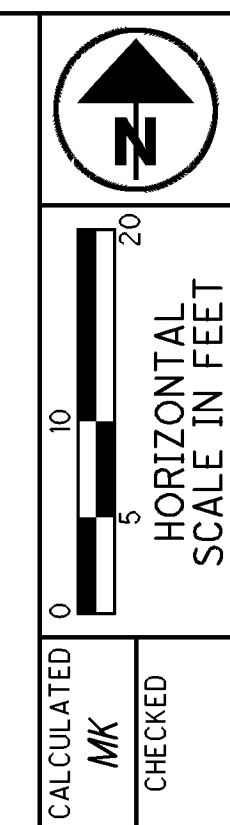
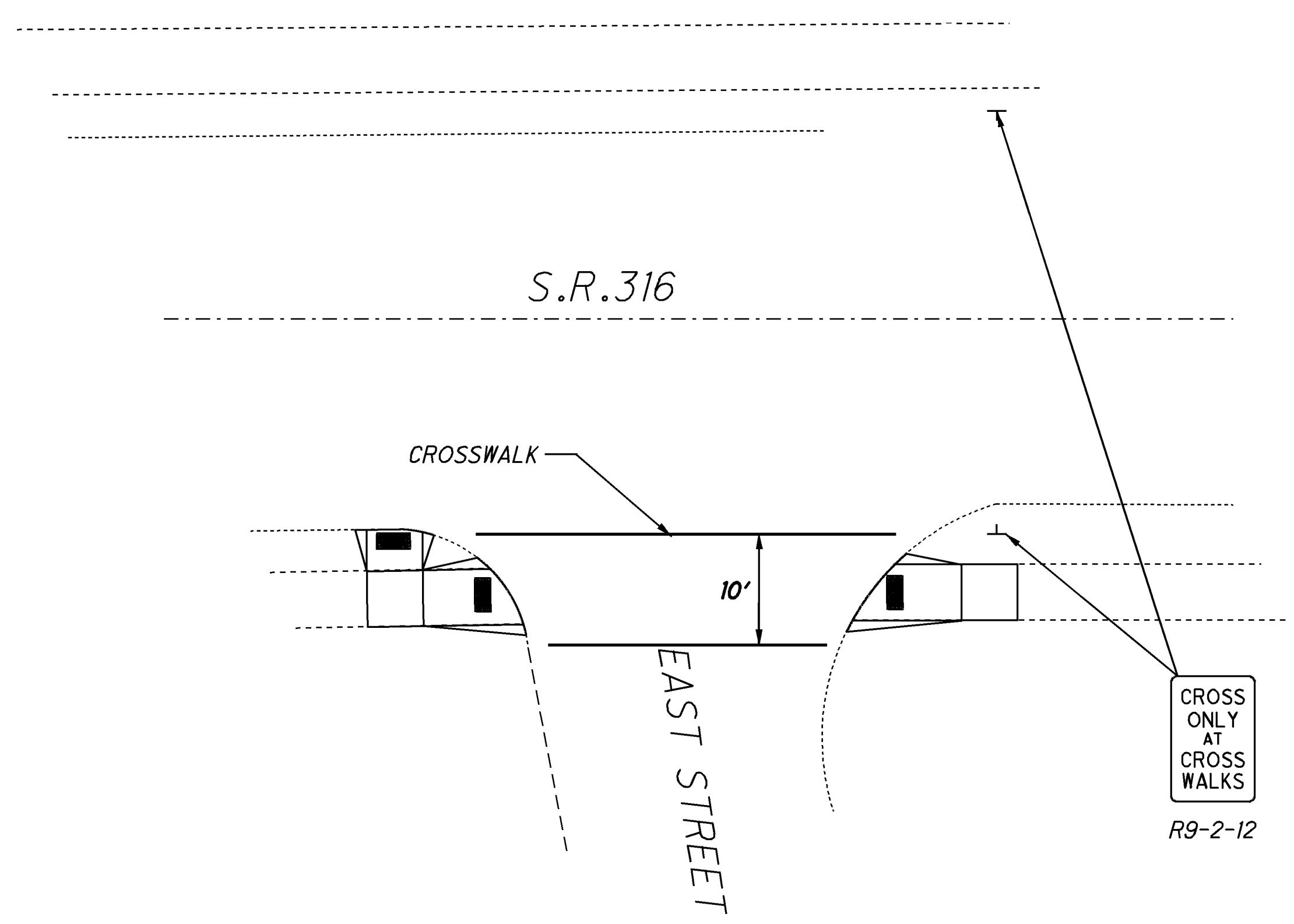
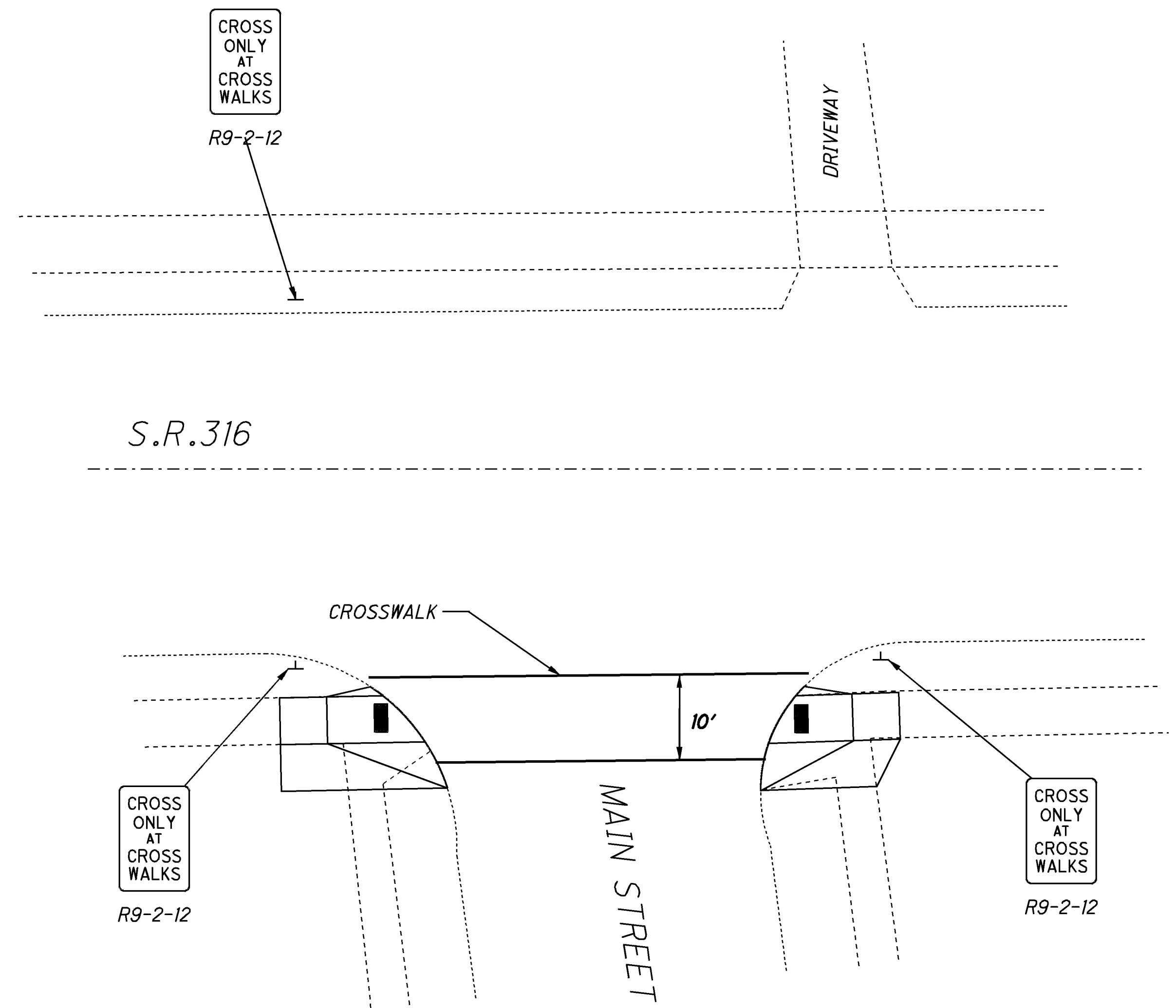


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**CURB RAMP LOCATIONS
VILLAGE OF DARBYVILLE**

LOCATION				QUANTITIES							REMARKS
L O C A T I O N	C O U N T Y	R O U T E	S L M	202		608			630		
				WALK REMOVED SQ FT	CURB REMOVED FT	4" CONCRETE WALK SQ FT	CURB RAMP, TYPE A1 SQ FT	CURB RAMP, TYPE A2 SQ FT	GROUND MOUNTED SUPPORT, NO. 2 POST FT	SIGN, FLAT SHEET SQ FT	
	PIC	316	3.91	102	14	57	98		24	3	MAIN STREET SW CORNER
	PIC	316	3.91	95	13	44	70		12	1.5	MAIN STREET SE CORNER
	PIC	316	3.97	64	9			93			EAST STREET SW CORNER
	PIC	316	3.97	66	9			74	12	1.5	EAST STREET SE CORNER
									12	1.5	EAST STREET NE CORNER
TOTALS CARRIED TO GENERAL SUMMARY				327	45	101	168	167	60	7.5	

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LONDON ROAD
CR 3

SR 316

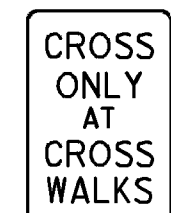
WEST STREET

CALCULATED
MK
CHECKED

HORIZONTAL
SCALE IN FEET

**CURB RAMP LOCATIONS
VILLAGE OF DARBYVILLE**

LOCATION				QUANTITIES							REMARKS
L O C A T I O N	C O U N T Y	R O U T E	S L M	202		608			630		
				WALK REMOVED	CURB REMOVED	4" CONCRETE WALK	CURB RAMP, TYPE A1	CURB RAMP, TYPE A2	GROUND MOUNTED SUPPORT, NO. 2 POST	SIGN, FLAT SHEET	
				SQ FT	FT	SQ FT	SQ FT	SQ FT	FT	SQ FT	
	PIC	316	3.75	32	10		38				LONDON ROAD NE CORNER
	PIC	316	3.86	25	7			57	12	1.5	WEST STREET
TOTALS CARRIED TO GENERAL SUMMARY				57	17		38	57	12	1.5	



R9-2-12

ON NE CORNER OF WEST STREET
(NOT SHOWN)

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NOTIFICATION OF CONSTRUCTION INITIATION:

AT LEAST FOURTEEN DAYS PRIOR TO ANY CONSTRUCTION ACTIVITIES, THE CONTRACTOR SHALL ADVISE THE DISTRICT OFFICE OF COMMUNICATIONS VIA EMAIL AT d06.pio@dot.state.oh.us AND THE DISTRICT WORK ZONE TRAFFIC MANAGER VIA EMAIL AT d06.mot@dot.state.oh.us OF THE ANTICIPATED START DATE OF ANY CONSTRUCTION ACTIVITIES, INCLUDING BUT NOT LIMITED TO THE PLACING OF WORK ZONE SIGNS. THE NOTIFICATION SHALL ALSO INCLUDE THE PROJECT NUMBER, PID, NAME AND PHONE NUMBER OF THE CONTRACTOR, A POINT OF CONTACT AND THE ANTICIPATED IMPACT ON TRAFFIC. THE CONTRACTOR WILL IMMEDIATELY INFORM THE DISTRICT OFFICE OF COMMUNICATIONS AND THE DISTRICT WORK ZONE TRAFFIC MANAGER OF ANY AND ALL DELAYS AND/OR CHANGES REGARDING THE CONSTRUCTION INITIATION DATE.

GENERAL:

THE CONTRACTOR SHALL SUBMIT IN WRITING A SCHEDULE OF OPERATIONS TO THE ENGINEER (SEE 108.02) AND RECEIVE APPROVAL IN WRITING BEFORE WORK IS STARTED ON THIS PROJECT. ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED, MAINTAINED, AND REMOVED BY THE CONTRACTOR IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

ALIGNMENT AND PROFILE:

THE WORK PROPOSED BY THIS PROJECT IS FOR THE RESURFACING OF THE EXISTING PAVEMENT. THE ALIGNMENT OF THE EXISTING PAVEMENT WILL NOT BE CHANGED, AND THE PROFILE OF THE PROPOSED SURFACE WILL BE SIMILAR TO THAT OF THE EXISTING PAVEMENT EXCEPT THAT IT MAY BE RAISED AN AMOUNT EQUAL TO THE THICKNESS OF THE RESURFACING COURSE SPECIFIED IN THESE PLANS.

CONTRACTORS EQUIPMENT - OPERATION AND STORAGE:

THE CONTRACTOR'S EQUIPMENT SHALL BE OPERATED IN THE DIRECTION OF TRAFFIC WHERE PRACTICAL. EQUIPMENT SHALL HAVE AT LEAST ONE AMBER FLASHING LIGHT. WHEN PARKED ALONG THE HIGHWAY, THE EQUIPMENT SHALL BE LOCATED EITHER A MINIMUM OF THIRTY FEET FROM THE EDGE OF PAVEMENT OR SIX FEET BEHIND GUARDRAIL WITH A MINIMUM OF 125 FEET OF GUARDRAIL PRECEDING THE EQUIPMENT. ALL OTHER EQUIPMENT, INCLUDING PRIVATE VEHICLES, SHALL BE STORED AT AN APPROVED CONTRACTOR'S STORAGE AREA.

CONTINGENCY QUANTITIES:

THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED "AS DIRECTED BY THE ENGINEER" UNLESS AUTHORIZED BY THE ENGINEER. THE ACTUAL WORK LOCATIONS AND QUANTITIES USED FOR SUCH ITEMS SHALL BE INCORPORATED INTO THE FINAL CHANGE ORDER GOVERNING COMPLETION OF THIS PROJECT.

REMOVAL ITEMS:

ASPHALT AND MISCELLANEOUS HARDWARE DESIGNATED FOR REMOVAL BECOME PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OF. PAYMENT FOR THE ABOVE SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE REMOVED ITEM.

WORK LIMITS:

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. THE INSTALLATION AND OPERATION OF ALL TEMPORARY TRAFFIC CONTROL AND TEMPORARY TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS SHALL BE PROVIDED BY THE CONTRACTOR WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

UNDERGROUND UTILITIES:

THE IDENTITY OF SOME OF THE EXISTING UNDERGROUND FACILITIES KNOWN TO BE LOCATED IN THE CONSTRUCTION AREA HAVE BEEN IDENTIFIED. THE CONTRACTOR SHALL GIVE NOTICE OF INTENT TO CONSTRUCT TO THE OHIO UTILITIES PROTECTION SERVICE, PRODUCERS UNDERGROUND PROTECTION SERVICE, AND OWNERS OF UNDERGROUND FACILITIES THAT ARE NOT MEMBERS OF A REGISTERED PROTECTION SERVICE IN ACCORDANCE WITH SECTION 153.64 OF THE OHIO REVISED CODE. THE ABOVE, MENTIONED NOTICE SHALL BE GIVEN AT LEAST TWO

UNDERGROUND UTILITIES (CONT.):

WORKING DAYS PRIOR TO THE START OF CONSTRUCTION THE FOLLOWING UTILITIES ARE LOCATED WITHIN THE WORK LIMITS OF THE PROJECT AND THE OWNERS SUBSCRIBE TO REGISTERED UNDERGROUND PROTECTION SERVICE.

OHIO UTILITY PROTECTION SERVICE 1-800-362-2764
PRODUCERS UNDERGROUND PROTECTION SERVICE 1-614-587-0486
NON-MEMBERS MUST BE CALLED DIRECTLY.

UTILITIES OWNERSHIP:

LISTED ON SHEET 10/32 ARE THE KNOWN UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS.

A. D. A. RAMPS:

A. D. A. RAMPS ARE BEING CONSTRUCTED AS PART OF THIS PLAN. THE CONTRACTOR SHALL EXERCISE CAUTION WHEN WORKING OR OPERATING AROUND EXISTING ADA COMPLIANT RAMPS. IF THE CONTRACTOR DAMAGES ANY EXISTING A. D. A. CURB RAMPS OR RENDERS AN EXISTING A. D. A. COMPLIANT RAMP TO BE NON-COMPLIANT AS A RESULT OF NEGLIGENCE OR INADEQUATE CONSTRUCTION PRACTICE, THEN THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE CORRECTION OF THE DEFICIENCY AT THE CONTRACTORS EXPENSE. THIS MAY INCLUDE, BUT IS NOT LIMITED TO, FULL REMOVAL AND REPLACEMENT OF THE RAMP, MILLING AND RESURFACING, ETC. NO EXCEPTIONS WILL BE GRANTED.

ITEM 202 - CURB REMOVED, AS PER PLAN:

THIS ITEM IS A CONTINGENCY QUANTITY HAS BEEN PROVIDED AND SHALL BE USED AS DIRECTED BY THE PROJECT ENGINEER AT VARIOUS LOCATIONS IN CONJUNCTION WITH THE PLACEMENT OF THE A. D. A. RAMPS OR INTERSECTION IMPROVEMENTS. THE CONTRACTOR SHALL PROVIDE ALL MATERIAL, LABOR, EQUIPMENT, AND HARDWARE NECESSARY TO REMOVE THE EXISTING CURB BELOW ASPHALT SUBGRADE.

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 202 - CURB REMOVED, AS PER PLAN = 100 FT

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR, AS PER PLAN (ENTIRE PROJECT):

REPAIR AREAS SHALL BE DETERMINED BY THE PROJECT ENGINEER BEFORE THE BEGINNING OF WORK. REPAIRS SHALL CONSIST OF REMOVING 4" OF PAVEMENT AND REPLACING 4" OF ITEM 301 - ASPHALT CONCRETE BASE, PG64-22. WORK SHALL BE PERFORMED PRIOR TO RESURFACING AND REPAIR AREAS SHALL BE INCLUDED INTO GENERAL RESURFACING.

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR, AS PER PLAN (ENTIRE PROJECT) = 500 SY

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR, AS PER PLAN (EDGE CRACK REPAIR):

THIS ITEM IS FOR THE REPAIR OF EDGE CRACKING FROM SLM 0.00 TO SLM 3.65 ONLY.

REPAIRS SHALL CONSIST OF REMOVING 4" OF PAVEMENT AND REPLACING 4" OF ITEM 301 - ASPHALT CONCRETE BASE, PG64-22 FOR AN AVERAGE WIDTH OF 4' (THREE FEET INSIDE THE EDGE LINE PLUS ONE FOOT OF SHOULDER). REPAIR AREAS SHALL BE DETERMINED BY THE ENGINEER BEFORE THE BEGINNING OF WORK. WORK SHALL BE PERFORMED PRIOR TO RESURFACING AND REPAIR AREAS SHALL BE INCLUDED INTO GENERAL RESURFACING.

THE FINAL EDGELINE STRIPING SHALL BE PLACED ONE FOOT (1') INSIDE THE NEWLY CONSTRUCTED EDGE OF PAVEMENT.

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR, AS PER PLAN (EDGE CRACK REPAIR):

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR, AS PER PLAN (FOR EDGE CRACK REPAIR) = 17,131 SQ YD

SAFETY EDGE PLAN NOTE:

IN ADDITION TO THE REQUIREMENTS OF 401.12, ATTACH A DEVICE TO THE SCREED OF THE PAVER THAT CONFINES THE MATERIAL AT THE END GATE AND EXTRUDES THE ASPHALT MATERIAL IN SUCH A WAY THAT RESULTS IN A COMPACTED WEDGE SHAPE PAVEMENT EDGE OF APPROXIMATELY 30 DEGREES (NOT STEEPER THAN 40 DEGREES). ENSURE THE DEVICE MAINTAINS CONTACT WITH THE EXISTING SURFACE, AND ALLOW FOR AUTOMATIC TRANSITION TO CROSS ROADS, DRIVEWAYS AND OBSTRUCTIONS. DO NOT USE CONVENTIONAL SINGLE PLATE STRIKE OFF.

