THE ENGINEER PRIOR TO PLACING ÁNY OF THE MIX ON THE PROJECT.

PAVEMENT SHALL BE PLANED BEFORE PAVEMENT IS REMOVED. THE FOLLOWING ESTIMATED QUANTITY IS TO BE USED FOR PAVEMENT REMOVAL AND RIGID REPLACMENT AS DIRECTED BY THE ENGINEER. THE CONTRACTOR MAY BRING THE CONCRETE TO THE MILLED SURFACE.

ITEM 255 - FULL DEPTH PAVEMENT REPAIR AND RIGID REPLACEMENT, MISC.: CLASS QC 3

60-6x12 JOINTS NB 75-6x12 JOINTS SB

= 480 SQ. YD. = 600 SQ. YD TOTAL = 1080 SQ. YD.

TEM 255 - FULL DEPTH PAVEMENT SAWING 2160 LF (NB) 2700 FL (SB)

TOTAL = 4860 LF

QUANTIES CARRIED TO THE GENERAL SUMMARY.

# LONGITUDINAL JOINTS (FLEXIBLE PAVEMENT)

LOCATE LONGITUDINAL JOINTS IN THE SURFACE COURSE SUBJECT TO THE FOLLOWING REQUIREMENTS:

PLACE THE MAINLINE PAVEMENT SURFACE COURSE WITH A SINGLE COLD LONGITUDINAL JOINT LOCATED AT THE LANE LINES. NO OTHER COLD JOINTS ARE PERMITTED IN THE SURFACE COURSE OF THE MAINLINE PAVEMENT.

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CHRISTMAS FOURTH OF JULY LABOR DAY NEW YEARS MEMORIAL DAY THANKSGIVING EASTER

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY TIME ALL LANES MUST OR EVENT BE OPEN TO TRAFFIC

SUNDAY 12:00N FRIDAY THROUGH 6:00 AM MONDAY MONDAY 12:00N FRIDAY THROUGH 6:00 AM TUESDAY TUESDAY 12:00N MONDAY THROUGH 6:00 AM WEDNESDAY WEDNESDAY 12:00N TUESDAY THROUGH 6:00 AM THURSDAY THURSDAY 12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY

THURSDAY (THANKSGIVING ONLY)

6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY

FRIDAY 12:00N THURSDAY THROUGH 6:00 AM MONDAY

*SATURDAY* 12:00N FRIDAY THROUGH 6:00 AM MONDAY

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT (PN 127).

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

PLEASE REFER TO THE SEQUENCE OF CONSTRUCTION LOCATED ON MOT NOTE SHEET 23 AND THE MAINTENANCE OF TRAFFIC DETAIL SHEETS INCLUDED IN THIS PLAN FOR LANE CLOSURES AND RESTRICTIONS OF I-75 AND I-475.

# WORK WITHIN CONSTRUCTION ZONE

THE CONTRACTOR MUST REMOVE ALL EQUIPMENT WHEN WORK IS NOT BEING PERFORMED OR THE CONTRACTOR MUST SET UP AN APPROVED MOT PLAN THAT INCLUDES A STAGING AREA WITH A MOT TRUCK AND APPROPRIATE IMPACT ATTENUATOR.

## CONCRETE MEDIAN BARRIER REPLACEMENT

REMOVING, GRADING AND INSTALLING THE REPLACEMENT BARRIER IN A CONTINUOUS OPERATION SHALL BE LIMITED TO 50 LINEAR FEET AND SHALL AT ALL TIMES BE SUBJECT TO THE APPROVAL OF THE ENGINEER. THE ENGINEER SHALL BE SATISFIED THAT ALL INSTALLATIONS WILL AFFORD MAXIMUM PROTECTION FOR TRAFFIC.

#### WORK ZONE MARKINGS AND SIGNS

ITEM 614 - WORK ZONE MARKING SIGN

ITEM 614 - WORK ZONE LANE LINE.

ITEM 614 - WORK ZONE SPEED LIMIT SIGN

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS IDENTIFIED BY THE ENGINEER FOR WORK ZONE PAVEMENT MARKINGS AND SIGNS PER THE REQUIREMENTS OF C&MS 614.04 AND 614.11.

60 EACH

12 EACH

32 MILE

2 MILE

CLASS I, 6", 642 PAINT	30 MILE
ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 6″, 642 PAINT	31 MILE
ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS III, 12″, 642 PAINT	7768 F
ITEM 614 - WORK ZONE DOTTED LINE, CLASS III, 6", 642 PAINT	6573 F
ITEM 614 - WORK ZONE STOP LINE, CLASS III, 642 PAINT	203 F1
ITEM 614 - WORK ZONE LANE LINE, CLASS I, 6″, 807 PAINT	31 MILE
ITEM 614 - WORK ZONE EDGE LINE,	

ITEM 614 - WORK ZONE CHANNELIZING LINE,

CLASS I, 6", 807 PAINT

ITEM 614 - WORK ZONE LANE LINE,

CLASS I, 6", 740.06, TYPE 1

ONLY CLASS I WORK ZONE PAVEMENT MARKINGS ON INTERSTATES AND MULTILANE HIGHWAYS EXPECTED TO BE IN PLACE LONGER THAN 14 DAYS WILL BE WET REFLECTIVE. WET REFLECTIVE WORK ZONE MARKINGS WILL NOT BE RECESSED. DUE TO CONTRACTORS MEANS AND METHODS, NOT KNOWING IF TEMPORARY MARKINGS WOULD BE IN PLACE LONGER THAN 14 DAYS, BOTH QUANTITIES WERE INCLUDED WITH THE CONTRACT.

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**NOTE ADDED** 

### ITEM 614, MAINTAINING TRAFFIC (NOTICE OF CLOSURE SIGN)

NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. [AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK.J

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD/RAMP FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.

NOTICE OF CLOSURE SIGN TIME TABLE												
ITEM	<u>DURATION OF</u>	SIGN DISPLAYED TO										
ITEIVI	<u>CLOSURE</u>	SIGN DISPLAYED TO PUBLIC  14 CALENDAR DAYS PRIOR TO CLOSURE 7 CALENDAR DAYS PRIOR TO CLOSURE 2 BUSINESS DAYS										
	> = 2 WEEKS	14 CALENDAR DAYS										
	>= 2 WEEKS	PRIOR TO CLOSURE										
RAMP & ROAD CLOSURES	> = 12 HRS & < 7	7 CALENDAR DAYS										
RAIVIP & ROAD CLOSURES	WEEKS	PRIOR TO CLOSURE										
	< 12 HRS	2 BUSINESS DAYS										
	\ 12 HK5	PRIOR TO CLOSURE										

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION. THIS PHONE NUMBER 419-373-4428.

