

STATE OF OHIO  
DEPARTMENT OF TRANSPORTATION

RIC-30-16.42

MIFFLIN TOWNSHIP  
RICHLAND COUNTY

PROJECT DESCRIPTION

REPAIR STRUCTURE DUE TO ACCIDENT BY HEAT STRAIGHTENING, CROSS FRAME ASSEMBLY REPLACEMENT AND PAINTING.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA = N/A  
ESTIMATED CONTRACTOR EARTH DISTURBED AREA = N/A  
NOTICE OF INTENT EARTH DISTURBED AREA = N/A

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

2013 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

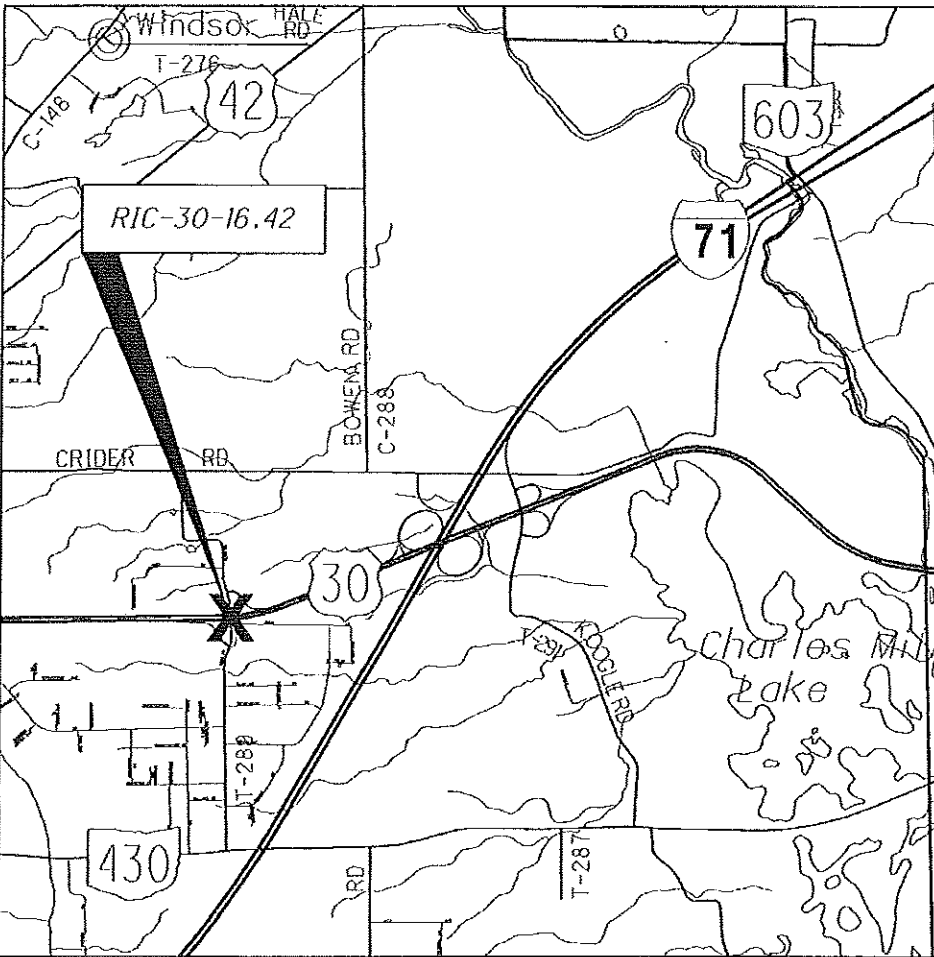
I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY EXCEPT FOR THE WESTBOUND ON AND OFF RAMP TO REED ROAD AS DESCRIBED ON SHEET 4 AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

UNDER AUTHORITY OF SECTION 4511.21, DIVISION (H) OF THE OHIO REVISED CODE, THE REVISED PRIMA FACIE SPEED LIMITS AS INDICATED HEREIN ARE DETERMINED TO BE REASONABLE AND SAFE, AND ARE HEREBY ESTABLISHED FOR THE DURATION OF THIS PROJECT. THE PRIMA FACIE SPEED LIMIT OR LIMITS HEREBY ESTABLISHED SHALL BECOME EFFECTIVE WHEN APPROPRIATE SIGNS GIVING NOTICE THEREOF ARE ERECTED.

APPROVED   
DATE 5-1-14 DISTRICT DEPUTY DIRECTOR

APPROVED   
DATE 5-20-14 DIRECTOR, DEPARTMENT OF TRANSPORTATION

**UNDERGROUND UTILITIES**  
CONTACT BOTH SERVICES  
CALL TWO WORKING DAYS  
BEFORE YOU DIG  
CALL  
1-800-362-2764  
(TOLL FREE)  
OHIO UTILITIES PROTECTION SERVICE  
NON-MEMBERS  
MUST BE CALLED DIRECTLY  
OIL & GAS PRODUCERS UNDERGROUND  
PROTECTION SERVICE CALL: 1-800-925-0988



DESIGN EXCEPTIONS  
NONE REQUIRED

LOCATION MAP

SCALE IN MILES



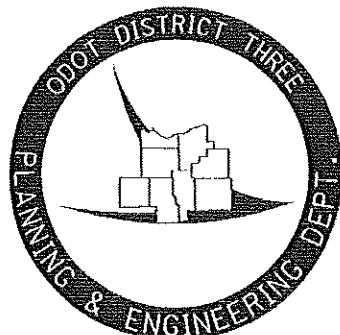
LATITUDE: 40° 46' 43" N  
LONGITUDE: 82° 25' 37" W

DESIGN DESIGNATION

CURRENT ADT (2014)	20,000
DESIGN YEAR ADT (2034)	24,000
DESIGN HOURLY VOLUME (2034)	2,200
DIRECTIONAL DISTRIBUTION	53%
TRUCKS (24 HOUR B&C)	27%
DESIGN SPEED	60 MPH
LEGAL SPEED	60 MPH

DESIGN FUNCTIONAL CLASSIFICATION:  
URBAN FREEWAY & EXPRESSWAY

PLANS PREPARED BY:



ENGINEERS SEAL:

STANDARD CONSTRUCTION DRAWINGS

DM-4.3	7/19/13	MT-95.30	7/19/13
DM-4.4	7/20/12	MT-95.50	7/19/13
		MT-98.10	7/19/13
FB-1-82	5/10/82	MT-98.11	7/19/13
		MT-98.29	7/19/13
GSD-1-96	7/19/02	MT-101.60	7/19/13
		MT-105.10	7/19/13
RB-1-55	7/19/13		
		TC-41.20	10/18/13
		TC-42.20	10/18/13
		TC-52.10	10/18/13
		TC-52.20	1/17/14
		TC-72.20	7/20/12

SUPPLEMENTAL SPECIFICATIONS

800	4/18/14
821	4/20/12
832	1/17/14
849	1/18/13

SPECIAL PROVISIONS

INDEX OF SHEETS:

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FEDERAL PROJECT NO.  
NON-FEDERAL

PID NO.  
97639

CONSTRUCTION PROJECT NO.

RAILROAD INVOLVEMENT  
NONE

RIC-30-16.42



**REFERENCES SHALL BE MADE TO STANDARD BRIDGE DRAWINGS:**

FB-1-82	DATED	5/10/82
GSD-1-96	DATED	7/19/02
RB-1-55	DATED	7/19/13

**REFERENCES SHALL BE MADE TO SUPPLEMENTAL SPECIFICATION:**

849	DATED	1/18/13
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**EXISTING STRUCTURE VERIFICATION:**

DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURES HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURES AND FROM FIELD OBSERVATION AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURES AND THE PROPOSED WORK BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO CMS SECTIONS 102.05, 105.02 AND 513.04.

BASE CONTRACT BID PRICES UPON A RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PRE-BID EXAMINATION OF THE EXISTING STRUCTURE. HOWEVER, THE DEPARTMENT WILL PAY FOR ALL PROJECT WORK BASED UPON ACTUAL DETAILS AND DIMENSIONS THAT HAVE BEEN VERIFIED IN THE FIELD.

**ORIGINAL CONSTRUCTION PLANS:**

THE ORIGINAL CONSTRUCTION PLANS OF THE EXISTING BRIDGE ARE AVAILABLE UPON REQUEST AT THE DISTRICT 3 OFFICE OF THE OHIO DEPARTMENT OF TRANSPORTATION, ASHLAND, OHIO.

PLAN NAME	DATE
RIC-30-16.37	1976

**DESIGN SPECIFICATIONS:**

THIS STRUCTURE CONFORMS TO "STANDARD SPECIFICATIONS FOR HIGHWAY BRIDGES" ADOPTED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS, 2002 INCLUDING THE 2003, 2004, 2005 AND 2006 SPECIFICATIONS AND THE ODOT BRIDGE DESIGN MANUAL.

**DESIGN LOADING:**

HS20-44

**DESIGN DATA:**

STRUCTURAL STEEL - ASTM A36 - YIELD STRENGTH 36,000 PSI  
CVN IS NOT REQUIRED FOR CROSS FRAME MATERIALS

**STEEL RESTRAINT OR PRELOAD LIMITS:**

EXISTING ASTM A36. DO NOT SUBJECT ANY PART OF THE STRUCTURE TO A JACKING, PULLING OR RESTRAINING UNIT STRESS EXCEEDING 18,000 PSI (50% OF YIELD).

