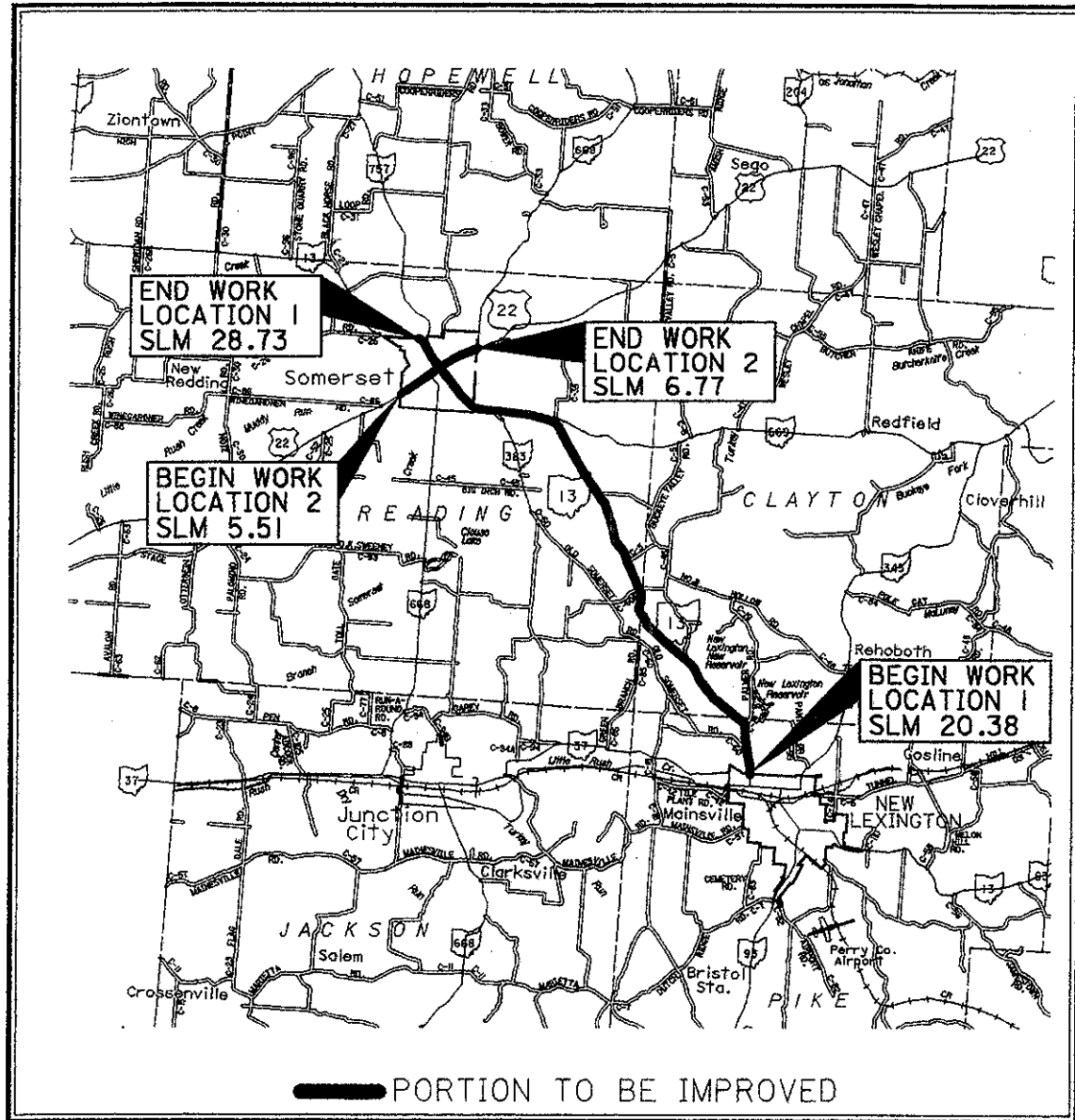


OHIO DEPARTMENT OF TRANSPORTATION
 PER-13-20.38 / PER-22-5.51
 READING/CLAYTON/PIKE TOWNSHIPS
 PERRY COUNTY

LOCATION MAP

PROJECT DESCRIPTION:
 2-LANE ASPHALT CONCRETE RESURFACING,
 INSTALL STORM SEWER, COMBINATION CURB
 AND GUTTER AND RELATED WORK



LOCATION	COUNTY	ROUTE	SECTIONS	PROJECT TERMINI		NET LENGTH MILES	CITY	VILLAGE
				BEGIN	END			
1	PER	SR 13	(20.38-28.43)	20.38	28.73	*8.31		SOMERSET
2	PER	US 22	(5.51-6.40)	5.51	6.77	1.26		SOMERSET

* STA. EQUATION 27.73BK = 27.77AH DEDUCT 0.04 MILE



INDEX OF SHEETS:

TITLE SHEET ----- 1
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 BRIDGE DECK TREATMENT ----- 17
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 PLAN AND PROFILE US 22 ----- 19,20
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 EDGE/CENTER LINE SUB-SUMMARY ----- 22
 PAVEMENT MARKING SUB-SUMMARY ----- 23,24
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1997 SPECIFICATIONS

THE STANDARD 1997 SPECIFICATIONS OF THE STATE OF OHIO DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND THE PROPOSAL SHALL GOVERN THESE IMPROVEMENTS.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THESE IMPROVEMENTS WILL NOT REQUIRE THE CLOSING OF THE HIGHWAY FOR LOCATION 1 AND PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS INDICATED IN THE PROPOSAL. HOWEVER, A CLOSURE SHALL BE REQUIRED FOR LOCATION 2. TRAFFIC SHALL BE DETOURED AS SHOWN ON SHEET 4/27.

APPROVED *Christopher T. Ruff*
 DATE 2/25/00 DISTRICT DEPUTY DIRECTOR

APPROVED *Gordon Proctor*
 DATE 3-3-00 DIRECTOR, DEPARTMENT OF TRANSPORTATION

UNDERGROUND UTILITIES
 TWO WORKING DAYS
 BEFORE YOU DIG
 CALL 1-800-362-2764 (TOLL FREE)
 OHIO UTILITIES PROTECTION SERVICE
 NON-MEMBERS MUST BE CALLED DIRECTLY

PER - 13-20.38/22-5.51
 000321
 DIST 5

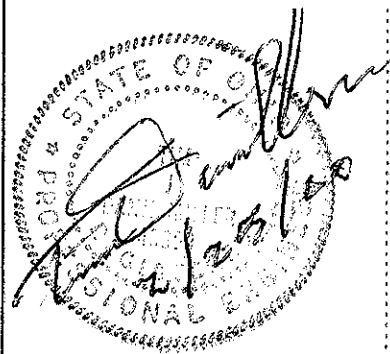
PID# 19948
 06-07-00

DESIGN DESIGNATION	LOCATION	
	1	2
Current ADT (1999)	6600	4300
Design Year ADT (2011)	8100	5300
Design Hourly Volume (2011)	800	500
Directional Distribution	50%	50%
Trucks (24 Hour B&C)	400	400
Design Speed	55 MPH	55 MPH
Legal Speed	55 MPH	55 MPH

STANDARD DRAWINGS		STANDARD DRAWINGS		SUPPLEMENTAL SPECIFICATIONS		SUPPLEMENTAL SPECIFICATIONS	
BP-3.1	2-21-92	TC-65.11	2-1-90	806	9/9/97	908	1/6/99
BP-4.1	2-21-92	TC-65.12	2-1-90	830	10/21/98	870	8/10/99
BP-5.1	10-28-94			842	1/6/99	857	10/21/98
MT-97.10	4-29-88			877	4/13/97		
MT-97.11	10-4-89			899	10/21/98		
				905	4/1/98		
MT-99.20	4-29-88			906	5/5/98		
TC-65.10	2-1-90			907	10/21/98		

PLAN PREPARED BY:
 District
 D5
 Production

PO130001.MTS 2-23-00



FEDERAL PROJECT NO.
 TE 21-G000(304)

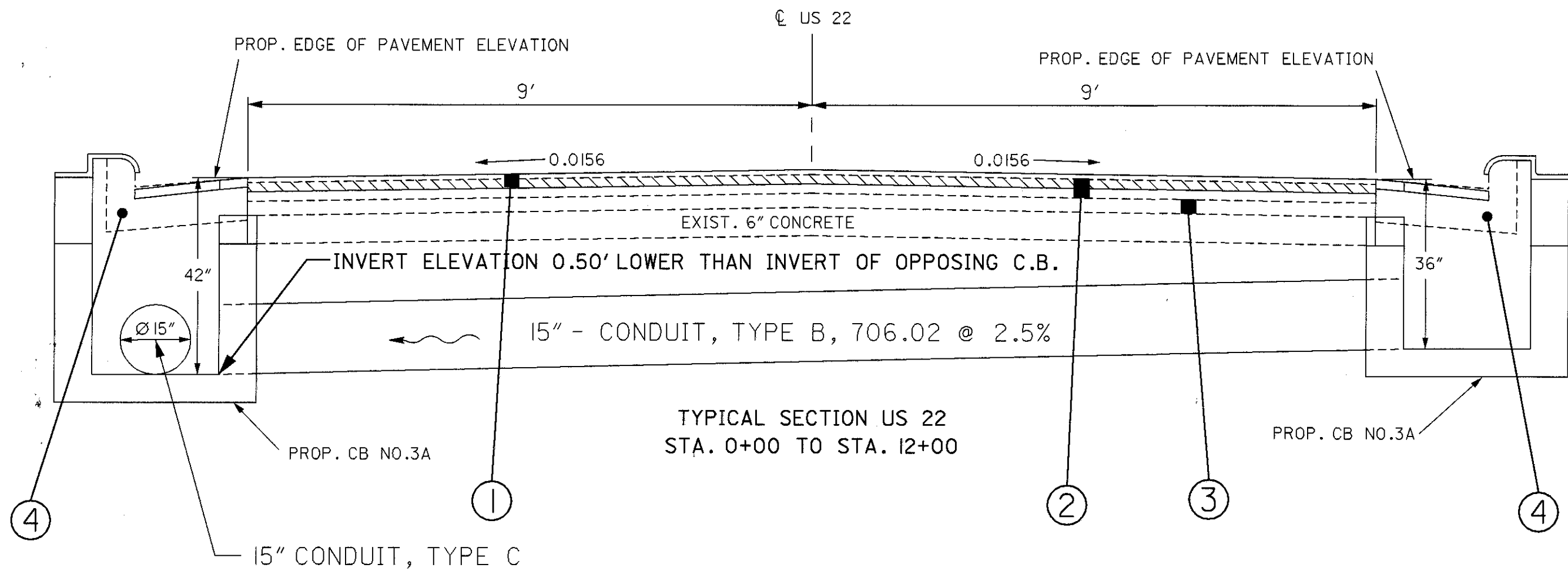
PID NO.
 19948

CONSTRUCTION PROJECT NO.

TITLE SHEET

PER-13-20.38
 PER-22-5.51

27



- ① PROP. 3" ASPHALT CONCRETE OVERLAY:
1.25" SURFACE COURSE & 1.75" INTERMEDIATE COURSE
 - ② ±4" EXISTING ASPHALT CONCRETE
 - ③ 3" EXISTING BRICK
 - ④ PROP. ITEM 609 COMBINATION CURB AND GUTTER, TYPE 2
- ▨ 2" PAVEMENT PLANING, BITUMINOUS, AS PER PLAN

TYPICAL SECTION

PER-13-20.38
PER-22-5.51

P0130001.TYP 2-23-00

UTILITIES

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT MAY AFFECT UNDERGROUND UTILITIES THAT EXIST UNDER OR ADJACENT TO THE WORK AREA. BELOW IS A LIST OF UTILITIES LOCATED WITHIN THE WORK AREA AND IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO CONTACT OWNERS AND VERIFY LOCATIONS.

AMERICAN ELECTRIC POWER
SOUTHERN OHIO REGION
95 EAST MAIN ST.
P.O. BOX 468
CHILICOTHE, OH. 45601-0468
ATTN: LEE GRIFFITH
PHONE: (740)774-7135

COLUMBIA GAS OF OHIO
216 HIGHLAND AVE.
P.O. BOX 250
CAMBRIDGE, OH. 43725
ATTN: JIM DEITRICT
PHONE: (740)432-8226

TEPPCO
P.O. BOX 813
LEBANON, OH. 45036
ATTN: KEN NEFF
PHONE: (513)932-1816

AMERITECH OF OHIO
160 NORTH 6TH ST.
ZANESVILLE, OH. 43701
ATTN: SANDY RANDOLPH
PHONE: (740)454-3455

FORAKER GAS COMPANY
430 SOUTH STATE ST.
NEW LEXINGTON, OH. 43764-0537
ATTN: DAN FORAKER
PHONE: (740)342-2852

ADELPHIA CABLE COMMUNICATIONS
5 SOUTH MAIN ST.
THORNVILLE, OH. 43076
ATTN: TOM DALRIMPLE
PHONE: (740)246-4245

NEW LEXINGTON CITY WATER
125 SOUTH MAIN ST.
NEW LEXINGTON, OH. 43783
ATTN: PAT HENERY
PHONE: (740)743-2963

NOTIFICATION OF ROAD CLOSURE OR RESTRICTION

IN ORDER FOR ODOT TO PROPERLY PERMIT OVERSIZE LOADS, PREPARE PROPER SIGNING WHEN REQUIRED AND FURTHER TO NOTIFY THE GENERAL MOTORING PUBLIC, THE CONTRACTOR SHALL NOTIFY (IN WRITING) THE DISTRICT 5 HIGHWAY MANAGEMENT ADMINISTRATOR WITH COPIES FOR THE DISTRICT 5 ROADWAY SERVICES MANAGER AND PROJECT ENGINEER NOT LESS THAN 21 DAYS BEFORE SUCH CLOSURE OR LANE RESTRICTIONS.

SEND NOTIFICATION TO:

DISTRICT 5 HIGHWAY MANAGEMENT ADMINISTRATOR
P.O. BOX 306
JACKSONTOWN, OH. 43030
PHONE: (740) 323-4400 EXT. 5241

FEATHERING

FEATHERING OF THE ASPHALT CONCRETE WHERE REQUIRED SHALL BE ACCORDING TO DRAWING BP-3.1, 2-21-92.

PAVEMENT MARKING

STOP LINES, CROSSWALK LINES, CHANNELIZING LINES, TURN ARROWS, ETC., SHOWN ON THE PLAN ARE TAKEN FROM EXISTING MARKINGS. IT SHALL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PLACE NEW PAVEMENT MARKINGS AS NEAR AS POSSIBLE TO THE EXISTING LOCATIONS UNLESS OTHERWISE DESIGNATED BY THE ENGINEER.

ITEM 617, COMPACTED AGGREGATE, TYPE A, AS PER PLAN

ALL AGGREGATE SHALL BE 100% CRUSHED LIMESTONE. ALL QUALITY REQUIREMENTS EXCEPT SHALE SHALL BE WAIVED. OTHER GRADATION REQUIREMENTS SHALL BE AS SPECIFIED EXCEPT THE PLASTICITY INDEX SHALL BE WAIVED. IF SO DESIRED, THE CONTRACTOR MAY USE RECYCLED ASPHALT CONCRETE PAVEMENT (RACP MEETING REQUIREMENTS OF 617.03) IN LIEU OF CRUSHED LIMESTONE.

PROFILE AND ALIGNMENT

THE PROPOSED PAVEMENT RESURFACING SHALL FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT.

TACK COAT

THE RATE OF APPLICATION OF THE 407 TACK COAT SHALL BE SUBJECT TO ADJUSTMENT AS DIRECTED BY THE ENGINEER. PLAN QUANTITIES INDICATE AN AVERAGE APPLICATION RATE OF 0.075 GALLONS PER SQUARE YARD FOR ESTIMATING PURPOSES ONLY.

TACK COAT FOR INTERMEDIATE COURSE

THE RATE OF APPLICATION OF THE 407 TACK COAT FOR INTERMEDIATE COURSE SHALL BE SUBJECT TO ADJUSTMENT AS DIRECTED BY THE ENGINEER. PLAN QUANTITIES INDICATE AN AVERAGE APPLICATION RATE OF 0.05 GALLONS PER SQUARE YARD FOR ESTIMATING PURPOSES ONLY.

