

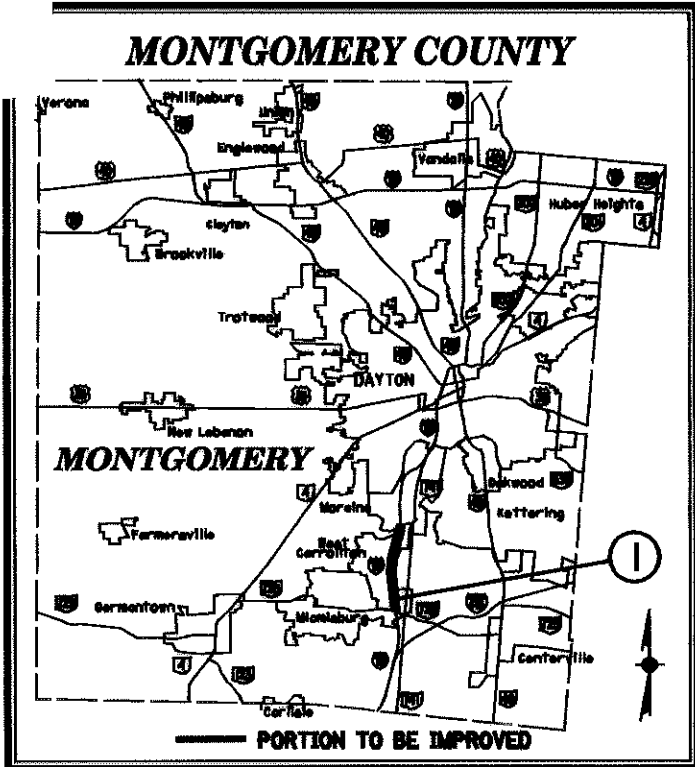
Dist 7 6/2/2004
 040364 PID - 76686
 MOT - IR 75-3.77

State Job No. 478177
 Project No.

PID No. 76686
 FED. No. E040589

STATE OF OHIO
OHIO DEPARTMENT OF TRANSPORTATION
MOT-75-3.77

LOCATION MAP



MAINTENANCE
FOUR-LANE RESURFACING

Project Earth Disturbed Area = 0.0 Acres
 Estimated Contractor Earth Disturbed Area = N/A
 Notice of Intent Earth Disturbed Area = N/A

2002 SPECIFICATIONS

The Standard 2002 Specifications of the State of Ohio Department of Transportation, including changes and supplemental specifications listed in the plans and the proposal shall govern these improvements.

I hereby approve these plans and declare that the making of this improvement will not require the closing of the highway to traffic and that provisions for the maintenance and safety of traffic will be as set forth in the plans and estimates.

2-2-04 *William J. Harrison* / *PM*
 Date Approved District Deputy Director
 3-1-04 *Jordan Proctor*
 Date Approved Director, Department of Transportation

PROJECT DESCRIPTION: The work proposed by this project consists of resurfacing the existing Interstate 75 roadway with Item 874, Ultrathin Bonded Asphalt Concrete with Supplement 1059 Warranty as set forth in these plans.

① MOT-75-3.77 (State Route 725 to SLM 6.14 at West Carrollton)

- 1 - Title Sheet
- 2 & 3 - Pavement Data
- 4 - Plan Sheet
- 5 - General Notes
- 6 & 7 - Maintenance of Traffic in Work Zones
- 8 thru 10 - Drop-Offs
- 11 - Raised Pavement Markers
- 12 - General Summary

UNDERGROUND UTILITIES
 TWO WORKING DAYS
BEFORE YOU DIG
 CALL 1-800-362-2764 (TOLL FREE)
 OHIO UTILITIES PROTECTION SERVICE
 NON-MEMBERS MUST BE CALLED DIRECTLY

ENGINEER'S SEAL

Signed *Paul Robert North*
 Date 1-30-04

PLAN PREPARED BY:

D7

District

STANDARD DRAWINGS				SUPPLEMENTAL SPECIFICATIONS	
BP-3.1	7-28-00	MT-98.15	4-19-02		
DM-4.3	7-19-02	MT-98.16	4-19-02	832	2-12-03
DM-4.4	7-19-02	MT-99.20M	1-30-95	833	2-12-03
MT-35.10	4-20-01				
MT-95.30	4-19-02	TC-65.10	10-19-01	874	7-19-02
MT-98.12	4-19-02	TC-65.11	10-19-01		
MT-98.13	4-19-02	TC-65.12	10-19-01		
MT-98.14	4-19-02	TC-71.10	4-19-02		

