

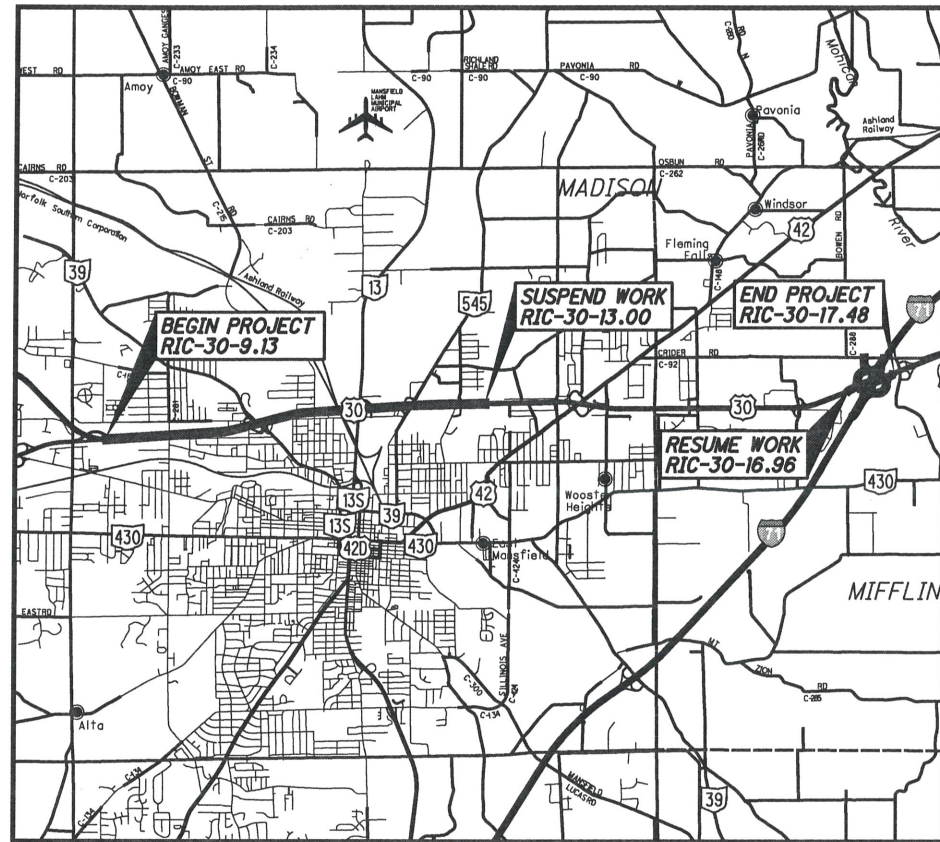
STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

RIC-30-9.13 FY17 RM

CITY OF MANSFIELD

MADISON TOWNSHIP
MIFFLIN TOWNSHIP

RICHLAND COUNTY



LOCATION MAP
SCALE IN MILES



PORTION TO BE IMPROVED -----
 INTERSTATE HIGHWAY -----
 FEDERAL ROUTES -----
 STATE ROUTES -----
 LATITUDE: N 40° 46' 44" LONGITUDE: W 82° 29' 32"

DESIGN INFORMATION

DESIGN FUNCTIONAL CLASSIFICATION: NON-INTERSTATE - FREEWAYS AND EXPRESSWAYS
 DESIGN DESIGNATIONS: SEE SHEET 2
 DESIGN EXCEPTIONS: NONE REQUIRED
 NHS PROJECT: YES

UNDERGROUND UTILITIES
 CONTACT BOTH SERVICES TWO WORKING DAYS BEFORE YOU DIG.

OHIO Utilities Protection SERVICE
 Call Before You Dig
 1-800-362-2764
 (Non-members must be called directly)

OIL & GAS PRODUCERS UNDERGROUND PROTECTION SERVICE
 1-800-925-0988

ENGINEERS SEAL:



SIGNED: *C. A. Devore*
 DATE: 2-27-2017

INDEX OF SHEETS:

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STANDARD CONSTRUCTION DRAWINGS

STANDARD CONSTRUCTION DRAWINGS						SUPPLEMENTAL SPECIFICATIONS	
BP-2.4	7/19/13	MT-98.21	7/18/14	TC-71.10	1/20/17	800-2016	1/20/17
BP-3.1	7/18/14	MT-98.22	1/20/17	TC-72.20	7/15/16	821	4/20/12
		MT-98.28	1/20/17	TC-73.20	7/15/16	832	1/17/14
DM-1.2	1/18/13	MT-98.30	1/20/17				
DM-4.3	1/15/16	MT-99.20	7/19/13				
DM-4.4	1/15/16	MT-101.90	7/17/15				
		MT-104.10	10/16/15				
RM-3.1	7/19/13	MT-105.10	7/19/13				
RM-4.2	4/18/14						
		TC-41.20	10/18/13				
MT-95.30	7/15/16	TC-42.10	10/18/13				
MT-95.40	1/20/17	TC-42.20	10/18/13				
MT-95.50	10/16/15	TC-52.10	10/18/13				
MT-98.10	1/20/17	TC-52.20	7/15/16				
MT-98.11	1/20/17	TC-65.10	1/17/14				
MT-98.20	7/18/14	TC-65.11	7/15/16				

SPECIAL PROVISIONS

PROJECT DESCRIPTION

THIS PROJECT WILL INCLUDE PAVEMENT REPAIRS, PAVEMENT PLANING, RESURFACING WITH ASPHALT CONCRETE, PAVEMENT MARKINGS AND BRIDGE MAINTENANCE.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA: N/A ACRES (MAINTENANCE PROJECT)
 ESTIMATED CONTRACTOR EARTH DISTURBED AREA: N/A ACRES (MAINTENANCE PROJECT)
 NOTICE OF INTENT EARTH DISTURBED AREA: N/A ACRES (MAINTENANCE PROJECT)

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

2016 SPECIFICATIONS

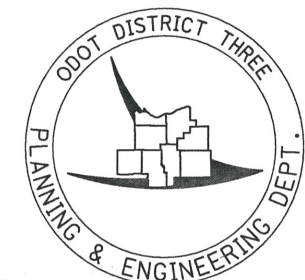
THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY EXCEPT AS NOTED ON SHEET 11, AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

APPROVED: *[Signature]*
 DATE: 2/27/17 DISTRICT DEPUTY DIRECTOR

APPROVED: _____
 DATE: _____ DIRECTOR, DEPARTMENT OF TRANSPORTATION

PLANS PREPARED BY:



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FEDERAL PROJECT NO. 150270

PID NO. 99487

CONSTRUCTION PROJECT NO.

RAILROAD INVOLVEMENT NONE

RIC-30-9.13 RM

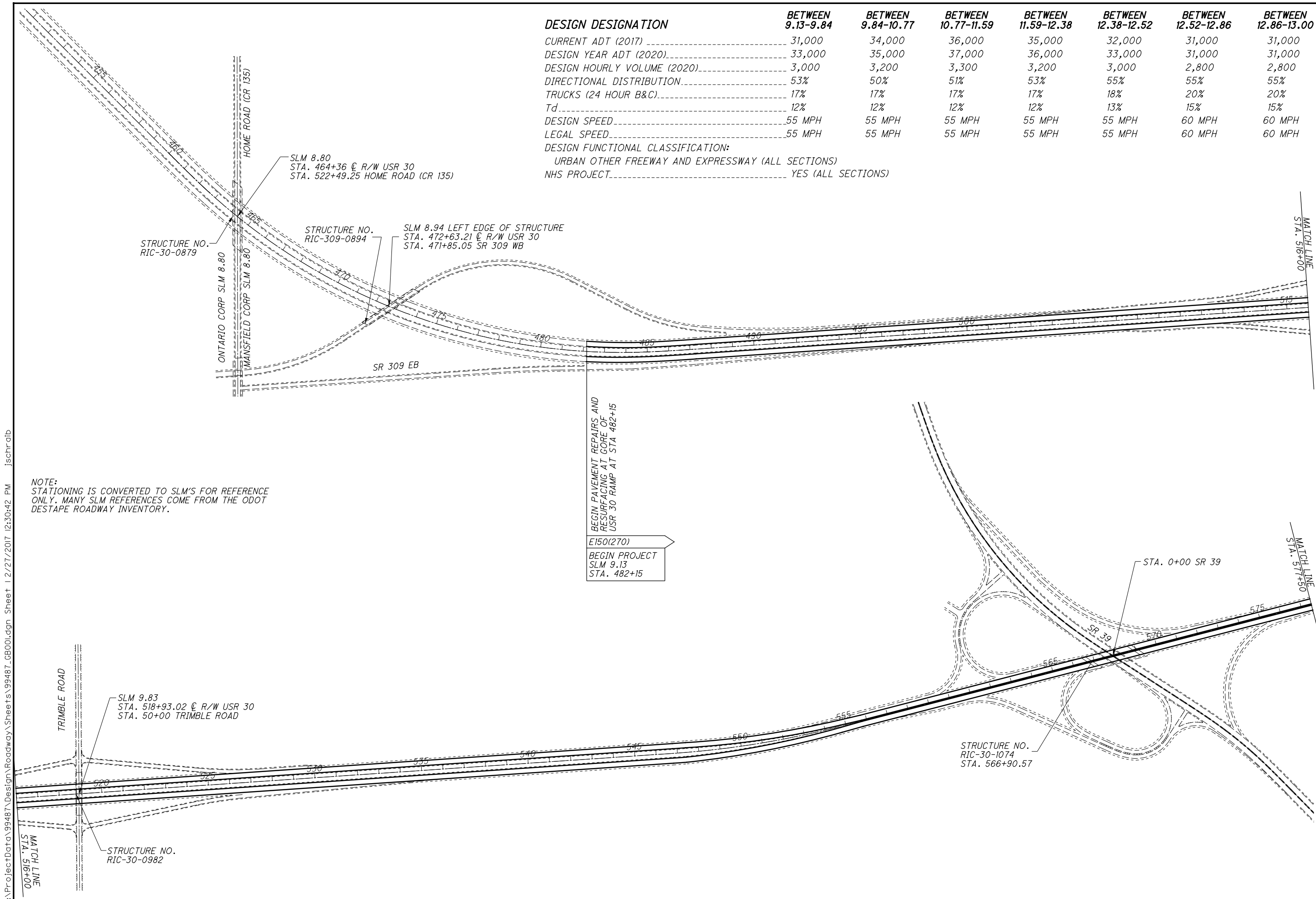
1/16

DESIGN DESIGNATION

	BETWEEN 9.13-9.84	BETWEEN 9.84-10.77	BETWEEN 10.77-11.59	BETWEEN 11.59-12.38	BETWEEN 12.38-12.52	BETWEEN 12.52-12.86	BETWEEN 12.86-13.00
CURRENT ADT (2017)	31,000	34,000	36,000	35,000	32,000	31,000	31,000
DESIGN YEAR ADT (2020)	33,000	35,000	37,000	36,000	33,000	31,000	31,000
DESIGN HOURLY VOLUME (2020)	3,000	3,200	3,300	3,200	3,000	2,800	2,800
DIRECTIONAL DISTRIBUTION	53%	50%	51%	53%	55%	55%	55%
TRUCKS (24 HOUR B&C)	17%	17%	17%	17%	18%	20%	20%
Td	12%	12%	12%	12%	13%	15%	15%
DESIGN SPEED	55 MPH	55 MPH	55 MPH	55 MPH	55 MPH	60 MPH	60 MPH
LEGAL SPEED	55 MPH	55 MPH	55 MPH	55 MPH	55 MPH	60 MPH	60 MPH
DESIGN FUNCTIONAL CLASSIFICATION: URBAN OTHER FREEWAY AND EXPRESSWAY (ALL SECTIONS)							
NHS PROJECT..... YES (ALL SECTIONS)							

CALCULATED JWS
CHECKED JWS

HORIZONTAL SCALE IN FEET



NOTE:
STATIONING IS CONVERTED TO SLM'S FOR REFERENCE ONLY. MANY SLM REFERENCES COME FROM THE ODOT DESTAPE ROADWAY INVENTORY.