#### ITEM 614, REPLACEMENT DRUM

DRUMS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT DRUMS SHALL BE NEW.

PAYMENT FOR THE NEW DRUMS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT DRUM, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGED DRUM, AND PROVIDING AND MAINTAINING THE REPLACEMENT DRUM IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL DRUM.

AN ESTIMATED QUANTITY OF 30 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

# **∠ITEM 614, REPLACEMENT SIGN**

FLATSHEET SIGNS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS. SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT SIGNS SHALL BE NEW. OTHER MATERIALS MAY BE IN USED. BUT GOOD, CONDITION SUBJECT TO **APPROVAL BY THE ENGINEER.** 

PAYMENT FOR THE NEW SIGNS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT SIGN, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF DAMAGED SIGNS, HARDWARE AND SUPPORTS, AND PROVIDING THE NECESSARY REPLACEMENT HARDWARE, SUPPORTS, ECT.

AN ESTIMATED QUANTITY OF 5 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

#### WORK ZONE INCREASED PENALTIES SIGN (R11-H5A)

R11-H5A-48 SIGNS SHALL BE FURNISHED, ERECTED, AND MAINTAINED IN GOOD CONDITION AND/OR REPLACED AS NECESSARY AND SUBSEQUENTLY REMOVED BY THE CONTRACTOR. SIGNS SHALL BE MOUNTED AT THE APPROPRIATE OFFSETS AND ELEVATIONS AS PRESCRIBED BY THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. THEY SHALL BE MAINTAINED ON SUPPORTS MEETING CURRENT SAFETY CRITERIA.

THE SIGNS MAY BE ERECTED OR UNCOVERED NO MORE THAN FOUR HOURS BEFORE THE ACTUAL START OF WORK. THE SIGNS SHALL BE REMOVED OR COVERED NO LATER THAN FOUR HOURS FOLLOWING RESTORATION OF ALL LANES TO TRAFFIC WITH NO RESTRICTIONS, OR SOONER AS DIRECTED BY THE ENGINEER. TEMPORARY SIGN COVERING AND UNCOVERING DUE TO TEMPORARY LANE RESTORATIONS SHALL BE GUIDED BY THE FOUR-HOUR LIMITATIONS STATED ABOVE. SUCH LANE RESTORATIONS SHOULD BE EXPECTED TO REMAIN IN EFFECT FOR 30 OR MORE CONSECUTIVE CALENDAR DAYS, SUCH AS DURING WINTER SHUTDOWNS.

(THE SIGNS ON THE MAINLINE SHALL BE DUAL MOUNTED UNLESS NOT PHYSICALLY POSSIBLE. THE FIRST SIGN SHALL BE PLACED BETWEEN THE ROAD WORK AHEAD (W20-1) SIGN AND THE NEXT SIGN IN THE SEQUENCE. SIGNS SHALL BE ERECTED ON EACH ENTRANCE RAMP AND EVERY 2 MILES THROUGH THE CONSTRUCTION WORK LIMITS. SIGNS ON THE MAINLINE SHALL BE R11-H5A-48. SIGNS USED ON THE RAMPS SHALL BE R11-H5A-24. R11-H5A-24 SIGNS MAY BE USED IN THE MEDIAN IN LIEU OF R11-H5A-48 SIGNS IF IT IS NOT PHYSICALLY POSSIBLE TO PROVIDE R11-H5A-48 SIGNS IN THE MEDIAN.)

THE CONTRACTOR MAY USE SIGNS AND SUPPORTS IN USED, BUT GOOD, CONDITION PROVIDED THE SIGNS MEET CURRENT ODOT SPECIFICATIONS. SIGN FACES SHALL BE RETROREFLECTORIZED WITH TYPE G SHEETING COMPLYING WITH THE REQUIREMENTS OF C&MS 730.19.

WORK ZONE INCREASED PENALTIES SIGNS AND SUPPORTS WILL BE MEASURED AS THE NUMBER OF SIGN INSTALLATIONS. INCLUDING THE SIGN AND NECESSARY SUPPORTS. IF A SIGN AND SUPPORT COMBINATION IS REMOVED AND REERECTED AT ANOTHER LOCATION AS DIRECTED BY THE ENGINEER, IT SHALL BE CONSIDERED ANOTHER UNIT.

PAYMENT FOR ACCEPTED QUANTITIES, COMPLETE, IN PLACE WILL BE MADE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIALS, LABOR, INCIDENTALS AND EQUIPMENT FOR FURNISHING, ERECTING, MAINTAINING, COVERING DURING SUSPENSION OF WORK, AND REMOVAL OF THE SIGN AND SUPPORT.

ITEM 614, WORK ZONE INCREASED PENALTIES SIGN 23 EACH

WORK ZONE INCREASED PENALTIES SIGNS WILL BE PLACED AT THE FOLLOWING LOCATIONS:

2 EACH
1 EACH
4 EACH
1 EACH
4 EACH
1 EACH
1 EACH
2 EACH
1 EACH
1 EACH
2 EACH
2 EACH
1 EACH

QUANTITY CARRIED TO THE GENERAL SUMMARY.

4

82

-7

-75/

LUC

Detour

I-75 SB to

Expressway

Dr to I-75

NB

Manhattan

Blvd to

Phillips Ave

to |-75 NB

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# SEQUENCE OF OPERATIONS

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#### CONSTRUCTION SEQUENCING FOR LUC-75.

MILL AND FILL RUMBLE STRIPS AS NECESSARY PRIOR TO THE START OF EACH STAGE.

THE INTENT IS THAT LONGITUDINAL JOINTS SHALL BE PLACED ON THE LANE LINE, EDGE LINE, OR CENTER OF A TRAVELED LANE.

WEDGING AT TRANSVERSE AND LONGITUDINAL JOINTS SHALL BE INSTALLED PER SCD MT-101.90.

STAGE 1: LIMITED TO FOUR WEEKENDS FROM FRIDAY 9PM TO MONDAY 6AM. THE RAMP FROM I-280 NB TO I-75 NB MAY BE CLOSED. A MINIMUM OF TWO LANES ON I-75 NB MUST BE MAINTAINED AT ALL TIMES EXCEPT WHEN ADDITIONAL LANE CLOSURES ARE ALLOWED PER THE PERMITTED LANE CLOSURE SCHEDULE. ALL LANE SHIFTS AND LANE CLOSURES SHALL BE COMPLETED PER SCD MT-95.30 AND SCD MT-102.20.

COMPLETE OVERLAY OF FORWARD AND REAR APPROACH SLABS FOR STRUCTURE LUC-75-0849.

COMPLETE OVERLAY OF FORWARD AND REAR APPROACH SLABS FOR STRUCTURE LUC-75-0861.

