

**ITEM 614, MAINTAINING TRAFFIC (CONTINUED)**

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION.

**SR 49 WILL BE  
CLOSED  
FOR 45 DAYS  
INFO: 419-999-6872**

**W20-H13-60**

NOTE: THE CONTRACTOR IS  
TO SUPPLY THE DATE

THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN STANDARD 48 X 30 INCH ROAD CLOSED SIGNS, SIGN SUPPORTS, BARRICADES AND LIGHTS, AS DETAILED IN SCD MT-101.60 AT THE FOLLOWING LOCATIONS DURING PERIODS IN WHICH THE AFFECTED ROADS ARE CLOSED TO TRAFFIC.

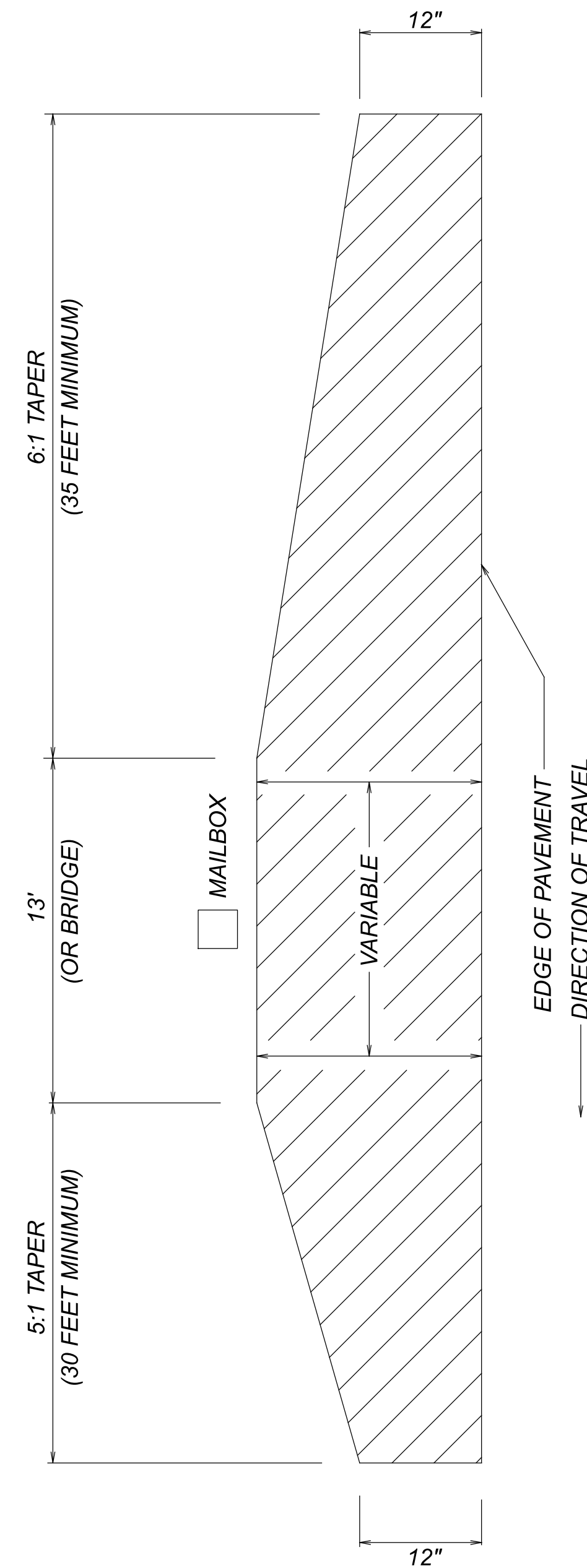
- STAGE 1:  
 BEGIN PROJECT SR 49  
 INTERSECTION OF SR 49 & CR 94  
 INTERSECTION OF SR 49 & TR 96  
 INTERSECTION OF SR 49 & CR 106  
 INTERSECTION OF SR 49 & CR 124  
 INTERSECTION OF SR 49 & SR 111  
 INTERSECTION OF SR 49 & CR 144  
 INTERSECTION OF SR 49 & TR 152  
 INTERSECTION OF SR 49 & TR 162.  
 END OF STAGE 1 SR 49

- STAGE 2:  
 BEGIN SR 49  
 INTERSECTION OF SR 49 & US 24 RAMPS  
 INTERSECTION OF SR 49 & CR 424  
 INTERSECTION OF SR 49 & CR 192  
 INTERSECTION OF SR 49 & CR 204  
 INTERSECTION OF SR 49 & TR 214  
 INTERSECTION OF SR 49 & CR 220  
 INTERSECTION OF SR 49 & CR 230  
 INTERSECTION OF SR 49 & DEFIANCE PAULDING COUNTY LINE RD  
 INTERSECTION OF SR 49 & JERICHO RD  
 END PROJECT

THE DEPARTMENT WILL PROVIDE DETOUR SIGNING AS PER 614.06.A

ACCESS TO ADJACENT PROPERTY WITHIN THE WORK LIMITS SHALL BE MAINTAINED BY THE CONTRACTOR AT ALL TIMES, AS PER 614.02.A.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.