GENERAL NOTES

PER-13-20.38
PER-22-5.51

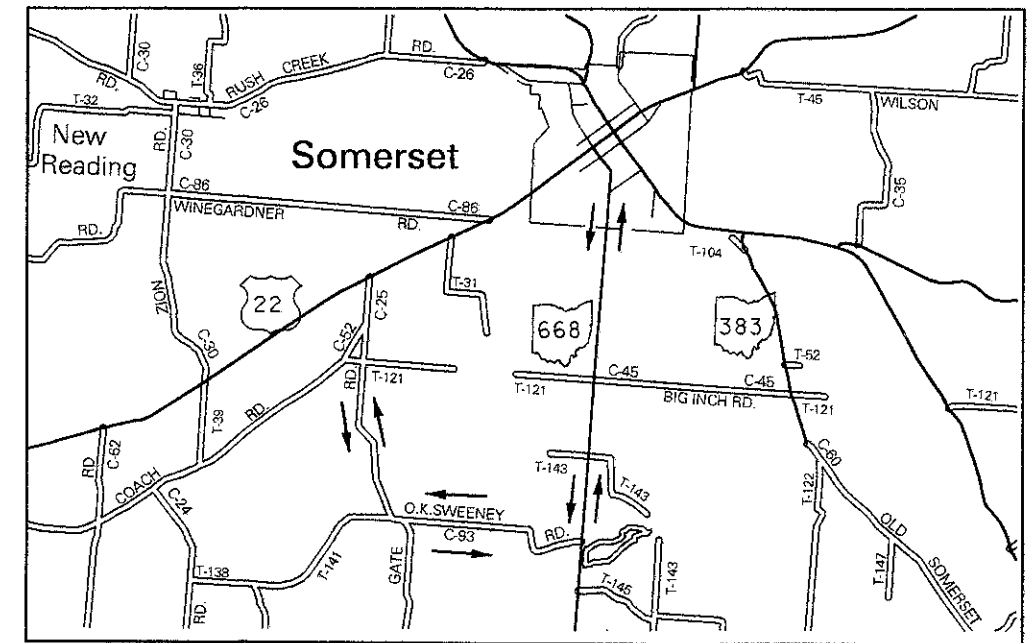
ITEM 614 - MAINTAINING TRAFFIC

A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES, EXCEPT IN LOCATION 2 BETWEEN SLM 5.51 AND SLM 6.00 WHERE A CLOSURE SHALL BE PERMITTED FOR STORM SEWER INSTALLATION. ROADWAY CLOSURE SHALL NOT EXCEED 10 CONSECUTIVE CALENDAR DAYS. WHEN THROUGH TRAFFIC MAY BE DETOURED AS SHOWN ON THIS SHEET. DRIVEWAY ACCESS SHALL BE MAINTAINED AT ALL TIMES THROUGHOUT CLOSURE AREA. LIQUIDATED DAMAGES SHALL BE ASSESSED (IN ACCORDANCE WITH 108.07) IN THE AMOUNT OF \$2000.00 PER DAY FOR EACH CALENDAR DAY THE ROADWAY REMAINS CLOSED TO TRAFFIC BEYOND THE SPECIFIED LIMIT.

THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN STANDARD 48" X 30" "ROAD CLOSED" SIGNS, SIGN SUPPORTS, BARRICADES, GATES, AND LIGHTS, AS DETAILED IN STANDARD CONSTRUCTION DRAWING MT-101.60.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ITEM 614 MAINTAINING TRAFFIC LUMP SUM



LOCAL DETOUR

DESIGNATED LOCAL DETOUR ROUTE

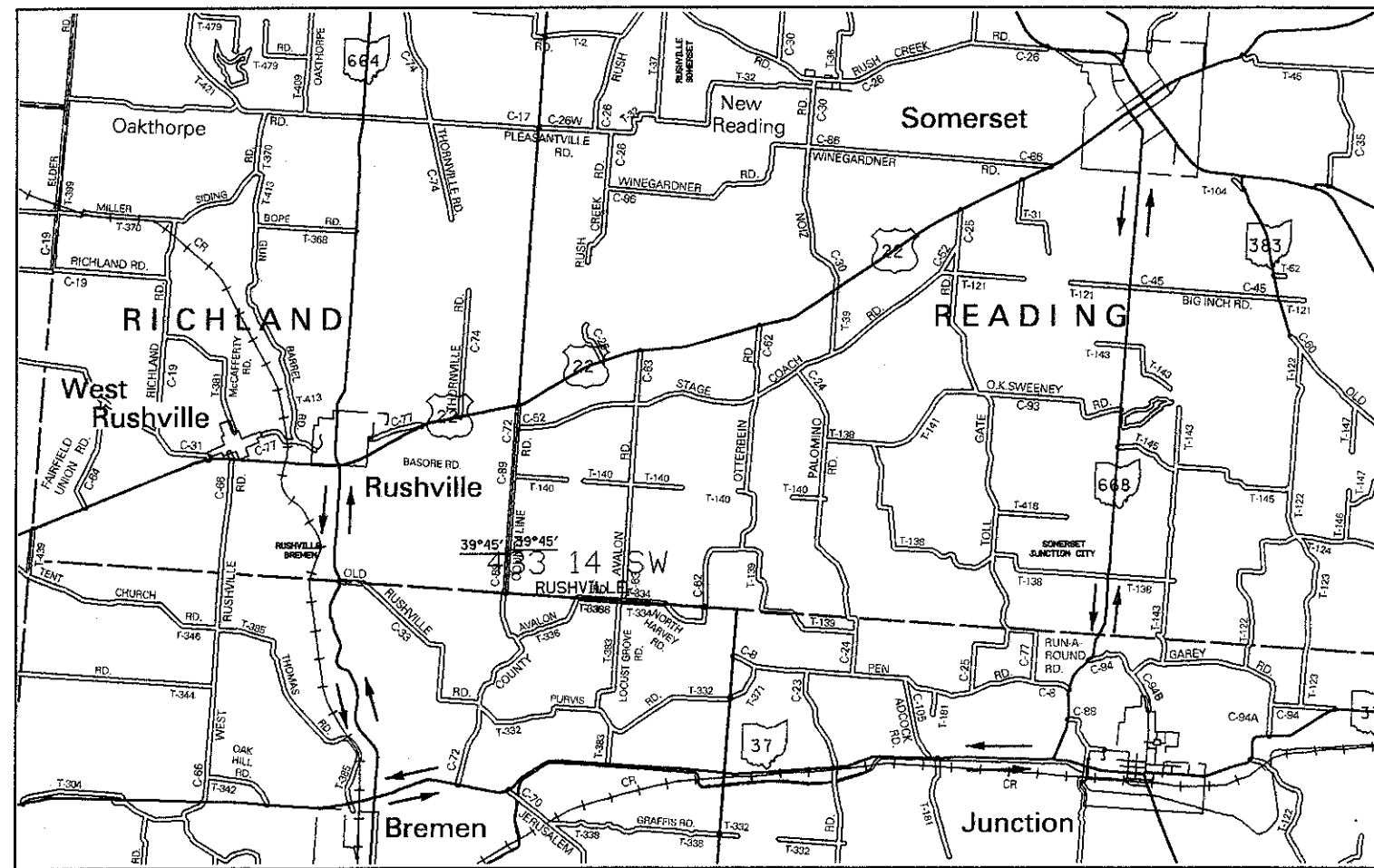
IN ADDITION TO THE OFFICIAL, SIGNED DETOUR ROUTE, A LOCAL ROUTE HAS BEEN DETERMINED TO BE THE SECONDARY, UNSIGNED DETOUR ROUTE OR "DESIGNATED LOCAL DETOUR ROUTE." THIS ROUTE IS SHOWN ON THIS SHEET.

CONTRACTOR TO COORDINATE WITH PERRY COUNTY ENGINEER (740) 342-2191, REGARDING THE QUANTITIES FOR THE DESIGNATED LOCAL DETOUR. THE CONTRACT CALLS FOR THESE PAY QUANTITIES FOR MAINTAINING THE LOCAL DETOUR.

ITEM 304 AGGREGATE BASE, AS PER PLAN (MATERIAL ONLY)
LOCATION 2 - 50 CU.YD.

448 - ASPHALT CONCRETE, SURFACE COURSE, TYPE 1, PG 64-22,
AS PER PLAN (MATERIAL ONLY) LOCATION 2 - 50 CU.YD.

THESE MATERIALS WILL BE PICKED UP BY THE PERRY COUNTY HIGHWAY DEPARTMENT ON AN "AS NEEDED" BASIS AT CHESTERHILL STONE AND SHELLY MATERIALS IN LANCASTER OR ANOTHER FACILITY THAT MAY BE LOCATED IN PERRY COUNTY, OHIO. THE CONTRACTOR IS TO COORDINATE THIS WITH THE PERRY COUNTY ENGINEER WITH THE APPROVAL OF THE ENGINEER. THE ASPHALT PRODUCER OR OTHER SOURCE FACILITY WILL DIRECT BILL THE CONTRACTOR. ODOT CONSTRUCTION PERSONNEL WILL VERIFY THE NEED AND THE ACTUAL QUANTITY USED.



STATE ROUTE DETOUR

RESIDENCE AND COMMERCIAL DRIVES

AN ESTIMATED QUANTITY OF ITEM 448 ASPHALT CONCRETE HAS BEEN INCLUDED IN THE PLAN TO BE USED AS DIRECTED BY THE ENGINEER TO PAVE APPROACH AREAS TO EXISTING DRIVEWAYS. PAVING SHALL EXTEND 10 FEET INTO THE DRIVEWAY, MEASURED FROM THE EDGE OF THE PAVEMENT, OR PAVED BERM. FIELD DRIVES AND OIL WELL DRIVES WILL NOT BE PAVED.

ANY GRADING OF EXISTING DRIVES, TACK OR PRIME COAT, ALL MATERIAL, LABOR, EQUIPMENT, TOOLS AND INCIDENTALS NECESSARY TO COMPLETE WORK ON DRIVES SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE APPROPRIATE ASPHALT ITEM SHOWN BELOW. BECAUSE OF PAVEMENT PLANING, AN ESTIMATED QUANTITY OF SURFACE COURSE ONLY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR FEATHERING AT DRIVES.

PAVING OF THE MAINLINE SHALL BE COMPLETED BEFORE THE WORK DESCRIBED ABOVE SHALL BEGIN ON DRIVES.

THE QUANTITIES SHOWN IN THE BELOW HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR THE PURPOSE DESCRIBED ABOVE.

ITEM 448 ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG 64-22 (DRIVEWAYS) (2.0" AVG. THICKNESS) LOCATION 1 - 177 CU.YD. LOCATION 2 - 33 CU.YD.

MAIL BOX TURN OUTS

A QUANTITY OF ASPHALT CONCRETE HAS BEEN PROVIDED IN THE PLAN TO COVER MAIL BOX TURN OUTS. TURN OUTS SHALL BE PAVED AS SHOWN IN THE DETAIL IN DRAWING BP-4.I, 2-21-92.

ANY EXTRA GRADING OF THE SHOULDERS, PRIME OR TACK COAT, MATERIALS, LABOR, EQUIPMENT TOOLS AND INCIDENTALS NECESSARY TO COMPLETE MAIL BOX TURN OUTS SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 448 ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, PG 64-22 AND ITEM 448 ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG 64-22 WITH SUPPLEMENT 1059 WARRANTY.

ITEM 448 ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, PG 64-22 (1.0" AVG. THICKNESS) LOCATION 1 - 62 CU.YD.

ITEM 448 ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG 64-22 WITH SUPPLEMENT 1059 WARRANTY (1.0" AVG. THICKNESS) LOCATION 1 - 62 CU.YD.

QUANTITIES CARRIED TO GENERAL SUMMARY

WEARING COURSE REMOVED, AS PER PLAN

THIS ITEM SHALL BE USED TO REMOVE ALL GRADER PATCHES THROUGHOUT THE PROJECT WHERE PLANING IS NOT DESIGNATED. IT ALSO SHALL BE USED AT LOCATIONS SPECIFIED BY THE ENGINEER TO RESTORE THE CROWN TO THE ROADWAY. AN ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR THE PURPOSES DESCRIBED ABOVE.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY LOCATION 1 - 2000 SQ.YD.

ITEM 253 - PAVEMENT REPAIR, AS PER PLAN

AN ESTIMATED QUANTITY FOR PAVEMENT REPAIR HAS BEEN INCLUDED IN THE PLAN TO BE USED AS DIRECTED BY THE ENGINEER WHERE THE EXISTING PAVEMENT HAS DETERIORATED. FINAL LOCATIONS OF PAVEMENT REPAIR SHALL BE DETERMINED BY THE ENGINEER AT THE TIME OF CONSTRUCTION. DEPTH OF EXCAVATION SHALL BE APPROXIMATELY 6". THE FACE OF THE REPAIR SHALL BE COATED WITH ITEM 407 TACK COAT. REPLACEMENT MATERIAL WILL BE 6" OF ITEM 301 BITUMINOUS AGGREGATE BASE, PG 64-22 (PLACED AND COMPACTED AS DIRECTED BY THE ENGINEER). ALL EXCAVATION NEEDED TO ACHIEVE THE PROPER SLOPES FOR DRAINAGE ON BERMS AND ALL MATERIALS, LABOR, EQUIPMENT, TOOLS AND INCIDENTALS NEEDED TO COMPLETE THE WORK DESCRIBED ABOVE SHALL BE PAID FOR UNDER ITEM 253 PAVEMENT REPAIR, AS PER PLAN. A CONTINGENCY QUANTITY OF ITEM 605 AGGREGATE DRAIN IS INCLUDED TO BE USED AS DIRECTED BY THE ENGINEER IN AREAS NEEDING PAVEMENT REPAIR. ALL MATERIALS, LABOR, TOOLS, EQUIPMENT AND INCIDENTALS NEEDED TO COMPLETE DRAINS SHALL BE PAID FOR AT THE UNIT PRICE BID FOR ITEM 605 AGGREGATE DRAINS.

THE FOLLOWING CONTINGENCY QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR THE ABOVE DESCRIBED PURPOSE.

ITEM 253 PAVEMENT REPAIR, AS PER PLAN LOCATION 1 - 4600 SQ.YD. LOCATION 2 - 50 SQ.YD.

ITEM 605 AGGREGATE DRAINS LOCATION 1 - 200 LIN.FT.

ITEM 254 PAVEMENT PLANING, BITUMINOUS, AS PER PLAN

PLANING SHALL BE PERFORMED SUCH THAT THE PAVEMENT SURFACE IS SLOPED AT A RATE OF 0.0156 FROM CENTERLINE TO RESTORE THE CROWN TO THE ROADWAY. DEPTH OF PLANING SHALL BE ±2.0". THE GRINDINGS SHALL BECOME THE PROPERTY OF THE O.D.O.T. PERRY COUNTY GARAGE. THE PERRY COUNTY MANAGER SHALL BE NOTIFIED AT LEAST 48 HOURS BEFORE PLANING SO THAT HE MAY BE ABLE TO SUPPLY TRUCKS FOR HAULING THE GRINDINGS. AFTER PLANING, THE ROADWAY SHALL BE RE-SURFACED WITH 1.75" ITEM 448 ASPHALT CONCRETE WITH GILSONITE, INTERMEDIATE COURSE, TYPE 2 AND 1.25" ITEM 448 ASPHALT CONCRETE WITH GILSONITE SURFACE COURSE, TYPE 1.

THE ENGINEER MAY ADJUST PLANING DEPTHS AT ANY TIME TO MEET EXISTING CONDITIONS AT THE TIME OF CONSTRUCTION.

ITEM 254 PAVEMENT PLANING BITUMINOUS, AS PER PLAN QUANTITIES SHOWN ON SHEET 14.

ITEM 254 PATCHING PLANED SURFACE

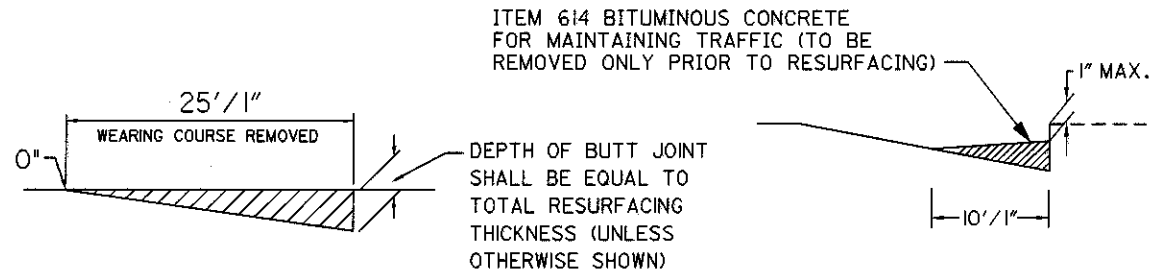
A QUANTITY OF SURFACE PATCHING HAS BEEN INCLUDED IN THE PLAN TO REPLACE UNSOUND PAVEMENT RESULTING FROM PLANING. THE ENGINEER WILL DETERMINE WHERE THIS WORK WILL BE PERFORMED.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 254 PATCHING PLANED SURFACE LOCATION 1 - 1000 SQ.YD. LOCATION 2 - 1000 SQ.YD.

BUTT JOINT

A BUTT JOINT WILL BE REQUIRED AT LOCATIONS LISTED IN THE TABLE BELOW AND AT BRIDGES SHOWN ON DETAIL SHEET 14. AFTER THE JOINT IS CONSTRUCTED, THE DROP OFF CREATED SHALL BE MINIMIZED BY TEMPORARILY FILLING THE VOID TO WITHIN AT LEAST 1" OF THE EXISTING ROADWAY SURFACE. REMOVAL OF THE WEDGE SHALL BE INCIDENTAL TO ITEM 614 BITUMINOUS CONCRETE FOR MAINTAINING TRAFFIC (SEE DETAIL BELOW).