COMPLETE DECK SEALING OF STRUCTURE LUC-75-0891.

COMPLETE PAVEMENT REPAIRS AND RESURFACING UP TO THE INTERMEDIATE COURSE FOR THE FOLLOWING LOCATIONS: RAMP LL STA 19+54/RAMP A STA 351+32- RAMP LL STA 30+03/RAMP A 362+15.

LUC-75 NB FROM STA 363+89 - STA 386+15

STAGE 2: THIS WORK SHALL BE LIMITED TO 10 DAYS. I-75 NB SHALL REMAIN THREE LANES FROM THE I-280 INTERCHANGE UNTIL THE LANE REDUCTION AT STA 407+16, WHERE I-75 NB CAN BE REDUCED TO TWO LANE, ADDITIONAL CLOSURES PERMITTED BY THE PLCS ARE ACCEPTABLE, I-75 SB SHALL REMAIN A MINIMUM OF TWO LANES EXCEPT WHEN ADDITIONAL LANE CLOSURES ARE ALLOWED PER THE PERMITTED LANE CLOSURE SCHEDULE. ALL LANE SHIFTS AND LANE CLOSURES SHALL BE COMPLETED PER SCD MT-95.30 AND SCD MT-102.20.

COMPLETE MEDIAN WALL REPAIR, PAVEMENT REPAIRS AND RESURFACING UP TO THE INTERMEDIATE COURSE FOR THE FOLLOWING LOCATIONS:

THE INSIDE PORTION AND LEFT LANE OF LUC-75 NB FROM STA 389+88.52 TO STA 448+50.

THE INSIDE PORTION AND LEFT LANE OF LUC-75 SB FROM STA 366+88.33 TO STA 448+50.

STAGE 3: THIS WORK SHALL BE LIMITED TO 10 DAYS. I-75 NB & I-75 SB SHALL REMAIN A MINIMUM OF TWO LANES IN EACH DIRECTION EXCEPT WHEN ADDITIONAL LANE CLOSURES ARE ALLOWED PER THE PERMITTED LANE CLOSURE SCHEDULE. ALL LANE SHIFTS AND LANE CLOSURES SHALL BE COMPLETED PER SCD MT-95.30 AND SCD MT-102.20.

COMPLETE PAVEMENT REPAIRS AND RESURFACING UP TO THE INTERMEDIATE COURSE FOR THE FOLLOWING LOCATIONS:

THE CENTER LANE, RIGHT LANE, AND OUTSIDE PORTION OF LUC-75 NB FROM STA 389+88.52 TO STA 448+50.

THE CENTER LANE, RIGHT LANE, AND OUTSIDE PORTION OF LUC-75 SB FROM 366+88.33 TO STA 448+50.

STAGE 4: THIS WORK SHALL BE LIMITED TO 21 DAYS. A MINIMUM OF TWO LANES IN EACH DIRECTION MUST BE MAINTAINED AT ALL TIMES EXCEPT WHEN ADDITIONAL LANE CLOSURES ARE ALLOWED PER THE PERMITTED LANE CLOSURE SCHEDULE. LANE SHIFTS AND LANE CLOSURES SHALL BE COMPLETED PER SCD MT-95.30 AND SCD MT-102.20.

COMPLETED THE PAVEMENT REPAIRS AND RESURFACING UP TO THE INTERMEDIATE COURSE FOR THE INSDIE PORTION FOR I-75 NB & S FROM STA 450+64.22 TO STA 539+50.

STAGE 5: THIS WORK SHALL BE LIMITED TO 21 DAYS. A MINIMUM OF TWO LANES IN EACH DIRECTION MUST BE MAINTAINED AT ALL TIMES EXCEPT WHEN ADDITIONAL LANE CLOSURES ARE ALLOWED PER THE PERMITTED LANE CLOSURE SCHEDULE. LANE SHIFTS AND THE LANE CLOSURES SHALL BE COMPLETED PER SCD MT-95.30 AND SCD MT-102.20.

COMPLETE THE PAVEMENT REPAIRS AND RESURFACING UP TO THE INTERMEDIATE COURSE FOR THE OUTSIDE PORTION OF LUC-75 NB AND SB FROM STA. 450+64.22 TO STA. 593.50.

STAGE 6: PLACE THE FINAL SURFACE COURSE. A MINIMUM OF 3 LANE MUST BE MAINTAINED AT ALL TIMES FROM 7AM TO 7PM. THE PERMITTED LANE CLOSURE SCHEDULE SHALL BE FOLLOWED ALL OTHER TIMES.

INSTALL PERMANENT PAVEMENT MARKINGS AND ALL OTHER SAFTEY ITEMS.

# CONSTRUCTION SEQUENCING FOR LUC-475.

STAGE 1: JOINT REPAIRS

FOR THREE (3) WEEKENDS, CONTRACTOR WILL BE ALLOWED MOT LANE REDUCTION AND CLOSURE PERIODS AS DETAILED BELOW TO PERFORM JOINT REPAIR WORK IN LIEU OF ODOT PERMITTED LANE CLOSURE TABLE TIMEFRAMES ALL WORK NOT COMPLETED DURING THESE 3 WEEKENDS WILL BE SUBJECT TO ODOT'S PERMITTED LANE CLOSURE TABLE. I 475 EXIT RAMPS TO PROMEDICA PARKWAY SHALL REMAIN OPEN AT ALL TIMES.

MILL AND FILL RUMBLE STRIPS PRIOR TO PERFORMING JOINT REPAIR WORK. (TO BE PREFORMED PRIOR TO THE START OF THESE 3 WEEKENDS)

WEEKEND 1: WB REPAIRS ON I-75/I-475 WB SYSTEM RAMPS, UP TO STATION 344+00 +/-.

-CLOSE THE SYSTEM RAMP FROM I-75 NB TO I-475 WB FOR HALF OF THE WEEKEND TO COMPLETE REPAIRS ALONG THE RAMP. REOPEN 175 NB TO I-475 WB SYSTEM RAMP TO WB 475, PRIOR TO PERFORMING 2ND HALF OF WEEKEND WORK. -CLOSE THE SYSTEM RAMP FROM I-75 SB TO I-475 WB FOR WTHE REMAINING HALF OF THE WEEKEND TO COMPLETE REPAIRS ALONG THE RAMP. (THIS RAMP CLOSURE MUST START BETWEEN THE HOURS OF 9PM AND 6AM.)