**WORK LIMITS:**

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

**EXISTING PAINT SYSTEM:**

THE EXISTING BRIDGE WAS PAINTED IN 1994 WITH A THREE COAT (OZEU) PAINT SYSTEM. THE FINISH COAT WAS BLUE MEETING FEDERAL COLOR FS-595A-15450.

**UTILITY LINES:**

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER OR ADJACENT TO THE WORK AREA.

**ITEM 513 - STRUCTURAL STEEL MEMBERS, LEVEL UP, AS PER PLAN:**

ALL REQUIREMENTS OF 513 APPLY TO SHOP FABRICATED MEMBERS. PERFORM WORK FOR FIELD FABRICATED MEMBERS ACCORDING TO ITEM 513, EXCEPT AS MODIFIED HEREIN. THE DEPARTMENT WILL NOT REQUIRE THE CONTRACTOR PERFORMING FIELD FABRICATION TO BE PRE-QUALIFIED AS SPECIFIED IN SUPPLEMENT 1078. SUBMIT A WRITTEN LETTER OF MATERIAL ACCEPTANCE, 501.06, TO THE ENGINEER. PROVIDE SHOP DRAWINGS ACCORDING TO 513.06 OR SUPPLY THE ENGINEER WITH "AS-BUILT" DRAWINGS MEETING 513.06 AFTER COMPLETION OF FIELD FABRICATION. THE ENGINEER WILL REVIEW THE SUBMITTED DRAWINGS FOR CONCURRENCE WITH THE FINAL AS-BUILT CONDITION. IF NECESSARY, THE ENGINEER MAY CONTACT THE OFFICE OF STRUCTURAL ENGINEERING FOR TECHNICAL ASSISTANCE. IF THE ENGINEER IS SATISFIED WITH THE "AS-BUILT" DRAWINGS AND THE DELIVERED MATERIALS, SUPPLY A COPY OF THE DRAWINGS, STAMPED AND DATED, TO THE STRUCTURAL, WELDING AND METALS SECTION OF THE OFFICE OF MATERIAL MANAGEMENT FOR RECORD PURPOSES.

COST TO REMOVE EXISTING CROSS FRAME MEMBERS AND ALL NECESSARY GRINDING SHALL BE INCLUDED IN THIS ITEM.

THE FOLLOWING MEMBERS ARE INCLUDED IN THIS ITEM: 3 X 3 X 5/16" ANGLE.

**ITEM 516 - JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN:**

THIS WORK CONSISTS OF RAISING OR REPOSITIONING EXISTING STRUCTURE TO THE DIMENSIONS AND REQUIREMENTS DEFINED IN THE PROJECT PLANS.

SUBMIT WORKING DRAWINGS AND CALCULATIONS IN ACCORDANCE WITH CMS 501.05.

IF, DURING THE JACKING OPERATIONS, CRACKING OF THE CONCRETE SUPERSTRUCTURE, OR OTHER DAMAGE TO THE STRUCTURE IS VISUALLY OBSERVED, IMMEDIATELY CEASE THE JACKING OPERATION AND INSTALL SUPPORTS TO THE SATISFACTION OF THE ENGINEER. ANALYZE THE DAMAGE AND SUBMIT A METHOD OF CORRECTION TO THE ENGINEER FOR APPROVAL. THE DEPARTMENT WILL NOT PAY FOR THE COST OF THE REQUIRED REPAIRS. THE BRIDGE BEARINGS SHALL BE FULLY SEATED AT ALL CONTACT AREAS. IF FULL SEATING IS NOT ATTAINED, SUBMIT A REPAIR PLAN TO THE ENGINEER. THE DEPARTMENT WILL NOT PAY FOR THE REPAIR COSTS TO ENSURE FULL SEATING ON BEARINGS.

THE DEPARTMENT WILL MEASURE THIS WORK ON A LUMP SUM BASIS.

THE DEPARTMENT WILL PAY FOR THE ACCEPTED QUANTITIES AT THE CONTRACT PRICE FOR ITEM 516, JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN.

**ITEM 516 - RESET BEARING:**

THIS ITEM SHALL INCLUDE ALL WORK NECESSARY TO PROPERLY ALIGN BRIDGE BEARINGS, INCLUDING THE REMOVAL AND REPLACEMENT OF THE BEARING END CAP. THE BEARINGS SHALL BE VERTICALLY ALIGNED AT 60°F [15°C]. ASSURE ALL BEARINGS ARE SHIMMED ADEQUATELY AND THAT NO BEAMS AND/OR BEARING DEVICES ARE "FLOATING". ALL WORK SHALL BE TO THE SATISFACTION OF THE ENGINEER.

PAYMENT FOR ALL OF THE ABOVE DESCRIBED LABOR AND MATERIALS WILL BE MADE AT THE CONTRACT PRICE BID FOR ITEM 516 - RESET BEARING. PAINTING OF BEARING IS INCIDENTAL TO ITEM 514 - FIELD PAINTING OF DAMAGED STRUCTURAL STEEL, AS PER PLAN (THREE COAT).

DESIGN FILE: \\projects\97639\structures\notes.dgn  
WORKSTATION: fofoster  
MODELNAME: Design  
DATE: 5/12/2014

DESIGN AGENCY  
ODOT DISTRICT THREE  
OFFICE OF  
PLANNING AND ENGINEERING

DATE	05/14
REVIEWED	RDN
DRAWN	NRF
DESIGNED	NRF
CHECKED	DJV

GENERAL NOTES

RIC-30-16.42

**ITEM 849 - DAMAGE ASSESSMENT:**

**ITEM 849 - SURFACE PREPARATION:**

**ITEM 849 - REPAIRING DAMAGED MEMBERS BY GRINDING:**

**ITEM 849 - STRAIGHTENING DAMAGED MEMBERS:**

THESE ITEMS SHALL BE IN ACCORDANCE WITH THE SUPPLEMENTAL SPECIFICATION FOR HEAT STRAIGHTENING OF DAMAGED STRUCTURAL STEEL.

PAYMENT FOR ALL OF THE ABOVE SHALL BE AT THE UNIT PRICE BID FOR EACH OF THE ABOVE ITEMS, WHICH SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK.

**ITEM 514 - FIELD PAINTING OF DAMAGED STRUCTURAL STEEL, AS PER PLAN (THREE COAT):**

**1.0 DESCRIPTION**

THIS ITEM CONSISTS OF CLEANING AND FIELD PAINTING STRUCTURAL STEEL AND NEW REPLACEMENT STEEL. THIS WORK CONSIST OF PERFORMING SURFACE PREPARATION AND APPLYING A THREE-COAT PAINT SYSTEM TO THE PREPARED STEEL AND FEATHERED REMOVAL AREAS OF EXISTING PAINT.

**2.0 GENERAL**

C&MS 514.05 THROUGH 514.10 AND 514.13.D APPLY UNLESS MODIFIED BY THESE NOTES.

**3.0 WASHING EXISTING OZEU PAINTED SURFACES**

CLEAN SURFACES TO BE COATED WITH LOW PRESSURE WATER CLEANING TO REMOVE ALL DIRT, DEBRIS, ANIMAL EXCREMENT, SALT CONTAMINANTS AND OTHER ACCUMULATED FOREIGN MATERIAL IN ACCORDANCE WITH SSPC-SPI2 (LP WC), LOW PRESSURE WATER CLEANING. THE PRESSURE WASHER SHALL BE CAPABLE OF ACHIEVING AT LEAST 2000 POUNDS PER SQUARE INCH AT THE NOZZLE. WHEN USING THE POWER WASHING EQUIPMENT, THE NOZZLE SHALL BE MAINTAINED NO MORE THAN 10 INCHES FROM THE SURFACE. SUPPLY AND USE POTABLE WATER. PROVIDE TO THE ENGINEER A LETTER OF WRITTEN ACCIPTANCE FOR ANY BIODEGRADABLE DETERGENTS OR CLEANERS USED IN CONJUNCTION WITH THIS METHOD.

COLLECT AND CONTAIN WATER AND DEBRIS REMOVED DURING WASHING OPERATIONS ABOVE WATER FEATURES IN CONFORMANCE WITH C&MS 514.08 AND C&MS 514.13.D FOR ANY DEBRIS. CREATE SETTLEMENT COLLECTION BASINS AND STRAIN ALL WASH WATER ABOVE LAND FEATURES AS NECESSARY TO PRODUCE VISIBLY CLEAR WATER AND COMPLY WITH C&MS 514.08 AND C&MS 514.13.D FOR ANY DEBRIS.

**4.0 SURFACE PREPARATION**

AFTER THE PRESSURE WASHED SURFACE HAS DRIED, REMOVE EXISTING PAINT COATING TO CONTRACT LIMITS OR AS DIRECTED BY THE ENGINEER ACCORDING TO: SSPC-SP 10, AS SHOWN ON THE PICTORIAL SURFACE PREPARATION STANDARDS FOR PAINTING STEEL SURFACES SHOWN IN SSPC-VIS 1. THE ENGINEER WILL USE THE SSPC-VIS 1 TO DETERMINE THE ACCEPTANCE OF THE SURFACE PREPARATION. FEATHER THE EXISTING PAINT TO EXPOSE A MINIMUM OF 1/2 INCH OF EACH COAT. CONTAIN AND DISPOSE DISPOSE OF WASTE GENERATED BY THE CLEANING ACCORDING TO C&MS 514.13.D.

ROUND ALL EXPOSED CORNERS OF MAIN MATERIAL TO BE PAINTED AS NECESSARY TO ACHIEVE A 1/16 INCH RADIUS OR EQUIVALENT FLAT SURFACE AT A 45 DEGREE ANGLE.