**SINGLE MAILBOX TURNOUT & BRIDGE APPROACHES**

IF THERE IS A DISTANCE OF 100 FEET OR LESS BETWEEN MAILBOXES, APPROACHES SHALL BE PAVED THRU TO THE LAST MAILBOX.

IF THERE IS A DISTANCE OF 50 FEET OR LESS BETWEEN DRIVEWAY AND MAILBOX, APPROACHES SHALL BE PAVED THRU TO THE LAST MAILBOX. THE CONTRACTOR SHALL BE RESPONSIBLE FOR EXCAVATION OF MATERIALS FROM ALL STONE DRIVEWAYS AND MAILBOX APPROACHES TO A DEPTH OF 2 INCHES BELOW EXISTING PAVEMENT. EXCAVATED MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR AT HIS OWN RESPONSIBILITY OUTSIDE THE LIMITS OF THE HIGHWAY RIGHT OF WAY.

IF NEITHER OF THE ABOVE CONDITIONS APPLY AND PER THE DIRECTION OF THE PROJECT ENGINEER, A MAILBOX TURNOUT SHALL BE PROVIDED AS PER THE ADJACENT DETAIL.

FOR MAILBOX TURNOUTS, WHEN UNSTABLE MATERIAL IS ENCOUNTERED, EXCAVATION OF THIS MATERIAL SHALL BE TO A DEPTH OF 6 INCHES BELOW EXISTING PAVEMENT ELEVATION. AN ESTIMATED QUANTITY OF 304 AGGREGATE BASE HAS BEEN SET UP FOR BACKFILL OF THESE AREAS. EXCAVATED MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR AT HIS OWN RESPONSIBILITY OUTSIDE THE LIMITS OF THE HIGHWAY RIGHT OF WAY.

AT BRIDGES WITH NO EXISTING BRIDGE APPROACHES AND PER THE DIRECTION OF THE PROJECT ENGINEER, BRIDGE APPROACHES SHALL BE PROVIDED PER THE ADJACENT DETAIL. THE AREA SHOWING THE LOCATION OF THE BRIDGE WILL HAVE VARIABLE LENGTH AND NO WORK IS NEEDED WITHIN THIS AREA UNLESS NOTED OTHERWISE IN THE PLANS.

FOR BRIDGE APPROACHES, THE BRIDGE APPROACHES SHALL BE EXCAVATED TO A DEPTH OF 9 INCHES BELOW EXISTING ADJACENT PAVEMENT ELEVATIONS. AN ESTIMATED QUANTITY OF 304 AGGREGATE BASE HAS BEEN SET UP FOR BACKFILL TO A DEPTH OF 6 INCHES FOR THESE AREAS, AND ESTIMATED QUANTITIES OF ASPHALT SURFACE COURSE AND TACK COAT HAVE BEEN SET UP FOR PLACEMENT OF TWO ASPHALT CONCRETE SURFACE COURSES, EACH COURSE BEING 1½ INCHES THICK, FOR THESE AREAS. EXCAVATED MATERIAL SHALL BE DISPOSED OF BY THE CONTRACTOR AT HIS OWN RESPONSIBILITY OUTSIDE THE LIMITS OF THE HIGHWAY RIGHT OF WAY.

AN ADDITIONAL QUANTITY OF ASPHALT CONCRETE HAS BEEN SET UP TO BE USED IN THOSE AREAS EXCAVATED FOR DRIVEWAYS, MAILBOX AND BRIDGE APPROACHES.

ALL WORK, MATERIALS, EXCEPT ITEM 304, LABOR AND EQUIPMENT NECESSARY TO COMPLETE THE ABOVE DESCRIBED WORK SHALL BE INCIDENTAL TO THE PLACEMENT OF THE ASPHALT CONCRETE.

