

STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

MOT-75-7.53

CITY OF MORaine

CITY OF DAYTON

MONTGOMERY COUNTY

PROJECT DESCRIPTION

IMPROVEMENT OF 3.56 MILES OF I.R.-75 BY DOING PAVEMENT REPAIRS, PATCHING BRIDGE DECKS, AND TOPS OF BACKWALLS, TO INCLUDE RESEALING LONGITUDINAL JOINTS ON DECKS AND APPROACHES, AND INLET REBUILT TO GRADE.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA: N/A ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA: N/A ACRES
NOTICE OF INTENT EARTH DISTURBED AREA: N/A ACRES
" MAINTENANCE PROJECT "

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

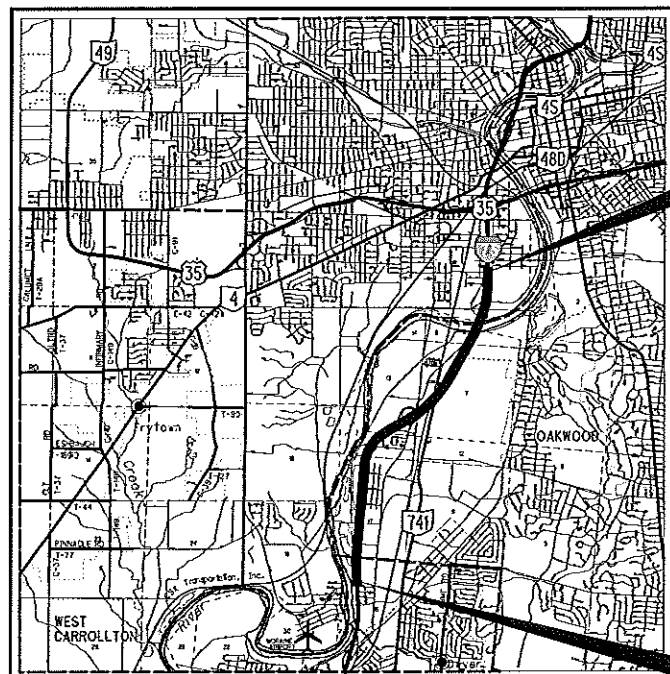
2016 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THESE IMPROVEMENTS WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY, AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

APPROVED Randy Chevally, P.E. P.S. /RPH
DATE 12-19-18 DISTRICT DEPUTY DIRECTOR

APPROVED [Signature]
DATE 1/21/19 DIRECTOR, DEPARTMENT OF TRANSPORTATION



LOCATION MAP

LATITUDE: 39° 43' 7.84" N LONGITUDE: 84° 12' 53.69" W



PORTION TO BE IMPROVED	
INTERSTATE HIGHWAY	
FEDERAL ROUTES	
STATE ROUTES	
COUNTY & TOWNSHIP ROADS	
OTHER ROADS	

DESIGN DESIGNATION

	S.L.M. 7.53 TO 9.31	S.L.M. 9.31 TO 11.09
CURRENT ADT (2016)	87,000	86,000
DESIGN YEAR ADT (2036)	96,000	91,000
DESIGN HOURLY VOLUME (2036)	8,600	8,200
DIRECTIONAL DISTRIBUTION	59%	54%
TRUCKS (24 HOUR B&C)	15%	16%
DESIGN SPEED	65	65
LEGAL SPEED	55	55
DESIGN FUNCTIONAL CLASSIFICATION:		
FREEWAY / EXPRESSWAY		
NHS PROJECT	YES	

DESIGN EXCEPTIONS

NONE

INDEX OF SHEETS:

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UNDERGROUND UTILITIES
CONTACT BOTH SERVICES TWO WORKING DAYS BEFORE YOU DIG.

Call Before You Dig
1-800-362-2764

(Non-members must be called directly)

OIL & GAS PRODUCERS
UNDERGROUND PROTECTION SERVICE
1-800-925-0988

PLAN PREPARED BY:
OHIO DEPARTMENT OF TRANSPORTATION
DISTRICT 7
1001 ST. MARYS AVE.
SIDNEY, OHIO 45365

ENGINEERS SEAL:

SIGNED: Chris Weiss
DATE: 12/19/18

STANDARD CONSTRUCTION DRAWINGS				SUPPLEMENTAL SPECIFICATIONS	SPECIAL PROVISIONS			
BP-3.1	7/18/14	TC-41.20	10/18/13	MT-95.30	7/21/17	800	1/18/19	
BP-9.1	7/21/17	TC-41.30	10/18/13	MT-95.50	7/21/17	808	7/20/18	
DM-4.4	1/15/16	TC-42.20	10/18/13	MT-98.10	1/20/17	821	4/20/12	
		TC-52.10	10/18/13	MT-98.11	1/20/17	832	1/17/14	
		TC-52.20	7/20/18	MT-98.20	7/18/14	908	10/20/17	
		TC-65.10	1/17/14	MT-98.22	1/20/17	921	4/20/12	
		TC-65.11	7/21/17	MT-99.20	7/20/18	987	1/16/09	
				MT-99.30	1/19/18			
		TC-72.20	7/20/18					
				MT-101.90	7/21/17			
				MT-102.20	7/18/14			
				MT-104.10	10/16/15			
				MT-105.10	7/19/13			

FEDERAL PROJECT NO. E151(022)
PID NO. 100862
CONSTRUCTION PROJECT NO. 00000
RAILROAD INVOLVEMENT NONE
MOT-75-7.53

MOT - IR 75-07.53
190223 PID - 100862
Dist 7 4/4/2019

Contract Proposal Available @
www.contracts.dot.state.oh.us/home

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ALIGNMENT AND PROFILE

THE WORK PROPOSED BY THIS PROJECT CONSISTS OF JOINT REPAIRING OF THE EXISTING PAVEMENT AND RESEALING OF EXISTING LONGITUDINAL JOINTS. THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT WILL NOT BE CHANGED.

**ITEM 253 - PAVEMENT REPAIR, AS PER PLAN (A)
ITEM 253 - PAVEMENT REPAIR, AS PER PLAN (B)
ITEM 253 - PAVEMENT REPAIR, AS PER PLAN (C)**

PAVEMENT REPAIR SHALL BE IN ACCORDANCE WITH ITEM 253 - PAVEMENT REPAIR, WITH THE FOLLOWING ADDITIONS:

THE AREAS OF ITEM 253, PAVEMENT REPAIR, AS PER PLAN ARE LOCATED THROUGHOUT THE PROJECT LIMITS. REPAIR AREAS INCLUDE THE DRIVING LANES AND LANE LINES.

THE ENGINEER SHALL DESIGNATE THE LOCATIONS AND LIMITS OF THE AREAS TO BE REPAIRED. THE AREAS SHALL BE ROUGHLY RECTANGULAR IN SHAPE AND SAWED OR MILLED TO A NEAT LINE. THE DEPTH OF REMOVAL, AS DIRECTED BY THE ENGINEER, SHALL BE SUFFICIENT TO REMOVE ALL DETERIORATED PAVEMENT. THE ENTIRE AREA INCLUDING VERTICAL FACES SHALL BE COATED PRIOR TO PLACING THE REPLACEMENT MATERIAL PER 253.03.

