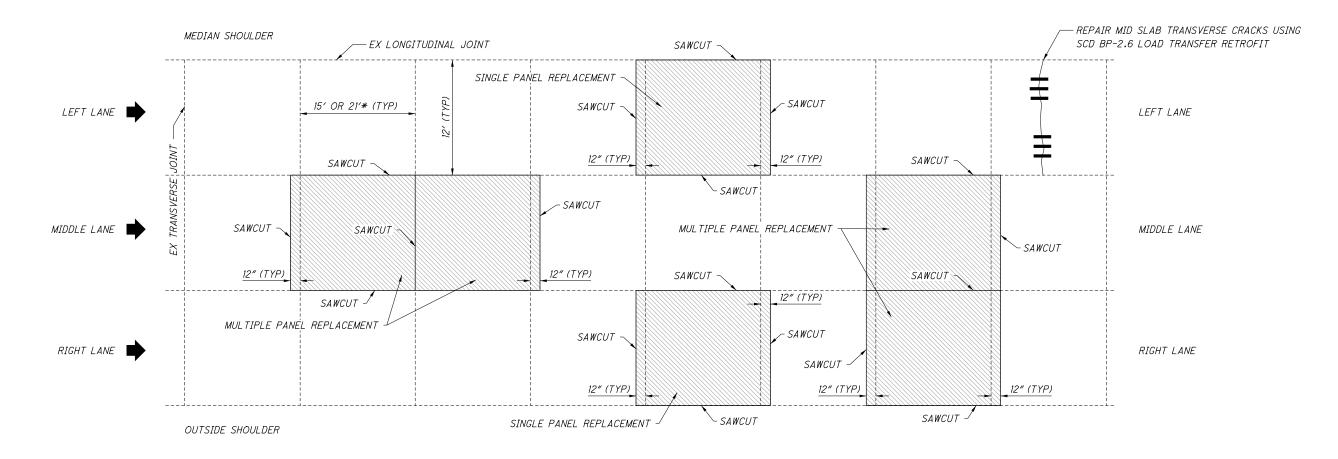
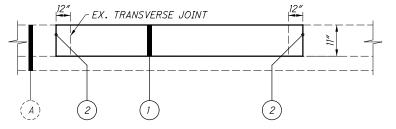
* THE EX. TRANSVERSE JOINTS ON USR 35 ARE SPACED 21' APART WEST OF THE JAMES MCGEE BLVD INTERSECTION AND ARE SPACED 15' APART EAST OF THE JAMES MCGEE BLVD INTERSECTION.



LOAD TRANSFER RETROFIT & PANEL REPLACEMENT (TYPICAL)

MOT-35-SLM 14.04 TO SLM 15.07





<u>LEGEND</u>

- (1) ITEM 255 FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC 1 OR CLASS QC MS
- (2) ITEM 255 FULL DEPTH PAVEMENT SAWING
- (3) ITEM 258 RETROFIT DOWEL BAR

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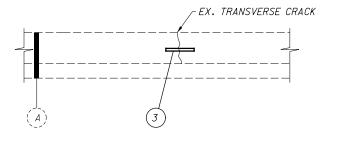
WEST OF JAMES MCGEE BLVD = EX. 11"± REINFORCED CONCRETE PAVEMENT ON 6" AGGREGATE BASE

EAST OF JAMES MCGEE BLVD = EX. 10" PLAIN CONCRETE PAVEMENT ON 4" ASPH. CONC. BASE ON 4" AGG. BASE

PANEL REPLACEMENT SECTION

REFER TO SCD BP-2.5 (T SHALL MATCH THE EXISTING SURROUNDING SLABS. MIN. 10" THICK)

MULTIPLE PANEL REPLACEMENT - WHERE MULTIPLE FULL CONCRETE PANELS ARE TO BE REPLACED, THE CONTRACTOR SHALL INSTALL JOINTS TO MATCH THE EXISTING ADJACENT PAVEMENT.