LOCATION	FLOOR	DESCRIPTION	202 WEARING COURSE REMOVED SQ.YD.	407 TACK COAT @ 0.075 GAL/S.Y.	614 BIT. CONC. FOR MAINTAINING TRAFFIC CU.YD.
1	SR 13	BEGIN WORK SLM 20.38	133	2	0.4
		BRIDGE: PER-13-2097	SHEET 13	2	0.4
		END WORK SLM 28.73	INCLUDED W/PLANING	5	2.0
1	SR 13	TOTALS	133	9	2.8
2	US 22	BEGIN WORK SLM 5.51	INCLUDED W/PLANING	4	1.3
		END WORK SLM 6.70	INCLUDED W/PLANING	5	2.0
2	US 22	TOTALS		9	3.3

QUANTITIES CARRIED TO THE GENERAL SUMMARY

ITEM 604 MANHOLE, CATCH BASIN AND ITEM 638 VALVE BOX ADJUSTED TO GRADE

EXISTING SEWER MANHOLES, CATCH BASINS AND WATER VALVE BOXES THAT ARE TO BE ADJUSTED TO GRADE ARE LISTED BELOW, THESE NUMBERS ARE TAKEN FROM FIELD COUNTS, HOWEVER THE ACTUAL NUMBER THAT ARE TO BE ADJUSTED TO GRADE WILL BE DETERMINED BY THE ENGINEER AT THE TIME OF CONSTRUCTION, PAYMENT SHALL BE FOR THE ACTUAL NUMBERS OF EACH ITEM THAT ARE ADJUSTED TO GRADE AS DETERMINED BY THE ENGINEER. WHEN ADJUSTING MANHOLES EXTREME CARE SHALL BE TAKEN WHEN REMOVING CONCRETE, SO AS NOT TO DAMAGE MANHOLE COVERS AND FRAMES. MANHOLES SHALL BE ADJUSTED USING CONCRETE SHOWN IN DRAWING BP-3.1, 2-21-92. WHEN ADJUSTING MANHOLES, CATCH BASINS AND WATER VALVE BOXES ALL MATERIALS, LABOR, EQUIPMENT, TOOLS AND ANY OTHER INCIDENTALS AND REMOVAL OF THE EXISTING CONCRETE SHALL BE PAID FOR UNDER EACH ITEM AS SHOWN ON THE GENERAL SUMMARY.

ANY GAS VALVE BOXES AND TELEPHONE COMPANY MANHOLES ON THIS PROJECT SHALL BE ADJUSTED TO GRADE BY THE RESPECTIVE OWNERS.

LOCATION 1

ITEM 604 CATCH BASIN ADJUSTED TO GRADE 10 EACH
ITEM 638 VALVE BOX ADJUSTED TO GRADE 7 EACH

LOCATION 2

ITEM 638 VALVE BOX ADJUSTED TO GRADE 3 EACH

ITEM 202 RAISED PAVEMENT MARKER REMOVED FOR STORAGE, AS PER PLAN

REMOVAL OF RAISED PAVEMENT MARKERS SHALL CONFORM WITH SECTION NO. 202.071 IN THE CONSTRUCTION AND MATERIAL SPECIFICATIONS MANUAL EXCEPT FOR THE FOLLOWING:
AFTER PAVEMENT MARKERS HAVE BEEN REMOVED BY THE CONTRACTOR, HE WILL THEN BE RESPONSIBLE TO TAKE THE REMOVED MARKERS TO A STATE GARAGE THAT WILL BE DESIGNATED BY THE ENGINEER. THE PROJECT ENGINEER SHALL GIVE THE COUNTY MANAGER 24 HOUR NOTICE PRIOR TO DELIVERY AND THE PROJECT ENGINEER SHALL BE RESPONSIBLE FOR FURNISHING ALL NECESSARY TRANSFER DOCUMENTATION WITH ALL DELIVERIES.
PAYMENT FOR ALL WORK DESCRIBED ABOVE SHALL BE PAID FOR UNDER ITEM 202 RAISED PAVEMENT MARKERS REMOVED FOR STORAGE, AS PER PLAN.

LOCATION 1 - 701 EACH

QUANTITIES CARRIED TO GENERAL SUMMARY

EXTRA ASPHALT FOR SPOT LEVELING

A CONTINGENCY QUANTITY OF 448 ASPHALT CONCRETE INTERMEDIATE COURSE HAS BEEN INCLUDED IN THE PLAN TO BE USED AT THE DIRECTION OF THE ENGINEER FOR SPOT LEVELING WHERE THE PAVEMENT IS LOW, DETERIORATED AND/OR IN SLIP REPAIR AREAS. THIS ITEM SHALL INCLUDE FOR PAYMENT ALL MATERIALS, LABOR, EQUIPMENT AND INCIDENTALS NECESSARY TO COMPLETE THE WORK DESCRIBED ABOVE.

THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY TO PERFORM THE WORK AS DESCRIBED ABOVE

ITEM 448 ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, PG 64-22

LOCATION 1 - 200 CU. YD.

QUANTITIES CARRIED TO GENERAL SUMMARY

ITEM 614 WORK ZONE MARKING SIGNS

A QUANTITY OF WORK ZONE MARKING SIGNS HAS BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER.

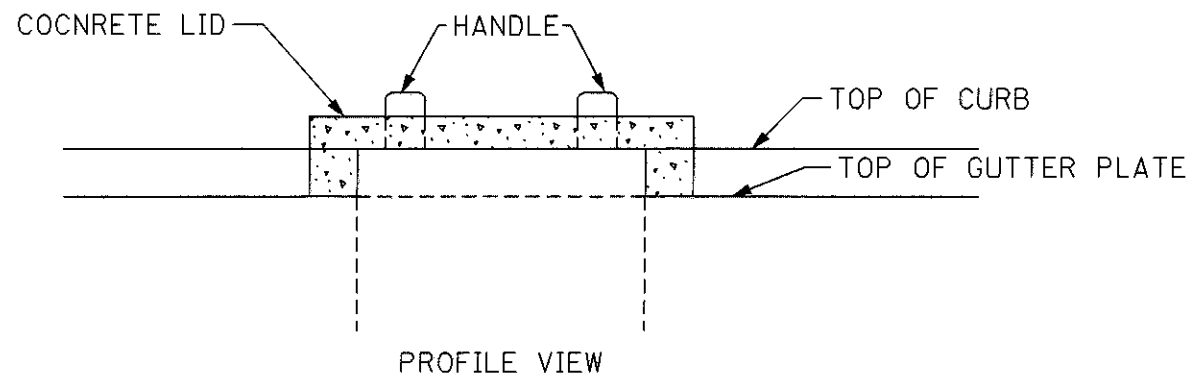
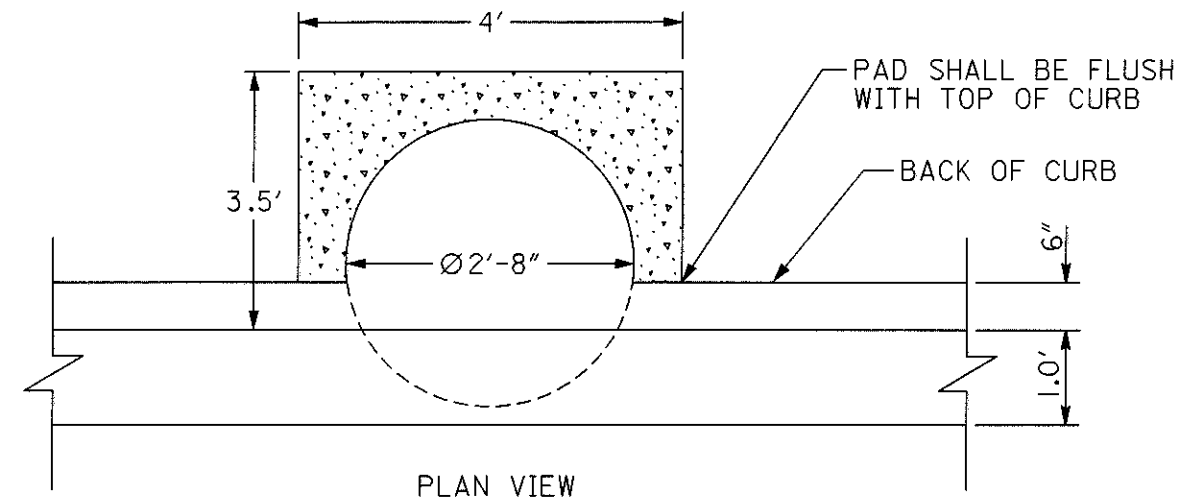
WORK ZONE MARKING SIGNS	LOCATION 1	LOCATION 2
OW-167 (NO EDGE LINES)	8	0
R-33 (DO NOT PASS)	26	2
R-34 (PASS WITH CARE)	15	0
OW-128 (ROAD CONSTRUCTION AHEAD)	18	4
OC-8 (END CONSTRUCTION)	18	4
TOTALS	85	10

QUANTITIES CARRIED TO GENERAL SUMMARY

ITEM 604 INLET ADJUSTED TO GRADE, AS PER PLAN

THROUGHOUT THE PROJECT, SIDE INLETS SHALL BE ADJUSTED TO GRADE AS PER THE DETAIL BELOW AND THIS NOTE. A CONCRETE PAD SHALL BE CONSTRUCTED AROUND EXISTING PIPE/INLET AS SHOWN BELOW. THE PAD SHALL BE A MINIMUM OF 4" THICK AND SHALL BE THE SAME ELEVATION AS THE TOP OF THE PROPOSED CURB IN THE NEW COMBINATION CURB AND GUTTER. A CONCRETE LID SHALL BE FURNISHED AND SHALL BE 4' X 3.5' X 4" THICK REINFORCED CONCRETE WITH HANDLES FOR LIFTING. THE HANDLES SHALL BE MADE OF REBAR AND SHALL BE ABLE TO ADEQUATELY SUPPORT WEIGHT OF LID. ALL INLETS TO BE ADJUSTED SHALL BE IDENTIFIED BY THE PROJECT ENGINEER BEFORE ANY WORK IS TO BE DONE. ALL MATERIALS, LABOR, EQUIPMENT AND INCIDENTALS TO COMPLETE THE WORK DESCRIBED ABOVE SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 604 INLET ADJUSTED TO GRADE, AS PER PLAN.

ITEM 604 INLET ADJUSTED TO GRADE, AS PER PLAN
LOCATION 1 - 7 EACH LOCATION 2 - 5 EACH



CLEARING AND GRUBBING

STUMPS LOCATED AT APPROXIMATELY SLM 5.95 ON US 22 WITHIN THE CONSTRUCTION LIMITS SHALL BE REMOVED UNDER THE LUMP SUM BID FOR ITEM 201, CLEARING AND GRUBBING. THE FOLLOWING IS AN APPROXIMATE ESTIMATE OF THE NUMBER OF TREES AND STUMPS TO BE REMOVED.

SIZES	NO. TREES	NO. STUMPS	TOTAL
30"		3	3

ITEM 608 CURB RAMPS

CURB RAMPS SHALL BE INSTALLED AT THE FOLLOWING LOCATIONS IN ACCORDANCE WITH STANDARD CONSTRUCTION DRAWING BP-7.1, 10-30-92. TYPE OF CURB RAMP TO BE USED AND LOCATIONS SHALL BE VERIFIED BY THE PROJECT ENGINEER. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR THE PURPOSE DESCRIBED ABOVE.

LOCATION 2 (VARIOUS LOCATIONS IN SOMERSET)

ITEM 608 CURB RAMP, TYPE 1 96 SQ.FT.
ITEM 202 WALK REMOVED 96 SQ.FT.

ITEM 608 CURB RAMP, TYPE 2 180 SQ.FT.
ITEM 202 WALK REMOVED 180 SQ.FT.

ITEM 604 - CATCH BASIN RECONSTRUCTED TO GRADE, AS PER PLAN

CATCH BASINS SHOWN BELOW SHALL BE RECONSTRUCTED TO GRADE AS WRITTEN IN THE CMS MANUAL EXCEPT FOR THE FOLLOWING:
ALL FRAMES, GRATES, CONCRETE, BRICKS, MORTAR, MATERIALS, LABOR, TOOLS AND INCIDENTALS NECESSARY TO RECONSTRUCT CATCH BASINS SHALL BE PAID FOR UNDER ITEM 604 CATCH BASIN RECONSTRUCTED TO GRADE, AS PER PLAN. WALLS OF CATCH BASINS SHALL BE REMOVED BEYOND THE POINT OF DETERIORATION. IT WILL BE THE RESPONSIBILITY OF THE CONTRACTOR TO PERFORM A FIELD INSPECTION PRIOR TO BIDDING AND CONSTRUCTION OF THIS PROJECT IN ORDER TO ESTIMATE QUANTITIES OF CONCRETE NEEDED AND SIZES OF FRAMES AND GRATES. WHEN THE CATCH BASIN IS BEING CONSTRUCTED IT SHALL BE CLEANED FREE OF ALL DEBRIS; IF WALLS OF CATCH BASINS NEED TO BE REMOVED BELOW EXISTING WINDOWS, INLET OR OUTLET PIPES, PROPOSED WALLS SHALL BE CAST-IN-PLACE AS WAS ORIGINALLY DESIGNED; INLET AND OUTLET PIPES SHALL BE STUBBED IN TO EXISTING PIPES. CATCH BASINS SHALL BE ADJUSTED TO GRADE AS APPROVED BY THE ENGINEER. FINAL GRATE ELEVATIONS SHALL BE APPROVED BY THE ENGINEER. AT THE TIME OF CONSTRUCTION; GRATES SHALL BE LOCATED TO ACHIEVE MAXIMUM DRAINAGE.

THE FOLLOWING QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR THE PURPOSE DESCRIBED ABOVE.

ITEM 604 CATCH BASIN RECONSTRUCTED TO GRADE, AS PER PLAN
LOCATION 2 (SLM 6.08 RT.) - 1 EACH

PUBLIC SAFETY

BEFORE WORK BEGINS, THE CONTRACTOR SHALL SUBMIT TO THE ENGINEER THE NAMES AND TELEPHONE NUMBERS OF PERSONS WHO CAN BE CONTACTED 24 HOURS A DAY BY THE OHIO DEPARTMENT OF TRANSPORTATION AND ALL INTERESTED POLICE AGENCIES. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PLACING CONTROL DEVICES NECESSARY TO MAINTAIN SAFETY TO THE TRAVELING MOTORIST.

NO HAZARD SHALL BE LEFT WITHOUT GUARDRAIL EXCEPT FOR THE MINIMUM TIME NECESSARY FOR REMOVAL, GRADING AND REINSTALLATION. THE PERMANENT GUARDRAIL SHALL BE ERECTED AND THE TYPE A ANCHOR ASSEMBLIES SHALL BE HELD RIGIDLY IN PLACE AT THE GROUND SURFACE PRIOR TO PERMANENT ATTACHMENT AT THE CONCRETE ANCHOR, IN A MANNER ACCEPTABLE TO THE ENGINEER.

BERM RESHAPING AND GUARDRAIL REMOVAL AND CONSTRUCTION SHALL NOT PROCEED SIMULTANEOUSLY ON BOTH THE LEFT AND RIGHT SHOULDERS OF THE ROADWAY. THE OPEN AREA DUE TO GUARDRAIL REMOVAL SHALL BE ADEQUATELY MAINTAINED AND PROTECTED AT ALL TIMES WITH TEMPORARY DRUMS OR BARRICADES AND OTHER WARNING DEVICES SATISFACTORY TO THE ENGINEER. NO OPEN AREAS DUE TO GUARDRAIL REMOVAL SHALL BE PERMITTED AFTER EACH WORK DAY IS COMPLETED.

PROTECTION OF INCOMPLETED WORK

ANY HAZARD DURING NON-WORKING HOURS SHALL BE ADEQUATELY PROTECTED WITH DRUMS OR BARRICADES, OR AS DIRECTED BY THE ENGINEER. PAYMENT FOR ANY SUCH WORK REQUIRED WILL BE CONSIDERED AS INCIDENTAL AND INCLUDED IN THE GUARDRAIL REPAIR ITEM.

GUARDRAIL POST AND POST HOLES

ALL HOLES REMAINING AFTER REMOVAL OF GUARDRAIL POSTS OR GUARD POSTS SHALL BE FILLED WITH EITHER GRANULAR MATERIAL, EXCESS MATERIAL RESULTING FROM GUARDRAIL CONSTRUCTION OR EXCESS MATERIAL FROM BERM RESHAPING. FILL MATERIAL CONTAINING SOD SHALL NOT BE USED. ALL FILL MATERIAL SHALL BE APPROVED BY THE ENGINEER. MATERIAL PLACED IN HOLES SHALL BE THOROUGHLY COMPACTED AND LEVELED OFF AS DIRECTED BY THE ENGINEER. PAYMENT FOR THE ABOVE SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE APPLICABLE GUARDRAIL ITEM.

ITEM 606 GUARDRAIL

ALL MATERIAL EXCAVATED FOR POST HOLES OR CONCRETE ANCHORS SHALL BE REMOVED AND DISPOSED OF IN ACCORDANCE WITH 203.05 OF THE SPECIFICATIONS AND AREA NEATLY RESTORED. THE COST OF THIS IS TO BE INCLUDED IN THE APPROPRIATE GUARDRAIL BID ITEM. THE LOCATIONS OF GUARDRAIL RUNS AS SHOWN IN THESE PLANS ARE SUBJECT TO ADJUSTMENT TO ASSURE THAT THE PLANNED INSTALLATION WILL AFFORD MAXIMUM PROTECTION FOR TRAFFIC.