PAVEMENT REPAIR (A) AREAS SHALL BE A MINIMUM OF 12.5 FEET IN WIDTH, 50 FEET IN LENGTH AND 2 INCHES IN DEPTH OR AS DIRECTED BY THE ENGINEER. THE REPLACEMENT MATERIAL SHALL BE ITEM 442, ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A, (448) AND FINISHED TO MATCH THE EXSTING PAVEMENT SURFACE. SEALING THE PERIMETER OF THE REPAIR AREA PER 253.03 IS INCLUDED IN THE PAYMENT OF ITEM 253.

PAVEMENT REPAIR (B) AREAS ALONG THE LANE AND EDGE LINES SHALL BE A MINIMUM OF 2 FEET IN WIDTH AND 2 INCHES IN DEPTH OR AS DIRECTED BY THE ENGINEER. THE REPLACEMENT MATERIAL SHALL BE ITEM 442, ASPHALT CONCRETE SURFACE COURSE, 12.5MM,TYPE A, (448) AND FINISHED TO MATCH THE EXSTING PAVEMENT SURFACE. SEALING THE PERIMETER OF THE REPAIR AREA PER 253.03 IS INCLUDED IN THE PAYMENT OF ITEM 253.

PAVEMENT REPAIR (C) AREAS AT THE BEGIN AND END OF BOTH APPROACH SLABS WHERE EXISTING ASPHALT MEETS EXISTING CONCRETE. FOR A WIDTH OF 48' THAT EXTENDS 2' BEYOND THE EXISTING EDGE LINES, AND A LENGTH OF 10', AND 6" IN DEPTH, OR AS DIRECTED BY THE ENGINEER. THE REPLACEMENT MATERIAL SHALL BE ITEM 442, ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A, (448) AND FINISHED TO MATCH BOTH THE EXISTING ASPHALT PAVEMENT AND THE EXISTING CONCRETE APPROACH SLABS. SEAL THE PERIMETER OF THE REPAIR AREA AS PER 253.03, IS INCLUDED IN THE PAYMENT OF ITEM 253.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER.

ITEM 253 - PAVEMENT REPAIR, AS PER PLAN (A)
MOT 75 - SLM 7.53 TO SLM 11.09 = 2000 SY

ITEM 253 - PAVEMENT REPAIR, AS PER PLAN (B)
MOT 75 - SLM 7.53 TO SLM 11.09 = 4500 SY

ITEM 253 - PAVEMENT REPAIR, AS PER PLAN (C)
MOT-75- SLM 9.32 TO SLM 11.09 = 889 SY

STRUCTURE WORK:

MOT-75-0932, SFN 5706998
ITEM 516 - JOINT SEALER, 705.04 695 FT.
ITEM 519 - PATCHING CONCRETE BRIDGE DECK, TYPE B
DECK AND BACKWALL PATCHING..... 50 SQ. FT.
APPROACH SLAB PATCHING 20 SQ. FT.
TOTAL (70 SF /9) 7.8 SQ. YD.

MOT-75-0958, SFN 5707021
ITEM 516 - JOINT SEALER, 705.04 1136 FT.
ITEM 519 - PATCHING CONCRETE BRIDGE DECK, TYPE B
DECK AND BACKWALL PATCHING..... 100 SQ. FT.
APPROACH SLAB PATCHING 20 SQ. FT.
TOTAL (120 SF /9) 13.3 SQ. YD.

MOT-75-1044, SFN 5707056
ITEM 516 - JOINT SEALER, 705.04 1977 FT.
ITEM 519 - PATCHING CONCRETE BRIDGE DECK, TYPE B
DECK AND BACKWALL PATCHING..... 50 SQ. FT.
APPROACH SLAB PATCHING 20 SQ. FT.
TOTAL (70 SF /9) 7.8 SQ. YD.

MOT-75-1078, SFN 5707080
ITEM 516 - JOINT SEALER, 705.04 513 FT.
ITEM 519 - PATCHING CONCRETE BRIDGE DECK, TYPE B
DECK AND BACKWALL PATCHING..... 50 SQ. FT.
APPROACH SLAB PATCHING 20 SQ. FT.
TOTAL (70 SF /9) 7.8 SQ. YD.

MOT-75-1109, SFN 5707110
ITEM 516 - JOINT SEALER, 705.04 418 FT.
ITEM 519 - PATCHING CONCRETE BRIDGE DECK, TYPE B
DECK AND BACKWALL PATCHING..... 00 SQ. FT.
APPROACH SLAB PATCHING 20 SQ. FT.
TOTAL (20 SF /9) 2.2 SQ. YD.

PAVEMENT MARKINGS AND RPM's

THESE QUANTITIES PROVIDED IN THE GENERAL SUMMARY ARE INTENDED TO BE USED TO REPLACE THE MARKINGS AND RPM's THAT HAVE BEEN REMOVED OR DAMAGED FROM THE PAVEMENT REPAIRS.

ITEM 614 - WORK ZONE LANE LINE, CLASS 1, 6", 642 PAINT = 2.00 MILES
ITEM 614 - WORK ZONE EDGE LINE, CLASS 1, 6", 642 PAINT = 1.00 MILE
ITEM 621 - RPM = 96 EACH
ITEM 621 - RAISED PAVEMENT MARKER REMOVED = 96 EACH
ITEM 642 - EDGE LINE, 6" = 1.00 MILE
ITEM 642 - LANE LINE, 6" = 2.00 MILE
ITEM 642 - CHANNELIZING LINE, 12" = 300 FT
ITEM 642 - DOTTED LINE, 6" = 300 FT

PROTECTION OF CARILLON PARK, THE GREAT MIAMI RIVER RECREATION TRAIL, AND THE GREAT MIAMI RIVER WATER TRAIL:

THE CONTRACTOR SHALL NOT STAGE EQUIPMENT OR MATERIALS WITHIN THE LIMITS OF THESE RECREATIONAL RESOURCES OR RESTRICT ACCESS TO THESE RECREATIONAL RESOURCES.

ITEM 611 - INLET RECONSTRUCTION TO GRADE, AS PER PLAN

THE CONTRACTOR MUST FIRST SAW CUT THE EXISTING ASPHALT CONCRETE ONE FOOT (1') OUTSIDE OF THE EXISTING FRAME. THE CONTRACTOR SHALL THEN BE RESPONSIBLE FOR REMOVING THE GRATE AND FRAME FOR REUSE. THE CONTRACTOR SHALL ALSO TAKE CARE IN REMOVING THE ONE FOOT (1') SECTION OF THE EXISTING ASPHALT CONCRETE AS NOT TO DAMAGE THE EXISTING ASPHALT CONCRETE TO REMAIN IN PLACE. THE PROPOSED SAW CUT EDGE SHALL BECOME THE CLEAN STRAIGHT EDGE TO FORM THE PROPOSED CONCRETE APRON. WITHIN THE EXISTING BASIN THE CONTRACTOR SHALL REMOVE, PATCH, OR REPLACE THE EXISTING DETERIORATED AREAS OF CONCRETE, AS PER CM&S 611 AND REBUILD BACK TO MATCH GRATE AND FRAME TO THE EXISTING GRADE. THE CONTRACTOR SHALL THEN INSTALL THE PROPOSED ONE FOOT (1') CONCRETE APRON TO FILL ALL THE VOIDS FROM THE REMOVAL OF EXISTING ASPHALT CONCRETE. THE CONTRACTOR SHALL USE A FAST SETTING CONCRETE (TYPE B) FOR ALL REPAIRS, INCLUDING A WIRE MESH WHEN POURING AND FORMING THE PROPOSED APRON. ANY DAMAGE TO THE EXISTING INLET MEDIAN BARRIER DUE TO CONTRACTOR NEGLIGENCE SHALL BE REPAIRED AT THE CONTRACTORS COST AND TO THE SATISFACTORY OF THE PROJECT ENGINEER.