**5.0 FIELD PAINTING**

APPLY THE PRIME, INTERMEDIATE AND FINISH COATS OF THE THREE-COAT PAINT SYSTEM SPECIFIED IN C&MS 708.02, ACCORDING TO C&MS 514.15, 514.16, 514.17, 514.19 AND 514.20 TO CONTRACT LIMITS OR AS DIRECTED BY THE ENGINEER. TINT THE FINISH COAT TO MATCH THE EXISTING BLUE COLOR MEETING FEDERAL COLOR FS-595A-15450 AND TO THE ENGINEERS SATISFACTION. THE ENGINEER WILL DETERMINE THE PRIME AND INTERMEDIATE COAT THICKNESS USING A TYPE 2 MAGNETIC GAGE AT SPOT LOCATIONS. THE PRIME, INTERMEDIATE AND FINISH COAT OF PAINT SHALL MEET THE MINIMUM DRY FILM THICKNESS REQUIREMENTS OF C&MS 514.20. APPLY PAINT AS FOLLOWS:

A. APPLY THE PRIME COAT ONLY TO THE PREPARED SURFACE OF THE BARE STEEL AND THE EXISTING PRIME COAT EXPOSED BY FEATHERING. DO NOT APPLY THE PRIME COAT TO THE ADJACENT INTERMEDIATE COAT.

B. APPLY CAULK AFTER PRIMING

C. APPLY THE INTERMEDIATE COAT TO THE NEW PRIME COAT AND TO THE EXISTING INTERMEDIATE COATS THAT ARE EXPOSED BY FEATHERING.

D. APPLY THE FINISH COAT TO THE NEW INTERMEDIATE COAT AND TO THE EXISTING FINISH COATS THAT ARE EXPOSED BY FEATHERING.

AT THE PERIMETER OF THE REPAIR AREA, APPLY THE PRIME, INTERMEDIATE AND FINISH COATS WITH A BRUSH. IN LIEU OF BRUSHING THE CONTRACTOR MAY DOUBLE MASK AREAS NOT TO BE COATED AND SPRAY TO FEATHERED REMOVAL LINES.

BLEND REPAIR AREAS WITH THE ADJACENT COATING TO PROVIDE A FINISHED SURFACE IN THE PATCHED AREAS THAT IS SMOOTH AND HAS AN EVEN PROFILE WITH THE ADJACENT SURFACE.

**6.0 MEASUREMENT**

THE DEPARTMENT WILL MEASURE FIELD PAINTING OF DAMAGED STRUCTURAL STEEL, AS PER PLAN (THREE COAT) BY THE NUMBER OF SQUARE FEET OF STRUCTURAL STEEL PAINTED.

THE DEPARTMENT WILL DETERMINE THE SURFACE AREA BY TAKING EXACT FIELD MEASUREMENTS OF ALL PAINTED SURFACES AND CALCULATIONS.

**7.0 BASIS OF PAYMENT**

THE DEPARTMENT WILL PAY FOR ACCEPTED QUANTITIES AT THE CONTRACT PRICES AS FOLLOWS:  
 THE DEPARTMENT MAY CONSIDER PAINT AS ELIGIBLE FOR PAYMENT FOR MATERIAL ON-HAND AS SPECIFIED IN 109.10, HOWEVER, ONLY PAINT THAT THE CONTRACTOR CAN PROVE TO THE ENGINEER WILL BE USED DURING THE CONSTRUCTION SEASON IS ELIGIBLE FOR PAYMENT. THE CONTRACTOR SHALL PROVIDE THE ENGINEER CALCULATIONS INDICATING THE TOTAL SQUARE FEET OF STEEL TO BE PAINTED DURING THE CONSTRUCTION SEASON. THE CONTRACTOR SHALL ALSO PROVIDE CALCULATIONS SHOWING THE TOTAL NUMBER OF GALLONS REQUIRED.

IF THE CONTRACTOR CAUSES DAMAGE OR INJURY TO PUBLIC OR PRIVATE PROPERTY, THE DEPARTMENT WILL NOT PAY FOR RESTORING THE PROPERTY TO ITS ORIGINAL CONDITION.

THE DEPARTMENT WILL NOT PAY FOR REPAIRING ADJACENT COATINGS DAMAGED DURING THE WASHING, POWER TOOL CLEANING OR BLAST CLEANING OPERATION.

THE DEPARTMENT WILL NOT PAY FOR REMOVING AND REPLACING AN AREA OF COATING BECAUSE A SPOT OR MAXIMUM AVERAGE THICKNESS EXCEEDS THE MAXIMUM SPOT THICKNESS.

THE DEPARTMENT WILL NOT PAY FOR ADDITIONAL TESTING REQUIRED BY ANY HAULER, TREATMENT FACILITY, DISPOSAL FACILITY OR LANDFILL.

THE DEPARTMENT WILL NOT PAY FOR ACCESSING, INSPECTING, AND REPAIRING AREAS THAT ARE NOT FOUND TO BE IN CONFORMANCE WITH THE SPECIFICATIONS AND PERTINENT CONTRACT DOCUMENTS.

ALL OTHER REQUIREMENTS OF THIS FIELD PAINTING SPECIFICATION ARE CONSIDERED INCIDENTAL TO THE WORK.

ITEM	UNIT	DESCRIPTION
514	SQUARE FEET	FIELD PAINTING OF DAMAGED STRUCTURAL STEEL, AS PER PLAN (THREE COAT)

**ITEM 614 - MAINTAINING TRAFFIC**

**DETOUR LIMITATION:**

ALL LANES OF TRAFFIC ON U.S. 30 SHALL BE MAINTAINED AT ALL TIMES EXCEPT THAT THROUGH TRAFFIC IN THE WEST BOUND LANES ON U.S. 30 SHALL HAVE LANE CLOSURES AS PER SHEETS 5 AND 6 AND AS PER STANDARD CONSTRUCTION DRAWINGS MT-95.30 AND MT-98.29. THE WEST BOUND EXIT AND ENTRANCE RAMP FROM REED ROAD MAY BE CLOSED FOR UP TO 30 CONSECUTIVE DAYS DURING PHASE II OF THE MAINTENANCE OF TRAFFIC. SEE SHEETS 5 AND 6 FOR DETAILS.

THE CONTRACTOR SHALL ALSO NOTIFY, IN WRITING, THE FOLLOWING AGENCIES AT LEAST TEN (10) DAYS PRIOR TO THE TIME WHEN THE US 30 RAMP DETOUR WILL BE IMPLEMENTED:

- RICHLAND COUNTY ENGINEER
- TOWNSHIP TRUSTEES (TWP. ROADS ONLY)
- LOCAL POLICE, FIRE, AND AMBULANCE DEPARTMENT(S)
- LOCAL SCHOOL DISTRICT(S)
- RICHLAND COUNTY SHERIFF

THE CONTRACTOR SHALL BE RESPONSIBLE FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING THE GATES AND BARRICADES AT THE APPROXIMATE WORK LIMITS OF THE PROJECT, AND THE ADVANCE WARNING SIGNS AS SHOWN ON STANDARD CONSTRUCTION DRAWING MT-101.60.

**INTERIM COMPLETION DATE:**

THE THIRTY (30) CONSECUTIVE CALENDAR DAYS SHALL BE CONSIDERED AN INTERIM COMPLETION DATE, AND FOR EACH CALENDAR DAY BEYOND THE THIRTY (30) CONSECUTIVE CALENDAR DAYS THAT THE ROADWAY REMAINS CLOSED TO TRAFFIC, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF \$1000 PER DAY.

ACCESS TO ADJACENT PROPERTIES SHALL BE MAINTAINED AT ALL TIMES, AS PER SECTION 614.02 (A).

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATION, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

**PROJECT DETOUR LIMITATIONS**

THE ROADWAY SHALL NOT BE CLOSED TO TRAFFIC FOR THE REPAIR OF THE EXISTING STRUCTURE UNTIL ALL MATERIAL AND EQUIPMENT NECESSARY TO PLACE THE ROADWAY BACK INTO SERVICE HAVE BEEN TESTED, APPROVED AND ARE READY FOR DELIVERY TO AND/OR USE ON THE PROJECT SITE.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED LIQUIDATED DAMAGES IN ACCORDANCE WITH CMS 108.07.

**DETOUR SIGNING**

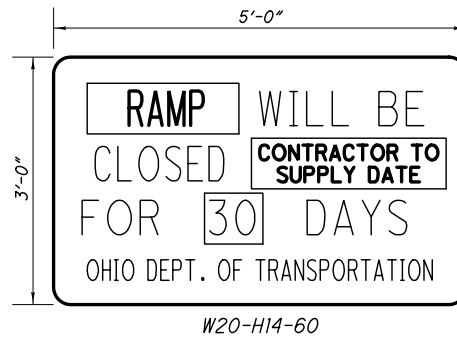
THE FOLLOWING QUANTITY IS INCLUDED FOR THE CONTRACTOR TO PROVIDE THE DETOUR SIGNING AS SHOWN AS PER 614.06 (B):

ITEM 614, DETOUR SIGNING - LUMP

THE CONTRACTOR SHALL NOTIFY THE ODOT DISTRICT THREE ROADWAY SERVICES MANAGER, IN WRITING, A MINIMUM OF FOURTEEN (14) DAYS IN ADVANCE OF THE DATE THE DETOUR IS NEEDED. THE CONTRACTOR WILL INSTALL, MAINTAIN AND SUBSEQUENTLY REMOVE THE DETOUR SIGNING.

