

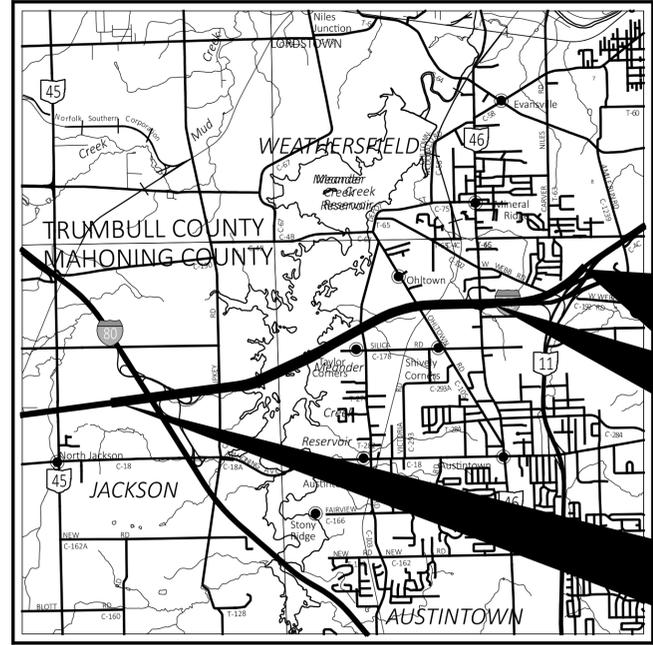
MAH-76/80-8.50/0.00

MODEL: Sheet PAPER/DATE: 3/18/2026 TIME: 14:39:39 PLTDRV: OHDOT_PDF.plt PENTBL: 122947_OHDOT_UTPen_Rev.tbl USER: Matthew.Phillips@dot.ohio.gov WORKSPACE: OHDOTCEV02_WORKSET: 122947_PRODUCT: OpenRoadsDesigner 24.00.00.205 p:\ohdot\p-w-bentley.com\ohdot\p-w-02\Documents\01 Active Projects\District 04\Maining\122947\400-Engineering\Roadway\Sheets\122947_GT001.dgn

STATE OF OHIO DEPARTMENT OF TRANSPORTATION

MAH-76/80-8.50/0.00

JACKSON TWP AND AUSTINTOWN TWP
MAHONING COUNTY



LOCATION MAP
LATITUDE: N 41°07'15" LONGITUDE: W 80°47'35"

END PROJECT
STA. 723+61.63
SLM 5.08

END WORK EASTBOUND
STA. 665+27.00

BEGIN PROJECT
STA. 445+00.00
SLM 8.50

PORTION TO BE IMPROVED	=====
INTERSTATE HIGHWAY	=====
FEDERAL ROUTES	=====
STATE ROUTES	=====
COUNTY & TOWNSHIP ROADS	=====
OTHER ROADS	=====

	I.R. 76 (BAILEY TO OTIC)	I.R. 80 (OTIC TO S.R. 46)	I.R. 80 (S.R. 46 TO S.R. 11 SB RAMP)
CURRENT ADT (2030)	40,500	54,500	60,000
DESIGN YEAR ADT (2050)	41,500	60,500	67,500
DESIGN HOURLY VOLUME (2050)	4,600	6,700	7,600
DIRECTIONAL DISTRIBUTION	50%	50%	50%
TRUCKS (24 HOUR B&C)	30%	38%	37%
DESIGN SPEED	75 MPH	70 MPH	70 MPH
LEGAL SPEED	70 MPH	65 MPH	65 MPH
DESIGN FUNCTIONAL CLASSIFICATION:			
01 INTERSTATE (RURAL AND URBAN)			
NHS PROJECT	YES		

DESIGN EXCEPTIONS

NONE

ADA DESIGN WAIVERS

NONE REQUIRED

UNDERGROUND UTILITIES
Contact Two Working Days
Before You Dig

OHIO811.org
Before You Dig

OHIO811, 8-1-1, or 1-800-362-2764
(Non members must be called directly)

PLAN PREPARED BY:
ODOT --- DISTRICT 04, CAPITAL PLANNING
2088 S. ARLINGTON ROAD
AKRON, OHIO 44306

INDEX OF SHEETS:

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STANDARD CONSTRUCTION DRAWINGS						SUPPLEMENTAL SPECIFICATIONS		SPECIAL PROVISIONS	
BP-2.3	7/18/14	MGS-5.2	7/15/16	MT-99.20	4/19/19	800-2023	7/18/25		
BP-3.1	1/19/24	MGS-5.3	7/15/16	MT-99.50	7/18/25	807	1/17/25		
BP-4.1	7/19/13	MGS-6.1	1/19/18	MT-101.60	1/17/25	808	7/19/24		
BP-5.1	7/18/25	MGS-6.2	7/18/25	MT-101.90	7/17/20	814	7/15/16		
BP-9.1	1/18/19			MT-102.20	4/19/19	820	7/15/22		
DM-1.1	1/17/25	RM-4.2	7/18/25	MT-104.10	1/19/24	821	4/20/12		
DM-1.2	1/17/25	MT-95.30	7/18/25	MT-105.10	1/17/20	832	7/18/25		
DM-4.3	1/15/16	MT-95.45	7/21/23	TC-22.20	7/18/25	850	7/21/23		
DM-4.4	1/15/16	MT-95.50	7/21/17	TC-61.30	7/19/24	905	1/17/25		
		MT-97.12	7/18/25	TC-65.10	1/17/14	908	1/17/25		
MGS-2.1	7/18/25	MT-98.10	1/17/20	TC-65.11	1/17/25	914	7/15/16		
MGS-3.1	7/18/25	MT-98.11	1/17/20			921	7/19/24		
MGS-3.2	7/18/25	MT-98.20	4/19/19						
MGS-4.2	7/18/25	MT-98.22	1/17/20						
MGS-4.3	7/18/25	MT-98.29	1/17/20						

FEDERAL PROJECT NUMBER

E250 (884)

RAILROAD INVOLVEMENT

NONE

PROJECT DESCRIPTION

CONCRETE PAVEMENT REHABILITATION OF I.R. 76/80 FROM JUST WEST OF THE OHIO TURNPIKE TO S.R. 46 (EASTBOUND) AND S.R. 11 (WESTBOUND) UTILIZING CRACKING AND SEATING WITH AN ASPHALT OVERLAY. IMPROVEMENTS INCLUDE MINIMAL STORM SEWER ADJUSTMENTS, PAVEMENT MARKINGS AND SIGNING.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA:	30.19 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA:	1.00 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA:	31.19 ACRES

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

2023 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY EXCEPT AS NOTED ON SHEET P.18, AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

Arthur G. Noirot Jr.
Arthur G. Noirot Jr., P.E.
District 04 Deputy Director

Pamela Boratyn
Pamela Boratyn
Director, Department of Transportation

R3
3-18-26: SS814 AND SS914 ADDED; NEW MOT SHEETS ADDED; SEALS REVISED

ENGINEER'S SEAL MAINTENANCE OF TRAFFIC	ENGINEER'S SEAL REMAINING PLAN SHEETS

TITLE SHEET

DESIGN AGENCY	
DESIGNER	MEP
REVIEWER	TJP
DATE	3/18/26
PROJECT ID	122947
SHEET	P.1
TOTAL	87

SEQUENCE OF CONSTRUCTION

WHEN WORKING IN THE EASTBOUND DIRECTION, IT IS ANTICIPATED THAT THE CONTRACTOR WILL BEGIN WEST OF THE OTIC INTERCHANGE AND PROGRESS EASTBOUND THROUGH THE PROJECT LIMITS. LANE CLOSURES AND WORK NEAR THE OTIC INTERCHANGE WILL BE PERFORMED IN ACCORDANCE WITH THE PERMITTED LANE CLOSURE SCHEDULE AND THE CONDITIONS GRANTED IN THE "APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTIONS" NOTE.

WHEN WORKING IN THE WESTBOUND DIRECTION, IT IS ANTICIPATED THAT THE FIRST PORTION OF THE CONTRACTOR'S WORK WILL BEGIN EAST OF THE I.R. 80 / I.R. 680 / S.R. 11 INTERCHANGE AND PROGRESS WEST TO THE OHLTOWN ROAD BRIDGE. THE WEST PORTION OF THE WORK WILL BE PHASED TO MAINTAIN I.R. 80 WB TRAFFIC WHILE REQUIRING RAMP CLOSURES AS DISCUSSED IN THE "WORK ZONE RESTRICTIONS" NOTE. WESTBOUND WORK FROM THE MEANDER BRIDGE TO THE WEST PROJECT LIMITS MAY BE PERFORMED INDEPENDENTLY OF OTHER WESTBOUND WORK.

AFTER COMPLETION OF THE WESTBOUND WORK THROUGH OHLTOWN ROAD THE CONTRACTOR WILL PROCEED WEST TOWARDS THE OTIC INTERCHANGE. LANE CLOSURES AND WORK NEAR THE OTIC INTERCHANGE WILL BE PERFORMED IN ACCORDANCE WITH THE PERMITTED LANE CLOSURE SCHEDULE AND THE CONDITIONS GRANTED IN THE "APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTIONS" NOTE.

WORK ZONE RESTRICTIONS

THE CONTRACTOR IS PERMITTED TO SHIFT TWO LANES OF TRAFFIC IN ACCORDANCE WITH SCD MT-102.20 ONTO THE ROADWAY SHOULDERS TO COMPLETE PHASED WORK. THE MINIMUM MAINTAINED LANE WIDTH SHALL NOT BE LESS THAN 11' WHILE MAINTAINING AT LEAST A 2' OFFSET FROM THE OUTSIDE EDGE OF SHOULDER. WORK ZONE STRIPING QUANTITIES TO DEFINE SHIFTED LANES DURING MULTI-PHASE CONSTRUCTION HAVE BEEN PROVIDED.

DUE TO THE RESTRICTIONS OF THE PERMITTED LANE CLOSURE SCHEDULE, IT IS EXPECTED THAT PORTIONS OF THE WORK WILL BE COMPLETED WITHIN A DEDICATED WORK ZONE WHILE OTHER PORTIONS OF THE WORK WILL BE PERFORMED DURING ALLOWABLE PLCS CLOSURES AND TIMEFRAMES ALLOWABLE BY THE APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTIONS LISTED IN THIS PLAN.

