

**ITEM 614, MAINTAINING TRAFFIC, AS PER PLAN**

THIS ITEM SHALL CONSIST OF MAINTENANCE OF TRAFFIC ON EXISTING ROADWAYS AND RAMPS IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, CURRENT EDITION, LATEST REVISION, THE SPECIFICATIONS AND THE FOLLOWING:

1. A MINIMUM OF THREE ELEVEN FOOT LANES OF TRAFFIC IN EACH DIRECTION ON USR 35 & ONE TEN FOOT LANE ON THE RAMPS SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT, THE COMPLETED PAVEMENT, ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC, AND ITEM 615 ROADS FOR MAINTAINING TRAFFIC. A REDUCTION IN THE NUMBER OF LANES ON USR 35 AND CLOSURE OF RAMP TRAFFIC IS ALLOWED AS LONG AS IT IS IN COMPLIANCE WITH THE NOTES LISTED HEREIN.
2. RAMP TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT FOR THE TIMES SHOWN IN THE SEQUENCE OF CONSTRUCTION AND DISINCENTIVE CONTRACT TABLE WHEN TRAFFIC MAY BE DETOURED AS SHOWN ON THE DETOUR PLANS.
3. CREEKSIDE TRAIL TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT FOR THE TIMES SHOWN IN THE SEQUENCE OF CONSTRUCTION AND DISINCENTIVE CONTRACT TABLE WHEN TRAFFIC MAY BE DETOURED AS SHOWN ON THE DETOUR PLANS.
4. ALL EXISTING LANES, INCLUDING RAMPS, ON US-35 SHALL BE OPEN AND AVAILABLE TO TRAFFIC IN THE ORIGINAL OR FINAL ALIGNMENT BETWEEN OCTOBER 15 AND APRIL 1. SHOULD THE CONTRACTOR FAIL TO MEET THESE REQUIREMENTS, A DISINCENTIVE SHALL BE ASSESSED IN THE AMOUNT OF \$7000 PER CALENDAR DAY (PN 121). WOODMAN DRIVE SHALL REMAIN IN PHASE OVER THE WINTER.
5. NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

CHRISTMAS      FOURTH OF JULY  
 NEW YEAR'S    LABOR DAY  
 MEMORIAL DAY    THANKSGIVING

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY OR EVENT	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00AM FRIDAY
THURSDAY	(THANKSGIVING ONLY)
FRIDAY	6:00AM WEDNESDAY THROUGH 6:00AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00AM MONDAY

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT (PN 127).

6. LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

7. NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK.

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD/RAMP FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.

ITEM	DURATION OF CLOSURE	SIGN DISPLAYED TO PUBLIC
RAMP & ROAD CLOSURES	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	7 CALENDAR DAYS PRIOR TO CLOSURE
	< 12 HOURS	2 BUSINESS DAYS PRIOR TO CLOSURE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION. THIS IS TO BE A SPECIFIC OFFICE WITHIN THE DISTRICT RATHER THAN THE GENERAL SWITCHBOARD NUMBER.

ROAD WILL BE  
 CLOSED MMM-DD  
 FOR XX DAYS  
 INFO: 888-200-9919

W20-H13-60

8. THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN STANDARD 48 X 30 INCH ROAD CLOSED SIGNS, SIGN SUPPORTS, BARRICADES AND LIGHTS, AS DETAILED IN SCD MT-101.60 AT LOCATIONS SHOWN ON DETOUR PLANS DURING PERIODS IN WHICH THE AFFECTED ROADS ARE CLOSED TO TRAFFIC.

9. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DETERMINED BY THE ENGINEER FOR THE MAINTENANCE OF TRAFFIC.

ITEM 614- ASPHALT CONCRETE FOR MAINTAINING TRAFFIC - 100 CY

10. ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, AS PER PLAN UNLESS SEPARATELY ITEMIZED IN THE PLAN.

**PLACEMENT OF ASPHALT CONCRETE**

TWO-WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT THAT ONE-WAY TRAFFIC WILL BE PERMITTED FOR MINIMUM PERIODS OF TIME CONSISTENT WITH THE REQUIREMENTS OF THE SPECIFICATIONS FOR PROTECTION OF COMPLETED ASPHALT CONCRETE COURSES.