CONSTRUCTION OF SAFETY EDGE CAN BE OMITTED AT LOCATIONS WHERE EXISTING WIDTH OF GRADED SHOULDER OR BERM IS LESS THAN 12". PROJECTS WITH VARYING CONDITIONS SHOULD USE SAFETY EDGE WHERE POSSIBLE. PLAN PREPARATION HAS MADE EVERY REASONABLE ATTEMPT TO IDENTIFY POSSIBLE SAFETY EDGE LOCATIONS.

USE THE TRANSTECH SHOULDER WEDGE MAKER, THE CARLSON SAFETY EDGE END GATE, THE ADVANT-EDGER, THE TROXLER SAFETSIPOE OR A SIMILAR APPROVED-EQUAL DEVICE THAT PRODUCES THE SAME WEDGE CONSOLIDATION RESULTS. CONTACT INFORMATION FOR THESE WEDGE SHAPE COMPACTION DEVICES IS THE FOLLOWING:

TRANSTECH SYSTEMS, INC
1594 STATE STREET
SCHENECTADY, NY 12304
1-800-724-6306
WWW.TRANSTECHSYS.COM

ADVANT-EDGE PAVING
EQUIPMENT, LLC.
P.O. BOX 9163
NISKAYUNA, NY 12309-0163
518-280-6090
WWW.ADVANTAGEPAVING.COM

CARLSON SAFETY EDGE END GATE
18425 50TH AVENUE EAST
TACOMA, WA 98446
253-875-8000

TROXLER ELECTRONIC
LABORATORIES, INC.
3008 E. CORWALLIS ROAD
RESEARCH TRIANGLE PARK,
NC 27709
1-877-TROXLER
WWW.TROXLERLABS.COM

IF ELECTING TO USE A SIMILAR DEVICE, PROVIDE PROOF THAT THE DEVICE HAS BEEN USED ON PREVIOUS PROJECTS WITH ACCEPTABLE RESULTS OR CONSTRUCT A TEST SECTION PRIOR TO THE BEGINNING OF WORK AND DEMONSTRATE WEDGE COMPACTION TO THE SATISFACTION OF THE ENGINEER. SHORT SECTIONS OF HANDWORK WILL BE ALLOWED WHEN NECESSARY FOR TRANSITIONS AND TURNOUTS OR OTHERWISE AUTHORIZED BY THE ENGINEER.

IN ADDITION TO THE REQUIREMENTS OF 401.16, MAKE THE FIRST ROLLER PASS 8 TO 12 INCHES (200 TO 300 MM) AWAY FROM TAPERED EDGE. DO NOT ROLL THE TAPER.

ITEM 209 - PREPARING SUBGRADE FOR SHOULDER PAVING, AS PER PLAN:

PREPARE THE SHOULDER FOR PAVING A CONSISTENT SAFETY EDGE IN BOTH THICKNESS AND WIDTH.

PRIOR TO PAVING THE SAFETY EDGE, GRADE AN AREA 10 INCHES WIDE, BEGINNING AT THE EDGE OF THE PAVED ROADWAY, TO PROVIDE A LEVEL SURFACE FREE OF VEGETATION FOR CONSTRUCTION OF THE SAFETY EDGE. IF NECESSARY, EXCAVATE THE GRADED AREA TO THE DEPTH NECESSARY TO CONSTRUCT THE SAFETY EDGE. COMPACT THE GRADED SHOULDER ACCORDING TO 617.05, OR AS DIRECTED BY THE ENGINEER.

ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE:

THE CONTRACTOR SHALL BE TOTALLY RESPONSIBLE FOR ANY AND ALL DAMAGE TO THE CONTRACTORS EQUIPMENT THAT MAY RESULT FROM THE

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GENERAL NOTES

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ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE (CONT.):

PLANING OPERATION, INCLUDING DAMAGE CAUSED BY CASTINGS AND LOOP DETECTORS. THE DEPTH OF PLANING CLOSE TO THE CASTINGS SHALL BE AS DIRECTED; TO ACHIEVE A SMOOTH RIDING FINISHED PAVEMENT. GREAT CARE SHALL BE TAKEN TO PREVENT THE REMOVAL OF THE EXISTING PAVEMENT CROSS-SLOPE (CROWN) DURING THE PLANING OPERATIONS. ALL PLANED PAVEMENT SHALL BE PLANED TO A DEPTH OF 1.5 INCHES AND RESURFACED WITH 1.5 INCHES OF THE ASPHALT CONCRETE SURFACE COURSE WITHIN THE SAME WORK PERIOD.

ITEM 407 - TACK COAT:

THE RATE OF APPLICATION OF ITEM 407 - TACK COAT AND ITEM 407 - TACK COAT FOR INTERMEDIATE COURSE SHALL BE SUBJECT TO ADJUSTMENT AS DIRECTED BY THE ENGINEER. FOR ESTIMATING PURPOSES ONLY, THE PLAN INDICATES AN AVERAGE APPLICATION RATE OF TACK COAT AT 0.075 GALLON PER SQUARE YARD THIS ITEM IS FOR USE WITH ITEM 446 AS DIRECTED BY THE ENGINEER. A COVER AGGREGATE SHALL BE USED IF HEAVY TRACKING OF THE TACK COAT ON TO THE EXISTING PAVEMENT SHOULD OCCUR DURING THE PAVING OPERATIONS. THE COST OF THE COVER AGGREGATE SHALL BE INCLUDED IN THE COST OF THIS ITEM.

ITEM 448 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG64-22:

GREAT CARE SHALL BE TAKEN TO MAINTAIN THE EXISTING PAVEMENT CROSS-SLOPE (CROWN), INTERSECTION CROSS-SLOPES (CROWN) AS WELL AS ALL LONGITUDINAL SLOPES WITHIN THE EXISTING CROSSWALKS DURING THE PAVING OPERATIONS.

ALL PLANED PAVEMENT SHALL BE PLANED TO A DEPTH OF 1.5 INCHES AND RESURFACED WITH 1.5 INCHES OF THE ASPHALT CONCRETE SURFACE COURSE WITHIN THE SAME WORK PERIOD.

ITEM 608 - 4" CONCRETE WALK:

THIS ITEM IS A QUANTITY TO BE USED AS DIRECTED BY THE PROJECT ENGINEER AT VARIOUS LOCATIONS TO PROVIDE ADEQUATE TRANSITIONS BETWEEN THE EXISTING SIDEWALK AND THE PROPOSED CURB RAMP. THIS ITEM SHALL INCLUDE THE COST OF FURNISHING ALL MATERIALS, GRADING, FORMING AND FINISHING OF THE SIDEWALK AT VARIOUS LOCATIONS AS SPECIFIED IN THE PLANS.

ITEM 630 - GROUND MOUNTED SUPPORT, NO. 2 POST:

ITEM 630 - GROUND MOUNTED SUPPORT, NO. 3 POST:

ALL OF THE PROPOSED GROUND MOUNTED NO. 2 AND NO. 3 SUPPORTS WITHIN THIS PROJECT ARE REQUIRED TO BE MADE AS SHOWN ON SHEET 23/32. PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID FOR ITEM 630 - GROUND MOUNTED SUPPORT AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT A COMPLETE AND FUNCTIONAL SIGN SUPPORT.

ITEM 604 - MONUMENT BOX ADJUSTED TO GRADE:

THIS ITEM IS A CONTINGENCY QUANTITY TO BE USED AS DIRECTED BY THE PROJECT ENGINEER AT VARIOUS LOCATIONS AND PROVIDE ALL MATERIAL, LABOR, EQUIPMENT, AND HARDWARE NECESSARY TO ADJUST TO GRADE THE EXISTING CATCH BASIN TO THE PROPOSED ASPHALT ELEVATION. THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL CARRIED TO THE GENERAL SUMMARY.

THE BOXES ARE LOCATED AT THE FOLLOWING LOCATIONS:

- DENNIS ROAD EAST (T-194)
- MATVILLE ROAD (C-33)
- COMMERCIAL POINT ROAD (C-4)
- GIBSON ROAD (T-136)

THE FOLLOWING QUANTITY HAS BEEN PROVIDED AND THE TOTAL HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 604 - MONUMENT BOX ADJUSTED TO GRADE = 4 EACH

ITEM 644 - THERMOPLASTIC PAVEMENT MARKING:

THE LOCATIONS, SIZES AND SHAPES OF PROPOSED AUXILIARY PAVEMENT MARKINGS WILL BE THE SAME AS EXISTING ON THE MAJORITY OF THIS PROJECT. ANY DEVIATION FROM EXISTING WILL BE

ITEM 644 - THERMOPLASTIC PAVEMENT MARKING (CONT):

IDENTIFIED ON PLAN SHEETS 14/31. IT IS THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION, SIZE AND SHAPE OF THESE EXISTING PAVEMENT MARKINGS BEFORE THE PAVEMENT PLANING AND RESURFACING OBLITERATES THEM. ANY PAVEMENT MARKING WHICH IS PLACED AT THE WRONG LOCATION SHALL BE REMOVED AND REPLACED AT THE CONTRACTORS EXPENSE.

SURVEYING PARAMETERS:

USE THE FOLLOWING VERTICAL POSITIONING AND HORIZONTAL POSITIONING PARAMETERS FOR ALL SURVEYING:

VERTICAL POSITIONING
 ORTHOMETRIC HEIGHT DATUM: NAVD 1988
 GEOID: GEOID 03
 HORIZONTAL POSITIONING
 REFERENCE FRAME: NAD 83 (CORS 96)
 ELLIPSOID: WGS84
 MAP PROJECTION: LAMBERT CONFORMAL CONIC
 COORDINATE SYSTEM: OHIO STATE PLAN (SOUTH)
 COMBINED SCALE FACTOR: 1.000057560

UNITS ARE IN U.S. SURVEY FEET. USE THE FOLLOWING CONVERSION FACTOR: 1 METER = 3.280833333 U.S. SURVEY FEET.

SCENIC RIVER PLAN NOTES:

NO TOXIC OR HAZARDOUS MATERIALS SUCH AS SEALANTS, PAINT, SOLVENTS, CLEANING AGENTS, EARTHEN MATERIALS, WASTE-WATER, FUELS OR DEBRIS OF ANY KIND SHALL BE DISCHARGED TO THE BIG DARBY CREEK. ALL ASPHALT OR CONCRETE GRINDINGS, EXCESS ASPHALTIC OR CONCRETE MATERIALS OR ANY OTHER DEBRIS GENERATED DURING RESURFACING OR OTHER SIMILAR ACTIVITIES SHALL BE REMOVED IMMEDIATELY FROM WITHIN 1,000 FEET OF THE BIG DARBY CREEK AND DISPOSED OF AT AN APPROPRIATE FACILITY ABOVE THE FEMA 100-YEAR FLOOD ELEVATION AND NOT WITHIN 1,000 FEET OF THE BIG DARBY CREEK.

IF PAINTING, WELDING, SAND AND/OR WATER BLASTING (CLEANING) IS INCORPORATED AS PART OF THE PROJECT AT OR OVER THE BIG DARBY CREEK, THEN APPROPRIATE APRONS SHALL BE UTILIZED TO PROVIDE FOR COMPLETE CONTAINMENT OF ALL PAINT, WELDING SLAG AND/OR SEALANT OVER SPRAY AND OTHER DEBRIS. APRONS SHALL BE UTILIZED ON ALL DECK REPLACEMENT PROJECTS WHEN USING HYDRO-DEMOLITION TECHNIQUES. ALL DEBRIS COLLECTED SHALL BE DISPOSED OF AT AN APPROPRIATE FACILITY ABOVE THE FEMA 100-YEAR FLOOD PLAIN AND NOT WITHIN 1,000 FEET OF THE BIG DARBY CREEK

NO WASTEWATER OF ANY KIND SHALL BE DIRECTLY DISCHARGED TO THE BIG DARBY CREEK OR ANY WATERCOURSE DRAINING DIRECTLY INTO THE BIG DARBY CREEK. IDLE EQUIPMENT, FUELS, LUBRICANTS OR STORAGE FOR AND/OR STORAGE OF POTENTIALLY TOXIC OR HAZARDOUS MATERIALS SHALL BE KEPT ABOVE THE FEMA 100 YEAR FLOOD PLAIN AND NOT WITHIN 1,000 FEET OF THE BIG DARBY CREEK.

DEBRIS REMOVAL SHALL BE CONDUCTED BY PULLING DEBRIS OUT OF THE STREAM CHANNEL WITH EQUIPMENT FROM THE BRIDGE DECK WHENEVER POSSIBLE. ALL DEBRIS COLLECTED SHALL BE DISPOSED OF AT AN APPROPRIATE FACILITY ABOVE THE FEMA 100-YEAR FLOOD PLAIN AND NOT WITHIN 1,000 FEET OF BIG DARBY CREEK. IF DEBRIS REMOVAL CAN NOT BE CONDUCTED FROM THE BRIDGE DECK, THEN ALL WORK SHALL BE ACCOMPLISHED FROM THE STREAM BANK BY CABLING AND PULLING MATERIALS FROM THE CHANNEL. NO HEAVY EQUIPMENT WILL BE PERMITTED IN THE STREAM.

WATER QUALITY PROTECTION:

NO TOXIC OR HAZARDOUS MATERIALS SUCH AS SEALANTS, PAINT, SOLVENTS, CLEANING AGENTS, EARTHEN MATERIALS, WASTE-WATER FUELS OR DEBRIS OF ANY KIND SHALL BE DISCHARGED TO ANY STREAMS, DRAINAGE COURSES OR BODIES OF WATER. ALL ASPHALT OR CONCRETE GRINDINGS, EXCESS ASPHALT OR CONCRETE MATERIALS OR ANY OTHER DEBRIS GENERATED DURING RESURFACING OR OTHER SIMILAR ACTIVITIES SHALL NOT BE DISPOSED OF WITHIN A

WATER QUALITY PROTECTION (CONT.):

FLOODPLAIN BELOW THE 100-YEAR FLOOD ELEVATION. THE CONTRACTOR SHALL TAKE ALL PRECAUTIONS NECESSARY TO PREVENT LIQUIDS USED TO REPAIR, CLEAN, SEAL, OR TREAT ANY BRIDGE STRUCTURE (E.G. PAINT, SEALER, SOLVENT) FROM ENTERING STREAMS, WETLANDS OR OTHER "WATERS OF THE UNITED STATES" AND TAKE THE APPROPRIATE ACTIONS IN THE EVENT OF A RELEASE.

ITEM 630 - SIGN, FLAT SHEET:

ITEM 630 - GROUND MOUNTED SUPPORT, NO. 2 POST:

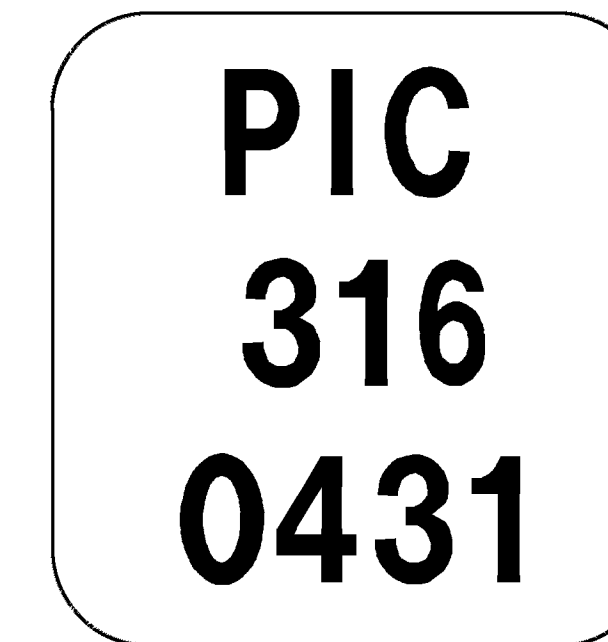
THESE ITEMS SHALL BE USED TO PLACE NEW STRUCTURE IDENTIFICATION SIGNS AT THE FOLLOWING STRUCTURES:

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EACH SIGN SHALL BE ATTACHED TO THE CONCRETE PARAPET WITH CONCRETE ANCHORS AT THE RIGHT REAR LOCATION. IF THE BRIDGE

DOES NOT HAVE A CONCRETE PARAPET, THE SIGN SHALL BE POST MOUNTED TO ONE NEW NO. 2 POST AS PER STANDARD CONSTRUCTION DRAWING TC-41.20 (MOST CURRENT REVISION) USING TWO 5/16" ALUMINUM BOLTS 2 1/2" IN LENGTH. THE POST SHALL BE 7'-0" LONG.

SIGNS SHALL BE SIZED AS PER SIGN I-H25a AS GIVEN IN THE ODOT SIGN DESIGN MANUAL (MOST CURRENT VERSION). ALL INCIDENTALS WILL BE INCLUDED IN THE UNIT BID PRICE FOR ITEM 630 - SIGN, FLAT SHEET.



BRIDGE ID SIGN

SIGN CODE I-H25a

THE FOLLOWING ITEMS HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 630 - SIGN, FLAT SHEET = 1.0 SQ FT
 ITEM 630 - GROUND MOUNTED SUPPORT, NO. 2 POST = 12 FT

ITEM 623 - CONSTRUCTION LAYOUT STAKES, AS PER PLAN:

THIS ITEM SHALL CONSIST OF STATIONING USING 3 FT LATH STAKES. THE STAKES SHALL BE SPACED AT 100 FT INTERVALS AND SHALL EXTEND THROUGHOUT THE LENGTH OF THE PROJECT AND THROUGHOUT THE LENGTH OF ALL RAMPS. PLACEMENT OF THE STAKES SHALL BE AS DIRECTED BY THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR REPLACING ANY DAMAGED OR MISSING STAKES.

CONSTRUCTION LAYOUT STAKES, AS PER PLAN WILL BE PAID FOR AT THE CONTRACT LUMP SUM BID, WHICH PRICE SHALL BE FULL COMPENSATION FOR ALL SERVICES, MATERIALS, LABOR, EQUIPMENT, TOOLS, AND INCIDENTALS, INCLUDING THE REMOVAL, NECESSARY TO COMPLETE THIS ITEM.

STOCKPILING GRINDINGS:

THE CONTRACTOR IS TO COORDINATE WITH BILL CUNNINGHAM (PICKAWAY COUNTY MANAGER) AS TO THE STOCKPILING OF PAVEMENT GRINDINGS.

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PAVEMENT MARKING:

QUANTITIES FOR THERMOPLASTIC (ITEM-644) PAVEMENT MARKINGS ARE INCLUDED IN THIS PLAN. ALL PROPOSED CENTERLINE PAVEMENT MARKINGS ARE TO BE REPLACED AS THEY ARE SHOWN ON SHEETS 31/32 - 32/32. THE AUXILIARY MARKINGS ARE TO BE REPLACED AS EXISTING. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO VERIFY THE LOCATION, SIZE, AND SHAPE OF THESE AUXILIARY MARKINGS BEFORE THE PAVING OPERATION OBLITERATES THE EXISTING AUXILIARY MARKINGS.

THE EQUIVALENT LENGTH OF SOLID LINE ON THIS PROJECT IS 5.648 MILES. THIS IS FOR INFORMATION ONLY.

THE ALLOWABLE DURATION FROM THE TIME THE EXISTING PAVEMENT MARKING HAS BEEN OBLITERATED UNTIL THE PERMANENT MARKINGS ARE TO BE APPLIED IS AS FOLLOWS:

MARKING TYPE	CALENDAR DAYS
CENTER LINE	3
EDGE LINE	14

ITEM 617 - WATER:

A QUANTITY OF 15 M GALLONS OF WATER HAS BEEN ADDED FOR USE IN HELPING WITH THE COMPACTION OF ITEM - 617 COMPACTED AGGREGATE, 2 IN. AVG.

THE TOTAL IS CARRIED TO THE GENERAL SUMMARY:

ITEM 617 - WATER 15 M GALLONS

ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE:

THE CONTRACTOR SHALL BE TOTALLY RESPONSIBLE FOR ANY AND ALL DAMAGE TO THE CONTRACTORS EQUIPMENT THAT MAY RESULT FROM THE PLANING OPERATION, INCLUDING DAMAGE CAUSED BY CASTINGS AND LOOP DETECTORS. THE DEPTH OF PLANING CLOSE TO THE CASTINGS SHALL BE AS DIRECTED; TO ACHIEVE A SMOOTH RIDING FINISHED PAVEMENT. GREAT CARE SHALL BE TAKEN TO PREVENT THE REMOVAL OF THE EXISTING PAVEMENT CROSS-SLOPE (CROWN) DURING THE PLANING OPERATIONS.