**PERSONAL PROTECTIVE EQUIPMENT (PPE)**

THE CONTRACTOR SHALL FOLLOW ALL REQUIREMENTS OF SECTIONS XXIV AND XXXIV OF THE OHIO DEPARTMENT OF TRANSPORTATION SAFETY AND HEALTH STANDARD OPERATING PROCEDURE 220-006(SP) EFFECTIVE: NOVEMBER 1, 2018 (EXCEPT AS AMENDED BELOW) AND ALL SUBSEQUENT UPDATES POSTED AT THE FOLLOWING WEBSITE:

HTTP://WWW.DOT.STATE.OH.US/POLICY/POLICIESANDSOPS/POLICIES/220-006(SP).PDF

AMENDMENTS TO THE REQUIREMENTS OF THIS DOCUMENT ARE:

XXIV. HEAD PROTECTION (HARD HATS)  
 ALL PERSONS WITHIN THE RIGHT-OF-WAY OF ANY HIGHWAY OR ANY OTHER TYPE OF ROADWAY OR CONSTRUCTION SITE WHO ARE EXPOSED TO EITHER TRAFFIC (VEHICLES USING THE HIGHWAY FOR PURPOSES OF TRAVEL) OR CONSTRUCTION EQUIPMENT WITHIN THE WORK AREA, REGARDLESS OF JOB TYPE, SHALL WEAR APPROPRIATE HEAD PROTECTION. ALL HARD HATS MUST MEET OR EXCEED ANSI Z89.1-2009 TYPE 1, CLASS E-G REQUIREMENTS.

XXXIV. SAFETY APPAREL AND VEST (HIGH VISIBILITY)  
 ALL PERSONS WITHIN THE RIGHT-OF-WAY OF ANY HIGHWAY OR ANY OTHER TYPE OF ROADWAY OR CONSTRUCTION SITE WHO ARE EXPOSED TO EITHER TRAFFIC (VEHICLES USING THE HIGHWAY FOR PURPOSES OF TRAVEL) OR CONSTRUCTION EQUIPMENT WITHIN THE WORK AREA, REGARDLESS OF JOB TYPE, SHALL WEAR A HIGH-VISIBILITY SAFETY VEST THAT MEETS THE PERFORMANCE CLASS II OR CLASS III REQUIREMENTS OF THE ANSI/ISEA 107-2015 PUBLICATION ENTITLED "AMERICAN NATIONAL STANDARD FOR HIGH-VISIBILITY SAFETY APPAREL AND ACCESSORIES."

WORKERS MAY WEAR AN ANSI CLASS II OR ANSI CLASS III APPROVED RAIN SUIT, JACKET OR OTHER APPAREL WITHOUT A SAFETY VEST OVER IT.

WORKERS MUST WEAR THE REQUIRED PPE AS DESCRIBED IN THE LATEST EDITION OF THE CSXT PUBLIC PROJECTS MANUAL, AT ALL TIMES WHILE WORKING WITHIN THE CSXT RIGHT OF WAY.

**ROLLER REQUIREMENTS WITHIN CORPORATION LIMITS**

WITHIN THE CORPORATION LIMITS OF THE VILLAGE OF ANTWERP, THE CONTRACTOR SHALL NOT USE THE VIBRATION MODE (VIBRATION SHALL BE TURNED OFF) ON VIBRATORY ROLLERS TO COMPACT THE ASPHALT CONCRETE. 446 DENSITY REQUIREMENTS SHALL APPLY.

**SEEDING AND MULCHING**

THE FOLLOWING QUANTITIES ARE PROVIDED TO PROMOTE GROWTH AND CARE OF PERMANENT SEEDED AREAS.

659, TOPSOIL	1 CU. YD.
659, SEEDING AND MULCHING	30 SQ. YD.
659, COMMERCIAL FERTILIZER	0.1 TON
659, WATER	0.2 M. GAL.

SEEDING AND MULCHING SHALL BE APPLIED TO ALL AREAS OF EXPOSED SOIL BETWEEN THE RIGHT OF WAY LINES, AND WITHIN THE CONSTRUCTION LIMITS FOR AREAS OUTSIDE THE RIGHT OF WAY LINES COVERED BY WORK AGREEMENT OR SLOPE EASEMENT. QUANTITY CALCULATIONS FOR SEEDING AND MULCHING ARE BASED ON THESE LIMITS.

GENERAL NOTES

PAU/DEF-49-6.94/0.00

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DESIGN AGENCY



DESIGNER	MPB
REVIEWER	GLI
PROJECT ID	1-30-24
	113043
SHEET	TOTAL
3	16

**ENVIRONMENTAL COMMITMENTS**

1. The Contractor shall not discharge toxic or hazardous materials such as sealants, paint, solvents, cleaning agents, earthen materials, waste-water, fuels or debris of any kind to a scenic river, its tributaries, or drainage ways. If refueling of immobile equipment is necessary within the floodplain or near any tributary drainage ways, ditches, or stream, the contractor shall provide secondary containment with enough capacity to completely contain and collect all potential liquid wastes in the event of a spill.
2. Any and all construction debris, earthen debris, excess asphalt or concrete, wood debris from clearing, excess fill material, and trash should be disposed of at an approved upland site or land fill above FEMA 100-year flood elevations. Disposal of any such materials within 1000 feet of the Maumee River is prohibited.
3. In accordance with ORC 3750.06, reportable spills must be reported to the local fire department (911), the local emergency coordinator (419-782-1130 (Defiance County) and 419-399-3500 (Paulding County)), and the Ohio Spill Line (1-800-282-9378).
4. The Contractor shall keep all idle equipment, fuels, lubricants, and any storage for/of potentially toxic or hazardous materials out of the FEMA designated special flood hazard area and not within 1000 feet of the Maumee River.
5. The Construction Engineer will contact Northwest Ohio Scenic Rivers Manager Rowan Coburn-Griffis (Rowan.Coburn-Griffis@dnr.ohio.gov) one week prior to work commencing, and within the first week after the work is finished.