WEEKEND 2: REPAIRS ON WB I-475 MAINLINE -CLOSE THE SYSTEM RAMP FROM I-75 NB TO I-475 WB. -PERFORM THE PAVEMENT REPAIRS (STATION 344+00+/- TO WEST END OF PROJECT). MAINTAIN MINIMUM TWO OPEN LANES OF TRAFFIC BY USE OF RIGHT SHOULDER AND RIGHT LANE ON I-475 WB. CONTRACTOR MAY REMOVE RIGHT LANE EDGE LINE UP TO 48 HOURS PRIOR TO THE START OF WEEKEND WORK. INSTALL NO EDGE LINE SIGNS. PLACE DRUMS IN CLOSED SHOULDER AREA. PLACE TEMP LANE LINE BETWEEN SHOULDER AND RIGHT LANE, PRIOR TO SHIFTING TRAFFIC TO SHOULDER FOR WEEKEND WORK. AT CONCLUSION OF WEEKEND WORK, RE-CLOSE SHOULDER WITH DRUMS. PLACE TEMPORARY EDGE LINE WITHIN

48 HOURS OF COMPLETING WEEKEND WORK. -THE RAMP FROM PROMEDICA TO I-475 WB WILL NEED TO BE CLOSED TO REDUCE CONGESTION

WEEKEND 3: REPAIRS NEAR DOUGLAS RD OFF RAMP AND PROMEDICA ON RAMP -CLOSE THE RIGHT LANE OF I-475 AFTER THE PROMEDICA EXIST RAMP TO MAKE REPAIRS ON THE PROMEDICA ENTRANCE RAMP, DOUGLAS EXIST RAMP, AND RIGHT LANE OF I-475 WB. -I-475 EB: REDUCE TO ONE LANE (FROM WEST END OF PROJECT UP TO STATION 287+00) FOR 24 HOURS DURING ONE OF THE THREE ALLOTTED WEEKENDS TO COMPLETE THE PAVEMENT REPAIRS. ALL LANES OF EB 475 SHALL REOPEN AS SOON AS JOINT REPAIR MORRIS DOMPLETED. TO THE

RAMPS AT PROMEDICA PKWY. WILL INCLUDE ONE OVERNIGHT CLOSURE FROM 9PM TO 6AM. DAMAGES WILL OCCUR AT \$2500/15 MINUTES FOR EACH 15 MINUTES THE RAMP REMAINS CLOSED AFTER THE DURATION.

ONLY ONE PROMEDICA PKWY. RAMP SHALL BE CLOSED AT A TIME. THE RAMPS ON PROMEDICA PKWY. SHALL NOT BE CLOSED AT THE SAME TIME AS THE RAMPS AT DOUGLAS RD.

PROMEDICA PKWY. EB EXIT RAMP DETOUR: SECOR RD. TO MONROE ST. TO PROMEDICA PKWY.

PROMEDICA PKWY, WB FXIT RAMP DETOUR: WB-475 TO DOUGLAS RD. TO EB-475 TO PROMEDICA PKWY.

PROMEDICA PKWY. EB ENTRANCE RAMP DETOUR: WB-475 TO DOUGLAS RD. TO EB-475.

PROMEDICA PKWY. WB ENTRANCE RAMP DETOUR: PROMEDICA PKWY. TO CENTRAL AVE. TO MONROE ST. TO SECOR RD. TO WB-475.

STAGE 2: RESURFACING

RESURFACE I-475 AND COMPLETE ALL SAFETY ITEMS.

A MINIMUM OF 3 LANES MUST BE MAINTAINED AT ALL TIMES FROM 7AM TO 7PM. A MINIMUM OF 2 LANES MUST BE MAIN-TAINED ON ALL SYSTEM RAMPS AND MUST BE MAINTAINED FROM 7AM TO 7PM. THE PERMITTED LANE CLOSURE SCHEDULE SHALL BE FOLLOWED ALL OTHER TIMES.

DESCRIPTION OF		DISTNOEMTY MONEY TIME	
CRITICAL LANE/RAMP TO BE MAINTAINED			
MAINTAIN A MINIMUM 2 LANES OF I-75	/MIN	\$100	
MAINTAIN A MINIMUM OF 3 LANES OF I-75 NB DURING STAGE 2	/MIN	\$100	
ALL STAGE COMPLETED WITHIN TIMEFRAME DESCRIBED IN THE CONSTRUCTION SEQUENCING	∕HR	\$1000	\
SYSTEM RAMP CLOSURES	/15 MIN	<b>\$</b> 1000	\

**NOTE ADDED** 

		10 1-75 14 15			
I-75 NB to	This was a second and the first	I-75 NB to			
Ottawa River	This ramp may be closed up to 5	Alexis Rd to			
Rd	nights from 9PM to 6AM.	Suder Ave			
D. D.	7	Suder Ave			
Ottawa River	This ramp may be closed up to 5	to Alexis Rd			
Rd to I-75 SB	nights from 9PM to 6AM.	to I-75 SB			
. ==		I-75 NB to			
I-75 NB to	This ramp may be closed up to 5	Erie Rd to			
Alexis Rd	nights from 9PM to 6AM.	I-75 SB			
		I-75 SB to			
Alexis Rd to	This ramp may be closed up to 5	Expressway			
I-75 NB	nights from 9PM to 6AM.	Dr to I-75			
)		NB			
		I-75 SB to			
I-75 SB to	This ramp may be closed up to 5	Expressway			
Alexis Rd	nights from 9PM to 6AM.	Dr to I-75			
		I-75 NB to			
Alexis Rd to	This ramp may be closed up to 5	Erie Rd to			
I-75 SB	nights from 9PM to 6AM.	I-75 SB			
	Related to I-475 Work	. , , , , ,			
	May be closed up to one full				
I-75 NB to	weekend and up to 29 hours of an	I-75 NB to			
I-475 WB	additional weekend according to	Phillips Ave			
)	the construction sequencing.	to I-75 SB			
	May be closed up to 29 hours of	I-75 SB to			
I-75 SB to	one weekend according to the	Detroit Ave			
I-475 WB	construction sequencing.	to I-75 NB			
		Promedica			
		Pkwy to			
Promedica	May be closed for two weekends	Central Ave			
Pkwy to I-475	according to the construction	to Monroe			
WB	sequencing.	251/0			
***	sequencing.	Secor Rd.			
		48147JWA			
	May be closed for one weekend	I-475 WB to			
I-475 WB to	according to the construction	Secor Rd to			
Douglas Rd	sequencing.	I-475 EB			
	j scydenenig.	1 7/3 20			
	DETOUR RD	CHANGED			

Ramp Closure | Maintenance of Traffic Operation

I-280 NB to

I-75 NB

Ramp LL

(Manhattan

to I-75 NB)

Related to I-75 Work

May be closed up to 4 weekends

according to the construction

sequencing.

May be closed up to 4 weekends

according to the construction

sequencing.