REMOVAL AND REPLACEMENT OF THE GRATE, FRAME AND BASE MATERIAL, REMOVAL OF EXISTING ASPHALT CONCRETE, AND PROPOSED APRON USING FAST SETTING CONCRETE (TYPE B) WITH WIRE MESH, SHALL BE INCLUDED IN THE COST OF THIS 611 ITEM.

ALL WORK TO RECONSTRUCT THIS INLET SHALL BE DONE DURING THE NIGHT TIME LANE CLOSURES FOR PAVEMENT REPAIRS, AND BACK OPEN TO TRAFFIC AT THE SAME TIME. CONCRETE BARRIER HAS BEEN PROVIDED IN THE MOT FOR PROTECTION OF ANY VOIDS NOT COMPLETED BY THE TIME THE ROADWAY IS TO BE OPEN TO TRAFFIC.

ALL WORK, LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO COMPLETE THIS ITEM OF WORK SHALL BE PAID FOR UNDER THE UNIT PRICE BID PER EACH FOR ITEM 611, INLET RECONSTRUCTION TO GRADE, AS PER PLAN, UNLESS OTHERWISE STATED IN THESE PLANS. (SEE TREATMENT LEGEND ON SHEET 6/8.)

ITEM 611 - INLET RECONSTRUCTION TO GRADE, AS PER PLAN 1 EACH

ITEM 618 - RUMBLE STRIPS, (ASPHALT CONCRETE), AS PER PLAN

THE INTENT OF THIS ITEM IS TO REPLACE ANY SECTIONS OF EXISTING RUMBLE STRIPS THAT ARE DISTURBED DURING THE REMOVAL OF ASPHALT FOR THE REPAIRS. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE PROJECT ENGINEER.

ITEM 618, RUMBLE STRIPS, (ASPHALT CONCRETE), AS PER PLAN 2.04 MILES

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GENERAL NOTES

MOT -75-7.53

ITEM 614, MAINTAINING TRAFFIC (LANES OPEN DURING HOLIDAYS OR SPECIAL EVENTS)

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

CHRISTMAS	FOURTH OF JULY
NEW YEARS	LABOR DAY
MEMORIAL DAY	THANKSGIVING
2019 NCAA FIRST FOUR - MARCH 19,20	

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY OR EVENT	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY
THURSDAY (THANKSGIVING ONLY)	6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

PERMITTED LANE CLOSURE

LANE CLOSURES SHALL ONLY BE IMPLEMENTED AT THE TIMES LISTED ON THE OHIO DEPARTMENT OF TRANSPORTATION'S PERMITTED LANE CLOSURE WEB SITE WHICH IS LOCATED AT:

HTTP://PLCM.DOT.STATE.OH.US

THE PERMITTED CLOSURE TIMES LISTED ON THE WEBSITE, FOURTEEN (14) CALENDAR DAYS PRIOR TO THE BID LETTING DATE, SHALL BE IN EFFECT FOR THIS PROJECT.

NO WORK WITHIN ACTIVE TRAVEL LANES OR WHICH WILL SLOW TRAFFIC IS PERMITTED AT ANY OTHER TIMES.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE FOR EACH MINUTE THE ABOVE DESCRIBED LANE CLOSURE RESTRICTIONS ARE VIOLATED PER THE LANE VALUE CONTRACT TABLE BELOW AND PN 127.

LANE VALUE CONTRACT TABLE

DESCRIPTION OF CRITICAL LANE/ RAMP TO BE MAINTAINED	RESTRICTED TIME PERIOD	TIME UNIT	DISINCENTIVE \$ PER TIME UNIT
IR-75	SEE ABOVE	MINUTE	280

ITEM 614 - REPLACEMENT DRUMS

DRUMS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT DRUMS SHALL BE NEW.

PAYMENT FOR THE NEW DRUMS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614 - REPLACEMENT DRUM, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGE DRUM, AND PROVIDING AND MAINTAINING THE REPLACEMENT DRUM IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL DRUM.

ITEM 614 - REPLACEMENT DRUMS = 10 EACH

FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC (PLAN NOTE 642-2).

ITEM 614 - MAINTAINING TRAFFIC

TRAFFIC SHALL BE MAINTAINED AT ALL TIMES. THE LENGTH OF RESTRICTED TRAFFIC WORK ZONES SHALL BE KEPT TO A MAXIMUM TWO (2.0) MILE WORK ZONE CONSISTENT WITH THE SPECIFICATION REQUIREMENTS FOR A PROTECTION OF COMPLETED COURSES. IN ADDITION TO THE REQUIREMENTS AS INDICATED IN THE "OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS", AND PERTINENT ITEMS OF THE CONSTRUCTION AND MATERIALS SPECIFICATIONS, THE FOLLOWING REQUIREMENTS SHALL APPLY.

IT IS THE INTENTION TO PERFORM THE REQUIRED WORK WITH THE LEAST INCONVENIENCE TO AND THE MAXIMUM SAFETY OF THE CONTRACTOR AND THE TRAVELING PUBLIC. ANY VARIANCES FROM THESE MAINTENANCE OF TRAFFIC NOTES MUST BE APPROVED IN ADVANCE IN WRITING BY THE DIRECTOR. TRAFFIC IS TO BE MAINTAINED IN A UNIFORM PATTERN THROUGHOUT THE ENTIRE LENGTH OF THE PROJECT AND NOT BE SUBJECTED TO CONSTANT LANE SHIFTS.

THE CONTRACTOR'S OPERATIONS SHALL BE ARRANGED TO PREVENT ANY INTERFERENCE TO THE CONTINUOUS FLOW OF TRAFFIC. ALL VEHICLES, EQUIPMENT, WORKERS AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIMES TO ONE SIDE OF THE PAVEMENT UNLESS OTHERWISE APPROVED BY THE ENGINEER.

THE CONTRACTOR SHALL BE REQUIRED TO PROVIDE, ERECT, MAINTAIN (IN PROPER POSITION, CLEAN, LEGIBLE AND GOOD WORKING CONDITION) AND REMOVE ALL LIGHTS, SIGNS BARRICADES, CONES AND ALL OTHER TRAFFIC CONTROL DEVICES NECESSARY FOR THE MAINTENANCE OF TRAFFIC, INCLUDING PAVEMENT MARKINGS.

ITEM 614 - MAINTAINING TRAFFIC (CONT.)

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

* DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

* DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC, OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

* FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION OR AT THE POINT OF ROAD CLOSURE, AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS (cont.)

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RE-TURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 120 HOURS.