BEGIN PAVEMENT REPAIRS AND RESURFACING AT GORE OF USR 30 RAMP AT STA 482+15

E150(270)

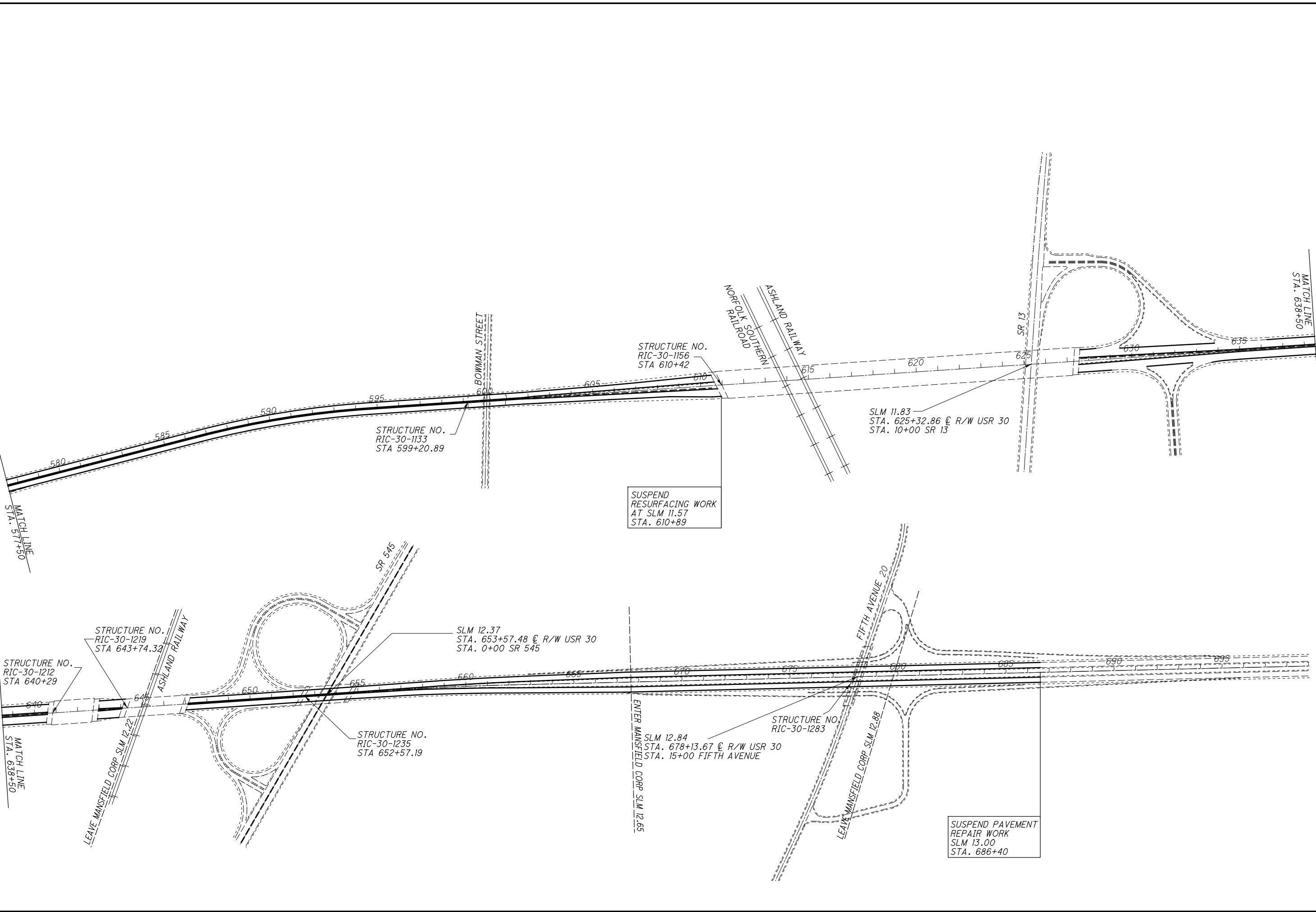
BEGIN PROJECT SLM 9.13 STA. 482+15

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SCHEMATIC / DESIGN DESIGNATION
RIC-30, SLM: 9.13-10.94

RIC-30-9.13 RM

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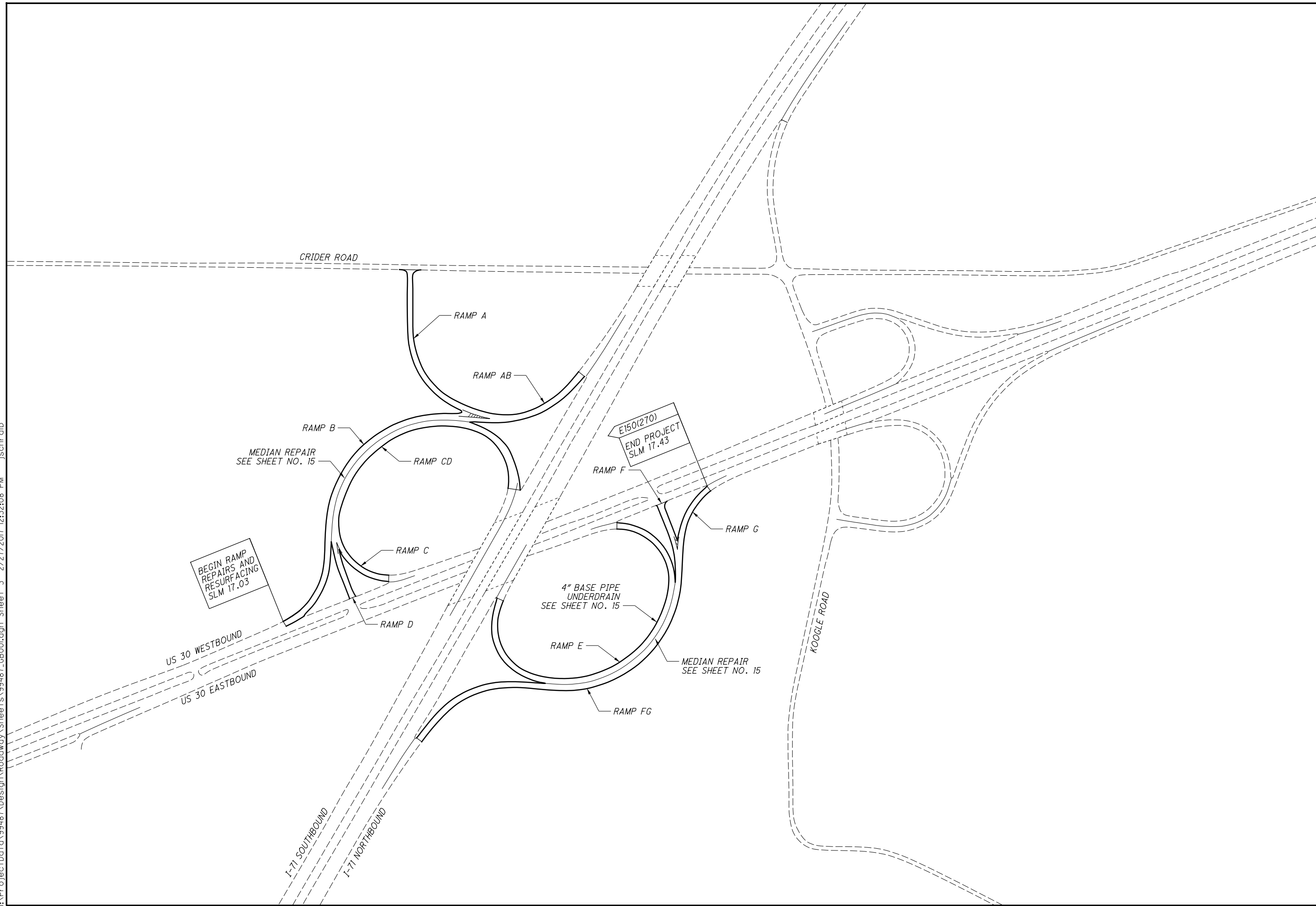