**NOTICE OF CLOSURE SIGNS**

THESE SIGNS SHALL BE ERECTED BY THE CONTRACTOR AT LEAST ONE WEEK IN ADVANCE OF THE SCHEDULED RAMP CLOSURES. THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. THEY SHOULD BE ERECTED AT THE POINT OF CLOSURE. PAYMENT FOR THIS WORK SHALL BE INCLUDED IN THE LUMP SUM BID FOR ITEM 614, MAINTAINING TRAFFIC, AND IT SHALL INCLUDE FURNISHING, ERECTING, MAINTAINING AND REMOVING THE SIGNS AND SUPPORTS.



**MAINTENANCE OF LOCAL DETOUR ROUTE**

A LOCAL DETOUR ROUTE, OTHER THAN THE OFFICIAL SIGNED ODOT DETOUR ROUTE, MAY BE SELECTED BY AGREEMENT BETWEEN ODOT AND LOCAL GOVERNMENTAL AGENCIES PRIOR TO THE HIGHWAY CLOSURE.

DURING THE TIME THAT TRAFFIC IS DETOURED, THE CONTRACTOR SHALL MAINTAIN THIS ROUTE IN A CONDITION WHICH IS REASONABLY SMOOTH AND FREE FROM HOLES, RUTS, RIDGES, BUMPS, DUST AND STANDING WATER. ONCE THE DETOUR IS REMOVED AND TRAFFIC RETURNED TO ITS NORMAL PATTERN, THE DESIGNATED LOCAL DETOUR ROUTE SHALL BE RESTORED TO A CONDITION THAT IS EQUIVALENT TO THAT WHICH EXISTED PRIOR TO ITS USE FOR THIS PURPOSE. ALL SUCH WORK SHALL BE PERFORMED WHEN AND AS DIRECTED BY THE ENGINEER. THE DESIGNATED LOCAL DETOUR ROUTE IS TO BE REVIEWED AND REPAIRED PRIOR TO THE ASPHALT CONTRACTOR OR SUBCONTRACTOR LEAVING THE PROJECT.

PAYMENT FOR THE WORK NECESSARY TO REPAIR THESE LOCAL ROADS WILL BE PERFORMED BY CHANGE ORDER.

**ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS**

IN ADDITION TO THE REQUIREMENTS OF CMS 614 AND THE LATEST EDITION OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD), A UNIFORMED LAW ENFORCEMENT OFFICER (AND OFFICIAL PATROL CAR WITH MOUNTED EMERGENCY FLASHING LIGHTS) SHALL BE PROVIDED FOR CONTROLLING TRAFFIC FOR THE FOLLOWING TASKS AS DIRECTED BY THE ENGINEER:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED.

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

LAW ENFORCEMENT OFFICERS (LEO'S) SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED. THE LEO'S ARE CONSIDERED TO BE EMPLOYED BY THE CONTRACTOR AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THEIR ACTIONS. ALTHOUGH THEY ARE EMPLOYED BY THE CONTRACTOR, THE PROJECT ENGINEER SHALL HAVE CONTROL OVER THEIR PLACEMENT. THE OFFICIAL PATROL CAR SHALL BE A PUBLIC SAFETY VEHICLE AS REQUIRED BY THE OHIO REVISED CODE. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEO'S SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR THESE SERVICES AND PROVIDE 72 HOURS ADVANCE NOTICE AS REQUIRED BY THE HIGHWAY PATROL LISTED BELOW:

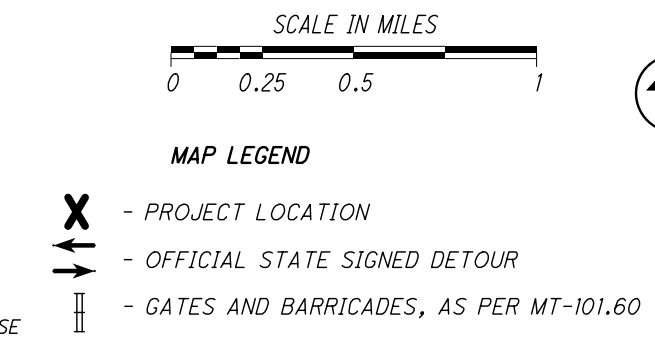
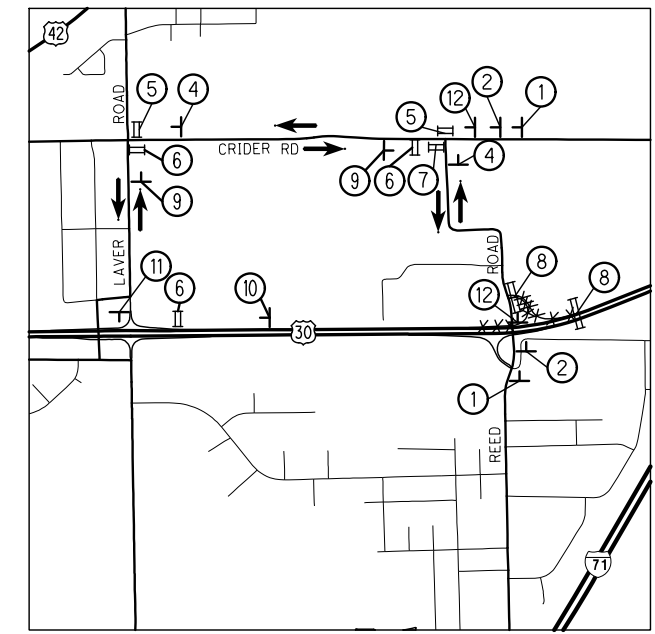
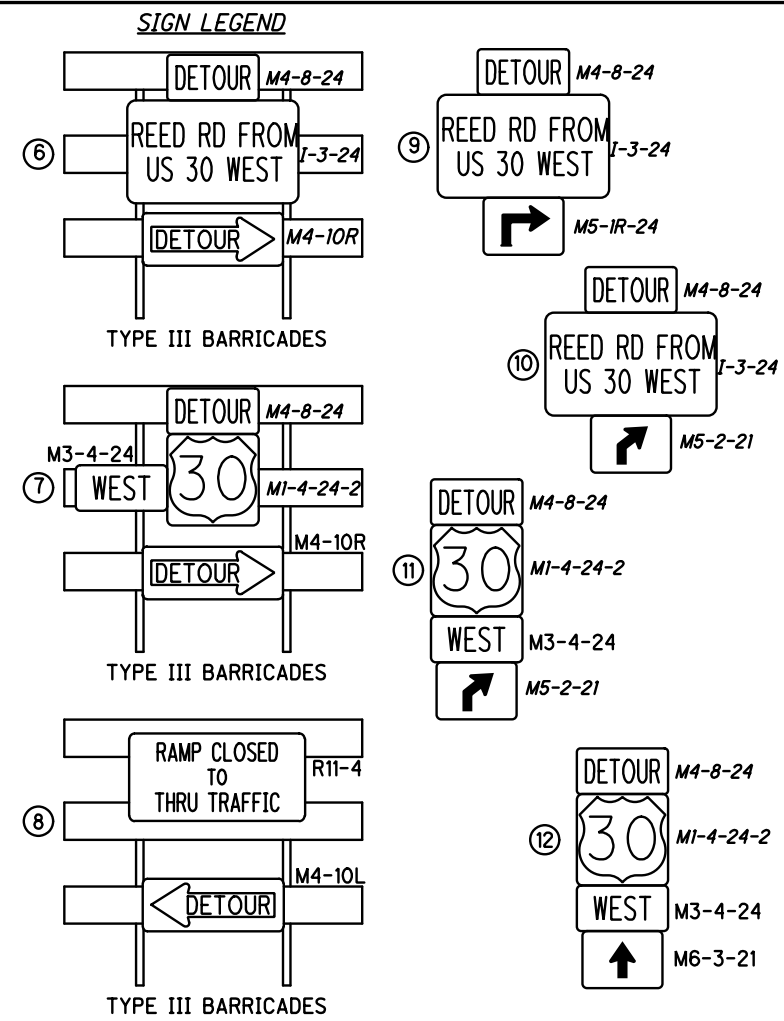
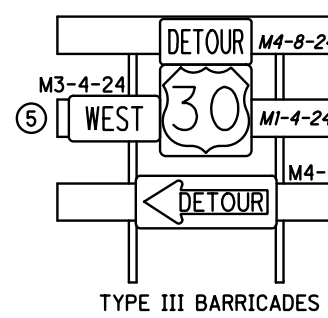
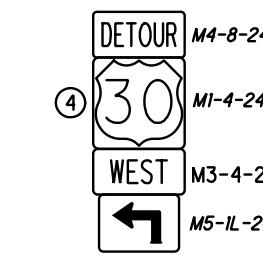
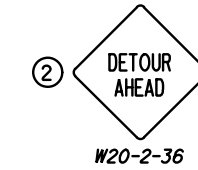
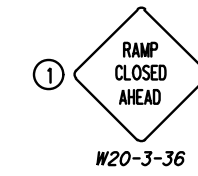
STATE HIGHWAY PATROL, RICHLAND COUNTY  
2221 SOUTH MAIN STREET  
MANSFIELD, OH 44907  
419.756.2222