ITEM SPECIAL RESHAPING BERM

AT SOME LOCATIONS OF GUARDRAIL REPLACEMENT, BERMS SHALL BE RESHAPED AT THE DIRECTION OF THE ENGINEER. ANY NECESSARY EXCAVATION AS A RESULT OF RESHAPING BERM SHALL BE INCLUDED IN THE CONTRACT PRICE BID PER FOOT FOR ITEM SPECIAL RESHAPING BERM .

A CONTINGENCY QUANTITY OF 800 FEET HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR RESHAPING BERM AS DIRECTED BY THE ENGINEER.

ITEM 202 GUARDRAIL REMOVED FOR STORAGE, AS PER PLAN

THIS ITEM SHALL CONSIST OF REMOVING GUARDRAIL ON SR 13 AT SLM 23.30 TO SLM 23.44, DISASSEMBLING GUARDRAIL PANELS AND DELIVERING PANELS, POSTS AND END TERMINAL ASSEMBLIES (AS SPECIFIED BELOW) TO THE LOCATIONS LISTED BELOW. THE PROJECT ENGINEER SHALL INSPECT ALL GUARDRAIL ELEMENTS BEFORE BEING DELIVERED (THE INTENT IS TO SEND ONLY THE BEST PANELS AND POST TO THE PERRY COUNTY GARAGE). A 24 HOUR NOTICE SHALL BE GIVEN PRIOR TO DELIVERING ANY GUARDRAIL, O.D.O.T. WILL PROVIDE LOADER AND PERSONNEL TO UNLOAD GUARDRAIL UPON ARRIVAL AT SPECIFIED LOCATIONS. ONLY THOSE QUANTITIES ACTUALLY DELIVERED TO THE LOCATIONS BELOW SHALL BE PAID FOR UNDER ITEM 202 GUARDRAIL REMOVED FOR STORAGE, AS PER PLAN.

THE FOLLOWING QUANTITY IS AN ESTIMATED QUANTITY, THE PROJECT ENGINEER SHALL DETERMINE WHICH PANELS CAN BE SALVAGED AND DELIVERED TO THE PERRY COUNTY GARAGE. ALL GUARDRAIL NOT DELIVERED TO THE PERRY COUNTY GARAGE SHALL BECOME THE PROPERTY OF THE CONTRACTOR AS PER THE STANDARD SPECIFICATIONS.

PERRY COUNTY GARAGE - 750' (INCLUDES 2 ANCHOR ASSEMBLY, TYPE A)

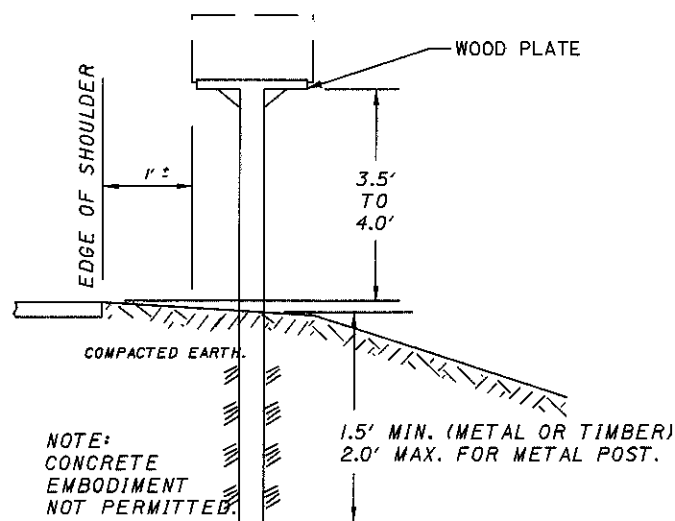
PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT BID PRICE FOR ITEM 202 GUARDRAIL REMOVED FOR STORAGE, AS PER PLAN 750 LIN.FT.

ITEM 606 GUARDRAIL, TYPE 5, USING 9' POSTS

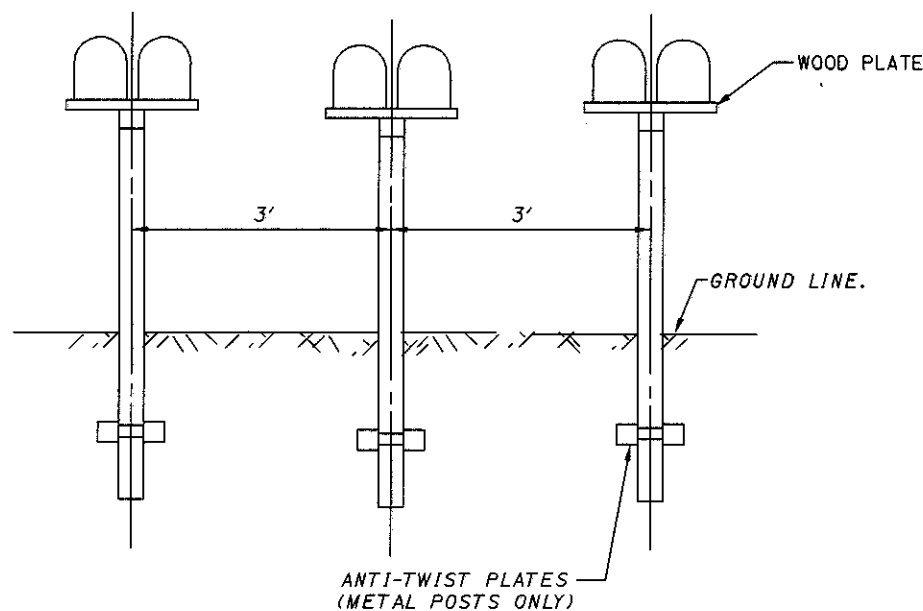
THIS ITEM SHALL CONSIST OF INSTALLING TYPE 5 GUARDRAIL WITH 9' POSTS BETWEEN SLM 23.30 AND SLM 23.44 (THE INTENT IS TO PLACE GUARDRAIL IN THE SAME LOCATION AS REMOVAL). AT THE BEGINNING AND THE END OF THE RUN THE GUARDRAIL SHALL BE WRAPPED AROUND THE DRIVES WITH RADIUS RAIL. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO DETERMINE THE AMOUNT OF RADIUS RAIL NEEDED, 50' HAS BEEN ESTIMATED AND IS INCLUDED WITH TOTAL LENGTH SHOWN BELOW. END TERMINALS SHALL BE TYPE T ANCHOR ASSEMBLIES. ANY EMBANKMENT, TOOLS, LABOR, EQUIPMENT AND INCIDENTALS NEEDED TO COMPLETE THIS WORK SHALL BE PAID FOR UNDER THE APPROPRIATE BID ITEM BELOW.

**ITEM 606 GUARDRAIL, TYPE 5, USING 9' POSTS LOCATION 1 - 800 LIN.FT.
ITEM 606 ANCHOR ASSEMBLY, TYPE T LOCATION 1 - 2 EACH**

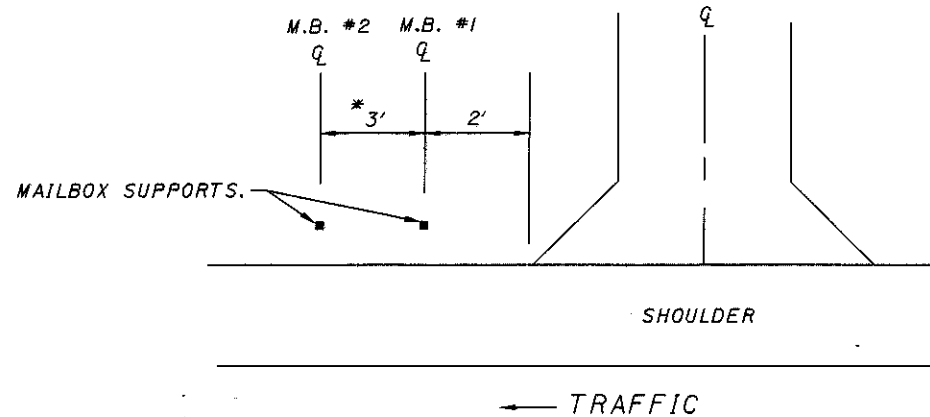
MAILBOX DETAILS



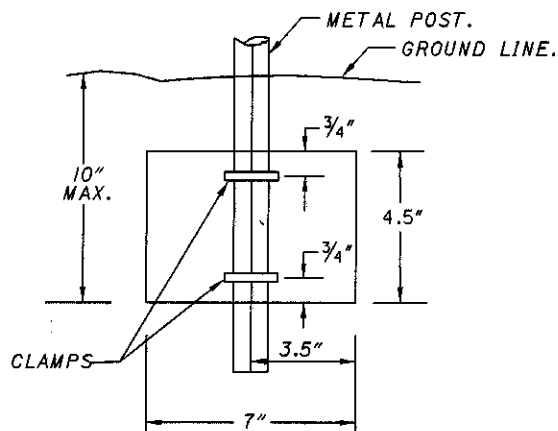
TYPICAL MAILBOX LOCATION AND MOUNTING HEIGHT



GROUP MAILBOX INSTALLATION



* ADD 3' FOR EACH ADDITIONAL MAILBOX.



ANTI-TWIST PLATE

ITEM SPECIAL - MAILBOX SUPPORT

DESCRIPTION

THIS WORK SHALL CONSIST OF FURNISHING AND ERECTING MAILBOX SUPPORTS AND ANY ASSOCIATED MOUNTING HARDWARE IN ACCORDANCE WITH PLAN DETAILS, AND ATTACHING AN OWNER-SUPPLIED MAILBOX AT LOCATION SPECIFIED IN THE PLAN, OR OTHERWISE ESTABLISHED BY THE ENGINEER. THIS ITEM SHALL INCLUDE THE REMOVAL OF THE EXISTING POSTS AND OTHER MATERIAL NOT CONSIDERED SALVAGEABLE AND DISPOSED OF IN ACCORDANCE WITH 202.02.

MATERIALS

WOOD POSTS SHALL BE NOMINAL 4" x 4" SQUARE OR 4" DIAMETER ROUND. ALL WOOD INCLUDING POST AND PLATES SHALL CONFORM TO 710.14.

STEEL POSTS SHALL BE NOMINAL PIPE SIZE 2" I.D., AND CONFORM TO AASHTO M 181.

HARDWARE (PLATES, SCREWS, BOLTS, ETC.) SHALL BE COMMERCIAL-GRADE GALVANIZED STEEL.

SETTING POSTS

POSTS SHALL BE SET PER THE FIRST PARAGRAPH OF 606.03 AND SHALL IN NO INSTANCE BE ENCASED IN CONCRETE.

MOUNTING BOXES

SUPPORT HARDWARE SHALL ACCOMMODATE EITHER A SINGLE OR A DOUBLE MAILBOX INSTALLATION, AND NO MORE THAN TWO BOXES MAY BE MOUNTED ON A SINGLE POST.

THE MAILBOX SHALL BE SECURELY AND NEATLY ATTACHED BY THE CONTRACTOR TO THE NEW SUPPORT. THE CONTRACTOR SHALL FURNISH ALL NECESSARY ATTACHMENT HARDWARE (NUTS, BOLTS, PLATES, SPACERS, AND WASHERS) AS NECESSARY TO ACCOMMODATE THE COMPLETE INSTALLATION.

IN THE ABSENCE OF A NEW BOX SUPPLIED BY THE OWNER, THE CONTRACTOR SHALL SALVAGE THE EXISTING BOX AND PLACE IT ON THE NEW SUPPORT. DUE CARE SHALL BE EXERCISED IN SUCH AN OPERATION, AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR REPAIRING OR REPLACING ANY BOX DAMAGED BY IMPROPER HANDLING ON HIS PART, AS JUDGED AND DIRECTED BY THE ENGINEER.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR COORDINATING WITH THE LOCAL POST MASTER REGARDING THE TIMING OF THE MOVEMENT OF ANY MAILBOX TO A NEW LOCATION.

BASIS OF PAYMENT

PAYMENT UNDER THIS ITEM SHALL BE LIMITED TO FINAL PERMANENT INSTALLATIONS. TEMPORARY INSTALLATIONS SHALL BE IN ACCORDANCE WITH 107.12. HOWEVER, THE SAME MATERIAL AND SIZE LIMITATIONS AS FOR PERMANENT INSTALLATIONS SHALL APPLY.

MAILBOX SUPPORTS WILL BE PAID FOR AT THE CONTRACT UNIT PRICE PER EACH, FOR THE TYPE SPECIFIED, COMPLETE IN PLACE.

PAYMENT WILL BE MADE UNDER:

ITEM	UNIT	DESCRIPTION
SPECIAL	EACH	MAILBOX SUPPORT SYSTEM SINGLE
SPECIAL	EACH	MAILBOX SUPPORT SYSTEM DOUBLE

QUANTITY

THE FOLLOWING QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR THE ABOVE PURPOSE

SPECIAL MAILBOX SUPPORT SYSTEM SINGLE
LOCATION 1 - 6 EACH LOCATION 2 - 1 EACH

MAILBOX.DGN 2-23-00

MAILBOX DETAILS AND QUANTITIES

PER-13-20.38
PER-22-5.51

10
27

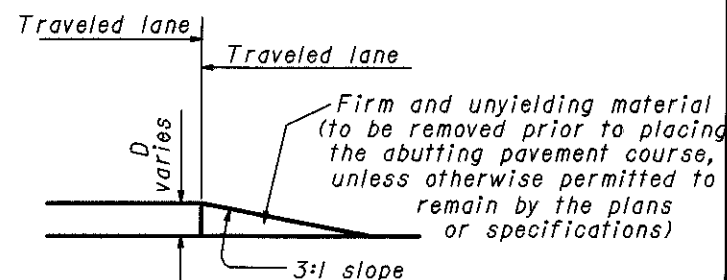
CALCULATED
LME
CHECKED
TJD

GENERAL NOTES

- It is intended that this drawing be used for treatment of drop-offs that develop during construction operations, and that are not otherwise provided for in the construction plans. Where the plans do not provide specific items for labor, equipment, or materials to implement the drop-off treatments specified hereon, they shall be included for payment in the lump sum bid for Item 614 - Maintaining Traffic.
- While the need for certain advisory signing is noted hereon, it is not intended that this be indicative of all signing that may be required to advise or warn motorists, and all requirements of the Ohio Manual of Uniform Traffic Control Devices (OMUTCD) must be fulfilled.
- In urban or otherwise heavily developed areas where pedestrians and/or bicyclists may be present in significant numbers, additional signing and protective measures other than those shown hereon may be required.
- The drop-off treatment selected for use at any given location shall be as appropriate for the prevailing conditions at the site.
- Where concrete barrier is specified, it shall be in accordance with Standard Construction Drawing MC-9.2 and Item 622.
- When drums are specified for a dropoff condition, a minimum number of four drums shall be used. Spacing shall be as indicated in the plans or as specified in the OMUTCD.
- When OW-151 (Low Shoulder) signs or OW-171 (Uneven Lanes) and OWP-171 signs are required, they shall be placed 750' in advance of the condition, on all intersecting entrance ramps within the limits of the condition and immediately beyond all intersecting roadways within the limits of the condition. When the dropoff condition extends more than one-half mile, additional signs should be erected at intervals of one mile or less.
- For locations, such as at ramps, lane shifts, lane closures, etc., where traffic is required to negotiate any difference in elevation between pavements, a 3:1 slope treatment similar to the Optional Wedge Treatment shall be provided.
- Portable concrete barrier shall be placed on the same level as the traffic surface and shall not encroach on lane width(s) designated as the minimum required for traffic use. Where drums are used, and their presence would reduce traveled lane widths to less than 10', drums may be placed on the opposite level from that of traffic provided the dropoff depth does not exceed 5" and approval is granted by the Project Engineer.
- Pavement Repairs (or similar work):
 - Lengths greater than 60 feet - utilize appropriate treatment from Condition I.
 - Lengths of 60 feet or less - repairs shall be effected in accordance with 255.08. Drums may be used as a separator adjacent to the traveled lane.

OPTIONAL WEDGE TREATMENT (MILLING OR RESURFACING)

- This treatment may be used when permitted for Condition I only.
- OW-171 and OWP-171 signs required.

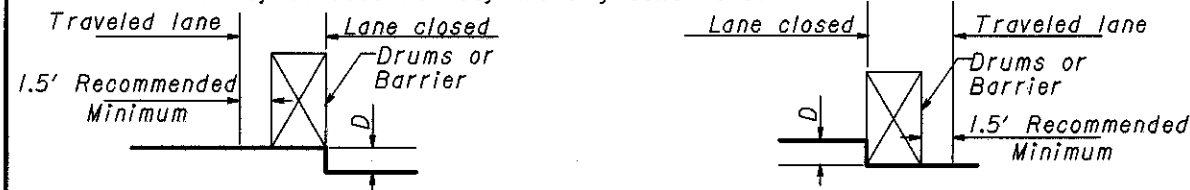


CONDITION I DROPOFFS BETWEEN TRAVELED LANES

- These treatments are to be used for resurfacing, pavement planing, excavation, etc. between or within traveled lanes.