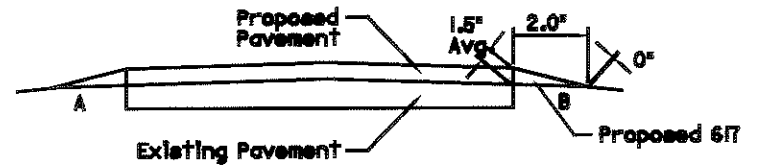
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MOT75377 January 16, 2004

PLAN NO. _____



** Item 254, Pavement Planing: Pavement Planing shall be performed one foot (1.0') along left and right edges of mainline pavement. Pavement Planing zero inches (0.0') to three quarters of an inch (0.75") for a butt joint at the front edge of the rumble strips.



TYPICAL - COMPACTED AGGREGATE

PAVEMENT DATA - MAINLINE

PART	ROUTE	LOG POINT TO LOG POINT SLM	LENGTH		PAVE- MENT WIDTH FEET	TYPICAL	PAVEMENT AREA SQ. YD.	PROPOSED PAVEMENT				617		254 PAVEMENT PLANING 0.0"-0.75" ** SQ. YD.
			MILE	FEET				407		ASPHALT CONCRETE		COMPACTED AGGREGATE		
								TACK COAT @0.10 gal/ sq. yard GALLONS	874	THICK INCH	CUBIC YARD	THICK INCH	SQUARE YARD	
I	I-75	3.77 - 3.91 NB	0.14	739	38.0	I	(Control Section)							
		3.91 - 6.14 NB	2.23	11774	38.0	I	4971/2	0.75	1036			1.5	218	2616
		3.77 - 3.91 SB	(0.14)	(739)	38.0	I	(Control Section)							
		3.91 - 6.14 SB	(2.23)	(11774)	38.0	I	4971/2	0.75	1036			1.5	218	2616
Mainline Total - Part I			2.37	12513			99424		2072				436	5232

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PAVEMENT DATA

PLAN NO.