WHEN WORKING ON THE TWO-LANE SEGMENTS IN THE VICINITY OF THE OTIC INTERCHANGE, THE CONTRACTOR SHALL BE PERMITTED TO REDUCE EASTBOUND TRAFFIC TO A SINGLE LANE TWICE AND SHALL BE PERMITTED TO REDUCE WESTBOUND TRAFFIC TO A SINGLE LANE TWICE. THE DURATION OF EACH SINGLE LANE CONDITION SHALL BE LIMITED FROM 5:00PM SUNDAY THROUGH 12:00PM THURSDAY. LANE CLOSURES TO A BOUND WITHIN THE TWO-LANE SECTION SHALL BE PERFORMED OVER CONSECUTIVE WEEKS. THE PERMITTED SINGLE LANE CONDITION SHALL BE LIMITED TO THE TWO-LANE SEGMENTS IN THE VICINITY OF THE OTIC INTERCHANGE. AREAS OUTSIDE OF THE TWO-LANE SEGMENTS NEAR THE OTIC INTERCHANGE SHALL BE SUBJECT TO THE PLCS RESTRICTIONS. EASTBOUND AND WESTBOUND CLOSURES ARE NOT REQUIRED TO BE PERFORMED CONCURRENTLY. WITH EACH SINGLE LANE CLOSURE, THE CONTRACTOR SHALL PROVIDE FIVE PORTABLE CHANGEABLE MESSAGE SIGNS AT APPROXIMATELY TWO-MILE INTERVALS BEGINNING ROUGHLY ONE MILE FROM THE START OF EACH CLOSURE. A QUANTITY OF PORTABLE CHANGEABLE MESSAGE SIGNS HAS BEEN PROVIDED IN THE PLANS. LEOS SHALL BE STATIONED TO ALERT TRAFFIC OF QUEUED TRAFFIC DURING PEAK HOURS.

WITH THE EXCEPTION OF THE PERMITTED I.R. 80 WB TO OTIC RAMP 5-DAY CLOSURE, THE CONTRACTOR SHALL BE REQUIRED TO MAINTAIN ONE WESTBOUND LANE AND ONE EXIT-ONLY LANE TO THE OTIC RAMP WHEN WORKING ON THE WESTBOUND TWO-LANE SECTION NEAR THE OTIC INTERCHANGE. WESTBOUND ACCESS TO THE OTIC SHALL NOT BE RESTRICTED WHEN WORKING IN THE WESTBOUND, TWO-LANE SECTION.

TWO-LANE SEGMENTS AFFECTED BY THIS WORK SHALL BE PHASED TO ENSURE THAT THE ENTIRE LENGTH OF THE SEGMENT IS CONSTRUCTED FULL-WIDTH WITHIN THE PERMITTED 5:00PM SUNDAY THROUGH 12:00PM THURSDAY CLOSURE PERIOD. BY THE END OF EACH PERMITTED CLOSURE PERIOD, THE FULL-WIDTH AND LENGTH OF EACH TWO-LANE SECTION SHALL BE CAPABLE OF MAINTAINING TWO-LANES OF UNRESTRICTED TRAFFIC. IT IS ANTICIPATED THAT THE FIRST CLOSURE OF A BOUND WILL BE UTILIZED TO CRACK AND SEAT PAVEMENT AND TO PLACE THE REQUIRED 3" INTERMEDIATE COURSE (19 MM) AS PER ITEM 321 - CRACKING AND SEATING NON-REINFORCED CONCRETE PAVEMENT, AS PER PLAN. IT IS ANTICIPATED THAT THE SECOND CLOSURE SHALL BE UTILIZED TO PLACE THE 1.75" INTERMEDIATE COURSE (12.5 MM).

TRAFFIC SHALL NOT BE PERMITTED TO TRAVEL UPON CRACKED/SEATED PAVEMENT PRIOR TO PAVING. FOR PAVING REQUIREMENTS SPECIFIC TO THIS PROJECT, REFER TO PLAN NOTE ITEM 321 - CRACKING AND SEATING NON-REINFORCED CONCRETE PAVEMENT, AS PER PLAN. WHEN PERFORMING CRACKING AND SEATING WORK, THE CONTRACTOR SHALL LIMIT THE LENGTH OF CRACKING AND SEATING TO ENSURE THAT THE FIRST LIFT OF ASPHALT CONCRETE INTERMEDIATE COURSE (19 MM, 3") MAY BE PAVED AND APPROPRIATELY STRIPED PRIOR TO OPENING THE ROADWAY TO TRAFFIC. THE CONTRACTOR IS PERMITTED TO MAINTAIN TRAFFIC ON THIS INITIAL LIFT OF INTERMEDIATE COURSE FOR A PERIOD NOT TO EXCEED 12 DAYS.

THE PLACEMENT OF THE SECOND INTERMEDIATE COURSE, SURFACE COURSE, TEMPORARY PAVEMENT MARKINGS AND PERMANENT PAVEMENT MARKINGS SHALL BE PHASED TO PROTECT TRAFFIC FROM DROP-OFFS AND GUARDRAIL MODIFICATIONS WHEN APPLICABLE AND IN COMPLIANCE WITH THE REQUIREMENTS OF THE PLCS AND THE APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTIONS LISTED IN THIS PLAN.

ALL DROP-OFFS THAT DEVELOP DURING CONSTRUCTION SHALL BE PROTECTED IN ACCORDANCE WITH SCD MT-101.90. DROP-OFF PROTECTION SHALL ALSO INCLUDE CLOSING THE SHOULDER WITH DRUMS UNTIL THE EDGE DROP-OFF HAS BEEN REMOVED AND GUARDRAIL HAS BEEN RESET TO THE PROPER HEIGHT.

WHEN WORKING WESTBOUND NEAR THE I.R. 80 / I.R. 680 / S.R. 11 INTERCHANGE THROUGH THE OHLTOWN ROAD BRIDGE, THE CONTRACTOR SHALL RESTRICT TRAFFIC AS LISTED BELOW.

EAST OF AND THROUGH THE INTERCHANGE, THE CONTRACTOR SHALL REDUCE I.R. 80 WB TO A SINGLE LANE FOR A PERIOD NOT TO EXCEED 12 CONSECUTIVE CALENDAR DAYS WHILE PERFORMING PHASE 1 AND PHASE 2 WORK. THE SINGLE LANE OF I.R. 80 WB TRAFFIC SHALL BE A DEDICATED LANE AND SEPARATE FROM THE I.R. 680 AND S.R. 11 RAMP TRAFFIC THAT WILL JOIN IN SEPARATE LANES. THE COMBINED DURATION OF PHASE 1 AND PHASE 2 SINGLE LANE RESTRICTIONS SHALL NOT EXCEED 12 CONSECUTIVE CALENDAR DAYS. DURING THE 12-DAY RESTRICTION THE CONTRACTOR SHALL PAVE UP TO AND INCLUDING THE 1.75" THICK, 12.5MM ASPHALT INTERMEDIATE COURSE. THE I.R. 80 WB TO S.R. 11 SB RAMP SHALL NOT BE CLOSED DURING PHASE 1 OR PHASE 2 WESTBOUND WORK.

A. DURING PHASE 1 OF THE 12-DAY, SINGLE LANE RESTRICTION, THE CONTRACTOR SHALL SHIFT THE SINGLE LANE OF I.R. 80 WB TRAFFIC TOWARDS THE INSIDE PAVEMENT. THE CONTRACTOR SHALL MAINTAIN I.R. 680 NB AND S.R. 11 NB TRAFFIC IN A LANE SEPARATE FROM THE I.R. 80 WB TRAFFIC. THE CONTRACTOR SHALL DETOUR S.R. 46 ACCESS AT I.R. 80 DURING PHASE 1. DURING PHASE 1 WESTBOUND WORK, THE CONTRACTOR SHALL CRACK AND SEAT AND PAVE THE OUTSIDE LANE AND SHOULDER INCLUDING THE WESTBOUND ACCELERATION AND DECELERATION LANES AT THE I.R. 80 / S.R. 46 INTERCHANGE.

B. DURING PHASE 2 OF THE 12-DAY, SINGLE LANE RESTRICTION, THE CONTRACTOR SHALL SHIFT THE SINGLE LANE OF I.R. 80 WB TRAFFIC TOWARDS THE OUTSIDE PAVEMENT. DURING PHASE 2 OF THE 12-DAY, SINGLE LANE RESTRICTION THE CONTRACTOR SHALL MAINTAIN I.R. 680 NB AND S.R. 11 NB RAMP ACCESS TO I.R. 80 WB BETWEEN THE HOURS OF 6:00AM AND 8:00PM. THE CONTRACTOR SHALL CLOSE AND DETOUR THE S.R. 11 NB AND I.R. 680 NB RAMPS TO I.R. 80 WB BETWEEN THE HOURS OF 8:00PM AND 6:00AM WHILE PERFORMING IMPROVEMENTS TO THE INSIDE LANE OF I.R. 80 THROUGH THE OHLTOWN ROAD BRIDGE. THE OVERNIGHT RAMP CLOSURES SHALL BE LIMITED TO A MAXIMUM DURATION OF 6 CONSECUTIVE CALENDAR DAYS.

C. ONCE PHASE 2 WORK HAS BEEN COMPLETED AND TWO LANES OF TRAFFIC HAVE BEEN RESTORED ON I.R. 80 WB, THE RAMPS FROM S.R. 11 NB AND I.R. 680 NB TO I.R. 80 WB SHALL REMAIN CLOSED AND DETOURED. THE CONTRACTOR SHALL MAINTAIN TWO LANES OF I.R. 80 WB THROUGH THE INTERCHANGE USING THE OUTSIDE SHOULDER AND PAVEMENT AND BEGIN PHASE 3 IMPROVEMENTS TO THE SOUTH SIDE OF THE I.R. 80 WB PAVEMENT. PHASE 3 WORK SHALL COMPLETE THE WESTBOUND I.R. 80 CRACK AND SEAT AND PAVING OPERATIONS UP TO AND INCLUDING THE 1.75" THICK, 12.5MM ASPHALT INTERMEDIATE COURSE FROM OHLTOWN ROAD THROUGH THE EAST PROJECT LIMITS. DURING PHASE 3 WESTBOUND OPERATIONS, THE CONTRACTOR SHALL CLOSE AND DETOUR THE RAMP FROM I.R. 80 WB TO S.R. 11 SB FOR A PERIOD NOT TO EXCEED 7 CONSECUTIVE CALENDAR DAYS WHILE PERFORMING IMPROVEMENTS TO THE S.R. 11 SB RAMP.

ITEM 614 - MAINTAINING TRAFFIC

THIS ITEM SHALL CONSIST OF MAINTENANCE OF TRAFFIC ON EXISTING ROADWAYS AND RAMPS IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, CURRENT EDITION, LATEST REVISION, THE SPECIFICATIONS AND THE FOLLOWING:

1. A MINIMUM OF TWO ELEVEN FOOT LANES OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES ALONG I.R. 76 AND I.R. 80 BY USE OF THE EXISTING PAVEMENT OR THE COMPLETED PAVEMENT WITH THE EXCEPTION OF THE TWO-LANE SECTIONS AT THE ENDS OF THE PROJECT WHERE THE CONTRACTOR SHALL BE ALLOWED TO CLOSE A LANE USING DRUMS DURING WORK HOURS AND IN ACCORDANCE WITH THE PERMITTED LANE CLOSURE SCHEDULE. IN THE VICINITY OF THE OTIC INTERCHANGE, THE CONTRACTOR SHALL ALSO BE PERMITTED TO REDUCE THE TWO-LANE SEGMENTS OF I.R. 76 AND I.R. 80 TO A SINGLE LANE PER BOUND FOR AN EXTENDED PERIOD, CONTRARY TO THE PLCS AND AS CLARIFIED IN THE WORK ZONE RESTRICTIONS AND THE APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTIONS LISTED IN THIS PLAN. LIMITED TIME CLOSURE IS PERMITTED FOR THE TWO-LANE SECTION OF I.R. 80 WB WITHIN THE S.R. 11/I.R. 680 INTERCHANGE. THE CONTRACTOR SHALL ARRANGE THE CLOSURE PERIODS SO THEY DO NOT COINCIDE WITH ANY CLOSURES OR CAPACITY RESTRICTIONS ON THE PRESCRIBED DETOUR ROUTE.

