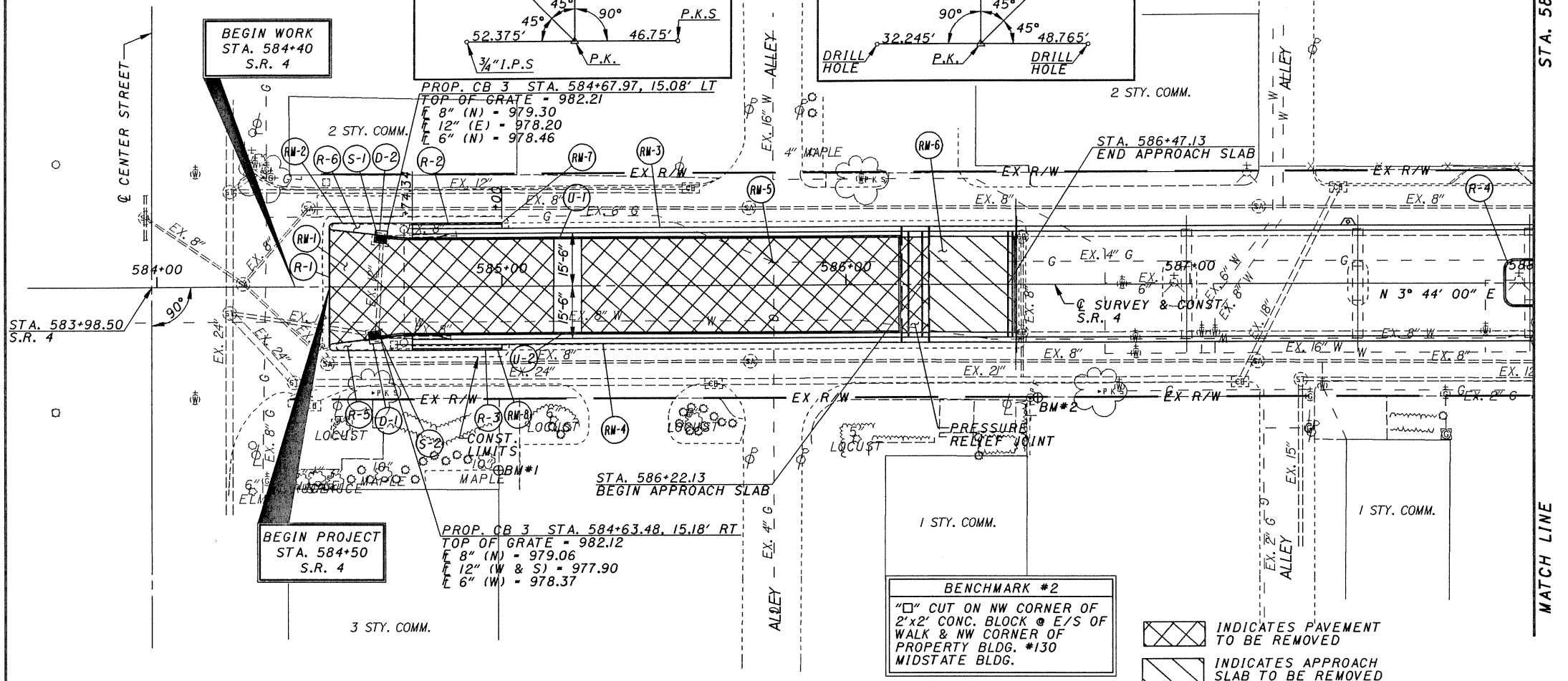


BENCHMARK # 1
 "□" CUT ON NW CORNER OF WALL AROUND AIR CONDITIONING UNIT @ NW CORNER OF UNITED CHURCH HOUSE @ NE. CORNER OF CENTER & STATE STREET

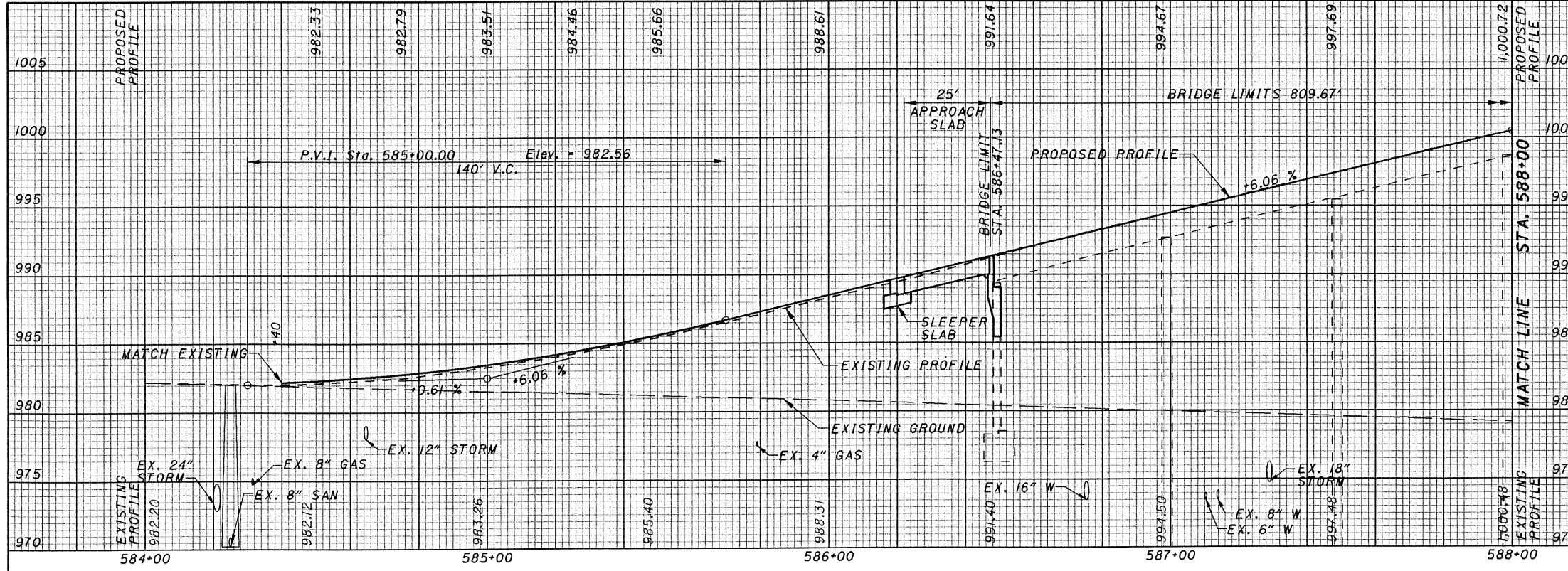
CENTERLINE REFERENCE (NOT TO SCALE)
 STATION 581+00

CENTERLINE REFERENCE (NOT TO SCALE)
 STATION 584+50



BENCHMARK # 2
 "□" CUT ON NW CORNER OF 2'x2' CONC. BLOCK @ E/S OF WALK & NW CORNER OF PROPERTY BLDG. #130 MIDSTATE BLDG.

INDICATES PAVEMENT TO BE REMOVED
 INDICATES APPROACH SLAB TO BE REMOVED



REF NO.	STATION		SIDE	QUANTITY	UNIT													
	FROM	TO																
D-1	584+63.00		RT															
D-2	584+68.00		LT															
R-1	584+50.00		L/R															
R-2	584+74.34	585+00.00	LT	.5														
R-3	584+74.34	585+00.00	RT	.5														
R-4	587+91.00	588+02.00	L/R	45														
R-5	584+50.00	584+74.34	LT	9														
R-6	584+50.00	584+74.34	RT	9														
RM-1	584+50.00	584+74.34	L/R															
RM-2	584+50.00	584+74.34	L/R															
RM-3	584+74.34	586+49.02	LT															
RM-4	584+74.34	586+49.02	RT															
RM-5	584+74.34	586+24.02	L/R															
RM-6	586+24.02	586+49.02	L/R															
RM-7	584+74.00	585+00.00	L/R															
RM-8	584+74.00	585+00.00	L/R															
S-1	584+68.00		LT		7													
S-2	584+63.00		RT		7													
U-1	584+78.00		LT															
U-2	584+63.00		RT															
TOTALS CARRIED TO GENERAL SUMMARY				85	610	300	18	52	9	2	15	2	300	150	150	7	7	14

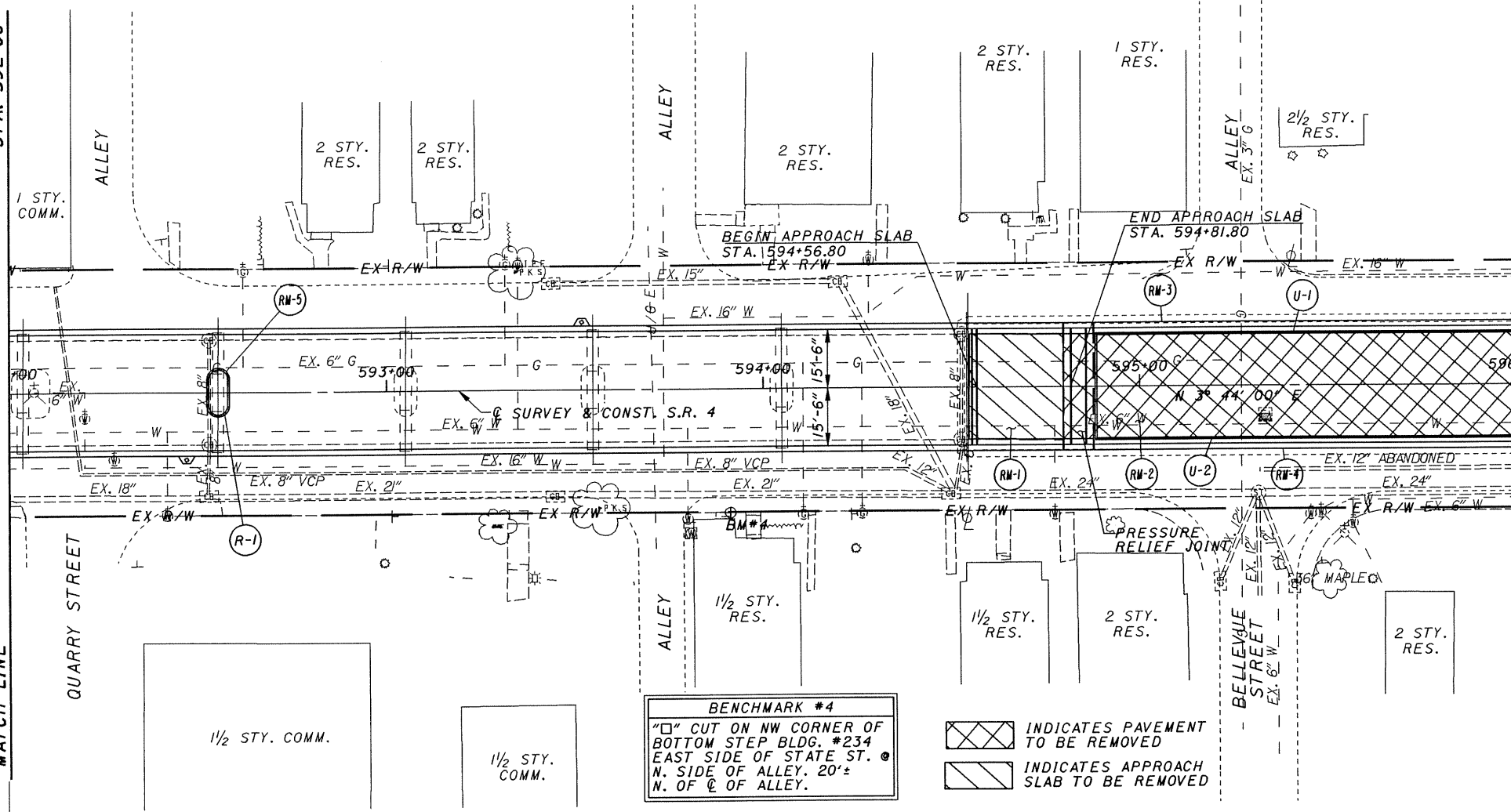
PLAN AND PROFILE
STA. 584+40 TO STA. 588+00

MAR-4-11.70

7
68

STA. 592+00

MATCH LINE



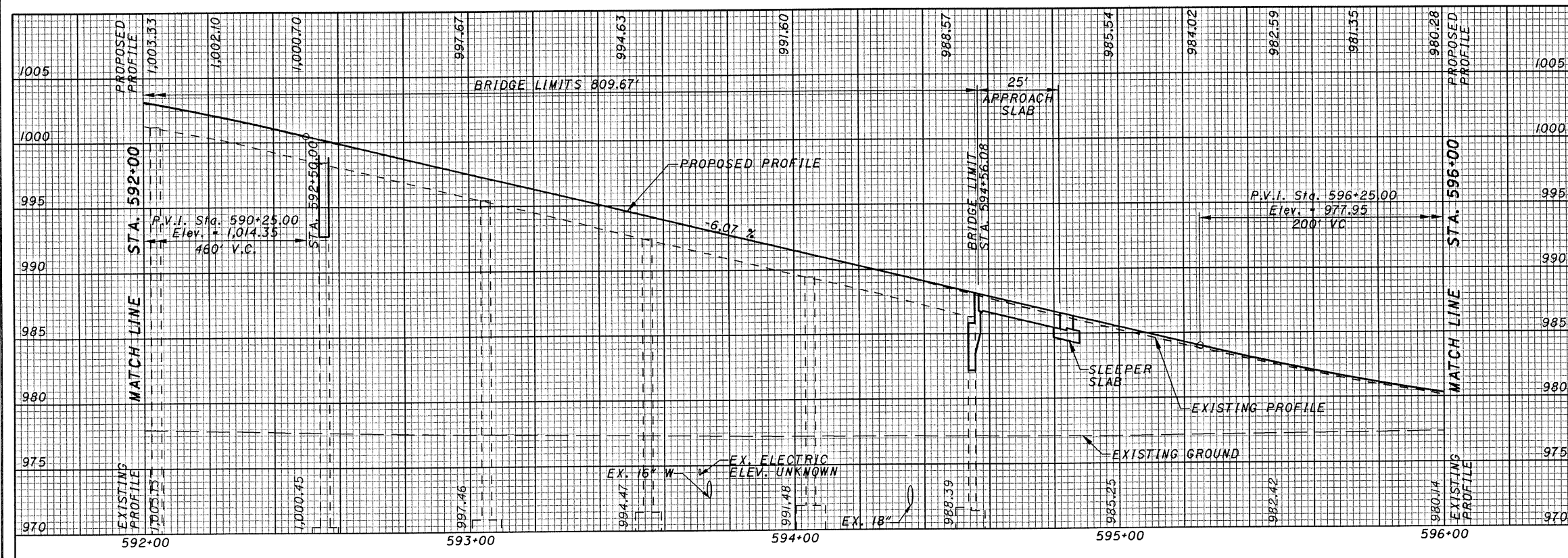
STA. 596+00

MATCH LINE

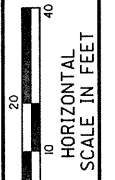
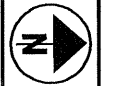
BENCHMARK #4
 "□" CUT ON NW CORNER OF
 BOTTOM STEP BLDG. #234
 EAST SIDE OF STATE ST.
 N. SIDE OF ALLEY. 20'±
 N. OF C. OF ALLEY.

