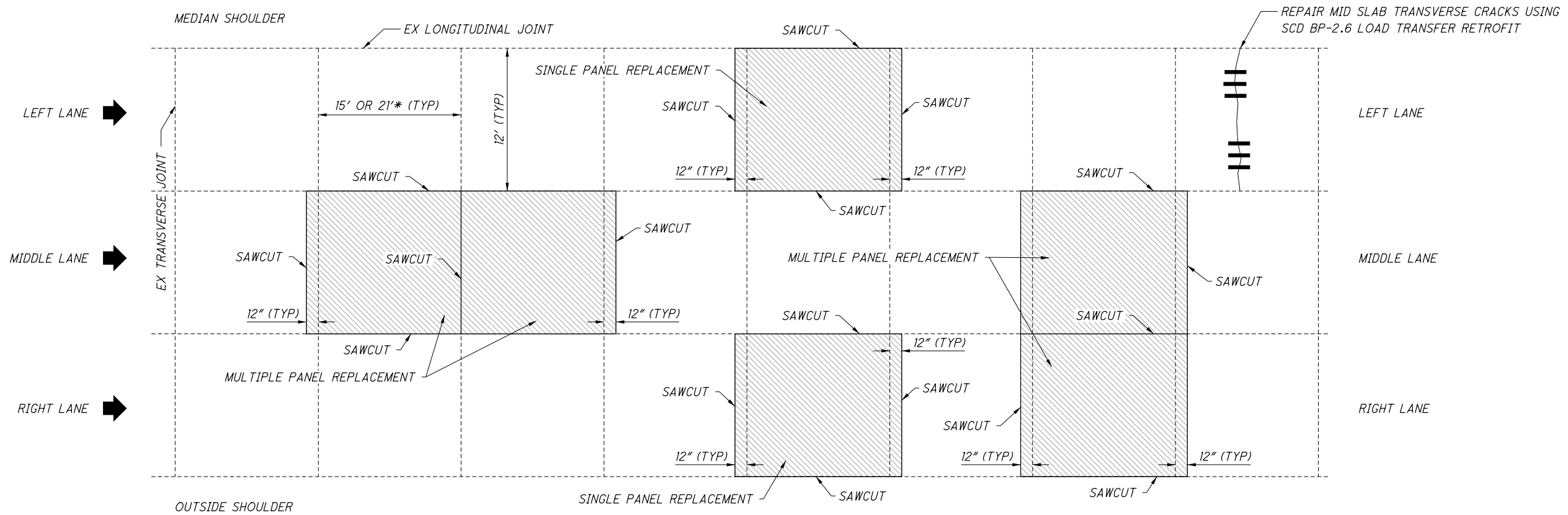


\* THE EX. TRANSVERSE JOINTS ON USR 35 ARE SPACED 21' APART WEST OF THE JAMES MCGEE BLVD INTERSECTION AND ARE SPACED 15' APART EAST OF THE JAMES MCGEE BLVD INTERSECTION.

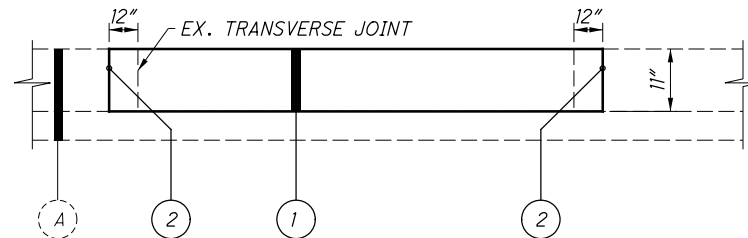


LOAD TRANSFER RETROFIT & PANEL REPLACEMENT (TYPICAL)

MOT-35-SLM 14.04 TO SLM 15.07

 PAVEMENT REPAIR AREA

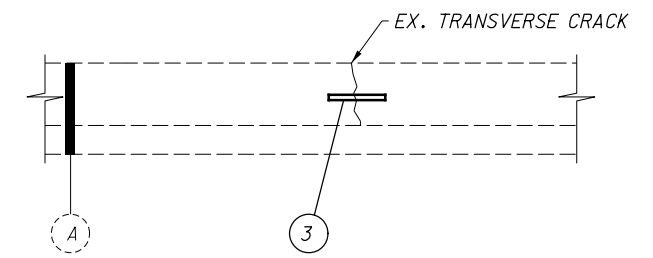
 RETROFIT DOWEL BARS



PANEL REPLACEMENT SECTION

REFER TO SCD BP-2.5 (T SHALL MATCH THE EXISTING SURROUNDING SLABS. MIN. 10" THICK)

MULTIPLE PANEL REPLACEMENT - WHERE MULTIPLE FULL CONCRETE PANELS ARE TO BE REPLACED, THE CONTRACTOR SHALL INSTALL JOINTS TO MATCH THE EXISTING ADJACENT PAVEMENT.