D (In.)	Treatment
≤ 1/2	Erect OW-171 and OWP-171 signs.
> 1/2 - 3	1) Lane closure utilizing drums* as shown below OR 2) Optional Wedge Treatment
> 3 - 5	Lane closure utilizing drums as shown below.
> 5	Lane closure utilizing portable concrete barrier as shown below.

*Cones may be used for daytime only conditions.

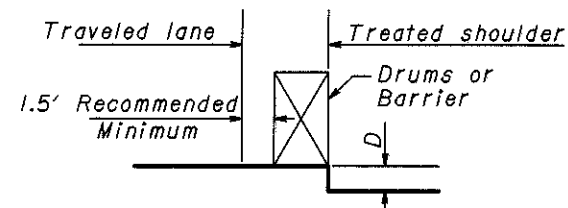


CONDITION II DROPOFFS WITHIN GRADED SHOULDER AREA

- The treatments indicated below are for use in conjunction with resurfacing, planing, or excavations within the graded shoulder area.
- The graded shoulder area is that flat or gradually sloping area between the edge of a normally traveled lane and the more steeply sloping ditch foreslope or embankment slope. Its surface may be soil or turf, and/or it may be inclusive of a "treated" area (improved with aggregates, asphaltic materials, or concrete). For the purposes herein, its maximum width shall be considered to be twelve (12) feet.

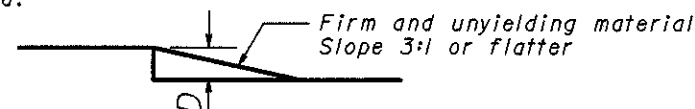
D (In.)	Treatment
≤ 1/2	1) If edgelines are present, no treatment necessary OR 2) Erect OW-171 and OWP-171 signs.
> 1/2 - 5	1) If min. lane width* requirements can be met, maintain lanes utilizing drums as shown below OR 2) If min. lane width* requirements cannot be met, close adjacent lane utilizing drums OR 3) Optional Shoulder Treatment.
> 5 - 12 Daylight only	If min. lane width* requirements can be met, maintain lanes utilizing drums as shown below.
> 5 - 24	1) If min. lane width* requirements can be met, maintain lanes utilizing portable concrete barrier as shown below. OR 2) If min. lane width* requirements cannot be met, close adjacent lane utilizing drums.
> 24	Lane closure utilizing portable concrete barrier as shown below.

*Minimum lane widths shall be 10' unless otherwise specified in the plans.



OPTIONAL SHOULDER TREATMENT

- This treatment may not be used within a bituminous shoulder where a hot longitudinal joint per 401.15 is required.
- OW-151 signs required.



PER-13-20.38
PER-22-5.51

11
27

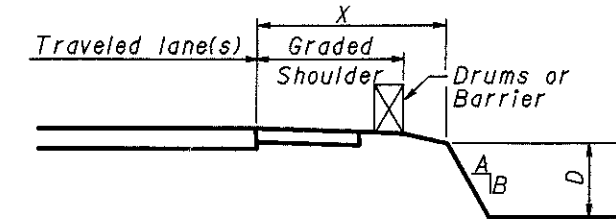
CONDITION III

DROPOFFS BEYOND GRADED SHOULDER OR BACK OF CURB

- See Note 2 under Condition II.
- Use Chart A or B below, as applicable.

CHART A

- USE FOR:
- Uncurbed Facilities.
 - Curbed Facilities, where:
 - Curbs are less than 6" in height.
 - Curbs are 6" or greater in height and the legal speed is greater than 40 mph.

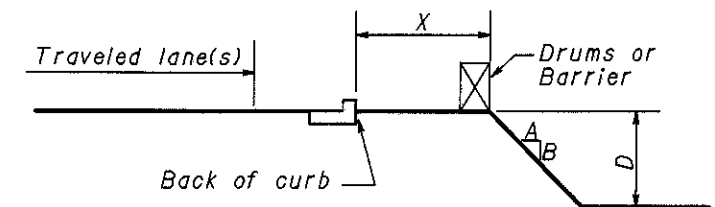


X (Ft.)	D (In.)	A/B	Treatment Required	
			Day	Night
0-4	Any	Any	(a)	(a)
4-30	Any	3:1 or Flatter	None	None
4-12	< 3	Steeper than 3:1	None	None
4-12	> 3 - < 12	Steeper than 3:1	Drums	Drums
4-12	> 12	Steeper than 3:1	Drums	Barrier
> 12 - 20	< 12	Steeper than 3:1	None	None
> 12 - 20	> 12 - < 24	Steeper than 3:1	Drums	Drums
> 12 - 20	> 24	Steeper than 3:1	Drums	Barrier
> 20 - 30	< 24	Steeper than 3:1	None	Drums
> 20 - 30	> 24	Steeper than 3:1	Drums	Barrier
> 30	Any	Any	None	None

(a) Use treatment specified under Condition II.

CHART B

- USE FOR: Curbed facilities, where the curb is 6" or greater in height and the legal speed is 40 mph or less.



X (Ft.)	D (In.)	A/B	Treatment Required	
			Day	Night
0-10	< 12	Any	None	Drums
0-10	> 12	Any	Drums	Drums
> 10	Any	Any	None	None

STATE OF OHIO
DEPARTMENT OF TRANSPORTATION
BUREAU OF LOCATION AND DESIGN

DROPOFFS IN WORK ZONES

DESIGNED DRAWN TRACED CHECKED REVIEWED DATE REVISED

RPM General Notes

Materials Supplied by The Department

All materials are to be Contractor furnished, except that the Department shall supply RPM materials in the quantities shown herein to the Contractor. Pay items for the Department supplied materials shall be indicated as "Installation Only". The quantity and type of Department supplied materials are shown on sheet 21 of this plan.

The Contractor shall pick up the department supplied RPM materials at the direction of the Project Engineer.

For some projects having quantities of less than 20 RPMs, the contractor may pick up RPM materials at the District Offices. Quantities over 20 RPMs will be picked up at the Recycler's Warehouse or as arranged with the District. The Contractor shall pick up Department supplied RPM materials at the specified location(s) for transport to the work site or to the Contractor's storage facility. The Recycled Raised Pavement Marker (RPM) Authorization Form is to be signed by the District Construction Engineer prior to pick up of the RPMs. The Contractor shall notify the District and / or the parties listed on the authorization form in writing at least five (5) calendar days prior to pick up of the department supplied materials. The contractor shall store the RPMs without damage or contamination with foreign matter. A deduction in the amount of the actual cost to the Department shall be made for materials damaged by the Contractor or for castings received by the Contractor which were not installed and were not returned to the Department.

Return of Non-performed Raised Pavement Marker Materials Supplied by the Department

Raised Pavement Marker Materials Supplied by the Department, that are non-performed shall be carefully repacked or packed in the boxes in the same style and quantity as originally received from the Department. Casting styles shall not be mixed within any one container. The Contractor shall clearly mark on the outside of each container, the color of the prismatic retro-reflector, the style of casting. Boxes shall be placed on skids or pallets in the same style (Low Profile or Conventional, reflectorised or non reflectorised) and no more than 420 RPMs (or 21 Boxes) on one skid.

Only use the boxes supplied by the Raised Pavement Marker Recycler. Boxes must be marked with the recycler's part or catalog number and the project number. The recycler's catalog or part numbers may be obtained from the Office of Traffic Engineering in Columbus, Ohio or from the recycler. Boxes not marked with the proper recycler's catalog or part numbers, and the department's project number will not be accepted at the recycler's warehouse. Non Performed Materials will be returned to the location as specified by the District Construction Engineer within 30 Days of the completion of the project.

The above work including all labor, equipment and material needed to perform the work, shall be considered incidental to the respective pay item.

If the department has to repackage the RPMs correctly, the Contractor will be assessed the actual cost for repackaging the Materials by the Department's Forces.

Loading of Materials Supplied by the Department at the Recycler's Warehouse

Trucks shall have a loading height of 48 inches and be able to back up flush to the loading dock.

Trucks shall not have any obstructions or protrusions that prevent the loading by a standard forklift or lift truck.

Semi trucks or 20 foot commercial trucks are the most appropriate trucks for loads in excess of 4 pallets (one pallet = 21 boxes = 2000 LBS).

Stake body trucks are appropriate to load less than 4 pallets, provided the truck is rated for the load and the load can be safely secured for transport by chaining or strapping down as needed.

Pickup trucks are appropriate for loads of approximately one pallet, provided the pickup truck is rated for the load and the load can be safely secured for transport.

Dump trucks, tilt bed trucks, and non commercial moving vans will not be loaded by the recycler's warehouse.

The warehouse supervisor will refuse to load any truck that is unsafe to load or unsuitable for the load being placed on the truck.



HORIZONTAL SCALE IN FEET

CALCULATED LINE CHECKED TJD

SOMERSET DETAIL

PER-13-20.38
PER-22-5.51

13
27

QUANTITIES AND CALCULATIONS

SOMERSET SQUARE AREA = 2828 SQ. YD.

LOCATION 1

ITEM 202 CURB REMOVED
116 LIN.FT.

ITEM 254 PAVEMENT PLANING, BITUMINOUS, AS PER PLAN
2828 SQ.YD.

ITEM 407 TACK COAT, 702.13
2828 SQ.YD. X 0.075 GAL/S.Y. = 212 GAL

ITEM 407 TACK COAT FOR INTERMEDIATE COURSE
2828 SQ.YD. X 0.05 GAL/S.Y. = 141 GAL

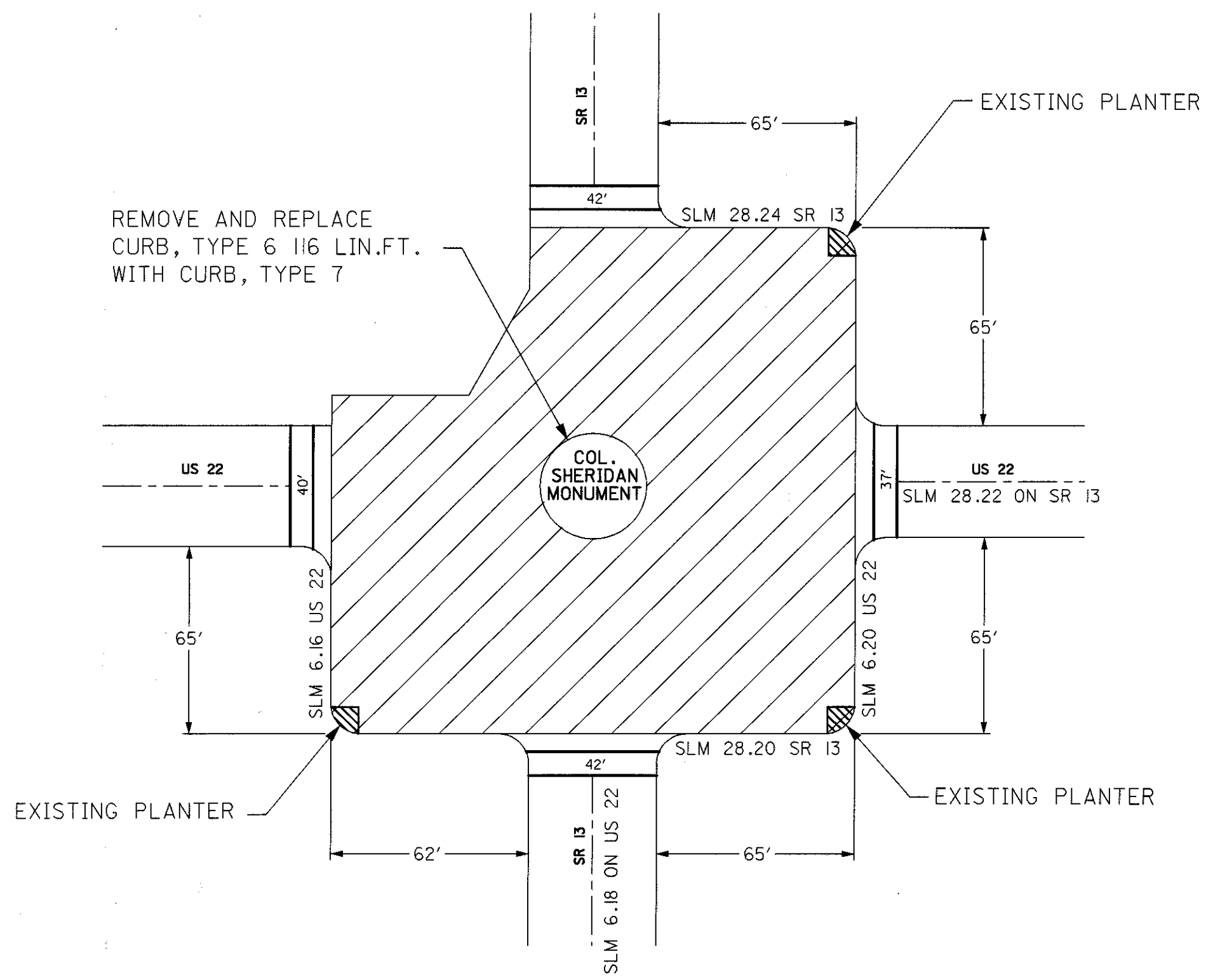
ITEM 830 CURB, TYPE 7
116 LIN.FT.

ITEM 857 ASPHALT CONCRETE WITH GILSONITE,
INTERMEDIATE COURSE, TYPE 2
2828 SQ.YD. X 1.75"/36 = 137.5 CU.YD.

ITEM 857 ASPHALT CONCRETE WITH GILSONITE,
SURFACE COURSE, TYPE 1
2828 SQ.YD. X 1.25"/36 = 98.2 CU.YD.

ITEM 830 CURB, TYPE 7

THIS ITEM SHALL ALSO INCLUDE IN THE UNIT PRICE BID
ANY EMBANKMENT NEEDED TO BACK UP NEWLY CON-
STRUCTED CURB, TYPE 7.



REMOVE AND REPLACE
CURB, TYPE 6 116 LIN.FT.
WITH CURB, TYPE 7

EXISTING PLANTER

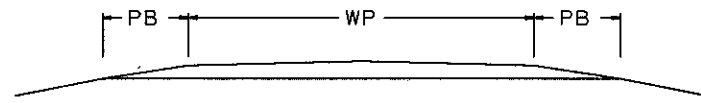
EXISTING PLANTER

EXISTING PLANTER

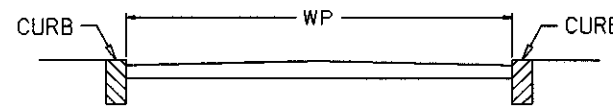
PO130001.DTL 2-23-00

ASPHALT CONCRETE

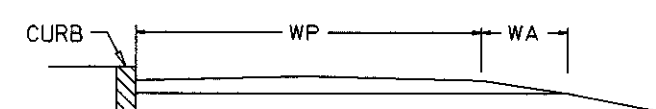
TYPICAL 1



TYPICAL 2



TYPICAL 3



BEGIN WORK LOCATION 1
NEW LEX. CORP. SLM 20.38

2097

2510

PER. CO. SR 13

SR 669
SLM 26.40

SR 383
SLM 27.04

SOMERSET S.CORP. SLM 27.44

STA EQ. 27.73 BACK = 27.77 AHEAD

US 22

SR 757
SLM 28.59

END WORK LOCATION 1
SOMERSET CORP. SLM 28.73

BEGIN WORK LOCATION 2
SOMERSET CORP. SLM 5.51

0699

SLM 6.18

PER. CO. US 22

END WORK LOCATION 2
SOMERSET CORP. SLM 6.77

* TACK COAT @ 0.25 GAL/SQ.YD.