**LONGITUDINAL PAVEMENT JOINTS**

IF THE FOLLOWING CONDITIONS APPLY AND WHERE TRAFFIC IS ALLOWED TO CROSS THE EDGE OF THE NEW PAVEMENT LANE, THE CONTRACTOR DOES NOT NEED TO COMPLETE THE LONGITUDINAL JOINT OF THE ADJACENT LANE WITHIN 24 HOURS AS STATED IN CMS 401.17.

1. THE DROP-OFF BETWEEN THE ADJACENT TRAVELED LANE IS LESS THAN OR EQUAL TO ONE AND A HALF INCHES (1 1/2") AND MEETS CONDITION 1 OF STANDARD CONSTRUCTION DRAWING MT-101.90.
2. THE CONTRACTOR PROVIDES AND ERECTS THE APPROPRIATE SIGNS AS PER SCD MT-101.90. ALL COST ASSOCIATED FOR PROVIDING AND PLACING THE SIGNS SHALL BE INCIDENTAL TO THIS ITEM.

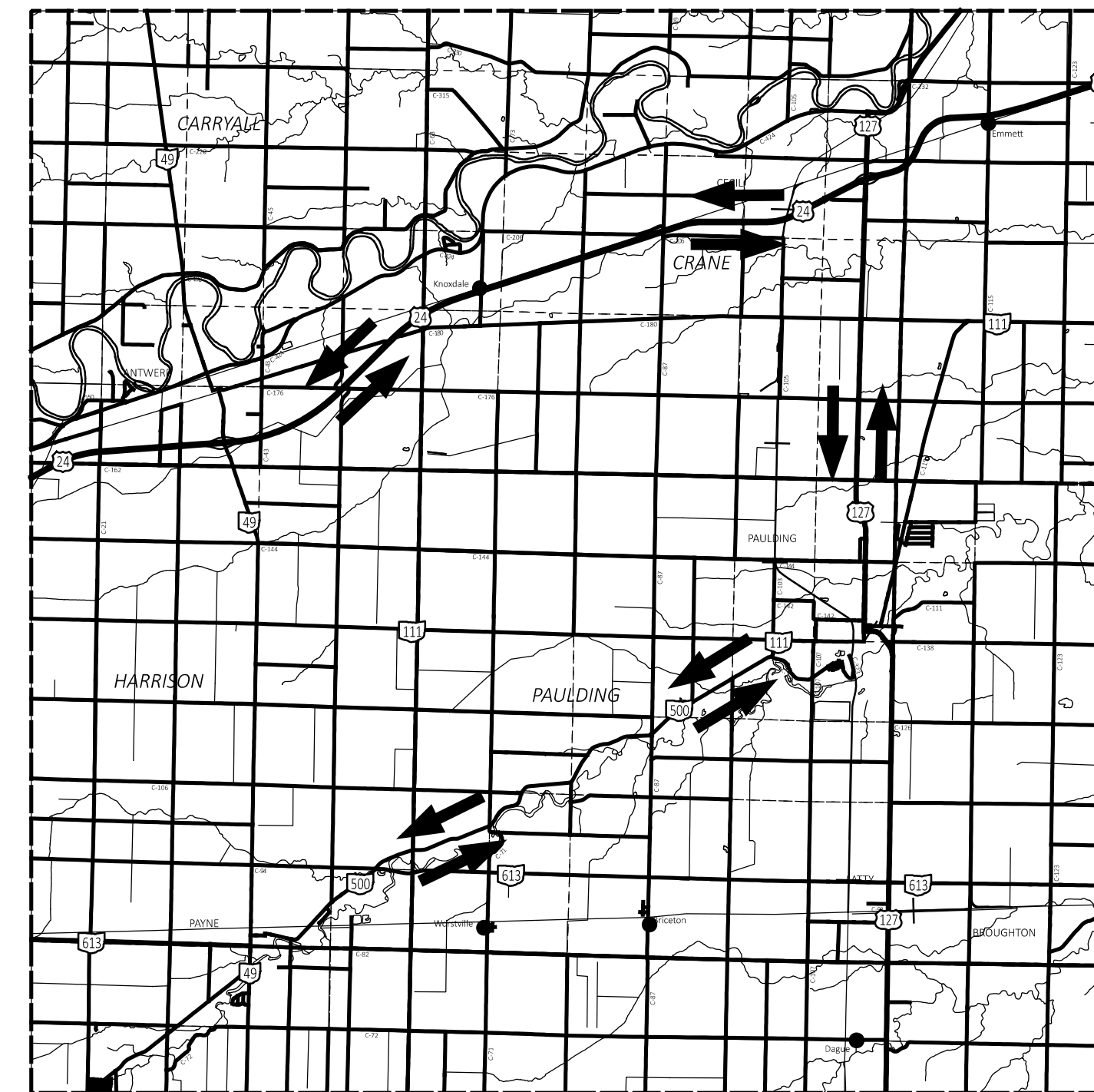
THE ABOVE SHALL BE APPLIED PER THE APPROVAL AND TO THE SATISFACTION OF THE PROJECT ENGINEER.