ALL PLANED PAVEMENT SHALL BE PLANED TO A DEPTH OF 1.5 INCHES AND RESURFACED WITH 1.5 INCHES OF THE ASPHALT CONCRETE SURFACE COURSE WITHIN THE SAME WORK PERIOD.

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CONSTRUCTION INITIATION:

AT LEAST 14 DAYS PRIOR TO ANY CONSTRUCTION ACTIVITIES, THE CONTRACTOR SHALL ADVISE THE DISTRICT OFFICE OF COMMUNICATIONS VIA EMAIL AT d06.pio@dot.state.oh.us AND THE DISTRICT WORK ZONE TRAFFIC MANAGER VIA EMAIL AT d06.mot@dot.state.oh.us OF THE ANTICIPATED START DATE OF ANY CONSTRUCTION ACTIVITIES, INCLUDING, BUT NOT LIMITED TO THE PLACING OF WORK ZONE SIGNS. THE NOTIFICATION SHALL ALSO INCLUDE THE PROJECT NUMBER, PID, NAME AND PHONE NUMBER OF THE CONTRACTOR, A POINT OF CONTACT AND THE ANTICIPATED IMPACT ON TRAFFIC. THE CONTRACTOR WILL IMMEDIATELY INFORM THE DISTRICT OFFICE OF COMMUNICATIONS AND THE DISTRICT WORK ZONE TRAFFIC MANAGER OF ANY AND ALL DELAYS AND/OR CHANGES REGARDING THE CONSTRUCTION INITIATION DATE.

ITEM 614 - MAINTAINING TRAFFIC:

A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING AND COMPLETED PAVEMENT. WORK ZONES SHALL BE LIMITED IN LENGTH TO THE AMOUNT OF WORK THAT CAN BE PERFORMED THAT DAY.

ITEM 614, WORK ZONE MARKING SIGN:

THE FOLLOWING QUANTITIES HAVE BEEN PROVIDED:

- R4-1-36 "DO NOT PASS" = 17 EACH
- R4-2-36 "PASS WITH CARE" = 15 EACH
- W8-H12a "NO EDGE LINES" = 12 EACH

TOTAL CARRIED TO THE GENERAL SUMMARY:

- ITEM 614 - WORK ZONE MARKING SIGN = 44 EACH

ITEM 614, MAINTAINING TRAFFIC:

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

NOTIFICATION OF TRAFFIC RESTRICTIONS:

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW. THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE BUT IS NOT LIMITED TO ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHOULD LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, DETOUR ROUTES IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION OF TRAFFIC RESTRICTIONS (CONT.):

NOTIFICATION TIME FRAME TABLE		
ITEM	DURATION OF CLOSURE	NOTIFICATION DUE TO DISTRICT 6 COMMUNICATIONS OFFICE
RAMP AND ROAD CLOSURES	>= 2 WEEKS	14 BUSINESS DAYS PRIOR TO CLOSURE
	> 12 HOURS AND < 2 WEEKS	7 BUSINESS DAYS PRIOR TO CLOSURE
	< 12 HOURS	2 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES/RESTRICTIONS	>= 2 WEEKS	7 BUSINESS DAYS PRIOR TO CLOSURE
	< 2 WEEKS	2 BUSINESS DAYS PRIOR TO CLOSURE

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS EQUARING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME FRAME TABLE.

FLOOD LIGHTING:

FLOODLIGHTING FOR THE WORK SITE FOR OPERATIONS DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE HIGHWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE AND PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDS SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

THE CONTRACTOR SHALL SUBMIT FOR APPROVAL, TO THE DISTRICT SIX MAINTENANCE OF TRAFFIC COORDINATOR, THE CONTRACTOR'S MAINTENANCE OF TRAFFIC PLAN WITH CONSTRUCTION PHASING DESCRIPTIONS, PRIOR TO BEGINNING WORK.

PAVEMENT DROPOFF:

THE DROPOFF ADJACENT TO THE TRAVELED LANE SHALL BE NO GREATER THAN 1 1/2 INCHES BELOW THE EXISTING PAVEMENT BY THE END OF EACH WORK DAY. THIS REQUIREMENT MAY BE MET BY TEMPORARILY PLACING SUBBASE AND BASE MATERIAL TO WITHIN 1 1/2 INCHES OF THE EXISTING GRADE ADJACENT TO THE TRAVELED LANE AND SLOPING THE MATERIAL AT 3:1 OR FLATTER WITHIN THE EXCAVATED AREA. PLACEMENT OF THE PROPOSED SUBBASE AND BASE MATERIAL SHALL FOLLOW AS CLOSELY AS POSSIBLE BEHIND EXCAVATION OPERATIONS DURING WORKING HOURS. THESE REQUIREMENTS SHALL BE MET AT NO ADDITIONAL COST.

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS:

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF CMS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

- IN ADDITION TO THE REQUIREMENT OF CMS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS (CONT.):

- FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP). IN GENERAL, LEOS SHOULD BE POSITIONED AT THE POINT OF LANE RESTRICTION OR ROAD CLOSURE AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH INTERSECTIONS IN WORK ZONES.

- WHEN CONSTRUCTION VEHICLES ARE ENTERING/EXITING THE ZONE DIRECTLY FROM/INTO AN OPEN LANE OF TRAFFIC. IF A LANE HAS BEEN CLOSED TO PROVIDE AN ACCELERATION/DECELERATION LANE FOR THE VEHICLE, THE LEO WILL NOT BE REQUIRED.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. ONCE THE LEO HAS COMPLETED THE DUTIES DESCRIBED ABOVE AND STILL HAS TIME REMAINING ON HIS/HER SHIFT, THE LEO MAY BE ASKED TO PATROL THROUGH THE WORK ZONE (WITH FLASHING LIGHTS OFF) OR BE PLACED AT A LOCATION TO DETER MOTORISTS FROM SPEEDING. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE = 40 HR

ITEM 614 - WORK ZONE CENTERLINE, CLASS II, 740.06, TYPE I:
THE FOLLOWING QUANTITY HAS BEEN PROVIDED FOR PIC-316-0.00

SLM 0.00 - 9.43 = 9.43 MI X 1 APPLICATION = 9.43 MI

ITEM 614 - WORK ZONE STOP LINE CLASS I, 740.06, TYPE I:
THE FOLLOWING QUANTITY HAS BEEN PROVIDED FOR PIC-316-0.00
= 50 FT

PUBLIC OUTREACH AND NOTIFICATION:

THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE DISTRICT 6 PUBLIC INFORMATION OFFICE AT (740) 833-8063 TO COORDINATE EFFORTS TO NOTIFY ADJACENT RESIDENTS AND BUSINESSES OF THE UPCOMING RESURFACING PROJECT. ADVANCE NOTIFICATION SHALL OCCUR NO LATER THAN FOURTEEN (14) DAYS PRIOR TO THE FIRST DAY OF WORK.

USE OF WEIGHTED CHANNELIZERS:

THE WEIGHTED CHANNELIZER MAY BE USED IN ACCORDANCE WITH THIS SECTION. THE WEIGHTED CHANNELIZER SHALL BE PREDOMINANTLY ORANGE IN COLOR AND SHALL BE MADE OF LIGHTWEIGHT, FLEXIBLE, AND DEFORMABLE MATERIAL. THEY SHALL BE AT LEAST 42 INCHES IN HEIGHT WITH A WEIGHTED BASE. THEY MAY HAVE A HANDLE OR LIFTING DEVICE WHICH EXTENDS ABOVE THE 42 INCHES MINIMUM HEIGHT.

THE MARKINGS ON THE WEIGHTED CHANNELIZER SHALL BE HORIZONTAL, CIRCUMFERENTIAL, ALTERNATING ORANGE AND WHITE RETROREFLECTIVE STRIPES 6 INCHES WIDE. EACH WEIGHTED CHANNELIZER SHALL HAVE A MINIMUM OF TWO ORANGE AND TWO WHITE STRIPES. ANY NON-RETROREFLECTIVE SPACES BETWEEN THE HORIZONTAL ORANGE AND WHITE STRIPES SHALL NOT EXCEED 2 INCHES WIDE. THE WEIGHTED CHANNELIZER SHALL HAVE A 4-INCH MINIMUM WIDTH, REGARDLESS OF ORIENTATION.

USE OF WEIGHTED CHANNELIZERS ON FREEWAYS AND MULTILANE HIGHWAYS SHALL BE LIMITED TO SHORT-TERM OPERATIONS FOR EITHER DAY OR NIGHT. UPON COMPLETION OF WORK, THE WEIGHTED CHANNELIZERS SHALL BE REMOVED. THE WEIGHTED CHANNELIZERS MAY AGAIN BE PLACED ON THE HIGHWAY WHEN THE WORK IS TO RESUME ON THE FOLLOWING DAY OR NIGHT. ANY LANE CLOSURE USING CHANNELIZATION DEVICES, EXPECTED TO REMAIN FOR MORE THAN TWELVE HOURS, SHALL REQUIRE THE USE OF DRUMS OR BARRICADES.

WHEN USED AT NIGHT, WEIGHTED CHANNELIZERS SHALL ONLY BE PLACED IN THE TANGENT AREA AND AT A MAXIMUM SPACING OF 40 FEET. THE TANGENT AREA IS DEFINED AS THE AREA AFTER THE TRANSITION TAPER WHERE THE WORK TAKES PLACE. DRUMS SHALL BE USED IN THE TRANSITION TAPERS FOR NIGHT OPERATIONS.

STEPS SHOULD BE TAKEN TO ENSURE THAT THE WEIGHTED CHANNELIZERS WILL NOT BE BLOWN OVER OR DISPLACED BY WIND OR MOVING TRAFFIC. BALLASTS SHOULD NOT PRESENT A HAZARD IF THE WEIGHTED CHANNELIZERS ARE IN ADVERTENTLY STRUCK, NOR SHOULD THEY AFFECT THE VISIBILITY OF THE WEIGHTED CHANNELIZERS. ALL BALLASTS USED SHOULD BE IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

PAYMENT SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614 MAINTAINING TRAFFIC.

CURB RAMPS:

ADA CURB RAMPS IN THE VILLAGE OF DARBYVILLE WILL BE RECONSTRUCTED PRIOR TO RESURFACING.

GENERAL MOT:

FLAGGING FOR ALL PAVING OPERATIONS SHALL BE AS PER STANDARD CONSTRUCTION DRAWING MT-97.12.

FOR THE REPAIR OF THE BACKWALL AND APPROACH SLABS OF STRUCTURE PIC-316-0431, THE REPAIR SHALL BE PERFORMED ONE SIDE OF THE ROAD AT A TIME. TRAFFIC SHALL BE MAINTAINED AS TWO WAY, ONE-LANE TRAFFIC AS PER STANDARD CONSTRUCTION DRAWING MT-97.10.

THE REPAIRS SHALL BE BEGUN AND COMPLETED IN THE SAME SESSION.

THESE REPAIRS SHALL BE MADE BEFORE BEGINNING THE PAVEMENT OVERLAY IN THIS AREA.

EVENT TRAFFIC RESTRICTIONS:

- NO TRAFFIC RESTRICTIONS WITHIN 1000 FEET OF SR 104 INTERSECTION DURING THE TOUR OF THE SCIOTO RIVER VALLEY BIKE EVENT.

NO TRAFFIC RESTRICTIONS DURING THE CIRCLEVILLE PUMPKIN SHOW.

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MAINTENANCE OF TRAFFIC NOTES	
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UTILITY OWNERSHIP:

THE UTILITIES AND THEIR PERSPECTIVE OWNERS LISTED (ON THE UTILITIES CONTACT LIST SHEETS) MAY BE LOCATED WITHIN THE PROJECT WORK AREAS.

MR. JERRY PENNINGTON
TIME WARNER CABLE
32 ENTERPRISE DRIVE
CHILLICOTHE, OH 45601
740-772-7813
jerry.pennington@twcable.com

MS TIFFANY WOODYARD
OPERATIONS ENGINEER
COLUMBIA GAS OF OHIO
843 PIATT AVENUE
CHILLICOTHE, OH 45601
740-772-9131
trwoodyard@nisource.com

MR. DAVE COATES
HORIZON TELEPHONE COMPANY
P.O. BOX 480
CHILLICOTHE, OH 45601
740-772-8396
dave.coates@horizontel.com

PICKAWAY COUNTY ENGINEER'S OFFICE
207 SOUTH COURT STREET
CIRCLEVILLE, OH 43113
740-474-3360

MS DEE REED
CENTURYLINK
P.O. BOX 1031
PATASKALA, OH 43062
740-927-8282
delores.a.reed@centurylink.com

MR. MIKE EDWARDS
FRONTIER COMMUNICATIONS
500 LANCASTER PIKE
CIRCLEVILLE, OH 43113
740-474-3114
michael.edwards@ftr.com

MR. DENNIS WILLIAMS
EARNHART HILL REGIONAL WATER AND SEWER DISTRICT
P.O. BOX 151
CIRCLEVILLE, OH 43113
dwilliams@ehrwsd.org

MR. JAMES SCOTT
COLUMBIA GAS TRANSMISSION CORPORATION
301 MAPLE STREET
P.O. BOX 330
SUGAR GROVE, OH 43155
740-746-2234
jscott@nisource.com

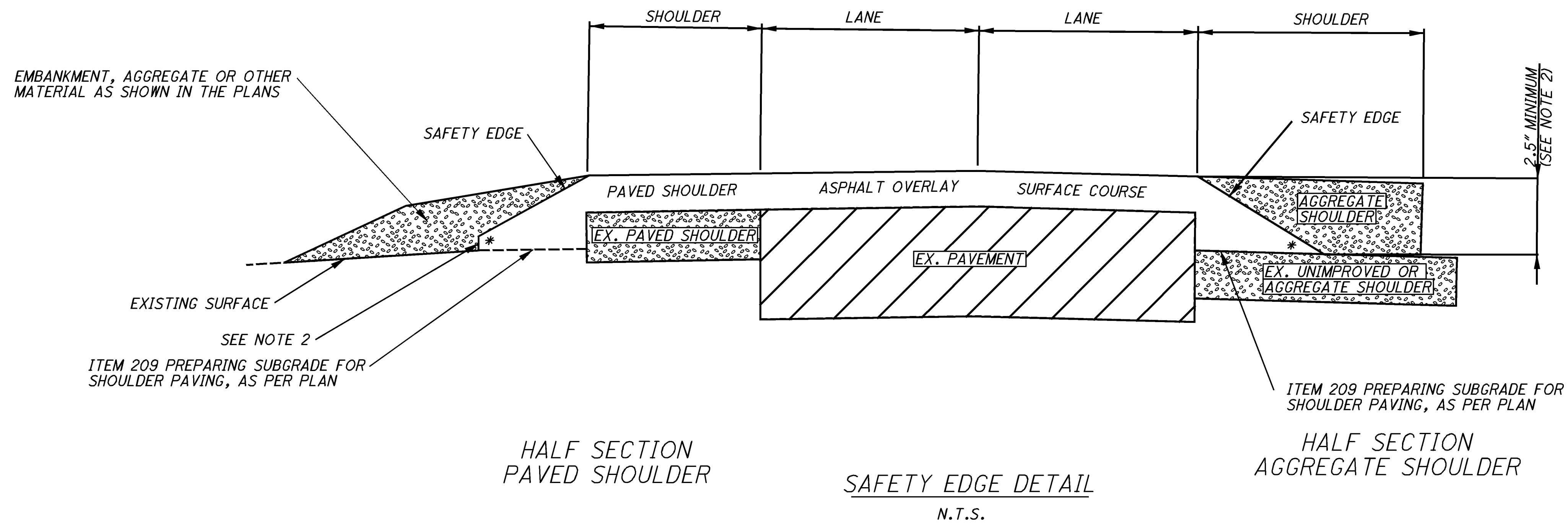
MR. TOM ZIOMEK
MANAGER - LEGAL SECTION
AT&T
111 NORTH FOURTH STREET - 8TH FLOOR
COLUMBUS, OH 43215
614-223-7162
tz7315@att.com

ODOT D-6 TRAFFIC
400 E. WILLIAM STREET
DELAWARE, OH 43015
740-833-8025

COLUMBIA GAS HIGH PRESSURE LINE
NEAR THE INTERSECTION OF S.R. 316 AND FLORENCE CHAPEL ROAD (SLM 5.39) COLUMBIA GAS HAS A RELATIVELY SHALLOW HIGH PRESSURE GAS MAIN. THE CONTRACTOR IS TO USE CAUTION WHEN WORKING NEAR THIS GAS LINE.

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UTILITY NOTES	
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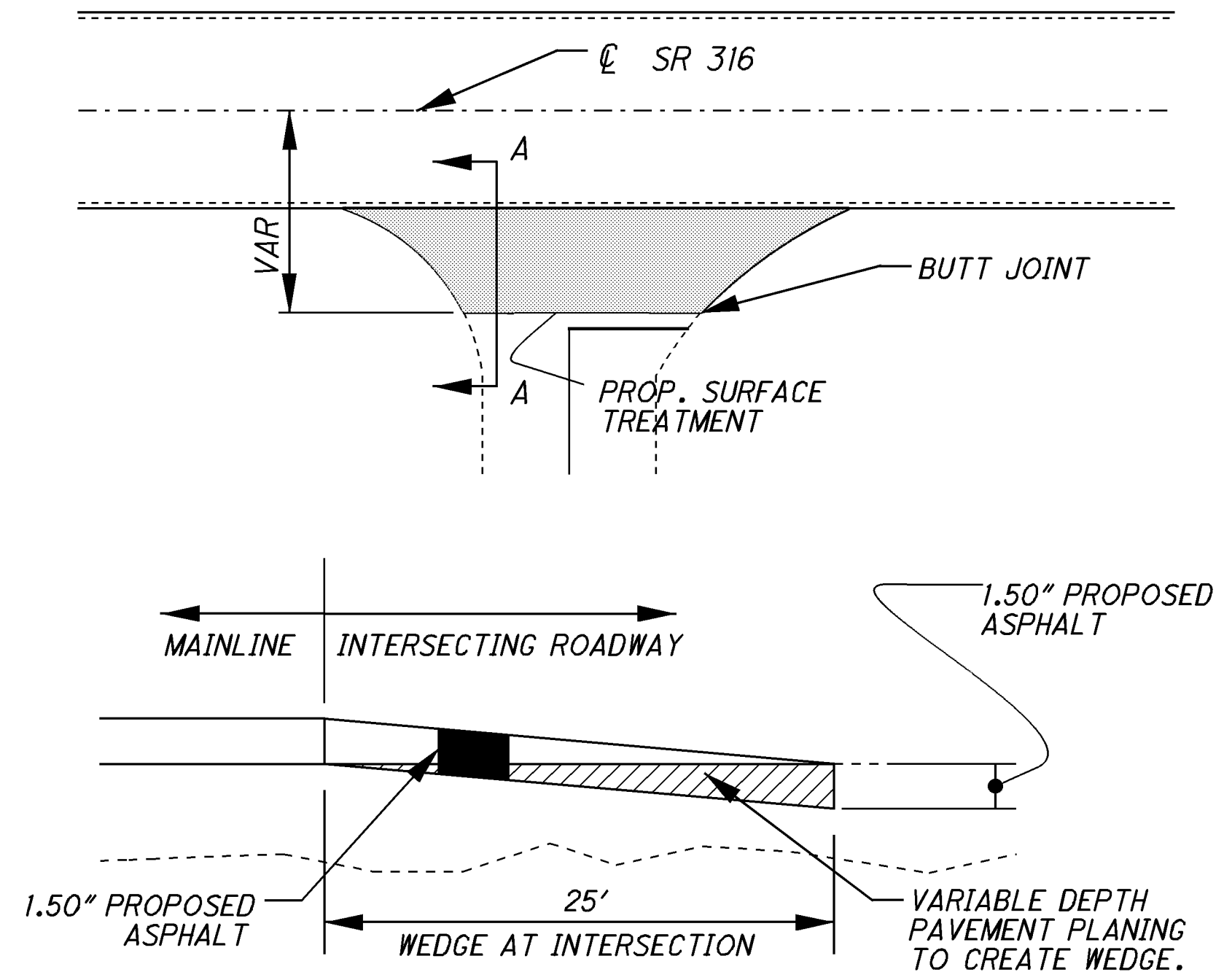
- 1.) SAFETY EDGES ARE REQUIRED AT THE OUTSIDE EDGES OF THE PAVED ROADWAY (EDGE OF TRAVEL LANE OR EDGE OF PAVED SHOULDER).
 - 2.) CONSTRUCT THE SAFETY EDGE THE FULL ASPHALT CONCRETE OVERLAY THICKNESS OR 2.5" (63MM) WHICHEVER IS GREATER, NOT TO EXCEED THE MAXIMUM SAFETY EDGE THICKNESS OF 6" (150MM). CONSTRUCT A NEAR-VERTICAL FACE BELOW THE SAFETY EDGE FOR THICKNESS GREATER THAN 6" (150MM).
 - 3.) BLADE AND SHAPE EXISTING SHOULDER MATERIAL TO FORM A UNIFORM SURFACE UNDER THE SAFETYEDGE PRIOR TO PLACEMENT OF THE ASPHALT CONCRETE OVERLAY.
- * 40° MAX.