INDICATES PAVEMENT
 TO BE REMOVED

INDICATES APPROACH
 SLAB TO BE REMOVED



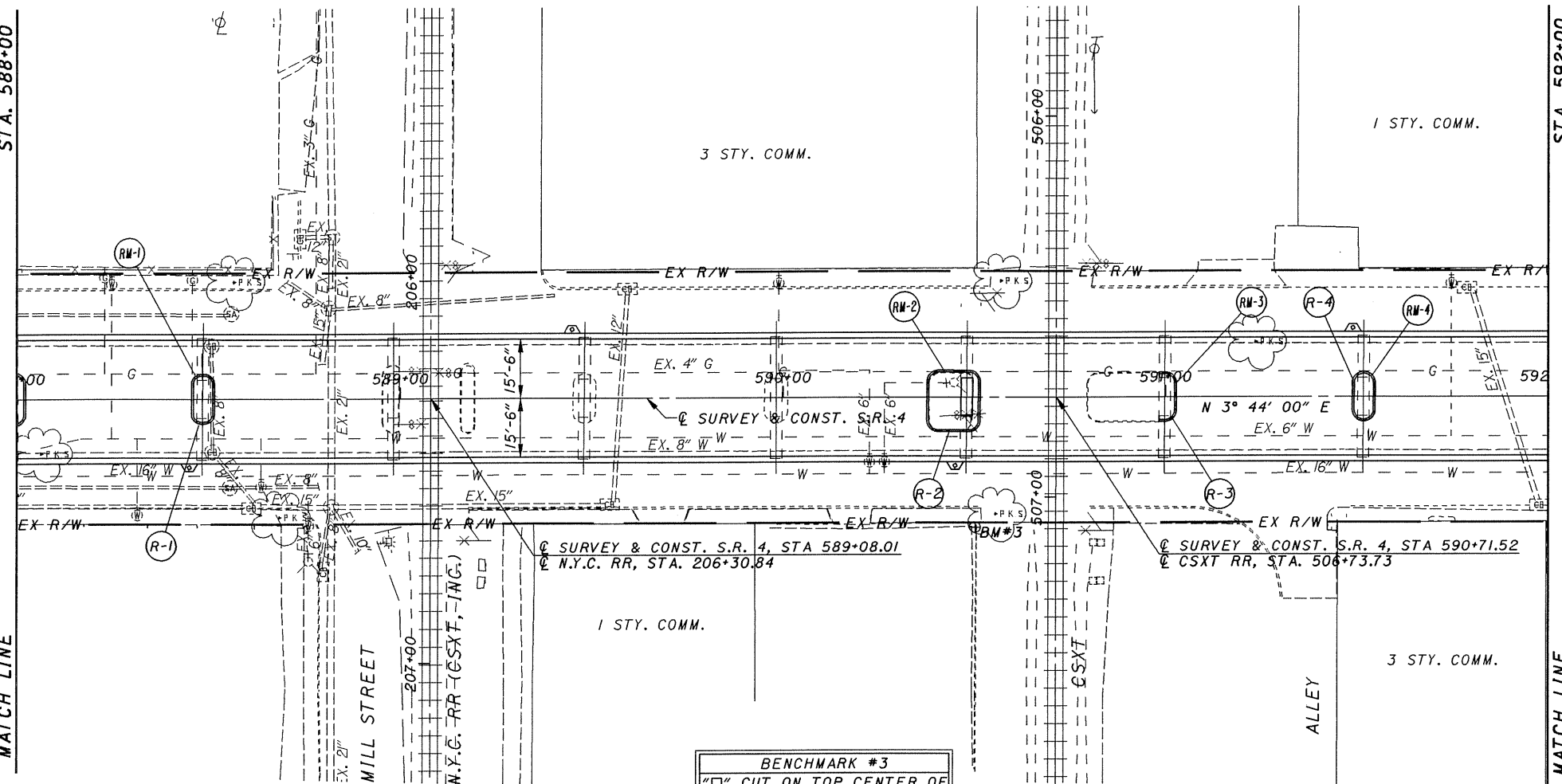
REF NO.	STATION		SIDE	QUANTITY	UNIT	TOTAL
	FROM	TO				
RM-1	594+94.03	596+00.00	L/R	85	SO. YD.	85
RM-2	595+19.03	596+00.00	L/R	275	SO. YD.	275
RM-3	594+94.03	596+00.00	LT	81	LIN. FT.	81
RM-4	594+94.03	596+00.00	RT	81	LIN. FT.	81
RM-5	592+52.00	592+58.00	L/R	33	LIN. FT.	33
R-1	592+52.00	592+58.00	L/R	2.78	SG. YD.	2.78
U-1	594+88.00	596+00.00	LT	112	LIN. FT.	112
U-2	594+88.00	596+00.00	RT	112	LIN. FT.	112
TOTALS CARRIED TO GENERAL SUMMARY				85		85
202	APPROACH SLAB REMOVED			85	SO. YD.	85
202	PAVEMENT REMOVED			275	SO. YD.	275
202	CONCRETE BARRIER REMOVED			162	LIN. FT.	162
202	CURB REMOVED			33	LIN. FT.	33
202	REMOVAL, MISC.			2.78	SG. YD.	2.78
203	EXCAVATION NOT INCLUDING EMBANKMENT CONSTRUCTION			15	CU. YD.	15
203	EXCAVATION NOT INCLUDING EMBANKMENT CONSTRUCTION			46	CU. YD.	46
304	AGGREGATE BASE			.23	CU. YD.	.23
605	6" SHALLOW PIPE UNDERDRAIN			112	LIN. FT.	112
605	6" SHALLOW PIPE UNDERDRAIN			112	LIN. FT.	112
830	CURB, TYPE 2-B, AS PER PLAN			33	LIN. FT.	33
842	CONCRETE, MISC.			.30	CU. YD.	.30



PLAN AND PROFILE
STA. 592+00 TO STA. 596+00

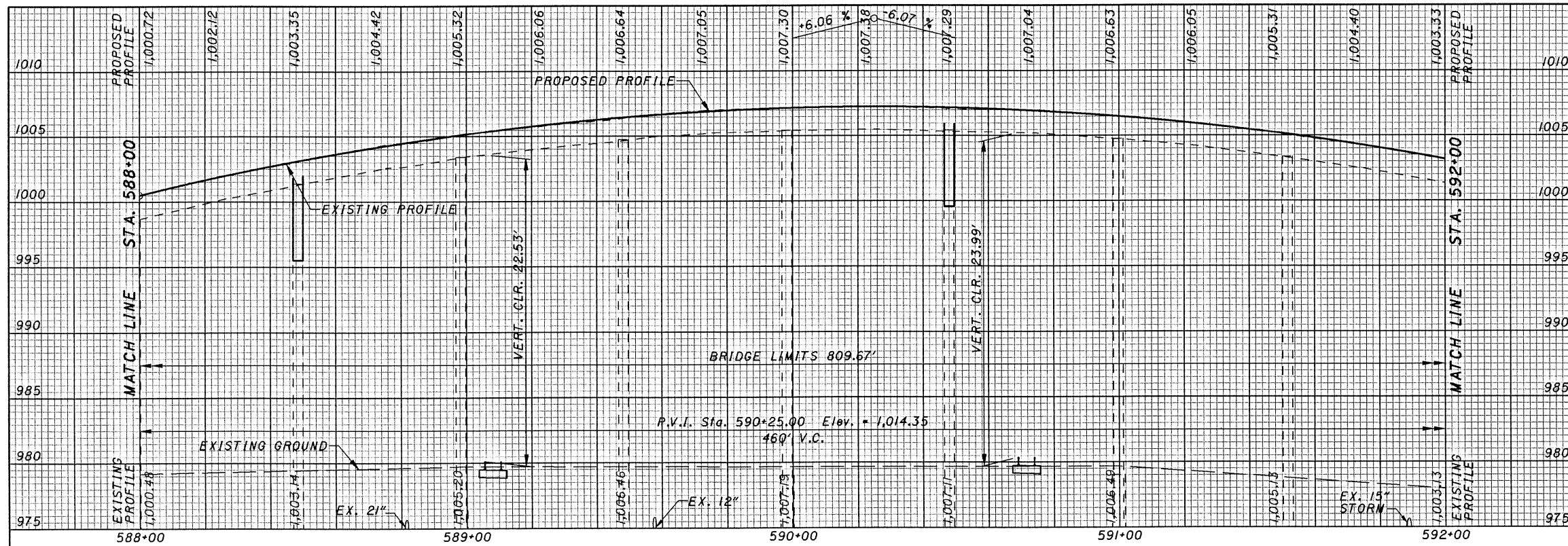
MAR-4-11.70

MATCH LINE STA. 588+00

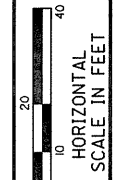


MATCH LINE STA. 592+00

BENCHMARK #3
 "□" CUT ON TOP CENTER OF WEST END OF OLD FOOTER
 @ E/S STATE ST. & S/S RAILROAD.



REF NO.	STATION		SIDE	QUANTITY	UNIT
	FROM	TO			
RM-1	588+45	588+52	L/R	37	CU. YD.
RM-2	590+37	590+52	L/R	55	CU. YD.
RM-3	590+98	591+03	L/R	20	CU. YD.
RM-4	591+48	591+54	L/R	33	CU. YD.
R-1	588+45	588+52	L/R	.40	CU. YD.
R-2	590+37	590+52	L/R	1.50	CU. YD.
R-3	590+98	591+03	L/R	.15	CU. YD.
R-4	591+48	591+54	L/R	.23	CU. YD.
TOTALS CARRIED TO GENERAL SUMMARY				145	2.30
842	CONCRETE MISC.		CU. YD.	3	
830	CURB, TYPE 2-B A.P.P.		LIN. FT.	145	
304	AGGREGATE BASE		CU. YD.	2.30	
202	REMOVAL MISC. CONCRETE CAP		SQ. YD.	27.50	
202	CURB REMOVED		LIN. FT.	145	

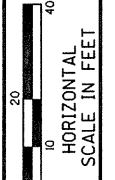
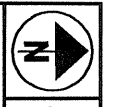