CALCULATED JWS
CHECKED JWS

0 200 400
HORIZONTAL SCALE IN FEET

SCHEMATIC / DESIGN DESIGNATION
RIC-30, SLM: 10.94-13.00

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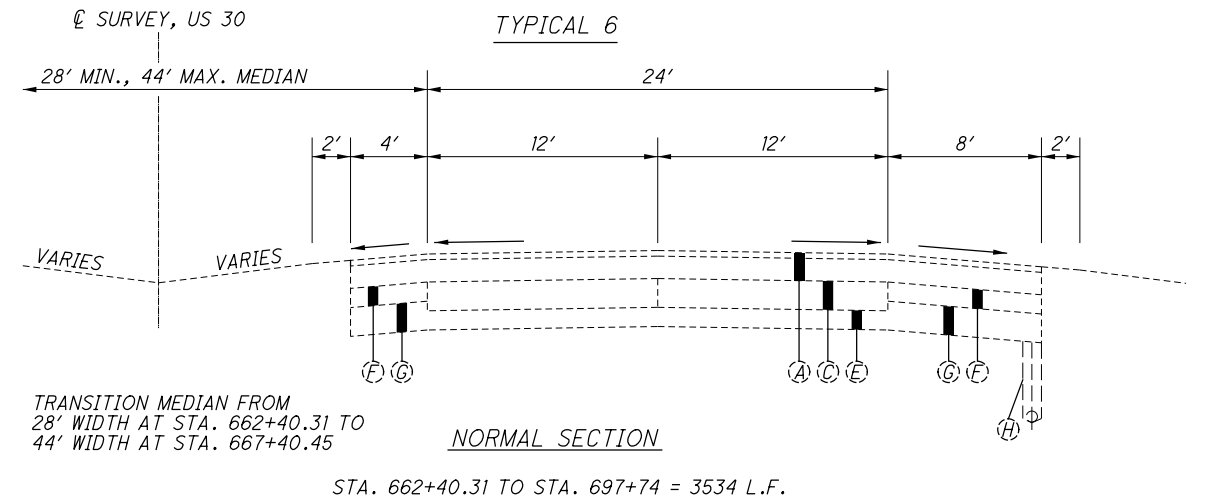
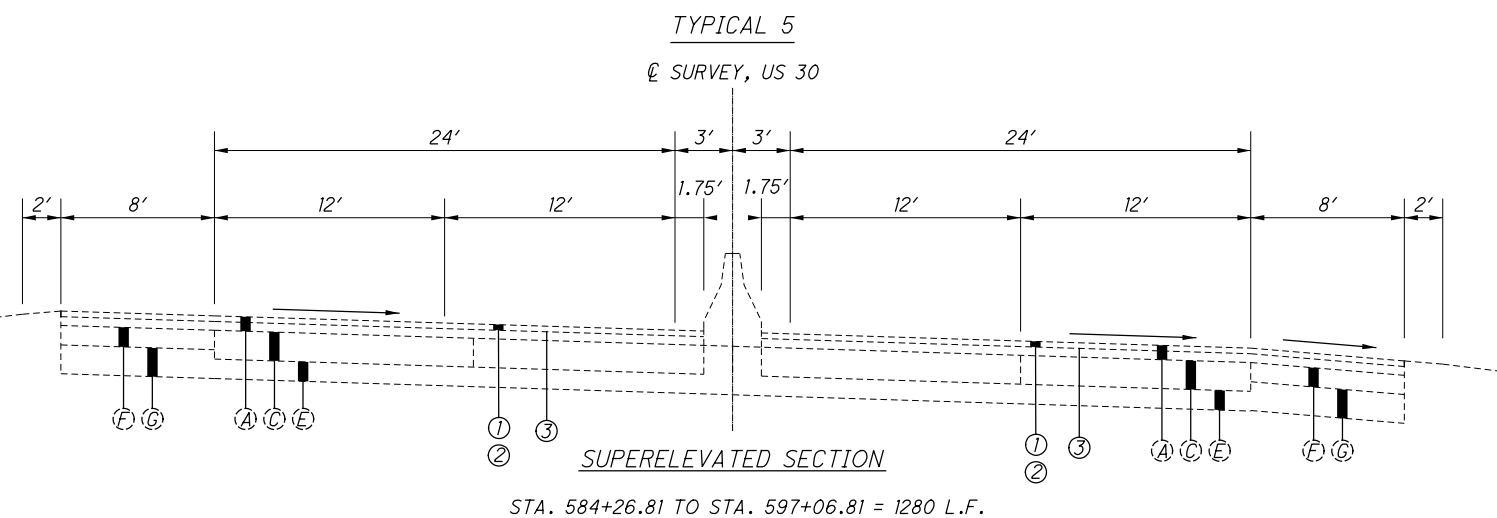
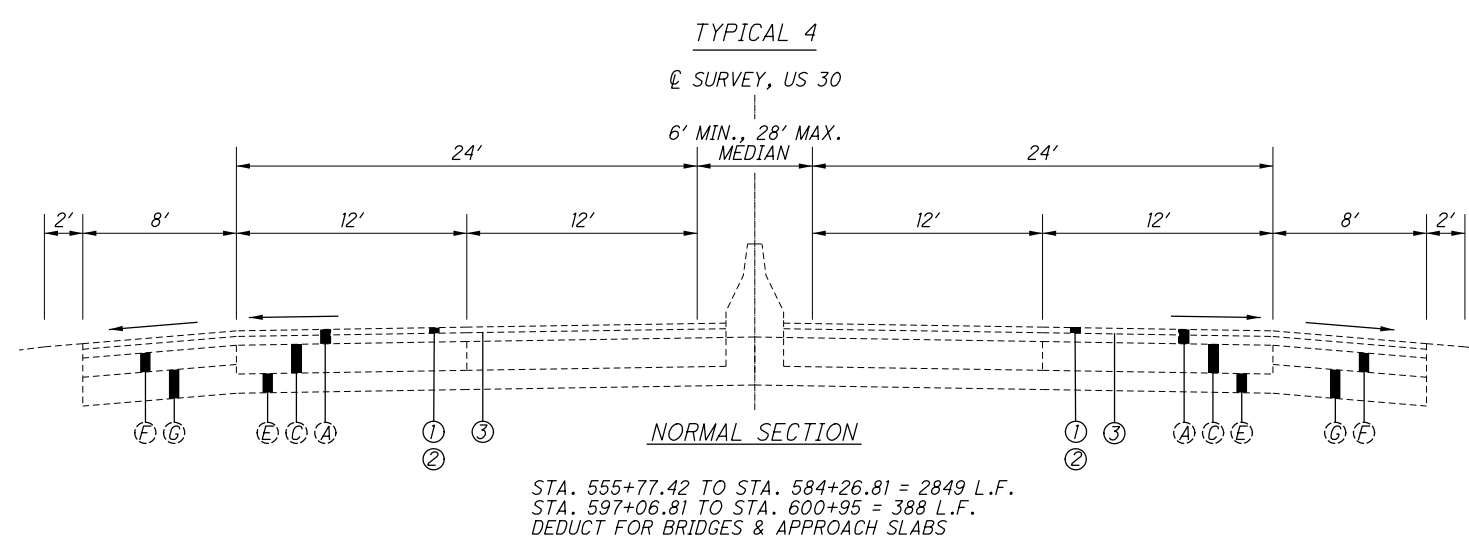
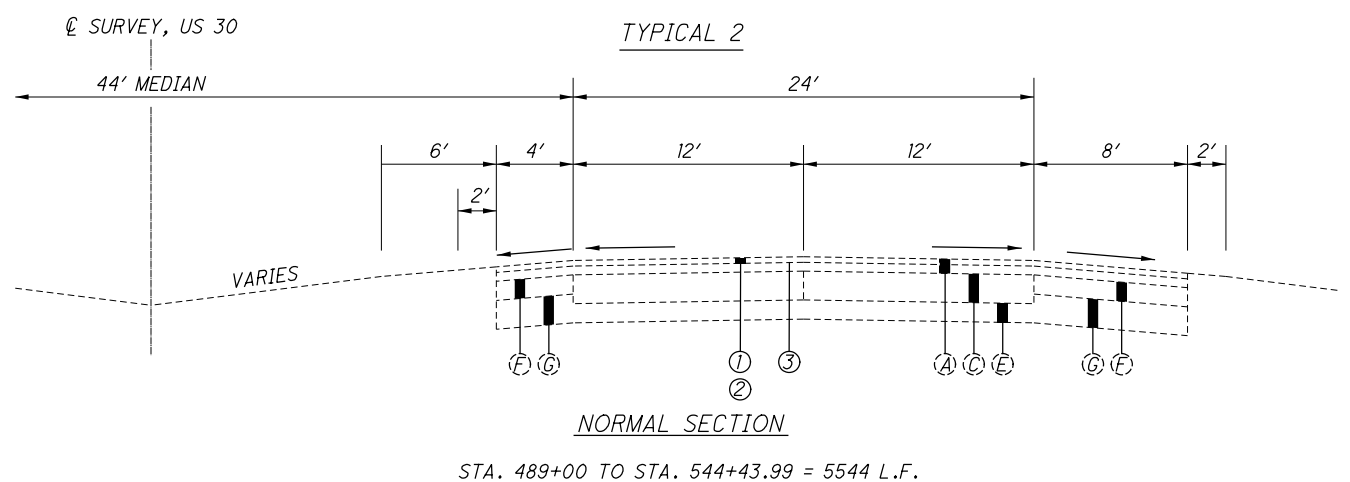
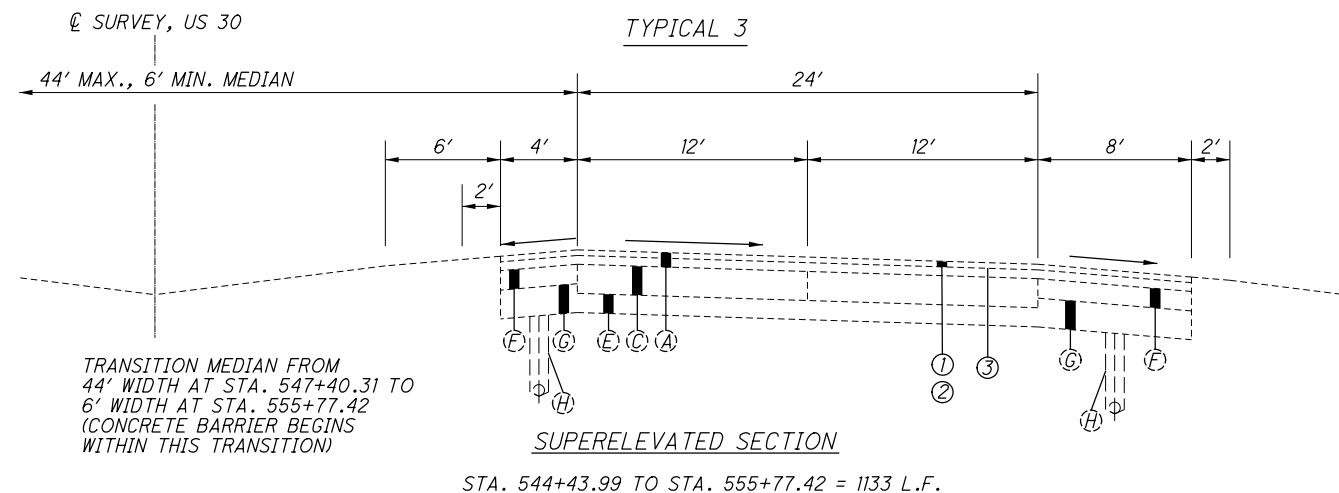
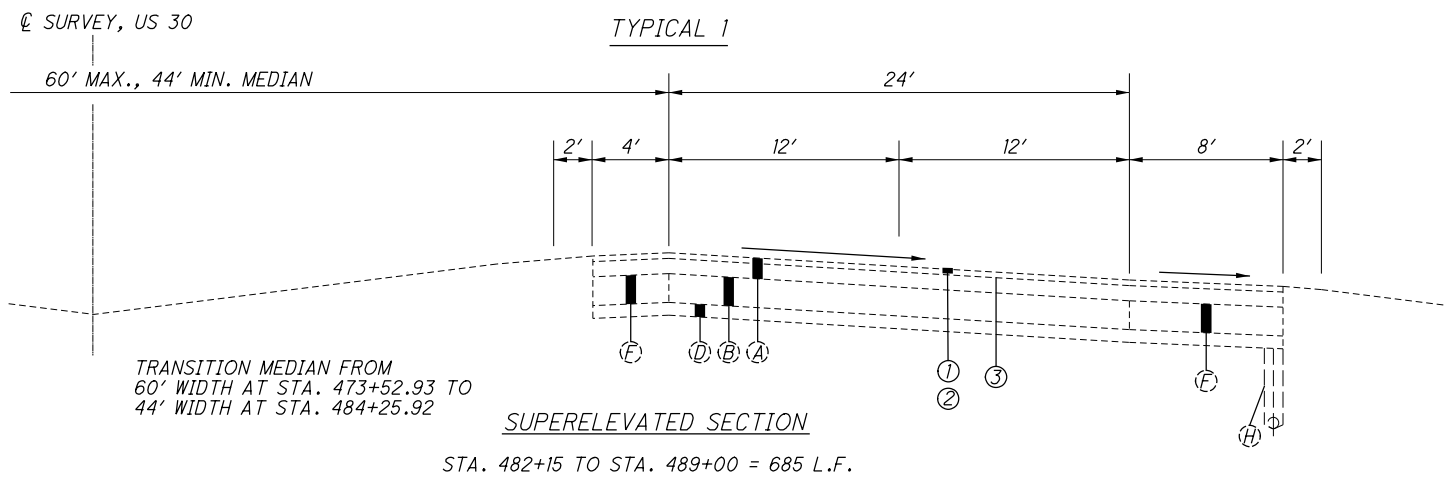