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LOCATION							642													644																				
SHEET #	LOCALITY	ROUTE	BEGLM	ENDSLM	LENGTH	EDGE LINE		CENTER LINE	CHANNELIZING LINE	STOP LINE	CROSSWALK LINE	TRANSVERSE/DIAGONAL LINE		RAILROAD SYMBOL MARKING	LANE ARROW					BIKE LANE SYMBOL MARKING	PAVEMENT MARKING, MISC.: DIRECTIONAL ARROW	EDGE LINE		LANE LINE	CENTER LINE	CENTER LINE, AS PER PLAN	CHANNELIZING LINE	STOP LINE	CROSSWALK LINE	TRANSVERSE/DIAGONAL LINE		RAILROAD SYMBOL MARKING	RAILROAD SYMBOL MARKING, APP	SCHOOL SYMBOL MARKING, 96'	LANE ARROW					DOTTED LINE
						W	Y					LT	LT THRU		THRU	RT THRU	RT	W	Y			W	Y							LT	LT THRU				THRU	RT THRU	RT			
						MI	MILE	MILE	MILE	FT	FT	FT	FT	FT	FT	FT	FT	FT	EACH	EACH	EACH	EACH	EACH	EACH	EACH	MILE	MILE	MILE	MILE	MILE	FT	FT	FT	FT	FT	EACH	EACH	EACH	EACH	EACH
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	PIC	T194																							30															
TOTALS CARRIED TO GENERAL SUMMARY																				18.96		9.48			50	252														

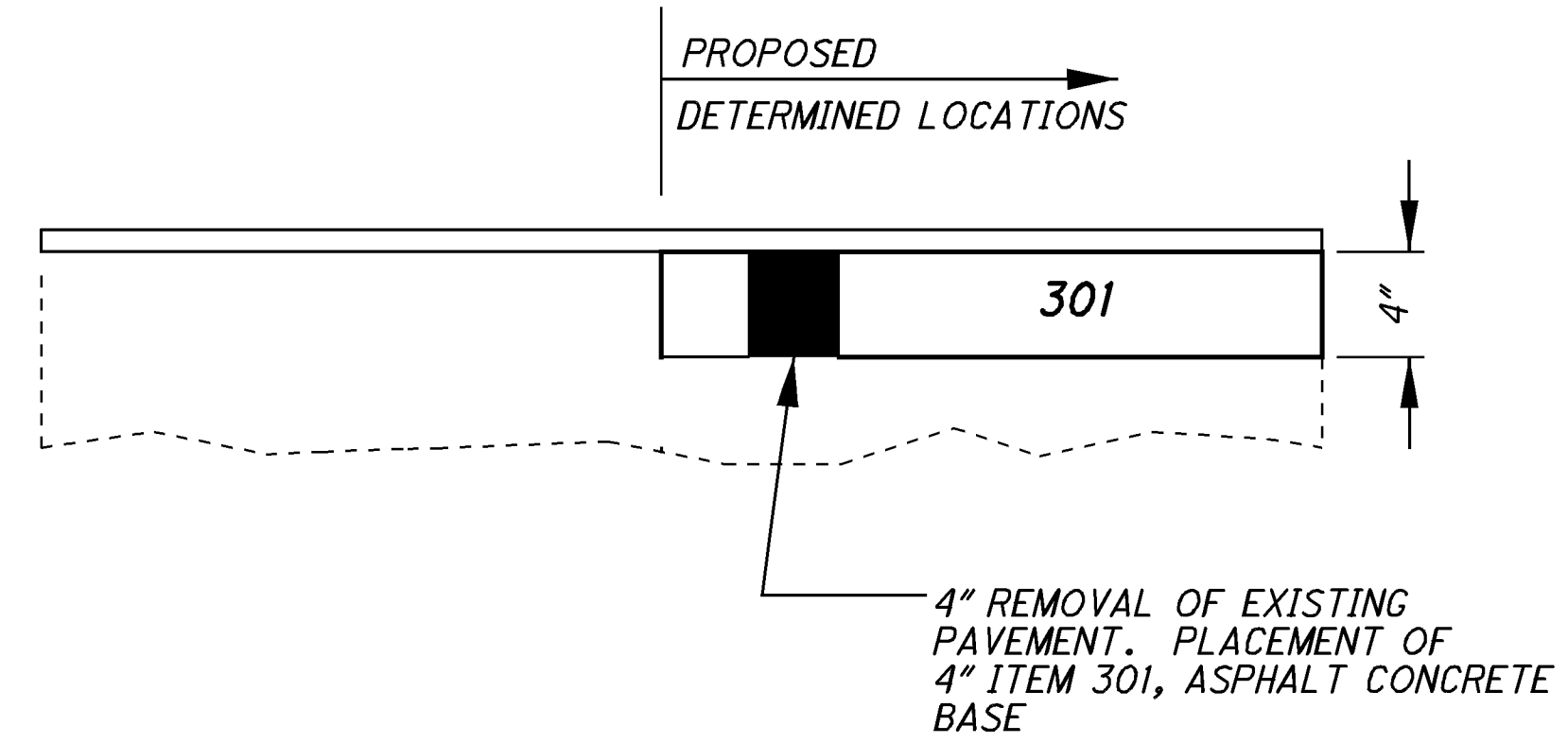
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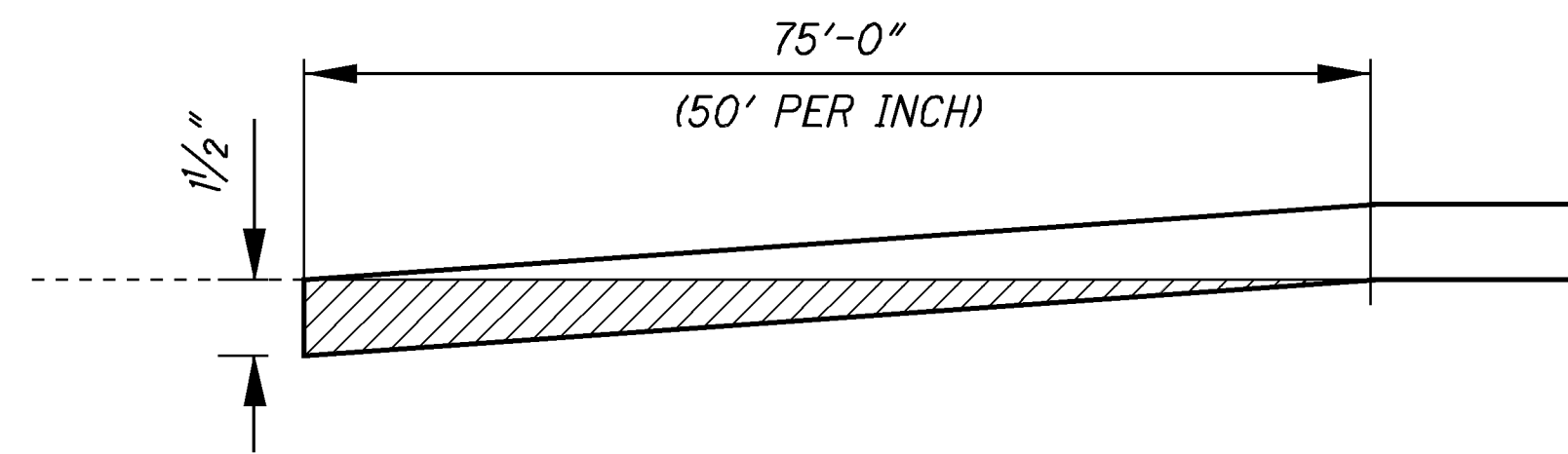
SECTION A - A
TYPICAL INTERSECTION DETAIL

DETAIL FOR ALL INTERSECTIONS WEST OF DARBYVILLE ONLY. IN DARBYVILLE AND AT INTERSECTIONS EAST OF DARBYVILLE, WHERE THE ROADWAY IS MILL AND FILL, THE CONTRACTOR IS NOT TO MILL THE SIDE ROADS.

DENNIS ROAD (T-194) LT & RT

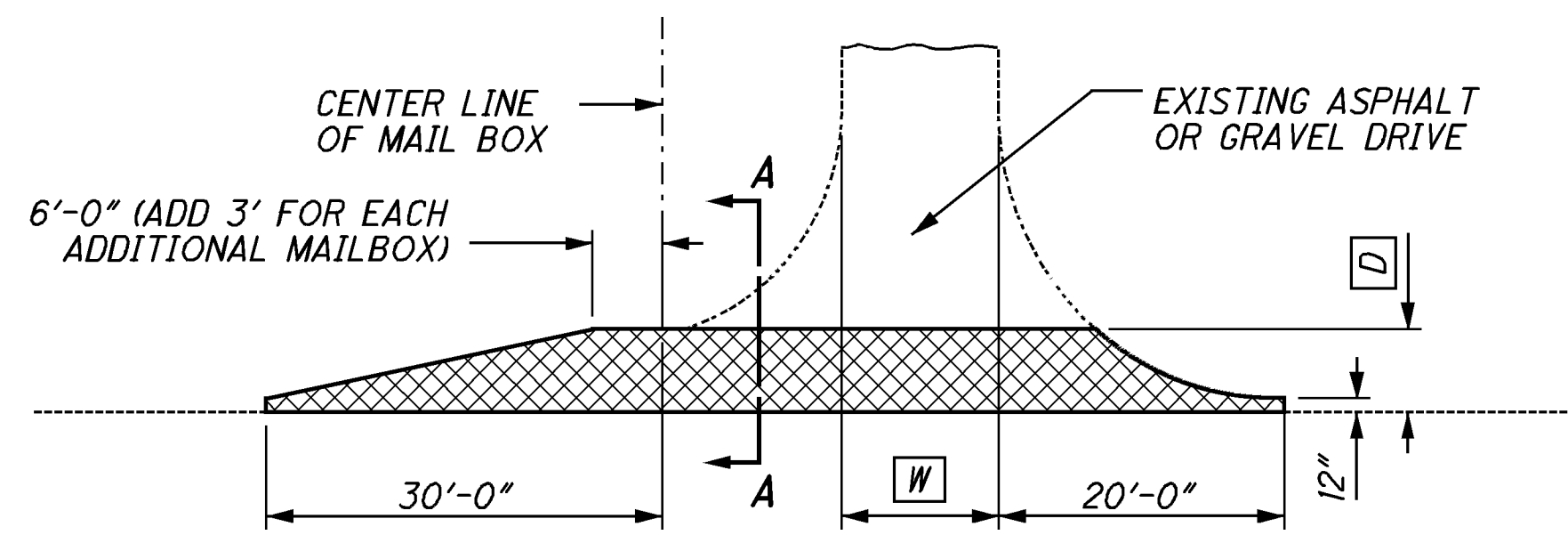


PARTIAL DEPTH PAVEMENT REPAIR DETAIL
SEE GENERAL NOTES SHEET 5/31 FOR MORE INFORMATION REGARDING ITEM 251-PARTIAL DEPTH PAVEMENT REPAIR, AS PER PLAN.

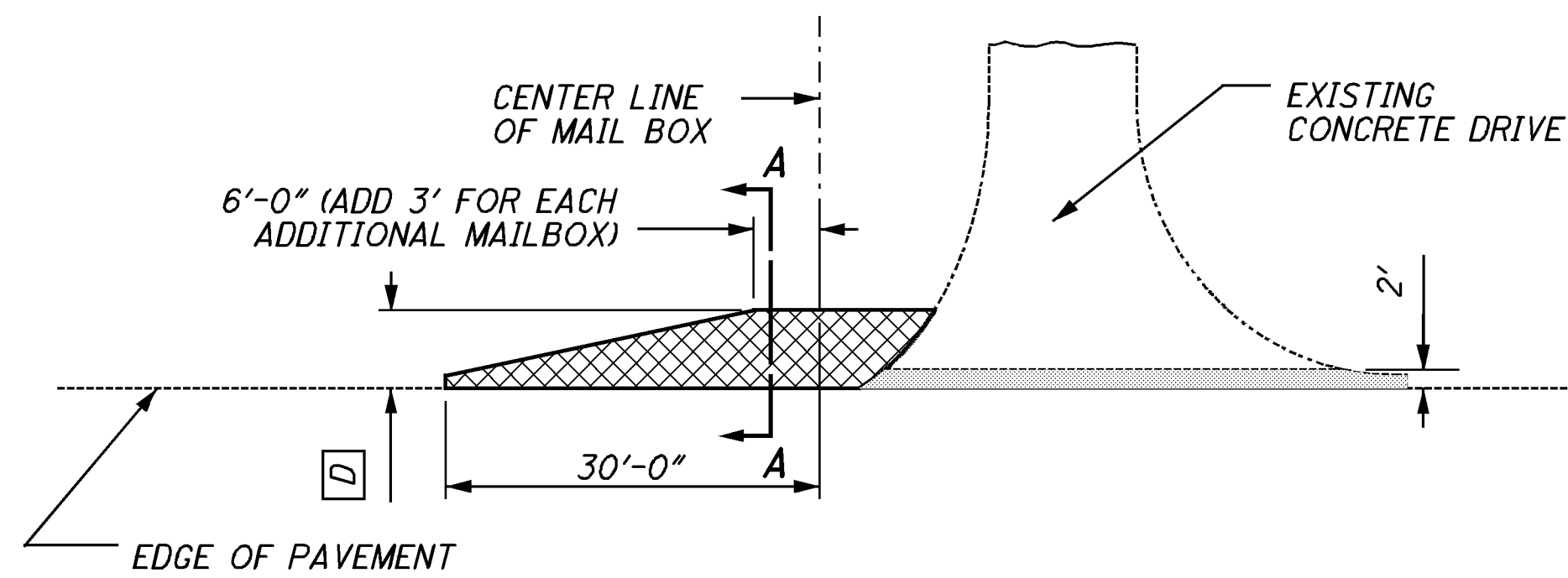


PAVEMENT PLANING TRANSITION DETAIL
VARIABLE DEPTH PAVEMENT PLANING TO ACHIEVE PROPER TRANSITION BETWEEN PLANED AND NON-PLANED TREATMENTS. THIS DETAIL ALSO INCLUDES BUTT JOINTS AT BEGIN AND END LOCATIONS AND AT STRUCTURES.
S.R. 56 & SR 104 INTERSECTIONS
STRUCTURE @ SLM 4.31 OVER THE BIG DARBY CREEK

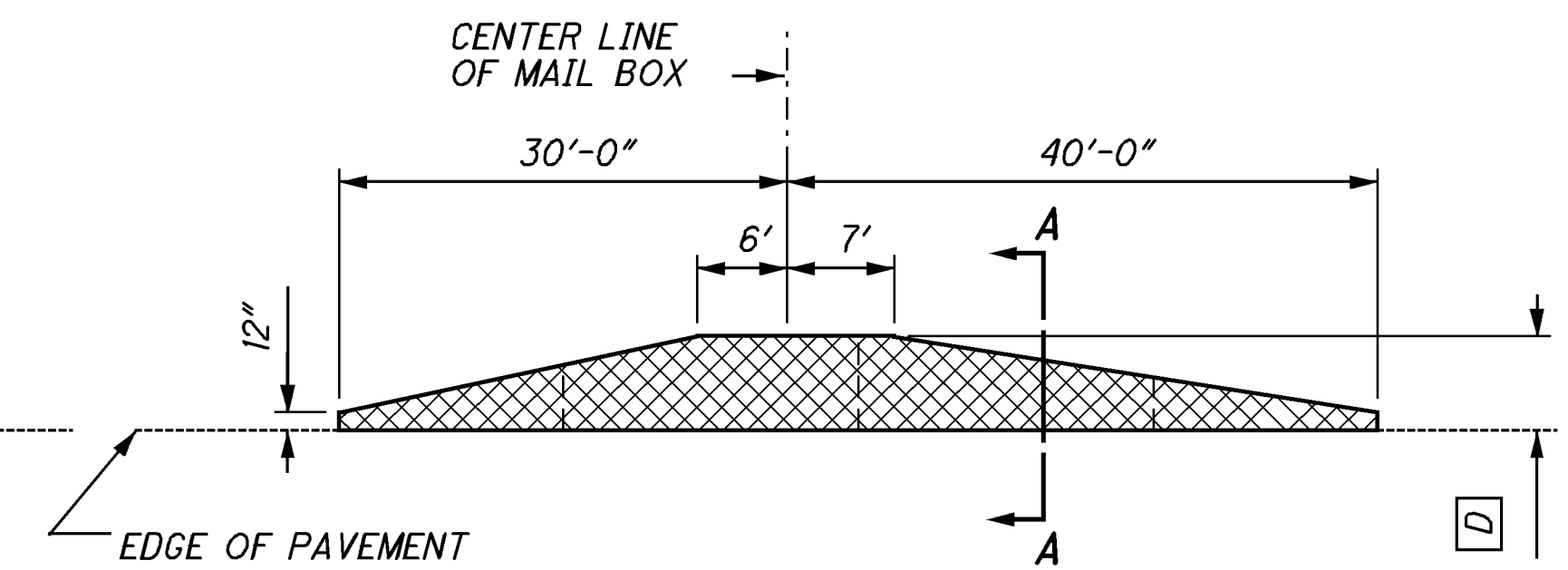
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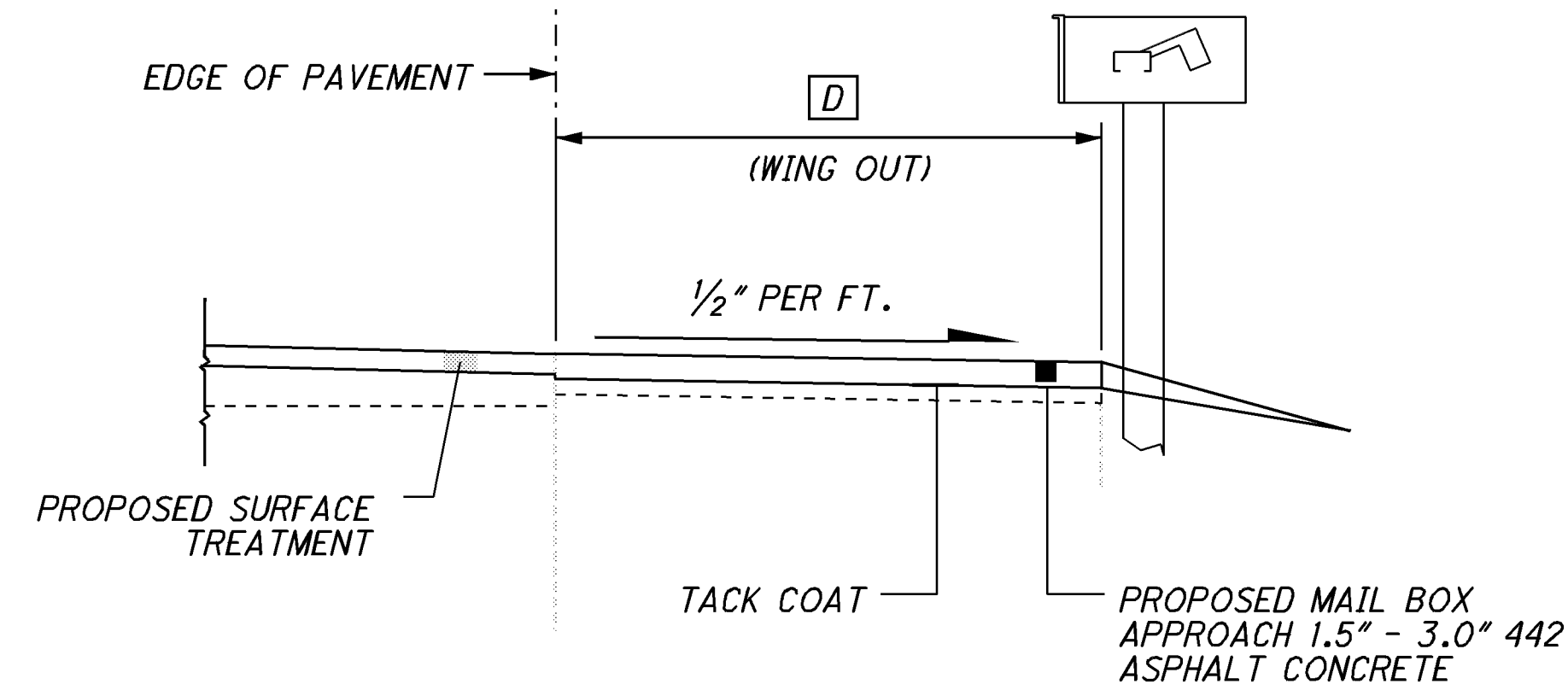
**TYPICAL APPROACH #1
COMBINED DRIVEWAY & MAIL BOX APPROACH**
MAIL BOX POSITIONED PAST DRIVE SHOWN. MIRROR FOR MAILBOX PRECEDING DRIVE. THE EXISTING MAIL BOX POSITION SHALL REMAIN. DIMENSION [D] SHALL NOT EXCEED 6'. [W] = WIDTH OF EXISTING DRIVEWAY.