**WINDOWS CONTRACT**

DESCRIPTION OF CRITICAL WORK	CALENDAR DAYS TO COMPLETE	DISINCENTIVE \$ PER DAY	WORK WINDOW	
			START	END
ALL ECHELON PAVING WORK ON PAUDEF-49	76	PER C&MS 108.07	5/31/2025	8/15/2025
ALL REMAINING PROJECT WORK	120	PER C&MS 108.07	5/1/2025	9/30/2025

**FULL WIDTH PAVING**

ASPHALT CONCRETE SHALL BE PLACED AT FULL WIDTH OF PAVEMENT. INSTEAD OF FULL WIDTH PAVING, THE CONTRACTOR MAY USE HOT JOINT CONSTRUCTION USING MULTIPLE PAVERS. THE PAVING OPERATION SHALL OCCUR UNDER FULL ROADWAY CLOSURE. FULL WIDTH PAVING WILL BE SUSPENDED FROM THE SOUTHERN RAMPS OF THE US 24 INTERCHANGE TO THE INTERSECTION OF CR 192. THIS AREA INCLUDES THE VILLAGE OF ANTWERP.



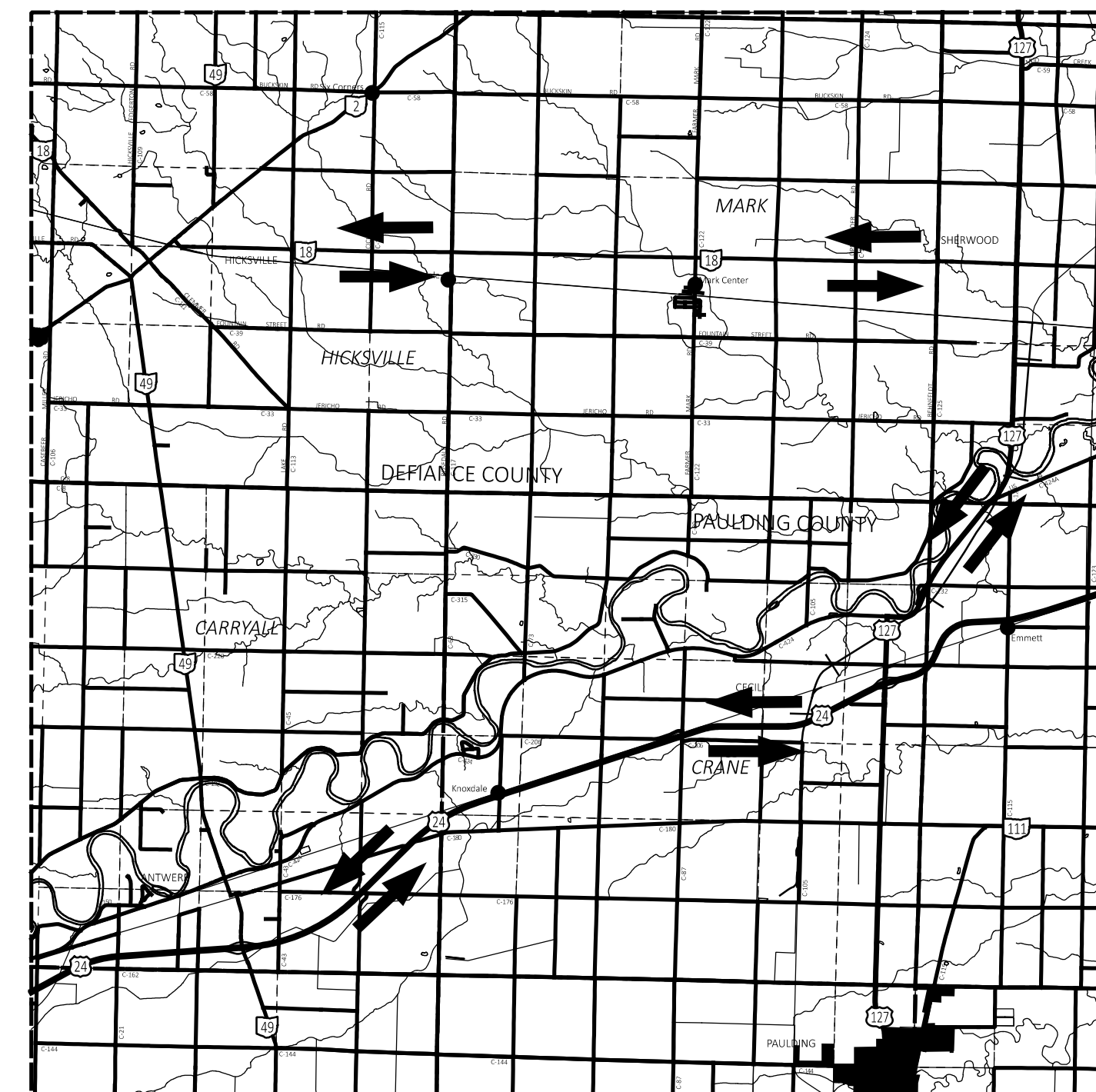
SR 49 DETOUR MAP STAGE 1



SCALE IN MILES



OFFICIAL SIGNED DETOUR FOR SR 49 (PAU MM 6.19 TO PAU MM 12.52)



SR 49 DETOUR MAP STAGE 2



SCALE IN MILES



OFFICIAL SIGNED DETOUR FOR SR 49 (PAU MM 12.39 TO DEF MM 3.06)

DESIGN AGENCY



DESIGNER

MPB

REVIEWER

GLI 1-30-24

PROJECT ID

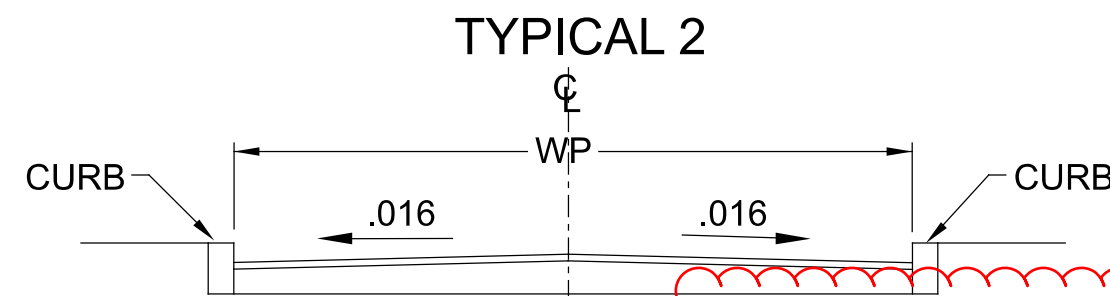
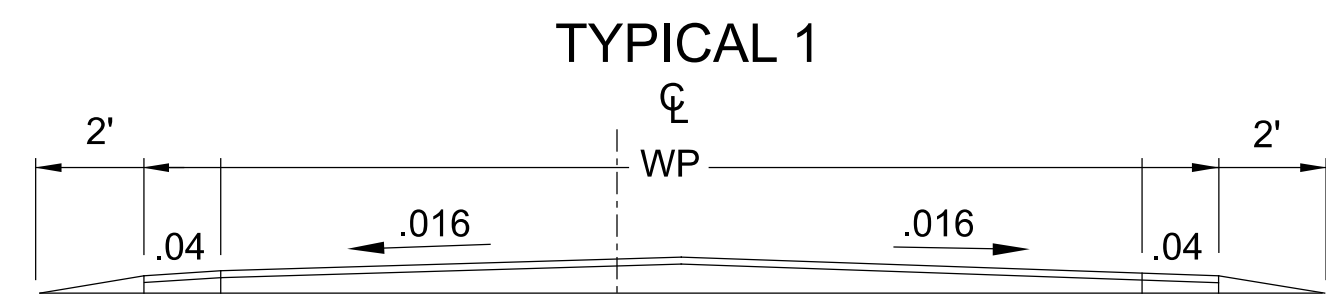
113043