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HORIZONTAL SCALE IN FEET

SCHEMATIC / DESIGN DESIGNATIONS
RIC-71 / 30 INTERCHANGE RAMP

RIC-30-9.13 RM



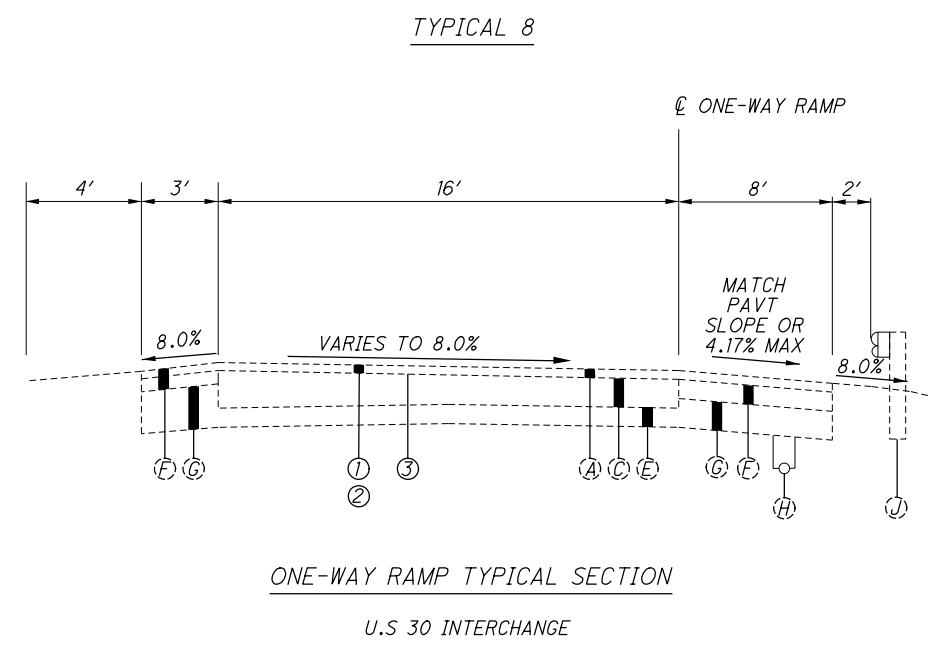
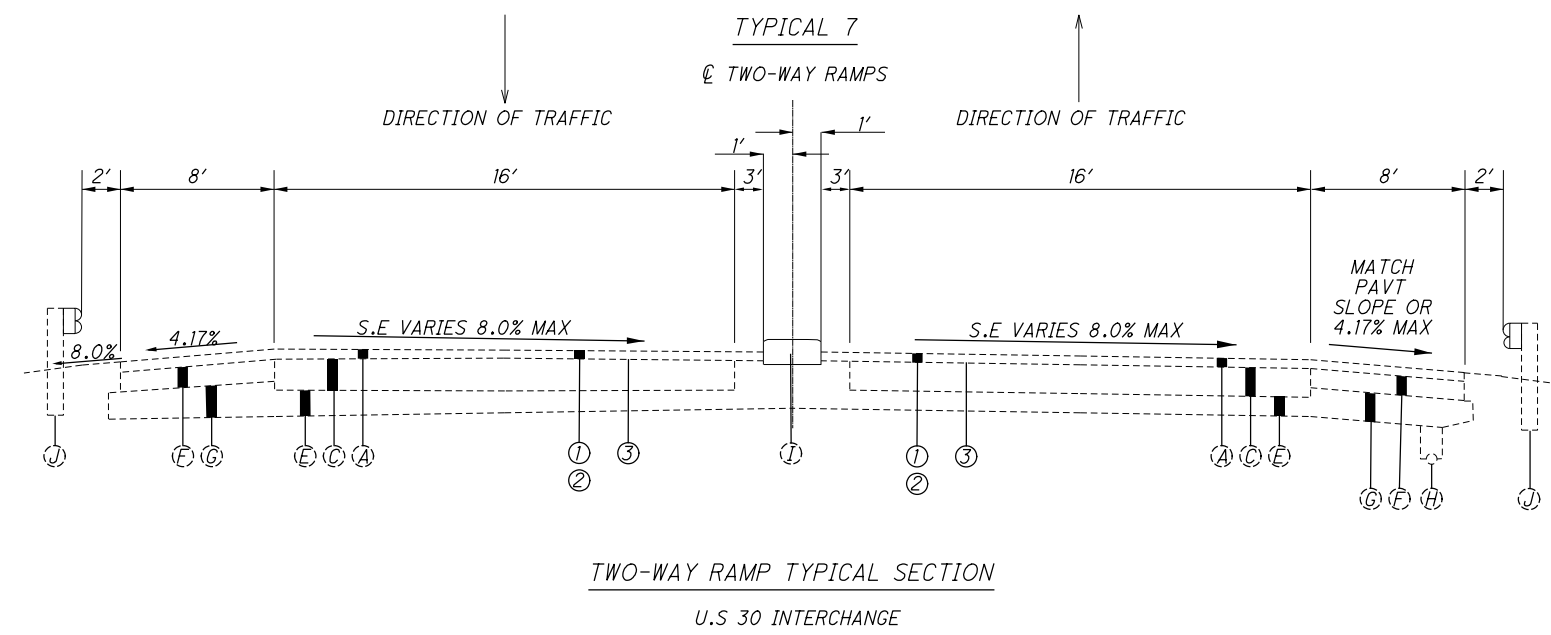
LEGEND - EXISTING

- (A) EXISTING ASPHALT CONCRETE VARIES, SEE PAVEMENT CORING INFORMATION
- (B) EXISTING 9" BITUMINOUS AGGREGATE BASE
- (C) EXISTING 9" REINFORCED CONCRETE
- (D) EXISTING 4" SUBBASE
- (E) EXISTING 6" SUBBASE
- (F) EXISTING VARIABLE AGGREGATE BASE
- (G) EXISTING VARIABLE SUBBASE
- (H) EXISTING 6" UNDERDRAIN

LEGEND - PROPOSED

- (1) ITEM 254 - 1.5" PAVEMENT PLANNING
- (2) ITEM 442 - 1.5" ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A (446)
- (3) ITEM 407 - TACK COAT

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LEGEND - EXISTING

- | | |
|--|------------------------------------|
| Ⓐ EXISTING ASPHALT CONCRETE VARIES,
SEE PAVEMENT CORING INFORMATION | Ⓕ EXISTING VARIABLE AGGREGATE BASE |
| Ⓑ EXISTING 9" BITUMINOUS AGGREGATE BASE | Ⓖ EXISTING VARIABLE SUBBASE |
| Ⓒ EXISTING 9" REINFORCED CONCRETE | Ⓗ EXISTING 6" UNDERDRAIN |
| Ⓓ EXISTING 4" SUBBASE | Ⓙ CONCRETE MEDIAN |
| Ⓔ EXISTING 6" SUBBASE | Ⓚ GUARDRAIL, TYPE 5 |

LEGEND - PROPOSED

- ① ITEM 254 - 1.5" PAVEMENT PLANNING
- ② ITEM 442 - 1.5" ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A (446)
- ③ ITEM 407 - TACK COAT

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GENERAL

ROUTINE MAINTENANCE

BETWEEN THE TIME THAT BIDS ARE TAKEN AND THE START OF CONSTRUCTION, THE MAINTAINING AGENCY MAY ENTER UPON THE PROJECT AND PERFORM ROUTINE MAINTENANCE SUCH AS CRACK SEALING, PATCHING, AND BERM AND SHOULDER REPAIR. THE EFFECTS, IF ANY, OF THE PERFORMANCE OF ROUTINE MAINTENANCE SHALL BE CONSIDERED AS INHERENT IN WORK OF THE CHARACTER PROVIDED FOR IN THE PLAN AND THE RESULTING CONDITIONS SHALL NOT BE CONSIDERED AS DIFFERING MATERIALLY FROM THOSE EXISTING AT THE TIME BIDS WERE TAKEN.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. THE INSTALLATION AND OPERATION OF ALL TEMPORARY TRAFFIC CONTROL AND TEMPORARY TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS SHALL BE PROVIDED BY THE CONTRACTOR WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

UTILITIES

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS.

THE LOCATION OF THE UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE AS OBTAINED FROM THE OWNERS AS REQUIRED BY SECTION 153.64 O.R.C.

AT&T, CONSULTANT FOR AT&T HLG CONSULTING GREG BELEW 5980-G WILCOX PLACE DUBLIN, OH 43016 704-906-2685	CENTURYLINK TIM BOWSER 175 ASHLAND ROAD P.O. BOX 3555 MANSFIELD, OHIO 44907 419-755-7956
--	---

CITY OF MANSFIELD ROBERT BIANCHI 30 NORTH DIAMOND ST. MANSFIELD, OH 44902 419-755-9702	CITY OF ONTARIO 555 STUMBO ROAD, ONTARIO, OHIO 44862 419-529-6402
--	--

COLUMBIA GAS OF OHIO JERROD SWINEHART 1021 N. MAIN ST. MANSFIELD, OHIO 44903 419-528-1137	ODOT D03 TRAFFIC MATT BLANKENSHIP 906 CLARK AVE ASHLAND, OH 44805 419-207-7045
---	--

OHIO EDISON COMPANY TRAVIS BALLOG 1717 ASHLAND ROAD MANSFIELD, OHIO 44905 419-521-6213	RICHLAND CO. SANITARY ENGINEER STEVE RISSER 50 PARK AVENUE EAST MANSFIELD, OHIO 44902 419-774-3548
--	--

TIME WARNER CABLE
DAVE ROUSH
1575 LEXINGTON AVENUE
MANSFIELD, OHIO 44901
419-756-6091 X. 419-555-5136

THE AFOREMENTIONED UTILITY COMPANIES AND AGENCIES HAVE VARIOUS FACILITIES IN THE AREA THAT WILL REMAIN IN PLACE DURING CONSTRUCTION.