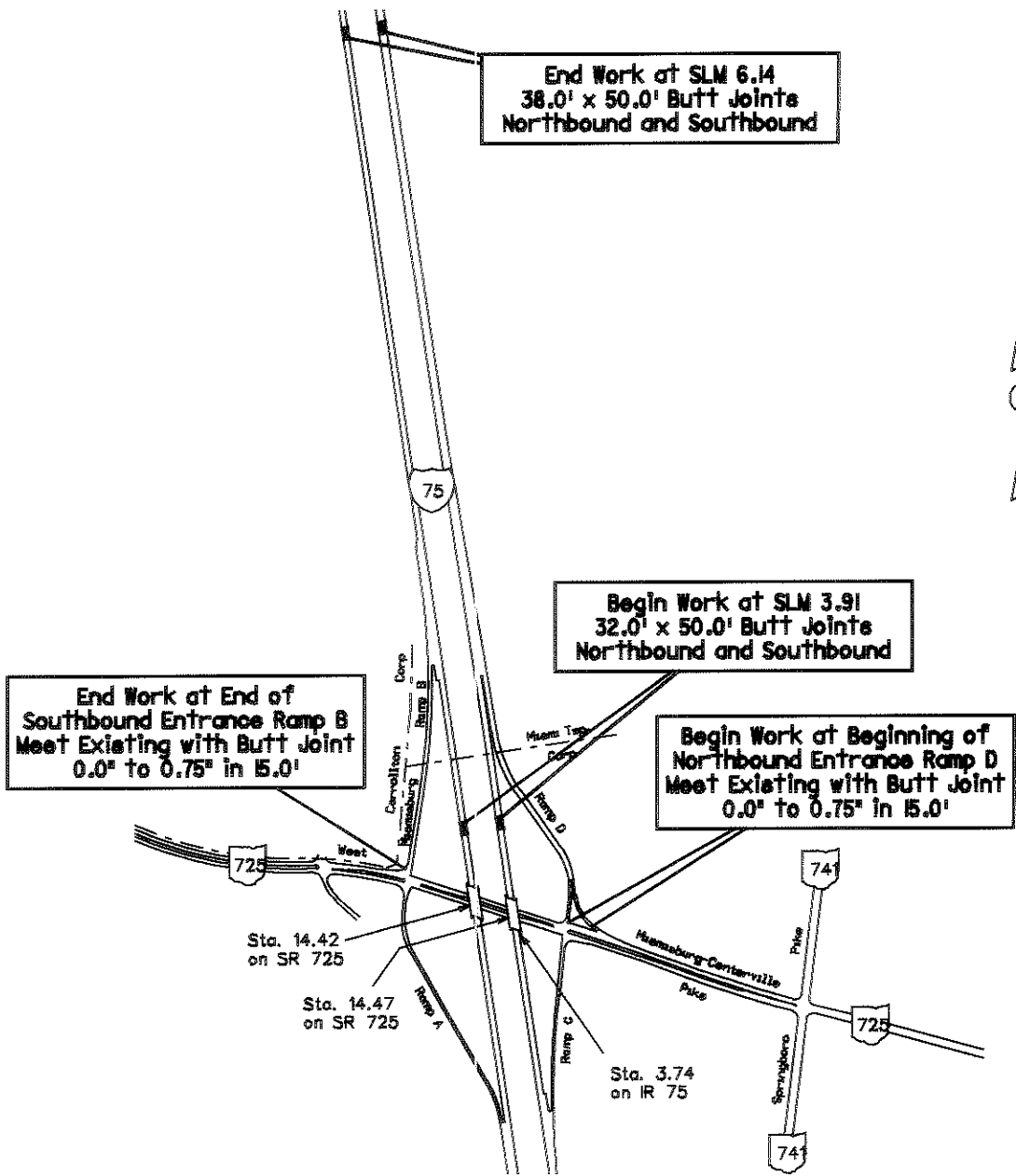
PAVEMENT DATA - EXTRA AREAS

PART	ROUTE	LOCATION	LENGTH		AVG. PAVEMENT WIDTH FEET	TYPICAL PAVEMENT AREA SQ. YD.	PROPOSED PAVEMENT				617				
			MILE	FEET			407	ASPHALT CONCRETE		COMPACTED AGGREGATE					
							TACK COAT @ 0.10 gal./ sq. yard GALLONS	874		THICK INCH	CUBIC YARD	THICK INCH			CUBIC YARD
I	I-75	Ramp B		1725	12.0	2300		0.75	48			1.5	16		
		Ramp B		740	38.0	3124		0.75	65			1.5	7		
		Ramp B		800	48.0	4267		0.75	89			1.5	15		
		Ramp D		725	22.0	1772		0.75	37			1.5	13		
		Ramp D		1900	36.0	7600		0.75	158			1.5	18		
		Ramp D		840	22.0	2053		0.75	43			1.5	8		
		Ramp D		1850	12.0	2467		0.75	51			1.5	17		
Extra Areas Total - Part I						23583			491				94		

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PAVEMENT DATA

PLAN NO.



INTERCHANGE
 IR 75 & SR 725
 MONTGOMERY COUNTY

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PLAN SHEET

II/STD/DGN/SLD/

GENERAL NOTES

PLAN NO.

ITEM SPECIAL, ASPHALT REJUVENATING AGENT

The Asphalt Rejuvenating Agent shall be applied to the four foot (4.0') and ten foot (10.0') paved shoulders Northbound and Southbound.

The Asphalt Rejuvenating Agent shall be composed of a cationic emulsion of Gilsonite Ore, uniformly emulsified with water. The material shall have a satisfactory record of service as an asphalt rejuvenating agent and in-depth sealer, such satisfactory service being based on the capability of the material to increase the ductility and penetration value of the asphalt binder in the pavement surface and to seal the pavement in depth to the intrusion of air and water.

The Contractor shall furnish the manufacturer's certification that the material proposed for use is in compliance and include copies of supporting tests and previous use documentation. The product "GSB-38", as manufactured by Asphalt Systems, Inc., of Salt Lake City, Utah is acceptable for these requirements.

The asphalt rejuvenating agent shall be applied by distributor at the temperature recommended by the manufacturer and at the pressure required for proper distribution. Before spreading, the asphalt rejuvenating agent shall be blended with water at the standard dilution ratio of 1:1. The combined mixture of asphalt rejuvenating agent and water shall be spread at the maximum rate of 0.12 gallon per square yard, as approved by the Engineer.