CALCULATED
 NAH
 CHECKED
 EBS

PLAN AND PROFILE
 STA. 588+00 TO STA. 592+00

MAR-4-11.70

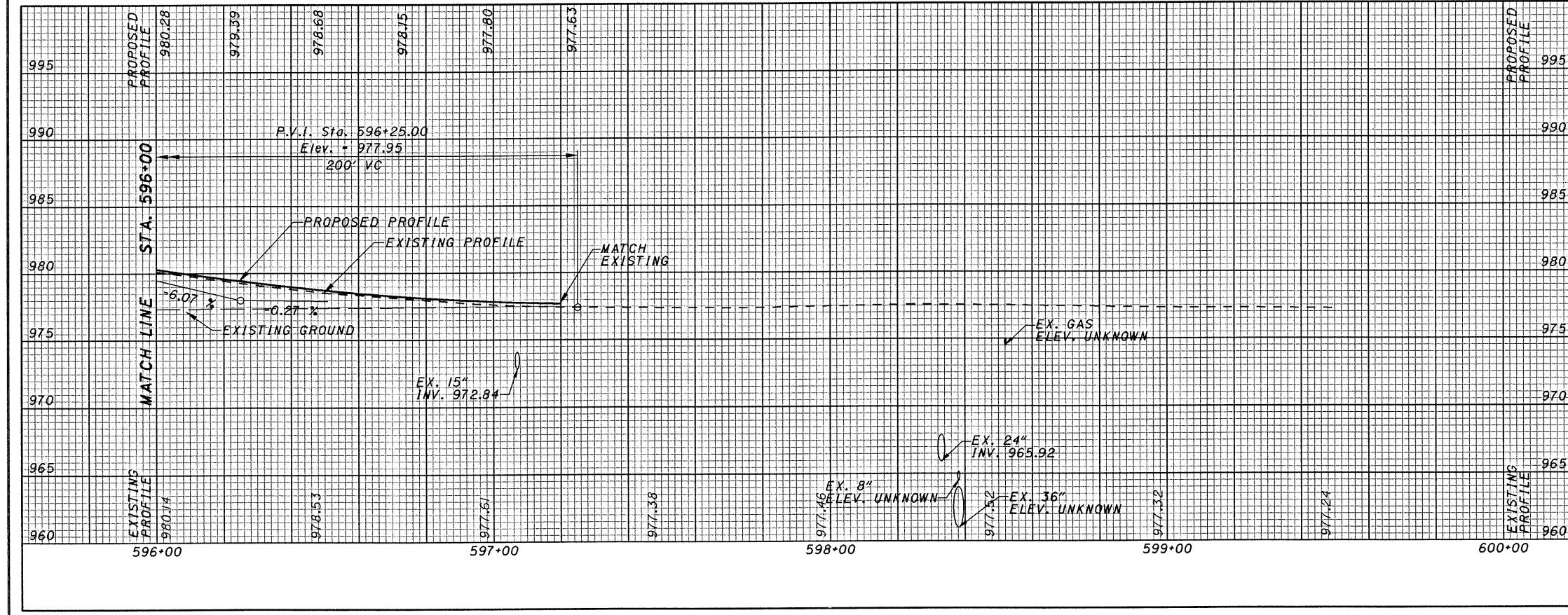
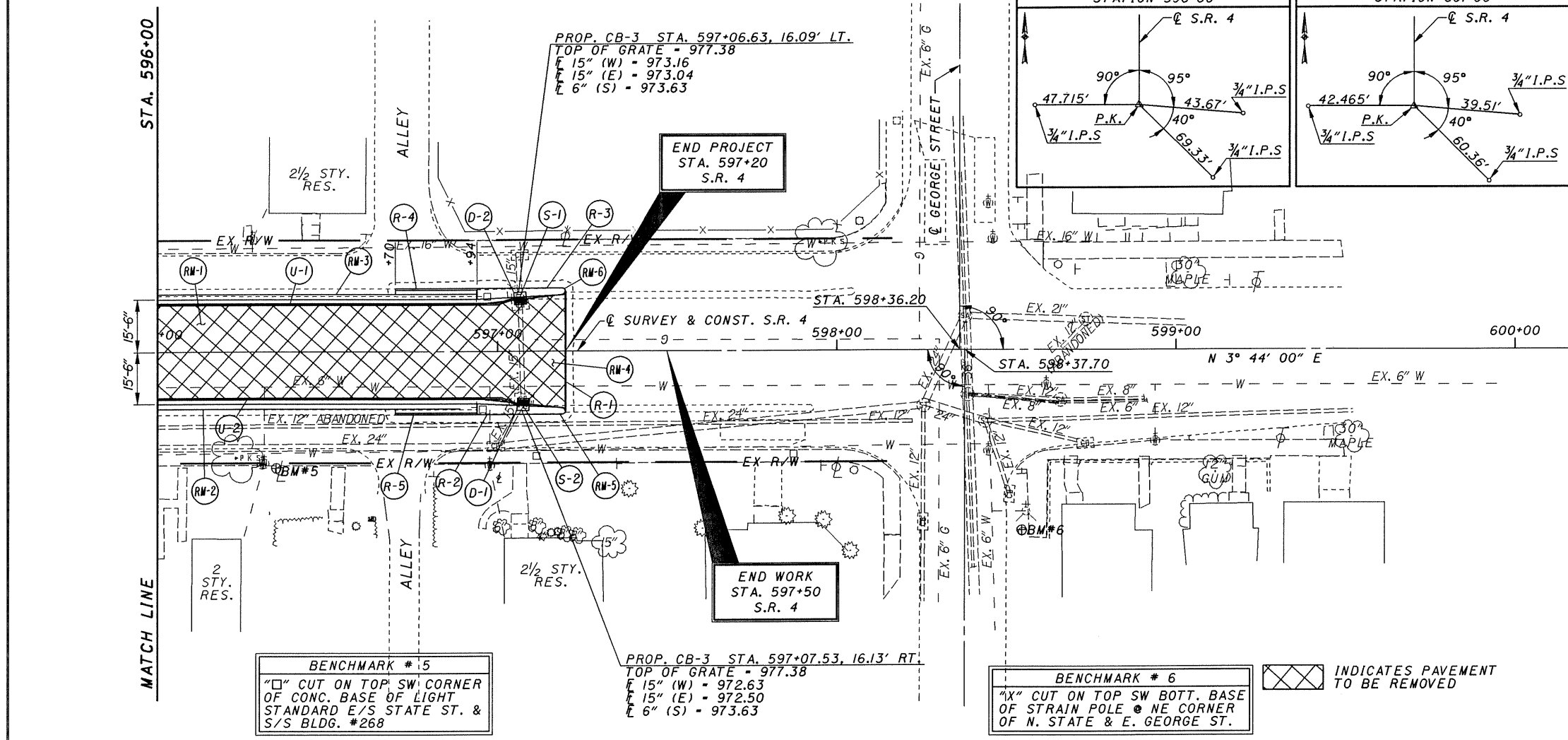
8
 68



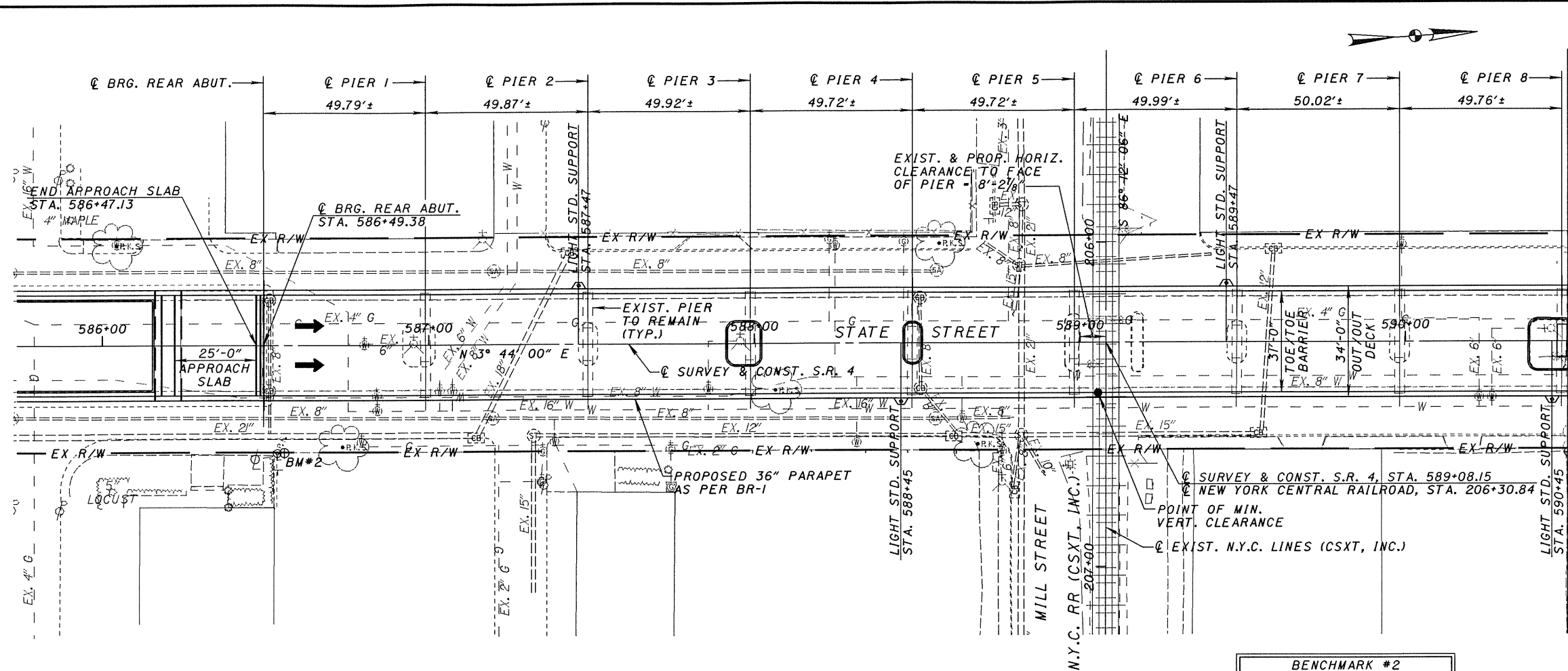
PLAN AND PROFILE
STA. 596+00 TO STA. 597+50

MAR-4-11.70

10
68



REF NO.	STATION	SIDE	GENERAL SUMMARY	
			FROM	TO
D-1	597+07.00	RT		
D-2	597+07.00	LT		
R-1	597+20.00	L/R		
R-2	596+94.00	RT	597+20.00	
R-3	596+94.00	LT	597+20.00	
R-4	596+70.00	LT	596+94.00	
R-5	596+70.00	RT	596+94.00	
RM-1	596+00.00	L/R	596+94.03	
RM-2	596+00.00	RT	596+94.03	
RM-3	596+00.00	LT	596+94.03	
RM-4	596+94.03	L/R	597+20.00	
RM-5	596+94.03	RT	597+20.00	
RM-6	596+94.00	LT	597+20.00	
S-1	597+08.00	LT		
S-2	597+08.00	RT		
U-1	596+00.00	LT	597+07.00	
U-2	596+00.00	RT	597+07.00	
TOTALS CARRIED TO GENERAL SUMMARY			428	188
			319	94
			109	94
			10	24
			10	24
			89	89
			10	10
			10	10
			2	2
			54	54
			89	89
			20	20
			2	2
			214	214
			48	48
			15	15
			1	1
			14	14



PLAN

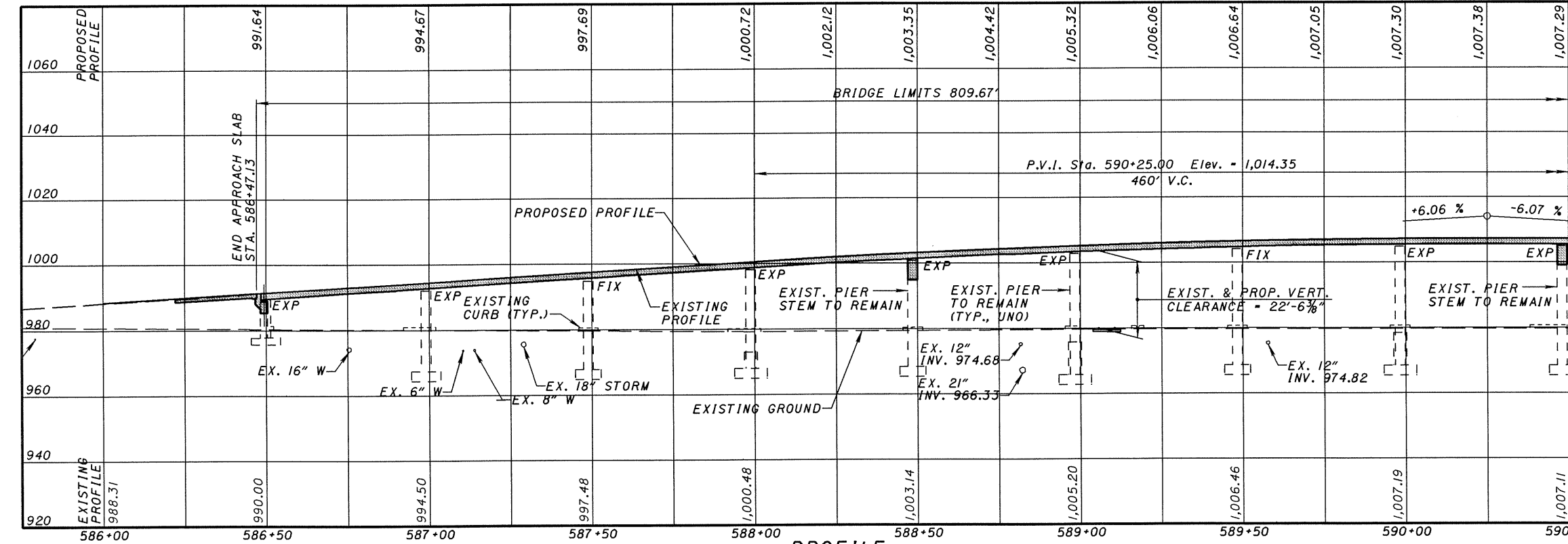
BENCHMARK #2
 "□" CUT ON NW CORNER OF
 2'x2' CONC. BLOCK @ E/S OF
 WALK & NW CORNER OF
 PROPERTY BLDG. #130
 MIDSTATE BLDG.
 STA. 586+55.82, 33.14' RT.

PROPOSED WORK:
 NEW COMPOSITE CONCRETE DECK, 36" CONCRETE DEFLECTOR PARAPETS, AND BOX BEAMS ON EXISTING SUBSTRUCTURES. EXISTING CONCRETE APPROACH SLABS TO BE REPLACED AS PER AS-1-81 (25'-0"). UNSOUND CONCRETE ON EXISTING RETAINING WALLS, ABUTMENTS AND PIERS TO BE REPAIRED. EXISTING STRIP SEAL EXPANSION JOINTS TO BE REPLACED AT BOTH ABUTMENTS AND PIERS 4, 8 & 12. EXISTING PIER CAPS TO BE REPLACED AT PIERS 4, 8 & 12. EXISTING BRIDGE LIGHTING TO BE UPGRADED AND REFINISHED AS NECESSARY. HIGHWAY TRAFFIC TO BE DETOURED DURING CONSTRUCTION, RAILROAD TRAFFIC TO BE MAINTAINED.

DESIGN AGENCY
PARSONS BRINCKERHOFF OHIO, INC.
 6235 ENTERPRISE COURT
 DUBLIN, OHIO 43016

DATE 02/11/02
 REVIEWED P.J.L.
 DRAWN M.W.E.
 DESIGNED E.B.S.
 CHECKED E.J.C.

MARION COUNTY
 STA. 586+47.13
 STA. 594+56.80



PROFILE
 ALONG & SURVEY & CONST.