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

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MAINTENANCE OF TRAFFIC NOTES

MOT-75-7.53

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NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO D7 PERMITS & PIO
RAMP & ROAD CLOSURES	>= 2 WKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HRS & < 2 WKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HRS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES AND RESTRICTIONS	>= 2 WKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&M 614.03.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN (CONTINUED)

THE PROBABLE PCMS LOCATIONS AND WORK LIMITS FOR THOSE LOCATIONS ARE SHOWN ON SHEET(S) OF THE PLAN. PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

(THE CONTRACTOR SHALL IMPLEMENT A SYSTEM WHEREBY CHANGEABLE MESSAGES WILL BE IMPLEMENTED WITHIN ONE (1) HOUR FOLLOWING TELEPHONE NOTIFICATION FROM THE PROJECT ENGINEER TO A DESIGNATED PHONE.)

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRE-CONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE. THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL (IN ACTIVE CELLULAR PHONE AREAS) ALLOW REMOTE SIGN ACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS AND REVISIONS TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES. ONE REMOTE DATA INPUT DEVICE (LAPTOP COMPUTER PLUS MODEM OR EQUIVALENT) SHALL BE FURNISHED FOR USE BY THE DISTRICT TRAFFIC ENGINEER, OR EQUIVALENT, AND SHALL BE INSURED AGAINST THEFT.

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&M 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN (CONTINUED)

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 4 SIGN MONTH (ASSUMING TWO PCMS FOR TWO MONTH)

ITEM 614, WORK ZONE SPEED ZONES (WZSZS)

THE FOLLOWING WORK ZONE SPEED ZONE (WZSZ) SPEED LIMIT REVISION(S) HAVE BEEN APPROVED FOR USE ON THIS PROJECT WHEN WORK ZONE CONDITIONS AND FACTORS ARE MET AS DESCRIBED BELOW:

WZSZ REVISION NUMBER	COUNTY & ROUTE	DIRECTION	POSTED
WZ-40315	MOT-IR 75	NB/SB	65 MPH
WZ-40316	MOT-IR 75	NB/SB	55 MPH

POTENTIAL WZSZ LOCATIONS SHALL HAVE AN ORIGINAL (PRE-CONSTRUCTION) POSTED SPEED LIMIT OF 55 MPH OR HIGHER, A QUALIFYING WORK ZONE CONDITION OF AT LEAST 0.5 MILE IN LENGTH, AN EXPECTED WORK DURATION OF AT LEAST THREE HOURS, AND A WORK ZONE CONDITION IN PLACE THAT REDUCES THE EXISTING FUNCTIONALITY OF THE TRAVEL LANES OR SHOULDERS (I.E., LANE CLOSURE, LANE SHIFT, CROSSOVER, CONTRAFLOW AND/OR SHOULDER CLOSURE). THE LENGTH OF THE WORK ZONE CONDITION IS MEASURED FROM THE BEGINNING OF THE TAPER FOR THE SUBJECT WORK ZONE CONDITION IMPACTING THE TRAVEL LANES AND/OR SHOULDER TO THE END OF THE DOWNSTREAM TAPER, WHERE DRIVERS ARE RETURNED TO TYPICAL ALIGNMENT. AN EXPECTED WORK DURATION OF AT LEAST THREE HOURS IS REQUIRED TO BALANCE THE ADDITIONAL EXPOSURE CREATED BY INSTALLING AND REMOVING WZSZ SIGNING WITH THE TIME NEEDED TO COMPLETE THE WORK.

IF THE WORK ZONE MEETS THESE MINIMUM CRITERIA, IT SHALL BE ANALYZED FURTHER USING TABLE 1 BELOW TO DETERMINE IF AND WHEN IT QUALIFIES FOR A SPEED LIMIT REDUCTION. DEPENDING ON THE ORIGINAL POSTED SPEED LIMIT, THE TYPE OF TEMPORARY TRAFFIC CONTROL USED, AND WHETHER OR NOT WORKERS ARE PRESENT, A WARRANTED WZSZ WILL VARY IN THE APPROVED SPEED LIMIT TO BE POSTED OVER TIME.

C&M ITEM 614, PARAGRAPH 614.02(B), INDICATES THAT TWO DIRECTIONS OF A DIVIDED HIGHWAY ARE CONSIDERED SEPARATE HIGHWAY SECTIONS. THEREFORE, IF THE WORK ON A MULTI-LANE DIVIDED HIGHWAY IS LIMITED TO ONLY ONE DIRECTION, A SPEED LIMIT REDUCTION IN THE DIRECTION OF THE WORK DOES NOT AUTOMATICALLY CONSTITUTE A SPEED LIMIT REDUCTION IN THE OPPOSITE DIRECTION. EACH DIRECTION SHALL BE ANALYZED INDEPENDENTLY FROM EACH OTHER.

ALL WZSZS FLUCTUATE BETWEEN TWO APPROVED REDUCED SPEED LIMITS OR BETWEEN AN APPROVED REDUCED SPEED LIMIT AND THE ORIGINAL POSTED SPEED LIMIT. ONLY ONE OF TWO SIGNING STRATEGIES SHALL BE USED TO IMPLEMENT A WZSZ.

WZSZS USING DSL SIGN ASSEMBLIES SHALL BE IN ACCORDANCE WITH THIS NOTE, APPROVED LIST SUPPLEMENTAL SPECIFICATIONS (SS) 808 AND 908, AND TRAFFIC SCD MT-104.10.

ITEM 614, WORK ZONE SPEED ZONES (WZSZS) (CONTINUED...)

ONLY ONE WARRANTED SPEED LIMIT APPLIES AT ANY ONE TIME; SPEED LIMIT REDUCTIONS ARE NOT CUMULATIVE. WZSZS SHALL NOT BE USED FOR MOVING/MOBILE ACTIVITIES, AS DEFINED IN ODOTCD PART 6.

WHEN LOOKING UP THE WARRANTED WORK ZONE SPEED LIMITS, ALWAYS USE THE ORIGINAL, PRE-CONSTRUCTION, POSTED SPEED LIMIT. DO NOT USE A PRIOR OR CURRENT WORK ZONE SPEED LIMIT AS A LOOK UP VALUE IN THE TABLE. POSITIVE PROTECTION IS GENERALLY REGARDED AS PORTABLE BARRIER OR OTHER RIGID BARRIER IN USE ALONG THE WORK AREA WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WITHOUT POSITIVE PROTECTION IS GENERALLY REGARDED AS USING DRUMS, CONES, SHADOW VEHICLE, ETC., ALONG THE WORK AREA WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WORKERS ARE CONSIDERED AS BEING PRESENT WHEN ON-SITE, WORKING WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WHEN THE WORK ZONE CONDITION REDUCING THE EXISTING FUNCTIONALITY OF THE TRAVEL LANES OR SHOULDERS IS REMOVED, THE SPEED LIMIT DISPLAYED SHALL RETURN TO THE ORIGINAL POSTED SPEED LIMIT.