SHEET

4

TOTAL

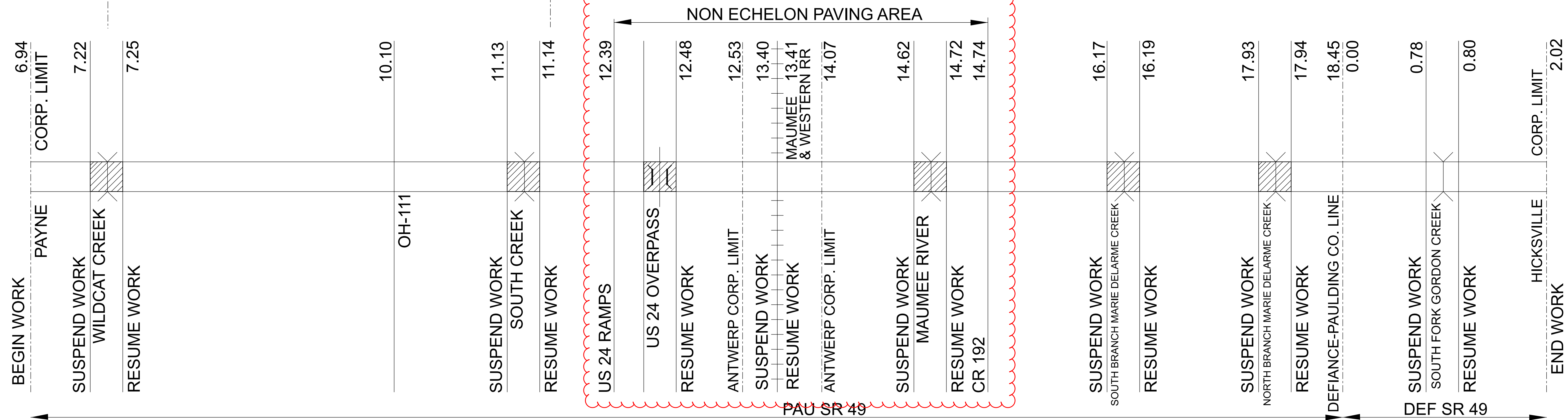
16



NOTE: DRAWINGS NOT TO SCALE

NOTE: ALL TOTALS CARRIED TO GENERAL SUMMARY

NOTE: MAILBOX AND DRIVE QUANTITIES ARE INCLUDED IN THE EXTRA AREAS



PAVEMENT DATA

ROUTE	FROM	TO	DISTANCE		PAVT WIDTH	TYPICAL SECTION	PLAN SPLITS	PAVT AREA	202	407	441	254		304	617	875
			WEARING COURSE REMOVED	NON-TRACKING TRACK COAT					ASPHALT CONCRETE SURFACE COURSE TYPE 1 (446)	PAVEMENT PLANING, ASPHALT CONCRETE	PATCHING PLANED, ASPHALT CONCRETE	AGGREGATE BASE	COMPACTED AGGREGATE AS PER PLAN	LONGITUDINAL JOINT ADHESIVE		
			THICKNESS 1 1/2"						THICKNESS 1 1/2"	THICKNESS 1 1/2"	2% PLANED AREA	QUANTITY FOR DRIVEWAYS	2 INCH AVG. TH.	1 LB. PER 4FT.		
	SLM	SLM	MILES	FEET	FEET		SY	SY	GAL	CY	SY	SY	CY	CY	LB	
SR 49	6.94	7.22	0.28	1478	24.25	1	01/STR/PV	3983		339	166	3983	80	2	37	
SR49	7.22	7.25	0.03	Bridge-No Work		1	01/STR/PV									
SR 49	7.25	8.08	0.83	4382	24.25	1	01/STR/PV	11808		1004	492	11808	236		108	
SR 49	8.08	11.13	3.05	16104	23.50	1	01/STR/PV	42049		3574	1752	42049	841	1	398	
SR 49	11.13	11.14	0.01	Bridge-No Work		1	01/STR/PV									
SR 49	11.14	12.13	0.99	5227	23.50	1	01/STR/PV	13649		1160	569	13649	273	2	129	
SR 49	12.13	12.25	0.12	634	24.58	1	01/STR/PV	1730		147	72	1730	35	3	16	
SR 49	12.25	12.39	0.14	739	40.42	1	01/STR/PV	3320		282	138	3320	66		18	
SR 49	12.39	12.43	0.04	211	40.42	1	01/STR/PV	949		81	40	949	19		5	53
SR 49	12.43	12.48	0.05	Overpass-No Work		1	01/STR/PV									
SR 49	12.48	12.73	0.25	1320	40.42	1	01/STR/PV	5928		504	247	5928	119		33	330
SR 49	12.73	13.21	0.48	2534	24.17	1	01/STR/PV	6806		579	284	6806	136		63	634
SR 49	13.21	13.54	0.33	1742	40.50	2	01/STR/PV	7841		666	327	7841	157		43	436
SR 49	13.54	13.61	0.07	370	35.00	1	01/STR/PV	1437		122	60	1437	29		9	92
SR 49	13.61	14.62	1.01	5333	25.17	1	01/STR/PV	14914		1268	621	14914	298	1	132	1333
SR 49	14.62	14.72	0.10	Bridge-No Work		1	01/STR/PV									
SR 49	14.72	14.74	0.02	106	25.25	1	01/STR/PV	296		25	12	296	6	3	3	26
SR 49	14.74	16.17	1.43	7550	25.25	1	01/STR/PV	21183		1801	888	21183	424		186	
SR 49	16.17	16.19	0.02	Bridge-No Work		1	01/STR/PV									
SR 49	16.19	17.93	1.74	9187	25.25	1	01/STR/PV	25775		2191	1074	25775	516	3	227	
SR 49	17.93	17.94	0.01	Bridge-No Work		1	01/STR/PV									
SR 49	17.94	18.45	0.51	2693	25.25	1	01/STR/PV	7555		642	315	7555	151	1	66	
SR 49	0.00	2.02	2.02	10666	25.61	1	01/STR/PV	30350		2580	1265	30350	607	6	263	
Extra Areas								2096	2096							
Intersections								3190	3190	271	133					
<b>TOTALS</b>									<b>5286</b>	<b>17236</b>	<b>8450</b>	<b>199572</b>	<b>3993</b>	<b>22</b>	<b>1736</b>	<b>2904</b>