When application is made under traffic, a light coat of dry, gritty sand shall be applied to the surface in sufficient amount to protect the traveling public. No treated area should be considered completed until that area is determined by the Project Engineer to be safe for travel. No material shall be applied opposite the treated area until the treated area is deemed safe for travel.

Testing - Prior to application of the asphalt rejuvenating agent to the entire project, the Contractor shall apply test strips of said agent as specified above. The Contractor shall apply three (3) test strips on areas of pavement which are representative samples of the pavement to be treated as directed by the Engineer. The test strips shall be ten feet (10') in width and approximately fifty feet (50') in length. The application rate shall be 0.10 gallon per square yard, 0.12 gallon per square yard, and 0.15 gallon per square yard, respectively. The Engineer may vary the application rates as field conditions may require. No work shall be permitted until the Engineer has evaluated the test areas and determined the required rate of application for the project.

BASIS OF PAYMENT

The unit price for all work items shall include all labor, materials, sand, equipment, etc. necessary to complete the items as specified.

The accepted quantity for the construction seal with rejuvenating agent will be the number of square yards treated, as determined by the Engineer.

Part I Quantity - 41,863.0 square yards = 5024.0 gallons

Item Special - Asphalt Rejuvenating Agent	5024.0 gallons
Item Special - Testing	Lump

ITEM 253 - PAVEMENT REPAIR

This item of work shall consist of the removal of the existing pavement in areas of existing pavement failure.

The Engineer shall designate the locations and limits of the areas to be repaired. The repair areas shall be roughly rectangular in shape and sawed to a neat line. The pavement shall be removed within the designated areas by methods which will not damage the adjacent pavement. The depth of removal, as directed by the Engineer, shall be sufficient to remove all deteriorated pavement (six inch (6.0") average for Item 253 - Pavement Repair).

The repair areas shall be tacked (702.13) prior to placing the Item 448, Asphalt Concrete, Type 2 for Item 253 - Pavement Repair, As Per Plan. The asphalt shall be placed and compacted to finish flush with the adjacent existing pavement surface prior to placing the proposed asphalt concrete overlay. Compaction shall be achieved by mechanical methods to the satisfaction of the Engineer.

Payment shall include all labor, equipment and materials necessary to complete the pavement repair. An estimated quantity is provided in the General Summary to be used as directed by the Engineer. Payment will be made at the unit price bid per square yard of Item 253 - Pavement Repair, As Per Plan.

Item 253 - Pavement Repair, As Per Plan 500.0 square yards

NOTIFICATION

The Contractor shall notify the Ohio Department of Transportation, District Seven Construction Engineer two weeks prior to beginning any work. Phone (937) 497-6848.

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GENERAL NOTES

MAINTENANCE OF TRAFFIC

PLAN NO.

In addition to the requirements as indicated in the "Ohio Manual of Uniform Traffic Control for Streets and Highways", and pertinent items of the Construction and Materials Specifications, the following requirements shall apply.

Due to traffic congestion on this project, the Contractor shall be required to expedite his work to meet the time detour or lane closure time limitations as detailed in the plans. The Contractor shall meet these dates using whatever measures are necessary including, but not limited to, performing work by multiple crews, multiple shifts, overtime, etc.

Work can be performed simultaneously in the Northbound and Southbound lanes. One lane of directional traffic on I-75 will be permitted while the Contractor is actively working on or immediately adjacent to the pavement, and then only for minimum periods of time consistent with the actual requirements of the specific type of work being performed. It is intended that the roadway not be subjected to any work closures unless active work is being performed within or immediately adjacent to the closed lane. The roadway shall not be restricted to one lane directional traffic during periods of intermittent or irregular work, nor closed solely for the convenience of the Contractor. The Engineer shall make the final determination as to what constitutes active work and whether or not the actual work being performed warrants the lane closure. If the lane closure is not justified, the Engineer may order all or part of the closed lane re-opened to traffic until such time that this condition is corrected. The duration and length of all lane closures shall at all times be commensurate with the actual work being performed. Maximum lane closure shall be four (4.0) miles.

All construction work on I-75 shall be completed within forty-five (45) consecutive calendar days.

The Contractor shall submit, in detail, a schedule of operations and maintenance of traffic to the District Construction Engineer and receive written approval before work is started on the project.