2. A MINIMUM OF ONE LANE OF TRAFFIC SHALL BE MAINTAINED AT ALL TIMES ALONG RAMPS BY USE OF THE EXISTING PAVEMENT OR THE COMPLETED PAVEMENT EXCEPT FOR THE SPECIFIC PERMITTED CLOSURE TIMES AND SEQUENCES NOTED IN THE PLANS. LIMITED TIME CLOSURES ARE PERMITTED FOR THE RAMPS AT OTIC, S.R. 46, S.R. 11/I.R. 680, SALT SPRINGS ROAD AND US 422. THE CONTRACTOR SHALL ARRANGE THESE CLOSURE PERIODS SO THEY DO NOT COINCIDE WITH ANY CLOSURES OR CAPACITY RESTRICTIONS ON THE PRESCRIBED DETOUR ROUTE.

3. THE CONTRACTOR SHALL INFORM THE DISTRICT OFFICE (330) 786-2208, EIGHTEEN (18) DAYS PRIOR TO THE BEGINNING OF WORK.

4. TRUCK MOUNTED ATTENUATORS [TMA'S] SHALL BE USED AS SHOWN IN THE STANDARD CONSTRUCTION DRAWINGS.

5. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE PERMITTED TO HAVE SUCCESSIVE WORK ZONES UNLESS THE DISTANCE BETWEEN THE DRUMS, BARRICADES OR CONES EXCEEDS TWO (2) MILES RURAL OR ONE (1) MILE URBAN.

6. PRIOR TO OPENING TO TRAFFIC EACH LANE SHALL BE IN A SAFE, PASSABLE CONDITION. ALL TRANSVERSE JOINTS SHALL EXTEND ACROSS THE FULL LANE AND SHOULDER WIDTH AND EACH LANE SHALL BE FREE FROM UNEVEN LONGITUDINAL JOINTS. THE CONTRACTOR SHALL PROVIDE ASPHALT WEDGES FOR TRANSVERSE JOINTS WHEREVER THERE ARE PAVEMENT ELEVATION DIFFERENCES.

7. TO ENSURE THAT WEIGHTED CHANNELIZERS WILL NOT BE BLOWN OVER OR DISPLACED BY WIND AND MOVING TRAFFIC, ALL WEIGHTED CHANNELIZERS UTILIZED ON INTERSTATES AND FREEWAYS SHALL BE FROM MANUFACTURERS ON THE OHIO DEPARTMENT OF TRANSPORTATION, OFFICE OF MATERIAL MANAGEMENT'S QUALIFIED PRODUCTS LIST (QPL) WHICH UTILIZE A MINIMUM OF A 30 POUND BALLAST.

8. DRUMS UTILIZED ON THE HIGH SIDE OF A SUPERELEVATED INTERSTATE OR FREEWAYS SHALL BE FROM MANUFACTURERS ON THE OFFICE OF MATERIAL MANAGEMENT'S QUALIFIED PRODUCTS LIST (QPL) WITH A MINIMUM BALLAST WEIGHT OF 30 POUNDS. ALL BALLASTS USED SHALL BE IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLAT SHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK.

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD/RAMP FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.

NOTICE OF CLOSURE SIGN TIME TABLE

ITEM	DURATION OF CLOSURE	SIGN DISPLAYED TO PUBLIC
RAMP & ROAD CLOSURES	≥ 2 WEEKS > 12 HOURS & < 2 WEEKS ≤ 12 HOURS	14 CALENDAR DAYS PRIOR TO CLOSURE 7 CALENDAR DAYS PRIOR TO CLOSURE 2 BUSINESS DAYS PRIOR TO CLOSURE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION. THIS IS TO BE A SPECIFIC OFFICE WITHIN THE DISTRICT RATHER THAN THE GENERAL SWITCHBOARD NUMBER.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&M 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

TIME LIMITATION, TRAFFIC ON A MILLED SURFACE

THE MAXIMUM ALLOWABLE TIME FOR TRAFFIC TO BE PLACED ON A MILLED SURFACE SHALL BE SEVEN (7) CONSECUTIVE CALENDAR DAYS. SHOULD THE CONTRACTOR FAIL TO MEET THIS REQUIREMENT, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF \$2,000 PER DAY THAT THE TRAFFIC IS PLACED ON A MILLED SURFACE BEYOND THE SPECIFIED LIMIT.



3-18-26: ALL NOTES RESORTED FOR CLARITY; REVISIONS TO COVER EAST END SCHEME AND MOTEC

DESIGN AGENCY



DESIGNER	MEP
REVIEWER	LOB 3/18/26
PROJECT ID	122947
SHEET	TOTAL
P.16	87

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTICE OF CLOSURE SIGN TIME TABLE

ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS & PIO
RAMP & ROAD CLOSURES	≥ 2 WEEKS > 12 HOURS & < 2 WEEKS ≤ 12 HOURS	21 CALENDAR DAYS PRIOR TO CLOSURE 14 CALENDAR DAYS PRIOR TO CLOSURE 4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	≥ 2 WEEKS < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE 5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES		14 CALENDAR DAYS PRIOR TO CLOSURE

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

ITEM 614 - MAINTAINING TRAFFIC (LANE OPEN DURING HOLIDAYS OR SPECIAL EVENTS)

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR SPECIAL EVENTS:

NEW YEAR'S (OBSERVED)	GENERAL/REGULAR ELECTION DAY (NOV)
MEMORIAL DAY	THANKSGIVING
FOURTH OF JULY (OBSERVED)	CHRISTMAS (OBSERVED)
LABOR DAY	(OTHER HOLIDAY OR SPECIAL EVENT)

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR SPECIAL EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY OR SPECIAL EVENT	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
TUES. (GEN./REG. ELECTION)	5:00 AM TUESDAY THROUGH 12:00 AM
WEDNESDAY	
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY
THURSDAY (THANKSGIVING ONLY)	
FRIDAY	6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT (PN 127).

DESCRIPTION OF CRITICAL LANE TO BE MAINTAINED	LANE VALUE CONTRACT TABLE	RESTRICTED TIME PERIOD #	TIME UNIT MINUTE	DISINCENTIVE \$ PER TIME UNIT \$100 PER LANE
TWO LANES OF I.R. 76 / 80				

AS PER THE ITEM 614 - MAINTAINING TRAFFIC (LANES OPEN DURING HOLIDAYS OR SPECIAL EVENTS) NOTE AND THE PERMITTED LANE CLOSURE SCHEDULE (PLCS). REFER TO THE "WORK ZONE RESTRICTIONS" NOTE FOR ADDITIONAL INFORMATION REGARDING THE TWO-LANE SEGMENTS OF I.R. 76 AND I.R. 80 IN THE VICINITY OF THE OTIC INTERCHANGE.

APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTIONS

PORTIONS OF THE MOT PLANS AS DESCRIBED BELOW HAVE APPROVED MOT EXCEPTIONS PER TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)).

APPROVED MOT EXCEPTIONS INCLUDE:
 1. WHEN WORKING WESTBOUND DURING PHASE 1 AND PHASE 2 OPERATIONS AT THE EAST PROJECT LIMITS, THE CONTRACTOR SHALL REDUCE I.R. 80 APPROACHING THE I.R. 80 / I.R. 680 / S.R. 11 INTERCHANGE TO A SINGLE LANE FOR A PERIOD NOT TO EXCEED 12 CONSECUTIVE CALENDAR DAYS.

2. DURING PHASE 2 WESTBOUND OPERATIONS, THE CONTRACTOR SHALL CLOSE THE S.R. 11 AND I.R. 680 RAMP AT THE I.R. 80 WB INTERCHANGE FOR OVERNIGHT WORK. RAMP CLOSURES SHALL BE LIMITED TO 6 CONSECUTIVE CALENDAR DAYS. PLEASE REFER TO THE "WORK ZONE RESTRICTIONS" AND "SEQUENCE OF CONSTRUCTION" NOTES FOR CONDITIONS REGARDING THE REQUIRED RAMP CLOSURE.

3. DURING PHASE 3 WESTBOUND OPERATIONS, THE CONTRACTOR SHALL CLOSE THE RAMP FROM I.R. 680 NB TO I.R. 80 WB FOR A PERIOD NOT TO EXCEED 45 CONSECUTIVE CALENDAR DAYS. PLEASE REFER TO THE "WORK ZONE RESTRICTIONS" NOTE FOR CONDITIONS REGARDING THE PERMITTED RAMP CLOSURE. I.R. 680 NB TO I.R. 80 WB DETOURS ARE EXEMPT FROM THE ITEM 614 - MAINTAINING TRAFFIC (LANE OPEN DURING HOLIDAYS OR SPECIAL EVENTS) NOTE.

4. THE CONTRACTOR IS PERMITTED TO CLOSE AND DETOUR THE I.R. 80 WESTBOUND RAMP AT THE OHIO TURNPIKE INTERCHANGE FOR UP TO FIVE CONSECUTIVE CALENDAR DAYS.

5. THE CONTRACTOR IS PERMITTED TO CLOSE AND DETOUR THE I.R. 80 EASTBOUND RAMP AT THE OHIO TURNPIKE INTERCHANGE FOR UP TO FIVE CONSECUTIVE CALENDAR DAYS.

6. THE CONTRACTOR IS PERMITTED TO CLOSE AND DETOUR THE I.R. 80 WB TO S.R. 11 SB RAMP FOR UP TO SEVEN CONSECUTIVE CALENDAR DAYS. TWO LANES OF I.R. 80 WB SHALL BE MAINTAINED DURING THE PERMITTED RAMP CLOSURE.

7. IN THE VICINITY OF THE OTIC INTERCHANGE, THE CONTRACTOR SHALL BE PERMITTED TO REDUCE THE TWO-LANE SEGMENTS OF I.R. 76 AND I.R. 80 TO A SINGLE LANE PER BOUND FOR TWO SEPARATE PERIODS. THE DURATION OF EACH SINGLE LANE CONDITION SHALL BE LIMITED FROM 5:00PM SUNDAY THROUGH 12:00PM THURSDAY. THE SINGLE LANE CLOSURES SHALL BE PERFORMED DURING CONSECUTIVE WEEKS. PLEASE REFER TO THE "WORK ZONE RESTRICTIONS" NOTE FOR ADDITIONAL MOT EFFORTS REQUIRED WITH THIS PERMITTED LANE REDUCTION. THE PERMITTED OTIC RAMP CLOSURE DETAILED IN THE CLOSURE TABLE MAY BE PERFORMED CONCURRENTLY WITH THIS WORK.