RETROFIT DOWEL BAR SECTION

REFER TO SCD BP-2.6



ALIGNMENT AND PROFILE

THE WORK PROPOSED BY THIS PROJECT CONSISTS OF REPAIRING THE EXISTING CONCRETE PAVEMENT. THE ALIGNMENT OF THE EXISTING PAVEMENT WILL NOT BE CHANGED AND THE PROFILE OF THE PROPOSED SURFACE WILL BE SIMILAR TO THAT OF THE EXISTING PAVEMENT.

COORDINATION OF WORK:

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MOT-35-14.04 (PID 109710) MOT-35-15.07 (PID 108102) D07-BP-FY22 (PID 105404) MOT-75-(10.44)(10.78) (PID 91606) MOT-75-11.25 (PID 101160)

THE CONTRACTOR IS ADVISED THAT ADJACENT CONSTRUCTION PROJECTS WITHIN OR NEAR THE WORK LIMITS OF THIS PLAN MAY IMPACT THE PROJECT SCHEDULE, SEQUENCE OF CONSTRUCTION AND/OR TRAFFIC CONTROL BETWEEN ADJACENT ZONES. THE CONTRACTOR IS REQUIRED TO COORDINATE ALL MAINTENANCE OF TRAFFIC OPERATIONS WITH ADJACENT CONSTRUCTION PROJECTS. COOPERATION WITH THE ENGINEER, INSPECTORS AND ALL OTHER CONTRACTORS ON OR ADJACENT TO THE PROJECT IS REQUIRED PER CMS 105.08.

ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC 1, AS PER PLAN

THE REPAIRS SHALL BE CONSTRUCTED AS PER SCD BP-2.5. ALL REPAIR AREAS SHALL BE DETERMINED BY THE PROJECT ENGINEER BEFORE BEGINNING WORK. REPAIR AREAS SHALL BE REPLACED TO MATCH THE EXISTING PAVEMENT ELEVATION. NEW CONCRETE SURFACES SHALL BE TEXTURED PER CMS 451.10. THE CONTRACTOR SHALL NOT REMOVE MORE PAVEMENT THAN WHAT CAN BE REPLACED THAT WORK SHIFT.

ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC MS, AS PER PLAN

THE REPAIRS SHALL BE CONSTRUCTED AS PER SCD BP-2.5. ALL REPAIR AREAS SHALL BE DETERMINED BY THE PROJECT ENGINEER BEFORE BEGINNING WORK. REPAIR AREAS SHALL BE REPLACED TO MATCH THE EXISTING PAVEMENT ELEVATION. NEW CONCRETE SURFACES SHALL BE TEXTURED PER CMS 451.10. THE CONTRACTOR SHALL NOT REMOVE MORE PAVEMENT THAN WHAT CAN BE REPLACED THAT WORK SHIFT.

ITEM 623 - CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN

CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO DOCUMENT THE LAYOUT OF THE EXISTING PAVEMENT INCLUDING EXISTING ALIGNMENT, PROFILE, PAVEMENT EDGE LOCATIONS AND ELEVATIONS IN A LOG FOR THE IR 75 TO US 35 WESTBOUND RAMP REPAIR AREAS AND SUBMIT TO THE DEPARTMENT FOR ACCEPTANCE. THE DEPARTMENT WILL NOT ALLOW THE CONTRACTOR TO PERFORM ANY PAVEMENT WORK FUNCTIONS UNTIL ACCEPTANCE OF THE SUBMITTED EXISTING PAVEMENT.

THE CONTRACTOR SHALL COORDINATE AND CORRABORATE THE PROPOSED LAYOUT OF ALL PAVEMENT AND APPLICABLE STANDARD CONSTRUCTION DRAWINGS WITH ODOT.