(I) BRIDGE LENGTH X PAVEMENT WIDTH

PAVEMENT DATA

LOCATION	CO.	ROUTE	LOG POINT TO LOG POINT	LENGTH		WP FEET	TYPICAL	EXISTING PAVEMENT TYPE	PAVEMENT AREA SQ. YDS.	407			448 ASPHALT CONCRETE			857 ASPHALT CONCRETE			254 PAVEMENT PLANING, BITUMINOUS, AS PER PLAN SQ. YDS.	614 TEMPORARY CENTER LINE, CLASS II MILE		
				MILES	LIN. FT.					TACK COAT @ 0.075 gal./s.y. GALS.	TACK COAT FOR INTERMEDIATE COURSE @ 0.05 gal./s.y. GALS.	TACK COAT, 702.13 @ 0.075 gal./s.y. GALS.	THICK INCHES	INTERMEDIATE COURSE, TYPE 1, PG 64-22 CU.YDS.	THICK INCHES	SURFACE COURSE, TYPE 1, PG64-22 WITH SUPPLEMENT 1059 WARRANTY CU.YDS.	THICK INCHES	WITH GILSONITE, INTERMEDIATE COURSE, TYPE 2 CU.YDS.			THICK INCHES	WITH GILSONITE, SURFACE COURSE, TYPE 1 CU.YDS.
1	PER	SR 13	20.38-26.12	5.74	30307	20	1	404	67349	5051	3367		1.0	1870.8	1.0	1870.8					11.48	
			26.12-27.44	1.32	6970	24	1	404	18586	1394	929		1.0	516.3	1.0	516.3					2.70	
			27.44-27.47	0.03	158	24	1	404	421		21	32					1.75	20.5	1.25	14.6	421	0.06
			27.47-27.50	0.03	158	26	3	404	456		23	34					1.75	22.2	1.25	15.8	456	0.06
			27.50-27.73B	0.23	1214	28	2	404	3777		189	283					1.75	183.6	1.25	131.1	3777	0.46
			27.77A-27.92	0.15	792	24	2	404	2112		106	158					1.75	102.7	1.25	73.3	2112	0.30
			27.92-27.97	0.05	264	40	2	404	1173		59	88					1.75	57.0	1.25	40.7	1173	0.10
			27.97-28.17	0.20	1056	30	2	404	3520		176	264					1.75	171.1	1.25	122.2	3520	0.40
			28.17-28.20	0.03	158	36	2	404	632		32	47					1.75	30.7	1.25	21.9	632	0.06
			28.24-28.32	0.08	422	36	2	404	1688		84	127					1.75	82.1	1.25	58.6	1688	0.16
			28.32-28.43	0.11	581	30	2	404	1936		97	145					1.75	94.1	1.25	67.2	1936	0.22
			28.43-28.59	0.16	845	24	2	404	2253		113	169					1.75	109.5	1.25	78.2	2253	0.32
			28.59-28.73	0.14	739	24	1	404	1971			98	148				1.75	95.8	1.25	68.4	1971	0.28
EXTRA TACK COAT FOR LONGITUDINAL JOINT										#172	#44											
BRIDGE DEDUCTIONS (I)									(349)	(26)	(18)		(9.7)	(9.7)						(0.03)		
1	PER	SR 13	TOTALS							6591	5276	1495		2377.4	2377.4		969.3		692.0	19939	16.57	
2	PER	US 22	5.51-5.90	0.39	2059	21	2	404	4804		240	360					1.75	233.5	1.25	166.8	4804	0.78
			5.90-6.00	0.10	502	30	2	404	1673		84	125					1.75	81.3	1.25	58.1	1673	0.20
			6.00-6.16	0.16	845	36	2	404	3380		169	254					1.75	164.3	1.25	117.4	3380	0.32
			6.20-6.31	0.11	581	36	2	404	2324		116	174					1.75	113.0	1.25	80.7	2324	0.22
			6.31-6.40	0.09	475	30	2	404	1584		79	119					1.75	77.0	1.25	55.0	1584	0.18
			6.40-6.72	0.32	1690	24	2	404	4506		225	338					1.75	219.0	1.25	156.4	4506	0.64
			6.72-6.77	0.05	264	24	1	404	704			35	53				1.75	34.2	1.25	24.4	704	0.10
EXTRA TACK COAT FOR LONGITUDINAL JOINT											#46											
2	PER	US 22	TOTALS								948	1423					922.3		658.8	18975	2.44	

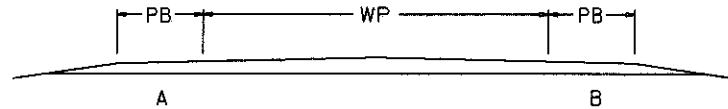
PO130001.MAC 2-23-00

ASPHALT CONCRETE

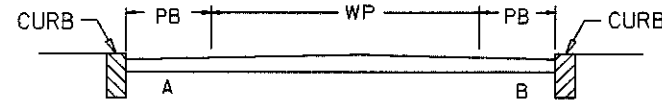
PER-13-20.38
PER-22-5.51

PAVED SHOULDERS

TYPICAL 1



TYPICAL 2



(I) BRIDGE LENGTH X SHOULDER WIDTH

PAVED SHOULDER DATA

LOCATION	COUNTY	ROUTE	LOG POINT TO LOG POINT	LENGTH		TYPICAL	PROPOSED WIDTH (FT.)		SHOULDER AREA SQ.YDS.	407			448 ASPHALT CONCRETE			857 ASPHALT CONCRETE			254	617			
				MILES	LIN.FT.		A	B		TACK COAT @ 0.075 gal./s.y.	TACK COAT, 702.13 @ 0.075 gal./s.y.	TACK COAT FOR INTERMEDIATE COURSE @ 0.05 gal./s.y.	THICK	INTERMEDIATE COURSE, TYPE 1, PG 64-22	THICK	SURFACE COURSE, TYPE 1, PG64-22 WITH SUPPLEMENT 1059 WARRANTY	THICK	WITH GILSONITE, INTERMEDIATE COURSE, TYPE 2	THICK	WITH GILSONITE, SURFACE COURSE, TYPE 1	PAVEMENT PLANING, BITUMINOUS, AS PER PLAN	COMPACTED AGGREGATE, TYPE A, AS PER PLAN	SHOULDER PREPARATION
										GALS.	GALS.	GALS.	INCH	CU.YD.	INCH	CU.YD.	INCH	CU.YD.	INCH	CU.YD.	SQ.YDS.	CU.YDS.	SQ.YDS.
1	PER	SR 13	20.38-27.44	7.06	37277	1	2	2	16568	1242		828	1.0	460.2	1.0	460.2					1151	16568	
			27.44-27.47	0.03	158	1	2	2	70		5	4					1.75	3.4	1.25	2.4	70		
			28.59-28.73	0.14	739	1	4	4	657		49	33					1.75	31.9	1.25	22.8	657		
			BRIDGE DEDUCTIONS (I)						(47.6)	(4)		(2)		(1.3)		(1.3)							
1	PER	SR 13	TOTALS							1238	54	863		458.9		458.9		35.3		25.2	727	1151	16568
2	PER	US 22	6.58-6.72	0.14	739	2	4	4	657		49	33					1.75	31.9	1.25	22.8	657		
			6.72-6.77	0.05	264	1	4	4	235		18	12					1.75	11.4	1.25	8.2	235	8	117
2	PER	US 22	TOTALS								67	45						43.3		31.0	892	8	117

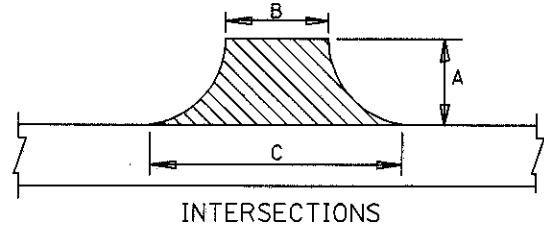
P0130001.MPS 2-23-00

QUANTITIES CARRIED TO GENERAL SUMMARY

PAVED SHOULDERS

PER-13-20.38
PER-22-5.51

EXTRA AREAS



LOCATION	COUNTY	ROUTE	SIDE	DESCRIPTION	INTERSECTIONS			AREA SQ.YD.	407		448 ASPHALT CONCRETE			202 WEARING COURSE REMOVED SQ.YD.	EXISTING SURFACE	
					A IN FEET	B IN FEET	C IN FEET		TACK COAT ● 0.075 gal./s.y. GAL.	TACK COAT FOR INTERMEDIATE COURSE ● 0.05 gal./s.y. GAL.	THICK INCH	INTERMEDIATE COURSE, TYPE 2, PG 64-22 (DRIVEWAYS) CU.YD.	THICK INCH			SURFACE COURSE, TYPE 1, PG 64-22 (DRIVEWAYS) CU.YD.
1	PER	SR 13		CO. RD. 60	65	24	107	473	36			2.0	26.2		ASPH	
				RT	CO. RD. 19	70	20	115	525	39			2.0	29.2		↑
				RT	TWP. RD. 149	50	15	65	222	17			2.0	12.4		
				RT	TWP. RD. 146	25	12	50	86	6			2.0	4.8		
				LT	CO. RD. 60E	25	20	60	111	8			2.0	6.2		
				RT	CO. RD. 5	25	19	50	96	7			2.0	5.4		
				RT	TWP. RD. 121	20	15	40	61	4			2.0	3.4		
				RT	SR 669	20	24	118	158	12			2.0	8.8	158	
				LT	SR 383	50	21	95	322	24			2.0	17.8	322	
				RT	ST. PATRICK'S DR.	35	23	85	210	16			1.0	5.8		
				LT	DR. MIKE CLOUSE DR.	100	15	112	706	53			1.0	19.6		
				LT	SUMMIT ST.	20	20	50	78	6			1.0	2.2		
				LT	SHERIDAN AVE.	20	18	39	63	5			1.0	1.8		
				RT	SHERIDAN AVE.	20	18	40	64	5			1.0	1.8		
				LT	ALLEY	15	15	21	30	2			1.0	0.8		
				RT	GAY ST.	15	19	41	50	4			1.0	1.4		
				LT	GAY ST.	15	18	41	49	4			1.0	1.4		
				LT	NORTH DR.	40	18	60	173	13			2.0	9.6	173	ASPH
1	PER	SR 13		TOTALS					261				158.6	653		
2	PER	US 22		ALLEY	14	13	21	26	2			1.0	0.7		ASPH	
				LT	ALLEY	17	11	21	30	2			1.0	0.8		↑
				RT	@ CHURCH BEFORE SR 668	10	70		78	6			1.0	2.2		
				RT	SR 668	14	24	46	54	4			2.0	3.0	54	
				LT	MARKET ST.	13	24	45	50	4			1.0	1.4		
				RT	ALLEY	16	11	21	28	2			1.0	0.8		
				LT	ALLEY	16	14	20	30	2			1.0	0.8		
				RT	ALLEY	16	11	22	29	2			1.0	0.8		
				LT	ALLEY	13	11	19	22	2			1.0	0.6		
				RT	ALLEY	17	11	20	29	2			1.0	0.8		
				LT	ALLEY	14	11	20	24	2			1.0	0.7		
				RT	ALLEY	18	10	26	36	3			1.0	1.0		
				LT	ALLEY	14	14	22	28	2			1.0	0.8		
				LT	N. HIGH ST.	16	29	68	86	6			1.0	2.4		
	RT	S. HIGH ST.	16	13	37	44	3			1.0	1.2					
	RT	SHERIDAN ST.	47	21	81	266	20			1.0	7.4					
	LT	SR 668	44	20	80	244	18			2.0	13.6	244	ASPH			
2	PER	US 22		TOTALS					82				39.0	298		

P0130001.MEA 2-23-00

EXTRA AREAS

PER-13-20.38
PER-22-5.51


PER-13-2097: BUTT JOINT AT APPROACH SLABS
PER-13-2510: REMOVE AND REPLACE 2.0" ASPHALT CONCRETE

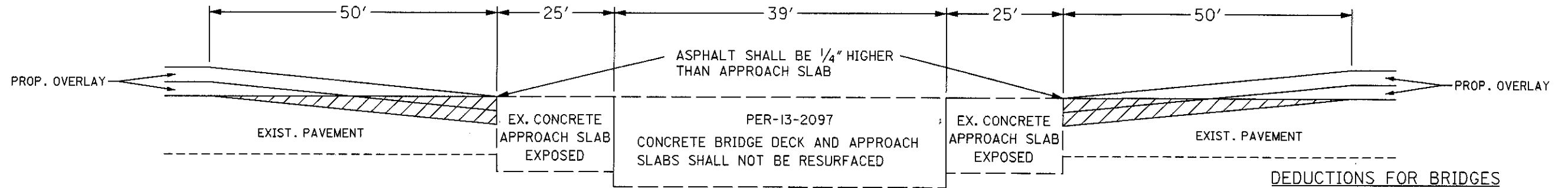
BRIDGE DECK TREATMENT

L O C A T I O N	C O U N T Y, R O U T E, B R I D G E N O.	L E N G T H (B R I D G E L I M I T S)	W I D T H	B R I D G E D E C K A R E S Q. Y D S.	B R I D G E D E C K D A T A												
					W E A R I N G C O U R S E R E M O V E D D E P T H V A R." S Q. Y D S.	B R I D G E D E C K R E P A I R			4 0 7		4 4 8 A S P H A L T C O N C R E T E					S P E C I A L	
						<input type="checkbox"/> SS-845 LATEX MODIFIED CONCRETE <input type="checkbox"/> SS-850 DENSE CONCRETE			TACK COAT	TACK COAT FOR INTERMEDIATE COURSE	I N T E R M E D I A T E C O U R S E, T Y P E I, P G 6 4 - 2 2	S U R F A C E C O U R S E, T Y P E I, P G 6 4 - 2 2 W I T H S U P P L E M E N T I 0 5 9 W A R R A N T Y	D E C K W A T E R P R O O F I N G				
						" THICK OVERLAY S Q. Y D S.	VARIABLE THICKNESS OVERLAY C U. Y D S.	FULL-DEPTH REPAIR C U. Y D S.	@ 0.075 GAL./S.Y. G A L.	@ 0.05 GAL./S.Y. G A L.			T H I C K I N C H	T H I C K I N C H	MEMBRANE WATERPROOFING SHEET TYPE 3 S Q. Y D S.	MEMBRANE WATERPROOFING S Q. Y D S.	
I	PER-13-2097	39	44	191	267	SEE DETAIL ① SHT.18											
	PER-13-2510	68	40	302	569	SEE DETAIL ② SHT. 18			23	15	1.0	8.4	1.0	8.4			
I	TOTAL				836				23	15	1.0	8.4	1.0	8.4			

NOTE: ALL WEARING COURSE REMOVED AND PAVEMENT TAPERS SHALL BE AT A RATE OF 25'/1"

DETAIL ①

 WEARING COURSE REMOVED



202 WEARING COURSE REMOVED:
 PER-13-2097
 $[2(50' \times 24')] / 9 = 267 \text{ SQ.YD.}$

QUANTITY CARRIED TO SHEET 17

DEDUCTIONS FOR BRIDGES
 APPROACH SLABS ADDED TO LENGTH
 $89' \times 20' \div 9 = 198 \text{ SQ.YD.}$


ITEM 448 ASPHALT CONCRETE SURFACE COURSE
 $198 \text{ S.Y.} \times 1''/36 = 5.5 \text{ CU.YD.}$
 ITEM 448 ASPHALT CONCRETE INTERMED. COURSE
 $198 \text{ S.Y.} \times 1''/36 = 5.5 \text{ CU.YD.}$
 ITEM 407 TACK COAT
 $198 \text{ S.Y.} \times 0.075 \text{ GAL/S.Y.} = 15 \text{ GAL.}$
 ITEM 407 TACK COAT FOR INTERMEDIATE COURSE
 $198 \text{ S.Y.} \times 0.05 \text{ GAL/S.Y.} = 10 \text{ GAL.}$

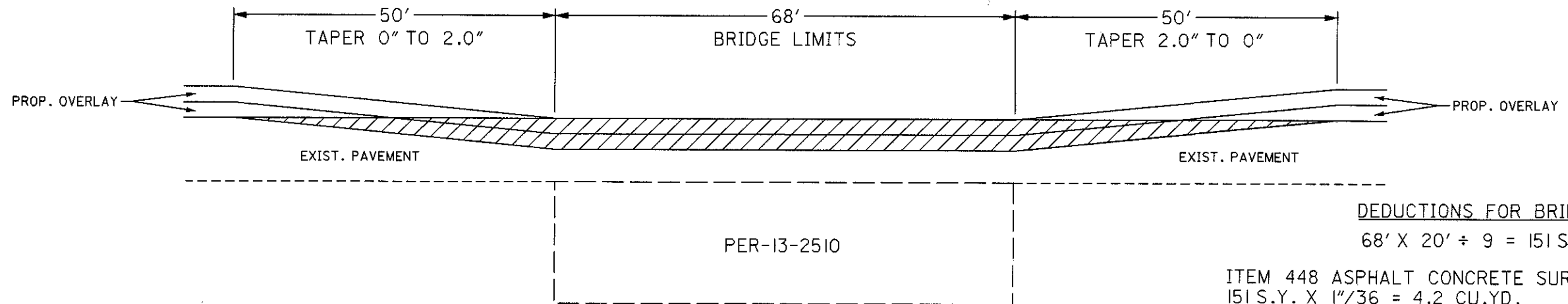
DEDUCTIONS CARRIED TO SHEET 14

BUTT JOINT @ APPROACH SLABS

NOTE: ALL WEARING COURSE REMOVED AND PAVEMENT TAPERS SHALL BE AT A RATE OF 25'/1"

DETAIL ②

 WEARING COURSE REMOVED



202 WEARING COURSE REMOVED:
 PER-13-2510
 $[2(50' \times 24') + (68' \times 40')] / 9 = 569 \text{ SQ.YD.}$

QUANTITY CARRIED TO SHEET 17

DEDUCTIONS FOR BRIDGES
 $68' \times 20' \div 9 = 151 \text{ SQ.YD.}$

ITEM 448 ASPHALT CONCRETE SURFACE COURSE
 $151 \text{ S.Y.} \times 1''/36 = 4.2 \text{ CU.YD.}$
 ITEM 448 ASPHALT CONCRETE INTERMED. COURSE
 $151 \text{ S.Y.} \times 1''/36 = 4.2 \text{ CU.YD.}$
 ITEM 407 TACK COAT
 $151 \text{ S.Y.} \times 0.075 \text{ GAL/S.Y.} = 11 \text{ GAL.}$
 ITEM 407 TACK COAT FOR INTERMEDIATE COURSE
 $151 \text{ S.Y.} \times 0.05 \text{ GAL/S.Y.} = 8 \text{ GAL.}$