A VIRTUAL MAINTENANCE OF TRAFFIC MEETING SHALL BE HELD A MINIMUM OF 30 CALENDAR DAYS PRIOR TO IMPLEMENTATION OF EACH APPROVED MOT EXCEPTION. THIS MEETING SHALL INCLUDE THE DISTRICT WORK ZONE TRAFFIC MANAGER (DWZTM), THE OHIO STATE HIGHWAY PATROL, THE MAHONING/TRUMBULL COUNTY SHERIFF'S OFFICES, THE OTIC, THE MAHONING/TRUMBULL COUNTY ENGINEER'S OFFICES, THE MAHONING/TRUMBULL COUNTY 911, THE MAHONING/TRUMBULL COUNTY EMERGENCY MANAGEMENT AGENCIES, THE CITY OF YOUNGSTOWN, THE CITY OF GIRARD, JACKSON TOWNSHIP, LIBERTY TOWNSHIP, WEATHERSFIELD TOWNSHIP, AUSTINTOWN TOWNSHIP AS WELL AS THE CONTRACTOR, WORKSITE TRAFFIC SUPERVISOR (WTS) AND ANY SUBCONTRACTORS INVOLVED WITH TEMPORARY TRAFFIC CONTROL.

IN ADDITION TO ANY NOTIFICATIONS REQUIRED IN OTHER NOTES, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER AT LEAST 3 BUSINESS DAYS IN ADVANCE OF IMPLEMENTATION OF THE APPROVED MOT EXCEPTIONS REFERENCED ABOVE SO THAT THE PROJECT ENGINEER CAN SEND EMAIL NOTIFICATION TO THE OFFICE OF ROADWAY ENGINEERING, STATEWIDE TMC, DWZTM AND SPECIAL HAULING PERMITS AT LEAST 2 BUSINESS DAYS IN ADVANCE OF THE IMPLEMENTATION OF THE APPROVED MOT EXCEPTIONS REFERENCED ABOVE. REFERENCE "EXCEPTION REQUEST APPROVALS DATED 3/17/26 FOR PID 122947" IN THE NOTIFICATION AND OTHER CORRESPONDENCE.

ANY CHANGES TO THE MOT THAT IMPACT THE PREVIOUSLY APPROVED MOT EXCEPTION(S) LISTED ABOVE SHALL BE APPROVED IN WRITING BY THE MOT EXCEPTION COMMITTEE (MOTEC). IN THE EVENT THAT SUCH CHANGES ARE PROPOSED, THE REQUEST SHALL BE COORDINATED THROUGH THE DWZTM A MINIMUM OF 30 CALENDAR DAYS PRIOR TO THE DESIRED IMPLEMENTATION DATE. IF THE DISTRICT AGREES WITH THE PROPOSED CHANGES THE DWZTM SHALL SEEK APPROVAL FROM THE MOTEC. IN THE EVENT THE PROPOSED CHANGES ARE APPROVED IN WRITING, THE CLOSURES ARE STILL SUBJECT TO NOTIFICATION REQUIREMENTS WITHIN THIS NOTE PRIOR TO IMPLEMENTATION.

WORK ZONE SPEED ZONES (WZSZs)

THE FOLLOWING WORK ZONE SPEED ZONE (WZSZ) SPEED LIMIT REVISION(S) HAVE BEEN APPROVED FOR USE ON THIS PROJECT WHEN WORK ZONE CONDITIONS AND FACTORS ARE MET AS DESCRIBED BELOW:

WZSZ REVISION NUMBER(S)	C-R-S	DIRECTION(S)
WZ-26242	MAH-76-8.12 TO 8.64	EASTBOUND
WZ-26242	MAH-76-8.40 TO 8.64	WESTBOUND
WZ-26242	MAH-80-0.00 TO 4.09	EASTBOUND
WZ-26242	MAH-80-0.00 TO 5.76	WESTBOUND
WZ-26242	TRU-80-0.00 TO 0.16	WESTBOUND

POTENTIAL WZSZ LOCATIONS SHALL HAVE AN ORIGINAL (PRE CONSTRUCTION) POSTED SPEED LIMIT OF 55 MPH OR GREATER, A QUALIFYING WORK ZONE CONDITION OF AT LEAST 0.5 MILE IN LENGTH, AN EXPECTED WORK DURATION OF AT LEAST THREE HOURS, AND A WORK ZONE CONDITION IN PLACE THAT REDUCES THE EXISTING FUNCTIONALITY OF THE TRAVEL LANES OR SHOULDERS (I.E., LANE CLOSURE, LANE SHIFT, Crossover, CONTRAFLOW AND/OR SHOULDER CLOSURE). THE LENGTH OF THE WORK ZONE CONDITION IS MEASURED FROM THE BEGINNING OF THE TAPER FOR THE SUBJECT WORK ZONE CONDITION IMPACTING THE TRAVEL LANES AND/OR SHOULDER TO THE END OF THE DOWNSTREAM TAPER, WHERE DRIVERS ARE RETURNED TO TYPICAL ALIGNMENT. AN EXPECTED WORK DURATION OF AT LEAST THREE HOURS IS REQUIRED TO BALANCE THE ADDITIONAL EXPOSURE CREATED BY INSTALLING AND REMOVING WZSZ SIGNING WITH THE TIME NEEDED TO COMPLETE THE WORK.

IF THE WORK ZONE MEETS THESE MINIMUM CRITERIA, IT SHALL BE ANALYZED FURTHER USING TABLE 1 BELOW TO DETERMINE IF AND WHEN IT QUALIFIES FOR A SPEED LIMIT REDUCTION. DEPENDING ON THE ORIGINAL POSTED SPEED LIMIT, THE TYPE OF TEMPORARY TRAFFIC CONTROL USED, AND WHETHER OR NOT WORKERS ARE PRESENT, A WARRANTED WZSZ WILL VARY IN THE APPROVED SPEED LIMIT TO BE POSTED OVER TIME.

C&MS ITEM 614, PARAGRAPH 614.02(B), INDICATES THAT TWO DIRECTIONS OF A DIVIDED HIGHWAY ARE CONSIDERED SEPARATE HIGHWAY SECTIONS. THEREFORE, IF THE WORK ON A MULTI-LANE DIVIDED HIGHWAY IS LIMITED TO ONLY ONE DIRECTION, A SPEED LIMIT REDUCTION IN THE DIRECTION OF THE WORK DOES NOT AUTOMATICALLY CONSTITUTE A SPEED LIMIT REDUCTION IN THE OPPOSITE DIRECTION. EACH DIRECTION SHALL BE ANALYZED INDEPENDENTLY FROM EACH OTHER.

ALL WZSZS FLUCTUATE BETWEEN TWO APPROVED REDUCED SPEED LIMITS OR BETWEEN AN APPROVED REDUCED SPEED LIMIT AND THE ORIGINAL POSTED SPEED LIMIT. ONLY ONE OF TWO SIGNING STRATEGIES SHALL BE USED TO IMPLEMENT A WZSZ.

WZSZS USING DSL SIGN ASSEMBLIES SHALL BE IN ACCORDANCE WITH THIS NOTE, APPROVED LIST, SUPPLEMENTAL SPECIFICATIONS (SS) 808 AND 908, AND TRAFFIC SCD MT-104.10.

ONLY ONE WARRANTED SPEED LIMIT APPLIES AT ANY ONE TIME; SPEED LIMIT REDUCTIONS ARE NOT CUMULATIVE. WZSZS SHALL NOT BE USED FOR MOVING/MOBILE ACTIVITIES, AS DEFINED IN OMUTCD PART 6.

WHEN LOOKING UP THE WARRANTED WORK ZONE SPEED LIMITS, ALWAYS USE THE ORIGINAL, PRE-CONSTRUCTION, POSTED SPEED LIMIT. DO NOT USE A PRIOR OR CURRENT WORK ZONE SPEED LIMIT AS A LOOK UP VALUE IN THE TABLE. POSITIVE PROTECTION IS GENERALLY REGARDED AS PORTABLE BARRIER OR OTHER RIGID BARRIER IN USE ALONG THE WORK AREA WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WITHOUT POSITIVE PROTECTION IS GENERALLY REGARDED AS USING DRUMS, CONES, SHADOW VEHICLE, ETC., ALONG THE WORK AREA WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WORKERS ARE CONSIDERED AS BEING PRESENT WHEN ON-SITE, WORKING WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WHEN THE WORK ZONE CONDITION REDUCING THE EXISTING FUNCTIONALITY OF THE TRAVEL LANES OR SHOULDERS IS REMOVED, THE SPEED LIMIT DISPLAYED SHALL RETURN TO THE ORIGINAL POSTED SPEED LIMIT.

TABLE 1: WARRANTED WORK ZONE SPEED LIMITS (MPH) FOR WORK ZONES ON HIGH-SPEED (55 MPH OR GREATER) MULTI-LANE HIGHWAYS

ORIG. POSTED SPEED LIMIT	W/ POSITIVE PROT.		W/O POSITIVE PROT.	
	WORKERS PRESENT	WORKERS NOT PRES.	WORKERS PRESENT	WORKERS NOT PRESENT
65	55	60	50	60
70	60	65	55	65

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY.
 ITEM 808 - DIGITAL SPEED LIMIT (DSL) SIGN ASSEMBLY 54 SNMT
 ASSUMING 6 DSL SIGN ASSEMBLIES FOR 9 MONTHS

DESIGN AGENCY	
DESIGNER	MEP
REVIEWER	3/18/26
PROJECT ID	122947
SHEET TOTAL	P.17 87

DETOUR AND CLOSURE RESTRICTIONS

CLOSURES AND DETOURS PROVIDED FOR THE CONSTRUCTION OF THE WORK ARE SUBJECT TO THE RESTRICTIONS LISTED BELOW:

1. THE CLOSURE OF THE I.R. 80 WESTBOUND RAMPS AT THE OHIO TURNPIKE INTERCHANGE SHALL NOT BE PERFORMED CONCURRENTLY WITH ANY OTHER RAMP CLOSURE. THE PERMITTED CLOSURE SHALL NOT EXCEED FIVE CONSECUTIVE CALENDAR DAYS.

2. THE CLOSURE OF THE I.R. 80 EASTBOUND RAMPS AT THE OHIO TURNPIKE INTERCHANGE SHALL NOT BE PERFORMED CONCURRENTLY WITH ANY OTHER RAMP CLOSURE. THE PERMITTED CLOSURE SHALL NOT EXCEED FIVE CONSECUTIVE CALENDAR DAYS.

RAMP CLOSURES

OTIC ON/OFF RAMPS ALONG THE SAME BOUND MAY BE CLOSED CONCURRENTLY.

DO NOT CLOSE ADJACENT INTERCHANGE RAMPS CONCURRENTLY.

CLOSURE TABLE (SEE NOTE A)		
RAMP	PERMITTED CLOSURE PERIOD	DISINCENTIVE
OTIC TO I.R. 76 WB	5 DAYS	\$8,000/DAY
I.R. 80 WB TO OTIC	5 DAYS	\$10,000/DAY
I.R. 80 EB TO OTIC	5 DAYS	\$8,000/DAY
OTIC TO I.R. 80 EB	21 DAYS	\$15,000/DAY
S.R. 46 TO I.R. 80 WB	5 DAYS	\$5,000/DAY
I.R. 80 WB TO S.R. 46	5 DAYS	\$5,000/DAY
I.R. 80 EB TO S.R. 46	7 DAYS	\$5,000/DAY
S.R. 11 NB TO I.R. 80 WB	45 DAYS	\$5,000/DAY
I.R. 680 NB TO I.R. 80 WB	45 DAYS	\$5,000/DAY
I.R. 80 WB TO S.R. 11 SB	7 DAYS	\$5,000/DAY

NOTE A:
"EXIT CLOSED" OVERLAYS SHALL BE PLACED AS SPECIFIED PER SCD MT-98.29.