						3nee	T NUM.	_						PA	RT.	ITEM	ITEM	GRAND	UNIT	DESCRIPTION	SEE SHEET
5	16	18	19	19A	20	21	22	23A	58	58A	59	82A	82C	01/IMS/PV	02/IMS/BR		EXT	TOTAL			NO.
																				ROADWAY	
MP									-~	<b></b>	<b></b>			LUMP	<b></b>	201	11000	-2		CLEARING AND GRUPPING	1
										3,271	***	***	***	3,271	***	202	23500	3,271	SY	WEARING COURSE REMOVED	
									1		15,035.5			15,035.5		202	38000	15,035.5	FT	GUARDRAIL REMOVED ANOTOR ASSEMBLY REMOVED, THEEL	
						-					18			18		202	42010 42041	17	EACH	ANCHOR ASSEMBLY REMOVED, TYPE T, AS PER PLAN	15
						1				1	10			10		202	12011	10	LATOIT	THE TOTAL PROPERTY OF THE TANK	10
											3			3		202	42050	3	EACH	ANCHOR ASSEMBLY REMOVED, TYPE B	
						-					35			35 LUMP		202	47000 98000	35 LS	EACH	BRIDGE TERMINAL ASSEMBLY REMOVED REMOVAL MISC.:TRAFFIC MONITORING EQUIPMENT	19A
						<del> </del>					167			167		202	15000	167	STA	RESHAPING UNDER GUARDRAIL	19A
									7.56	6.01				13.57		209	60200	13.57	STA	LINEAR GRADING	
									4		40.070			40.070		000	45050	40.070		OLIABBBBAIL TYPE NOO	
								<del>                                     </del>			13,070			13,070		606 606	15050 26150	13,070 20	FT EACH	GUARDRAIL, TYPE MGS ANCHOR ASSEMBLY, MGS TYPE E. MASH 2016	
								1/			18			18		606	26550	18	EACH	ANCHOR ASSEMBLY, MGS TYPE T	
								/			25			25		606	35002	25	EACH	MGS BRIDGE TERMINAL ASSEMBLY, TYPE 1	
								4			10			10		606	35102	10	EACH	MGS BRIDGE TERMINAL ASSEMBLY, TYPE 2	1
	93													93		622	24001	93	FT	CONCRETE BARRIER, TYPE D, AS PER PLAN	16
2														2		623	39500	2	EACH	MONUMENT BOX ADJUSTED TO GRADE	
							<del>/</del>													EROSION CONTROL	1
						<del>                                     </del>														EROSION CONTROL	1
											2,904			2,904		659	10000	2,904	SY	SEEDING AND MULCHING	
		QUAN	ITITIES	5		/					0,39			0.39		659	20000	0,39	TON	COMMERCIAL FERTILIZER	
		REVIS				1					16			1,000		659 832	35000 30000	1,000	MGAL EACH	WATER EROSION CONTROL	
														1,000		332		1,555			
																				PRANTOS	
						-														DRAINAGE	1
8						1								8		611	98630	8	EACH	CATCH BASIN ADJUSTED TO GRADE	<del> </del>
3														3		611	99654	3	EACH	MANHOLE ADJUSTED TO GRADE	
																					-
																				PAVEMENT	
	~		~	~	<b>\</b>		$\leftarrow$	<b>/</b>	041 05		~	~	~	241,995	m	254	01000	241,995	SY	PAVEMENT PLANING, ASPHALT CONCRETE, § 1/4"	1
<b>-</b>	4,540								241,333					4,540		254	01000	4,540	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 1 1/2"	
	ىد	ىد	ىد	بد	ىد	ببا	ىد	ببا	ىيا	14,328	سا	w	ىد	14,328	ىد	254	01000	14,328	SY	PAVEMENT RLANING ASRHALT CONCRETE, 1"	
-(	4,860				77		Y						XX	4 860		255 255	20000	4,860	FT	FULL DEPTH PAVEMENT SAWING	16
7	387								33,879	8,972				43,238		407	20000	43,238	GAL	NON-TRACKING TACK COAT	
	とく		<b>SAN</b>	A A			<b>VV</b>	*	<b>VV</b>	2,932			S	3,032		424	12001			PINE GRADED POLYMER ASPHALT CONCREVE, MYPE'B, AG PER PLAN	
<del>-(</del>	100 190	•			<b>, ,</b>	<del>}                                    </del>	<b>'''</b>	<del>} ``</del>	<b>, , ,</b>	2,932	, , ,	, ,	, , ,	190		441	50000	3,0 <b>8</b> 2 190	CY	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), PG64-22	16
						1		1	15,202					15,202		442	00100		CY		
			<b>\</b>		$\sim$	$\sim$	$\sim$	$\omega$	10,085		$\sim$		$\sim$	10,085	$\sim$	442	00100	15,202 10,083		ANTI-SEGREGATION EQUIPMENT ASPHALT CONCRETE SURPACE COURSE, 12.5 Mm, 1 YPE A (447), AS PER PLAN	16
			277				11		11,764					12,041		442	10101	12,041	CY	ASPHALT CONCRETE INTERMEDIATE COURSE, 19 MM, TYPE A (446), AS PER PLAN, PG72-2	22M 16
			211		<u> </u>		1/		693	480	<u> </u>	<u> </u>		1,173		617	10101	1,173	CY	COMPACTED AGGREGATE	1,1,10
			2,774				1/							2,774		SPECIAL	69012060	2,774	SY	PAVEMENT OVERLAY FABRIC COMPOSITE	19
		<u> </u>	3,330		1		<del>/</del>	-		5,867	-	-		5,867 3,330	-	875 897	10000 01010	5,867 3,330	LB SY	LONGITUDINAL JOINT ADHESIVE PAVEMENT PLANING, ASPHALT CONCRETE, CLASS A, 3"	<del> </del>
			0,000			/	1							0,000		007	31010	0,000		TATEMENT LEMMO, NOT TIME TOURONETE, OLDOOD, O	1
								1							-					WATER WORK	1
		Ql	JANTIT	IES		<del>                                     </del>		1			<del>                                     </del>	<del>                                     </del>		2	<del>                                     </del>	638	10800	2	EACH	VALVE BOX ADJUSTED TO GRADE	+
2			VISED															_		_	
2		RH	, <b> </b>			_															
2		RE				1	i	1	1	I										LIQUETINO	1
2		RE			1								1	1				I		I IGHTING	
2		RE																		LIGHTING	
2		102												102		625	26253	102	EACH	LUMINAIRE, CONVENTIONAL, SOLID STATE (LED), AS PER PLAN, COOPER GALLEON OR TY	
2														102 1 LUMP		625 625 SPECIAL	26253 34001 62540000	102 1 LS	EACH EACH		YPE H103LOF 18 18