ITEM 258 - RETROFIT DOWEL BAR

THIS PAY ITEM IS TO BE USED TO REPAIR TRANSVERSE CRACKS. THE REPAIRS SHALL BE CONSTRUCTED AS PER SCD BP-2.6. THE CRACKS TO BE REPAIRED ARE TO BE MARKED BY THE ENGINEER.

PAYMENT FOR THIS ITEM OF WORK SHALL INCLUDE ALL LABOR, MATERIALS, AND INCIDENTALS REQUIRED FOR ITEM 258 RETROFIT DOWEL BAR .

PAVEMENT MARKINGS

THE QUANTITIES PROVIDED IN THE GENERAL SUMMARY ARE INTENDED TO BE USED TO REPLACE THE MARKINGS AND RPMS THAT HAVE BEEN REMOVED OR DAMAGED FROM THE PAVEMENT REPAIRS.

ITEM 614 - WORK ZONE LANE LINE, CLASS I, 6" 642 PAINT = 0.25 MILE ITEM 614 - WORK ZONE EDGE LINE, CLASS 1, 6" 642 PAINT = 0.50 MILE ITEM 621 - RPM = 20 EACH

ITEM 621 - RAISED PAVEMENT MARKER REMOVED = 20 EACH

ITEM 646 - EDGE LINE, 6" = 0.25 MILE

ITEM 646 - LANE LINE, 6" = 0.50 MILE

ITEM 646 - CHANNELIZING LINE, 12" = 500 FT

ITEM 646 - DOTTED LINE. 6" = 200 FT

ITEM 530 SPECIAL - STRUCTURES MISC .: CLEANING OF JOINTS FOR FULL WIDTH OF THE BRIDGE

THE CONTRACTOR SHALL FURNISH ALL MATERIAL, EQUIPMENT. LABOR AND INCIDENTAL ITEMS NECESSARY TO PROPERLY REMOVE AND DISPOSE OF ALL DEBRIS AND OTHER FOREIGN MATERIAL FROM THE EXPANSION JOINTS BY POWER SWEEPING, SCRAPING, ETC. FOLLOWED BY POWER WASHING WITH WATER. THE FOLLOWING PROCEDURE SHALL BE USED:

- 1) REMOVE AND COLLECT LOOSE DEBRIS FROM THE JOINTS PRIOR TO INTRODUCING WATER. CARE SHOULD BE TAKEN WHEN BREAKING UP DEBRIS IN THE JOINTS IN ORDER NOT TO DAMAGE THE ELASTOMERIC SEAL (IF PRESENT).
- 2) SWEEP AND COLLECT SAND, DEBRIS AND SEDIMENT FROM THE JOINTS BY VACUUMING WASH WATER USING A VACUUM

THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING THE WATER SUPPLY USED DURING THE SWEEPING AND CLEANING OPERATION. THE CONTRACTOR IS RESPONSIBLE FOR THE PROPER DISPOSAL OF ANY DEBRIS COLLECTED DURING OPERATIONS IN ACCORDANCE WITH C&MS 105.16. DISCHARGING TO OR COLLECTING POTABLE WATER FROM STREAMS IS PROHIBITED.

PAYMENT FOR ALL OF THE AFOREMENTIONED WORK WILL BE MADE AT THE CONTRACT BID PRICE PER BRIDGE AS FOLLOWS:

ITEM 530 SPECIAL - STRUCTURE, MISC.: CLEANING OF JOINTS FOR THE FULL WIDTH OF THE BRIDGE 7 EACH

FOR ESTIMATING PURPOSES ONLY, LISTED BELOW ARE THE STRUCTURES AND THE APPROXIMATE LENGTHS OF THE EXPANSION JOINTS THAT ARE TO BE CLEANED:

> MOT-35-14.41 = 105 FTMOT-35-14.73 L = 134 FT.MOT-35-14.73 R = 134 FT.MOT-35-14.81 L = 234 FT.MOT-35-14.81 R = 230 FT.MOT-35-14.91 L = 135 FT.MOT-35-14.91 R = 137 FT.TOTAL = 1109 FT.