LAW ENFORCEMENT OFFICERS WITH PATROL CAR REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

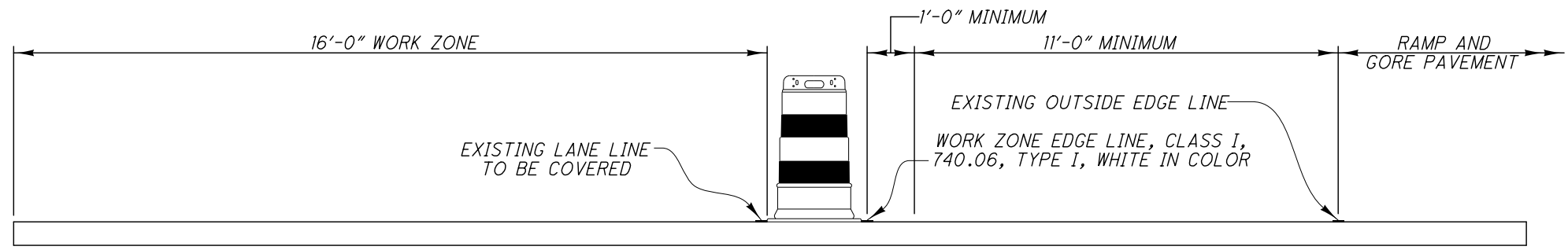
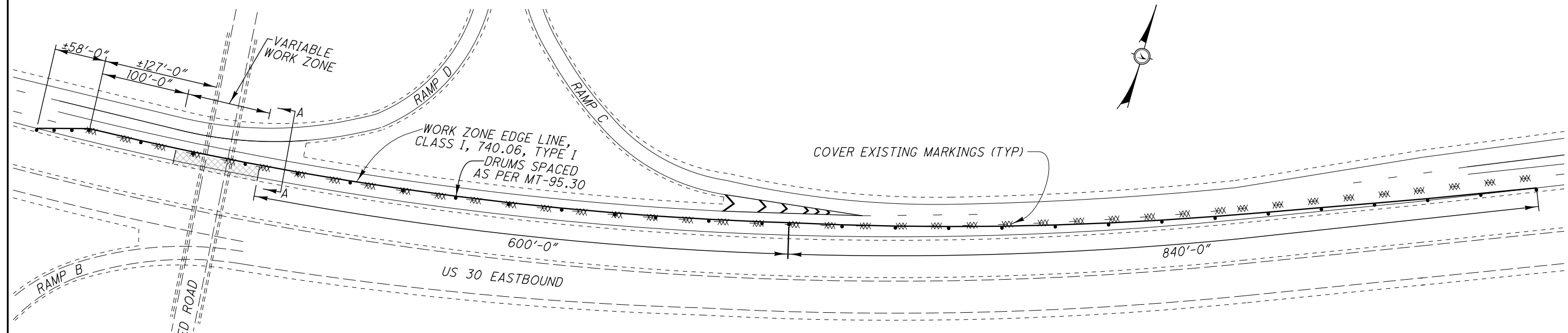
ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 40 HOURS

THE HOURS PAID SHALL INCLUDE MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

IF THE CONTRACTOR WISHES TO UTILIZE LEO'S FOR FLAGGING AND TRAFFIC CONTROL OTHER THAN FOR THAT REQUIRED IN THESE PLANS, THEY MAY DO SO AT THEIR OWN EXPENSE.



DESIGN FILE: \\projects\97639\roadway\sheet\97639MD001.dgn  
WORKSTATION: fofoster DATE: 5/12/2014 MODELNAME: Design



TYPICAL SECTION A-A

- NOTES:
- ALL TEMPORARY WORK ZONE MARKINGS SHALL BE MADE USING ITEM 614 WORK ZONE EDGE LINE, CLASS I, 740.06, TYPE I, WHITE IN COLOR.
  - ALL EXISTING CONFLICTING PAVEMENT MARKINGS AS DETAILED IN THE PLANS SHALL BE COVERED USING ITEM 614 WORK ZONE EDGE LINE, CLASS I, 740.06, TYPE I OR ITEM 614 WORK ZONE LANE LINE, CLASS I, 740.06, TYPE I, BLACK IN COLOR.
  - THE WORK ZONE SHALL BE INSPECTED DAILY OR AFTER EVERY MAJOR CHANGE IN LAYOUT FOR CONSPICUITY OF THE WORK ZONE PAVEMENT MARKINGS. SHOULD ANY MARKING (INCLUDING THE COVERING MARKINGS) BE DEEMED DEFICIENT BY THE ENGINEER, IT SHOULD BE REPLACED OR REPAIRED IMMEDIATELY.
  - ANY DETAILS, MARKINGS, SIGNAGE AND DRUM SPACING NOT SHOWN ON THESE PLANS SHALL CONFORM TO MT-95.30.
  - PHASE II SHALL BE PERMITTED TO BE CLOSED FOR ONE (1) PERIOD ONLY WITH A MAXIMUM DURATION OF THIRTY (30) DAYS. FOR EVERY DAY OVER THIRTY (30) DAYS THAT PHASE II REMAINS IN PLACE, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE FEE OF \$1000.
  - PHASE I MAY BE ERECTED FOR A SECOND PERIOD AFTER THE COMPLETION OF ALL WORK ASSOCIATED WITH PHASE II.

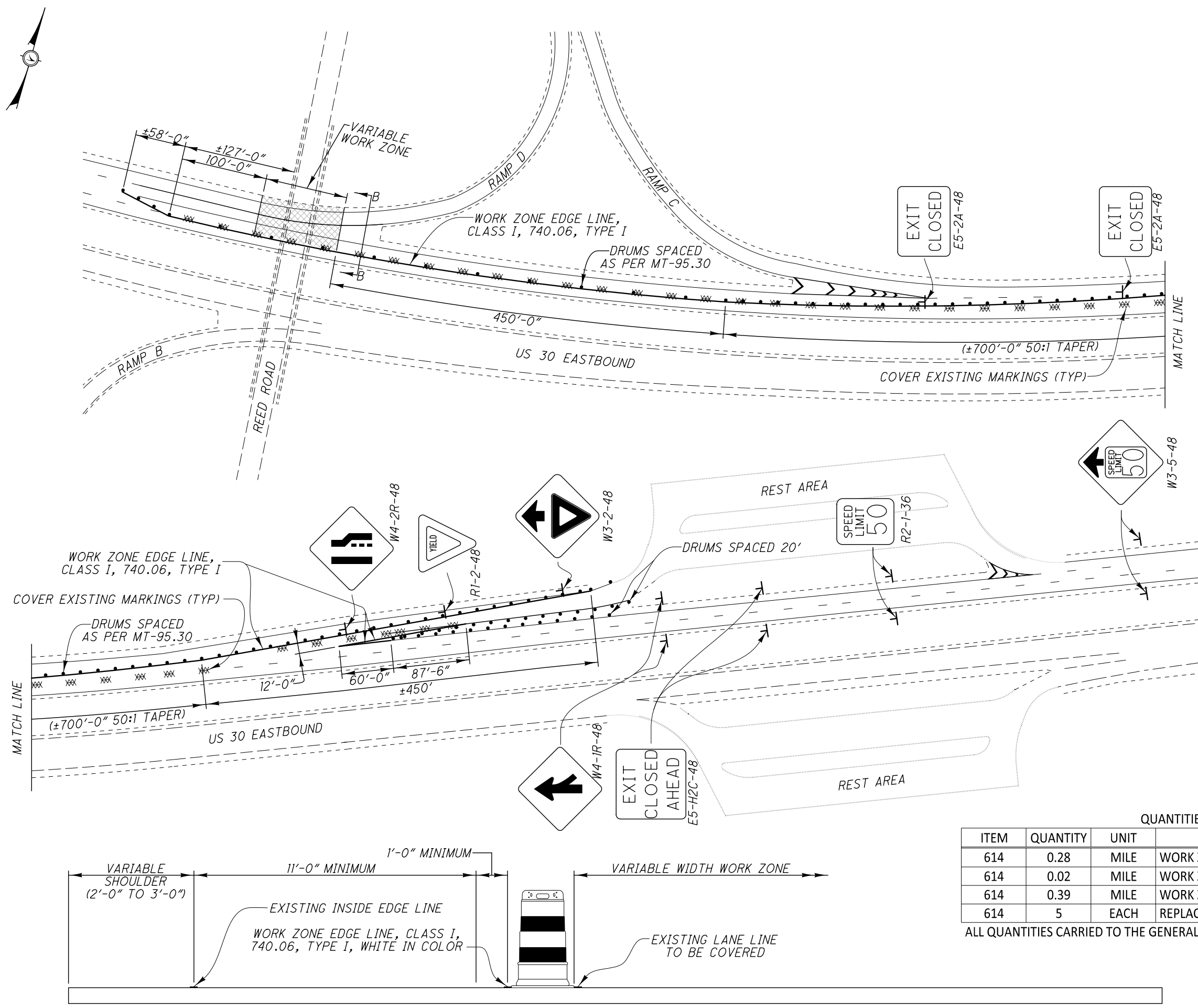
QUANTITIES FOR PHASE ONE\*\*

ITEM	QUANTITY	UNIT	DESCRIPTION
614	0.32	MILE	WORK ZONE LANE LINE, CLASS I, 740.06, TYPE I (BLACK)
614	0.31	MILE	WORK ZONE EDGE LINE, CLASS I, 740.06, TYPE I (WHITE)
614	5	EACH	REPLACEMENT DRUM

\*\* QUANTITIES FOR ONE IMPLEMENTATION ONLY. QUANTITIES SHOULD BE MULTIPLIED FOR MULTIPLE IMPLEMENTATIONS.  
ALL QUANTITIES CARRIED TO THE GENERAL SUMMARY

DESIGN FILE: \\projects\97639\roadway\sheets\97639MD001.dgn  
WORKSTATION: foster  
DATE: 5/12/2014  
MODELNAME: Design