**TRENCH FOR WIDENING**

TRENCH EXCAVATION FOR BASE WIDENING SHALL BE ONLY ON ONE SIDE OF THE PAVEMENT AT A TIME. THE OPEN TRENCH SHALL BE ADEQUATELY MAINTAINED AND PROTECTED WITH DRUMS OR BARRICADES AT ALL TIMES. PLACEMENT OF PROPOSED SUBBASE AND BASE MATERIAL SHALL FOLLOW AS CLOSELY AS POSSIBLE BEHIND EXCAVATION OPERATIONS. THE LENGTH OF WIDENING TRENCH WHICH IS OPEN AT ANY ONE TIME SHALL BE HELD TO A MINIMUM AND SHALL AT ALL TIMES BE SUBJECT TO APPROVAL OF THE ENGINEER.

**FLOODLIGHTING**

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

**TEMPORARY ASPHALT WEDGES**

TEMPORARY PAVEMENT WEDGES SHALL BE PROVIDED AT ALL TIMES WHERE TRAFFIC IS REQUIRED TO TRAVEL FROM OR ONTO A PAVEMENT SURFACE OF A DIFFERENT ELEVATION. THE MINIMUM SLOPE OF THE TEMPORARY PAVEMENT WEDGE SHALL BE 6:1 ALONG LONGITUDINAL JOINTS AND 120:1 AT TRANSVERSE JOINTS. SHORT DURATION STOPPAGES OF TRAFFIC TO PLACE TEMPORARY PAVEMENT WEDGES WILL BE PERMITTED WITH THE PRIOR APPROVAL OF THE ENGINEER. THESE WEDGES SHALL BE REMOVED PRIOR TO PLACING THE SPECIFIED PAVEMENT COURSE.

WEDGES ARE ANTICIPATED AT THE TRANSITION FROM EXISTING PAVEMENT TO PROPOSED PAVEMENT AT THE FOLLOWING LOCATIONS:

- PHASE 3A - EX. RAMP A - 7 CY
- PHASE 3A - EX. RAMP B - 7 CY

PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS ASSOCIATED WITH THIS ITEM SHALL BE PAID UNDER ITEM 614, MAINTAINING TRAFFIC, AS PER PLAN.

**COOPERATION BETWEEN CONTRACTORS**

THE CONTRACTOR SHOULD BE AWARE OF THE FOLLOWING PROJECT(S) ADJACENT TO OR WITHIN THE PROJECT LIMITS THAT ARE EITHER UNDER CONTRACT OR PLANNED TO BE UNDER CONTRACT DURING THIS PROJECT. REFER TO CMS 105.08.

MOT-201/202/835-2.04/1.06/0.02, PID 110194 SALE - 2024 DESCRIPTION - RESURFACING OF SR-201/202 AND SR-835.

**PERMITTED LANE CLOSURE SCHEDULE (PLCS)**

LANE CLOSURES ON US-35 SHALL ONLY BE IMPLEMENTED BETWEEN THE TIMES LISTED BELOW:

WEEKDAY (MONDAY - FRIDAY):  
 ONE LANE CLOSURE - 8PM - 6AM  
 2 LANE CLOSURE - 10PM - 5AM

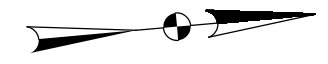
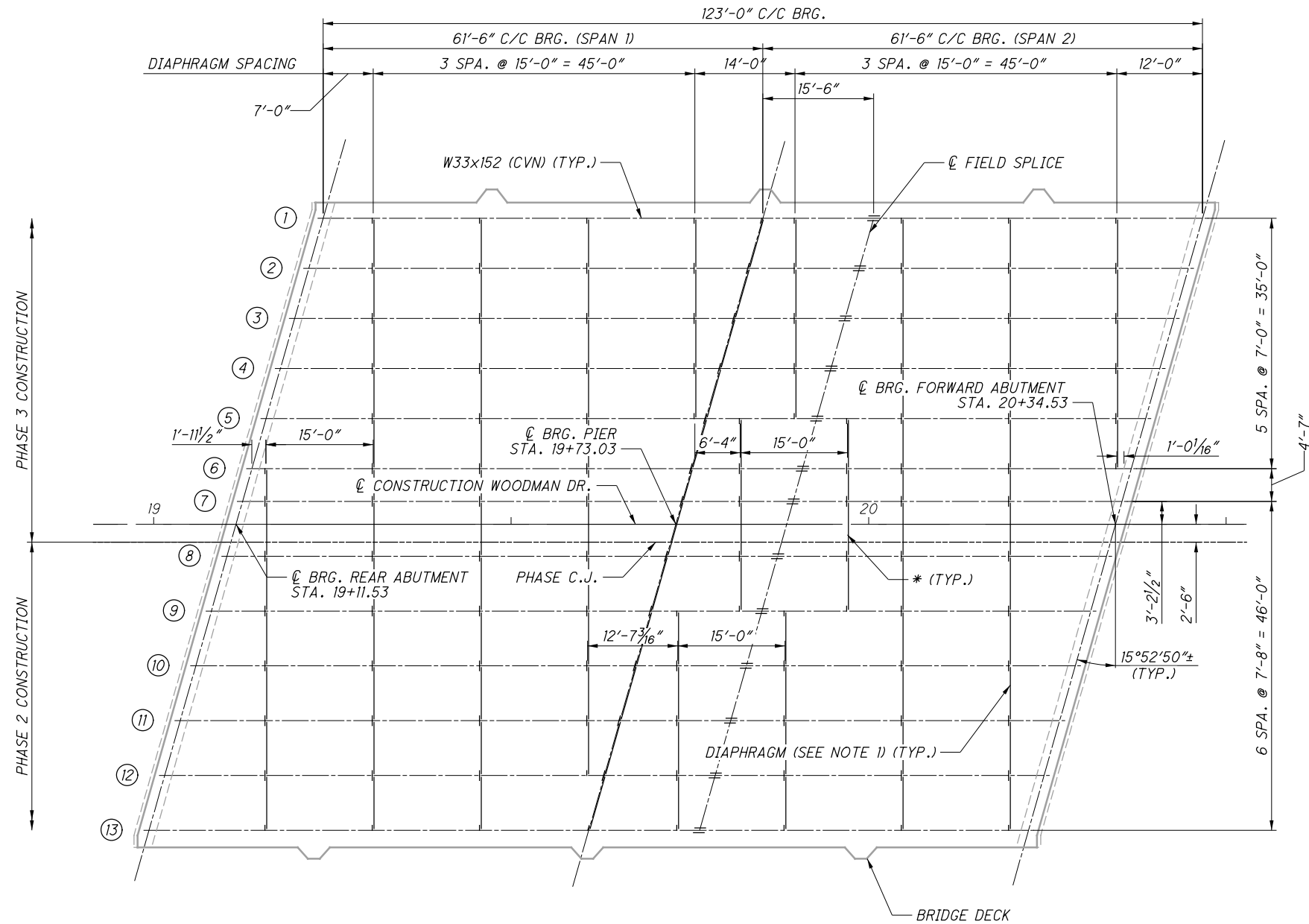
WEEKEND (SATURDAY - SUNDAY):  
 ONE LANE CLOSURE ALLOWED - 7PM - 6AM  
 2 LANE CLOSURE - 9PM - 5AM

NO WORK WITHIN ACTIVE TRAVEL LANES OR WHICH WILL SLOW TRAFFIC SHALL BE PERMITTED AT ANY OTHER TIMES.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT TABLE (PN 127).

**RTA COORDINATION**

THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER AND RTA (TOM NICHOLS (937) 425-8625) TWO WEEKS PRIOR TO PHASE CHANGES. RTA WILL PROVIDE 1 WEEK NOTICE TO RIDERS OF THE UPCOMING CLOSURE AND WILL INSTALL SIGNAGE FOR ANY IMPACTED RTA STOPS.



**FRAMING PLAN**

**NOTES:**

- FOR DIAPHRAGM DETAILS, SEE ODOT STANDARD DRAWING GSD-1-19, INTERMEDIATE DIAPHRAGM DETAILS, SHEET 2 OF 4. THE DIAPHRAGM SIZE IS MC18x42.7. DIAPHRAGM DESIGN IS BASED ON A 33" DEEP BEAM AND A 2.4' OVERHANG.