DETOUR ROUTES

OTIC TO I.R. 76 WB:
OTIC TO I.R. 80 EB TO S.R. 46 NB TO I.R. 80 WB TO I.R. 76 WB

I.R. 80 WB TO OTIC:
I.R. 80 WB TO I.R. 76 WB TO BAILEY RD SB TO I.R. 76 EB TO OTIC

I.R. 80 EB TO OTIC:
I.R. 80 EB TO S.R. 46 NB TO I.R. 80 WB TO OTIC

OTIC TO I.R. 80 EB:
OTIC TO I.R. 76 WB TO BAILEY RD SB TO I.R. 76 EB TO I.R. 80 EB
NOTE: PORTABLE CHANGEABLE MESSAGE SIGNS SHALL BE PLACED ON THE OTIC BEFORE EB OTIC EXIT 215, BEFORE EB OTIC EXIT 218, AND BEFORE WB OTIC EXIT 218 AT LEAST TWO WEEKS PRIOR TO AND DURING THE CLOSURE OF THE OTIC RAMP TO I.R. 80 EB. THE APPROVED LOCATIONS OF THE MESSAGE SIGNS SHALL BE COORDINATED WITH THE PROJECT ENGINEER.

S.R. 46 TO I.R. 80 WB:
S.R. 46 TO I.R. 80 EB TO SALT SPRINGS RD NB TO I.R. 80 WB

I.R. 80 WB TO S.R. 46:
I.R. 80 WB TO I.R. 76 WB TO BAILEY RD SB TO I.R. 76 EB TO I.R. 80 EB TO S.R. 46

I.R. 80 EB TO S.R. 46:
I.R. 80 EB TO SALT SPRINGS RD NB TO I.R. 80 WB TO S.R. 46

S.R. 11 NB TO I.R. 80 WB:
S.R. 11 NB TO S.R. 711 TO U.S. 422 TO I.R. 80 WB

I.R. 680 NB TO I.R. 80 WB:
I.R. 680 NB TO S.R. 711 TO U.S. 422 TO I.R. 80 WB

I.R. 80 WB TO S.R. 11 SB:
I.R. 80 WB TO S.R. 46 SB TO I.R. 80 EB TO S.R. 11 SB

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR DETOUR SIGNING PURPOSES:

ITEM 614 - PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN
ASSUMING 3 PCMS FOR 2 MONTHS ALONG OTIC 6 SNMT
ASSUMING 7 PCMS FOR 1 MONTH FOR OTIC RAMPS 7 SNMT
ASSUMING 6 PCMS FOR 1 MONTH FOR REMAINING RAMPS 6 SNMT
ASSUMING 5 PCMS FOR 1 MONTH FOR TWO-LANE SEGMENT 5 SNMT
ASSUMING 7 PCMS FOR 2 MONTHS FOR S.R. 11 AND I.R. 680 DETOURS 14 SNMT

FINAL PCMS LOCATIONS SHALL BE COORDINATED WITH AND APPROVED BY THE PROJECT ENGINEER AND THE OTIC PRIOR TO PLACEMENT.

FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614 - MAINTAINING TRAFFIC.

ADVANCED NOTICE TO PAVE

THE CONTRACTOR SHALL SUBMIT FOR APPROVAL TO THE DISTRICT CONSTRUCTION ENGINEER A DETAILED SCHEDULE 15 DAYS PRIOR TO THE PLACEMENT OF THE PAVEMENT OVERLAY COURSES, ON HOW THEY PROPOSE TO PROSECUTE THE PAVING OPERATIONS. THE DETAILS SHALL SHOW THE ORDER OF PERFORMANCE OF EACH STAGE (START TO FINISH) OF THE WORK INCLUDING THE MAINTENANCE OF TRAFFIC THAT WILL BE USED.

WORK ZONE MARKINGS

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS IDENTIFIED BY THE ENGINEER FOR WORK ZONE PAVEMENT MARKINGS AND SIGNS PER THE REQUIREMENTS OF C&MS 614.04 AND 614.11.

ITEM 614 - WORK ZONE LANE LINE, CLASS I, 6", 807 PAINT 52.50 MILE
ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 6", 807 PAINT 60.00 MILE
ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS I, 12", 807 PAINT 34,650 FT
ITEM 614 - WORK ZONE DOTTED LINE, CLASS I, 6", 807 PAINT 18,180 FT
ASSUMING THREE (3) APPLICATIONS OF WORK ZONE MARKINGS

ITEM 614 - WORK ZONE LANE LINE, CLASS III, 6", 642 PAINT 17.50 MILE
ITEM 614 - WORK ZONE EDGE LINE, CLASS III, 6", 642 PAINT 20.00 MILE
ITEM 614 - WORK ZONE CHANNELIZING LINE, CLASS III, 12", 642 PAINT 11,550 FT
ITEM 614 - WORK ZONE DOTTED LINE, CLASS III, 6", 642 PAINT 0,060 FT
ASSUMING ONE (1) APPLICATION OF WORK ZONE MARKINGS

ITEM 614 - PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

PCMS LOCATIONS SHALL BE IN ADVANCE OF THE I.R. 80 WORK AS DIRECTED BY THE ENGINEER. PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614 - PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN
ASSUMING 4 PCMS SIGNS FOR 9 MONTHS 36 SNMT

WORK ZONE INCREASED PENALTIES SIGN (R11-H5A)

R11-H5A-48 SIGNS SHALL BE FURNISHED, ERECTED, AND MAINTAINED IN GOOD CONDITION AND/OR REPLACED AS NECESSARY AND SUBSEQUENTLY REMOVED BY THE CONTRACTOR. SIGNS SHALL BE MOUNTED AT THE APPROPRIATE OFFSETS AND ELEVATIONS AS PRESCRIBED BY THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. THEY SHALL BE MAINTAINED ON SUPPORTS MEETING CURRENT SAFETY CRITERIA.

THE SIGNS MAY BE ERECTED OR UNCOVERED NO MORE THAN FOUR HOURS BEFORE THE ACTUAL START OF WORK. THE SIGNS SHALL BE REMOVED OR COVERED NO LATER THAN FOUR HOURS FOLLOWING RESTORATION OF ALL LANES TO TRAFFIC WITH NO RESTRICTIONS, OR SOONER AS DIRECTED BY THE ENGINEER. TEMPORARY SIGN COVERING AND UNCOVERING DUE TO TEMPORARY LANE RESTORATIONS SHALL BE GUIDED BY THE FOUR-HOUR LIMITATIONS STATED ABOVE. SUCH LANE RESTORATIONS SHOULD BE EXPECTED TO REMAIN IN EFFECT FOR 30 OR MORE CONSECUTIVE CALENDAR DAYS, SUCH AS DURING WINTER SHUT-DOWNS.

THE SIGNS ON THE MAINLINE SHALL BE DUAL MOUNTED UNLESS NOT PHYSICALLY POSSIBLE. THE FIRST SIGN SHALL BE PLACED BETWEEN THE ROAD WORK AHEAD (W20-1) SIGN AND THE NEXT SIGN IN THE SEQUENCE. SIGNS SHALL BE ERECTED ON EACH ENTRANCE RAMP AND EVERY 2 MILES THROUGH THE CONSTRUCTION WORK LIMITS. SIGNS ON THE MAINLINE SHALL BE R11-H5A-48. SIGNS USED ON THE RAMPS SHALL BE R11-H5A-24. R11-H5A-24 SIGNS MAY BE USED IN THE MEDIAN IN LIEU OF R11-H5A-48 SIGNS IF IT IS NOT PHYSICALLY POSSIBLE TO PROVIDE R11-H5A-48 SIGNS IN THE MEDIAN.

THE R11-H5A-48 SIGNS SHALL BE MOUNTED ON 2 NO. 3 POSTS WHEN LOCATED WITHIN CLEAR ZONES.

THE CONTRACTOR MAY USE SIGNS AND SUPPORTS IN USED, BUT GOOD, CONDITION PROVIDED THE SIGNS MEET CURRENT ODOT SPECIFICATIONS.

WORK ZONE INCREASED PENALTIES SIGNS AND SUPPORTS WILL BE MEASURED AS THE NUMBER OF SIGN INSTALLATIONS, INCLUDING THE SIGN AND NECESSARY SUPPORTS. IF A SIGN AND SUPPORT COMBINATION IS REMOVED AND REERECTED AT ANOTHER LOCATION AS DIRECTED BY THE ENGINEER, IT SHALL BE CONSIDERED ANOTHER UNIT.

PAYMENT FOR ACCEPTED QUANTITIES, COMPLETE, IN PLACE WILL BE MADE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIALS, LABOR, INCIDENTALS AND EQUIPMENT FOR FURNISHING, ERECTING, MAINTAINING, COVERING DURING SUSPENSION OF WORK, AND REMOVAL OF THE SIGN AND SUPPORT.

ITEM 614 - WORK ZONE INCREASED PENALTIES SIGN 12 EACH

DESIGN AGENCY



DESIGNER
MEP

REVIEWER

LOB 3/18/26

PROJECT ID
122947

SHEET TOTAL
P.18 | 87

DUST CONTROL

THE CONTRACTOR SHALL FURNISH AND APPLY WATER FOR DUST CONTROL AS DIRECTED BY THE ENGINEER. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED FOR DUST CONTROL PURPOSES:

ITEM 616 - WATER 100 MGAL

ITEM 614. LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW SHALL NOT BE PERMITTED AT PROJECT COST NOR TIME COMPENSATION. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

DURING PERIODS WHERE TRAFFIC NEEDS TO BE DIRECTED CONTRARY TO A TRAFFIC CONTROL DEVICE (FLAGGER, SIGN [E.G. STOP SIGN, STREET OR HIGHWAY SIGNS, ETC], SIGNAL OR OTHER DEVICE USED TO REGULATE, WARN OR GUIDE TRAFFIC).

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

FOR LANE CLOSURES THAT MEET ALL OF THE CRITERIA LISTED BELOW: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

- CRITERIA
- ON A MULTI-LANE DIVIDED INTERSTATE, OTHER FREEWAY OR EXPRESSWAY; AND,
- AN AUTHORIZED SPEED LIMIT OF 45 MPH OR GREATER THAT IS IN EFFECT AT THE TIME OF THE OPERATION; AND,
- AADT OF 50,000 (OR AADT OF 30,000 WITH 25% OR HIGHER PERCENT TRUCKS)

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION (OR AT THE POINT OF ROAD CLOSURE), AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS AND/OR IN CONTRARY TO OTHER TRAFFIC CONTROL DEVICES IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE THAT SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 250 HOUR

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

PERMITTED LANE CLOSURE SCHEDULE (PLCS)
LANE CLOSURE(S) SHALL CONFORM TO THE PLCS. PUBLISHED PLCS INFORMATION CAN BE FOUND ON THE ODOT WEBSITE.