**TYPICAL APPROACH #2
COMBINED DRIVEWAY & MAIL BOX APPROACH**
MAIL BOX POSITIONED PAST DRIVE SHOWN. MIRROR FOR MAILBOX PRECEDING DRIVE. THE EXISTING MAIL BOX POSITION SHALL REMAIN. DIMENSION [D] SHALL NOT EXCEED 6'. [W] = WIDTH OF EXISTING DRIVEWAY.



**TYPICAL APPROACH #3
TYPICAL MAIL BOX APPROACH**
THE EXISTING MAIL BOX POSITION SHALL REMAIN. DIMENSION [D] SHALL NOT EXCEED 6'. [W] = WIDTH OF EXISTING DRIVEWAY. *FOR TYPICAL MAIL BOX APPROACH AND OTHER ADDITIONAL INFORMATION SEE STA. DWG. BP 4.1 (10/18/07)



**SECTION A-A
MAILBOX APPROACH DETAIL**
IT MAY BE NECESSARY FOR THE CONTRACTOR TO PAVE AN INTERMEDIATE LAYER OF THE PROPOSED ASPHALT MATERIAL PRIOR TO THE "WING OUT" IN ORDER TO MEET LIFT REQUIREMENTS.

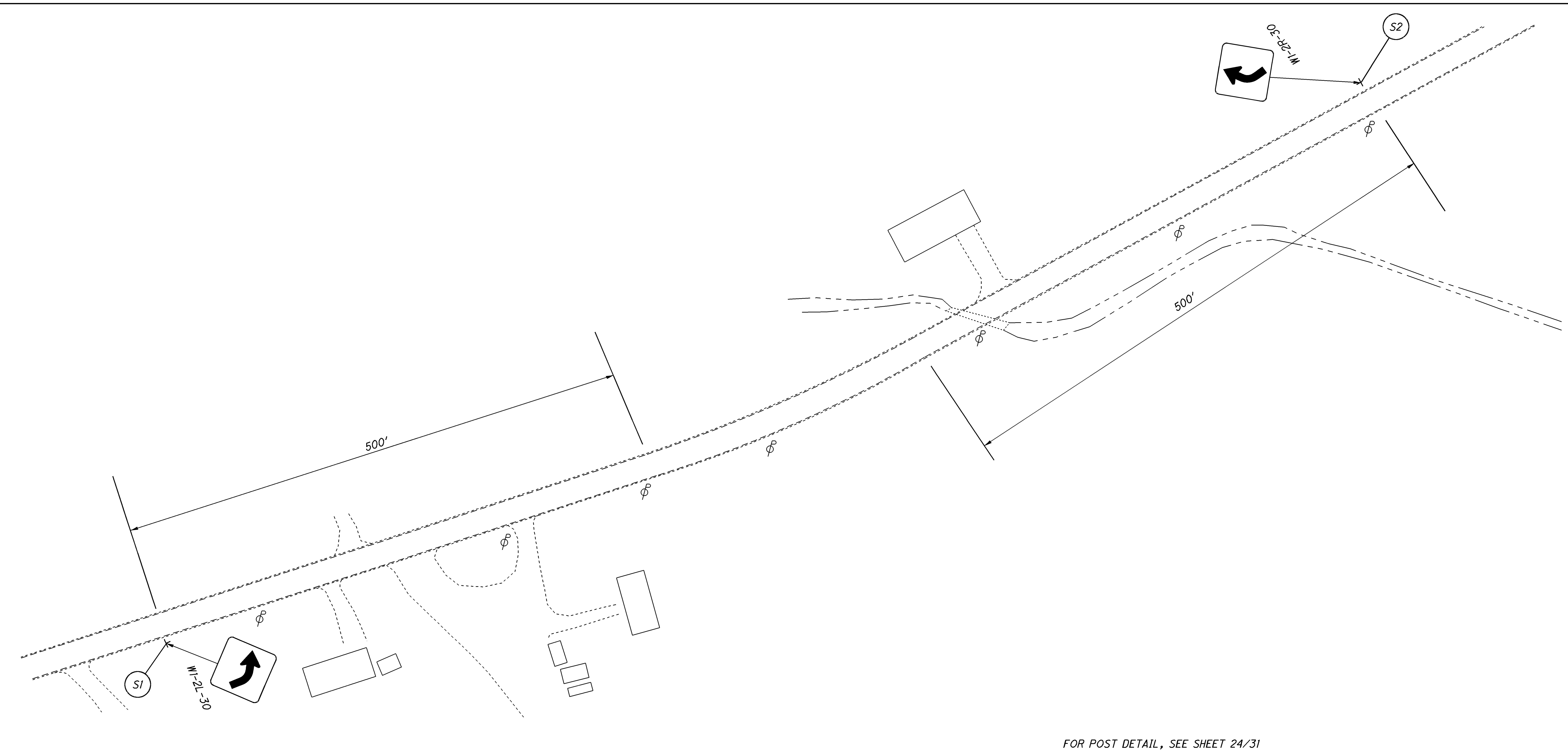
QUANTITIES FOR ALL MAILBOX PULLOUTS ARE CARRIED TO SHEET 2/32.

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TYPICAL DETAILS
MAILBOX APPROACHES AND DRIVEWAYS

PIC-316-0.00

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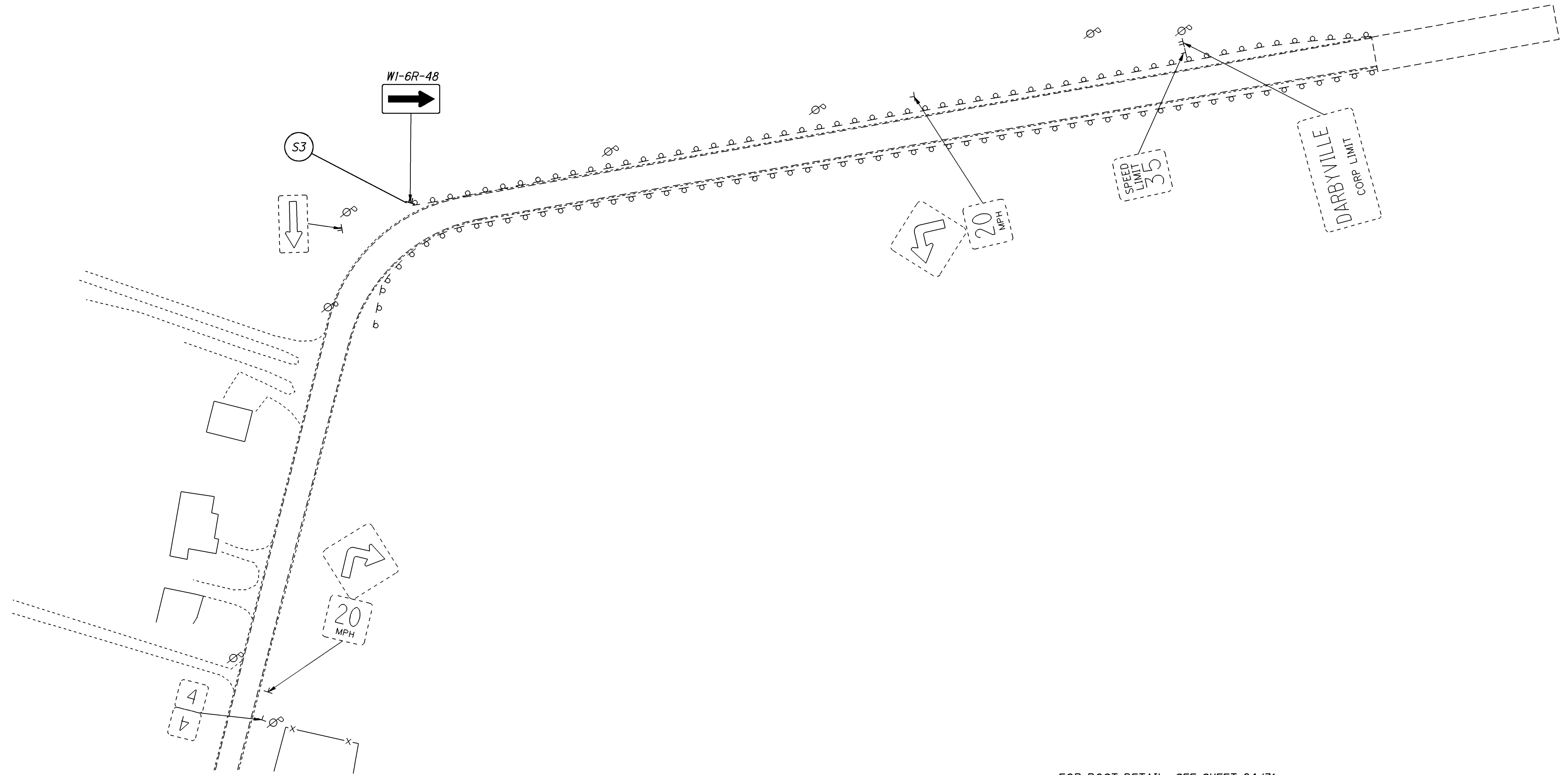
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0 50 100
HORIZONTAL
SCALE IN FEET

SIGN LAYOUT PLAN
SLM 0.94

FOR POST DETAIL, SEE SHEET 24/31

REFERENCE NO.	LOCATION	SLM	SIDE	CODE	SIZE (INCHES)	630															
						GROUND MOUNTED SUPPORT, NO. 2 POST FT	GROUND MOUNTED SUPPORT, NO. 3 POST FT	SIGN, FLAT SHEET SQ FT	SIGN POST REFLECTOR (YELLOW) EACH												
S1		0.94	R	WI-2L	30X30	7	10	6.25	1												
S2		0.94	L	WI-2R	30X30	7	10	6.25	1												
TOTALS CARRIED TO GENERAL SUMMARY						14	20	12.5	2												



FOR POST DETAIL, SEE SHEET 24/31

SHEET NO.	REFERENCE NO.	LOCATION	SLM	SIDE	CODE	SIZE (INCHES)	630														
							GROUND MOUNTED SUPPORT, NO. 2 POST FT	GROUND MOUNTED SUPPORT, NO. 3 POST FT	SIGN, FLAT SHEET SQ FT	SIGN POST REFLECTOR (YELLOW) EACH											
	S3		4.13	L	WI-6R	48X24	14	20	8	2											
TOTALS CARRIED TO GENERAL SUMMARY							14	20	8	2											

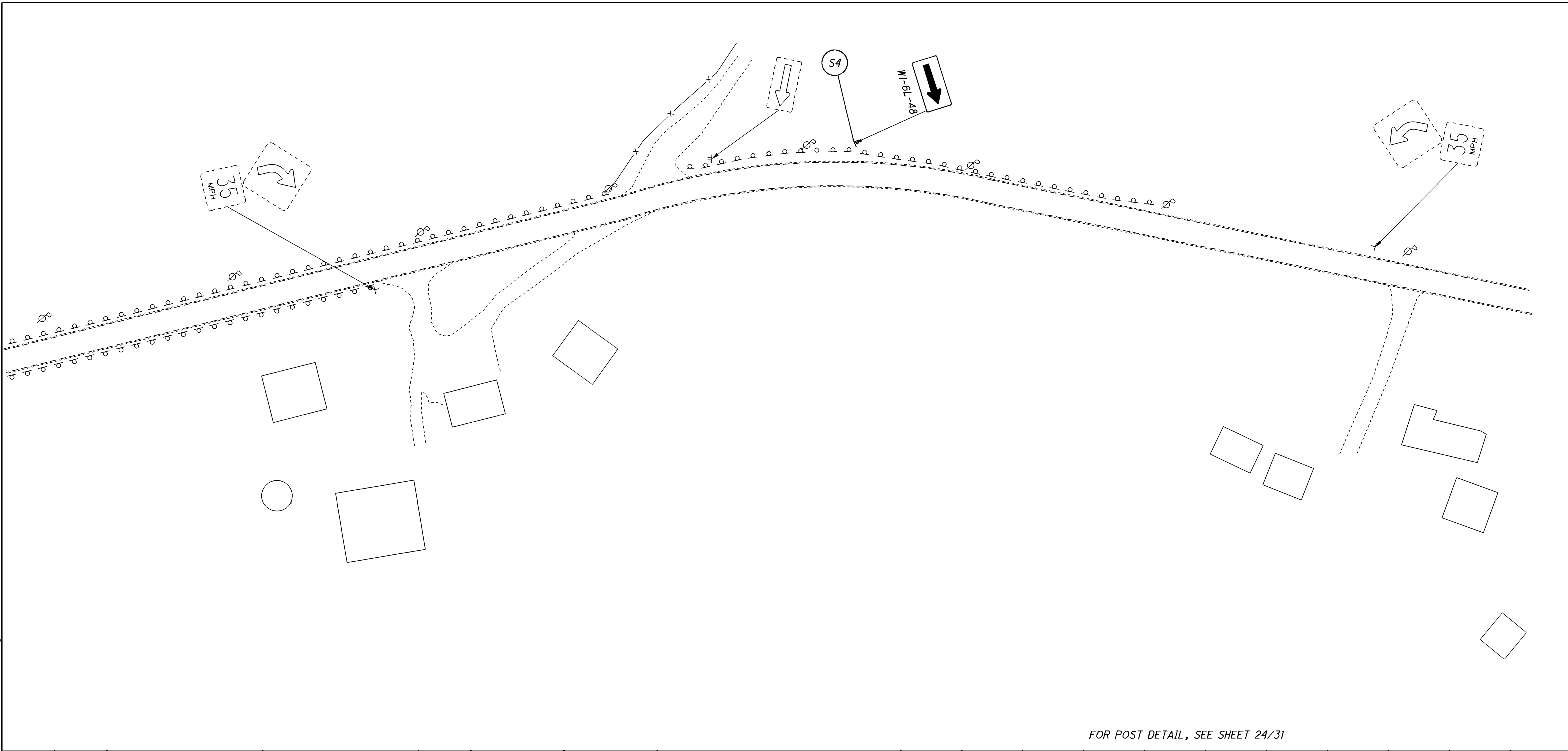
SIGN LAYOUT PLAN
SLM 4.65

CALCULATED: MK
CHECKED:

PIC - 316 - 0.00



19
32

I:\Projects\PIC\316\0000.014\92406\production\roadway\sheets\TRAFFIC_CONTROL.dgn SHEET_SNI04 09-OCT-2012 8:05AM mkatona

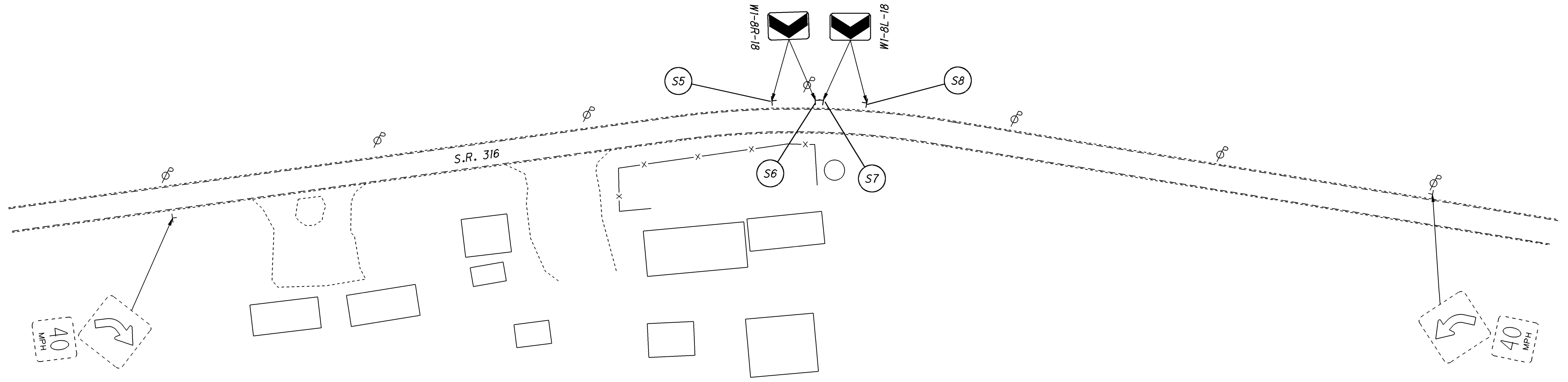


FOR POST DETAIL, SEE SHEET 24/31

SHEET NO.	REFERENCE NO.	LOCATION	SLM	SIDE	CODE	SIZE (INCHES)	630														
							GROUND MOUNTED SUPPORT, NO. 2 POST FT	GROUND MOUNTED SUPPORT, NO. 3 POST FT	SIGN, FLAT SHEET SQ FT	SIGN POST REFLECTOR (YELLOW) EACH											
	S4		4.65	L	WI-6R	48X24	14	20	8	2											
TOTALS CARRIED TO GENERAL SUMMARY							14	20	8	2											



 HORIZONTAL SCALE IN FEET
 CALCULATED MK
 CHECKED
SIGN LAYOUT PLAN
SLM 8.10
PIC - 316 - 0.00
 20
 32

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FOR POST DETAIL, SEE SHEET 24/31

SHEET NO.	REFERENCE NO.	LOCATION	SLM	SIDE	CODE	SIZE (INCHES)	630															
							GROUND MOUNTED SUPPORT, NO. 2 POST FT	GROUND MOUNTED SUPPORT, NO. 3 POST FT	SIGN, FLAT SHEET SQ FT	SIGN POST REFLECTOR (YELLOW) EACH												
	S5		8.10	L	WI-8R	18X24	7	10	3	1												
	S6		8.10	L	WI-8R	18X24	7	10	3	1												
	S7		8.10	L	WI-8L	18X24	7	10	3	1												
	S8		8.10	L	WI-8L	18X24	7	10	3	1												
TOTALS CARRIED TO GENERAL SUMMARY							28	40	12	4												