Should the Contractor fail to meet any of these requirements or date set for final completion, the Contractor shall be subject to liquidated damages per Specification 108.07.

In lieu of the schedule of liquidated damages found in Section 108.07 of the Construction and Material Specifications, the Department shall assess five thousand dollars (\$5000) in liquidated damages for each calendar day beyond the number of allotted days that the specified work remains uncompleted.

It is the intention to perform the required work with the least inconvenience to and the maximum safety of the Contractor and the traveling public. Any variances from these Maintenance of Traffic Notes must be approved in advance in writing by the Director.

The Contractor shall arrange his operations to prevent any interference to the continuous flow of traffic. All vehicles, equipment, men and their activities are restricted at all times to one side of the pavement unless otherwise approved by the Engineer.

The use of berms to maintain traffic is prohibited. Should any berm areas become damaged or destroyed due to the Contractor's negligence or failure to provide adequate signs, barricades, cones, flagmen or other traffic control devices, the restoration of the berms will be at the Contractor's expense, unless otherwise approved by the Engineer.

During all hours when traffic is restricted to less than two (2) lanes in the same direction of flow, the Contractor shall employ at least one (1) qualified person to continuously patrol, twenty-four (24) hours a day, the restricted areas. The Contractor shall maintain all lights, barricades, signs, cones, drums, etc. in order to provide a safe facility for the traveling public. He shall have available all tools and materials necessary to perform this function at all times. This will be in addition to the required Law Enforcement Officers with Patrol Cars.

A minimum lane width of eleven linear feet (11.0') shall be provided at all times. A fourteen (14) day advance notice is required when the lane width is to be reduced to eleven linear feet (11.0').

Before work begins, the Contractor shall submit to the Engineer, names and telephone numbers of a person or persons who can be contacted twenty-four (24) hours a day by the Ohio Department of Transportation and all interested police agencies. This person or persons shall be responsible for placing or replacing necessary traffic control devices to maintain the traveled pavement safely.

The Contractor shall be required to provide, erect, maintain (in proper position, clean, legible and good working condition) and remove all lights, signs, barricades, cones and all other traffic control devices necessary for the maintenance of traffic, including pavement markings.

Special Duty Law Enforcement Officer(s) shall be present when any lanes are closed. They shall also be present during the setup and takedown periods.

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MAINTENANCE OF TRAFFIC

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MAINTENANCE OF TRAFFIC

PLAN NO.

The Contractor shall notify the Engineer forty-eight (48) hours in advance of any lane closures. Information regarding arrangements by the Contractor for Special Duty Law Enforcement Officer(s) may be obtained by contacting the Ohio Highway Patrol, 660 East Main Street, Columbus, Ohio. Telephone (614) 466-2660.

An estimated quantity has been provided in the General Summary to be used as directed by the Engineer.

Item Special, Law Enforcement Officer with Patrol Car 150 Hours

The Contractor shall furnish and install two (2) "Watch for Stopped Traffic" signs (OW-166) one thousand feet (1000') downstream from the "Road Construction Ahead" sign (OW-128). If traffic backups reach the "Watch for Stopped Traffic" signs, the Contractor shall install two (2) additional "Watch for Stopped Traffic" signs every two thousand feet (2000') upstream from the "Road Construction Ahead" signs. The necessity for these signs shall be constantly monitored by the Contractor.

The Contractor shall furnish and install two (2) "Road Construction Next 5 Miles" signs (OC-8) at each end of the project.

HOLIDAYS AND SPECIAL EVENTS

No work shall be performed and existing lanes shall be open to traffic during the following designated holidays or events:

Memorial Day, July 4th, Labor Day and Thanksgiving.

The period of time that the lanes are to be open depends on the day of the week on which the holiday or event falls. The following schedule shall be used to determine this period:

Day of the Week	Time All Lanes Must be Open to Traffic
Sunday	6:00 a.m. on Friday thru 8:00 p.m. on Monday
Monday	6:00 a.m. on Friday thru 8:00 p.m. on Tuesday
Tuesday	6:00 a.m. on Monday thru 8:00 p.m. on Wednesday
Wednesday	6:00 a.m. on Tuesday thru 8:00 p.m. on Thursday
Thursday	6:00 a.m. on Wednesday thru 8:00 p.m. on Monday
Friday	6:00 a.m. on Thursday thru 8:00 p.m. on Monday
Saturday	6:00 a.m. on Friday thru 8:00 p.m. on Monday

Should the Contractor fail to meet any of these requirements, the Contractor shall be assessed liquidated damages in accordance with 108.07 of the Construction and Material Specifications.