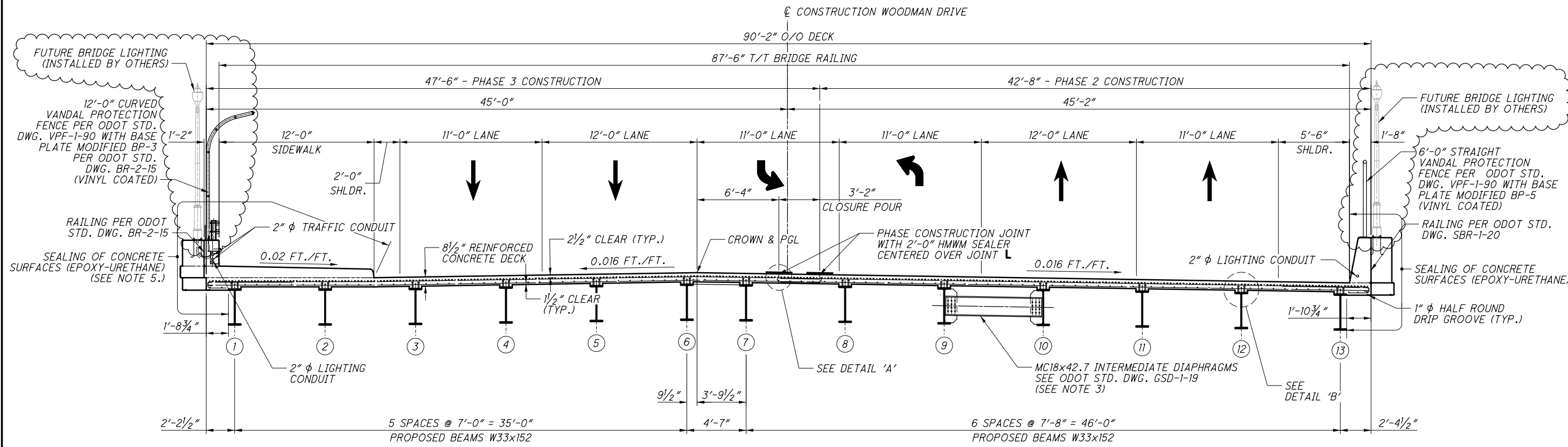
**LEGEND:**

- \* - ALL CROSSFRAMES LOCATED IN THE BAY BETWEEN BEAM 7 AND BEAM 8 TO BE INSTALLED AFTER THE DECK HAS BEEN PLACED IN PHASE 3. FIELD DRILL THE DIAPHRAGM BOLT HOLES IN THE BEAM 7 CONNECTION PLATES AFTER THE DECK HAS BEEN PLACED IN PHASE 3.

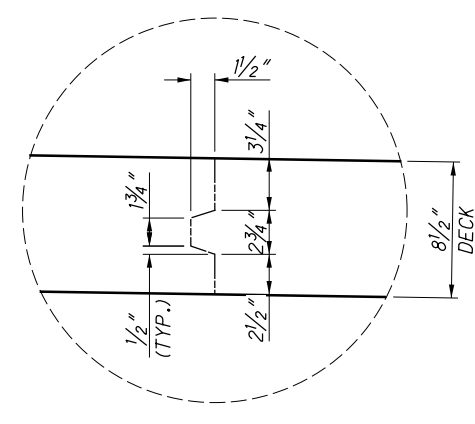
# - BEAM NUMBER DESIGNATION

		DESIGNED	DRAWN	REVIEWED	DATE
		MRV	GLA	DFT	9/1/2021
<b>FRAMING PLAN</b> BRIDGE NO. MOT-00835-00020 WOODMAN DRIVE OVER U.S. 35		CHECKED	REVISED	STRUCTURE FILE NUMBER	
<b>MOT - 35 - 19.80</b> <b>PID No. 90273</b>		TAS	-	5703069	
24 / 47		# 271 / 351			