TRAFFIC DATA

ADT (2000) = 5917
ADT (2020) = 7969
ADTT (2020) = 274

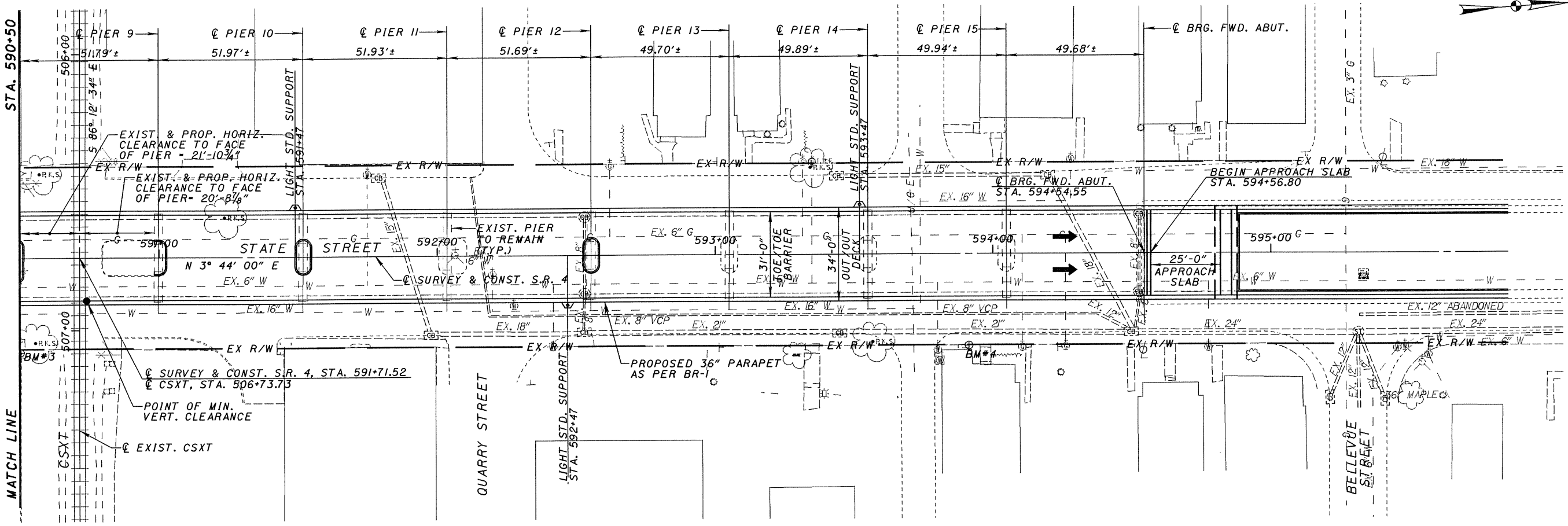
EXISTING STRUCTURE
 TYPE: PRESTRESSED COMPOSITE BOX BEAM BRIDGE W/ CONC. DECK & SUBSTRUCTURE
 SPANS: (APPROX. LENGTHS) 8 @ 50'-0", 4 @ 52'-0" & 4 @ 50'-0"
 DESIGN LOADING: S20-60
 SKEW: NONE
 ROADWAY: 28'-0" W/ 2'-0" SAFETY CURB
 WEARING SURFACE: 1" MONOLITHIC
 APPROACH SLABS: AS-1-54 (25' LONG)
 RAILING: BRIDGE RAILING, TYPE I
 ALIGNMENT: TANGENT

PROPOSED STRUCTURE
 PROPOSED WORK: NEW COMPOSITE CONCRETE DECK AND REINFORCED CONCRETE DEFLECTOR PARAPETS ON PROPOSED BOX BEAMS AND EXISTING SUBSTRUCTURE.
 SPANS: 49.79'±, 49.87'±, 49.92'±, 49.72'±, 49.72'±, 49.99'±, 50.02'±, 49.76'±, 51.79'±, 51.97'±, 51.93'±, 51.69'±, 49.70'±, 49.89'±, 49.94'±, 49.68'±
 ROADWAY: 31'-0" TOE/TOE BARRIER
 DESIGN LOADING: HS-25 & ALTERNATE MILITARY LOADING
 SKEW: NONE
 WEARING SURFACE: MONOLITHIC
 APPROACH SLABS: AS-1-81 (25' LONG)
 ALIGNMENT: TANGENT
 LATITUDE: 40°35'30" NORTH
 LONGITUDE: 83°07'35" WEST

SITE PLAN
 BRIDGE NO. MAR-4-1171
 OVER NEW YORK CENTRAL LINES LLC
 (CSXT, INC.) & CSXT

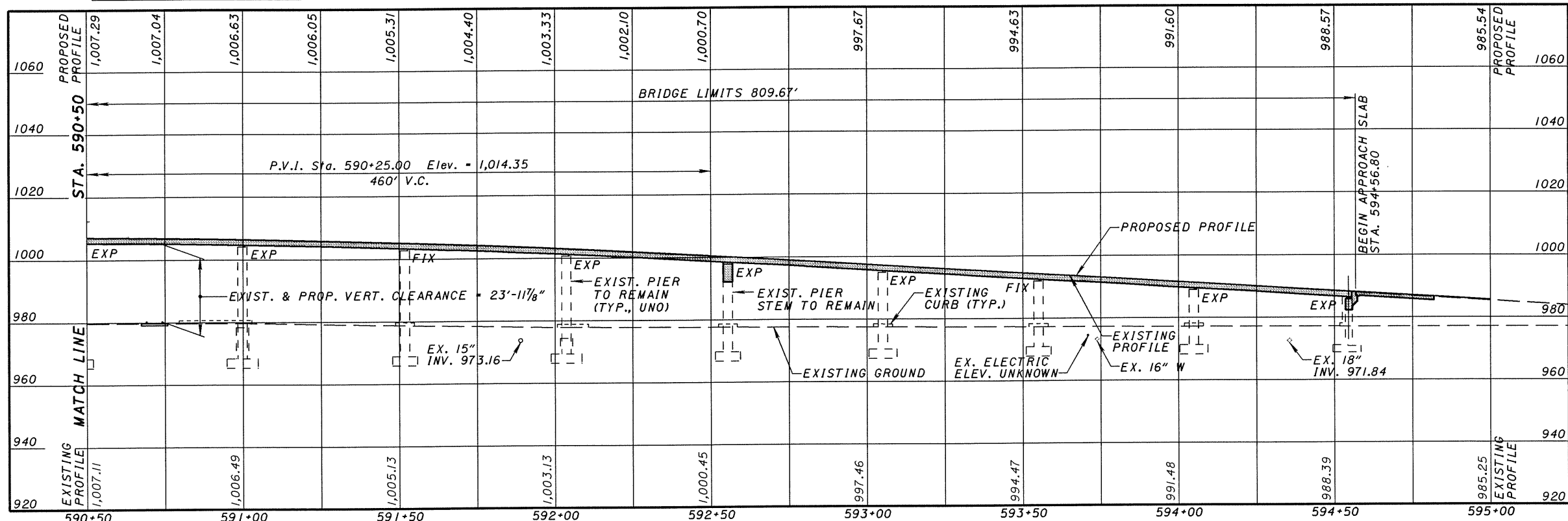
MAR-4-11.70

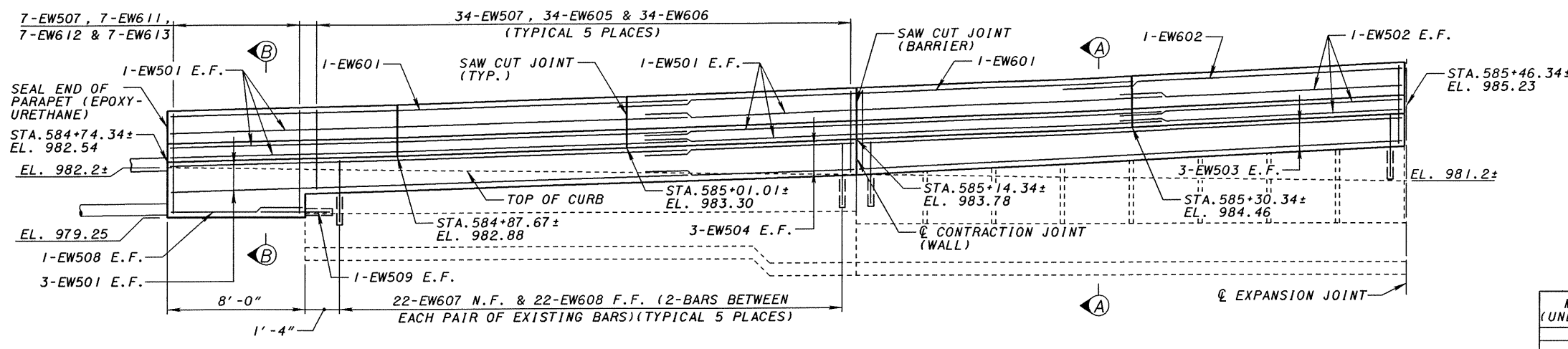
1/38
 31
 68



BENCHMARK #3
 "□" CUT ON TOP CENTER OF
 WEST END OF OLD FOOTER
 @ E/S STATE ST. & S/S
 RAILROAD.
 STA. 590+50.26, 34.16' RT.

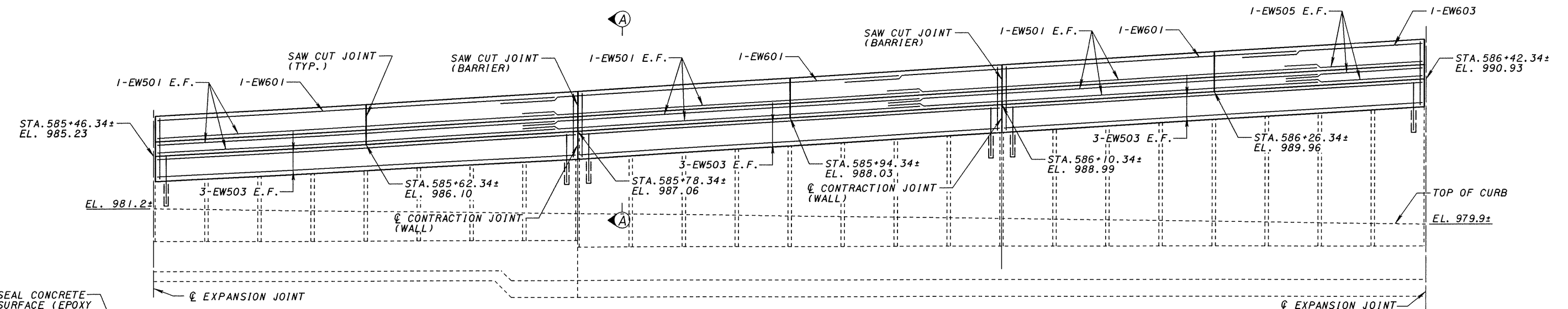
BENCHMARK #4
 "□" CUT ON NW CORNER OF
 BOTTOM STEP BLDG. #234
 EAST SIDE OF STATE ST. @
 N. SIDE OF ALLEY, 20'±
 N. OF C/OF ALLEY.
 STA. 593+91.39, 33.15' RT.



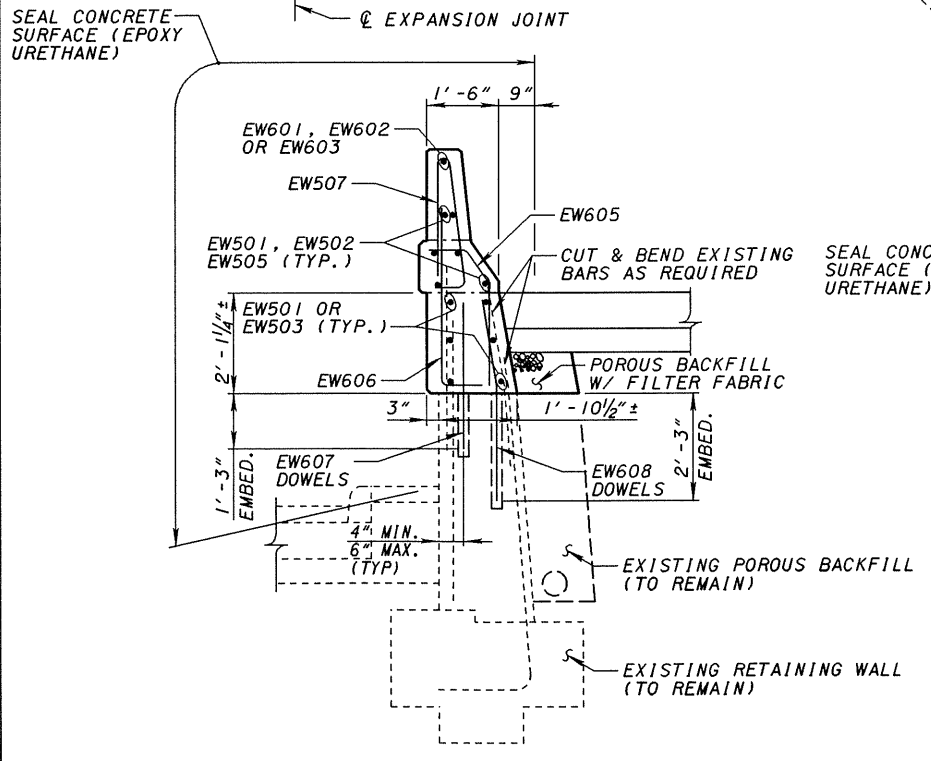


ELEVATION
(EAST WALL SHOWN,
WEST WALL OPPOSITE HAND)

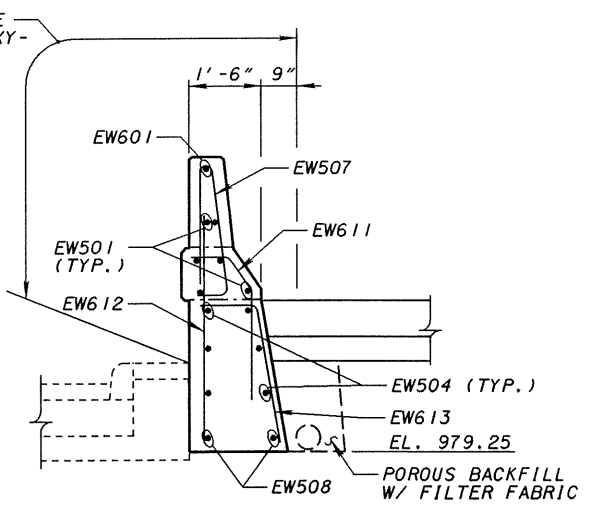
MINIMUM LAP LENGTH (UNLESS NOTED OTHERWISE)	
#5 BAR	= 2'-5"
#6 BAR	= 4'-1"



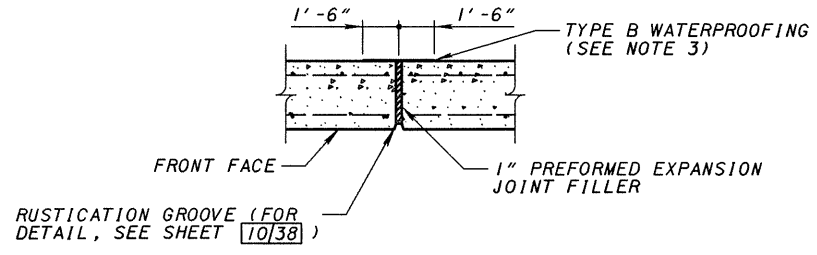
ELEVATION
(EAST WALL SHOWN,
WEST WALL OPPOSITE HAND)