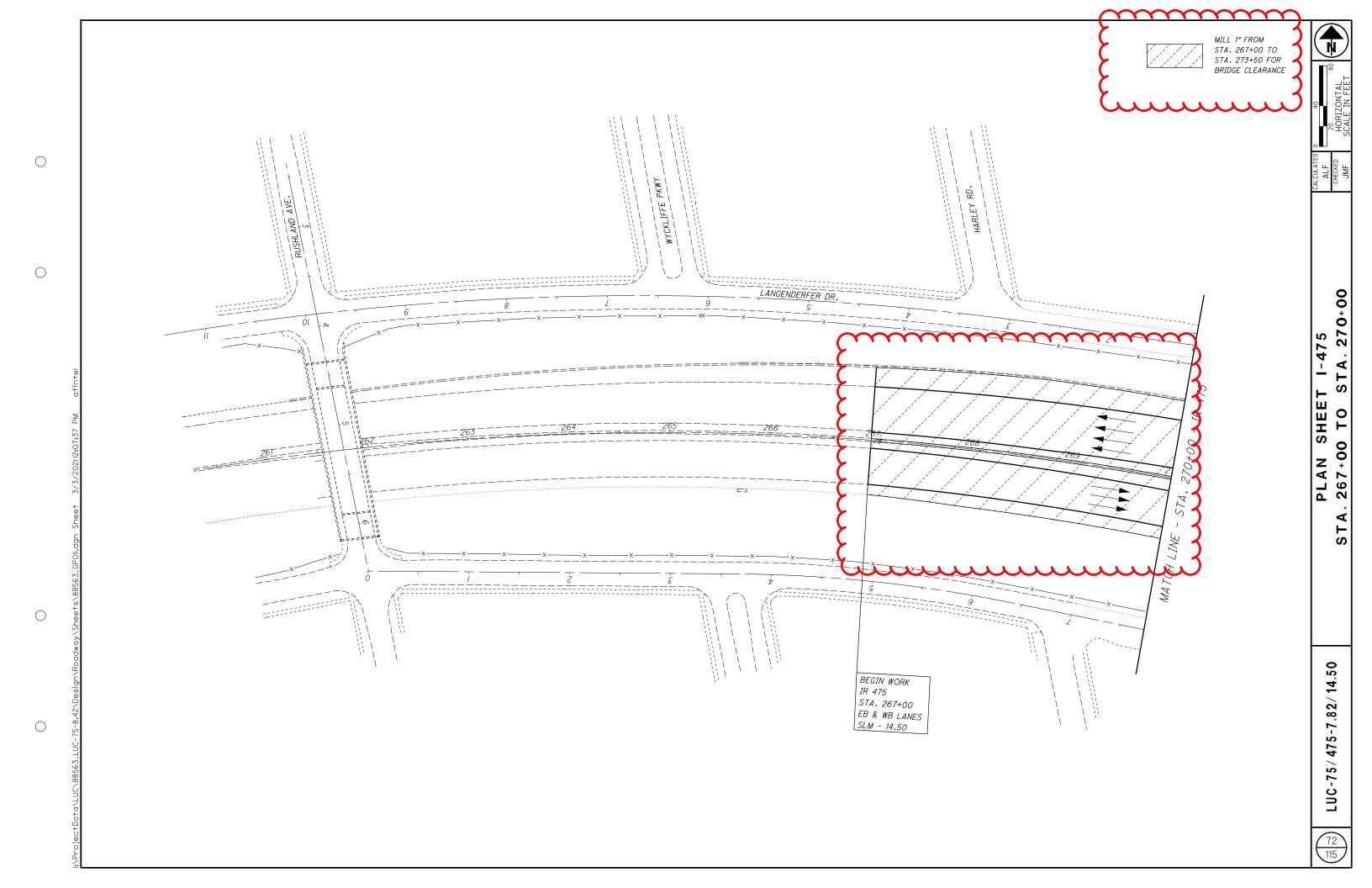
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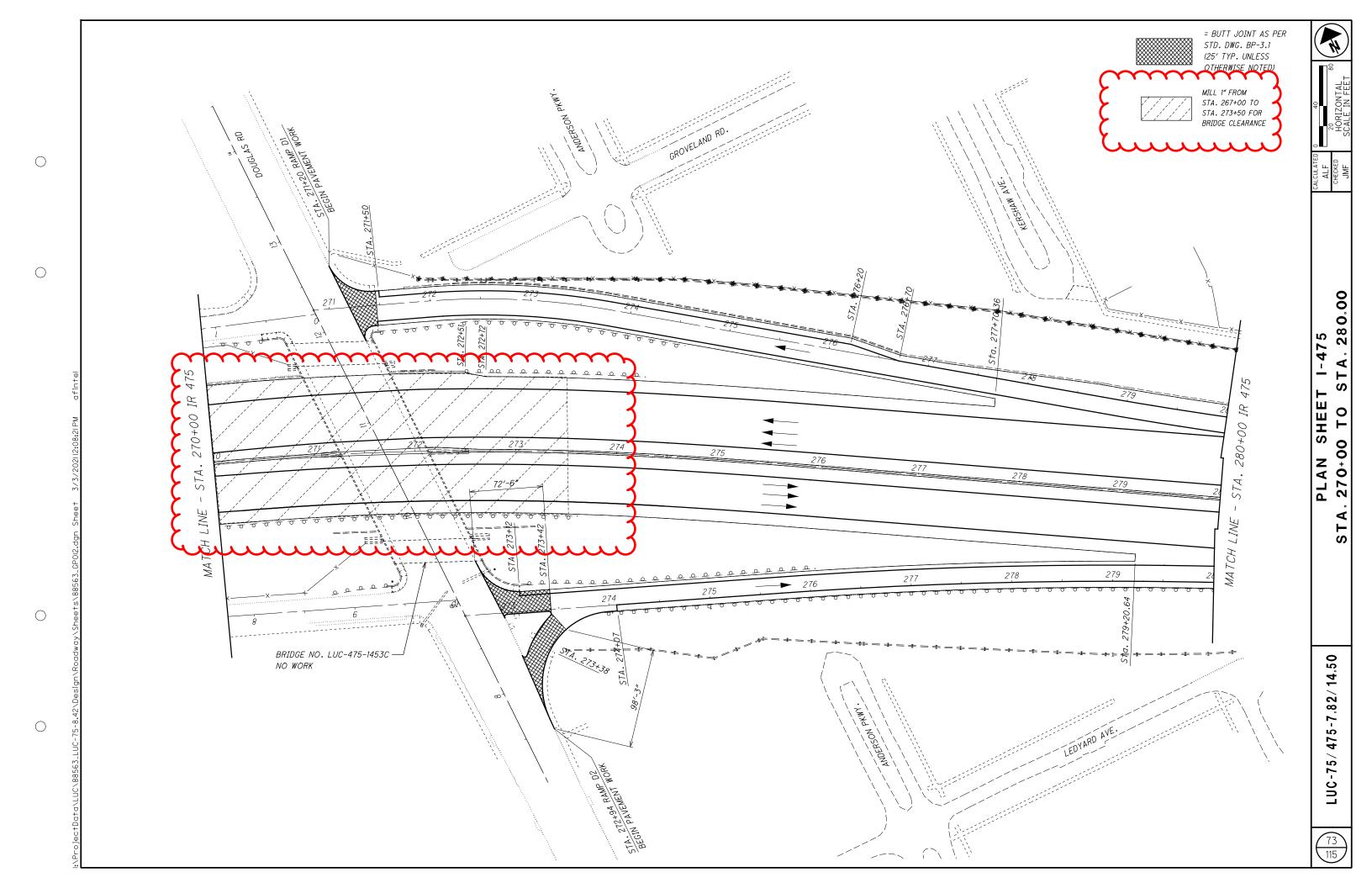
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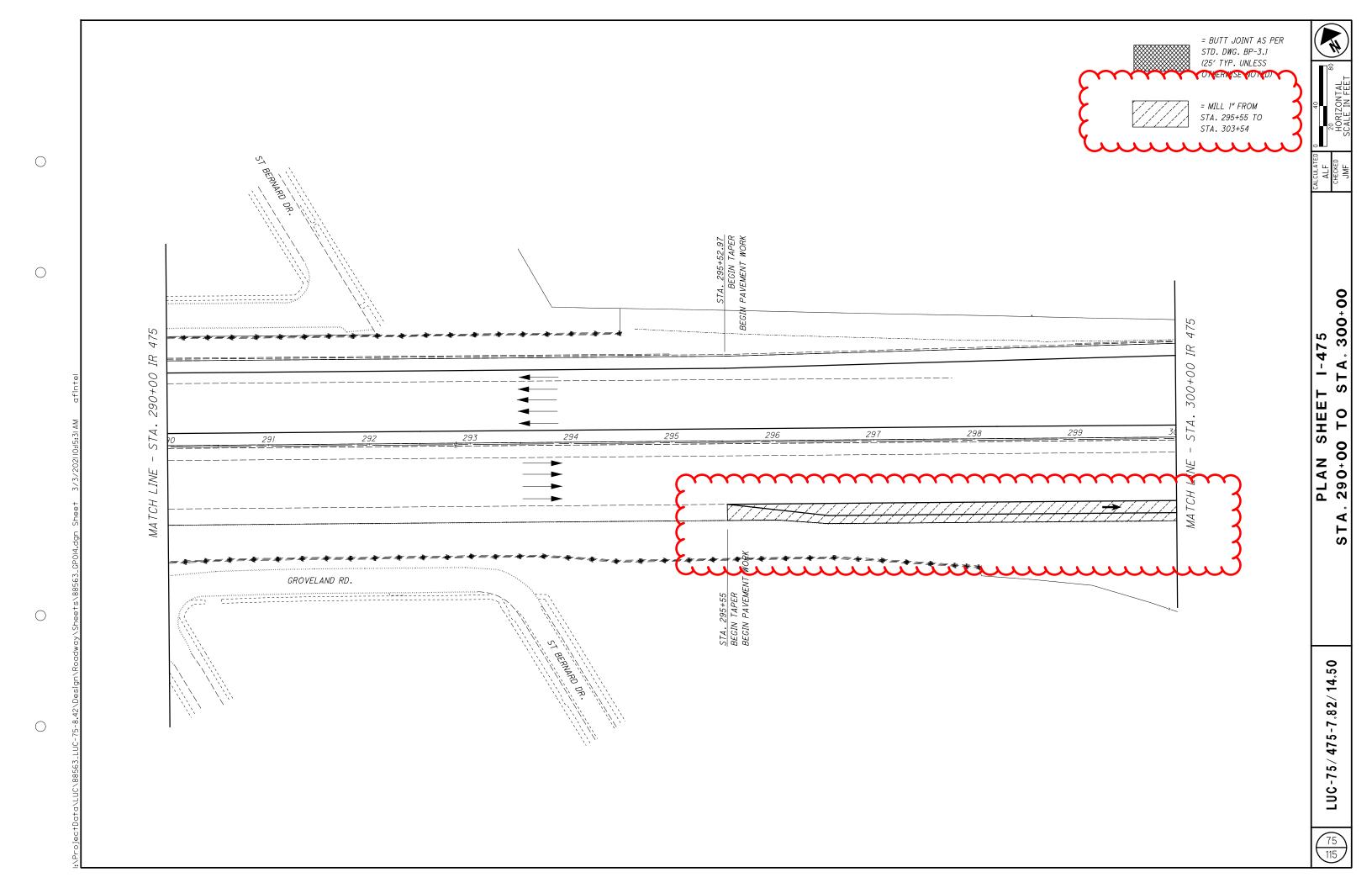
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					T							$\overline{\gamma}$								
					_			AREA	202	-39		254	407		424	617	875			ATED F
					TYPICAL SECTION				ш			ن <u>ت</u> ق	<b>₹</b>		POLYMER ;RETE, TYPE PLAN, 1"	EGATE	<u> </u>			ALCUL CHEC
				111	ECT	DISTANCE (D)	AVERAGE WIDTH (W)	GENERATED	URS			PAVEMENT PLANING ASPHALT CONCRETE,	<b>4</b>		OLYI AN, 1	3RE(	LONGITUDINAL JOINT ADHESIVE			75
	STAT	TION RAI		SIDE	L S	T (C)	ERA MD1 (W)	ER/	3 CO IOVE	4		ONC	<b>4</b>		(ADDED POLY) CONCRETE, PER PLAN,	) AGGR	DINAL			
		I-475	)		2		\ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \	GEN	ARING COU!	*		ILT C	\$		RADI T CO S PE	ACTED	ADH			
					🖺				WE/	賃		SPHA	<u>}</u>		VE GR. HALT B, AS	MPA	ONO:			
								CADD		2(		A SA	<b>)</b>		FINE (ASPHA)	COMP				
						FT	FT	SY	SY	N			GAL		CY	CY	LB			
	267+00 279+20.64	TO TO	279+20.64 282+68.55	EB EB	E	1220.64 347.91	57.3 67.0	7765 2590		-30	76 2		660 <b>2</b> 20		216 72	45 13	1526 435			
$\bigcirc$	282+68.55	TO	286+96	EB	F	427.45	78.5	3728	212	<del>- 3</del> (			3 7		104	2	107			
										27			1							
	295+55	ТО	300+50.18	EB	F	495.18	18.0	990		<b>{</b> ₹		768	4		28	2	124			
	267+00	ТО	272+51	WB	Α	551.00	76.00	4653		- 36	21		3 5		129	20	138			5
	272+51	TO	272+72	WB	Α	21.00	74.00	173		37	1	5000	<b>1</b> 5		5	1	5			47
	272+72	ТО	277+70.36	WB	Α	498.36	72.00	3987		<b></b>	9		<b>3</b> 39		111	18	125			<u> </u>
	277+70.36 282+44.38	TO TO	282+44.38 295+52.97	WB WB	B	474.02 1308.59	72.00 84.00	3792 12214		3	50		322 1038		105 339	18 48	119 327			,
$\bigcirc$	295+52.97	TO	302+03.11	WB	В	650.14	88.50	6393		<b>-3</b> >			543		178	70	321			SN