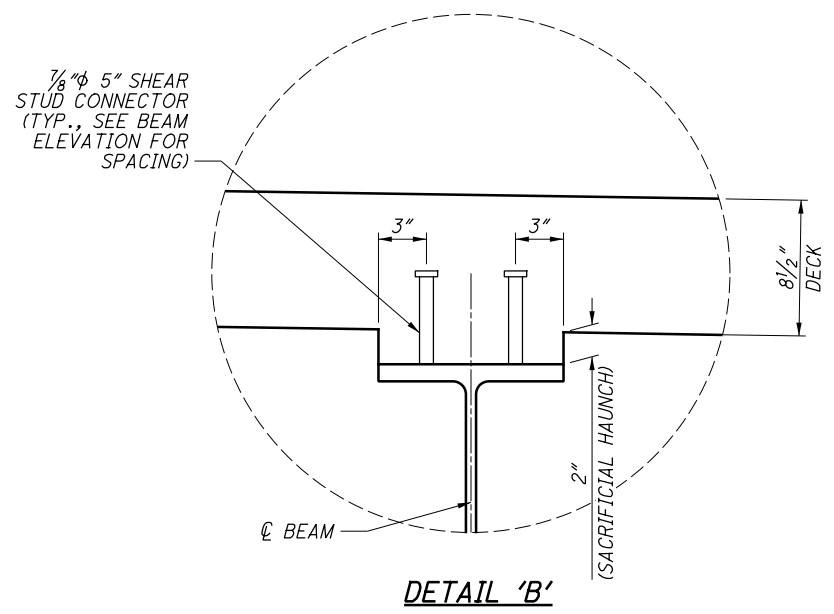
P:\90273\Design\Structures\MOT835\_0002C\Sheets\835\_0002CST001.dgn Sheet 12/8/2022 9:11:09 AM mvogt



**TRANSVERSE SECTION**



**DETAIL 'A'**  
(TYP. AT BOTH CONSTRUCTION JOINTS)



**DETAIL 'B'**  
(SACRIFICIAL HAUNCH)

**LEGEND:**

Ⓝ - BEAM NUMBER DESIGNATION

**NOTES:**

- FOR DECK REINFORCING DETAILS, SEE SHEETS 29/47, 30/47, & 32/47.
- DECK SLAB CONCRETE QUANTITY: THE ESTIMATED QUANTITY OF DECK SLAB CONCRETE IS BASED ON THE CONSTANT DECK SLAB THICKNESS, AS SHOWN, PLUS THE QUANTITY OF CONCRETE THAT FORMS EACH BEAM HAUNCH. THE ESTIMATE ASSUMES A CONSTANT SACRIFICIAL HAUNCH THICKNESS OF 2" AT BEAMS 1 AND 2 AND 2 1/8" INCHES AT BEAMS 3 THRU 13 AND A HAUNCH WIDTH EQUAL TO THE TOP FLANGE WIDTH. DEVIATE FROM THIS HAUNCH THICKNESS AS NECESSARY TO PLACE THE DECK SURFACE AT THE FINISHED GRADE.  
  
THE HAUNCH THICKNESS IS MEASURED AT THE CENTERLINE OF THE BEAM, FROM THE SURFACE OF THE DECK TO THE TOP OF THE TOP FLANGE MINUS THE DECK SLAB THICKNESS.
- ALL DIAPHRAGMS LOCATED BETWEEN BEAMS 7 AND 8 SHALL BE INSTALLED AFTER THE DECK HAS BEEN PLACED IN PHASE 3. DIAPHRAGMS IN THIS BAY TO BE DETAILED TO FIT AT THE COMPLETION OF THE DECK PLACEMENTS ON EACH SIDE OF THE CLOSURE POUR. FIELD DRILL THE DIAPHRAGM BOLT HOLES IN THE BEAM 7 CONNECTION PLATE AFTER THE DECK HAS BEEN PLACED IN PHASE 3.
- FOR SIDEWALK REINFORCING DETAILS, SEE SHEET 38/47.
- INTEGRATE SILICA SAND INTO SIDEWALK SURFACE TO PRODUCE NON-SKID SURFACE PER ODOT C&MS 512.03.
- CONTRACTOR TO INSTALL ACCESS OPENINGS AT LOCATIONS OF THE FUTURE LIGHT POLES PER ODOT STD. DWG. VPF-1-90.

<b>E.L. ROBINSON</b> ENGINEERING 950 Goodale Blvd., Suite 100 - Grandview Heights, OH 43212 www.elrobinsonengineering.com		DATE	9/1/2021
		REVIEWED	DFT
DRAWN	FIB/GLA	DESIGNED	MRV
CHECKED	TAS	TRANSVERSE SECTION	BRIDGE NO. MOT-00835-00020
TAS	TAS		WOODMAN DRIVE OVER U.S. 35
<b>MOT - 35 - 19 - 80</b> PID No. 90273		31 / 47 278 351	