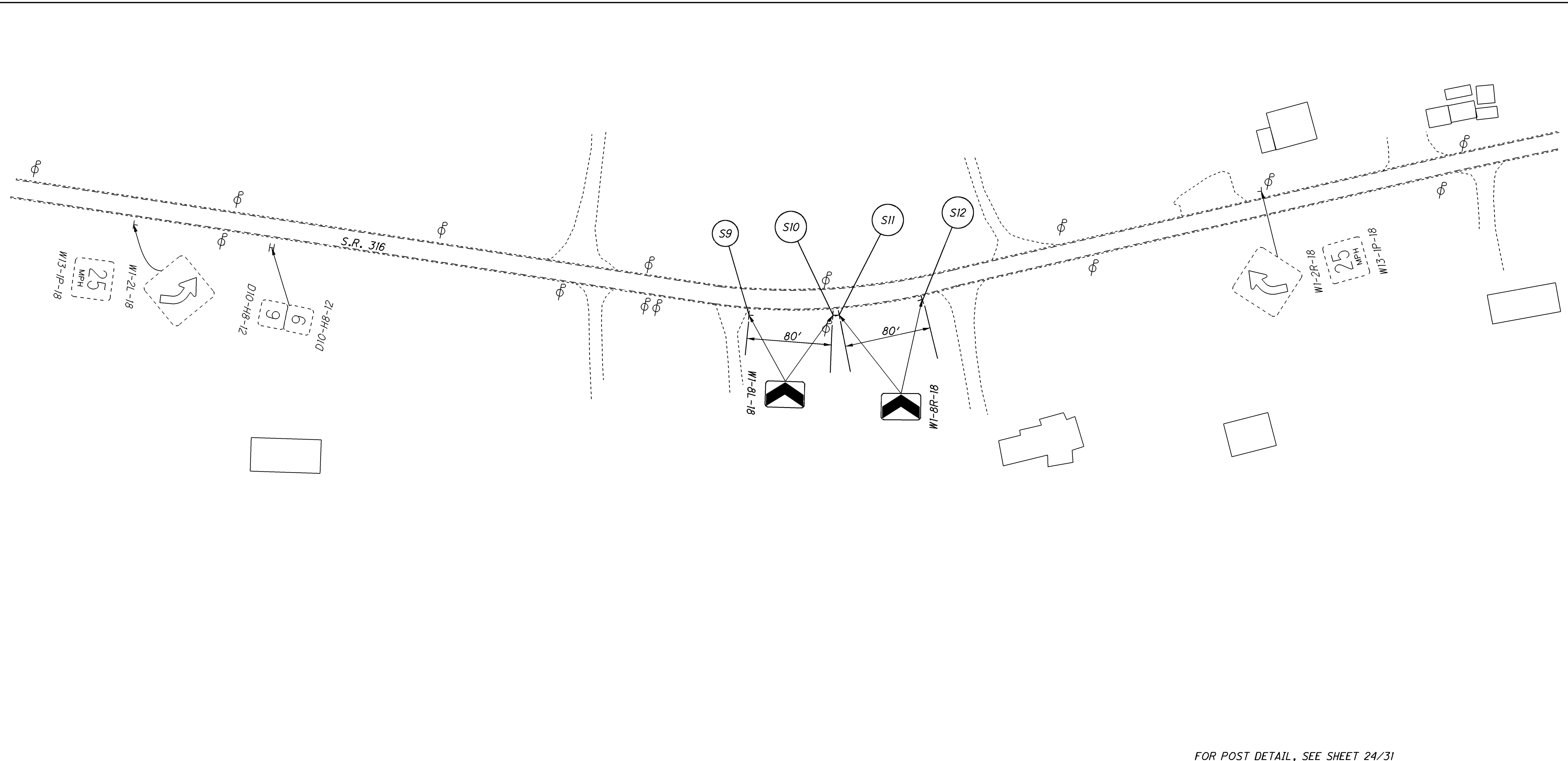
SIGN LAYOUT PLAN
SLM 8.10

PIC - 316 - 0.00

21
32

CALCULATED
MK
CHECKED

0 50 100
HORIZONTAL
SCALE IN FEET



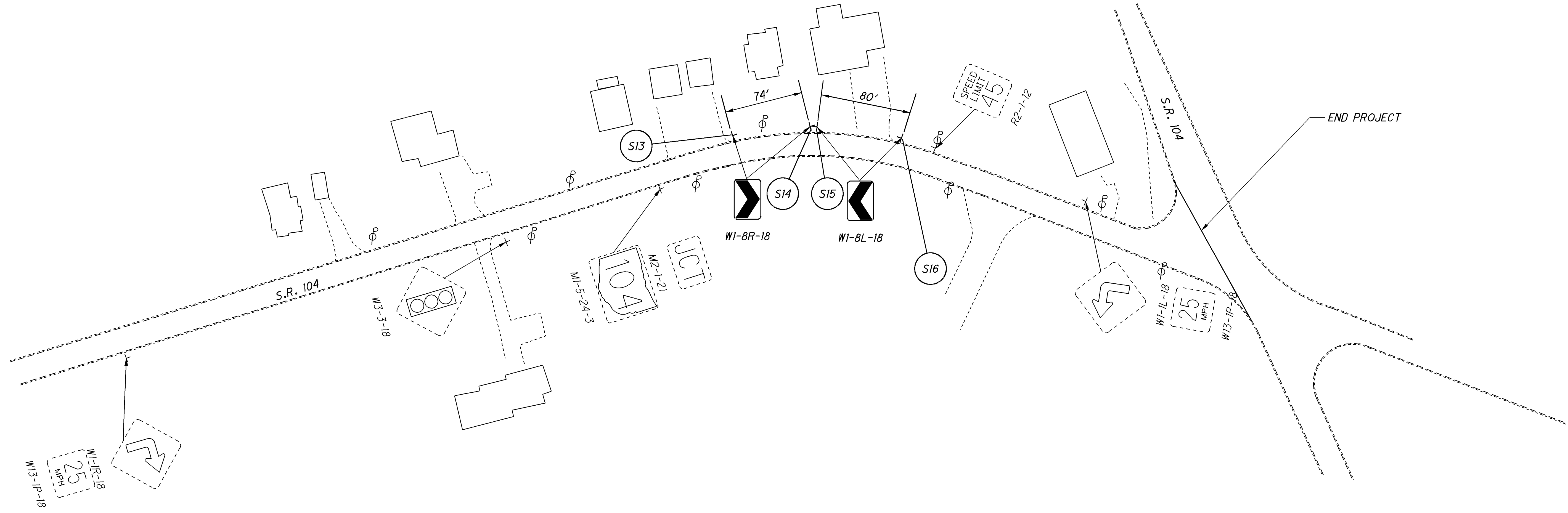
CALCULATED
MK
CHECKED

0 50 100
HORIZONTAL
SCALE IN FEET

SIGN LAYOUT PLAN
SLM 9.40

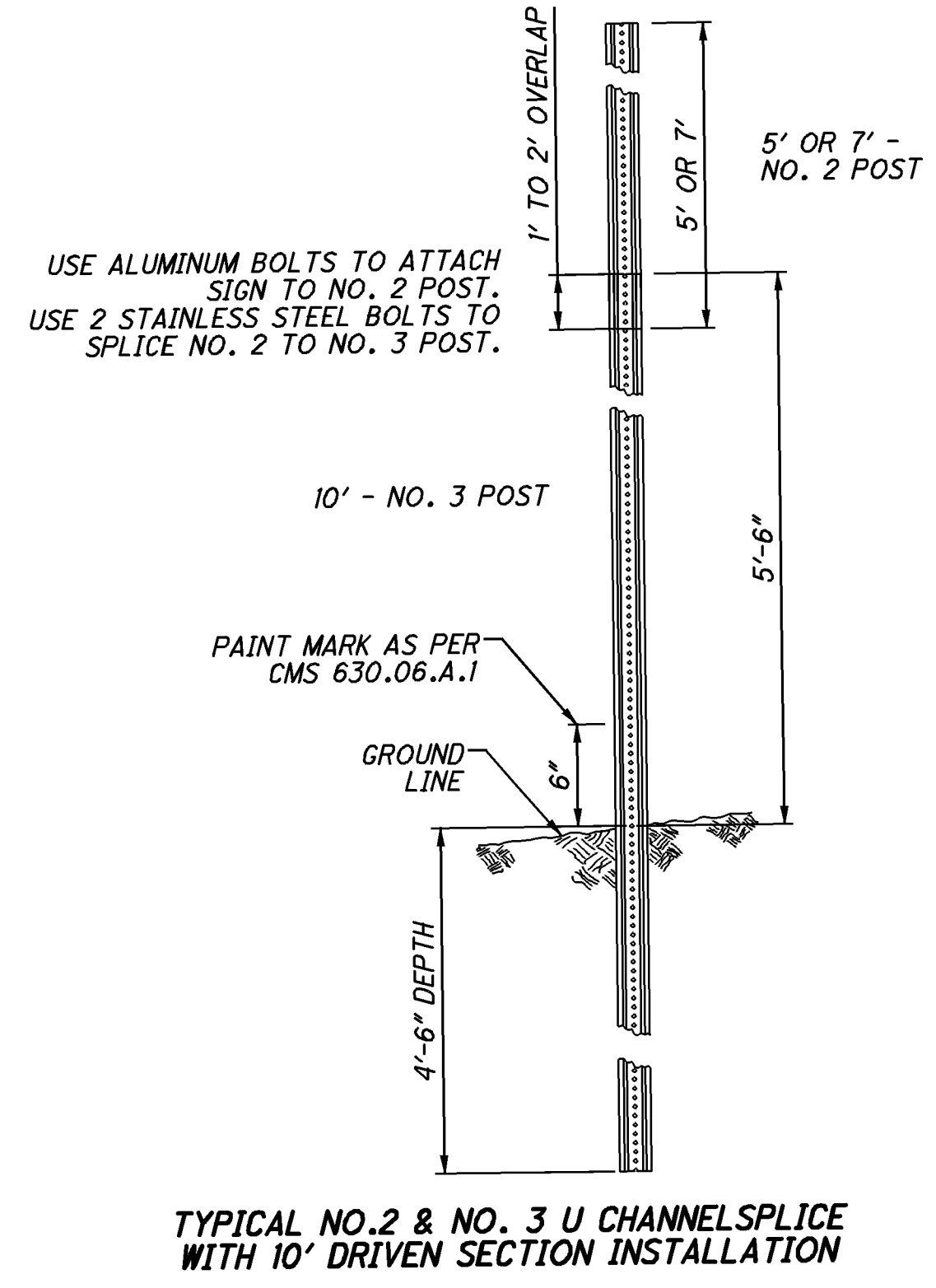
FOR POST DETAIL, SEE SHEET 24/31

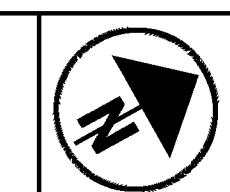
REFERENCE NO.	LOCATION	SLM	SIDE	CODE	SIZE (INCHES)	630															
						GROUND MOUNTED SUPPORT, NO. 2 POST FT	GROUND MOUNTED SUPPORT, NO. 3 POST FT	SIGN, FLAT SHEET SQ FT	SIGN POST REFLECTOR (YELLOW) EACH												
S9		9.10	R	W1-8L	18X24	7	10	3	1												
S10		9.10	R	W1-8L	18X24	7	10	3	1												
S11		9.10	R	W1-8R	18X24	7	10	3	1												
S12		9.10	R	W1-8R	18X24	7	10	3	1												
TOTALS CARRIED TO GENERAL SUMMARY						28	40	12	4												



FOR POST DETAIL, SEE SHEET 24/31

REFERENCE NO.	LOCATION	SLM	SIDE	CODE	SIZE (INCHES)	630														
						GROUND MOUNTED SUPPORT, NO. 2 POST FT	GROUND MOUNTED SUPPORT, NO. 3 POST FT	SIGN, FLAT SHEET SQ FT	SIGN POST REFLECTOR (YELLOW) EACH											
S13		9.40	L	W1-8R	18X24	7	10	3	1											
S14		9.40	L	W1-8R	18X24	7	10	3	1											
S15		9.40	L	W1-8L	18X24	7	10	3	1											
S16		9.40	L	W1-8L	18X24	7	10	3	1											
TOTALS CARRIED TO GENERAL SUMMARY						28	40	12	4											





DESIGN AGENCY
DIST. #6
IN-HOUSE DESIGN

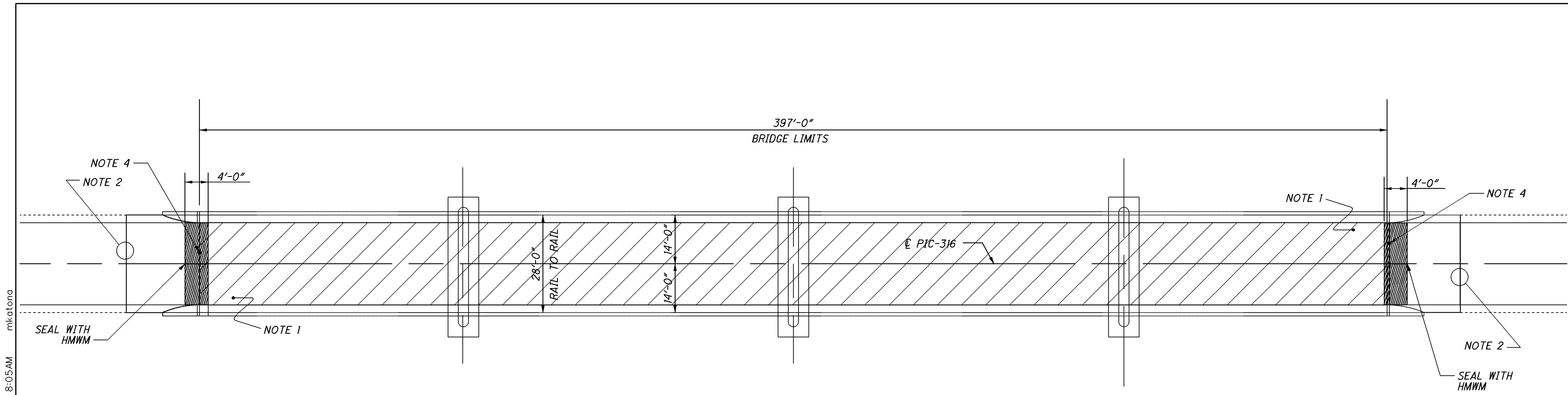
REVIEWED DATE
MAK STRUCTURE FILE NUMBER
6503020

**STRUCTURE PLAN
OVER BIG DARBY CREEK**

PIC-316-0.00
PID. # 92406

1 / 2

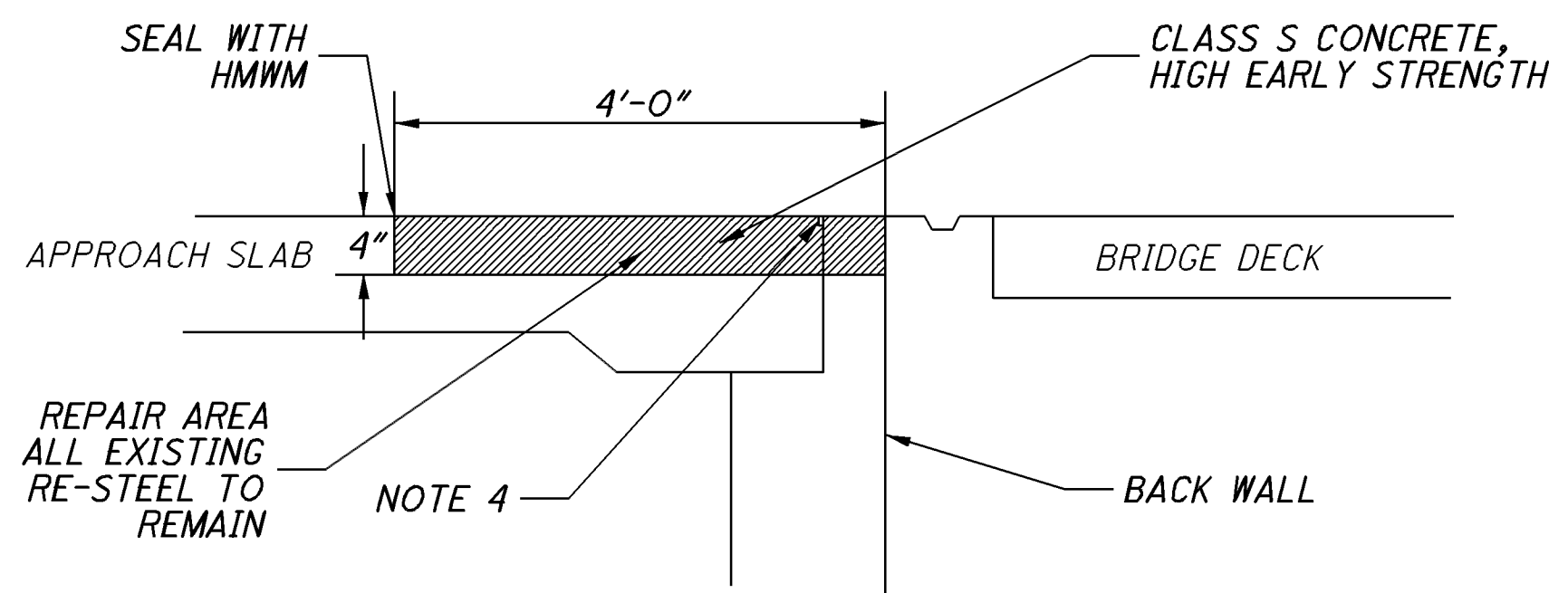
25
32



NOTES:

- NOTE 1:**
DO NOT PAVE DECK. PROPOSED TREATING OF CONCRETE BRIDGE DECK WITH SRS.
- NOTE 2:**
EXISTING BRIDGE (APPROACH) ELEVATION TO MATCH PROPOSED ROADWAY RESURFACING ELEVATION (BY BUTT JOINT). SEE SHEET 15/32 FOR BUTT JOINT DETAIL.
- NOTE 3:**
REPAIR TOP OF BACKWALL AT FORWARD AND REAR ABUTMENTS. REPAIR AREA TO BE 4 FT WIDE BY 4 IN. DEEP, THE FULL WIDTH OF BRIDGE.
- NOTE 4:**
SAW CUT JOINT BETWEEN BACKWALL AND APPROACH SLAB AND INSTALL PREFORMED COMPRESSION JOINT AS PER AS-1-81, DETAIL B.

BRIDGE PLAN
PIC-316-0431 SFN: 6503020
SKEW 0.0°
CONTINUOUS STEEL BEAM OVER BIG DARBY CREEK



LEGEND:

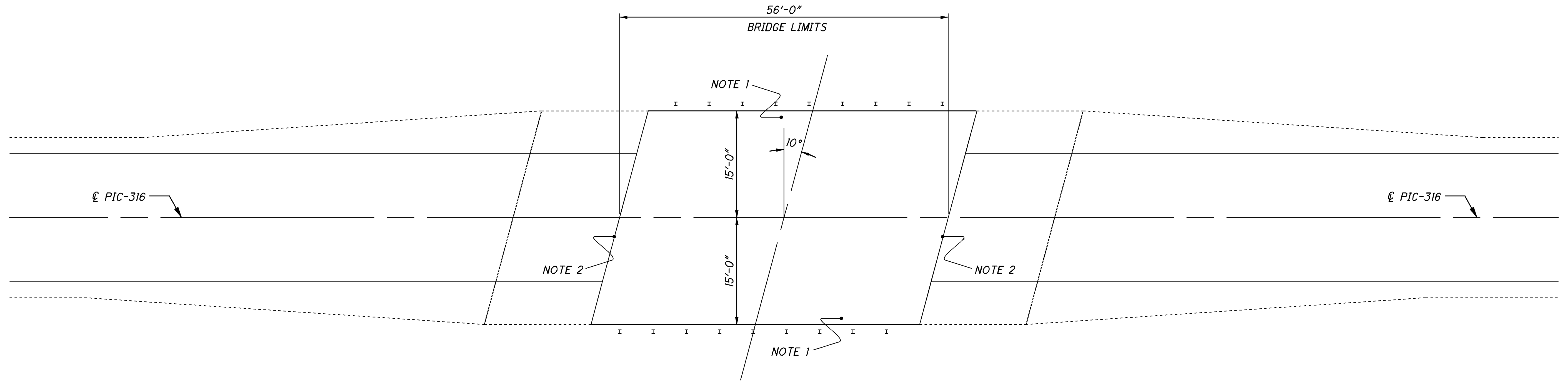
- PROPOSED BRIDGE TREATMENT
- BACKWALL AND APPROACH SLAB REPAIR SEE NOTE 3

REPAIR AREA DETAIL (N.T.S.)
SEE NOTE 3

LOCATION				QUANTITIES					REMARKS	STRUCTURE NOTES
L	C	R	S	202	511	512	516	519		
O	O	O	L							
C	U	U	M	PORTIONS OF STRUCTURE REMOVED, AS PER PLAN	CLASS S CONCRETE, HIGH EARLY STRENGTH	TREATING OF CONCRETE BRIDGE DECK WITH SRS	PREFORMED ELASTOMERIC COMPRESSION JOINT SEAL	SPECIAL PATCHING CONCRETE BRIDGE DECK, TYPE B		
A	T	E		SQ YD	CU YD	SQ YD	FT	SQ YD		
T										
I										
O										
N										
	PIC	316	4.31	25	3	1,233	56	62		
ITEM 202 - PORTIONS OF STRUCTURE REMOVED, AS PER PLAN SHALL INCLUDE THE QUANTITY OF APPROACH SLAB AND BACKWALL CONCRETE AS PROVIDED ON REPAIR DETAIL. ALL EXISTING RE-STEEL IS TO REMAIN IN PLACE.										
PAVEMENT OVERLAY TO MATCH EXISTING ENDS OF BRIDGE BY MILLING BUTT JOINTS.										
PATCH THE EXISTING BRIDGE DECK. AN ESTIMATED QUANTITY OF 5% OF THE BRIDGE DECK SURFACE HAS BEEN PROVIDED. ONCE THE PROJECT ENGINEER HAS IDENTIFIED THE AREAS TO BE REPAIRED, THE AREAS AROUND THE REPAIRS ARE TO BE SOUNDED TO DETERMINE THE LIMITS OF THE PATCH. DO NOT SOUND THE ENTIRE DECK.										
PIC-316-0431 SFN: 6503020										
TOTALS CARRIED TO BRIDGE SUMMARY				25	3	1,233	56	62		

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NOTES:
 NOTE 1:
 PAVE OVER BRIDGE, SAME TREATMENT AS ROADWAY
 NOTE 2: SAW AND SEAL JOINTS OVER ABUTMENT JOINT.