ROADWAY

ITEM 202 - REMOVAL MISC.: (MEDIAN)

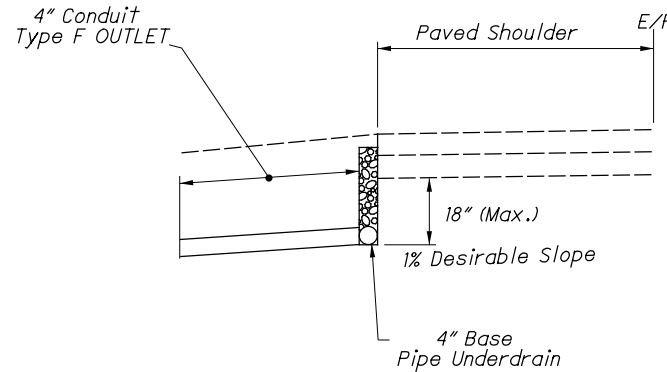
THE EXISTING CONCRETE MEDIAN LOCATED ON RAMPS CD AND E SHALL BE REMOVED IN AREAS MARKED BY THE ENGINEER THAT ARE DETERIORATED AND NEED REPLACED.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, SAW CUTTING, AND MATERIALS NECESSARY TO COMPLETE THE REMOVAL. PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER SQUARE YARD OF ITEM 202 REMOVAL MISC.: (MEDIAN)

DRAINAGE

ITEM 605 - 4" BASE PIPE UNDERDRAIN WITH GEOTEXTILE FABRIC

THIS ITEM IS DETAILED ON SHEET NO. 15 AND SHALL INCLUDE ALL LABOR, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT THIS ITEM. UNDERDRAINS ARE TO BE INSTALLED ON I71/SR30 INTERCHANGE RAMP E, BETWEEN STA. 5+20.00 TO STA. 13+88.00. INCLUDED WILL BE THREE OUTLETS AT 5+20.00, 7+24.00 AND 9+38.00. THE INTENT OF THIS ITEM IS TO DRAIN WATER FROM THE BASE THAT HAS BEEN FLUSHING UP THROUGH THE SHOULDER PAVEMENT.



PAVEMENT

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (442), AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF THE REMOVAL OF THE EXISTING PAVEMENT OR PAVED BERM WHICH MAY BE ASPHALT, BRICK, CONCRETE, OR A COMBINATION OF EACH, IN AREAS OF EXISTING PAVEMENT FAILURE. CORING HAS BEEN PERFORMED TO HELP DETERMINE THE COMPONENTS THAT MAY BE ENCOUNTERED DURING THIS ITEM OF WORK. THIS PAY ITEM IS NOT TO BE USED WHERE 252/255 REPAIRS WILL BE DONE.

ALL RAMP PAVEMENT REPAIRS SHALL BE PERFORMED AFTER PAVEMENT PLANING AND BEFORE PLACEMENT OF THE INTERMEDIATE AND/OR SURFACE COURSE. REPLACEMENT MATERIAL SHALL BE ITEM 301 PLACED AND COMPACTED TO FINISH FLUSH WITH PLANNED SURFACE.

THE DEPTH OF REMOVAL SHALL BE SUFFICIENT TO REMOVE ALL DETERIORATED PAVEMENT WITH A MAXIMUM DEPTH OF 4", BASED ON THE PAVEMENT DESIGN AND AN AVERAGE DEPTH OF 3" AND AN AVERAGE WIDTH OF 12 FT FOR ESTIMATING PURPOSES.

THE CONTRACTOR SHALL BE CAPABLE OF PERFORMING PAVEMENT REPAIRS 2 FEET WIDE.

REPLACEMENT MATERIAL ON US 30 SHALL BE ITEM 301 IN THE FIRST LIFT AND 1.5" OF ITEM 442 ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A (446) IN THE SECOND LIFT. THE SURFACE COURSE SHALL BE PLACED AND COMPACTED TO FINISH FLUSH WITH THE ADJACENT PAVEMENT SURFACE.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT REPAIR. PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER CUBIC YARD, (BY TICKET WEIGHT CONVERSION), OF ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (442), AS PER PLAN.

ITEM 252. FULL DEPTH RIGID PAVEMENT REMOVAL AND FLEXIBLE REPLACEMENT, AS PER PLAN

THE EXISTING 9" CONCRETE PAVEMENT AND APPROXIMATELY 6.5" OF ASPHALT CONCRETE SHALL BE REMOVED AS PART OF THIS PAY ITEM. CLEAN AND COAT THE VERTICAL FACES WITH AN ASPHALT MATERIAL COMPLYING WITH 407.02. PLACE APPROXIMATELY 14" OF ITEM 301 ASPHALT CONCRETE BASE, PG 64-22 AND 1.5" OF ITEM 442 ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A (448). THE SURFACE COURSE SHALL BE PLACED AND COMPACTED TO FINISH FLUSH WITH THE ADJACENT PAVEMENT SURFACE PRIOR TO OPENING TO TRAFFIC. THE CONTRACTOR MAY USE AN ITEM 302 MIX INSTEAD OF AN ITEM 301 MIX.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE ABOVE DESCRIBED PAVEMENT REPAIR WORK, IN ADDITION TO THE REST OF THE REQUIREMENTS IN CMS ITEM 252. PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER SQUARE YARD OF ITEM 252 FULL DEPTH RIGID PAVEMENT REMOVAL AND FLEXIBLE REPLACEMENT, AS PER PLAN.

ITEM 255. FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, AS PER PLAN (RAMPS)

THE EXISTING 10" CONCRETE BASE PAVEMENT AND APPROXIMATELY 4" OF ASPHALT CONCRETE ON THE US 30 AND I71 INTERCHANGE RAMPS SHALL BE REMOVED AS PART OF THIS PAY ITEM. REPLACE THE CONCRETE BASE IN ACCORDANCE OF THE SPEC AND MAKE FLUSH WITH THE EXISTING CONCRETE BASE. REPLACE ASPHALT CONCRETE INTERMEDIATE TO BE FLUSH THE PLANNED SURFACE.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE ABOVE DESCRIBED PAVEMENT REPAIR WORK, IN ADDITION TO THE REST OF THE REQUIREMENTS IN CMS ITEM 255. PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER SQUARE YARD OF ITEM 255 FULL DEPTH RIGID PAVEMENT REMOVAL AND REPLACEMENT, AS PER PLAN.

ITEM 511 - CLASS QC2 CONCRETE, MISC.: (MEDIAN)

THE EXISTING CONCRETE MEDIAN LOCATED ON RAMPS CD AND E SHALL BE REPLACED IN AREAS MARKED BY THE ENGINEER THAT ARE DETERIORATED. AN AVERAGE DEPTH OF 3.5" AND AN AVERAGE WIDTH OF 2 FT WERE USED FOR ESTIMATING PURPOSES.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE REPLACEMENT. PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER CUBIC YARDS OF ITEM 511 CLASS QC2 CONCRETE, MISC.: (MEDIAN)

CALCULATED
JWS
CHECKED
JWS

GENERAL NOTES

RIC-30-9.13 RM

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PAVEMENT (CONTINUED)

PAVEMENT CORING INFORMATION

BELOW IS PAVEMENT CORING INFORMATION TO HELP DETERMINE THE EXISTING PAVEMENT COMPOSITION.

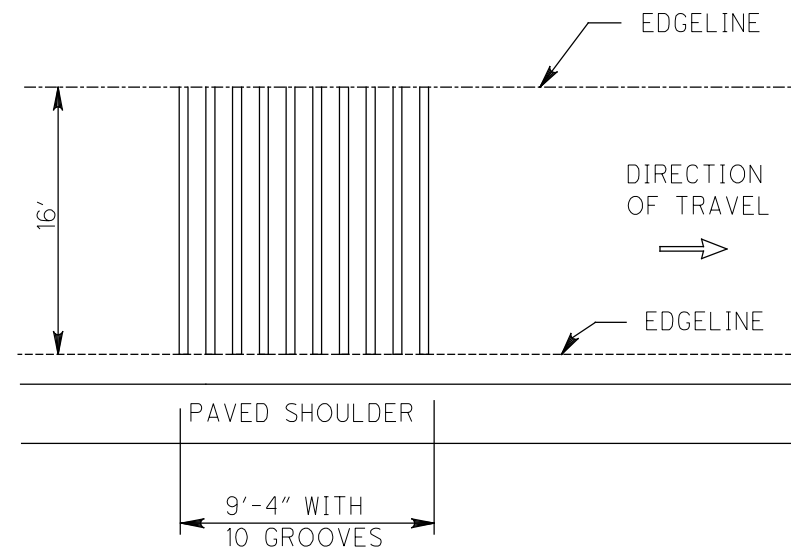
ROUTE	COUNTY	SLM	DRIVING LANE	ASPHALT (IN.)	CONCRETE (IN.)	DIRECTION
30	RIC	9.00	INSIDE	14.00		EB
30	RIC	9.00	OUTSIDE	14.00		EB
30	RIC	9.00	SHOULDER	6.00		EB
30	RIC	9.00	INSIDE	14.00		EB
30	RIC	9.00	OUTSIDE	14.00		EB
30	RIC	9.00	SHOULDER	7.00		EB
30	RIC	9.80	INSIDE	6.50	9.00	WB
30	RIC	9.80	OUTSIDE	6.50	9.00	WB
30	RIC	9.80	SHOULDER	7.50		WB
30	RIC	9.80	INSIDE	6.50	15.00	WB
30	RIC	9.80	OUTSIDE	6.50	15.00	WB
30	RIC	9.80	SHOULDER	9.00		WB
30	RIC	9.90	INSIDE	7.00	14.50	EB
30	RIC	9.90	OUTSIDE	6.50	14.00	EB
30	RIC	9.90	SHOULDER	8.50		EB
30	RIC	9.90	INSIDE	6.50	9.00	EB
30	RIC	9.90	OUTSIDE	6.50	8.50	EB
30	RIC	9.90	SHOULDER	8.50		EB
30	RIC	11.00	INSIDE	6.00	8.50	WB
30	RIC	11.00	OUTSIDE	6.00	13.00	WB
30	RIC	11.00	SHOULDER	6.50		WB
30	RIC	11.00	INSIDE	5.00	8.50	WB
30	RIC	11.00	OUTSIDE	5.00	8.50	WB
30	RIC	11.00	SHOULDER	6.00		WB

ITEM 618 - RUMBLE STRIPS (ASPHALT CONCRETE) AS PER PLAN

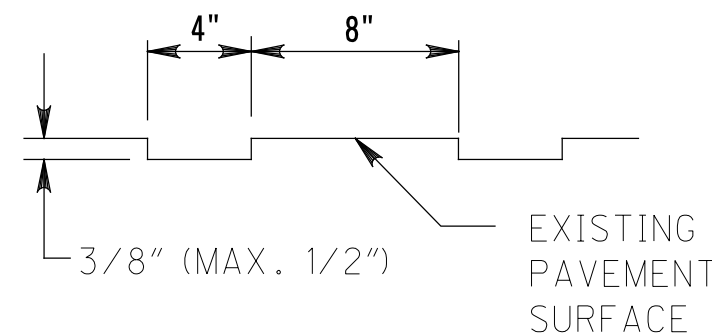
THIS ITEM CONSISTS OF REPLACING RUMBLE STRIPS ON RAMPS CD, B AND E AS SEEN ON SHEET 15. THE PROPOSED RUMBLE STRIPS SHALL CONSIST OF PARALLEL GROOVES CUT AT ONE FOOT INTERVALS.