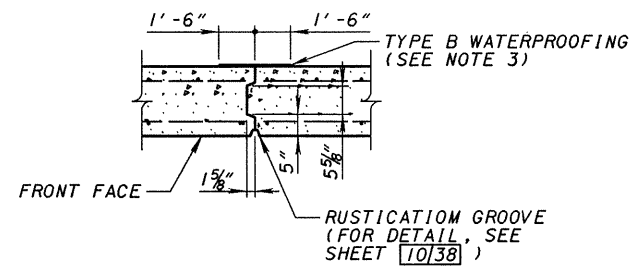
SECTION A-A



SECTION B-B

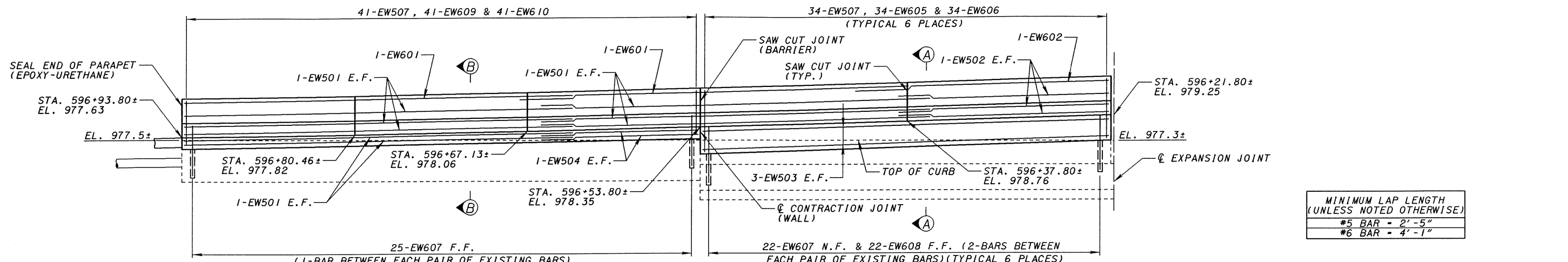


EXPANSION JOINT DETAIL

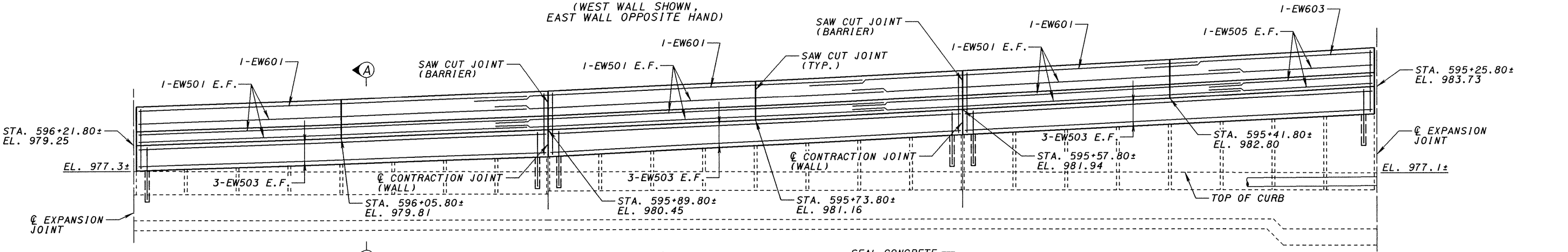


CONTRACTION JOINT DETAIL

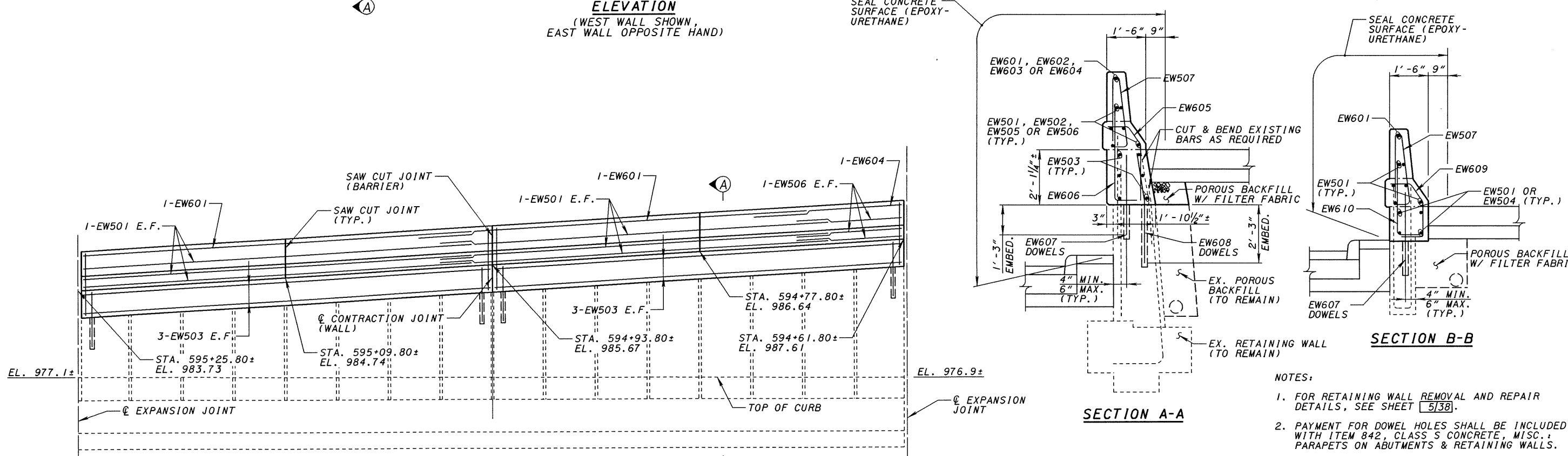
- NOTES:
- FOR RETAINING WALL REMOVAL AND REPAIR DETAILS, SEE SHEET 5/38.
 - PAYMENT FOR DOWEL HOLES SHALL BE INCLUDED WITH ITEM 842, CLASS 5 CONCRETE, MISC.; PARAPETS ON ABUTMENTS & RETAINING WALLS.
 - EXTEND TYPE B WATERPROOFING 6" BEYOND REMOVAL ELEVATION AND TO THE BOTTOM OF THE ROADWAY PAVEMENT.



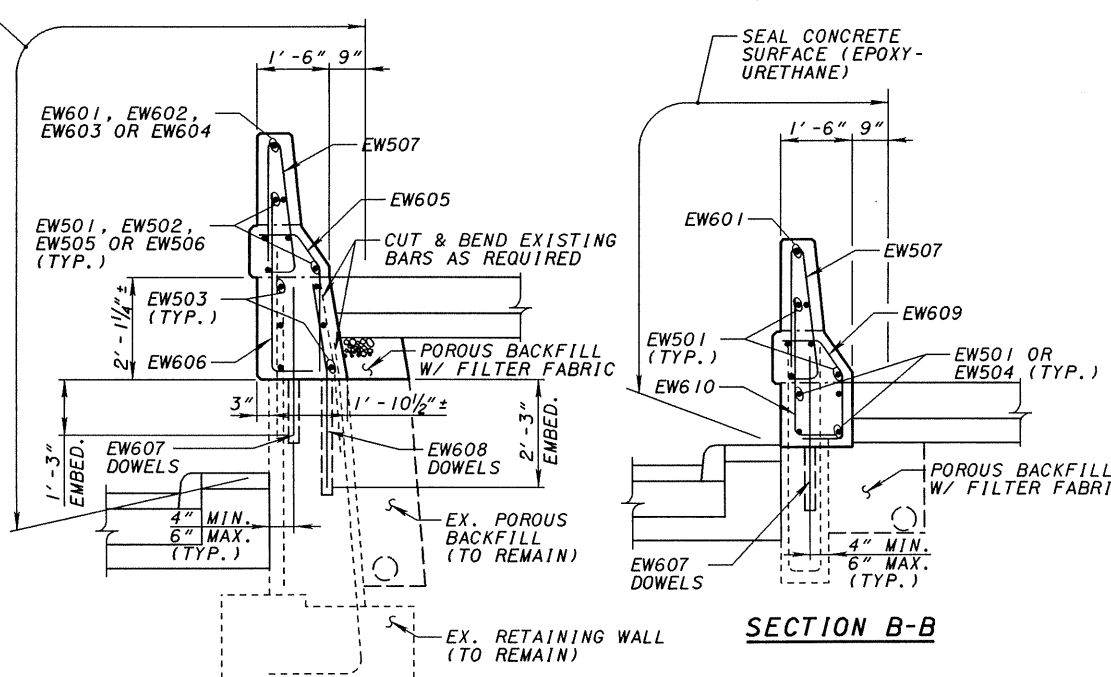
ELEVATION
(WEST WALL SHOWN,
EAST WALL OPPOSITE HAND)



ELEVATION
(WEST WALL SHOWN,
EAST WALL OPPOSITE HAND)



ELEVATION
(WEST WALL SHOWN,
EAST WALL OPPOSITE HAND)

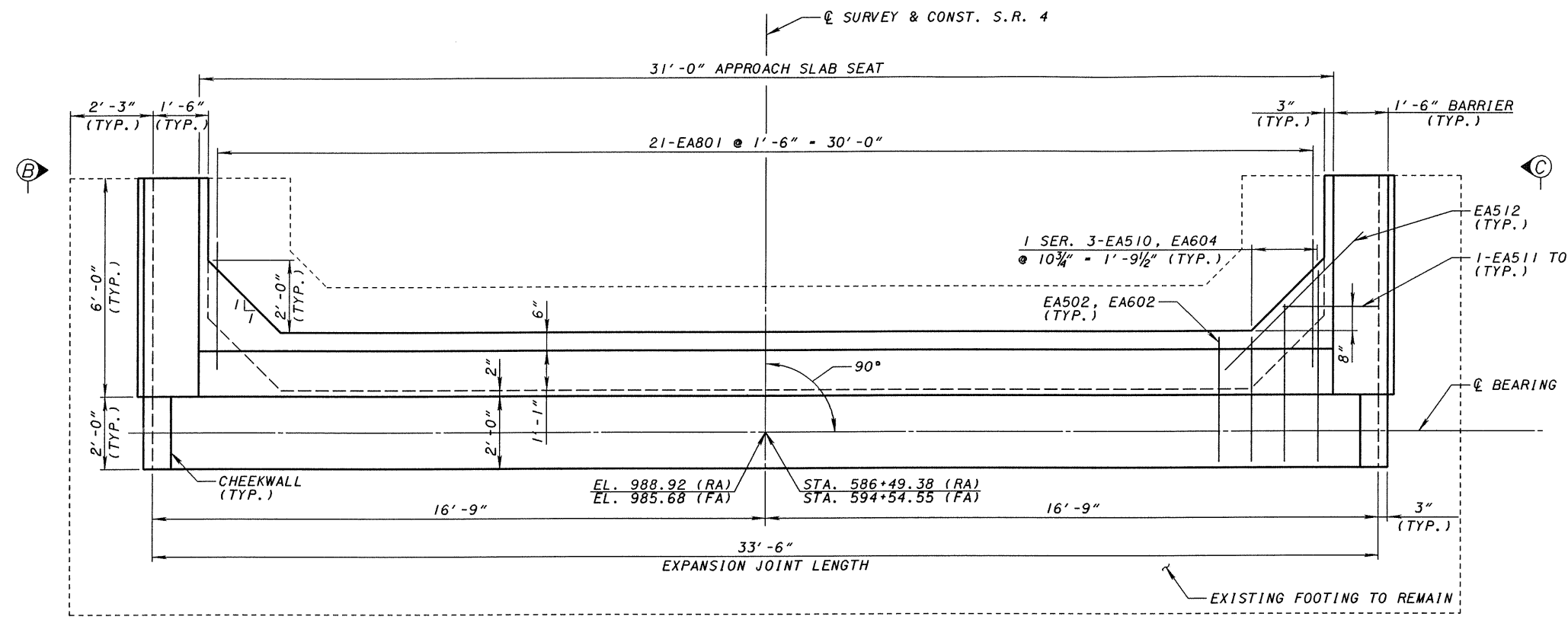


SECTION A-A

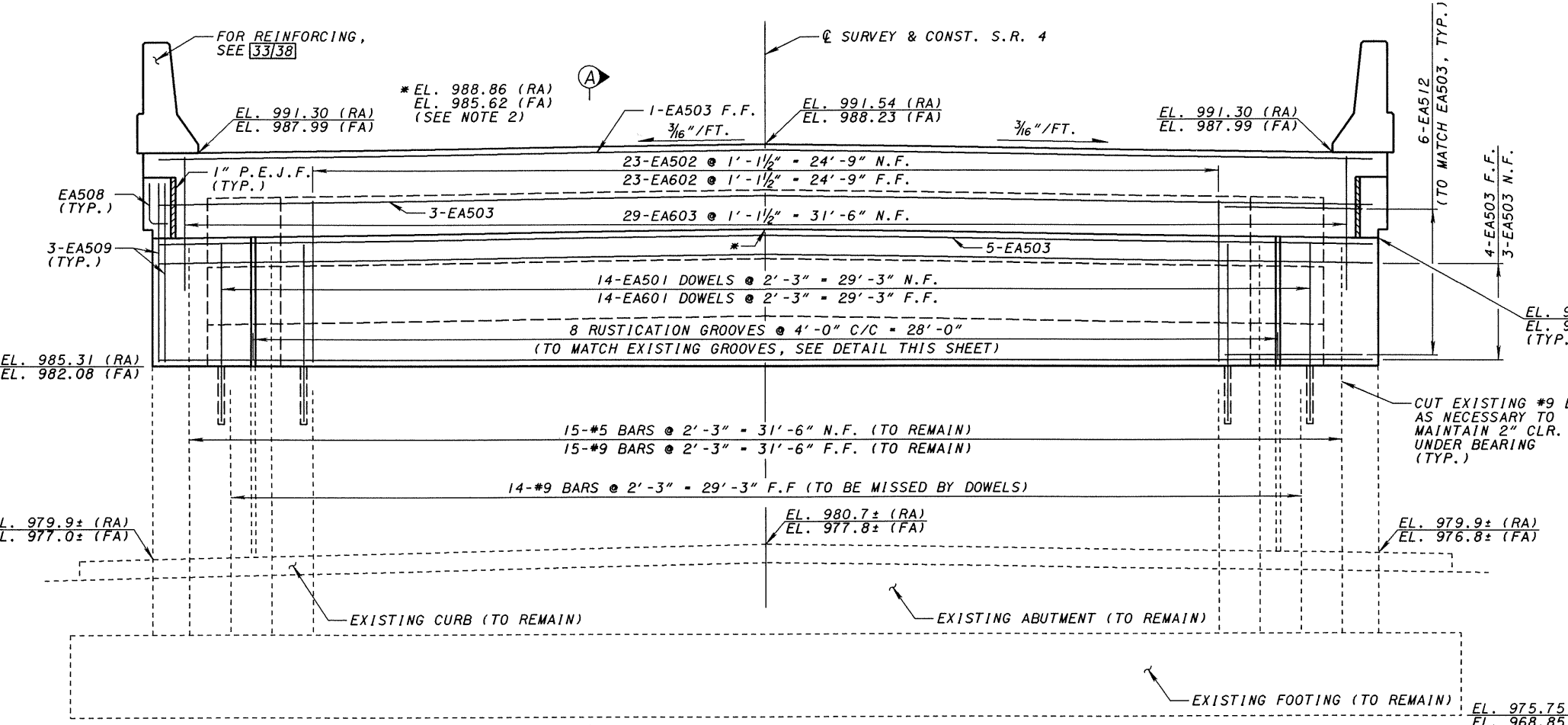
SECTION B-B

- NOTES:
1. FOR RETAINING WALL REMOVAL AND REPAIR DETAILS, SEE SHEET [5/38].
 2. PAYMENT FOR DOWEL HOLES SHALL BE INCLUDED WITH ITEM 842, CLASS S CONCRETE, MISC.; PARAPETS ON ABUTMENTS & RETAINING WALLS.
 3. EXTEND TYPE B WATERPROOFING 6" BEYOND REMOVAL ELEVATION AND TO THE BOTTOM OF THE ROADWAY PAVEMENT
 4. FOR EXPANSION AND CONTRACTION JOINT DETAILS, SEE SHEET [6/38].