TABLE 1: WARRANTED WORK ZONE SPEED LIMITS (MPH) FOR WORK ZONES ON HIGH-SPEED (55 MPH OR GREATER) MULTI-LANE HIGHWAYS

Original Posted Speed Limit	WITH Positive Protection		WITHOUT Positive Protection	
	Workers Present	Workers Not Present	Workers Present	Workers Not Present
55	45	50	40	50
65	55	60	50	60
60	55	60	50	60
55	50	55	45	55

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, DIGITAL SPEED LIMIT (DSL) SIGN ASSEMBLY 8 SIGN MNTH ASSUMING 4 DSL SIGN ASSEMBLIES FOR 2 MONTH(S)

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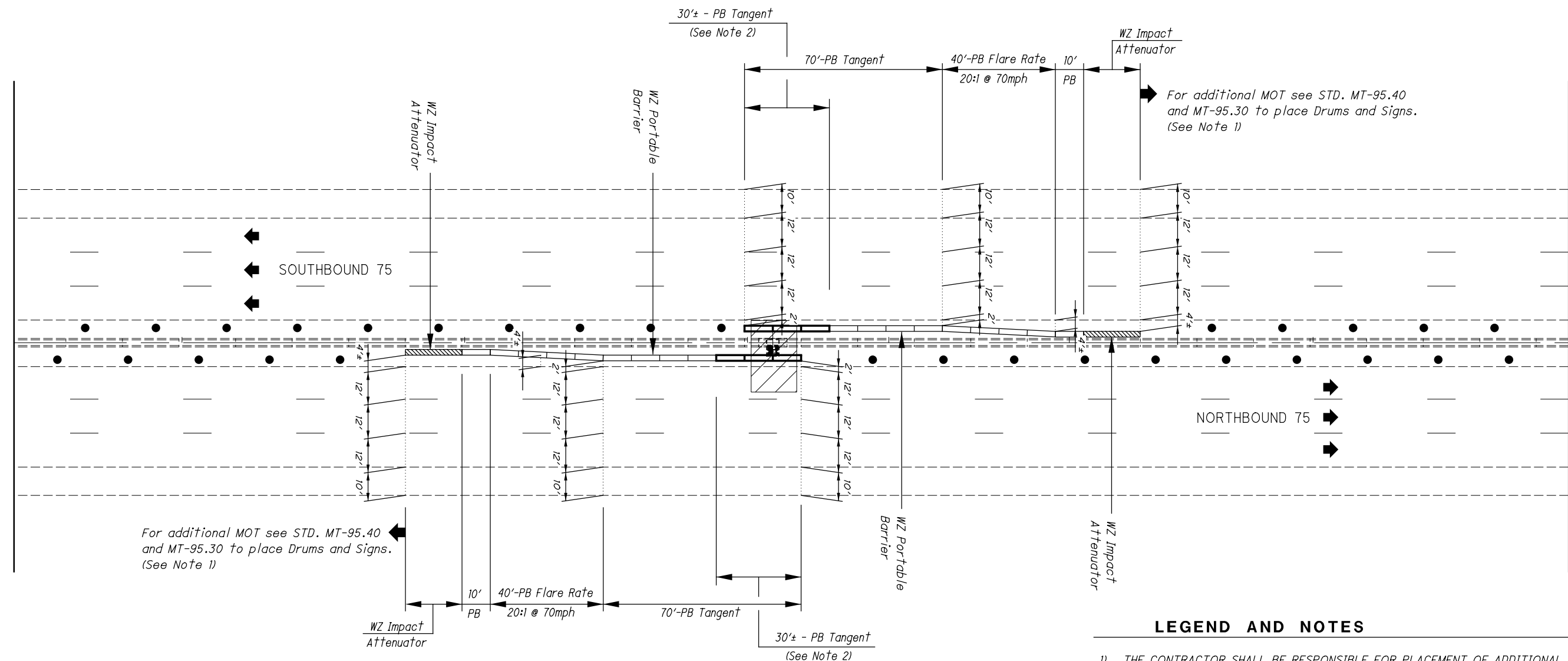
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MAINTENANCE OF TRAFFIC NOTES

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For additional MOT see STD. MT-95.40 and MT-95.30 to place Drums and Signs. (See Note 1)

For additional MOT see STD. MT-95.40 and MT-95.30 to place Drums and Signs. (See Note 1)

LEGEND AND NOTES

- 1). THE CONTRACTOR SHALL BE RESPONSIBLE FOR PLACEMENT OF ADDITIONAL MAINTENANCE OF TRAFFIC NOT SHOWN WITH THIS DETAIL, FOR THE PLACEMENT OF DRUM AND SIGNING TO BE USED DURING PERMITTED LANE CLOSURE HOURS FOR THIS PROJECT. ALL DRUMS, SIGNS AND ANY OTHER M.O.T. RELATED ITEMS SHALL BE REMOVED OR SET ASIDE DURING THE HOURS PERMITTED LANES CLOSURE IS PROHIBITED.
- 2). THE CONTRACTOR SHALL BE ALLOWED TO MOVE 30' OF PORTABLE BARRIER LOCATED AT THE INLET REPAIR LOCATION, DURING ACTIVE WORK PERIODS ONLY. THE CONTRACTOR SHALL GAIN APPROVAL OF THE TEMPORARY PLACEMENT LOCATION FOR THESE SECTIONS OF PORTABLE BARRIER FROM THE ENGINEER. AT THE END OF EACH ACTIVE WORK PERIOD THESE SECTIONS OF PORTABLE BARRIER SHALL BE PUT BACK IN PLACE, AS SHOWN. ALL MOVING AND RESETING OF PORTABLE BARRIER, SHALL BE INCLUDED IN THE COST OF THE INITIAL BARRIER PLACEMENT.
- 3). FOR ADVANCED SIGNING ON BOTH NB AND SB SIDES SEE STANDARD DRAWING MT-102.10.
- 4). QUANTITIES CARRIED TO THE GENERAL SUMMARY:

ITEM 614 - WORK ZONE IMPACT ATTENUATOR.....	2 EACH
ITEM 622 - PORTABLE BARRIER, 32"	240 FT.