ALL DIMENSIONS SHOWN ARE NOMINAL AND SHOULD BE CONSIDERED TO BE ± 1/8 INCH. EACH GROOVE SHALL BE CUT TO A DEPTH OF APPROXIMATE 3/8 INCH WITH ALLOWANCE FOR PAVEMENT SURFACE IRREGULARITIES AND VARIATIONS. WIDTH OF THE GROOVE AT THE PAVEMENT SURFACE IS TO BE 4 INCHES.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE STRIPS. PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER FOOT OF ITEM 618 - RUMBLE STRIPS (ASPHALT CONCRETE) AS PER PLAN, WITH 16' AS AN AVERAGE PER STRIP FOR ESTIMATING PURPOSES.



TYPICAL RUMBLE STRIP



TYPICAL GROOVE DETAIL

MAINTENANCE OF TRAFFIC

ITEM 614. MAINTAINING TRAFFIC (LANES OPEN DURING HOLIDAYS OR SPECIAL EVENTS)

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS:

CHRISTMAS	FOURTH OF JULY
NEW YEARS	LABOR DAY
MEMORIAL DAY	THANKSGIVING

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF THE WEEK	TIME ALL LANES MUST BE OPEN TO TRAFFIC
-----------------	--

SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

ALSO, NO WORK SHALL BE PERFORMED AT THE TRIMBLE RD INTERCHANGE DURING THE RICHLAND COUNTY FAIR (08/06/2017 THRU 08/12/2017, NO WORK SHALL BE PERFORMED WHICH DETOURS TRAFFIC ONTO TRIMBLE RD DURING THE RICHLAND COUNTY FAIR, NO WORK SHALL BE PERFORMED AND RAMPS SHALL BE OPEN AT THE SR545 INTERCHANGE DURING HALLOWEEN WEEKEND, AND NO WORK SHALL BE PERFORMED AND RAMPS SHALL BE OPEN AT THE SR545 INTERCHANGE AND FIFTH AVE INTERCHANGE DURING MANSFIELD MOTOR SPEEDWAY EVENTS.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE OF \$50/MINUTE FOR EACH MINUTE THE ABOVE DESCRIBED LANE CLOSURE RESTRICTIONS ARE VIOLATED.

ITEM 614. MAINTAINING TRAFFIC

ALL ADVANCE WARNING SIGNS FOR ANY CONDITION WHICH RESTRICTS TRAFFIC SHALL BE ERECTED BEFORE ANY SUCH RESTRICTION IS PUT INTO EFFECT. ALL SUCH SIGNS SHALL BE COVERED OR REMOVED FROM THE VIEW OF TRAFFIC WHEN THEY ARE NOT APPLICABLE, WITH THE APPROVAL OF THE ENGINEER.

IF THE CONTRACTOR FAILS TO COMPLY WITH THE PROVISIONS FOR TRAFFIC CONTROL AS SET FORTH IN THESE PLANS OR WITH PROVISIONS OF THE OMUTCD, AND SUCH FAILURE RESULTS IN A CONDITION AT THE WORK SITE WHICH IS UNSAFE FOR TRAFFIC, THE ENGINEER SHALL SUSPEND WORK UNTIL THE CONTRACTOR COMPLIES WITH THE NECESSARY REQUIREMENTS.

ALL MAINTENANCE OF TRAFFIC SIGNS ARE PAID UNDER ITEM 614, MAINTAINING TRAFFIC.

ITEM 614 - MAINTAINING TRAFFIC: GENERAL

ONE 11' LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES. ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH ITEM 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, PLAN DETAILS, STANDARD DRAWINGS, AND AS OUTLINED IN THE CONSTRUCTION AND MAINTENANCE SECTION OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES CURRENT EDITION WITH THE LATEST REVISIONS. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614 - MAINTAINING TRAFFIC UNLESS SEPARATELY ITEMIZED ON THIS PLAN.

THE FOLLOWING REQUIREMENTS SHALL ALSO APPLY: THE CONTRACTOR SHALL SUBMIT, IN WRITING, A SCHEDULE OF OPERATIONS TO THE ENGINEER AND RECEIVE APPROVAL BEFORE WORK IS STARTED ON THE PROJECT. PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL COORDINATE THE MAINTENANCE OF TRAFFIC OPERATIONS WITH THE LOCAL STATE HIGHWAY PATROL.

NIGHT WORK IS PERMITTED.

THE CONTRACTOR IS REQUIRED TO MAINTAIN ALL PAVEMENT THROUGHOUT THE PROJECT UNDER ITEM 614 ASPHALT CONCRETE FOR MAINTAINING TRAFFIC DURING THE PERIOD FROM THE START OF WORK TO THE COMPLETION OF ALL WORK.

CALCULATED
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GENERAL NOTES

RIC-30-9.13 RM

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MAINTENANCE OF TRAFFIC FOR MARKING PAVEMENT REPAIRS

A MINIMUM OF 24 HOURS PRIOR TO PERFORMING PAVEMENT REPAIRS, THE CONTRACTOR SHALL PROVIDE LANE CLOSURES, PER THE MAINTENANCE OF TRAFFIC NOTES IN THESE PLANS, FOR THE ENGINEER TO IDENTIFY THE AREAS IN NEED OF REPAIR.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT UNIT PRICE FOR ITEM 614 - MAINTAINING TRAFFIC.

WORK OPERATIONS

IN ADDITION TO THE REQUIREMENTS OF SECTION 614 OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS THE FOLLOWING SHALL APPLY:

THE CONTRACTOR'S EQUIPMENT SHALL BE OPERATED IN THE DIRECTION OF TRAVEL WHERE PRACTICAL. A FLAGGER SHALL BE USED WHERE THE CONTRACTOR'S EQUIPMENT MUST MERGE WITH THE TRAFFIC STREAM.

THE CONTRACTOR SHALL ARRANGE CONSTRUCTION OPERATIONS SO AS TO PREVENT ANY INTERFERENCE TO THE CONTINUOUS FLOW OF TRAFFIC. ALL VEHICLES, EQUIPMENT, WORKERS AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIMES TO THE CLOSED LANES UNLESS OTHERWISE APPROVED BY THE ENGINEER.

THE CONTRACTOR IS REQUIRED TO WORK AT NIGHT. FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHT TIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE HIGHWAY. TO INSURE THE ADEQUACY OF THE FLOODLIGHTING PLACEMENT PRIOR TO COMMENCING ANY WORK, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614 MAINTAINING TRAFFIC.

EQUIPMENT MAY BE PARKED IN AREAS ALONG THE HIGHWAY A MIN. OF 6 FT BEHIND GUARDRAIL OR 30 FT FROM THE NEAREST EDGE OF PAVEMENT WHEN VARIOUS OPERATIONS ARE SCHEDULED TO CONTINUE THE NEXT WORKDAY. ON WEEKENDS OR AT OTHER TIMES OF SUSPENSION OF WORK, THE EQUIPMENT SHALL BE STORED AT A STORAGE AREA OUTSIDE OF THE ROADWAY RIGHT-OF-WAY. THE LOCATION SHALL HAVE PRIOR APPROVAL OF THE ENGINEER. ADEQUATE BARRICADES AND LIGHTS SHALL BE PLACED ON THE PAVEMENT SIDE OF THE EQUIPMENT TO IDENTIFY THE LIMITS OF THE EQUIPMENT. ALL OTHER EQUIPMENT, INCLUDING PRIVATE VEHICLES, SHALL BE STORED AT THE APPROVED CONTRACTOR'S STORAGE AREA.

ITEM 614. WORK ZONE INCREASED PENALTIES SIGN

R11-H5a SIGNS SHALL BE FURNISHED, ERECTED, AND MAINTAINED IN GOOD CONDITION AND/OR REPLACED AS NECESSARY AND SUBSEQUENTLY REMOVED BY THE CONTRACTOR.

THE SIGNS MAY BE ERECTED OR UNCOVERED NO MORE THAN FOUR HOURS BEFORE THE ACTUAL START OF WORK. THE SIGNS SHALL BE REMOVED OR COVERED NO LATER THAN FOUR HOURS FOLLOWING RESTORATION OF ALL LANES TO TRAFFIC WITH NO RESTRICTIONS, OR SOONER AS DIRECTED BY THE ENGINEER. TEMPORARY SIGN COVERING AND UNCOVERING DUE TO TEMPORARY LANE RESTORATIONS SHALL BE GUIDED BY THE FOUR-HOUR LIMITATIONS STATED ABOVE. SUCH LANE RESTORATIONS SHOULD BE EXPECTED TO REMAIN IN EFFECT FOR 30 OR MORE DAYS, SUCH AS DURING WINTER SHUT-DOWNS.

THE SIGNS SHALL BE DUAL MOUNTED ON THE MAINLINE AND PLACED PER STANDARD CONSTRUCTION DRAWING MT-95.50.

THE CONTRACTOR MAY USE SIGNS AND SUPPORTS IN USED, BUT GOOD, CONDITION PROVIDED THE SIGNS MEET CURRENT ODOT SPECIFICATIONS. SIGN FACES SHALL BE REFLECTORIZED WITH TYPE G SHEETING COMPLYING WITH THE REQUIREMENTS OF CMS 730.19.

WORK ZONE INCREASED PENALTIES SIGNS AND SUPPORTS WILL BE MEASURED AS THE NUMBER OF SIGN INSTALLATIONS, INCLUDING THE SIGN AND NECESSARY SUPPORTS. IF A SIGN AND SUPPORT COMBINATION IS REMOVED AND REERECTED AT ANOTHER LOCATION AS DIRECTED BY THE ENGINEER, IT SHALL BE CONSIDERED ANOTHER UNIT.

PAYMENT FOR ACCEPTED QUANTITIES, COMPLETE, IN PLACE WILL BE MADE AT THE CONTRACT UNIT BID PRICE. PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIALS, LABOR, INCIDENTALS AND EQUIPMENT FOR FURNISHING, ERECTING, MAINTAINING, COVERING DURING SUSPENSION OF WORK AND REMOVAL OF THE SIGN AND SUPPORT.