THE MONTHLY PUBLISHED SCHEDULES REQUIRED TO BE USED, FOR EACH PLCS SEGMENT WITHIN THE PROJECT AREA, ARE THOSE THAT COMPRISE THE CONSECUTIVE 12-MONTH PERIOD BEGINNING 15 MONTHS PRIOR TO THE MONTH AND YEAR OF SALE AND ENDING 4 MONTHS PRIOR TO THE MONTH AND YEAR OF SALE. THESE SAME 12 MONTHS APPLY FOR THE LIFE OF THE PROJECT AND SHALL BE APPLIED TO EACH RESPECTIVE MONTH OF CONSTRUCTION (MONTH OF LANE CLOSURE(S) SHALL MATCH MONTH OF PLCS USED). LANE CLOSURE(S) IN PLACE FOR MULTIPLE MONTHS SHALL ALWAYS COMPLY WITH THE CURRENT RESPECTIVE MONTH.

(FOR EXAMPLE: IF THE SALE DATE FOR THE PROJECT WAS MARCH OF 2021, THE MONTHLY PUBLISHED SCHEDULES FOR EACH APPLICABLE PLCS SEGMENT WOULD BE DECEMBER 2019 TO NOVEMBER 2020. IF THIS WAS A THREE-YEAR PROJECT, YEAR THREE WOULD STILL BE USING THE DECEMBER 2019 TO NOVEMBER 2020 MONTHLY SCHEDULES. IF THE PROJECT DESIRED TO CLOSE TWO LANES IN JUNE 2021, REFERENCE WOULD BE MADE TO THE JUNE 2020 SCHEDULE(S) FOR THE RESPECTIVE PLCS SEGMENT(S). IF THE SAME TWO LANES WERE DESIRED TO BE CLOSED AGAIN IN JULY 2021, REFERENCE WOULD BE MADE TO THE JULY 2020 SCHEDULE(S) FOR THE RESPECTIVE PLCS SEGMENT(S).)

MORE RESTRICTIVE CHANGES TO THE ALLOWABLE LANE CLOSURE HOURS ARE AT THE DISCRETION OF THE ENGINEER IN ORDER TO COMPLY WITH THE TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)).

LESS RESTRICTIVE CHANGES TO THE ALLOWABLE LANE CLOSURE HOURS ARE SUBJECT TO THE TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)) AND SHALL NOT BE IMPLEMENTED UNTIL, AND UNLESS, APPROVED BY THE PROPER ODOT AUTHORITY. EXISTING MOT EXCEPTIONS THAT HAVE ALREADY BEEN APPROVED IN ACCORDANCE TO THE TRAFFIC MANAGEMENT IN WORK ZONES POLICY AND STANDARD PROCEDURE ARE DETAILED IN THE APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTION(S) PLAN NOTE.

ALLOWABLE LANE CLOSURE HOURS FOR FACILITIES NOT COVERED BY THE PLCS, IF ANY, SHALL BE AS SPECIFIED ELSEWHERE IN THE PLANS.

DELINEATION OF BARRIER

BARRIER REFLECTORS AND OBJECT MARKERS SHALL BE INSTALLED ON ALL GUARDRAIL AND CONCRETE BARRIER (INCLUDING BRIDGE PARAPETS) LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE.

BARRIER REFLECTORS SHALL CONFORM TO C&MS 626 AND THE SPACING SHALL BE AT APPROXIMATELY 10-FOOT INTERVALS.

GUARDRAIL-MOUNTING OF OBJECT MARKERS SHALL BE MADE BY INSTALLING THE OBJECT MARKERS ON THE EXTENSION BLOCKS RATHER THAN DIRECTLY ONTO THE GUARDRAIL ITSELF. OBJECT MARKERS SHALL CONFORM TO C&MS 614.03 AND THE SPACING SHALL BE AT APPROXIMATELY 25-FOOT INTERVALS.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE PLANS AND CARRIED TO THE GENERAL SUMMARY:

ITEM 614 - BARRIER REFLECTOR, TYPE 1, ONE WAY	250 EACH
ITEM 614 - BARRIER REFLECTOR, TYPE 1, BIDIRECTIONAL	250 EACH
ITEM 614 - BARRIER REFLECTOR, TYPE 2, ONE WAY	250 EACH
ITEM 614 - BARRIER REFLECTOR, TYPE 2, BIDIRECTIONAL	250 EACH
ITEM 614 - OBJECT MARKER, ONE WAY	50 EACH
ITEM 614 - OBJECT MARKER, TWO WAY	50 EACH

DETOUR NOTIFICATION

THE CONTRACTOR SHALL ADVISE THE PROJECT ENGINEER AND OTIC EIGHTEEN (18) DAYS IN ADVANCE OF WHEN THE RAMP DETOUR ROUTE SHOULD BE IN EFFECT. ALL WORK ZONE DEVICES REQUIRED SHALL BE FURNISHED, ERECTED, MAINTAINED, AND SUBSEQUENTLY REMOVED BY THE CONTRACTOR. PAYMENT FOR ALL WORK ASSOCIATED WITH THE DETOUR SHALL BE INCLUDED UNDER THE LUMP SUM BID FOR ITEM 614, DETOUR SIGNING.

OHIO TURNPIKE AND INFRASTRUCTURE COMMISSION
CHIEF ENGINEER
ATTN.: JULIUS SZAHLENDER
682 PROSPECT ROAD
BEREA, OH 44017
440-971-2014
JULIUS.SZAHLENDER@OHIOTURNPIKE.ORG



3-18-26: ALL NOTES RESORTED FOR CLARITY; REVISIONS TO COVER EAST END SCHEME AND MOTEC

DESIGN AGENCY



DESIGNER
MEP

REVIEWER
LOB 3/18/26

PROJECT ID
122947

SHEET TOTAL
P.19 | 87

WORKSITE TRAFFIC SUPERVISOR

SUBJECT TO APPROVAL OF THE ENGINEER, THE CONTRACTOR SHALL EMPLOY AND IDENTIFY (SOMEONE OTHER THAN THE SUPERINTENDENT) A PREQUALIFIED WORKSITE TRAFFIC SUPERVISOR (WTS) BEFORE STARTING WORK IN THE FIELD. THE WTS SHALL BE TRAINED IN ACCORDANCE WITH C&MS 614.03, SHALL HAVE SUCCESSFULLY COMPLETED ODOT ADMINISTERED WTS TESTING (AND RE-TESTING WHEN APPLICABLE) AND BE LISTED ON THE ODOT PREQUALIFIED WTS ROSTER. PREQUALIFICATION EXPIRES EVERY 5 YEARS. RE-TESTING SHALL BE SUCCESSFULLY REPEATED EVERY 5 YEARS TO REMAIN PREQUALIFIED.

THE NAME OF THE PREQUALIFIED WTS AND RELATED 24-HOUR CONTACT INFORMATION SHALL BE PROVIDED TO THE ENGINEER AT THE PRECONSTRUCTION CONFERENCE. IF THE DESIGNATED WTS WILL NOT BE AVAILABLE FULL TIME (24/7), THE CONTRACTOR MAY DESIGNATE AN ALTERNATE (SECONDARY) WTS TO BE AVAILABLE WHEN THE PRIMARY IS OFF DUTY; HOWEVER, THE PRIMARY WTS SHALL REMAIN THE POINT OF CONTACT AT ALL TIMES. ANY ALTERNATE (SECONDARY) WTS IS SUBJECT TO THE SAME TRAINING, PREQUALIFICATION AND OTHER REQUIREMENTS OUTLINED WITHIN THIS PLAN NOTE. AT ALL TIMES THE ENGINEER, OR ENGINEER'S REPRESENTATIVES, MUST BE INFORMED OF WHO THE PRIMARY WTS (AND SECONDARY WTS, IF APPLICABLE) IS AT THE CURRENT TIME.

THE WTS POSITION HAS THE PRIMARY RESPONSIBILITY OF IMPLEMENTING THE TRAFFIC MANAGEMENT PLAN (TMP), MONITORING THE SAFETY AND MOBILITY OF THE ENTIRE WORK ZONE, AND CORRECTING TEMPORARY TRAFFIC CONTROL (TTC) DEFICIENCIES FOR THE ENTIRE WORK ZONE. THE WTS, AND ALTERNATE WTS WHEN ON DUTY, SHALL HAVE SUFFICIENT AUTHORITY TO EFFECTIVELY CARRY OUT THE IDENTIFIED WTS RESPONSIBILITIES AND DUTIES. THE DUTIES OF THE WTS ARE AS FOLLOWS:

1. BE AVAILABLE ON A 24-HOUR PER DAY BASIS.
2. BE ON SITE FOR ALL EMERGENCY TTC NEEDS WITHIN ONE HOUR OF NOTIFICATION BY POLICE OR PROJECT STAFF, AND EFFECT CORRECTIVE MEASURES IMMEDIATELY ON EXISTING WORK ZONE TTC DEVICES.
3. ATTEND PRECONSTRUCTION MEETING AND ALL PROJECT MEETINGS WHERE TTC MANAGEMENT IS DISCUSSED.
4. BE AVAILABLE ON SITE FOR OTHER MEETINGS OR DISCUSSIONS WITH THE ENGINEER UPON REQUEST.
5. BE AWARE OF ALL EXISTING AND PROPOSED TTC OPERATIONS OF THE CONTRACTOR, SUBCONTRACTORS AND SUPPLIERS, AND ENSURE COORDINATION OCCURS BETWEEN THEM TO ELIMINATE CONFLICTING TEMPORARY AND/OR PERMANENT TRAFFIC CONTROL.
6. COORDINATE PROJECT ACTIVITIES WITH ALL LAW ENFORCEMENT OFFICERS (LEOS). THE WTS SHALL ALSO BE THE MAIN CONTACT PERSON WITH THE LEOS WHILE LEOS ARE ON THE PROJECT.
7. COORDINATE AND FACILITATE MEETINGS WITH ODOT PERSONNEL, LEOS AND OTHER APPLICABLE ENTITIES BEFORE EACH PLAN PHASE SWITCH TO DISCUSS THE WORK ZONE TTC FOR IMPLEMENTING THE PHASE SWITCH. SUBMIT A WRITTEN DETAIL OF MOT OPERATIONS AND SCHEDULE OF EVENTS TO IMPLEMENT THE SWITCH BETWEEN PHASE PLANS TO THE ENGINEER 5 CALENDAR DAYS PRIOR TO THIS MEETING.
8. BE PRESENT, ON SITE FOR, AND INVOLVED WITH, EACH TTC SET UP/TAKE DOWN AND EACH PHASE CHANGE IN ACCORDANCE WITH C&MS 614.03.
9. ON A CONTINUAL BASIS ENSURE THAT THE TTC ZONE AND ALL RELATED DEVICES ARE INSTALLED, MAINTAINED AND REMOVED IN COMPLIANCE WITH THE CONTRACT DOCUMENTS.
10. ON A CONTINUAL BASIS FACILITATE CORRECTIVE ACTION(S) NECESSARY TO BRING DEFICIENT TTC ZONES AND ALL RELATED DEVICES INTO COMPLIANCE WITH CONTRACT DOCUMENTS IN THE TIME FRAME DETERMINED BY THE ENGINEER.