= WORK ZONE AREA

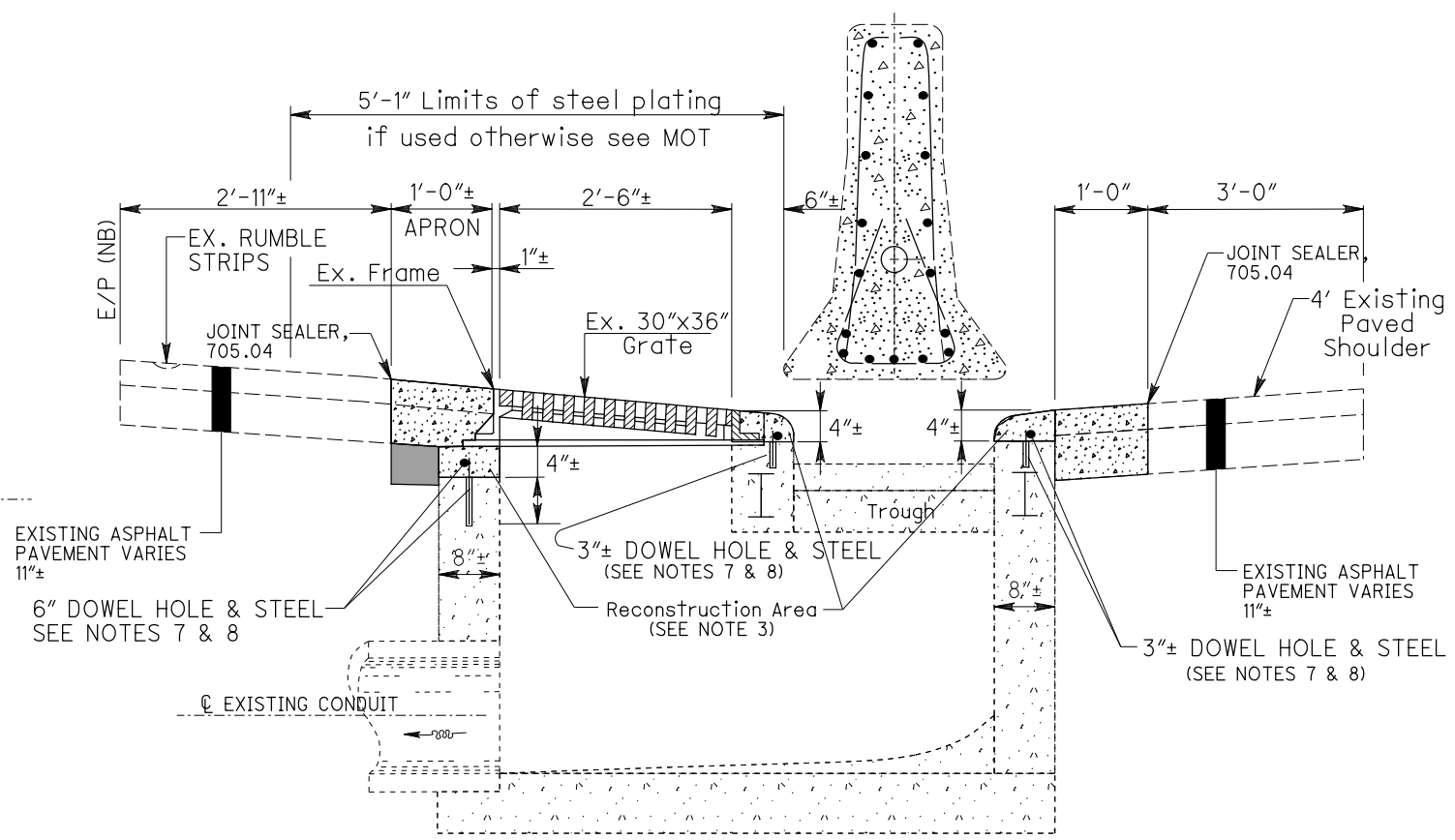
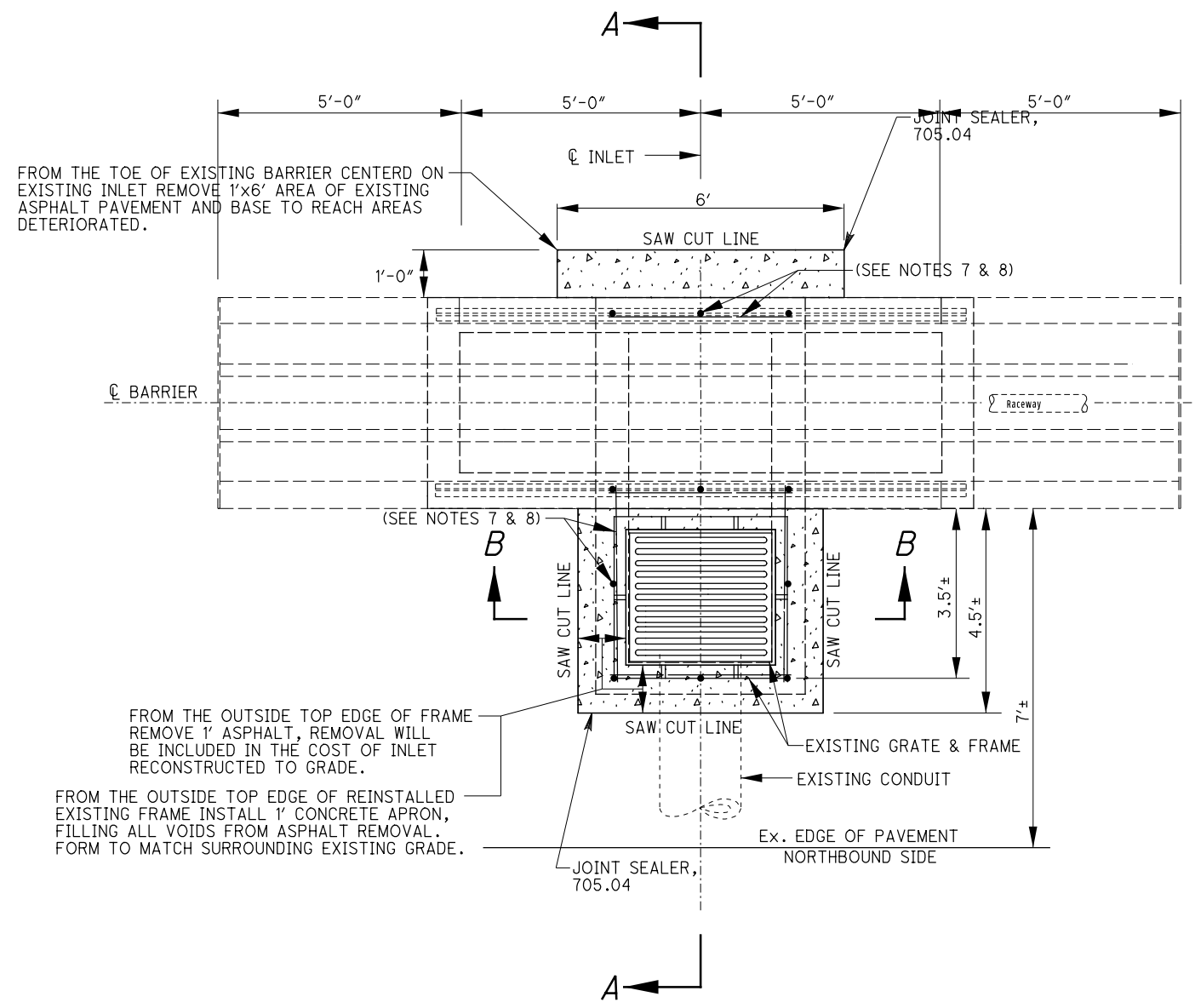
= WORK ZONE IMPACT ATTENUATOR