BRIDGE PLAN
 PIC-316-0168 SFN: 6502989
 PRESTRESSED CONCRETE BOX BEAM OVER DRY RUN CREEK

QUANTITIES				REMARKS	STRUCTURE NOTES
	516				PIC-316-0168 SFN: 6502989
	SPECIAL - SAWING AND SEALING BITUMINOUS CONCRETE JOINTS				PAVE OVER BRIDGE USING SAME TREATMENT AS ROADWAY
	FT				
	60			SAW AND SEAL JOINTS OVER ABUTMENT JOINT	
	60			CARRIED TO BRIDGE SUMMARY	



DESIGN AGENCY
 DIST. #6
 IN-HOUSE DESIGN

REVIEWED DATE
 MAK STRUCTURE FILE NUMBER
 6502989

**STRUCTURE PLAN
 OVER DRY RUN CREEK**

PIC-316-0.00
 PID. # 92406

2 / 2

26
 32

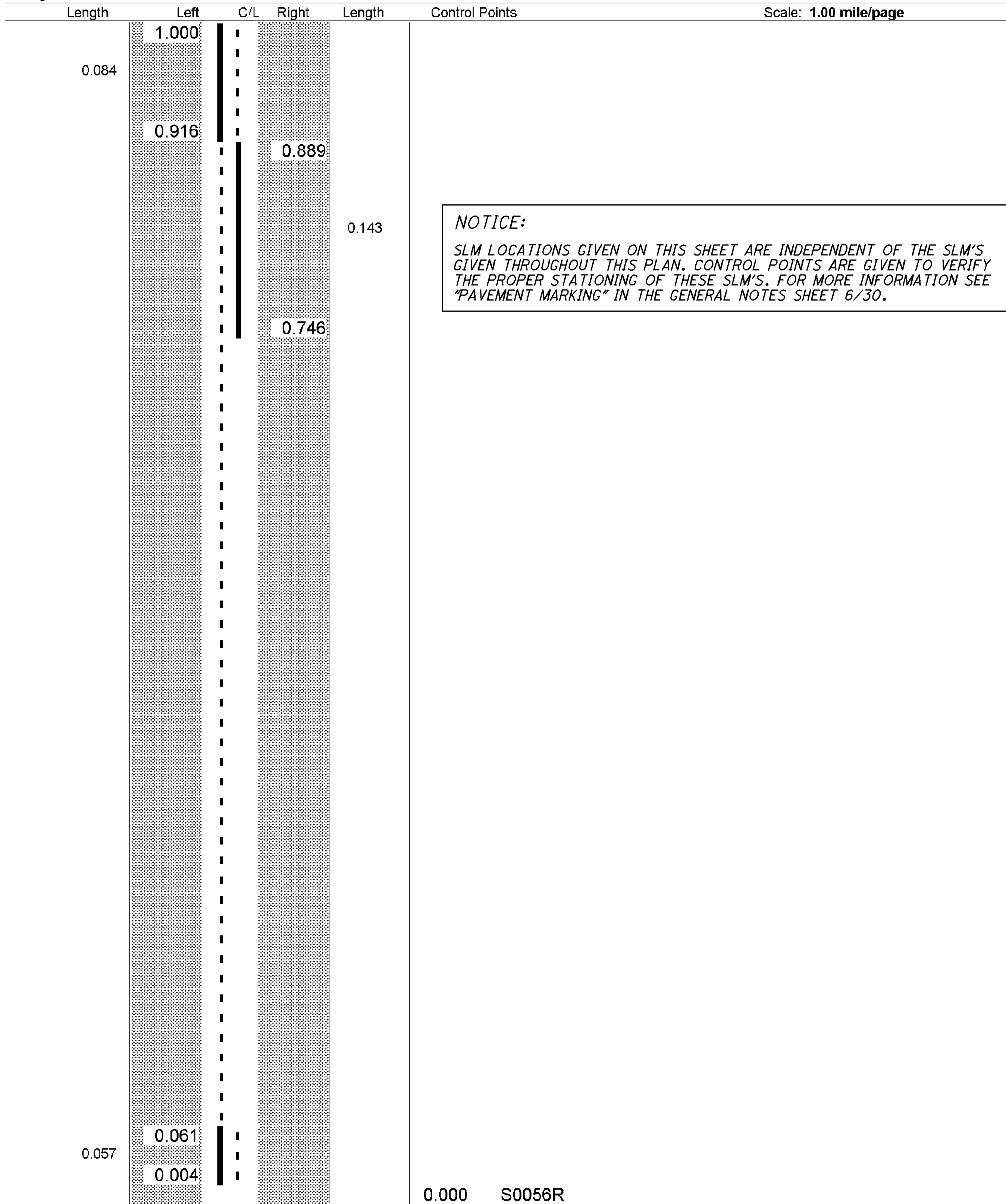
**PIC-316 SLM 0.00 TO SLM 9.48
PASSING AND NO PASSING ZONES**

CALCULATED
MK
CHECKED

**ODOT District 6
No Passing Zone Log (PIC)**

Page: 1 of 10

Route: **0316R (SPICSR00316**C)** From: **0.000 S0056R**
 Length: **9.480** Direction: **East** To: **9.480 S0752R**



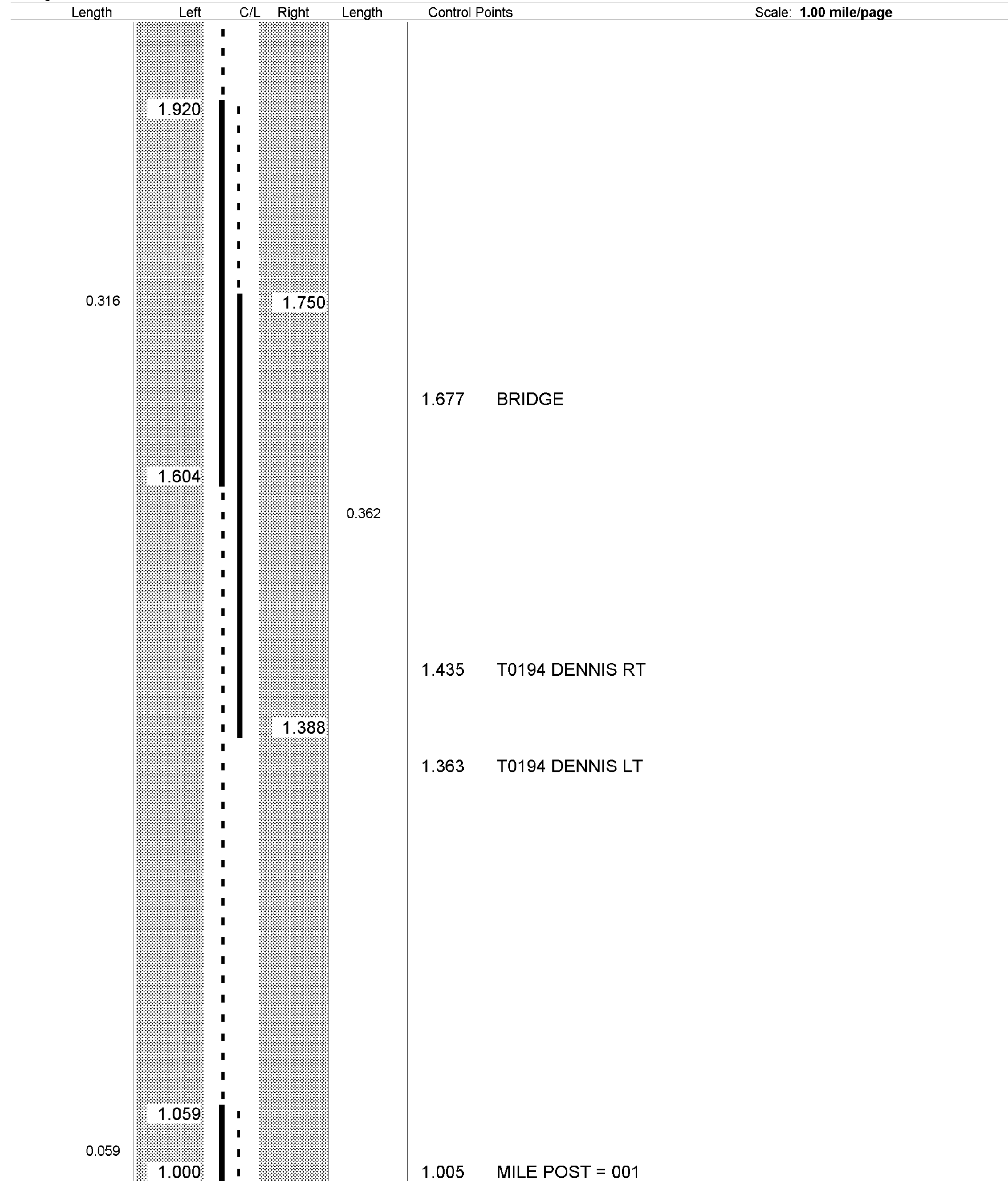
NOTICE:
 SLM LOCATIONS GIVEN ON THIS SHEET ARE INDEPENDENT OF THE SLM'S GIVEN THROUGHOUT THIS PLAN. CONTROL POINTS ARE GIVEN TO VERIFY THE PROPER STATIONING OF THESE SLM'S. FOR MORE INFORMATION SEE "PAVEMENT MARKING" IN THE GENERAL NOTES SHEET 6/30.

Sheet total equivalent length of solid line: 0.533 Accum. total: 0.533 © 2012 - MasterMind Systems, Inc. - Traffic Safety Suite

**ODOT District 6
No Passing Zone Log (PIC)**

Page: 2 of 10

Route: **0316R (SPICSR00316**C)** From: **0.000 S0056R**
 Length: **9.480** Direction: **East** To: **9.480 S0752R**



Sheet total equivalent length of solid line: 0.951 Accum. total: 1.484 © 2012 - MasterMind Systems, Inc. - Traffic Safety Suite

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**PICKAWAY COUNTY
PASSING - NO PASSING ZONES**

PIC-316-0.00

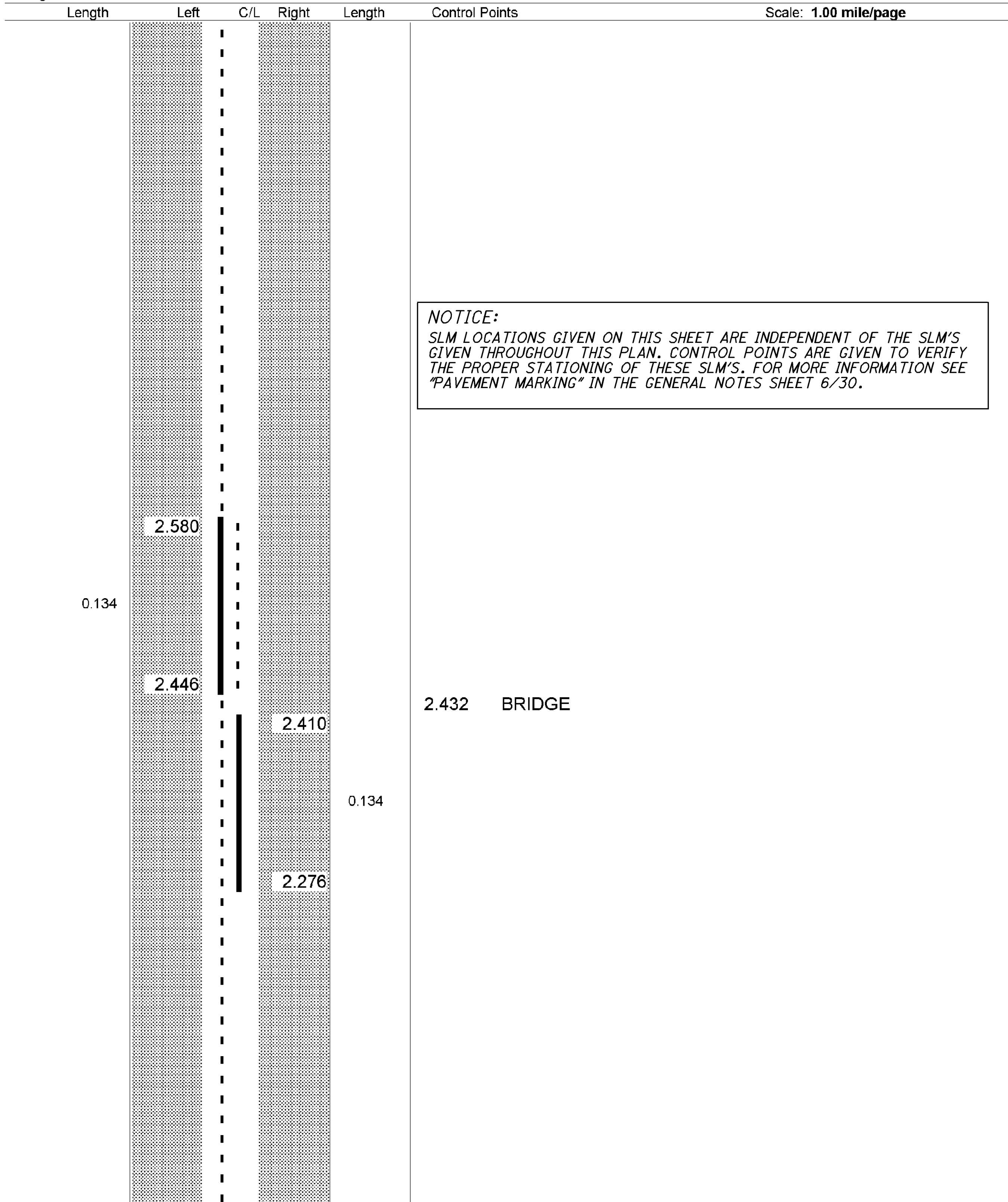
28
32

**PIC-316 SLM 0.00 TO SLM 9.48
PASSING AND NO PASSING ZONES**

**ODOT District 6
No Passing Zone Log (PIC)**

Page: 3 of 10

Route: **0316R (SPICSR00316**C)** From: **0.000 S0056R**
 Length: **9.480** Direction: **East** To: **9.480 S0752R**



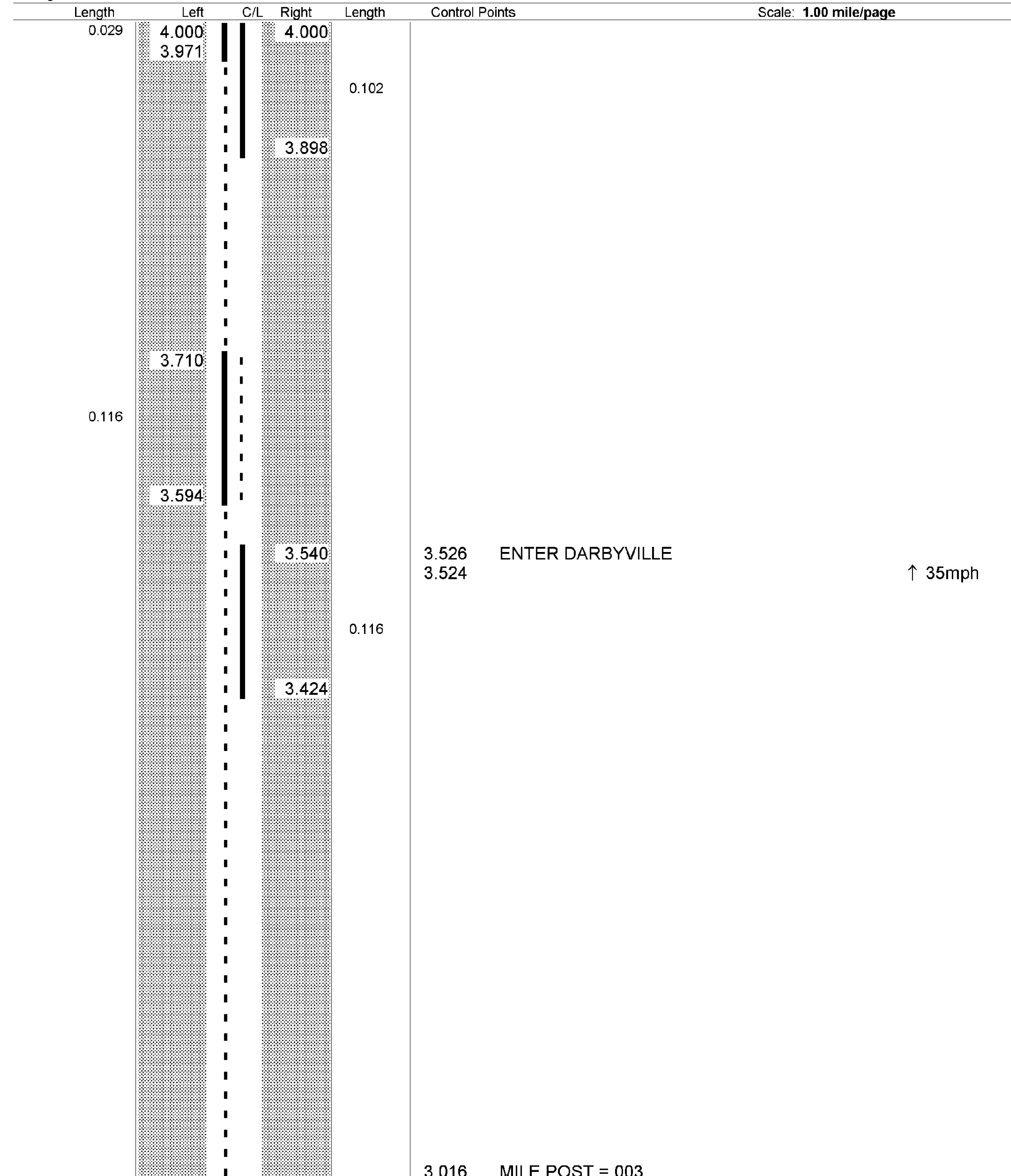
NOTICE:
 SLM LOCATIONS GIVEN ON THIS SHEET ARE INDEPENDENT OF THE SLM'S GIVEN THROUGHOUT THIS PLAN. CONTROL POINTS ARE GIVEN TO VERIFY THE PROPER STATIONING OF THESE SLM'S. FOR MORE INFORMATION SEE "PAVEMENT MARKING" IN THE GENERAL NOTES SHEET 6/30.