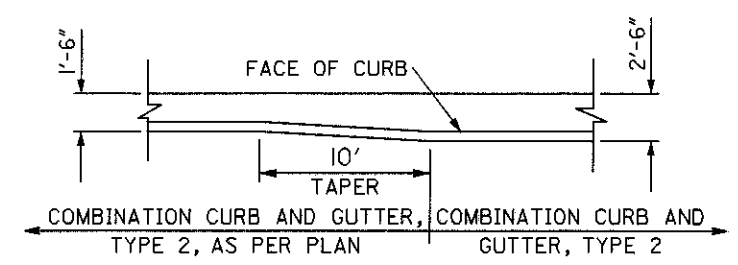
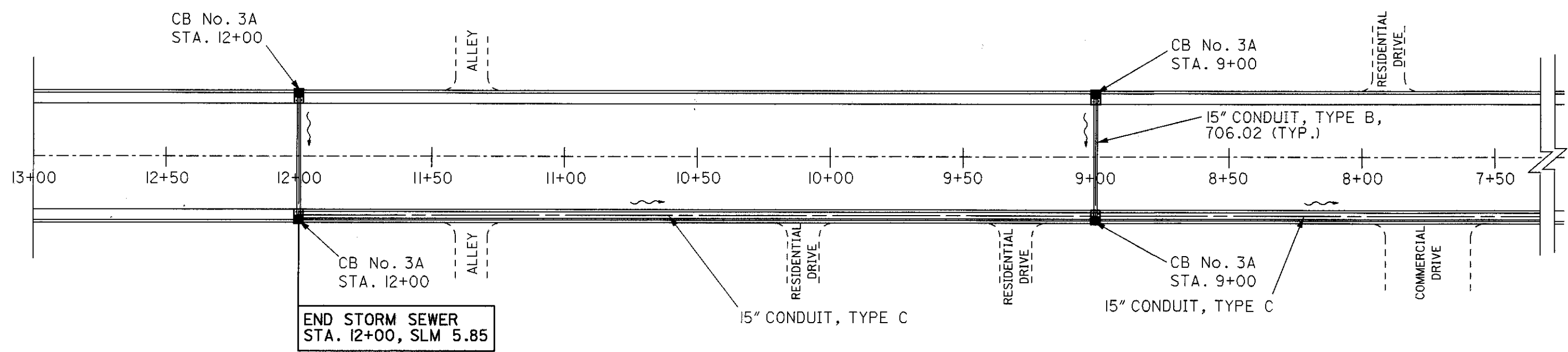
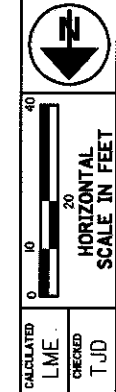
DEDUCTIONS CARRIED TO SHEET 14

REMOVE AND REPLACE 2.0" ASPHALT CONCRETE

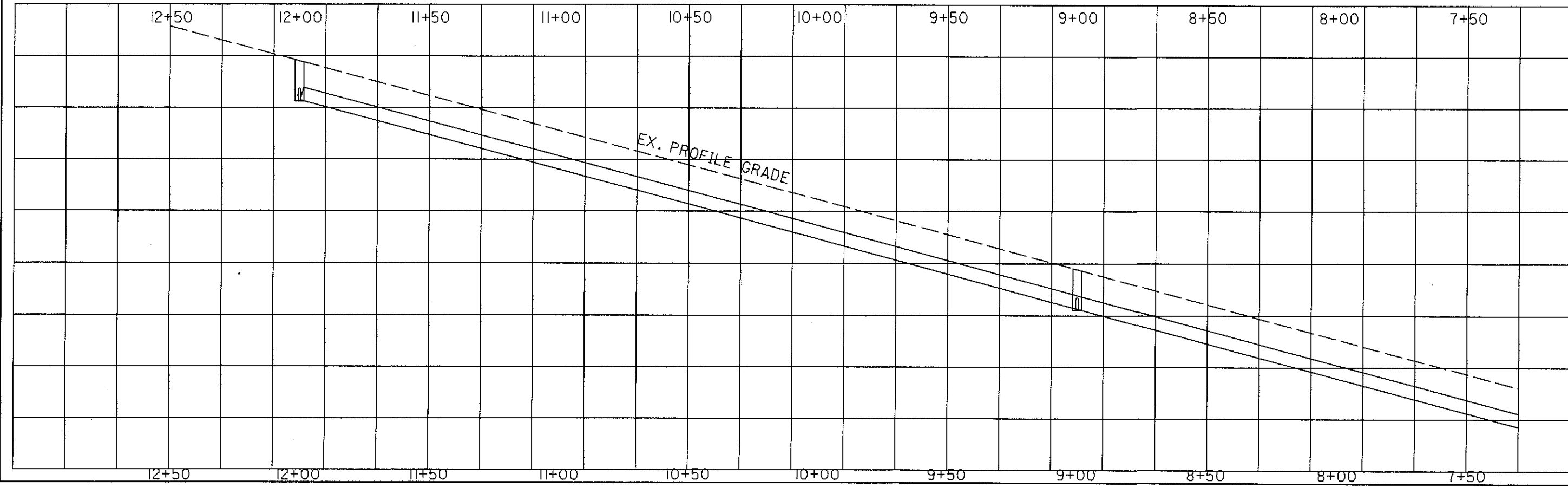
PO130002.MBT 2-23-00

BRIDGE DECK DETAILS

PER-13-20.38
 PER-22-5.51



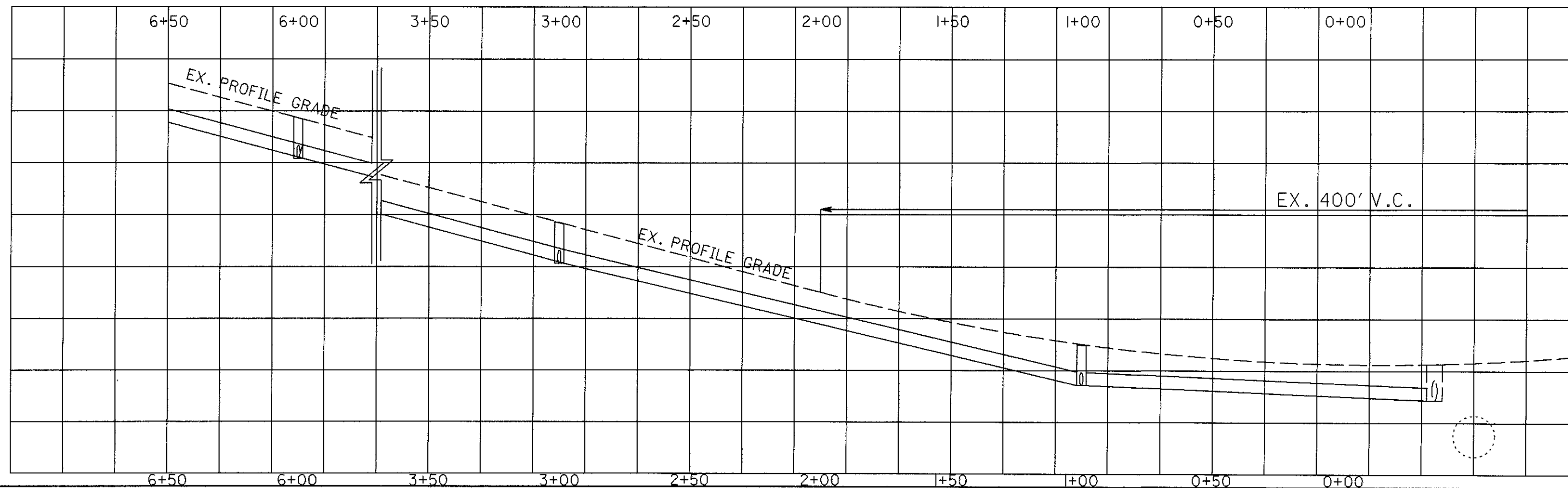
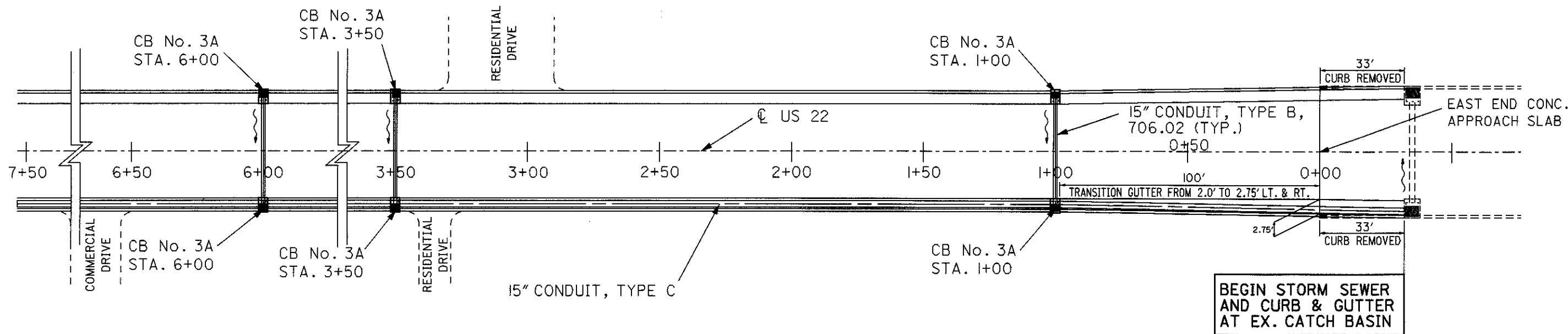
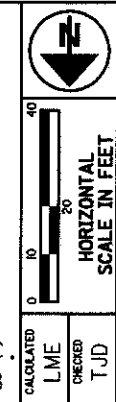
CURB AND GUTTER TRANSITION



PLAN AND PROFILE

PER-13-20.38
PER-22-5.51

VERTICAL SCALE = 2:1



PLAN AND PROFILE

PER-13-20.38
PER-22-5.51

LOCATION SUB-SUMMARY

CALC. BY SAB
 DATE 9-28-99
 CHKD. BY TD
 DATE 11-1-99

DETAIL	
1	TAPERED ACCELERATION LANE
2	DECELERATION LANE
3	MULTILANE DIVIDED/ CONTROLLED ACCESS

DETAIL	
4	4 LANE DIVIDED TO 2 LANE TRANSITION
5	4 LANE UNDIVIDED TO 2 LANE TRANSITION
6	ONE LANE BRIDGE
7	STOP APPROACH
8	THRU APPROACH
9	TWO WAY LEFT TURN LANE

DETAIL	
10	APPROACH W/LT. TURN LANE
11	HORIZONTAL CURVE 40' (NOTE 2)
12	HORIZONTAL CURVE ALT. (NOTE 3)
GAP	CENTERLINE AT 80' TYP.

LOCATION NUMBER	LOCATION				DETAIL	FED.	100% STATE	ITEM QUANTITIES				PRISMATIC RETRO-REFLECTOR COLORS					REMARKS		
	COUNTY	ROUTE	S.L.M. SECTION					INSTALLATION ONLY			PRISMATIC RETRO-REFLECTOR	ONE-WAY		TWO-WAY					
			FROM	TO				RPM	RPM CASTING	PRISMATIC RETRO-REFLECTOR		WHITE	YELLOW	YELLOW/YELLOW	WHITE/RED	YELLOW/RED			
I	PER	SR 13	20.38	20.91	GAP			35							35				START NEW LEXINGTON CORP.
I	PER	SR 13	20.91	20.96	II			7							7				PC 20.91 PT 20.96 L=264' DEG 6
I	PER	SR 13	20.96	21.19	I2			47							47				PC 20.98 PT 21.10 L=634' DEG 9
I	PER	SR 13	21.19	23.05	GAP			123							123				
I	PER	SR 13	23.05	23.10	II			7							7				PC 23.05 PT 23.10 L=264' DEG 8
I	PER	SR 13	23.10	23.18	GAP			5							5				
I	PER	SR 13	23.18	23.42	I2			40							40				PC 23.27 PT 23.30 L=317' DEG 15
I	PER	SR 13	23.42	23.52	GAP			7							7				
I	PER	SR 13	23.52	23.58	II			8							8				PC 23.52 PT 23.58 L=317' DEG 8
I	PER	SR 13	23.58	24.33	GAP			50							50				
I	PER	SR 13	24.33	24.40	II			9							9				PC 24.33 PT 24.40 L=370' DEG 7
I	PER	SR 13	24.40	27.44	GAP			201							201				END SOMERSET CORP.
TOTAL LOCATION I								539							539				

P0130001.trm 02-23-00

RPM LOCATION SUB-SUMMARY

PER-13-20.38
 PER-22-5.51

CENTER LINE SUB-SUMMARY

QUANTITIES INCLUDE CL AROUND OUTSIDE OF PAINTED ISLAND

LOCATION	COUNTY	ROUTE	S.L.M.		CENTER LINES QUANTITIES		PARTICIPATION TYPE				TOTAL CENTER LINE MILES	REMARKS
			FROM	TO	TOTAL MILES	EQUIVALENT SOLID LINE	IRG	FG	RSG	NON FED STATE		
1	PER	SR 13	20.38	28.73	8.35	12.986					8.35	NEW LEXINGTON N. CORP. TO SOMERSET N. CORP.
2	PER	US 22	5.51	6.77	1.26	2.420					1.26	SOMERSET W. CORP. TO SOMERSET E. CORP.

EDGE LINE SUB-SUMMARY

LOCATION	COUNTY	ROUTE	S.L.M.		WHITE EDGE LINE QU.		YELLOW EDGE LINE QU.			PARTICIPATION TYPE				EDGE LINE TOTAL MILES	REMARKS
			FROM	TO	TOTAL MILES	HIGHWAY	RAMP	TOTAL MILES	HIGHWAY	RAMP	IRG	FG	RSG		
1	PER	SR 13	20.38	27.47	14.18	14.18								14.18	NEW LEXINGTON N. CORP. TO BEGIN CURB IN SOMERSET END CURB TO SOMERSET N. CORP.
			28.59	28.73	0.28	0.28								0.28	
1		TOTALS												14.46	
2	PER	US 22	6.56	6.77	0.42	0.42								0.42	

TOTALS CARRIED TO GENERAL SUMMARY

PO130001.TCL 2-23-00

CALCULATED
LME
CHECKED
TJD

CENTER/EDGE LINE SUB-SUMMARY

PER-13-20.38
PER-22-5.51

22
27

PAVEMENT MARKING SUB-SUMMARY

CALCULATED
LME
CHECKED
TJD

LOCATION	COUNTY	ROUTE	LOG POINT TO LOG POINT	644 THERMOPLASTIC														REMARKS		
				SIDE	24" TRANSVERSE LINES		STOP LINE	12" CROSSWALK LINES	8" CROSSWALK LINES	WORD ON PAVEMENT				LANE ARROWS			R/R SYMBOL		PAINTED ISLAND	8" CHANNEL LINE
					WHITE	YELLOW	24"	WHITE	WHITE	ONLY		SCHOOL		TURN						
					LIN.FT.	LIN.FT.	LIN.FT.	LIN.FT.	LIN.FT.	72"	96"	72"	96"	LEFT	RIGHT	COMB				
I	PER	SR 13	CO. RD. 60	LT			28												PLACE 19' FROM SR 13 C	
			CO. RD. 19	RT			37												PLACE 20' FROM SR 13 C	
			TWP. RD. 149	RT			25												PLACE 16' FROM SR 13 C	
			TWP. RD. 146	RT			14												PLACE 16' FROM SR 13 C	
			CO. RD. 60E	LT			18												PLACE 17' FROM SR 13 C	
			CO. RD. 5	RT			15												PLACE 17' FROM SR 13 C	
			TWP. RD. 121	RT			13												PLACE 18' FROM SR 13 C	
			SR 669	RT			25												PLACE 25' FROM SR 13 C	
			SR 383	LT			26												PLACE 23' FROM SR 13 C	
			ST. PATRICK'S DR.	RT			30												PLACE 20' FROM SR 13 C	
			DR. MIKE CLOUSE DR.	LT			48												PLACE 22' FROM SR 13 C	
			ON SR 13	LT															PLACE AS DIRECTED	
			SUMMIT ST.	LT				54											PLACE AS DIRECTED	
			ON SR 13 @ CHURCH/SCHOOL	RT			40	80											PLACE AS DIRECTED	
			ON SR 13	LT															PLACE AS DIRECTED	
			SHERIDAN AVE.	RT				38											PLACE AS DIRECTED	
			SHERIDAN AVE.	LT				36											PLACE AS DIRECTED	
			ALLEY	RT				30											PLACE AS DIRECTED	
			ON SR 13 @ SQUARE (SOUTH)					84											PLACE AS DIRECTED	
			ON SR 13 @ SQUARE NORTH()					84											PLACE AS DIRECTED	
			GAY ST.					44											PLACE AS DIRECTED	
			GAY ST.					40											PLACE AS DIRECTED	
			NORTH DR.				17												PLACE 20' FROM SR 13 C	
I	PER	SR 13	TOTALS				336	490												

PAVEMENT MARKING

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PER-13-20.38
PER-22-5.51

23
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PAVEMENT MARKING SUB-SUMMARY