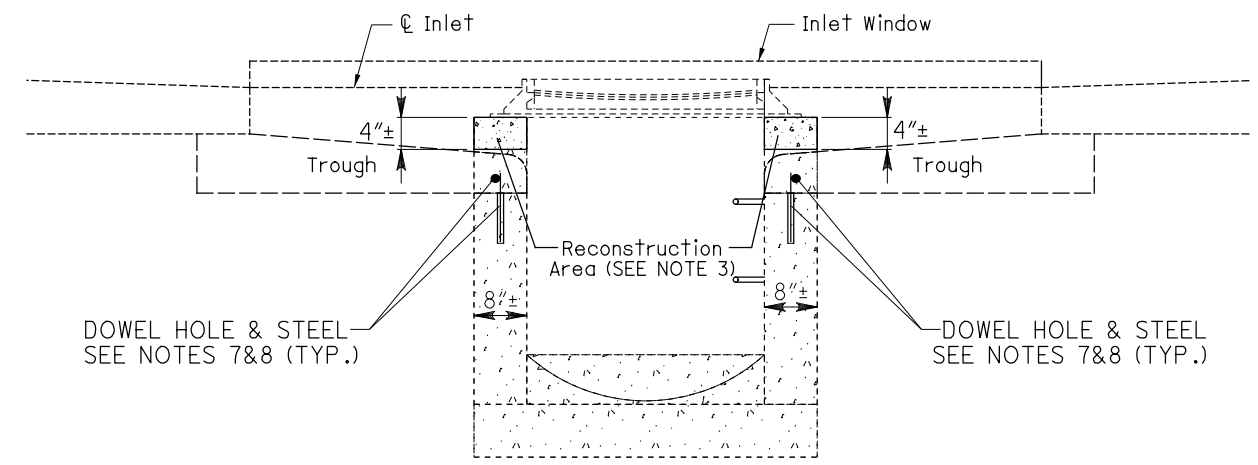
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**MAINTENANCE OF TRAFFIC SHEET
BARRIER PROTECTION FOR INLET REPAIR**

MOT-75-7.53



SECTION A-A



SECTION B-B

TREATMENT LEGEND (ALL INCLUDED AND PAID FOR IN ITEM 611)

- 1). SAW CUT EX. PAVEMENT 1' FROM OUTSIDE EDGE OF FRAME (3 SIDES). THIS CUT WILL BE THE FORM FOR PROPOSED CONCRETE APRON.
- 2). REMOVE EX. PAVEMENT WITHIN THE SAW CUT, USING CARE TO REMOVE EXISTING GRATE AND FRAME, FOR REUSE.
- 3). WITHIN THE EX. BASIN REMOVE AND REFORM AREAS OF DETERIORATION, AND PATCH ALL VOIDS. (SEE DETAILS FOR AREAS OF CONCERN.) THE CONTRACTOR SHALL BE AWARE THESE AREAS MAY VARY AND ALL QUANTITIES MAY HAVE TO ADJUSTED IN THE FIELD. ALL AREAS OF REMOVAL MUST FIRST BE APPROVED BY THE ENGINEER BEFORE REMOVAL.
- 4). CLEAN BOTH GRATE AND FRAME AND RECONSTRUCT TO GRADE.
- 6). POUR AND FORM NEW CONCRETE APRON TO GRADE. (SEE GENERAL NOTES INLET RECONSTRUCTED TO GRADE, SHEET 2)
- 7). DRILL DOWEL HOLES WITH NONSHRINK, NONMETALLIC GROUT IN ACCORDANCE WITH 2016 CMS, SECTION 510 AND AS SHOWN ON DETAILS. DOWEL DEPTH WILL VARY AS SHOWN IN SEC A-A, BECAUSE OF THE EXISTING I-BEAMS.
- 8). #4 EPOXY COATED REINFORCING STEEL WILL HAVE TO BE FIELD CUT TO EXTEND 6" BELOW REMOVAL LINE TO 2" FROM TOP OF RECONSTRUCTION. STEEL WILL VARY TO MATCH PROPOSED DOWEL HOLES. (EST. 20LBS)
- 9.) SEAL ALL COLD JOINTS WITH JOINT SEALER, 705.04

Legend:

- = EXISTING CONCRETE INLET
- = AREAS OF RECONSTRUCTION
- = AREAS OF CONCRETE APRON
- = AREAS OF VARIABLE DEPTH BASE

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ITEM 519 - PATCHING CONCRETE BRIDGE DECK, TYPE B

REPAIR ALL VISIBLY UNSOUND AREAS OR ASPHALT PATCHES AS MARKED OUT BY THE ENGINEER IN ACCORDANCE WITH PROPOSAL NOTE 512, ITEM SPECIAL - PATCHING CONCRETE BRIDGE DECKS. THE DECKS AND APPROACH SLABS ARE NOT TO BE SOUNDED FOR LOCATING PATCHES. ONLY AROUND VISIBLY UNSOUND OR ASPHALT PATCHES TO DETERMINE LIMITS. LOCATIONS AND APPROXIMATE QUANTITIES ARE DETAILED ON THIS SHEET AND 2/8.

SEAL ALL JOINTS FROM PATCHING WITH HOT-APPLIED JOINT SEALER, 705.04.

ALL LABOR, MATERIALS, EQUIPMENT, AND INCIDENTALS NECESSARY TO COMPLETE THIS WORK SHALL BE INCLUDED FOR PAYMENT UNDER ITEM 519 - PATCHING CONCRETE BRIDGE DECK, TYPE B.