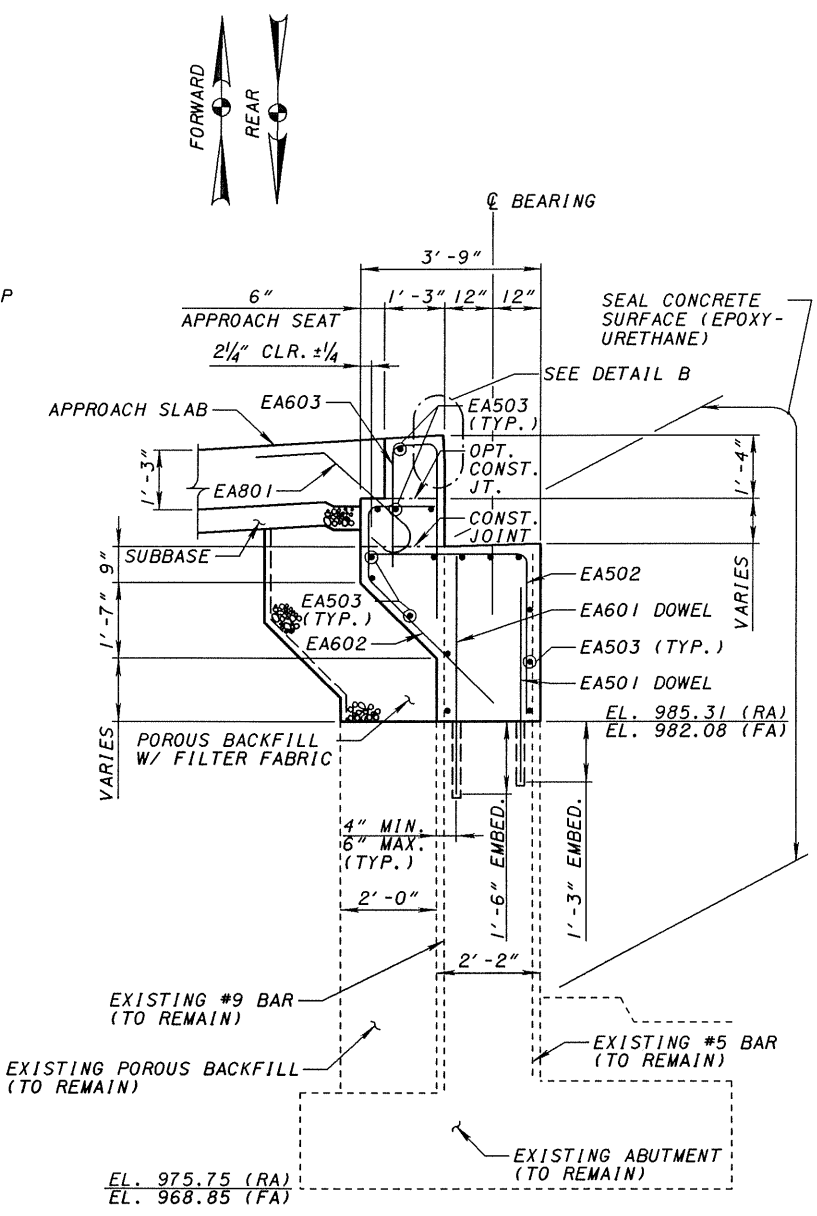
MINIMUM LAP LENGTH (UNLESS NOTED OTHERWISE)	
#5 BAR	= 2'-5"
#6 BAR	= 4'-1"



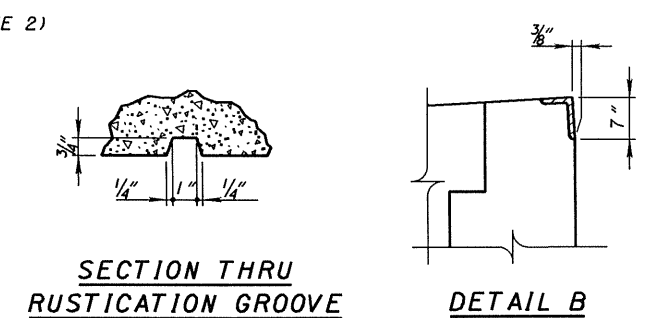
PLAN



ELEVATION
 (REAR ABUTMENT LOOKING SOUTH,
 FORWARD ABUTMENT LOOKING NORTH)

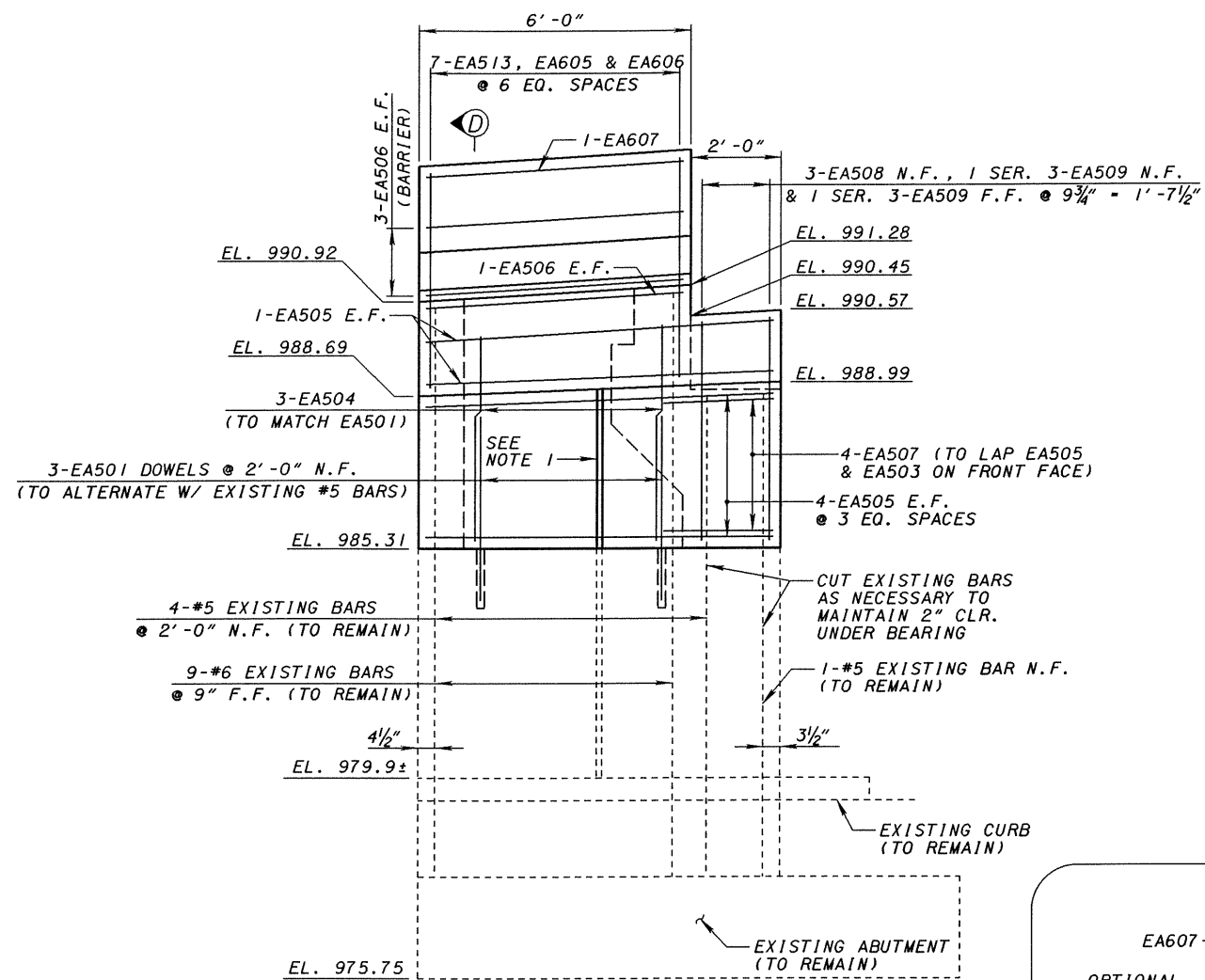


SECTION A-A

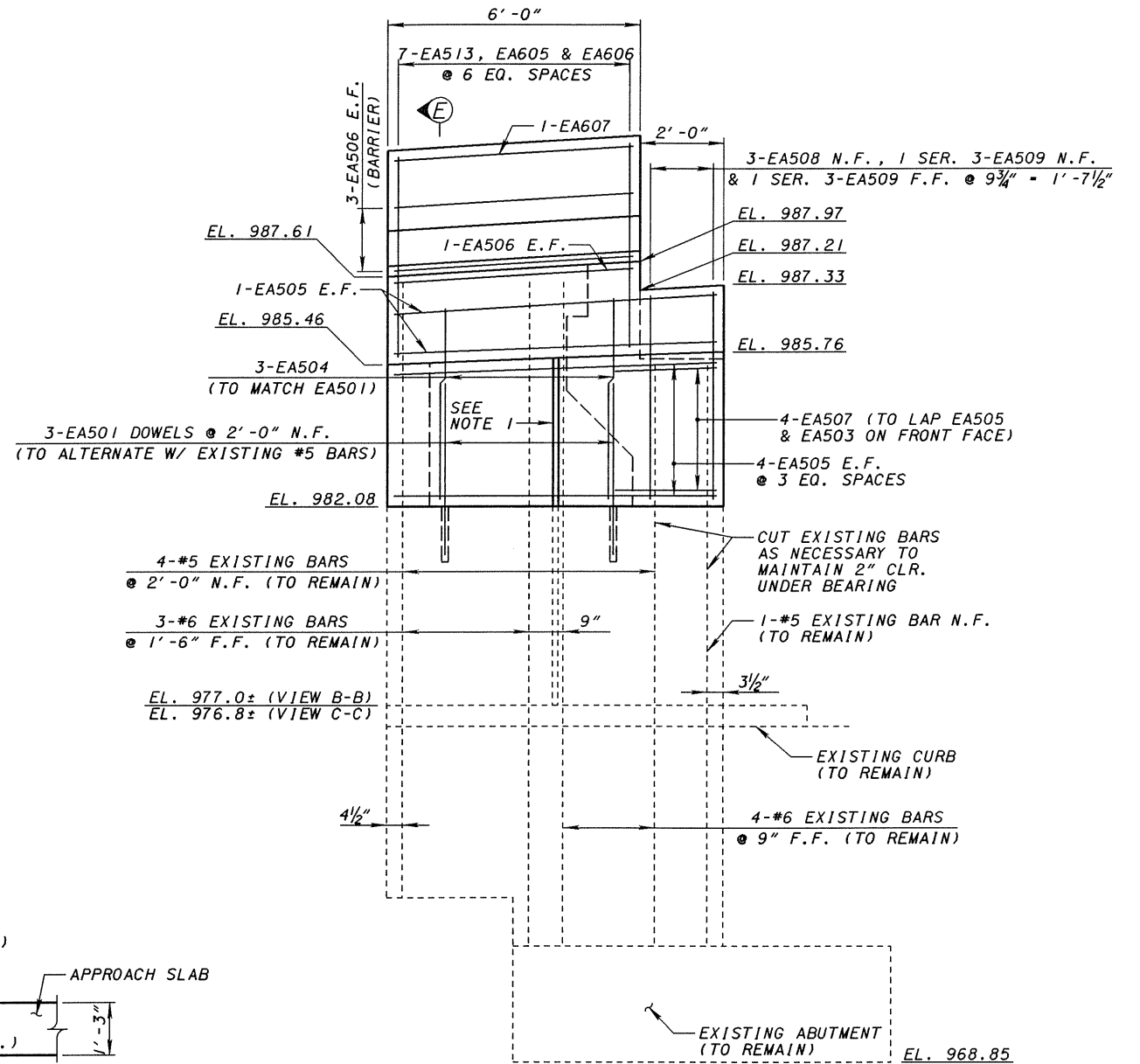


SECTION THRU RUSTICATION GROOVE
DETAIL B

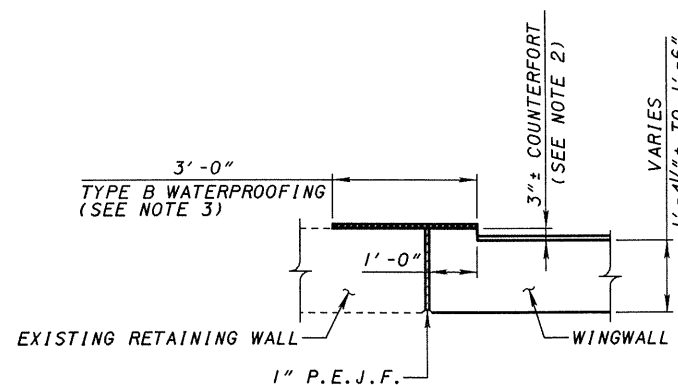
- NOTES:
- FOR VIEW B-B & VIEW C-C, SEE SHEET 11/38.
 - ELEVATIONS ALONG BACKWALL ARE GIVEN AT THE FRONT FACE OF BACKWALL.
 - PAYMENT FOR DOWEL HOLES SHALL BE INCLUDED WITH ITEM 842, CLASS C CONCRETE, ABUTMENT.
 - ABUTMENT CONCRETE ABOVE THE BRIDGE SEAT CONSTRUCTION JOINT SHALL NOT BE PLACED UNTIL THE PRESTRESSED CONCRETE BOX BEAMS HAVE BEEN ERECTED.