TRAFFIC

Traffic shall be maintained at all times. The length of restricted traffic zones shall be kept to a minimum consistent with the specification requirements for protection of completed courses.

The use of paved berms to maintain traffic is prohibited. Should any existing or new berm areas become damaged or destroyed due to the Contractor's negligence or failure to provide adequate signs, barricades, cones, flagmen or other traffic control devices, the restoration of the berms will be at the Contractor's expense, unless otherwise approved by the Engineer.

WORK COORDINATION

The Contractor shall coordinate the Maintenance of Traffic with the Montgomery County Interstate 75 project to the South of this project.

NIGHT TIME WORK

All work shall be completed between the hours of 8:00 p.m. and 6:00 a.m.

PAYMENT

Payment for all of the above are to be included in the lump sum bid for Item 614 - Maintaining Traffic with the following exceptions: Item 614 - Law Enforcement Officer with Patrol Car, and Item 614 - Work Zone Marking Sign (only those in addition to standard drawings).

PAVEMENT MARKINGS

Existing Pavement Markings shall be documented to be replaced in kind. The width of the Edge Line and Lane Line shall be increased from four inches (4.0") to six inches (6.0").

Item 644 - Edge Line, As Per Plan	=	5.91 miles
Item 644 - Lane Line, As Per Plan	=	10.26 miles

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MAINTENANCE OF TRAFFIC

DROP-OFFS IN WORK ZONES

PLAN NO. _____

1. It is intended that the "Dropoffs in Work Zones" drawing be used for treatment of drop-offs that develop during construction operations, and that are not otherwise provided for in the construction plans. Where the plans do not provide specific items for labor, equipment or materials to implement the drop-off treatments specified herein, they shall be included for payment in the lump sum bid for Item 614, Maintaining Traffic.
2. While the need for certain advisory signing is noted herein, it is not intended that this be indicative of all signing that may be required to advise or warn motorists, and all requirements of the Ohio Manual of Uniform Traffic Control Devices (OMUTCD) must be fulfilled.
3. In urban or otherwise heavily developed areas where pedestrians and/or bicyclists may be present in significant numbers, additional signing and protective measures other than those shown herein may be required.
4. The drop-off treatment selected for use at any given location shall be as appropriate for the prevailing conditions at the site.
5. Where concrete barrier is specified, it shall be in accordance with Standard Construction Drawing MC-9.2 and Item 622.
6. When drums are specified for a drop-off condition, a minimum number of four (4) drums shall be used. Spacing shall be as indicated in the plans or as specified in the Ohio Manual of Uniform Traffic Control Devices (OMUTCD).
7. When OW-151 (Low Shoulder) signs or OW-171 (Uneven Lanes) and OWP-171 signs are required, they shall be placed seven hundred and fifty feet (750') in advance of the condition, on all intersecting entrance ramps within the limits of the condition and immediately beyond all intersecting roadways within the limits of the condition. When the drop-off condition extends more than one-half mile, additional signs should be erected at intervals of one (1) mile or less.
8. For locations, such as at ramps, lane shifts, lane closures, etc., where traffic is required to negotiate any difference in elevation between pavements, a 3:1 slope treatment similar to the Optional Wedge Treatment shall be provided.
9. Portable concrete barrier shall be placed on the same level as the traffic surface and shall not encroach on lane width(s) designated as the minimum required for traffic use. Where drums are used, and their presence would reduce traveled lane widths to less than ten feet (10'), drums may be placed on the opposite level from that of traffic, provided the drop-off depth does not exceed five inches (5") and approval is granted by the Project Engineer.

10. Pavement Repairs (or similar work):

- a. Lengths greater than sixty feet (60') - utilize appropriate treatment from Condition I.
- b. Lengths of sixty feet (60') or less - repairs shall be affected in accordance with 255.08. Drums may be used as a separator adjacent to the traveled lane.

OPTIONAL SHOULDER TREATMENT

1. This treatment may not be used within a bituminous shoulder where a hot longitudinal joint per 401.15 is required.
2. OW-151 signs are required.