	302+03.11	ТО	304+53.57	WB	В	250.46	74.00	2059		3			<b>4</b> 75		57	9	63			
	304+53.57 318+25	TO	316+51.29 319+14.24	WB WB	C	1197.72 89.24	75.00 78.00	9981 773	253 253	3			848 66		277 21	3	299 22			🚊
	319+14.24	TO	319+14.24	WB	C	202.80	69.00	1555	200	3	)3 8		32 132		43	8	51			໔
	321+17.04	ТО	322+56.44	WB	С	139.40	60.00	929	462	36			<b>7</b> 9		26	5	35			
	322+56.44	TO	322+89.46	WB	С	33.02	80.00	294	464	_39			<b>2</b> 5		8	1	8	_	QUANTITIES	00
$\overline{\Phi}$	326+53.93 330+24	TO	330+24 330+66.69	WB WB	C D	370.07 42.69	80.00 76.00	3290 360		30			230 21		91	7 2	185 43		MODIFIED	-
fint	330+66.69	TO	340+91.69	WB	D	1025.00	72.00	8200		36			697		228	38	+0			   🗸
Ö	340+91.69	ТО	345+41.69	WB	D	450.00	75.50	3775		<b>-3(</b>	7	2917	321		105	17				0
M	345+41.69	ТО	349+85.34	RAMP NW	Н	443.65	41.00	2021		-)(	7		1/2		56	8	222			⊢
39 7	349+85.34	ТО	353+16.5	RAMP NW	Н	331.16	52.25	1923	324	4	3		1)3		53	6	166			Z
:47:									-				)							<b>■</b>
02110	345+41.69 349+85.34	TO TO	349+85.34 354+40.8	RAMP SW RAMP SW	G	443.65 455.46	45.50 39.25	2243 1986	113	-33	7		191 169		62 55	16 17	111			
3/20	01010001		00111010			100110	00120	1000	110	7(			<u> </u>							>
3,	271+20	TO	271+50	RAMP D1	l I	30.00	58.50	195	149	30		4	<b>7</b>		5	1	8			<u>4</u>
+ 0	271+50 276+20	TO TO	276+20 276+70	RAMP D1	l	470.00 50.00	34.00 30.00	1776 167		-36	32		451 14		49 5	17	118 25			-
She	276+70	TO	277+70.36	RAMP D1	i	100.36	27.00	301		20			26		8	4	50			
L D	277+70.36	ТО	282+44.38	RAMP D1	ı	474.02	24.00	1264		2.7	8		107		35	9	237			
102.4	272+36	ТО	274+07	RAMP D2	1	171.00	42.50	808	142	36	16		<b>6</b> 9		22	3	43			
089	272+36	TO	274+07	SLIP	J	171.00	33.00	627	142	3		-	33		17	3	43			
563.	274+07	ТО	279+20.64	RAMP D2	J	513.64	26.00	1484		2)	9		<b>1</b> 26		41	10	128			