DESIGN FILE: \\projects\97639\roadway\sheet\97639MD001.dgn  
 WORKSTATION: foster  
 MODELNAME: Design  
 DATE: 5/12/2014



- NOTES:**
- ALL TEMPORARY WORK ZONE MARKINGS SHALL BE MADE USING ITEM 614 WORK ZONE EDGE LINE, CLASS I, 740.06, TYPE I, WHITE IN COLOR.
  - ALL EXISTING CONFLICTING PAVEMENT MARKINGS AS DETAILED IN THE PLANS SHALL BE COVERED USING ITEM 614 WORK ZONE EDGE LINE, CLASS I, 740.06, TYPE I OR ITEM 614 WORK ZONE LANE LINE, CLASS I, 740.06, TYPE I, BLACK IN COLOR.
  - THE WORK ZONE SHALL BE INSPECTED DAILY OR AFTER EVERY MAJOR CHANGE IN LAYOUT FOR CONSPICUITY OF THE WORK ZONE PAVEMENT MARKINGS. SHOULD ANY MARKING (INCLUDING THE COVERING MARKINGS) BE DEEMED DEFICIENT BY THE ENGINEER, IT SHOULD BE REPLACED OR REPAIRED IMMEDIATELY.
  - ANY DETAILS, MARKINGS, SIGNAGE AND DRUM SPACING NOT SHOWN ON THESE PLANS SHALL CONFORM TO MT-95.30.
  - PHASE II SHALL BE PERMITTED TO BE CLOSED FOR ONE (1) PERIOD ONLY WITH A MAXIMUM DURATION OF THIRTY (30) DAYS. FOR EVERY DAY OVER THIRTY (30) DAYS THAT PHASE II REMAINS IN PLACE, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE FEE OF \$1000.
  - PHASE I MAY BE ERECTED FOR A SECOND PERIOD AFTER THE COMPLETION OF ALL WORK ASSOCIATED WITH PHASE II.

**QUANTITIES FOR PHASE TWO**

ITEM	QUANTITY	UNIT	DESCRIPTION
614	0.28	MILE	WORK ZONE LANE LINE, CLASS I, 740.06, TYPE I (BLACK)
614	0.02	MILE	WORK ZONE EDGE LINE, CLASS I, 740.06, TYPE I (BLACK)
614	0.39	MILE	WORK ZONE EDGE LINE, CLASS I, 740.06, TYPE I (WHITE)
614	5	EACH	REPLACEMENT DRUM

ALL QUANTITIES CARRIED TO THE GENERAL SUMMARY

TYPICAL SECTION B-B

CALCULATED  
KCK  
CHECKED  
DUJ

**LANE CLOSURE DETAIL - PHASE II**

**RIC-30-16.78**

SHEET NUMBER				PARTICIPATION				ALT (X)	ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
4	5	6	8	01/BRO/BR										
STRUCTURE 20 FOOT SPAN AND OVER (RIC-30-1642)														
			21	21				512	10100	21	SY	SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)		
			178	178				512	10600	178	FT	CONCRETE REPAIR BY EPOXY INJECTION		
			21	21				512	74000	21	SY	REMOVAL OF EXISTING COATINGS FROM CONCRETE SURFACES		
			1115	1115				513	10201	1115	LB	STRUCTURAL STEEL MEMBERS, LEVEL UP, AS PER PLAN	2	
			656	656				514	20001	656	SF	FIELD PAINTING OF DAMAGED STRUCTURAL STEEL, AS PER PLAN (THREE COAT)	3	
			1	1				516	46700	1	EACH	RESET BEARING		
			LS	LS				516	47001	LS		JACKING AND TEMPORARY SUPPORT OF SUPERSTRUCTURE, AS PER PLAN	2	
			LS	LS				849	10000	LS		DAMAGE ASSESSMENT		
			LS	LS				849	10500	LS		SURFACE PREPARATION		
			8	8				849	10600	8	HOUR	REPAIRING DAMAGED MEMBERS BY GRINDING		
			LS	LS				849	10700	LS		STRAIGHTENING DAMAGED MEMBERS		
MAINTENANCE OF TRAFFIC														
40				40				614	11110	40	HOUR	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE		
LS				LS				614	12420	LS		DETOUR SIGNING		
	5	5		10				614	12600	10	EACH	REPLACEMENT DRUM		
	0.64	0.28		0.92				614	20200	0.92	MILE	WORK ZONE LANE LINE, CLASS I, 740.06, TYPE I (BLACK)		
		0.02		0.02				614	22200	0.02	MILE	WORK ZONE EDGE LINE, CLASS I, 740.06, TYPE I (BLACK)		
	0.62	0.39		1.01				614	22200	1.01	MILE	WORK ZONE EDGE LINE, CLASS I, 740.06, TYPE I (WHITE)		
				LS				614	11000	LS		MAINTAINING TRAFFIC		
				LS				624	10000	LS		MOBILIZATION		

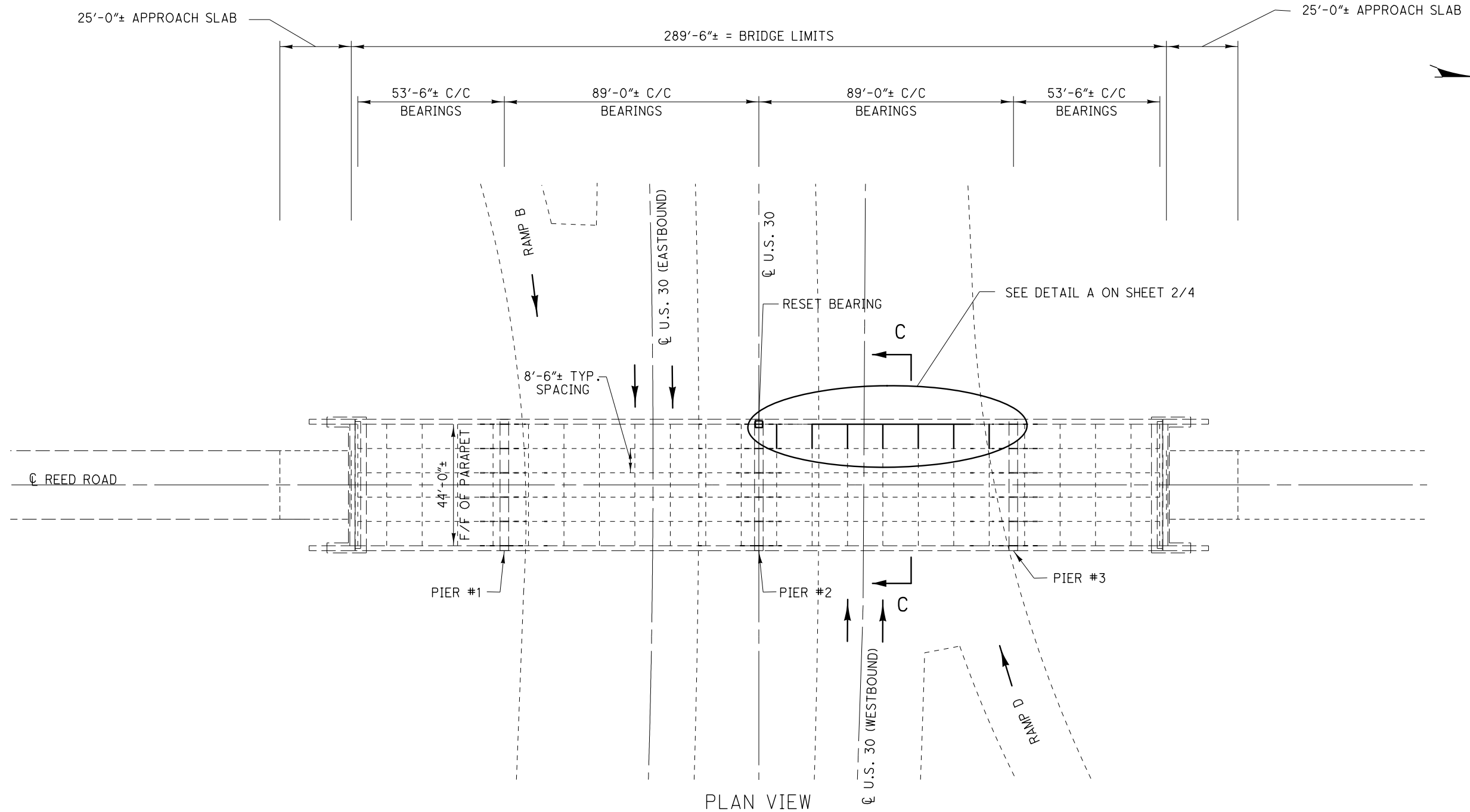
GENERAL SUMMARY

RIC - 30 - 16 . 42

7  
11

CALCULATED	NRF
	CHECKED
	DJV

DESIGN FILE: \\projects\97639\structures\RIC-30-16.4.2.dgn  
 WORKSTATION: foster DATE: 5/12/2014 MODELNAME: Design



PLAN VIEW

ITEM	QUANTITY	UNIT	DESCRIPTION
512	21	SQ YD	SEALING OF CONCRETE SURFACES (EPOXY-URETHANE)
512	178	FT	CONCRETE RREPAIR BY EPOXY INJECTION
512	21	SQ YD	REMOVAL OF EXISTING COATINGS FROM CONCRETE SURFACES
513	1,115	LB	STRUCTURAL STEEL MEMBERS, LEVEL UF, AS PER PLAN
514	656	SQ FT	FIELD PAINTING OF DAMAGED STRUCTURAL STEEL, AS PER PLAN (THREE COAT)
516	1	EACH	RESET BEARING
516	LUMP		JACKING AND TEMPORARY SUPPORT OF SUPPERSTRUCTURE, AS PER PLAN
849	LUMP		DAMAGE ASSESSMENT
849	LUMP		SURFACE PREPARATION
849	8	HOUR	REPAIRING DAMAGED MEMBERS BY GRINDING
849	LUMP		STRAIGHTENING DAMAGED MEMBERS