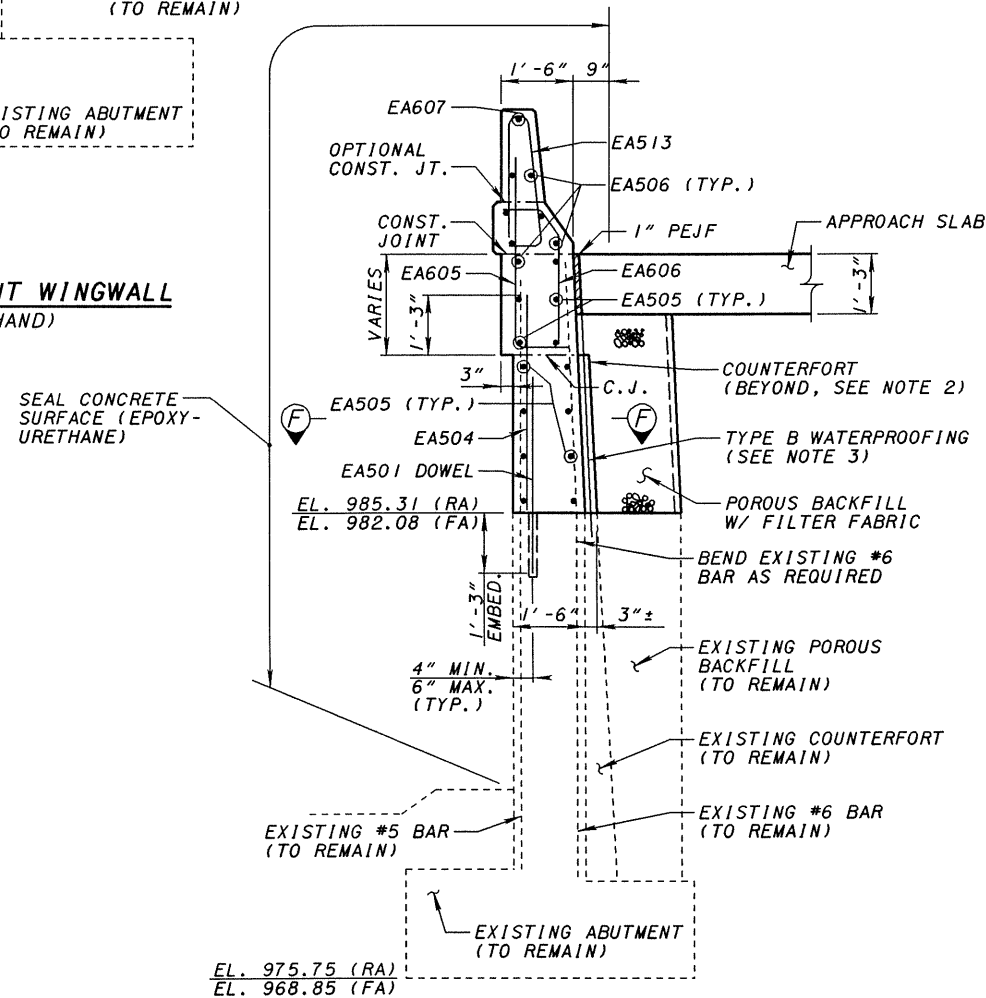
VIEW B-B - REAR ABUTMENT WINGWALL
(VIEW C-C OPPOSITE HAND)



VIEW B-B - FORWARD ABUTMENT WINGWALL
(VIEW C-C OPPOSITE HAND)



SECTION F-F

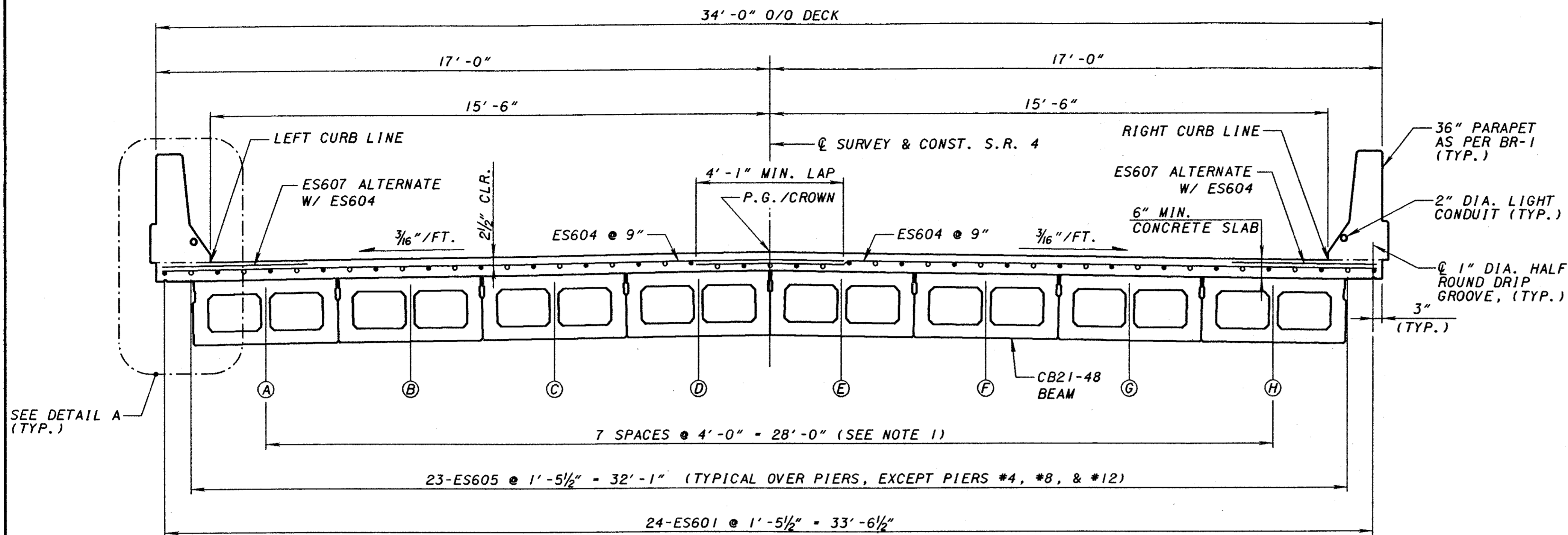


SECTION D-D
(SECTION D-D SHOWN, SECTION E-E SIMILAR)

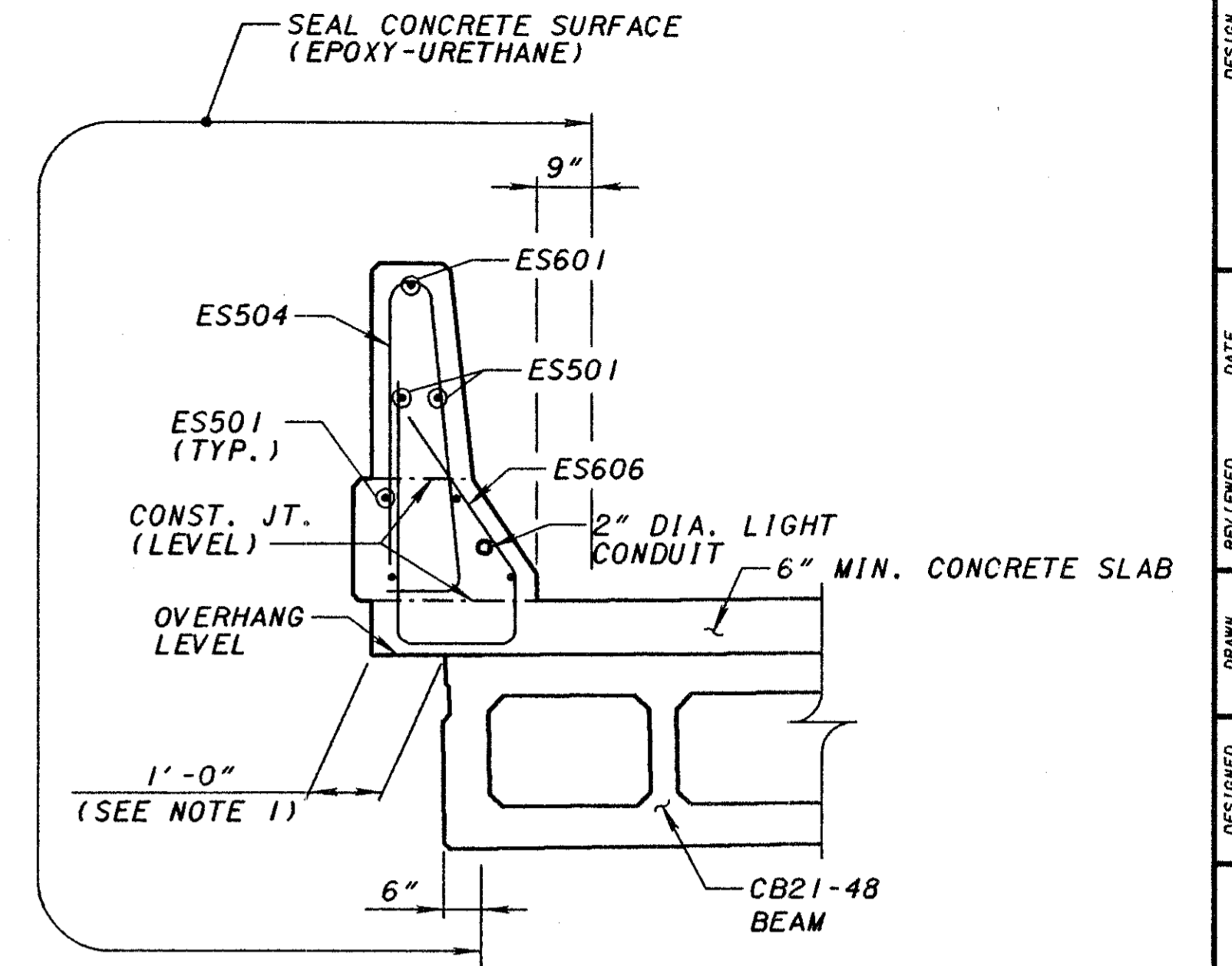
NOTES:

1. MATCH EXISTING RUSTICATION GROOVE, SEE DETAIL ON SHEET 10/38.
2. COUNTERFORT TO MATCH EXISTING RETAINING WALL BEYOND.
3. EXTEND TYPE B WATERPROOFING 6" BEYOND DEMOLITION ELEVATION AND TO THE BOTTOM OF THE APPROACH SLAB.
4. PAYMENT FOR DOWEL HOLES SHALL BE INCLUDED WITH ITEM 842 CLASS C CONCRETE, ABUTMENT.

DESIGN AGENCY: PARSONS BRINCKERHOFF OHIO, INC. 6235 ENTERPRISE COURT DUBLIN, OHIO 43016
 DATE: 02/11/02
 REVISIONS: EBS 02/11/02 STRUCTURE FILE NUMBER 5100127
 DRAWN: TJM
 CHECKED: BMG
 DESIGNED: TJM
 ABUTMENT MODIFICATION DETAILS
 BRIDGE NO. MAR-4-1171
 OVER NEW YORK CENTRAL LINES LLC (CSXT, INC.) & CSXT
 MAR-4-11.70
 11/38
 41/68



TRANSVERSE SECTION



DETAIL A

SEE DETAIL A (TYP.)

NOTES:

1. THIS DIMENSION MAY VARY DUE TO TOLERANCE OF 1/2" ALLOWED BETWEEN PRECAST UNITS. THE DIMENSION MAY VARY FROM 1'-0" TO 10 1/4".

DESIGNED	TJM	CHECKED	BNG
DRAWN	TJM	REVISED	
REVIEWED	EBS	DATE	02/11/02
STRUCTURE FILE NUMBER	5-100/27	DESIGN AGENCY	PARSONS BRINCKERHOFF OHIO, INC. 6235 ENTERPRISE COURT DUBLIN, OHIO 43016
TRANSVERSE SECTION			
BRIDGE NO. MAR-4-1171			
OVER NEW YORK CENTRAL LINES LLC (CSXT, INC.) & CSXT			
MAR-4-11.70			
33/38		63/68	