ITEM 614, WORK ZONE INCREASED PENALTIES SIGN = 6 EACH

ITEM 614 - WORK ZONE SPEED ZONES (WZSZS)

THE FOLLOWING WORK ZONE SPEED ZONE (WZSZ) SPEED LIMIT REVISION(S) HAVE BEEN APPROVED FOR USE ON THIS PROJECT WHEN WORK ZONE CONDITIONS AND FACTORS ARE MET AS DESCRIBED BELOW:

WZSZ REVISION NUMBER	COUNTY & ROUTE	DIRECTION
WZ-20488	RIC-30	EB & WB

POTENTIAL WZSZ LOCATIONS SHALL HAVE AN ORIGINAL (PRE-CONSTRUCTION) POSTED SPEED LIMIT OF 55 MPH OR GREATER, A QUALIFYING WORK ZONE CONDITION OF AT LEAST 0.5 MILE IN LENGTH, AN EXPECTED WORK DURATION OF AT LEAST THREE HOURS, AND A WORK ZONE CONDITION IN PLACE THAT REDUCES THE EXISTING FUNCTIONALITY OF THE TRAVEL LANES OR SHOULDERS (I.E., LANE CLOSURE, LANE SHIFT, Crossover, CONTRAFLOW AND/OR SHOULDER CLOSURE). THE LENGTH OF THE WORK ZONE CONDITION IS MEASURED FROM THE BEGINNING OF THE TAPER FOR THE SUBJECT WORK ZONE CONDITION IMPACTING THE TRAVEL LANES AND/OR SHOULDER TO THE END OF THE DOWNSTREAM TAPER, WHERE DRIVERS ARE RETURNED TO TYPICAL ALIGNMENT. AN EXPECTED WORK DURATION OF AT LEAST THREE HOURS IS REQUIRED TO BALANCE THE ADDITIONAL EXPOSURE CREATED BY INSTALLING AND REMOVING WZSZ SIGNING WITH THE TIME NEEDED TO COMPLETE THE WORK.

IF THE WORK ZONE MEETS THESE MINIMUM CRITERIA, IT SHALL BE ANALYZED FURTHER USING TABLE 1 BELOW TO DETERMINE IF AND WHEN IT QUALIFIES FOR A SPEED LIMIT REDUCTION. DEPENDING ON THE ORIGINAL POSTED SPEED LIMIT, THE TYPE OF TEMPORARY TRAFFIC CONTROL USED, AND WHETHER OR NOT WORKERS ARE PRESENT, A WARRANTED WZSZ WILL VARY IN THE APPROVED SPEED LIMIT TO BE POSTED OVER TIME.

C&MS ITEM 614, PARAGRAPH 614.02(B), INDICATES THAT TWO DIRECTIONS OF A DIVIDED HIGHWAY ARE CONSIDERED SEPARATE HIGHWAY SECTIONS. THEREFORE, IF THE WORK ON A MULTI-LANE DIVIDED HIGHWAY IS LIMITED TO ONLY ONE DIRECTION, A SPEED LIMIT REDUCTION IN THE DIRECTION OF THE WORK DOES NOT AUTOMATICALLY CONSTITUTE A SPEED LIMIT REDUCTION IN THE OPPOSITE DIRECTION. EACH DIRECTION SHALL BE ANALYZED INDEPENDENTLY FROM EACH OTHER.

ALL WZSZS FLUCTUATE BETWEEN TWO APPROVED REDUCED SPEED LIMITS OR BETWEEN AN APPROVED REDUCED SPEED LIMIT AND THE ORIGINAL POSTED SPEED LIMIT. ONLY ONE OF TWO SIGNING STRATEGIES SHALL BE USED TO IMPLEMENT A WZSZ.

[WZSZS USING DSL SIGN ASSEMBLIES SHALL BE IN ACCORDANCE WITH THIS NOTE, APPROVED LIST, SUPPLEMENTAL SPECIFICATIONS (SS) 808 AND 908, AND TRAFFIC SCD MT-104.10.]

WHEN LOOKING UP THE WARRANTED WORK ZONE SPEED LIMITS, ALWAYS USE THE ORIGINAL, PRE-CONSTRUCTION, POSTED SPEED LIMIT. DO NOT USE A PRIOR OR CURRENT WORK ZONE SPEED LIMIT AS A LOOK UP VALUE IN THE TABLE. POSITIVE PROTECTION IS GENERALLY REGARDED AS PORTABLE BARRIER OR OTHER RIGID BARRIER IN USE ALONG THE WORK AREA WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WITHOUT POSITIVE PROTECTION IS GENERALLY REGARDED AS USING DRUMS, CONES, SHADOW VEHICLE, ETC., ALONG THE WORK AREA WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WORKERS ARE CONSIDERED AS BEING PRESENT WHEN ON-SITE, WORKING WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WHEN THE WORK ZONE CONDITION REDUCING THE EXISTING FUNCTIONALITY OF THE TRAVEL LANES OR SHOULDERS IS REMOVED, THE SPEED LIMIT DISPLAYED SHALL RETURN TO THE ORIGINAL POSTED SPEED LIMIT.

55 MPH SHALL BE USED AS THE BASE SPEED LIMIT FOR THE USR 30 PORTION OF THE PROJECT. THIS INCLUDES THE SECTION BETWEEN SLM 12.52-13.00 WHERE THE CURRENT POSTED SPEED LIMIT IS 60 MPH.

TABLE 1: WARRANTED WORK ZONE SPEED LIMITS (MPH) FOR WORK ZONES ON HIGH-SPEED (55 MPH OR GREATER) MULTI-LANE HIGHWAYS. POTENTIAL WZSZ LOCATIONS SHALL HAVE AN ORIGINAL (PRE-CONSTRUCTION) POSTED SPEED LIMIT OF 55 MPH OR GREATER, A QUALIFYING WORK ZONE CONDITION OF AT LEAST 0.5 MILE IN LENGTH, AN EXPECTED WORK DURATION OF AT LEAST THREE HOURS, AND A WORK ZONE CONDITION IN PLACE THAT REDUCES THE EXISTING FUNCTIONALITY OF THE TRAVEL LANES OR SHOULDERS (I.E., LANE CLOSURE, LANE SHIFT, Crossover, CONTRAFLOW AND/OR SHOULDER CLOSURE). THE LENGTH OF THE WORK ZONE CONDITION IS MEASURED FROM THE BEGINNING OF THE TAPER FOR THE SUBJECT WORK ZONE CONDITION IMPACTING THE TRAVEL LANES AND/OR SHOULDER TO THE END OF THE DOWNSTREAM TAPER, WHERE DRIVERS ARE RETURNED TO TYPICAL ALIGNMENT. AN EXPECTED WORK DURATION OF AT LEAST THREE HOURS IS REQUIRED TO BALANCE THE ADDITIONAL EXPOSURE CREATED BY INSTALLING AND REMOVING WZSZ SIGNING WITH THE TIME NEEDED TO COMPLETE THE WORK.

ORIGINAL POSTED SPEED LIMIT	WITH POSITIVE PROTECTION		WITHOUT POSITIVE PROTECTION	
	WORKERS PRESENT	WORKERS NOT PRESENT	WORKERS PRESENT	WORKERS NOT PRESENT
70	60	65	55	65
65	55	60	50	60
60	55	60	50	60
55	50	55	45	55

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY

ITEM 614, DIGITAL SPEED LIMIT (DSL) SIGN ASSEMBLY-----12 SIGN MNTH ASSUMING 3 DSL SIGN ASSEMBLIES FOR 4 MONTHS

ITEM 614. WORKSITE TRAFFIC SUPERVISOR

SUBJECT TO APPROVAL OF THE ENGINEER, THE CONTRACTOR SHALL EMPLOY AND IDENTIFY (SOMEONE OTHER THAN THE SUPERINTENDENT) A CERTIFIED WORKSITE TRAFFIC SUPERVISOR (WTS) BEFORE STARTING WORK IN THE FIELD. THE WTS MAY BE CERTIFIED FROM ONE OF THE FOLLOWING ORGANIZATIONS:

1. AMERICAN TRAFFIC SAFETY SERVICE ASSOCIATION (ATSSA), PHONE NUMBER 1-800-272-8772, CERTIFIED TRAFFIC CONTROL SUPERVISOR (TCS).
2. NATIONAL HIGHWAY INSTITUTE, DESIGN AND OPERATION OF WORK ZONE TRAFFIC CONTROL, PHONE NUMBER 1-703-235-0528.
3. THE OHIO CONTRACTORS ASSOCIATION, TRAFFIC CONTROL SUPERVISOR (OCA/TCS) WORK ZONE CLASS, ONLY IF TAKEN AFTER MAY 5, 2004, PHONE NUMBER 1-614-599-7915.
4. OHIO LABORERS TRAINING, TRAFFIC CONTROL SUPERVISORS CLASS, PHONE NUMBER 1-740-599-7915.

A COPY OF EACH WTS'S CERTIFICATION AND 24-HOUR CONTACT INFORMATION SHALL BE PROVIDED TO THE ENGINEER AT THE PRECONSTRUCTION CONFERENCE. IF THE DESIGNATED WTS WILL NOT BE AVAILABLE FULL TIME (24/7) THE CONTRACTOR MAY DESIGNATE AN ALTERNATE WTS TO BE AVAILABLE WHEN THE PRIMARY IS OFF DUTY. EACH WTS SHALL HAVE A CURRENT WTS CERTIFICATION (WITH AN EXPIRATION DATE NO MORE THAN 5 YEARS FROM THE DATE OF ISSUE) FROM ANY OF THE APPROVED ORGANIZATIONS.