11. INSPECT, EVALUATE, PROPOSE NECESSARY MODIFICATIONS TO, AND DOCUMENT THE EFFECTIVENESS OF, THE TTC DEVICES AND TRAFFIC OPERATIONS ON A DAILY BASIS (7 DAYS A WEEK). IN ADDITION, PERFORM ONE WEEKLY NIGHT INSPECTION OF THE WORK ZONE SETUP FOR DAYTIME WORK OPERATIONS; AND ONE DAYTIME INSPECTION PER WEEK FOR NIGHTTIME PROJECTS. THIS SHALL INCLUDE (BUT NOT BE LIMITED TO) DOCUMENTATION ON THE FOLLOWING PROJECT EVENTS:

- A. INITIAL TTC SETUP (DAY AND NIGHT REVIEW).
- B. DAILY TTC SETUP AND REMOVAL.
- C. WHEN CONSTRUCTION STAGING CAUSES A CHANGE IN THE TTC SETUP.
- D. CRASH OCCURRENCES WITHIN THE CONSTRUCTION AREA AND WITHIN THE INFLUENCE AREA(S) APPROACHING THE WORK ZONE.
- E. REMOVAL OF TTC DEVICES AT THE END OF A PHASE OR PROJECT.
- F. ALL OTHER EMERGENCY TTC NEEDS.

12. COMPLETE THE DEPARTMENT APPROVED (CA-D-8) WITHIN GOFORMZ AFTER EACH INSPECTION AS REQUIRED IN #11 AND SUBMIT IT TO THE ENGINEER BY THE END OF THE WORKDAY IN WHICH THE INSPECTION OCCURRED. THE CA-D-8 INCLUDES A CHECKLIST OF ALL TTC MAINTENANCE ITEMS TO BE REVIEWED. CONTACT GOFORMZ.HELP@DOT.OHIO.GOV TO OBTAIN A USER ACCOUNT. ANY DEFICIENCIES OBSERVED SHALL BE NOTED ON THE CA-D-8, ALONG WITH RECOMMENDED OR COMPLETED CORRECTIVE ACTIONS AND THE DATES BY WHICH SUCH CORRECTIONS WERE, OR WILL BE, COMPLETED. A COPY OF THE CURRENT CA-D-8 DOCUMENT CAN BE FOUND ON THE OFFICE OF CONSTRUCTION ADMINISTRATION'S INSPECTION FORMS WEBSITE.

13. HAVE COPIES OF THE ODOT TEMPORARY TRAFFIC CONTROL MANUAL AND CONTRACT DOCUMENTS AVAILABLE AT ALL TIMES ON THE PROJECT.

THE DEPARTMENT WILL DEDUCT:

- A. THE PRORATED DAILY AMOUNT OF ITEM 614 - MAINTAINING TRAFFIC FOR ANY DAY IN WHICH THE WTS FAILS TO PERFORM THE DUTIES SET FORTH ABOVE. THE PRORATED DAILY AMOUNT WILL BE EQUAL TO THE ORIGINAL BID AMOUNT FOR ITEM 614 - MAINTAINING TRAFFIC DIVIDED BY THE DIFFERENCE BETWEEN THE ORIGINAL COMPLETION DATE AND THE FIRST DAY OF WORK, IN CALENDAR DAYS.
- B. 1% OF THE ORIGINAL BID AMOUNT FOR ITEM 614 - MAINTAINING TRAFFIC FOR ANY DAY THAT A FAILURE TO PERFORM WTS DUTIES REOCCURS OR A TTC ISSUE IS IDENTIFIED IN THE FIELD AND IS NOT CORRECTED IN THE GIVEN TIME FRAME PER THE ENGINEER. DEDUCTION B SHALL NOT APPLY TO SITUATIONS COVERED BY DEDUCTION C.
- C. 1% OF THE ORIGINAL BID AMOUNT FOR ITEM 614 - MAINTAINING TRAFFIC FOR ANY DAY THAT A LANE OR RAMP IS BLOCKED (FULLY OR PARTIALLY) WITHOUT TTC, AS DETERMINED BY THE ENGINEER. THIS DEDUCTION SHALL BE IN ADDITION TO ANY OTHER DISINCENTIVES ESTABLISHED FOR UNAUTHORIZED LANE USE.

FOR DAYS IN WHICH MORE THAN ONE DEDUCTION LISTED ABOVE OCCUR, THE HIGHEST DEDUCTION AMOUNT WILL APPLY.

IF THREE OR MORE TOTAL DAYS RESULT IN ISSUES DESCRIBED IN DEDUCTION B OR C ABOVE, THE PRIMARY WTS (AND ANY ALTERNATE WTS, IF APPLICABLE) SHALL BE IMMEDIATELY REMOVED FROM THE WORK IN ACCORDANCE WITH C&MS 108.05. UPON REMOVAL THE ENGINEER SHALL NOTIFY ODOT CENTRAL OFFICE (WTSPREQUALIFICATION@DOT.OHIO.GOV) TO REGISTER A REMOVAL AT THE PROJECT LEVEL AGAINST THE STATEWIDE PREQUALIFICATION FOR THE PRIMARY WTS (AND ALTERNATE WTS, IF APPLICABLE). ACCUMULATION OF THREE PROJECT LEVEL REMOVALS (FROM ANY PROJECTS STATEWIDE) SHALL CAUSE STATEWIDE DISQUALIFICATION FOR ANY FORMERLY PREQUALIFIED WTS. A WTS (AND ALTERNATE WTS, IF APPLICABLE) MAY BE IMMEDIATELY AND CONCURRENTLY REMOVED FROM THE WORK AT THE PROJECT LEVEL IN ACCORDANCE WITH C&MS 108.05 AND DISQUALIFIED STATEWIDE FROM THE ODOT PREQUALIFIED WTS ROSTER (REGARDLESS OF THE NUMBER OF PROJECT LEVEL REMOVALS), AS WELL AS BEING SUBJECT TO OTHER POTENTIAL CONSEQUENCES, IN CASES OF FALSIFIED, DISHONEST OR OTHERWISE UNETHICAL ACTIVITY OR DOCUMENTATION.

PAYMENT FOR THE ABOVE REQUIREMENTS, RESPONSIBILITIES AND DUTIES SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614 - MAINTAINING TRAFFIC.

DESIGN AGENCY



DESIGNER

MEP

REVIEWER

LOB 3/18/26

PROJECT ID

122947

SHEET

P.19A

TOTAL

87



1 DETOUR AHEAD
W20-2

2 DETOUR TO WEST
M4-8P
M4-5P
M3-4P
M1-1
M5-1P

3 DETOUR TO WEST
M4-8P
M4-5P
M3-4P
M1-1
M5-1P

4 DETOUR TO WEST
M4-8P
M4-5P
M3-4P
M1-1
M6-3P

EXIT 41B
New York City
Youngstown
EXIT ONLY

EXIT 41A

LEFT EXIT 41A
Cleveland
New York

BLACK ON ORANGE OVERLAY

LEFT EXIT 41A
Cleveland

DETOUR TO WEST
EXIT 41B
New York
Youngstown
EXIT ONLY

BLANK ORANGE OVERLAY (TYPICAL)

DETOUR TO WEST
EXIT 41A
Ohio Turnpike
NEXT LEFT

EXIT 41A-B
Cleveland
Youngstown
JCT 1 MILE



MAINTENANCE OF TRAFFIC
DETOUR PLAN

DESIGN AGENCY	
DESIGNER	JMW
REVIEWER	3/18/26
PROJECT ID	122947
SHEET TOTAL	P.19B 87

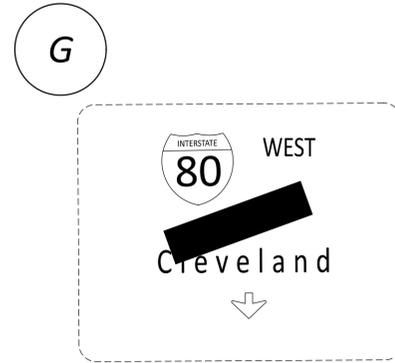
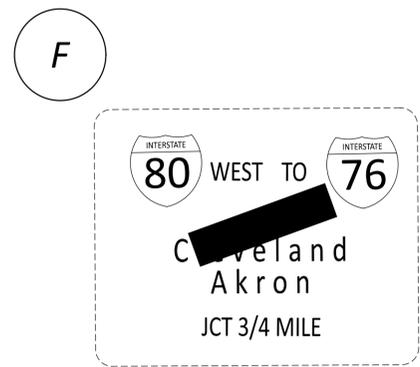
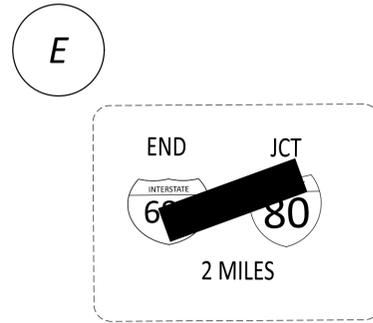
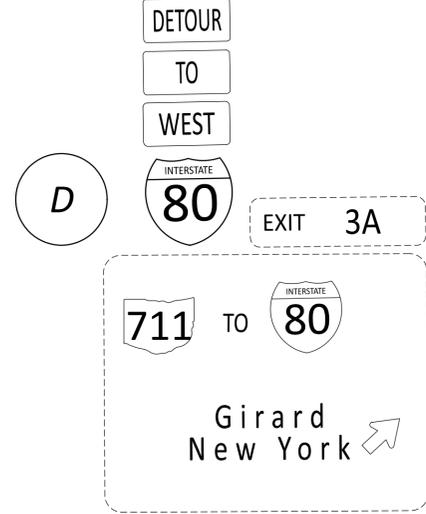
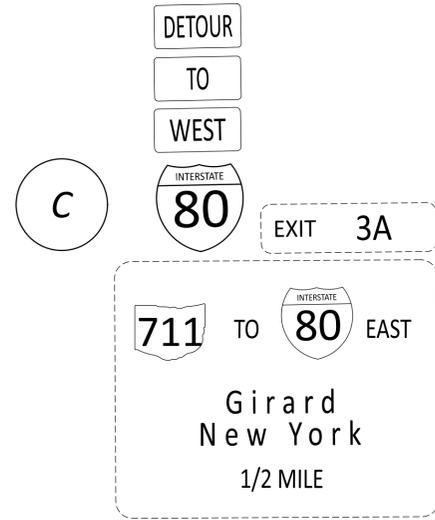
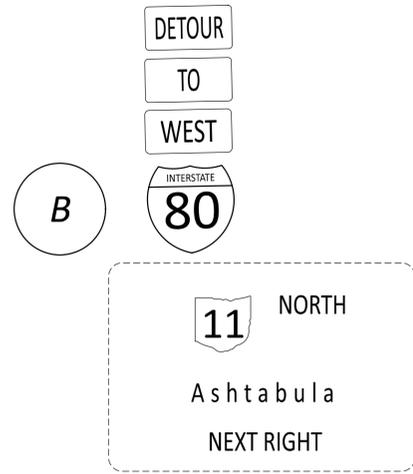
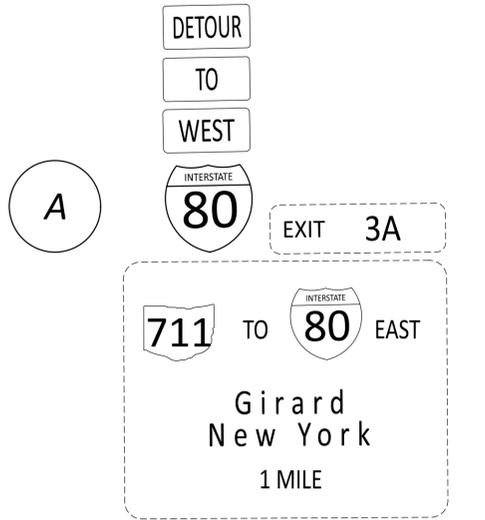
FOR LEGEND ON 680N APPROACH
SEE NEXT SHEET