ALL QUANTITIES CARRIED TO GENERAL SUMMARY

PROPOSED WORK:

- REMOVE EXISTING CROSS FRAME ASSEMBLIES THAT ARE TO BE REPLACED. COST INCLUDED IN ITEM 513.
- HEAT STRAIGHTEN BEAM PER ITEM 849.
- INSTALL NEW CROSS FRAME ASSEMBLIES PER ITEM 513, SEE SHEET 4/4 FOR DETAILS.
- JACK BRIDGE TO RESET BEARING AS DETAILED IN THE PLANS.
- PAINT ALL REPAIR AREAS, TO INCLUDE THE RESET BEARING, AND NEW ANGLES PER ITEM 514-FIELD PAINTING OF DAMAGED STRUCTURAL STEEL, AS PER PLAN (THREE COAT).
- EPOXY INJECT AREA BETWEEN BEAM AND DECK USING ITEM 512. ANY EXPOSED REINFORCEMENT ON THE BOTTOM OF THE DECK SHALL BE BRUSHED WITH GALVANIZING AS PER ASTM A 780 PRIOR TO THE EPOXY APPLICATION. ALL COSTS INCLUDED IN ITEM 512.
- SEAL AREA OF DECK EDGE AS SHOWN ON SHEET 3/4 USING ITEM 512.

NOTES:

FOR SECTION VIEW C-C, SEE SHEET 4/4.

DESIGN AGENCY  
 ODOT DISTRICT THREE  
 OFFICE OF  
 PLANNING AND ENGINEERING

DATE  
 05/14

REVIEWED  
 RDN

DRAWN  
 NRF

DESIGNED  
 NRF

CHECKED  
 DUJ

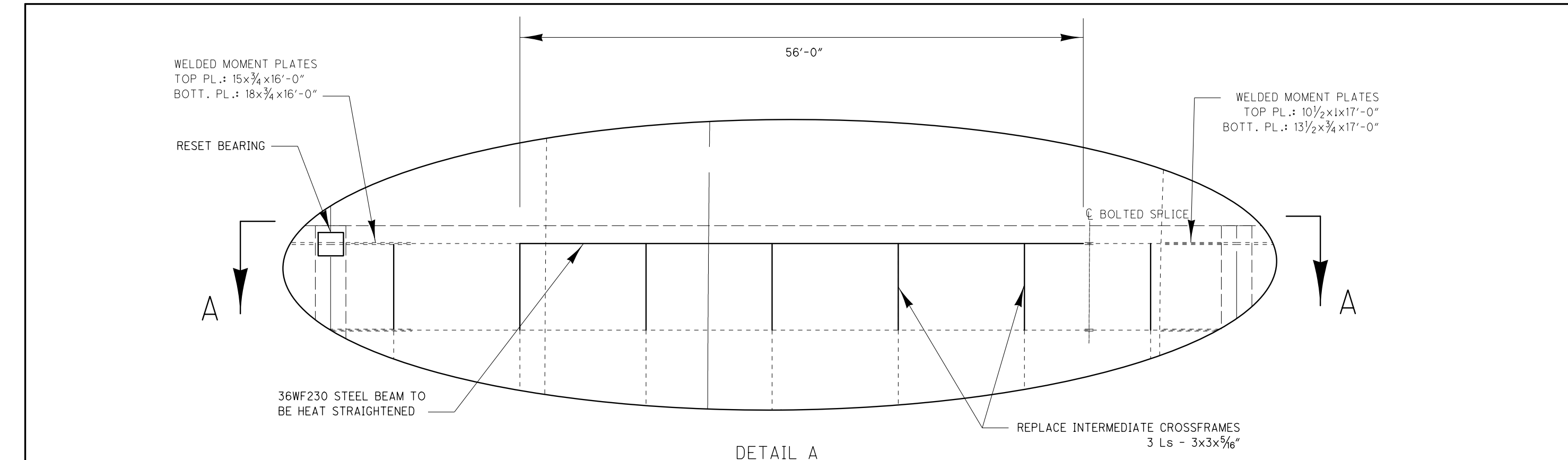
STRUCTURE FILE NUMBER  
 7001517

PLAN VIEW  
 U.S. 30 UNDER REED ROAD.

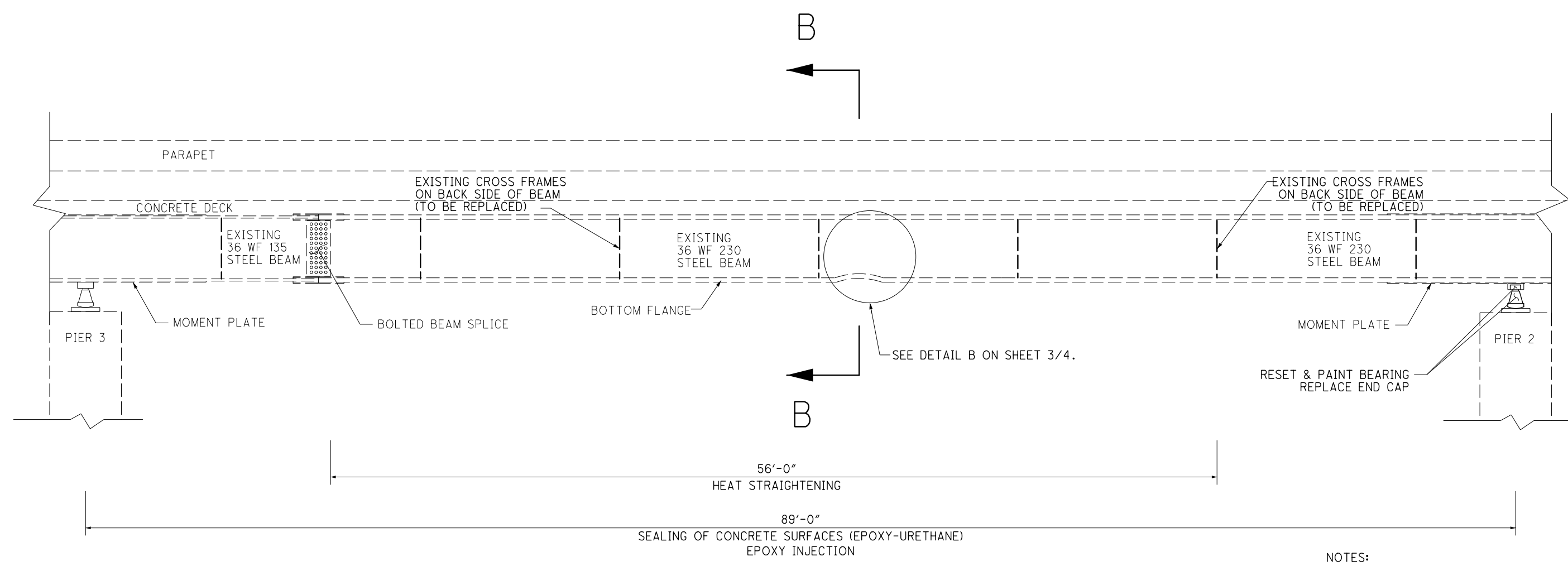
RIC-30-16.42



DESIGN FILE: \\projects\97639\structures\RIC-30-16.42.dgn  
 WORKSTATION: foster DATE: 5/12/2014