Sheet 1 of 3

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DROP-OFFS IN WORK ZONES

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DROP-OFFS IN WORK ZONES

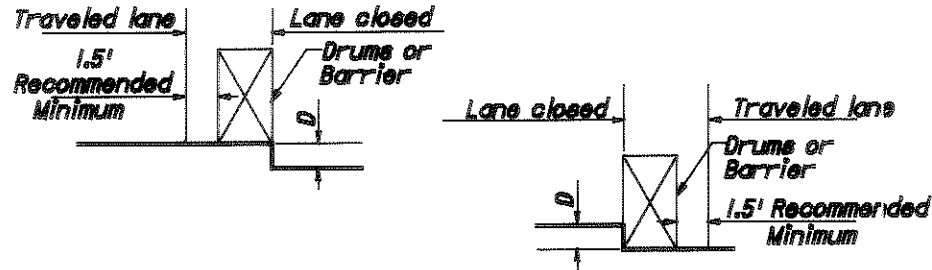
PLAN NO.

CONDITION I - DROP-OFFS BETWEEN TRAVELED LANES

1. These treatments are to be used for resurfacing, pavement planing, excavation, etc. between or within traveled lanes.

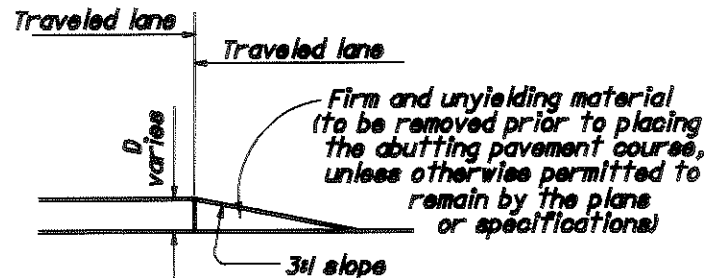
D (Inches)	Treatment
$< 1/2$	Erect OW-171 and OWP-171 signs
$> 1/2 - 3$	1) Lane closure utilizing drums* as shown below or 2) Optional Wedge Treatment
$> 3 - 5$	Lane closure utilizing drums as shown below
> 5	Lane closure utilizing portable concrete barrier as shown below

* Cones may be used for daytime only conditions



OPTIONAL WEDGE TREATMENT (MILLING OR RESURFACING)

1. This treatment may be used when permitted for Condition I only.
2. OW-171 and OWP-171 signs are required.

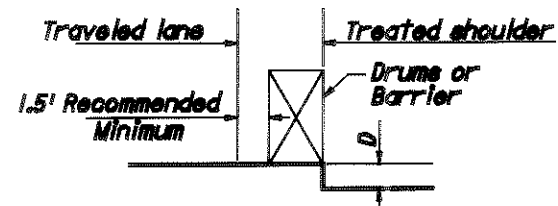


CONDITION II - DROP-OFFS WITHIN GRADED SHOULDER AREA

1. The treatments indicated below are for use in conjunction with planing, resurfacing or excavations within the graded shoulder area.
2. The graded shoulder area is that flat or gradually sloping area between the edge of a normally traveled lane and the more steeply sloping ditch foreslope or embankment slope. It's surface may be soil or turf, and/or it may be inclusive of a "treated" area (improved with aggregates, asphaltic materials or concrete). For the purposes herein, it's maximum width shall be considered to be twelve feet (12').

D (Inches)	Treatment
$< 1/2$	1) If edgelines are present, no treatment necessary, or 2) Erect OW-171 and OWP-171 signs.
$> 1/2 - 5$	1) If minimum lane width* requirements can be met, maintain lanes utilizing drums as shown below, or 2) If minimum lane width* requirements cannot be met, close adjacent lane utilizing drums, or 3) Optional Shoulder Treatment.
$> 5 - 12$ Daylight Only	If minimum lane width* requirements can be met, maintain lanes utilizing drums as shown below.
$> 5 - 24$	1) If minimum lane width* requirements can be met, maintain lanes utilizing portable concrete barrier as shown below, or 2) If minimum lane width* requirements cannot be met, close adjacent lane utilizing drums.
> 24	Lane closure utilizing portable concrete barrier as shown below.

* Minimum lane widths shall be 10' unless otherwise specified in the plans.