RETROFIT DOWEL BAR SECTION

REFER TO SCD BP-2.6

LEGEND

- ① ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC 1 OR CLASS QC MS
- ② ITEM 255 - FULL DEPTH PAVEMENT SAWING
- ③ ITEM 258 - RETROFIT DOWEL BAR

- Ⓐ WEST OF JAMES MCGEE BLVD = EX. 11"± REINFORCED CONCRETE PAVEMENT ON 6" AGGREGATE BASE
- Ⓐ EAST OF JAMES MCGEE BLVD = EX. 10" PLAIN CONCRETE PAVEMENT ON 4" ASPH. CONC. BASE ON 4" AGG. BASE

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TYPICAL SECTIONS

MOT-35-14.04

**ALIGNMENT AND PROFILE**

THE WORK PROPOSED BY THIS PROJECT CONSISTS OF REPAIRING THE EXISTING CONCRETE PAVEMENT. THE ALIGNMENT OF THE EXISTING PAVEMENT WILL NOT BE CHANGED AND THE PROFILE OF THE PROPOSED SURFACE WILL BE SIMILAR TO THAT OF THE EXISTING PAVEMENT.

**COORDINATION OF WORK:**

- MOT-35-14.04 (PID 109710)
- MOT-35-15.07 (PID 108102)
- D07-BP-FY22 (PID 105404)
- MOT-75-(10.44)(10.78) (PID 91606)
- MOT-75-11.25 (PID 101160)

THE CONTRACTOR IS ADVISED THAT ADJACENT CONSTRUCTION PROJECTS WITHIN OR NEAR THE WORK LIMITS OF THIS PLAN MAY IMPACT THE PROJECT SCHEDULE, SEQUENCE OF CONSTRUCTION AND/OR TRAFFIC CONTROL BETWEEN ADJACENT ZONES. THE CONTRACTOR IS REQUIRED TO COORDINATE ALL MAINTENANCE OF TRAFFIC OPERATIONS WITH ADJACENT CONSTRUCTION PROJECTS. COOPERATION WITH THE ENGINEER, INSPECTORS AND ALL OTHER CONTRACTORS ON OR ADJACENT TO THE PROJECT IS REQUIRED PER CMS 105.08.

**ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC 1, AS PER PLAN**

THE REPAIRS SHALL BE CONSTRUCTED AS PER SCD BP-2.5. ALL REPAIR AREAS SHALL BE DETERMINED BY THE PROJECT ENGINEER BEFORE BEGINNING WORK. REPAIR AREAS SHALL BE REPLACED TO MATCH THE EXISTING PAVEMENT ELEVATION. NEW CONCRETE SURFACES SHALL BE TEXTURED PER CMS 451.10. THE CONTRACTOR SHALL NOT REMOVE MORE PAVEMENT THAN WHAT CAN BE REPLACED THAT WORK SHIFT.

**ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS QC MS, AS PER PLAN**

THE REPAIRS SHALL BE CONSTRUCTED AS PER SCD BP-2.5. ALL REPAIR AREAS SHALL BE DETERMINED BY THE PROJECT ENGINEER BEFORE BEGINNING WORK. REPAIR AREAS SHALL BE REPLACED TO MATCH THE EXISTING PAVEMENT ELEVATION. NEW CONCRETE SURFACES SHALL BE TEXTURED PER CMS 451.10. THE CONTRACTOR SHALL NOT REMOVE MORE PAVEMENT THAN WHAT CAN BE REPLACED THAT WORK SHIFT.

**ITEM 623 - CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN**

CONSTRUCTION LAYOUT STAKES AND SURVEYING, AS PER PLAN IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO DOCUMENT THE LAYOUT OF THE EXISTING PAVEMENT INCLUDING EXISTING ALIGNMENT, PROFILE, PAVEMENT EDGE LOCATIONS AND ELEVATIONS IN A LOG FOR THE IR 75 TO US 35 WESTBOUND RAMP REPAIR AREAS AND SUBMIT TO THE DEPARTMENT FOR ACCEPTANCE. THE DEPARTMENT WILL NOT ALLOW THE CONTRACTOR TO PERFORM ANY PAVEMENT WORK FUNCTIONS UNTIL ACCEPTANCE OF THE SUBMITTED EXISTING PAVEMENT.