Sheet total equivalent length of solid line: 0.518 Accum. total: 2.002 © 2012 - MasterMind Systems, Inc. - Traffic Safety Suite

**ODOT District 6
No Passing Zone Log (PIC)**

Page: 4 of 10

Route: **0316R (SPICSR00316**C)** From: **0.000 S0056R**
 Length: **9.480** Direction: **East** To: **9.480 S0752R**



Sheet total equivalent length of solid line: 0.606 Accum. total: 2.608 © 2012 - MasterMind Systems, Inc. - Traffic Safety Suite

CALCULATED
MK
CHECKED

**PICKAWAY COUNTY
PASSING - NO PASSING ZONES**

PIC-316-0.00

29
32

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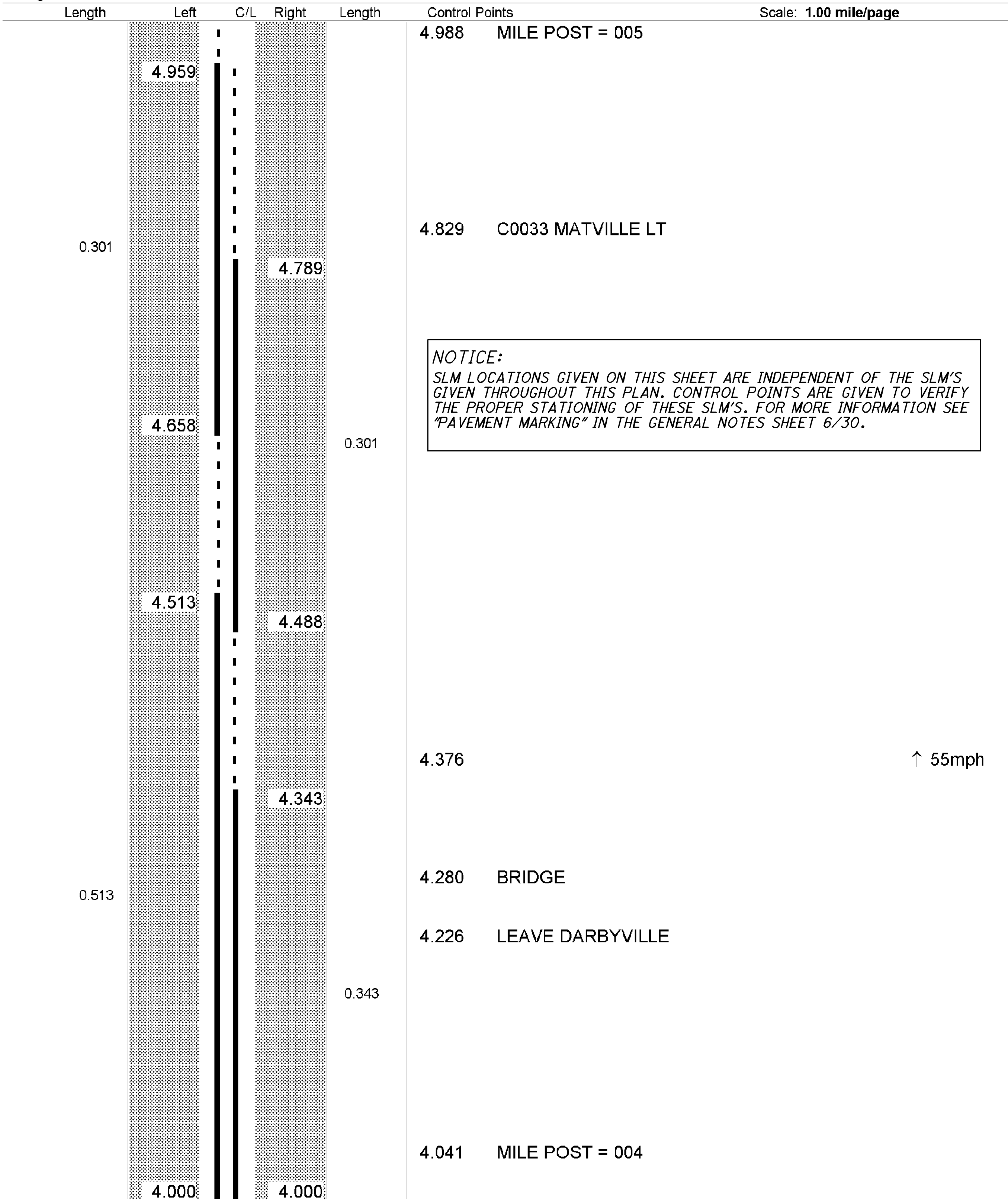
**PIC-316 SLM 0.00 TO SLM 9.48
PASSING AND NO PASSING ZONES**

CALCULATED
MK
CHECKED

**ODOT District 6
No Passing Zone Log (PIC)**

Page: 5 of 10

Route: **0316R (SPICSR00316**C)** From: **0.000 S0056R**
 Length: **9.480** Direction: **East** To: **9.480 S0752R**



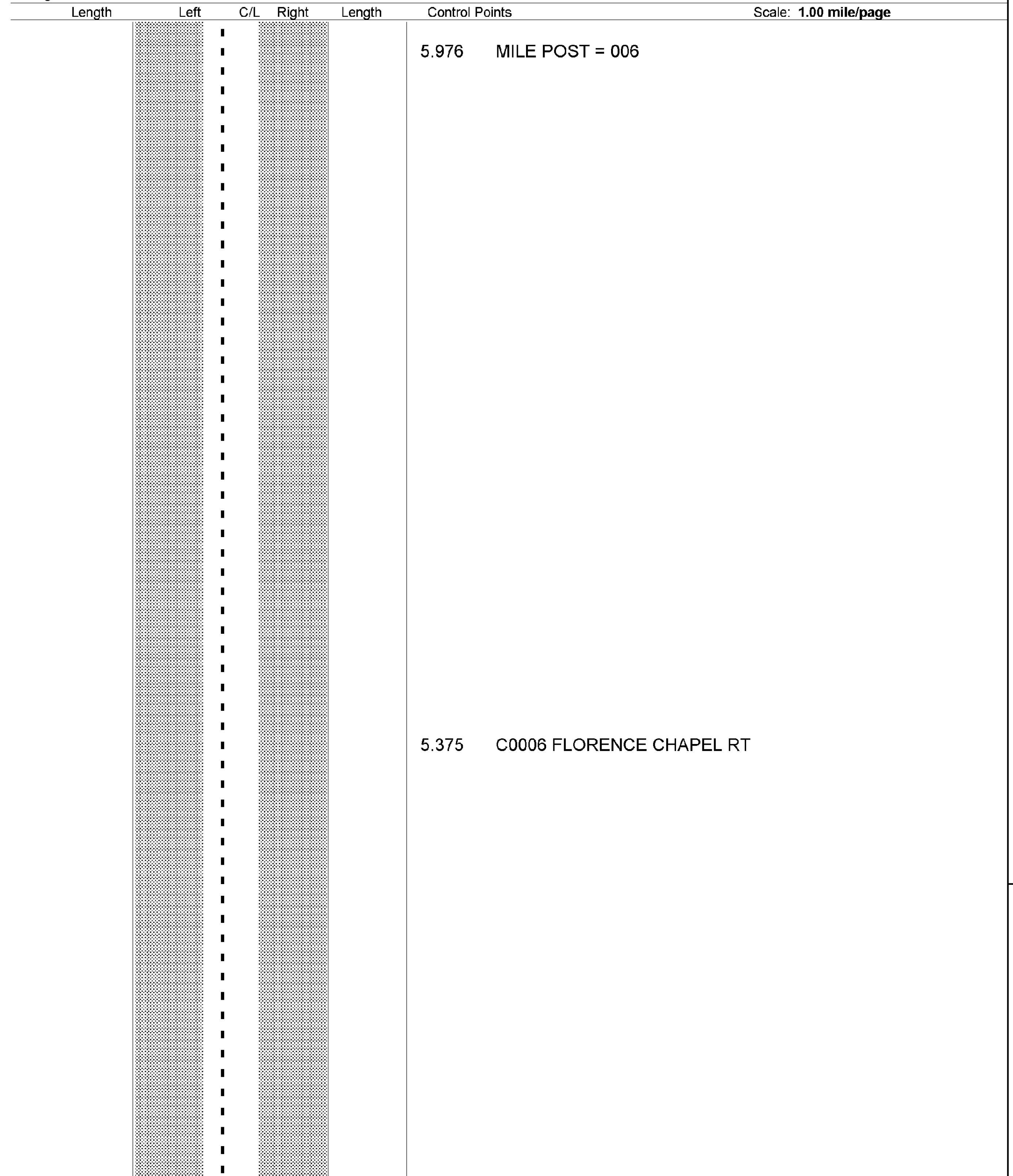
NOTICE:
 SLM LOCATIONS GIVEN ON THIS SHEET ARE INDEPENDENT OF THE SLM'S GIVEN THROUGHOUT THIS PLAN. CONTROL POINTS ARE GIVEN TO VERIFY THE PROPER STATIONING OF THESE SLM'S. FOR MORE INFORMATION SEE "PAVEMENT MARKING" IN THE GENERAL NOTES SHEET 6/30.

Sheet total equivalent length of solid line: 1.583 Accum. total: 4.191 © 2012 - MasterMind Systems, Inc. - Traffic Safety Suite

**ODOT District 6
No Passing Zone Log (PIC)**

Page: 6 of 10

Route: **0316R (SPICSR00316**C)** From: **0.000 S0056R**
 Length: **9.480** Direction: **East** To: **9.480 S0752R**



Sheet total equivalent length of solid line: 0.250 Accum. total: 4.441 © 2012 - MasterMind Systems, Inc. - Traffic Safety Suite

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**PICKAWAY COUNTY
PASSING - NO PASSING ZONES**

PIC-316-0.00

30
32

**PIC-316 SLM 0.00 TO SLM 9.48
PASSING AND NO PASSING ZONES**

CALCULATED
MK
CHECKED

**ODOT District 6
No Passing Zone Log (PIC)**

Page: 7 of 10

Route: **0316R (SPICSR00316**C)** From: **0.000 S0056R**
 Length: **9.480** Direction: **East** To: **9.480 S0752R**

Length	Left	C/L	Right	Length	Control Points
				6.982	MILE POST = 007
				6.920	T0148 MESSMORE LT

NOTICE:
 SLM LOCATIONS GIVEN ON THIS SHEET ARE INDEPENDENT OF THE SLM'S GIVEN THROUGHOUT THIS PLAN. CONTROL POINTS ARE GIVEN TO VERIFY THE PROPER STATIONING OF THESE SLM'S. FOR MORE INFORMATION SEE "PAVEMENT MARKING" IN THE GENERAL NOTES SHEET 6/30.

Sheet total equivalent length of solid line: 0.250 Accum. total: 4.691 © 2012 - MasterMind Systems, Inc. - Traffic Safety Suite

**ODOT District 6
No Passing Zone Log (PIC)**

Page: 8 of 10

Route: **0316R (SPICSR00316**C)** From: **0.000 S0056R**
 Length: **9.480** Direction: **East** To: **9.480 S0752R**

Length	Left	C/L	Right	Length	Control Points
			8.000		
0.078			7.922	7.955	MILE POST = 008
				7.685	C0004 COMMERCIAL POINT
				7.160	BRIDGE
				7.054	T0132 TURNEY CALDWELL RT

Sheet total equivalent length of solid line: 0.328 Accum. total: 5.019 © 2012 - MasterMind Systems, Inc. - Traffic Safety Suite

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**PICKAWAY COUNTY
PASSING - NO PASSING ZONES**

PIC-316-0.00

31
32

**PIC-316 SLM 0.00 TO SLM 9.48
PASSING AND NO PASSING ZONES**

CALCULATED
MK
CHECKED

**ODOT District 6
No Passing Zone Log (PIC)**

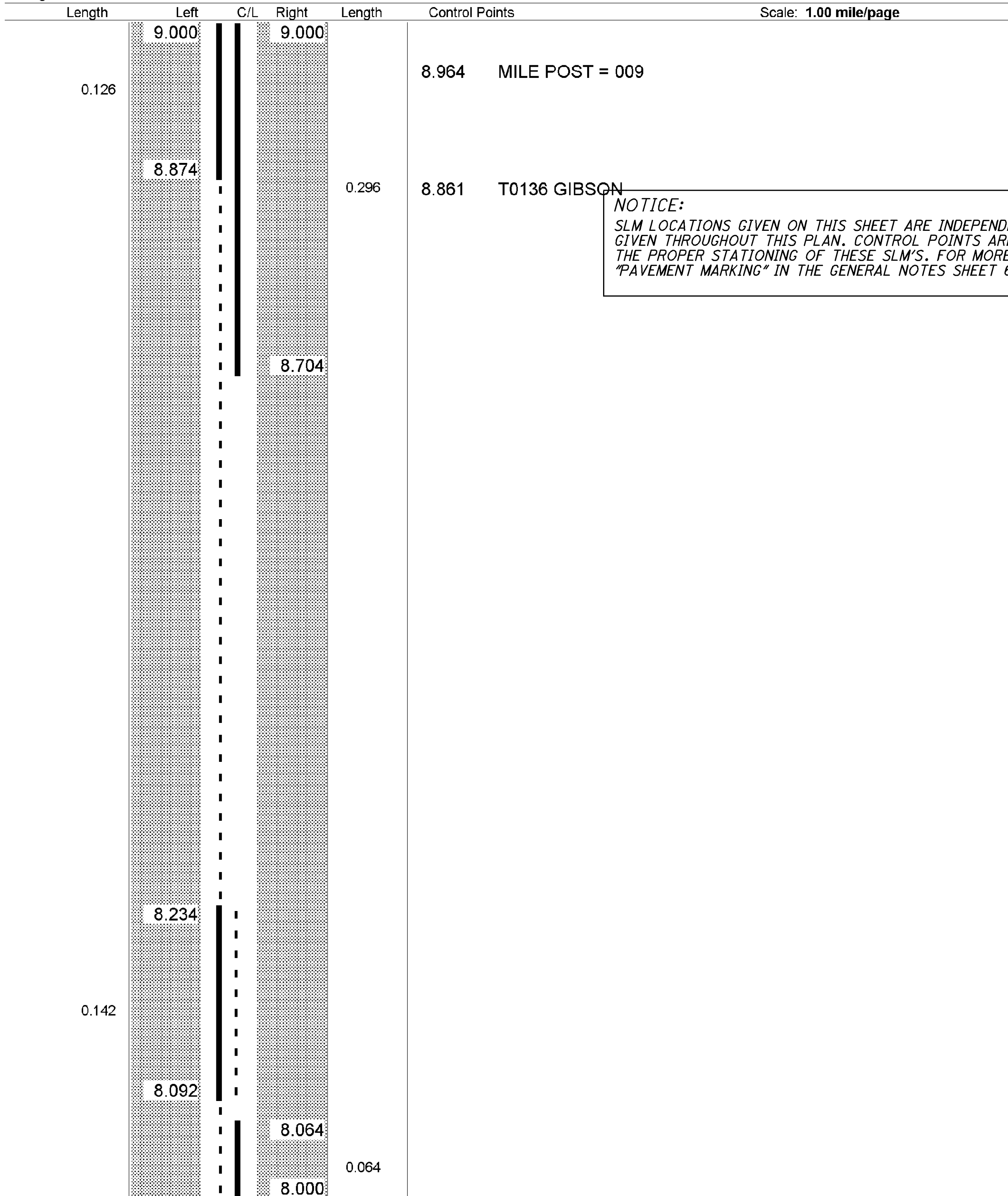
Page: 9 of 10

**ODOT District 6
No Passing Zone Log (PIC)**

Page: 10 of 10

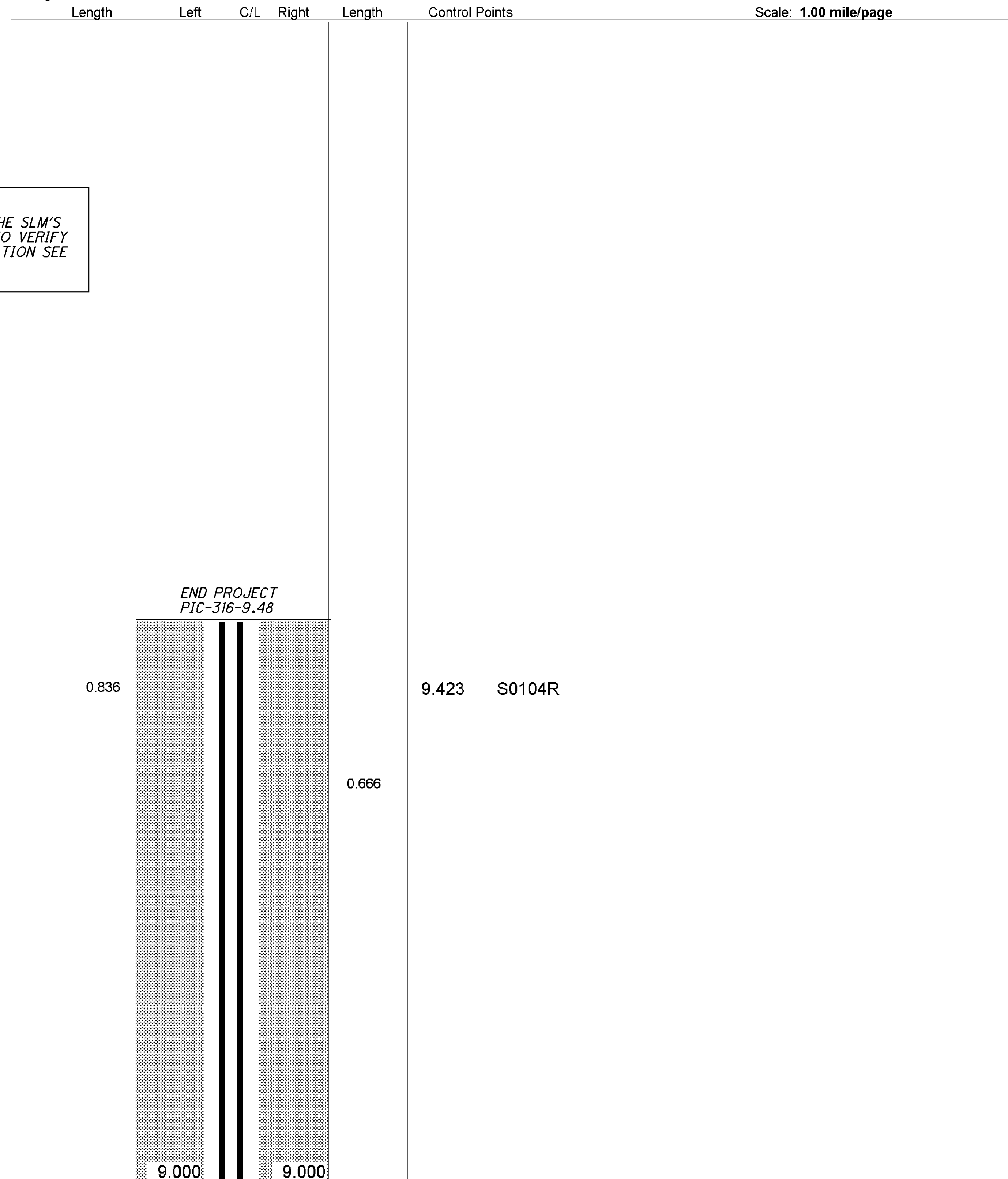
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Length: **9.480** Direction: **East** To: **9.480 S0752R**

Route: **0316R (SPICSR00316**C)** From: **0.000 S0056R**
Length: **9.480** Direction: **East** To: **9.480 S0752R**



NOTICE:
SLM LOCATIONS GIVEN ON THIS SHEET ARE INDEPENDENT OF THE SLM'S GIVEN THROUGHOUT THIS PLAN. CONTROL POINTS ARE GIVEN TO VERIFY THE PROPER STATIONING OF THESE SLM'S. FOR MORE INFORMATION SEE "PAVEMENT MARKING" IN THE GENERAL NOTES SHEET 6/30.

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**STRUCTURE PLAN
OVER BIG DARBY CREEK**

PIC-316-0.00

32
32