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												2,841	2,841	255	10161	2,841	SY	FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC MS, AS PER PLAN	3	
												10,754	10,754	255	20000	10,754	FT	FULL DEPTH PAVEMENT SAWING		_
												1,296	1,296	258	10000	1,296	EACH	RETROFIT DOWEL BAR		_
												1,290	1,290	230	10000	1,290	EACH	RETROFTI DOWEL BAR	-+-	-
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								255	255	255	258	SPECIAL				
						NUMBER OF MID SLAB CRACKS REPAIRED WITH RDB	REPAIRS	FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC MS, AS PER PLAN	FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC1, AS PER PLAN	SAWING	R RACK	TYPE C				CALCULAT
TION		SECTION				LAB (B REF	NT RE ENT,	NT RE IENT, PLAN		RETROFIT DOWEL BAR 6 BARS PER MID SLAB CRACK	JOINT,				
PARTICIPATION	ROUTE		LOCA	NOITA	(L)	IN C	SLAB	/EMEN ACEM	/EMEN ACEM PER 1	DEPTH PAVEMENT	DOWE ID SL	, OD 7				
ARTI	ě	TYPICAL			3	OF N	3 OF	PAV REPL,	PAV REPL AS I	Н РА	FIT ER M.	RELIEF				
۵						BER	NUMBER	EPTH GID R	EPTH GID F QC1,	DEPT	ETRC 3S Pf	RE R				
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	EDOM BEC	IN DDO ITOT T	FROM O JAMES MCGE	TO	MILE FT	FT	SY	SY	SY	FT	EACH	FT				-
						20	11	770		770	170					-
		S1, RD, PJ	14.04	14.41		22	11	338		770	132	57]
1	US 35 EB	S1, RD, PJ	14.04	14.41		25	27	828		1890	150	57				
	FROM JAM	ES MCGEE BLV	D. TO DUNBAR	ST.												
1	US 35 WB	S2, RD, PJ	14.41	14.73		36	25	567		1450	216	66				
1	US 35 EB	S2, RD, PJ	14.41	14.73		34	9	204		522	204	66				
			ROADWAY ST.]
1	US 35 WB	RD, PJ	14.73	14.91		22					132	264				
1	US 35 EB	RD, PJ	14.73	14.91		18					108	264				
	FROM BRO	DADWAY ST. T	D END PROJECT													
		S4, RD, PJ		15.07		5	20	400		852	30	66				
		S2, RD, PJ		15.07		39	2	46		116	234	71				
	US 35 X J	IAMES MCGEE I	BLVD INTERCHAI	NGE												
1	WB ENTRA	NCE RAMP S3, RD				3	8	242		528	18					
	WB EXIT R					3	Ü	212		020	10					
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1		S3, RD				4	7	216		462	24					
1	EB ENTRAI	NCE RAMP RD				3					18					-
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	US 35 X I	R 75 INTERCH	ANGE													3
1	WB DUAL E	ENTRANCE RAN S5	IPS				120		2400	4164						∐
1		33					120		2400	4104						<u>ן</u> נ
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LEGEND FOR AUXILLARY AREAS IN "TYPICAL" COLUMN						PART.					SUBTOTALS					
S1: 23' X 12' SLAB REPLACEMENTS - US 35 WEST OF JAMES MCGEE S2: 17' X 12' SLAB REPLACEMENTS - US 35 EAST OF JAMES MCGEE							1	2841	2400	10754	1296	911				∏ ≥
S3: 17	′ X 16′ SLA	AB REPLACEME	NTS - JAMES M	ICGEE RAMPS												
				B LARGE AREA	300' X 12' E AREA 900' X 24'											-
	TROFIT DO				JOINT REPLACEMENT											$ _{2}$
	ТОТ	TALS CARRIE	ED TO GENER	AL SUMMARY				2841	2400	10754	1296	911				23

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