ASPHALT CONCRETE

DESIGN AGENCY



DESIGNER  
MPB

REVIEWER  
GLI 1-30-24

PROJECT ID  
113043

SHEET TOTAL  
5 16

SHEET NUM.										PART.			ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
2	3	5	6	8		01/STR/05												
<b>ROADWAY</b>																		
		5,286				5,286		202	23500	5,286	SY	WEARING COURSE REMOVED						
				2,834		2,834		202	30000	2,834	SF	WALK REMOVED						
				1,650		1,650		608	10000	1,650	SF	4" CONCRETE WALK						
				1,184		1,184		608	52000	1,184	SF	CURB RAMP, TYPE A1						
				344		344		608	53020	344	SF	DETECTABLE WARNING						
<b>EROSION CONTROL</b>																		
		1				1		659	00300	1	CY	TOPSOIL						
		30				30		659	10000	30	SY	SEEDING AND MULCHING						
		0.1				0.1		659	20000	0.1	TON	COMMERCIAL FERTILIZER						
		0.2				0.2		659	35000	0.2	MGAL	WATER						
1,000						1,000		832	30000	1,000	EACH	EROSION CONTROL						
<b>PAVEMENT</b>																		
700						700		253	02000	700	CY	PAVEMENT REPAIR						
		199,572				199,572		254	01000	199,572	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 1.5" THICKNESS						
		3,993				3,993		254	01600	3,993	SY	PATCHING PLANED SURFACE						
		22				22		304	20000	22	CY	AGGREGATE BASE						
		17,236				17,236		407	20000	17,236	GAL	NON-TRACKING TACK COAT						
		8,450				8,450		441	10000	8,450	CY	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (446), PG64-22						
		1,736				1,736		617	10101	1,736	CY	COMPACTED AGGREGATE, AS PER PLAN	2					
			13.53			13.53		618	43000	13.53	MILE	RUMBLE STRIPES, CENTER LINE (ASPHALT CONCRETE)						
		2,904				2,904		875	10000	2,904	LB	LONGITUDINAL JOINT ADHESIVE						
<b>TRAFFIC CONTROL</b>																		
			880			880		621	00100	880	EACH	RPM						
			880			880		621	54000	880	EACH	RAISED PAVEMENT MARKER REMOVED						
			26.62			26.62		642	00104	26.62	MILE	EDGE LINE, 6", TYPE 1						
			13.31			13.31		642	00300	13.31	MILE	CENTER LINE, TYPE 1						
			178			178		644	00500	178	FT	STOP LINE						
			393			393		644	00620	393	FT	CROSSWALK LINE, 12"						
			575			575		644	00630	575	FT	CROSSWALK LINE, 24"						
			2			2		644	01000	2	EACH	RAILROAD SYMBOL MARKING						
			750			750		644	01200	750	FT	PARKING LOT STALL MARKING						
			5			5		644	01600	5	EACH	HANDICAP SYMBOL MARKING						
<b>MAINTENANCE OF TRAFFIC</b>																		
70						70		614	12460	70	EACH	WORK ZONE MARKING SIGN						
13.31						13.31		614	21000	13.31	MILE	WORK ZONE CENTER LINE, CLASS I						
26.62						26.62		614	21400	26.62	MILE	WORK ZONE CENTER LINE, CLASS II						
<b>INCIDENTALS</b>																		
						LS		614	11000	LS		MAINTAINING TRAFFIC						
						LS		624	10000	LS		MOBILIZATION						