DETAIL A
SHOWN ON NEXT PAGE

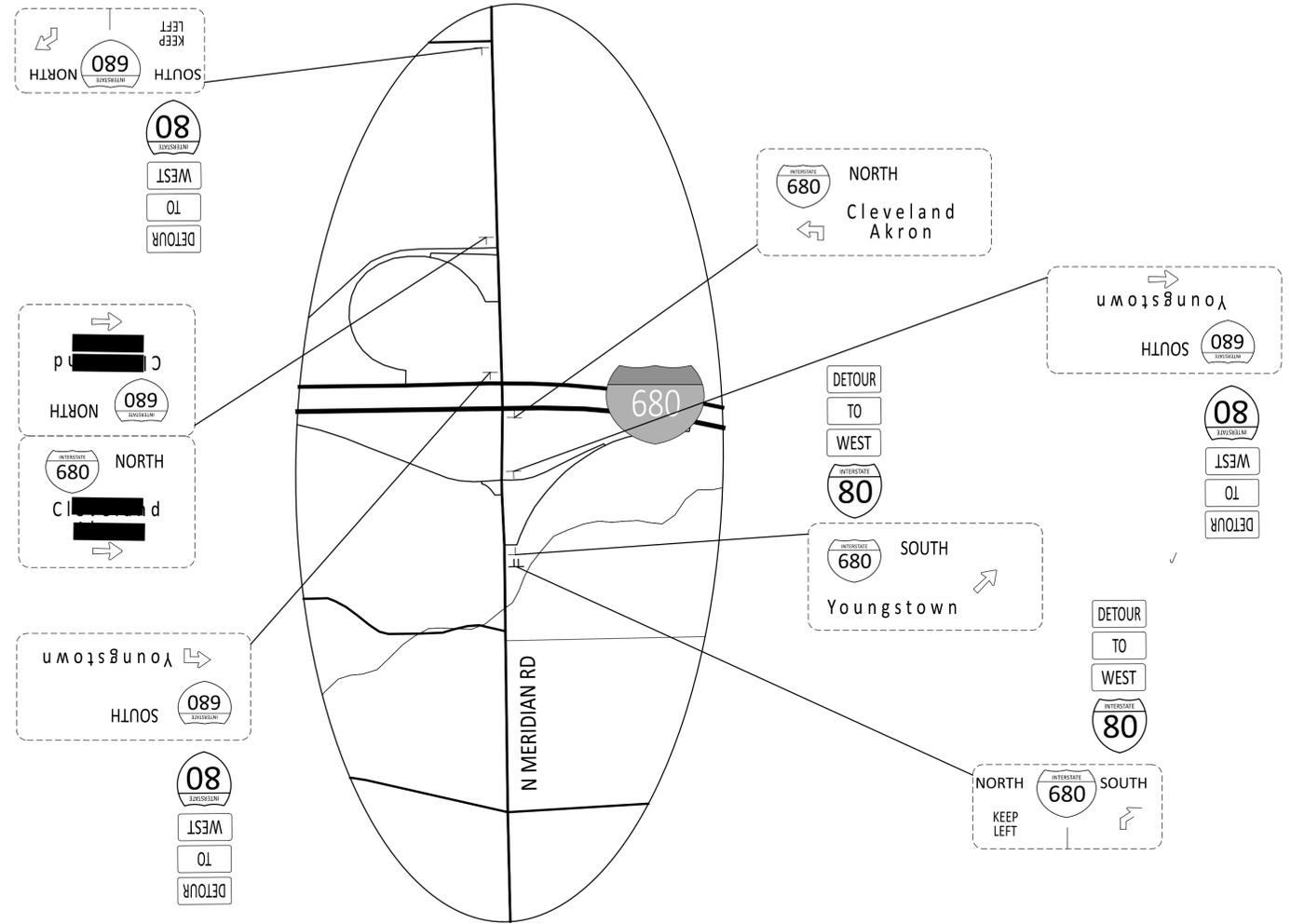
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LOCATION

SUGGESTED PCMS
LOCATION

SIGNS FOR I 680N APPROACH



DETAIL A



MAINTENANCE OF TRAFFIC DETOUR PLAN

DESIGN AGENCY



DESIGNER
JMW
REVIEWER
LOB 3/18/26
PROJECT ID
122947
SHEET TOTAL
P.19C 87

 3-18-26: MOT SECTION REVISED FOR NOTES REVISIONS

SHEET NUMBER											PART.	ITEM	ITEM	GRAND	UNIT	DESCRIPTION	SEE SHEET NO.
P.17	P.18	P.19									01/IMS	EXT	TOTAL				
		250									250	614	11110	250	HOURL	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	
		LS									LS	614	12420	LS		DETOUR SIGNING	
	12										12	614	12484	12	EACH	WORK ZONE INCREASED PENALTIES SIGN	
		250									250	614	13310	250	EACH	BARRIER REFLECTOR, TYPE 1, ONE WAY	
		250									250	614	13310	250	EACH	BARRIER REFLECTOR, TYPE 1, BIDIRECTIONAL	
		250									250	614	13312	250	EACH	BARRIER REFLECTOR, TYPE 2, ONE WAY	
		250									250	614	13312	250	EACH	BARRIER REFLECTOR, TYPE 2, BIDIRECTIONAL	
		50									50	614	13350	50	EACH	OBJECT MARKER, ONE WAY	
		50									50	614	13360	50	EACH	OBJECT MARKER, TWO WAY	
	74										74	614	18601	74	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN	P.18
	52.5										52.5	614	20056	52.5	MILE	WORK ZONE LANE LINE, CLASS I, 6", 807 PAINT	
	17.5										17.5	614	20560	17.5	MILE	WORK ZONE LANE LINE, CLASS III, 6", 642 PAINT	
	60										60	614	22056	60	MILE	WORK ZONE EDGE LINE, CLASS I, 6", 807 PAINT	
	20										20	614	22360	20	MILE	WORK ZONE EDGE LINE, CLASS III, 6", 642 PAINT	
	34,650										34,650	614	23110	34,650	FT	WORK ZONE CHANNELIZING LINE, CLASS I, 12", 807 PAINT	
	11,550										11,550	614	23690	11,550	FT	WORK ZONE CHANNELIZING LINE, CLASS III, 12", 642 PAINT	
	18,180										18,180	614	24102	18,180	FT	WORK ZONE DOTTED LINE, CLASS I, 6", 807 PAINT	
	6,060										6,060	614	24612	6,060	FT	WORK ZONE DOTTED LINE, CLASS III, 6", 642 PAINT	
		100									100	616	10000	100	MGAL	WATER	
54											54	808	18700	54	SNMT	DIGITAL SPEED LIMIT (DSL) SIGN ASSEMBLY	
											LS	614	11000	LS		MAINTAINING TRAFFIC	P.16
											12	619	16020	12	MNTH	FIELD OFFICE, TYPE C	
											LS	623	10000	LS		CONSTRUCTION LAYOUT STAKES AND SURVEYING	
											LS	624	10000	LS		MOBILIZATION	

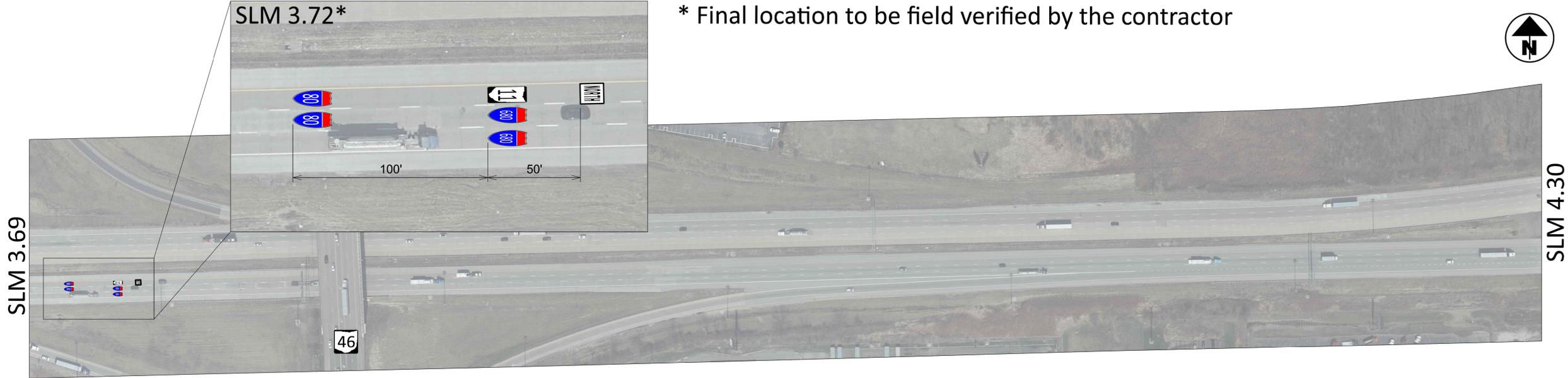


DESIGN AGENCY

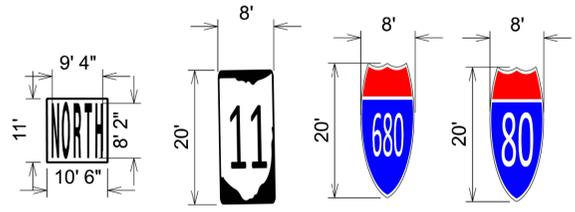
 DESIGNER
 MEP
 REVIEWER
 TJP 3/18/26
 PROJECT ID
 122947
 SHEET TOTAL
 P.22 87

ELONGATED PAVEMENT MARKINGS
 THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

- ITEM 814 - INTERSTATE ELONGATED ROUTE SHIELD SYMBOL MARKING, TYPE B125 4 EACH
- ITEM 814 - STATE ROUTE SHIELD SYMBOL MARKING, TYPE B125 1 EACH
- ITEM 814 - CARDINAL DIRECTION (NORTH, SOUTH, WEST & EAST) MARKING, TYPE B125 1 EACH



* Final location to be field verified by the contractor



3-18-26: PAY ITEMS REVISED