CALCULATED
LME
CHECKED
TJD

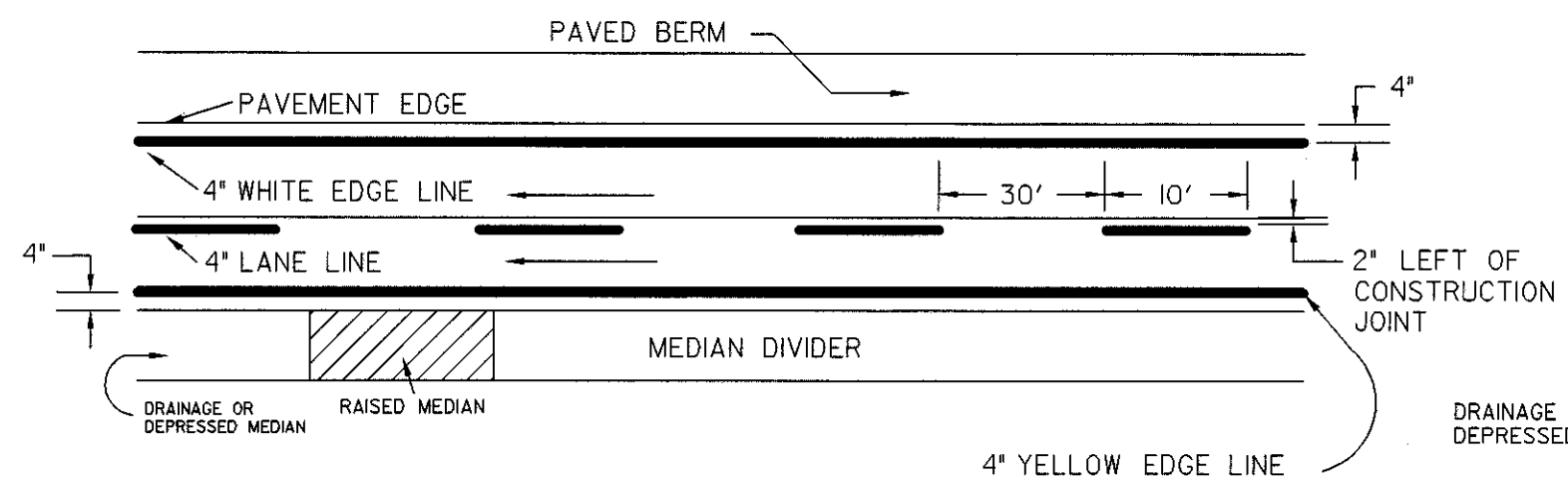
LOCATION	COUNTY	ROUTE	LOG POINT TO LOG POINT	644 THERMOPLASTIC														REMARKS		
				SIDE	24" TRANSVERSE LINES		STOP LINE	12" CROSSWALK LINES	8" CROSSWALK LINES	WORD ON PAVEMENT				LANE ARROWS			R/R SYMBOL		PAINTED ISLAND	8" CHANNEL LINE
					WHITE	YELLOW	24"	WHITE	WHITE	ONLY		SCHOOL		TURN						
					LIN.FT.	LIN.FT.	LIN.FT.	LIN.FT.	LIN.FT.	72"	96"	72"	96"	LEFT	RIGHT	COMB				
2	PER	US 22	IN SOMERSET																	
			ALLEY	RT				26												PLACE AS DIRECTED
			ALLEY	LT				22												PLACE AS DIRECTED
			SR 668	RT				58												PLACE AS DIRECTED
			MARKET ST.	LT				50												PLACE AS DIRECTED
			ALLEY	RT				22												PLACE AS DIRECTED
			ALLEY	LT				28												PLACE AS DIRECTED
			ALLEY	RT				22												PLACE AS DIRECTED
			ALLEY	LT				22												PLACE AS DIRECTED
			ON US 22 @ SQUARE (WEST)					80												PLACE AS DIRECTED
			ON US 22 @ SQUARE (EAST)					74												PLACE AS DIRECTED
			ALLEY	RT				22												PLACE AS DIRECTED
			ALLEY	LT				22												PLACE AS DIRECTED
			ALLEY	RT				20												PLACE AS DIRECTED
			ALLEY	LT				28												PLACE AS DIRECTED
			ON US 22 @ SLM 6.27	CL							1									PLACE AS DIRECTED
			ON US 22 BEFORE HIGH ST.	LT			10	60												PLACE AS DIRECTED
			N. HIGH ST.	LT			13	64												PLACE AS DIRECTED
			ALLEY	RT				38												PLACE AS DIRECTED
			ON US 22 AFTER HIGH ST.	RT			13	76												PLACE AS DIRECTED
			SHERIDAN ST.	RT			53													PLACE AS DIRECTED
			ON US 22 @ SLM 6.57	CL							1									PLACE AS DIRECTED
			SR 668	LT			12													PLACE 14' FROM SR 13 E/P
2	PER	US 22	TOTALS				101	734			2									

PAVEMENT MARKING

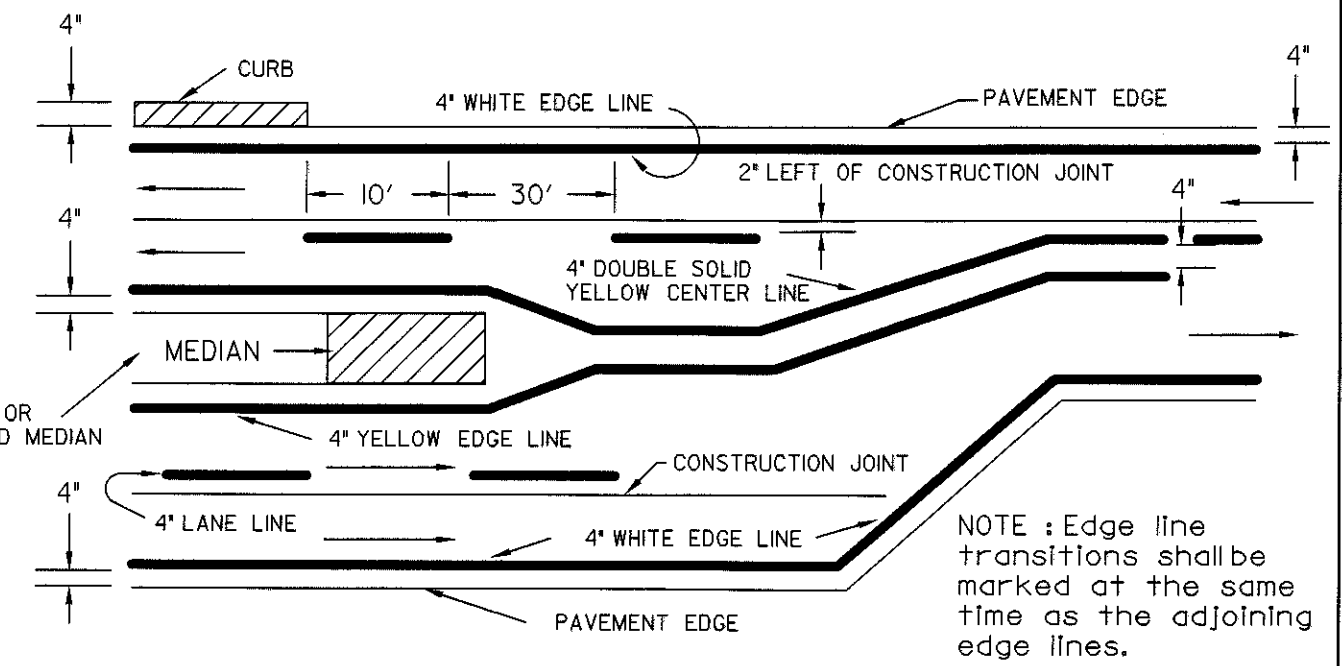
P0130002.TAS 2-23-00

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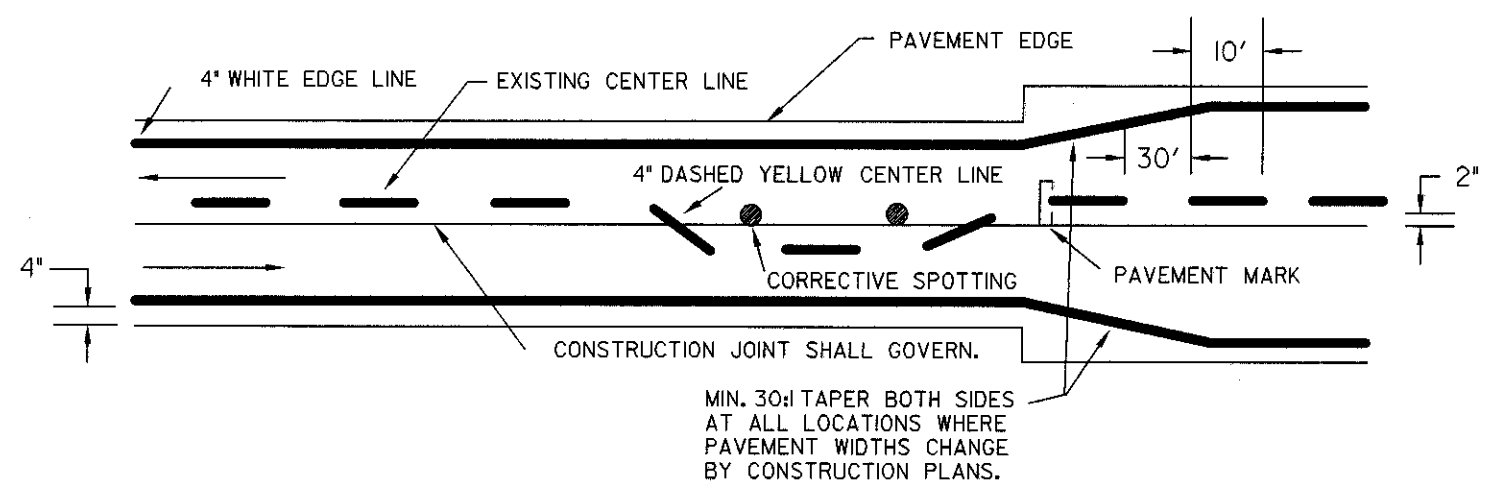
FREEWAY & EXPRESSWAY MAINLINE MARKINGS



MULTILANE DIVIDED & UNDIVIDED HIGHWAY MARKINGS



TWO LANE MARKINGS



NOTES:

1. The distance from the pavement edge to the nearside edge of the edgeline may be increased with the approval of the engineer in order to maintain uniform lane width.
2. See TC-72.20 for entrance and exit ramp markings.
3. The cycle length for dashed lines shall be 40 feet plus or minus 6 inches. The minimum length of dash shall be sufficiently long to maintain a 3:1 ratio between length of gap and length of dash.

Ohio Department of Transportation
Pavement Marking
Typical Details

DATE
1-80
9-86
9-91

PER-13-20.38
PER-22-5.51

PAVEMENT MARKING TYPICALS

GENERAL SUMMARY

CALCULATED
LME
CHECKED
TJD

LOCATION 1 (SHEET TOTALS)											LOCATION 2 (SHEET TOTALS)										ITEM	ITEM EXT. NO.	TOTAL	UNIT	DESCRIPTION
5	6	7	8	9	10	13	14	15	16	17	4	5	6	7	8	10	14	15	16						
																					201	11000		LUMP	CLEARING AND GRUBBING
	133								653											298	202	23500	1084	SQ.YD.	WEARING COURSE REMOVED
2000																					202	23501	2000	SQ.YD.	WEARING COURSE REMOVED, AS PER PLAN
															276						202	30000	276	SQ.FT.	WALK REMOVED
						116															202	32000	116	LIN.FT.	CURB REMOVED
	9300														8300						202	32500	17600	LIN. FT.	CURB AND GUTTER REMOVED
				750																	202	38101	750	LIN.FT.	GUARDRAIL REMOVED FOR STORAGE, AS PER PLAN
	701																				202	54101	701	EACH	RAISED PAVEMENT MARKER REMOVED FOR STORAGE, AS PER PLAN
4600												50									253	01001	4650	SQ.YD.	PAVEMENT REPAIR, AS PER PLAN
						2828	19939	727									18975	892			254	01001	43361	SQ.YD.	PAVEMENT PLANING, BITUMINOUS, AS PER PLAN
1000												1000									254	01600	2000	SQ.YD.	PATCHING PLANED SURFACE
												50									304	20001	50	CU.YD.	AGGREGATE BASE, AS PER PLAN
	9					6591	1292	261	23				9					67	82	407	10000	8334	GALLON	TACK COAT	
						212	1495										1423			407	13900	3130	GALLON	TACK COAT, 702.13	
						141	5276	863		15						948	45			407	14000	7288	GALLON	TACK COAT FOR INTERMEDIATE COURSE	
62	200					2377	459		8											448	46020	3106	CU.YD.	ASPHALT CONCRETE INTERMEDATE COURSE, TYPE I, PG 64-22	
										50										448	47021	50	CU.YD.	ASPHALT CONCRETE SURFACE COURSE,TYPE I,PG 64-22,AS PER PLAN	
62						2377	459		8											448	47022	2906	CU.YD.	ASPHALT CONCRETE SURFACE COURSE, TYPE I, PG 64-22 WITH SUPPLEMENT 1059 WARRANTY	
177								159		33									39	448	48020	408	CU.YD.	ASPHALT CONCRETE SURFACE COURSE, TYPE I, PG 64-22 (DRIVEWAYS)	
						98	692	25									659	31		857	10000	1505	CU.YD.	ASPHALT CONCRETE WITH GILSONITE, SURFACE COURSE, TYPE I	
						138	969	35									922	43		857	20000	2107	CU.YD.	ASPHALT CONCRETE WITH GILSONITE, INTERMEDIATE COURSE, TYPE 2	
														100						603	05900	100	LIN.FT.	15" CONDUIT, TYPE B (706.02)	
														1235						603	06100	1235	LIN.FT.	15" CONDUIT, TYPE C	
														10						604	00800	10	EACH	CATCH BASIN, NO. 3A	
	10																			604	09000	10	EACH	CATCH BASIN ADJUSTED TO GRADE	
															1					604	09501	1	EACH	CATCH BASIN RECONSTRUCTED TO GRADE, AS PER PLAN	
			7												5					604	20601	12	EACH	INLET ADJUSTED TO GRADE, AS PER PLAN	
200																				605	31100	200	LIN.FT.	AGGREGATE DRAIN	
				800																606	13030	800	LIN.FT.	GUARDRAIL, TYPE 5, USING 9 FOOT POSTS	
				2																606	26500	2	EACH	ANCHOR ASSEMBLY, TYPE T	
				800																SPECIAL	60650000	800	LIN.FT.	RESHAPING BERM	
															96					608	53000	96	SQ.FT.	CURB RAMP, TYPE 1	
															96					608	54000	96	SQ.FT.	CURB RAMP, TYPE 2	

GENERAL SUMMARY

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GENERAL SUMMARY

LOCATION 1 (SHEET TOTALS)									LOCATION 2 (SHEET TOTALS)							ITEM	ITEM EXT. NO.	TOTAL	UNIT	DESCRIPTION	
7	9	10	13	14	15	21	22	23	7	9	10	14	15	22	24						
	85									10						614	12460	95	EACH	WORK ZONE MARKING SIGN	
	3									3						614	13000	6	CU.YD.	BITUMINOUS CONCRETE FOR MAINTAINING TRAFFIC	
				16.57								2.44				614	21400	19.01	MILE	TEMPORARY CENTER LINE, CLASS II	
					1151								8			617	10101	1159	CU.YD.	COMPACTED AGGREGATE, TYPE A, AS PER PLAN	
					16568								117			617	20000	16685	SQ.YD.	SHOULDER PREPARATION	
						539										621	00200	539	EACH	RAISED PAVEMENT MARKER, INSTALLATION ONLY	
	7									3						638	10800	10	EACH	VALVE BOX ADJUSTED TO GRADE	
							14.46							0.42		642	00100	14.88	MILE	EDGE LINE, TYPE I	
							8.35							1.26		642	00300	9.61	MILE	CENTERLINE TYPE I	
								336							101	644	00500	437	LIN. FT.	STOP LINE	
								490							734	644	00600	1224	LIN. FT.	CROSSWALK LINE	
								2							2	644	01100	4	LIN. FT.	SCHOOL SYMBOL MARKING, 72"	
		6									1					SPECIAL	69050100	7	EACH	MAILBOX SUPPORT SYSTEM, SINGLE	
			116													830	28000	116	LIN.FT.	CURB, TYPE 7	
6300									6666							830	12000	12966	LIN.FT.	COMBINATION CURB AND GUTTER TYPE 2	
3000									1700							830	12001	4700	LIN.FT.	COMBINATION CURB AND GUTTER TYPE 2, AS PER PLAN	
2100									2000							870	10000	4100	SQ.YD.	SEEDING AND MULCHING	
																614	11000		LUMP	MAINTAINING TRAFFIC	
																623	10000		LUMP	CONSTRUCTION LAYOUT STAKES	
																624	10000		LUMP	MOBILIZATION	
																806	16010	3	MONTH	FIELD OFFICE, TYPE B	

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GENERAL SUMMARY

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