THE CONTRACTOR SHALL COORDINATE AND CORRABORATE THE PROPOSED LAYOUT OF ALL PAVEMENT AND APPLICABLE STANDARD CONSTRUCTION DRAWINGS WITH ODOT.

**ITEM 258 - RETROFIT DOWEL BAR**

THIS PAY ITEM IS TO BE USED TO REPAIR TRANSVERSE CRACKS. THE REPAIRS SHALL BE CONSTRUCTED AS PER SCD BP-2.6. THE CRACKS TO BE REPAIRED ARE TO BE MARKED BY THE ENGINEER.

PAYMENT FOR THIS ITEM OF WORK SHALL INCLUDE ALL LABOR, MATERIALS, AND INCIDENTALS REQUIRED FOR ITEM 258 RETROFIT DOWEL BAR .

**PAVEMENT MARKINGS**

THE QUANTITIES PROVIDED IN THE GENERAL SUMMARY ARE INTENDED TO BE USED TO REPLACE THE MARKINGS AND RPMS THAT HAVE BEEN REMOVED OR DAMAGED FROM THE PAVEMENT REPAIRS.

- ITEM 614 - WORK ZONE LANE LINE, CLASS I, 6" 642 PAINT = 0.25 MILE
- ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 6" 642 PAINT = 0.50 MILE
- ITEM 621 - RPM = 20 EACH
- ITEM 621 - RAISED PAVEMENT MARKER REMOVED = 20 EACH
- ITEM 646 - EDGE LINE, 6" = 0.25 MILE
- ITEM 646 - LANE LINE, 6" = 0.50 MILE
- ITEM 646 - CHANNELIZING LINE, 12" = 500 FT
- ITEM 646 - DOTTED LINE, 6" = 200 FT

**ITEM 530 SPECIAL - STRUCTURES MISC.: CLEANING OF JOINTS FOR FULL WIDTH OF THE BRIDGE**

THE CONTRACTOR SHALL FURNISH ALL MATERIAL, EQUIPMENT, LABOR AND INCIDENTAL ITEMS NECESSARY TO PROPERLY REMOVE AND DISPOSE OF ALL DEBRIS AND OTHER FOREIGN MATERIAL FROM THE EXPANSION JOINTS BY POWER SWEEPING, SCRAPING, ETC. FOLLOWED BY POWER WASHING WITH WATER. THE FOLLOWING PROCEDURE SHALL BE USED:

- 1) REMOVE AND COLLECT LOOSE DEBRIS FROM THE JOINTS PRIOR TO INTRODUCING WATER. CARE SHOULD BE TAKEN WHEN BREAKING UP DEBRIS IN THE JOINTS IN ORDER NOT TO DAMAGE THE ELASTOMERIC SEAL (IF PRESENT).
- 2) SWEEP AND COLLECT SAND, DEBRIS AND SEDIMENT FROM THE JOINTS BY VACUUMING WASH WATER USING A VACUUM TRUCK.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING THE WATER SUPPLY USED DURING THE SWEEPING AND CLEANING OPERATION. THE CONTRACTOR IS RESPONSIBLE FOR THE PROPER DISPOSAL OF ANY DEBRIS COLLECTED DURING OPERATIONS IN ACCORDANCE WITH C&MS 105.16. DISCHARGING TO OR COLLECTING POTABLE WATER FROM STREAMS IS PROHIBITED.

PAYMENT FOR ALL OF THE AFOREMENTIONED WORK WILL BE MADE AT THE CONTRACT BID PRICE PER BRIDGE AS FOLLOWS:

ITEM 530 SPECIAL - STRUCTURE, MISC.: CLEANING OF JOINTS FOR THE FULL WIDTH OF THE BRIDGE 7 EACH

FOR ESTIMATING PURPOSES ONLY, LISTED BELOW ARE THE STRUCTURES AND THE APPROXIMATE LENGTHS OF THE EXPANSION JOINTS THAT ARE TO BE CLEANED:

MOT-35-14.41	=	105 FT
MOT-35-14.73 L	=	134 FT.
MOT-35-14.73 R	=	134 FT.
MOT-35-14.81 L	=	234 FT.
MOT-35-14.81 R	=	230 FT.
MOT-35-14.91 L	=	135 FT.
MOT-35-14.91 R	=	137 FT.
TOTAL	=	1109 FT.

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GENERAL NOTES

MOT - 35 - 14.04