SCREED TABLE

		€ BRG. REAR ABUT.	0.25	0.5	0.75	€ PIER 1	0.25	0.5	0.75	€ PIER 2	0.25	0.5	0.75	€ PIER 3	0.25	0.5	0.75	€ PIER 4
	STATION	586+49.38	586+61.80	586+74.21	586+86.63	586+99.04	587+11.51	587+23.98	587+36.44	587+48.91	587+61.39	587+73.87	587+86.35	587+98.83	588+11.26	588+23.69	588+36.12	588+48.55
LEFT TOE OF BARRIER	TOP OF SLAB EL.	991.36	992.11	992.86	993.61	994.37	995.12	995.88	996.63	997.39	998.14	998.90	999.65	1000.41	1001.13	1001.81	1002.44	1003.04
	DEFLECTION	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00
	SCREED EL.	991.36	992.13	992.89	993.63	994.37	995.14	995.90	996.65	997.39	998.16	998.92	999.67	1000.41	1001.14	1001.83	1002.46	1003.04
PROFILE GRADE/ CENTERLINE	TOP OF SLAB EL.	991.60	992.35	993.10	993.86	994.61	995.36	996.12	996.87	997.63	998.38	999.14	999.89	1000.65	1001.37	1002.05	1002.69	1003.28
	DEFLECTION	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00
	SCREED EL.	991.60	992.37	993.13	993.87	994.61	995.38	996.15	996.89	997.63	998.40	999.17	999.91	1000.65	1001.39	1002.07	1002.70	1003.28
RIGHT TOE OF BARRIER	TOP OF SLAB EL.	991.36	992.11	992.86	993.61	994.37	995.12	995.88	996.63	997.39	998.14	998.90	999.65	1000.41	1001.13	1001.81	1002.44	1003.04
	DEFLECTION	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00
	SCREED EL.	991.36	992.13	992.89	993.63	994.37	995.14	995.90	996.65	997.39	998.16	998.92	999.67	1000.41	1001.14	1001.83	1002.46	1003.04
		€ PIER 4	0.25	0.5	0.75	€ PIER 5	0.25	0.5	0.75	€ PIER 6	0.25	0.5	0.75	€ PIER 7	0.25	0.5	0.75	€ PIER 8
	STATION	588+48.55	588+60.98	588+73.41	588+85.84	588+98.27	589+10.77	589+23.27	589+35.76	589+48.26	589+60.77	589+73.27	589+85.78	589+98.28	590+10.72	590+23.16	590+35.60	590+48.04
LEFT TOE OF BARRIER	TOP OF SLAB EL.	1003.04	1003.60	1004.11	1004.59	1005.02	1005.42	1005.78	1006.09	1006.36	1006.59	1006.78	1006.93	1007.04	1007.11	1007.13	1007.12	1007.06
	DEFLECTION	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00
	SCREED EL.	1003.04	1003.62	1004.14	1004.61	1005.02	1005.44	1005.80	1006.11	1006.36	1006.61	1006.81	1006.95	1007.04	1007.13	1007.16	1007.14	1007.06
PROFILE GRADE/ CENTERLINE	TOP OF SLAB EL.	1003.28	1003.84	1004.36	1004.83	1005.27	1005.66	1006.02	1006.33	1006.60	1006.84	1007.03	1007.18	1007.28	1007.35	1007.38	1007.36	1007.31
	DEFLECTION	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00
	SCREED EL.	1003.28	1003.86	1004.38	1004.85	1005.27	1005.68	1006.05	1006.35	1006.60	1006.85	1007.05	1007.19	1007.28	1007.37	1007.40	1007.38	1007.31
RIGHT TOE OF BARRIER	TOP OF SLAB EL.	1003.04	1003.60	1004.11	1004.59	1005.02	1005.42	1005.78	1006.09	1006.36	1006.59	1006.78	1006.93	1007.04	1007.11	1007.13	1007.12	1007.06
	DEFLECTION	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00
	SCREED EL.	1003.04	1003.62	1004.14	1004.61	1005.02	1005.44	1005.80	1006.11	1006.36	1006.61	1006.81	1006.95	1007.04	1007.13	1007.16	1007.14	1007.06
		€ PIER 8	0.25	0.5	0.75	€ PIER 9	0.25	0.5	0.75	€ PIER 10	0.25	0.5	0.75	€ PIER 11	0.25	0.5	0.75	€ PIER 12
	STATION	590+48.04	590+60.99	590+73.94	590+86.88	590+99.83	591+12.82	591+25.82	591+38.81	591+51.80	591+64.78	591+77.77	591+90.75	592+03.73	592+16.65	592+29.58	592+42.50	592+55.42
LEFT TOE OF BARRIER	TOP OF SLAB EL.	1007.06	1006.96	1006.82	1006.63	1006.39	1006.11	1005.79	1005.42	1005.01	1004.55	1004.05	1003.51	1002.92	1002.29	1001.61	1000.89	1000.13
	DEFLECTION	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00
	SCREED EL.	1007.06	1006.98	1006.85	1006.65	1006.39	1006.14	1005.82	1005.45	1005.01	1004.58	1004.09	1003.53	1002.92	1002.31	1001.64	1000.91	1000.13
PROFILE GRADE/ CENTERLINE	TOP OF SLAB EL.	1007.31	1007.20	1007.06	1006.87	1006.64	1006.36	1006.03	1005.66	1005.25	1004.80	1004.29	1003.75	1003.16	1002.53	1001.85	1001.13	1000.37
	DEFLECTION	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00
	SCREED EL.	1007.31	1007.23	1007.09	1006.89	1006.64	1006.38	1006.07	1005.69	1005.25	1004.82	1004.33	1003.77	1003.16	1002.55	1001.89	1001.16	1000.37
RIGHT TOE OF BARRIER	TOP OF SLAB EL.	1007.06	1006.96	1006.82	1006.63	1006.39	1006.11	1005.79	1005.42	1005.01	1004.55	1004.05	1003.51	1002.92	1002.29	1001.61	1000.89	1000.13
	DEFLECTION	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00
	SCREED EL.	1007.06	1006.98	1006.85	1006.65	1006.39	1006.14	1005.82	1005.45	1005.01	1004.58	1004.09	1003.53	1002.92	1002.31	1001.64	1000.91	1000.13
		€ PIER 12	0.25	0.5	0.75	€ PIER 13	0.25	0.5	0.75	€ PIER 14	0.25	0.5	0.75	€ PIER 15	0.25	0.5	0.75	€ BRG. FWD ABUT.
	STATION	592+55.42	592+67.85	592+80.27	592+92.70	593+05.12	593+17.59	593+30.07	593+42.54	593+55.01	593+67.49	593+79.98	593+92.46	594+04.94	594+17.34	594+29.75	594+42.15	594+54.55
LEFT TOE OF BARRIER	TOP OF SLAB EL.	1000.13	999.37	998.62	997.87	997.11	996.36	995.60	994.84	994.09	993.33	992.57	991.82	991.06	990.31	989.55	988.80	988.05
	DEFLECTION	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00
	SCREED EL.	1000.13	999.39	998.65	997.89	997.11	996.38	995.63	994.86	994.09	993.35	992.60	991.83	991.06	990.33	989.58	988.82	988.05
PROFILE GRADE/ CENTERLINE	TOP OF SLAB EL.	1000.37	999.62	998.86	998.11	997.36	996.60	995.84	995.09	994.33	993.57	992.82	992.06	991.30	990.55	989.80	989.04	988.29
	DEFLECTION	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00
	SCREED EL.	1000.37	999.64	998.89	998.13	997.36	996.62	995.87	995.10	994.33	993.59	992.84	992.08	991.30	990.57	989.82	989.06	988.29
RIGHT TOE OF BARRIER	TOP OF SLAB EL.	1000.13	999.37	998.62	997.87	997.11	996.36	995.60	994.84	994.09	993.33	992.57	991.82	991.06	990.31	989.55	988.80	988.05
	DEFLECTION	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00	0.02	0.03	0.02	0.00
	SCREED EL.	1000.13	999.39	998.65	997.89	997.11	996.38	995.63	994.86	994.09	993.35	992.60	991.83	991.06	990.33	989.58	988.82	988.05

SCREED ELEVATIONS SHOWN ARE FOR THE DECK SLAB SURFACE
 PRIOR TO CONCRETE PLACEMENT. ALLOWANCE HAS BEEN MADE
 FOR ANTICIPATED CALCULATED DEAD LOAD DEFLECTIONS.

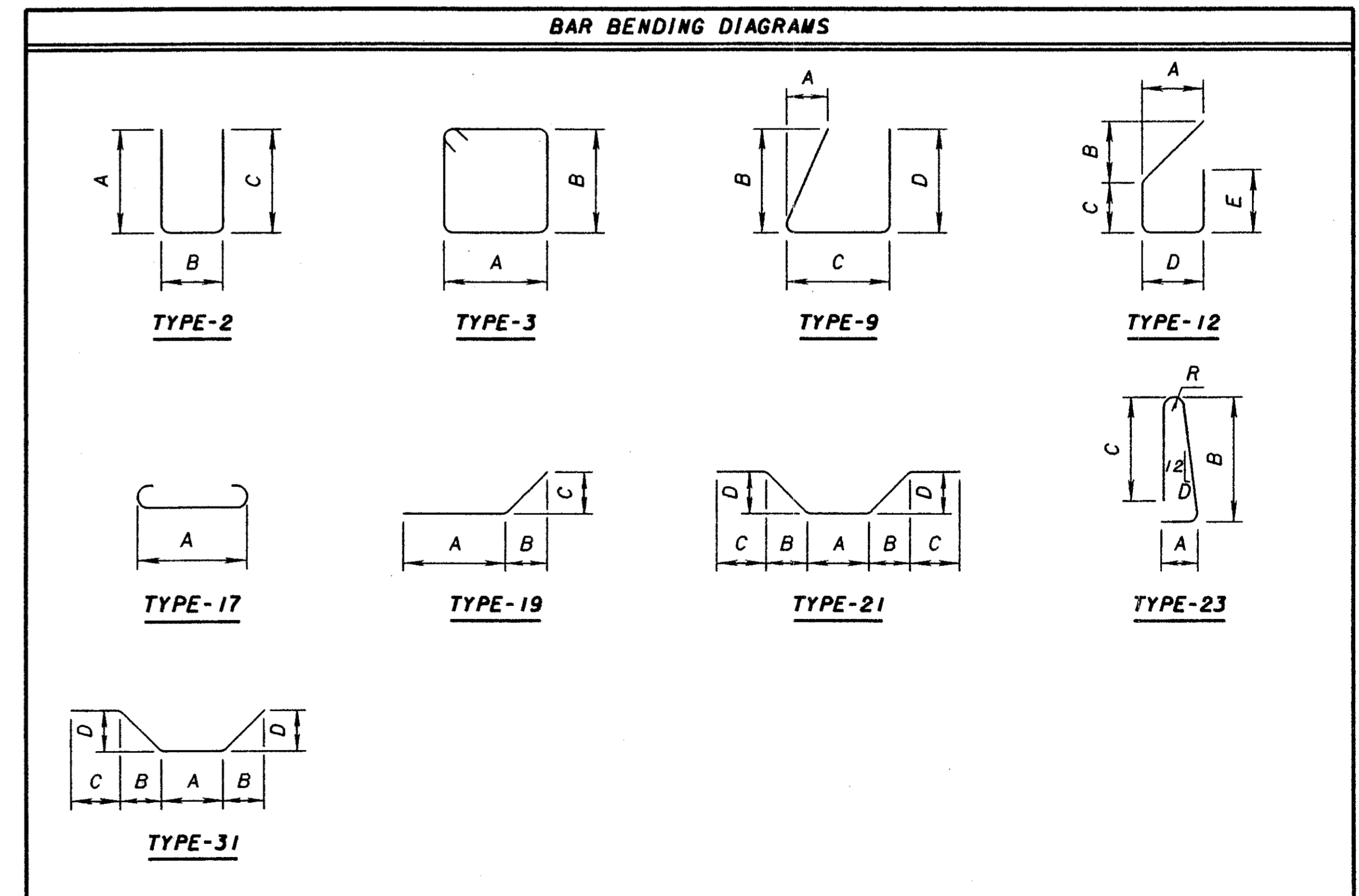
MARK	NUMBER				LENGTH	WEIGHT	TYPE	DIMENSIONS								REMARKS
	PIER 4	PIER 8	PIER 12	TOTAL				A	B	C	D	E	R	INC.		
PIERS																
EP501	2	2	2	6	10'-0"	63	STR									
EP502	2	2	2	6	21'-5"	134	STR									
EP503	2	2	2	6	31'-8"	198	STR									
EP504	6	6	6	18	12'-1"	227	2	4'-10"	2'-8"	4'-10"						
EP505	2	2	2	6	15'-6"	97	3	2'-8"	4'-10"							
EP506	2	2	2	6	15'-3"	95	3	2'-8"	4'-8"							
EP507	2 SER OF 4	2 SER OF 4	2 SER OF 4	6 SER OF 4	13'-9" TO 14'-10"	358	3	2'-8"	3'-11" TO 4'-6"					4 3/8"		
EP508	2 SER OF 5	2 SER OF 5	2 SER OF 5	6 SER OF 5	9'-9" TO 13'-3"	360	3	2'-8"	1'-11" TO 3'-8"					10 1/2"		
EP601	4	4	4	12	14'-0"	252	19	11'-3"	2'-2"	0'-7"						
EPI401	2	2	2	6	34'-0"	1557	2	0'-8"	31'-8"	0'-8"						
EPI402	8	8	8	24	31'-8"	5814	STR									
TOTAL WEIGHT = 9155 LBS *																

MARK	NUMBER	LENGTH	WEIGHT	TYPE	DIMENSIONS								REMARKS
					A	B	C	D	E	R	INC.		
SUPERSTRUCTURE													
ES501	336	30'-0"	10514	STR									PARAPET
ES502	36	6'-4"	238	STR									PARAPET
ES503	12	13'-9"	172	STR									PARAPET
ES504	1792	6'-0"	11214	23	0'-8"	2'-9"	2'-6"	0'-1 1/4"			1 1/2"		PARAPET
ES505	28	2'-9"	80	2	0'-7"	1'-10"	0'-7"						LIGHT PILASTER
ES506	28	8'-5"	246	9	0'-5"	3'-2"	2'-4"	3'-2"					LIGHT PILASTER
ES507	42	7'-4"	322	21	1'-4"	1'-10"	0'-6"	1'-10"					LIGHT PILASTER
ES508	28	3'-2"	93	STR									LIGHT PILASTER
ES509	7	6'-11"	51	31	1'-4"	1'-10"	0'-6"	1'-10"					PILASTER @ PIER 8
ES601	728	30'-0"	32804	STR									DECK/PARAPET
ES602	78	17'-11"	2099	STR									DECK/PARAPET
ES603	26	25'-5"	993	STR									DECK/PARAPET
ES604	2188	18'-11"	62167	STR									DECK
ES605	276	20'-10"	8637	STR									DECK
ES606	1792	5'-5"	14579	12	0'-11 1/2"	1'-4 1/2"	0'-7 3/4"	1'-2"	2'-4"				PARAPET
ES607	2116	4'-0"	12713	STR									DECK
S401	576	5'-2"	2051	17	4'-2"								OVER PIERS
S402	48	33'-8"	1080	STR									OVER PIERS
S601	12	33'-8"	270	STR									OVER PIERS
TOTAL WEIGHT = 160323 LBS *													

REINFORCING SAMPLES:

REFER TO CMS SECTIONS 106.03, 700, 709.01 THROUGH 709.05 AND 709.08 SUFFICIENT ADDITIONAL REINFORCING STEEL SHALL BE PROVIDED FOR SAMPLING. RANDOM SAMPLES SHALL BE REPLACED IN THE STRUCTURES BY THE ADDITIONAL STEEL. SPLICED IN ACCORDANCE WITH 509.08.

THE BAR SIZE NUMBER IS SPECIFIED ON THE PLANS IN THE BAR MARK COLUMN. THE FIRST DIGITS WHERE THREE DIGITS ARE USED, AND THE FIRST TWO DIGITS WHERE FOUR DIGITS ARE USED, INDICATES THE BAR SIZE NUMBER. FOR EXAMPLE, P601 IS A NO. 6 BAR. BAR DIMENSIONS SHOWN ARE OUT TO OUT UNLESS OTHERWISE INDICATED. "R" INDICATES INSIDE RADIUS, UNLESS OTHERWISE NOTED. "STD." WRITTEN IN PLACE OF A DIMENSION INDICATES A STANDARD BEND AT THE END OF THE BAR.



NOTES:

ALL REINFORCING STEEL TO BE EPOXY COATED
 * REINFORCING STEEL WEIGHTS ARE PROVIDED FOR INFORMATIONAL PURPOSES ONLY.