DETAIL A

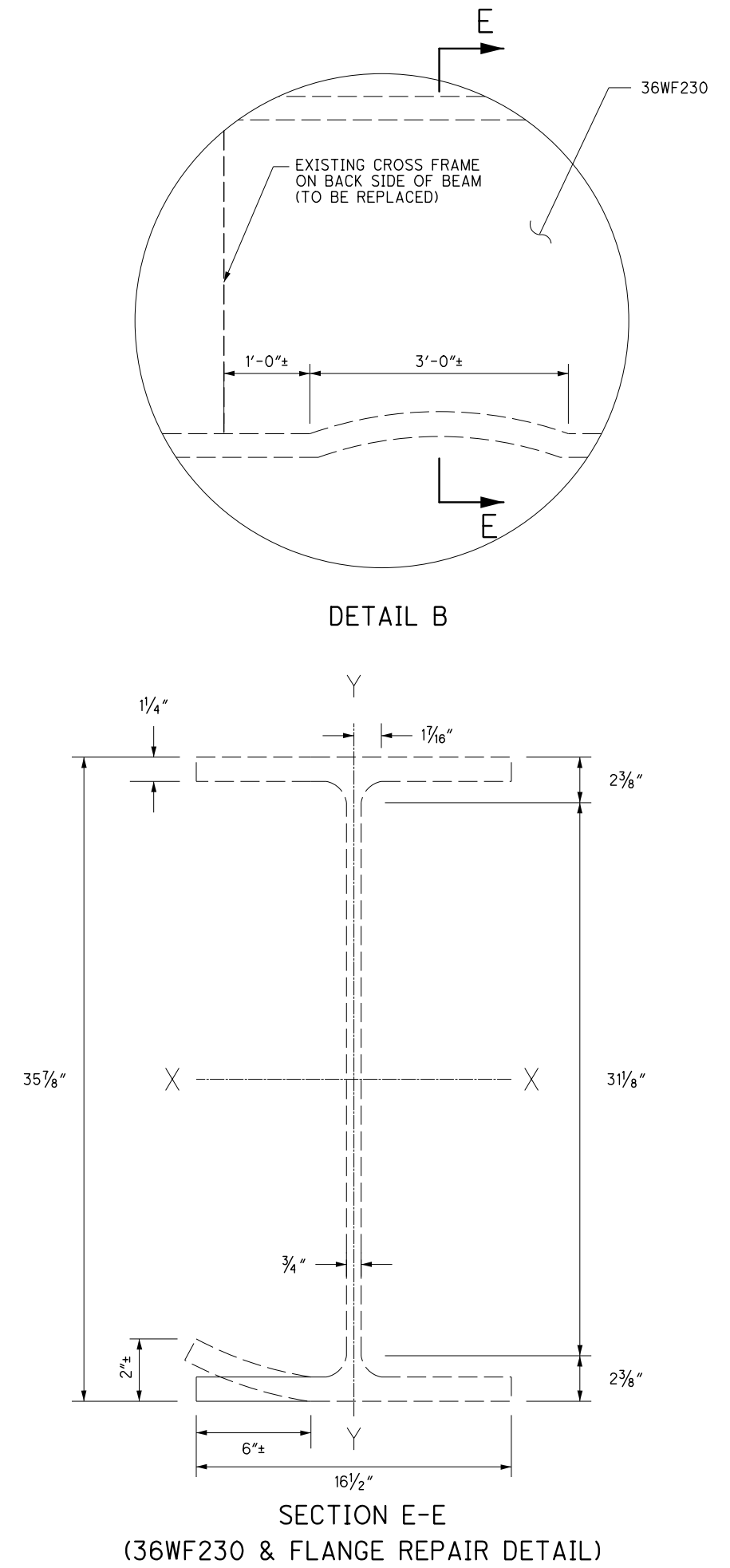
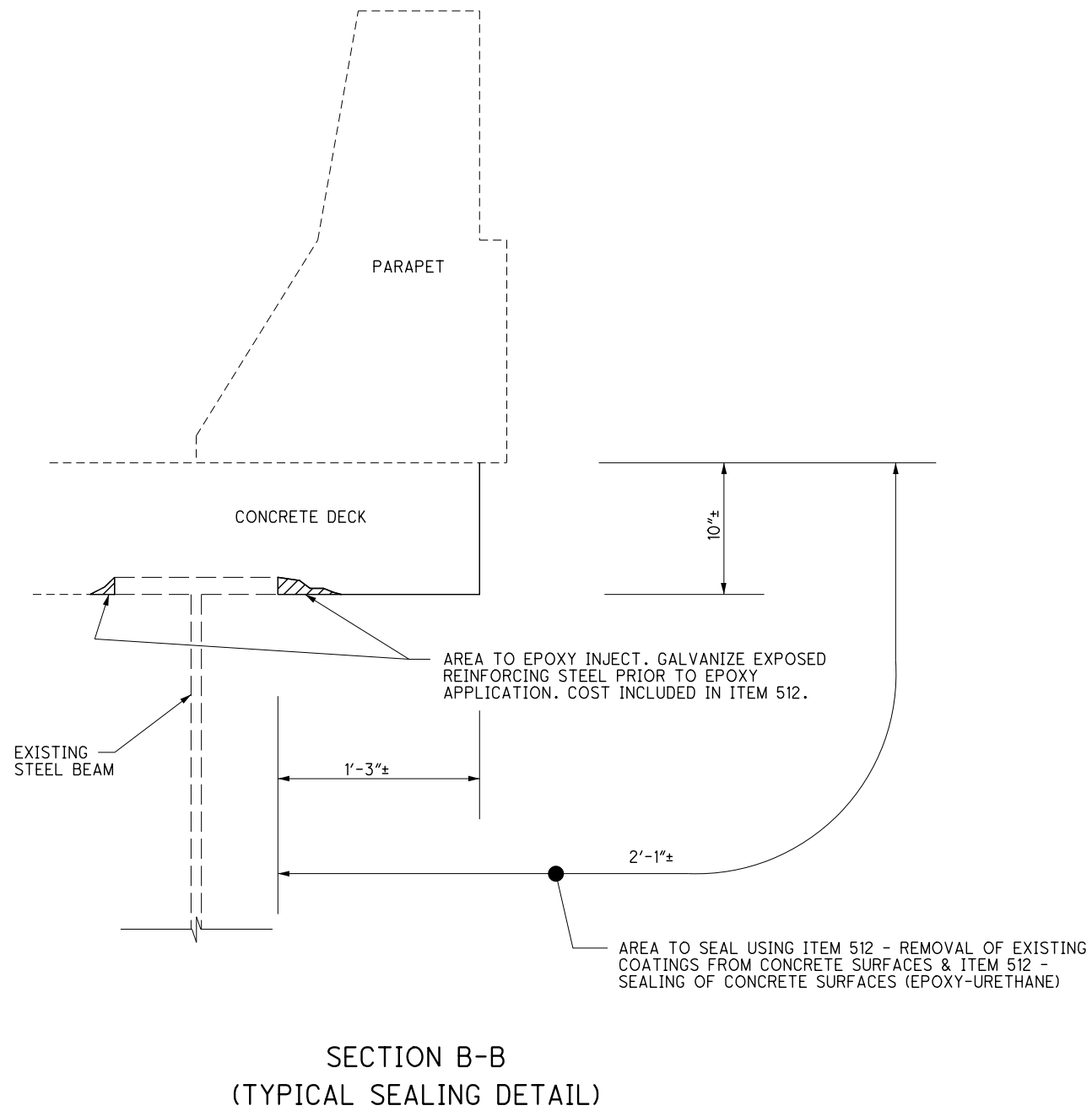


SECTION A-A

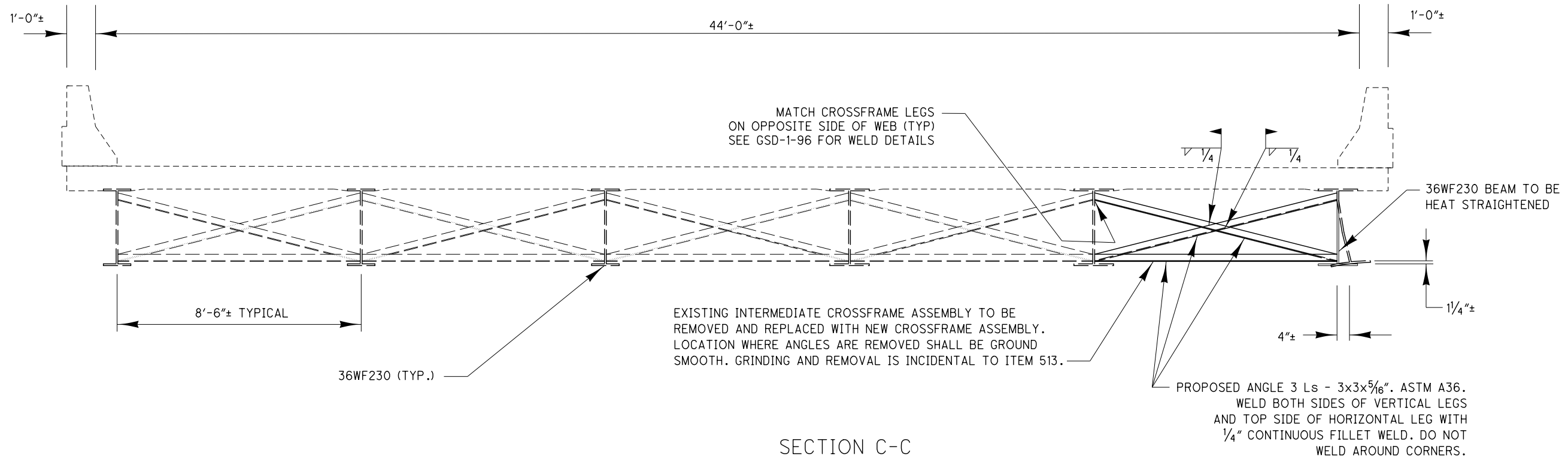
- NOTES:  
 1) REPLACE ALL SEVEN (7) CROSS FRAMES ILLUSTRATED IN SECTION VIEW A-A.  
 1) FOR SECTION VIEW B-B, SEE SHEET 3/4.

DESIGN AGENCY <b>ODOT DISTRICT THREE</b> OFFICE OF <b>PLANNING AND ENGINEERING</b>	
DATE 05/14	STRUCTURE FILE NUMBER 7001517
REVIEWED RDN	DRAWN NRF
DESIGNED NRF	CHECKED DUJ
DETAIL A AND SECTION A-A U.S. 30 UNDER REED ROAD.	
<b>RIC-30-16.42</b>	
2 / 4	
9 11	

DESIGN FILE: \\projects\97639\structures\RIC-30-16.42.dgn  
 WORKSTATION: foster DATE: 5/12/2014 MODELNAME: Design



DESIGNED		DRAWN		REVIEWED		DATE		DESIGN AGENCY	
NRF		NRF		RON		05/14		ODOT DISTRICT THREE	
CHECKED		REVISED		STRUCTURE FILE NUMBER		7001517		OFFICE OF PLANNING AND ENGINEERING	
DJV									
DETAIL B, SECTION B-B AND E-E					U.S. 30 UNDER REED ROAD.				
RIC-30-16.42					3 / 4				
10					11				



SECTION C-C

NOTES:

- 1) FOR WELD DETAILS NOT SHOWN, SEE GSD-1-96.