DROP-OFFS IN WORK ZONES

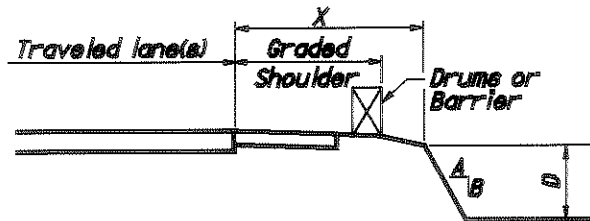
PLAN NO.

CONDITION III - DROP-OFFS BEYOND GRADED SHOULDER OR BACK OF CURB

1. See Note 2 under Condition II.
2. Use Chart A or B below, as applicable.

CHART A

- Use for:
1. Uncurbed Facilities.
 2. Curbed Facilities, where:
 - a. Curbs are less than six inches (6") in height.
 - b. Curbs are six inches (6") or greater in height and the legal speed is greater than forty (40) mph.

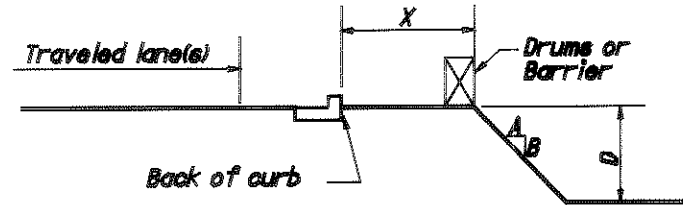


X Feet	D Inches	A/B	Treatment Required	
			Day	Night
0 - 4	Any	Any	(a)	(a)
4 - 30	Any	3:1 or Flatter	None	None
4 - 12	< 3	Steeper than 3:1	None	None
4 - 12	> 3 - > 12	Steeper than 3:1	Drums	Drums
4 - 12	> 12	Steeper than 3:1	Drums	Barrier
> 12 - 20	< 12	Steeper than 3:1	None	None
> 12 - 20	> 12 - > 24	Steeper than 3:1	Drums	Drums
> 12 - 20	> 24	Steeper than 3:1	Drums	Barrier
> 20 - 30	< 24	Steeper than 3:1	None	Drums
> 20 - 30	> 24	Steeper than 3:1	Drums	Barrier
> 30	Any	Any	None	None

(a) Use treatment specified under Condition II

CHART B

- Use for: Curbed Facilities, where the curb is six inches (6") or greater in height and the legal speed is forty (40) mph or less.



X Feet	D Inches	A/B	Treatment Required	
			Day	Night
0 - 10	< 12	Any	None	Drums
0 - 10	> 12	Any	Drums	Drums
> 10	Any	Any	None	None

GENERAL SUMMARY

PLAN NO.

PART 1	PART 2	ITEM	ITEM EXTENSION	GRAND TOTAL	UNIT	DESCRIPTION
578		202	23500	578	SQ. YD.	Wearing Course Removed
485		202	54100	485	EACH	Raised Pavement Marker Removed for Storage
500		253	01001	500	SQ. YD.	Pavement Repair, As Per Plan
5232		254	01000	5232	SQ. YD.	Pavement Planing, Asphalt Concrete
150		614	11100	150	HOURS	Law Enforcement Officer with Patrol Car
10		614	12460	10	EACH	Work Zone Marking Sign
8.92		614	20000	8.92	MILE	Work Zone Lane Line, Class I
530		617	10100	530	CU. YD.	Compacted Aggregate, Type A
493		621	00200	493	EACH	RPM, Installation Only
5.91		644	00101	5.91	MILE	Edge Line, As Per Plan
10.26		644	00201	10.26	MILE	Lane Line, As Per Plan
1610		644	00400	1610	FT.	Channelizing Line
65		644	00500	65	FT.	Stop Line
177		644	00700	177	FT.	Transverse Line
6		644	01300	6	EACH	Lane Arrow
2		644	01410	2	EACH	Word on Pavement, 96 Inch
5024		SPECIAL	690 12150	5024	GALLON	Asphalt Rejuvenating Agent
LUMP		SPECIAL	690 12160	LUMP		Testing
2563		874	10020	2563	CU. YD.	Ultrathin Bonded Asphalt Concrete with Supplement 1059 Warranty
LUMP		614	11000	LUMP		Maintaining Traffic
LUMP		624	10000	LUMP		Mobilization

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GENERAL SUMMARY

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