88	279+20.64	ТО	282+68.55	RAMP D2	J	347.91	27.00	1044		<del>-{}</del>	3		<del>4</del> 9		29	13	87			
0 6 + 8	302+03.11	ТО	304+53.57	RAMP D3	K	250.46	25.00	696		<b>-3</b>	9		39		19	9	63			
She	304+53.57	ТО	310+20	RAMP D3	K	566.43	26.00	1636	73		7		<b>4</b> 39		45	21	142			
\ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \ \	244+20	TO	244.70	DAMP D4	1	E0 00	70.05	10F	100	_{}}	12		37		10	4	40			
, PDC	311+20 311+72	TO	311+70 313+91	RAMP D4	L	50.00 219.00	78.25 34.00	435 827	196	3	)2 /8		<del>3</del> / <b>1</b> 0		12 23	1 4	13 55			
) NRC	314+46.05	ТО	315+49.59	RAMP D4	L	103.54	34.00	391	97	0.0	<b>)</b>		<b>3</b>		11	2	26			4.5
ısigr.	313+91	TO	314+46.05	RAMP D4	L	55.05	31.00	190	108		12		<b>1</b> 6		5	1	14			+
∑\De	317+35.95 319+14.24	TO TO	319+14.24 321+17.04	RAMP D4	M,N N	178.29 202.80	28.00 35.50	555 800		30			47 8		15 22		45 51			.82
8.42	321+17.04	TO	322+56.44	RAMP D4	N	139.40	23.00	356		<b>1</b>	15		<b>2</b> 0		10		35			<b>'</b> -
-75-8										36			<b>)</b>		0	0	0			75
LUC	300+50.18 301+90	TO	301+90 303+54	RAMP D5	0	139.82 164.00	26.00 36.50	404 665		30	05	1113	34 7		11 18	3	35 41			47
563_	303+54	TO	303+54	RAMP D5	0	347.00	27.00	1041		₹(			8		29	6	87			75/
7885	307+01	ТО	307+51	RAMP D5	0	50.00	30.00	167		<b>3</b> (			14		5	1	13			'-5
LUC	307+51	TO	310+00	RAMP D5	0	249.00	34.00	941	202	30			30		26	5	62			
/D+	310+00	ТО	310+58	RAMP D5	0	58.00	84.25	543	203	<del>- 3</del> 8	_		<b>3</b> 6		15	1	15			-
+Da	311+19	ТО	316+93	RAMP D6	P,Q	574.00	26.00	1658	73		22		1)11		46	11	144			
) jec	316+93	TO	317+30.29	RAMP D6	Q	37.29	28.00	116		<b>₹</b>			0		3	1	9			58A
Pro	318+90.43	TO	321+31.65	01/STR/PV	Q TOTALS	241.22 CARRIED T	29.67 O GENERAL	795 SUMMARY	3271	-36			48 8972		22 2932	480	60 5867			115
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STA. 310+00

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LUC-75/475-7.82/14.50