EXISTING STRUCTURE VERIFICATION

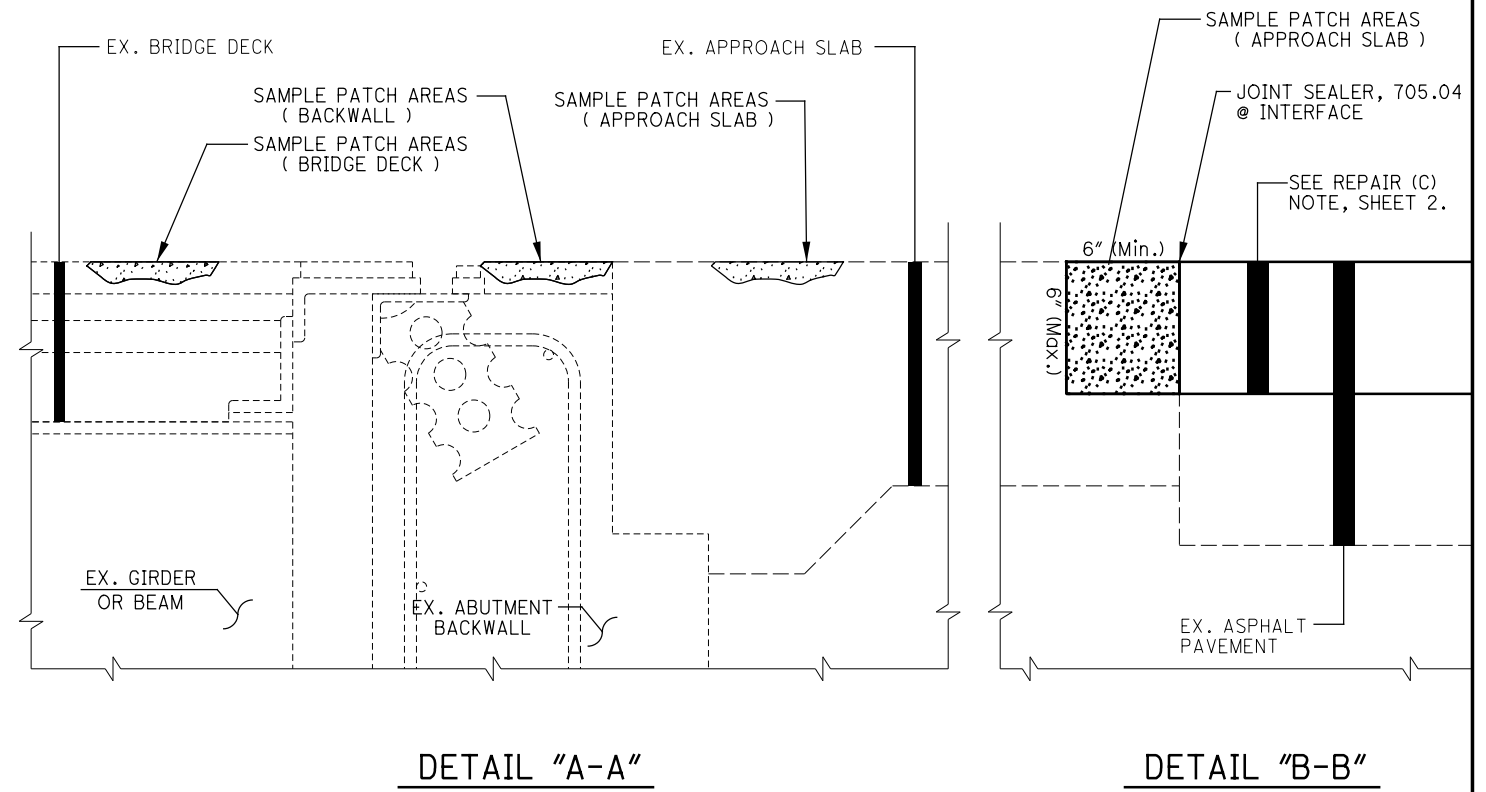
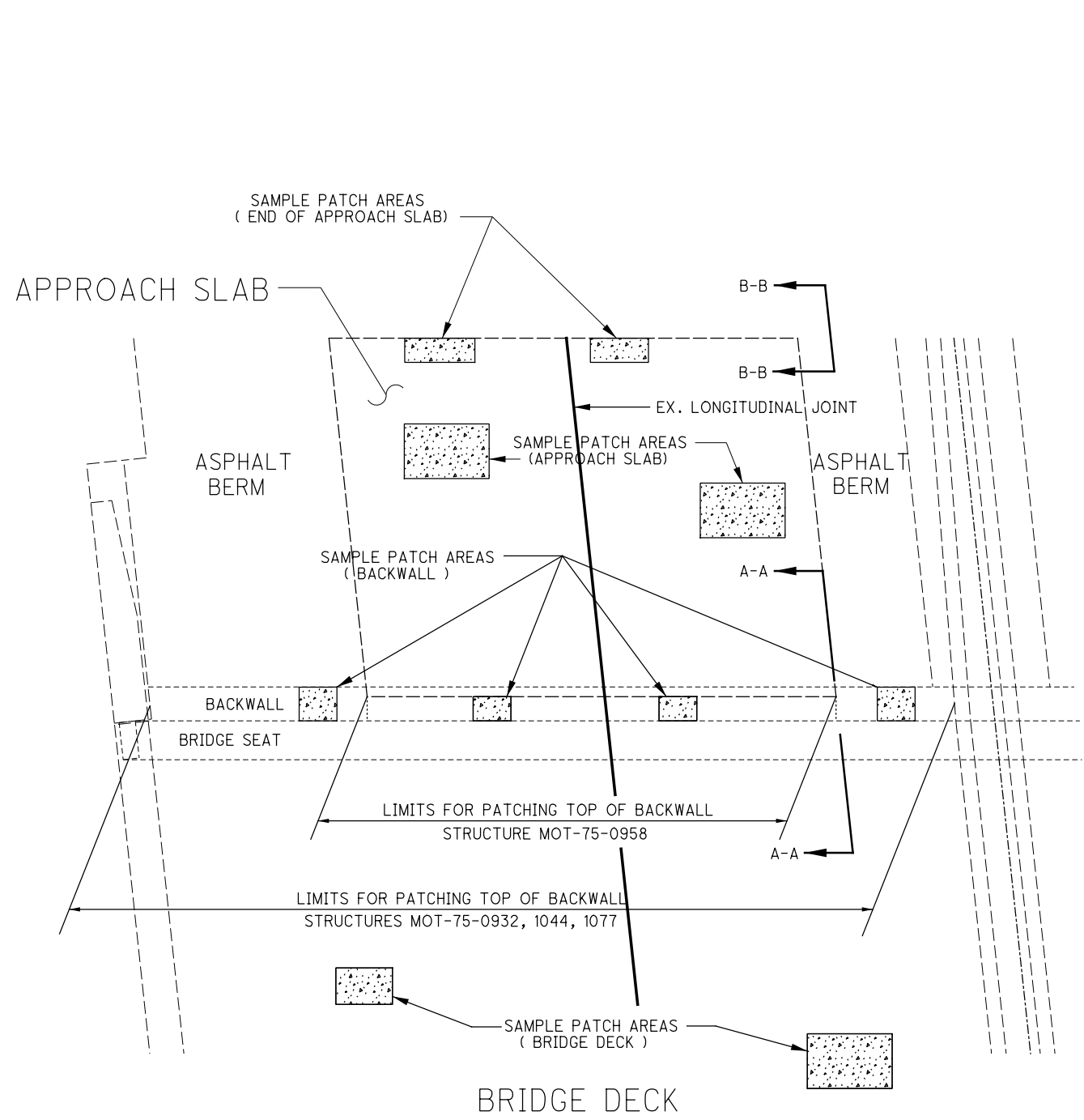
DETAILS AND DIMENSIONS SHOWN IN THESE PLANS HAVE BEEN OBTAIN FROM EXISTING PLANS, FIELD OBSERVATIONS AND MEASUREMENTS. ALL DETAILS AND DIMENSIONS ARE THEREFORE ARE TO BE CONSIDERED ±, AND FOR INFORMATION ONLY. THE CONTRACTOR IS REFERRED TO THE 2016 CMS SECTIONS 102.05 AND 105.02.

CONTRACT BID PRICE SHALL BE BASED UPON A RECOGNITION OF THE UNCERTAINTIES MENTIONED ABOVE AND UPON A PREBID EXAMINATION OF THE EXISTING STRUCTURE BY THE CONTRACTOR. HOWEVER ALL PROJECT WORK SHALL BE BASED UPON ACTUAL DETAILS AND DIMENSIONS, WHICH HAVE BEEN VERIFIED BY THE CONTRACTOR IN THE FIELD. PLANS OF THE EXISTING STRUCTURES MAY BE EXAMINED AT THE ODOT DISTRICT 7 OFFICE IN SIDNEY, OHIO OR THE OFFICE OF STRUCTURAL ENGINEERING IN COLUMBUS, OHIO.

LEGEND

DETAILS ARE NOT TO SCALE, FOR INFORMATION ONLY.

-  = POTENTIAL AREAS FOR PATCHING
-  = LONGITUDINAL JOINT



DETAIL "A-A"

DETAIL "B-B"

FOR LOCATIONS AND QUANTITIES SEE SHEET 2.

PROPOSED WORK

- 1). PATCH APPROACH SLABS, TOP OF BACKWALLS, AND BRIDGE DECKS, PER ITEM 519 - PATCHING CONCRETE BRIDGE DECK, TYPE B.
- 2). CLEANING AND SEALING EXISTING LONGITUDINAL JOINTS, PER ITEM 516 - JOINT SEALER, 705.04.

GENERAL STRUCTURAL DETAILS AND NOTES

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