THE WTS POSITION HAS THE RESPONSIBILITY OF MONITORING TRAFFIC CONTROL DEFICIENCIES FOR THE ENTIRE WORK ZONE. THE DUTIES OF THE WTS ARE AS FOLLOWS:

1. BE AVAILABLE ON A 24-HOUR PER DAY BASIS, AND BE ABLE TO BE ON SITE FOR ALL EMERGENCY TRAFFIC CONTROL NEEDS WITHIN ONE HOUR OF NOTIFICATION BY POLICE OR PROJECT STAFF AND BE PREPARED TO EFFECT CORRECTIVE MEASURES IMMEDIATELY ON EXISTING WORK ZONE TRAFFIC CONTROL DEVICES.
2. ATTEND PRECONSTRUCTION MEETING AND ALL PROJECT MEETINGS WHERE TRAFFIC CONTROL MANAGEMENT IS DISCUSSED.
3. BE AVAILABLE FOR MEETINGS OR DISCUSSIONS WITH THE ENGINEER UPON REQUEST OR WITHIN 36 HOURS.
4. BE AWARE OF, AND COORDINATE IF NECESSARY, ALL TRAFFIC CONTROL OPERATIONS, INCLUDING THOSE OF SUBCONTRACTORS AND SUPPLIERS.
5. COORDINATE PROJECT ACTIVITIES WITH ALL LAW ENFORCEMENT OFFICERS (LEOS). A WTS SHALL ALSO BE THE MAIN CONTACT PERSON WITH THE LEO'S WHILE THEY ARE ON THE PROJECT.
6. COORDINATE MEETINGS WITH ODOT PERSONNEL, LEO'S AND OTHER APPLICABLE ENTITIES BEFORE EACH PLAN PHASE SWITCH TO DISCUSS WORK ZONE TRAFFIC CONTROL.
7. ENSURE COMPLIANCE WITH THE CONTRACT DOCUMENTS FOR SIGNS, BARRICADES, TEMPORARY CONCRETE BARRIER, PAVEMENT MARKINGS, PORTABLE MESSAGE SIGNS, AND OTHER TRAFFIC CONTROL DEVICES ON A DAILY BASIS; AND FACILITATE ANY CORRECTIVE ACTION NECESSARY.
8. NOTIFY THE CONTRACTOR OF THE NEED FOR CLEANING AND MAINTENANCE OF ALL TRAFFIC CONTROL DEVICES, INCLUDING THE COVERING AND REMOVAL OF INAPPLICABLE SIGNS.
9. INSPECT, EVALUATE, PROPOSE NECESSARY MODIFICATIONS TO, AND DOCUMENT THE EFFECTIVENESS OF, THE TRAFFIC CONTROL DEVICES AND/OR TRAFFIC OPERATIONS ON A DAILY BASIS (7 DAYS A WEEK). IN ADDITION, A WEEKLY NIGHT INSPECTION OF THE WORK ZONE SETUP FOR DAYTIME WORK OPERATIONS; AND ONE DAYTIME INSPECTION PER WEEK FOR NIGHTTIME PROJECTS. THIS SHALL INCLUDE (BUT NOT BE LIMITED TO) DOCUMENTATION ON THE FOLLOWING PROJECT EVENTS:
 - A. INITIAL TRAFFIC CONTROL SETUP (DAY AND NIGHT REVIEW).
 - B. DAILY TRAFFIC CONTROL SETUP AND REMOVAL.
 - C. WHEN CONSTRUCTION STAGING CAUSES A CHANGE IN THE TRAFFIC CONTROL SETUP.
 - D. CRASH OCCURRENCES WITHIN THE CONSTRUCTION AREA.
 - E. REMOVAL OF TRAFFIC CONTROL DEVICES AT THE END OF A PHASE OR PROJECT.
 - F. ALL OTHER EMERGENCY TRAFFIC CONTROL NEEDS.
10. COMPLETE THE DEPARTMENT APPROVED LONG TERM INSPECTION FORM (CA-D-8) AFTER EACH INSPECTION AS REQUIRED IN # 9 AND SUBMIT IT TO THE ENGINEER THE FOLLOWING WORK DAY. THESE REPORTS SHALL INCLUDE A CHECKLIST OF ALL TRAFFIC CONTROL MAINTENANCE ITEMS TO BE REVIEWED. A COPY OF THE FORM WILL BE PROVIDED AT THE PRE-CONSTRUCTION MEETING. ANY DEFICIENCIES OBSERVED SHALL BE NOTED, ALONG WITH RECOMMENDED CORRECTIVE ACTIONS AND THE DATES BY WHICH SUCH CORRECTIONS WERE, OR WILL BE, COMPLETED. A COPY OF THIS DOCUMENT CAN BE FOUND IN THE DEPARTMENT OF TRANSPORTATION CONSTRUCTION INSPECTION FORMS MANUAL DATED 10/15/06 OR CURRENT REVISION.

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ITEM 614, WORKSITE TRAFFIC SUPERVISOR (CONTINUED)

11. VERIFY THAT ALL FLAGGING OPERATIONS ARE BEING CONDUCTED PER THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

12. HAVE COPIES OF THE ODOT TEMPORARY TRAFFIC CONTROL MANUAL AND APPLICABLE STANDARDS AND SPECIFICATIONS INCLUDED IN THE CONTRACT DOCUMENTS AVAILABLE AT ALL TIMES ON THE PROJECT.

THE DEPARTMENT WILL NOT PAY THE UNIT PRICE BID FOR THE WTS FOR ANY DAY ON WHICH THE CONTRACTOR FAILS TO PERFORM THE DUTIES SET FORTH ABOVE. SHOULD THE CONTRACTOR'S FAILURE TO PERFORM ANY OF THE DUTIES DESCRIBED ABOVE RESULT IN A MAINTENANCE OF TRAFFIC SAFETY ISSUE, THE DEPARTMENT WILL DEDUCT THE PRORATED DAILY AMOUNT FOR ITEM 614 MAINTENANCE OF TRAFFIC FROM THE CONTRACTOR'S NEXT SCHEDULED ESTIMATE.

IF THREE OR MORE FAILURES TO PERFORM THE DUTIES SET FORTH ABOVE OCCUR, THE WTS SHALL BE IMMEDIATELY REMOVED FROM THE WORK IN ACCORDANCE WITH C&MS 108.05.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN INCLUDED FOR THE WORKSITE TRAFFIC SUPERVISOR:

ITEM 614 WORKSITE TRAFFIC SUPERVISOR = 4 MONTHS

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

IN ADDITION TO THE REQUIREMENTS OF CMS 614 AND THE LATEST EDITION OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD), A UNIFORMED LAW ENFORCEMENT OFFICER (AND OFFICIAL PATROL CAR WITH MOUNTED EMERGENCY FLASHING LIGHTS) SHALL BE PROVIDED FOR CONTROLLING TRAFFIC FOR THE FOLLOWING TASKS AS DIRECTED BY THE ENGINEER:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED.

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

DURING A TRAFFIC SIGNAL INSTALLATION.

LAW ENFORCEMENT OFFICERS (LEO'S) SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED. THE LEO'S ARE CONSIDERED TO BE EMPLOYED BY THE CONTRACTOR AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THEIR ACTIONS. ALTHOUGH THEY ARE EMPLOYED BY THE CONTRACTOR, THE PROJECT ENGINEER SHALL HAVE CONTROL OVER THEIR PLACEMENT. THE OFFICIAL PATROL CAR SHALL BE A PUBLIC SAFETY VEHICLE AS REQUIRED BY THE OHIO REVISED CODE. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEO'S SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR THESE SERVICES AND PROVIDE 72 HOURS ADVANCE NOTICE AS REQUIRED BY THE HIGHWAY PATROL LISTED BELOW:

STATE HIGHWAY PATROL
2255 SOUTH MAIN ST.
MANSFIELD, OH 44907
419 756-2222

LAW ENFORCEMENT OFFICERS WITH PATROL CAR REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614-LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE = 160 HOURS

THE HOURS PAID SHALL INCLUDE MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

IF THE CONTRACTOR WISHES TO UTILIZE LEO'S FOR FLAGGING AND TRAFFIC CONTROL OTHER THAN FOR THAT REQUIRED IN THESE PLANS, THEY MAY DO SO AT THEIR OWN EXPENSE.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN, ON SITE, FOR THE DURATION OF THE PROJECT. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS MAINTAINED BY THE DIRECTOR (OFFICE OF MATERIALS MANAGEMENT). THE APPROVED LIST OF PORTABLE CHANGEABLE MESSAGE SIGNS CAN BE FOUND ON THE ODOT WEBSITE BY CLICKING ON THE SERVICES MENU, THEN CLICKING ON MATERIALS MANAGEMENT. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 650 FT. AND 475 FT., RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. PCMS TRAILERS SHALL BE DELINEATED ON A PERMANENT BASIS BY AFFIXING CONSPICUITY TAPE CONFORMING TO CMS 614.03, IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER AS SEEN BY ONCOMING ROAD USERS.

THE PROBABLE PCMS LOCATIONS WILL BE DETERMINED BY THE ENGINEER PRIOR TO BEGINNING WORK ON THIS PROJECT. PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED, FACING AWAY FROM ALL TRAFFIC, AND SHALL DISPLAY ONE OR MORE YELLOW RETROREFLECTIVE SHEETING SURFACES OF 9-INCH BY 15-

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

(THE CONTRACTOR SHALL IMPLEMENT A SYSTEM WHEREBY CHANGEABLE MESSAGES WILL BE IMPLEMENTED WITHIN 4 HOURS FOLLOWING TELEPHONE NOTIFICATION FROM THE PROJECT ENGINEER TO A DESIGNATED PHONE.)

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PREPROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PREPROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR

(THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL (IN ACTIVE CELLULAR PHONE AREAS) ALLOW REMOTE SIGN ACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS AND REVISIONS TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES. ONE REMOTE DATA INPUT DEVICE (LAPTOP COMPUTER PLUS MODEM OR EQUIVALENT) SHALL BE FURNISHED FOR USE BY THE DISTRICT TRAFFIC ENGINEER, OR EQUIVALENT, AND SHALL BE INSURED

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF CMS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN (CON'T)

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK. THE CONTRACTOR SHALL ONLY BE PAID FOR PCMS UNITS WHEN THEY ARE IN OPERATION ON THE PROJECT AS SPECIFIED IN THE PLANS OR BY THE ENGINEER.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN = 5 SN MNTHS

ITEM 614 - MAINTAINING TRAFFIC LANE CLOSURE/REDUCTION REQUIRED

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

USR 30 LANE CLOSURES SHALL ONLY BE PERMITTED FROM 7 PM - 6 AM. IF A LARGE BACK-UP OCCURS THE CONTRACTOR WILL BE REQUIRED TO OPEN THE ROAD TO ALL LANES AS SOON AS FEASIBLY POSSIBLE PER THE PROJECT ENGINEERS REQUEST.

RAMP WORK SHALL BE DONE ONLY AT NIGHTTIME FROM 10 PM TO 6 AM. RAMP CLOSURES ARE NOT PERMITTED EXCEPT FOR RAMP E, US 30 EAST TO I71 NORTH RAMP. NO HOUR RESTRICTIONS FOR WORK ON RAMP E. SEE DETOUR PLAN FOR DETAILS.

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GENERAL NOTES

RIC-30-9.13 RM

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DETOUR SIGNING

THE FOLLOWING QUANTITY IS INCLUDED FOR THE CONTRACTOR TO PROVIDE THE DETOUR SIGNING AS SHOWN AS PER 614.06 (B):

ITEM 614 - DETOUR SIGNING, AS PER PLAN LUMP

MAINTAINING TRAFFIC

ACCESS TO I-71 NORTH FROM U.S. 30 EAST CAN BE DETOURED TO KOOGLE RD AND THE RAMP CAN BE CLOSED FOR DRAINAGE AND PAVEMENT WORK. HOUR RESTRICTIONS DO NOT APPLY TO THIS RAMP WHILE IT IS CLOSED.

THE CONTRACTOR SHALL NOTIFY THE ROADWAY SERVICES MANAGER, IN WRITING, A MINIMUM OF 21 DAYS IN ADVANCE OF THE DETOUR BEING PLACED.

THE CONTRACTOR SHALL NOTIFY THE DISTRICT WORK ZONE TRAFFIC MANAGER, IN WRITING, A MINIMUM OF 21 DAYS IN ADVANCE OF THE DETOUR BEING PLACED.

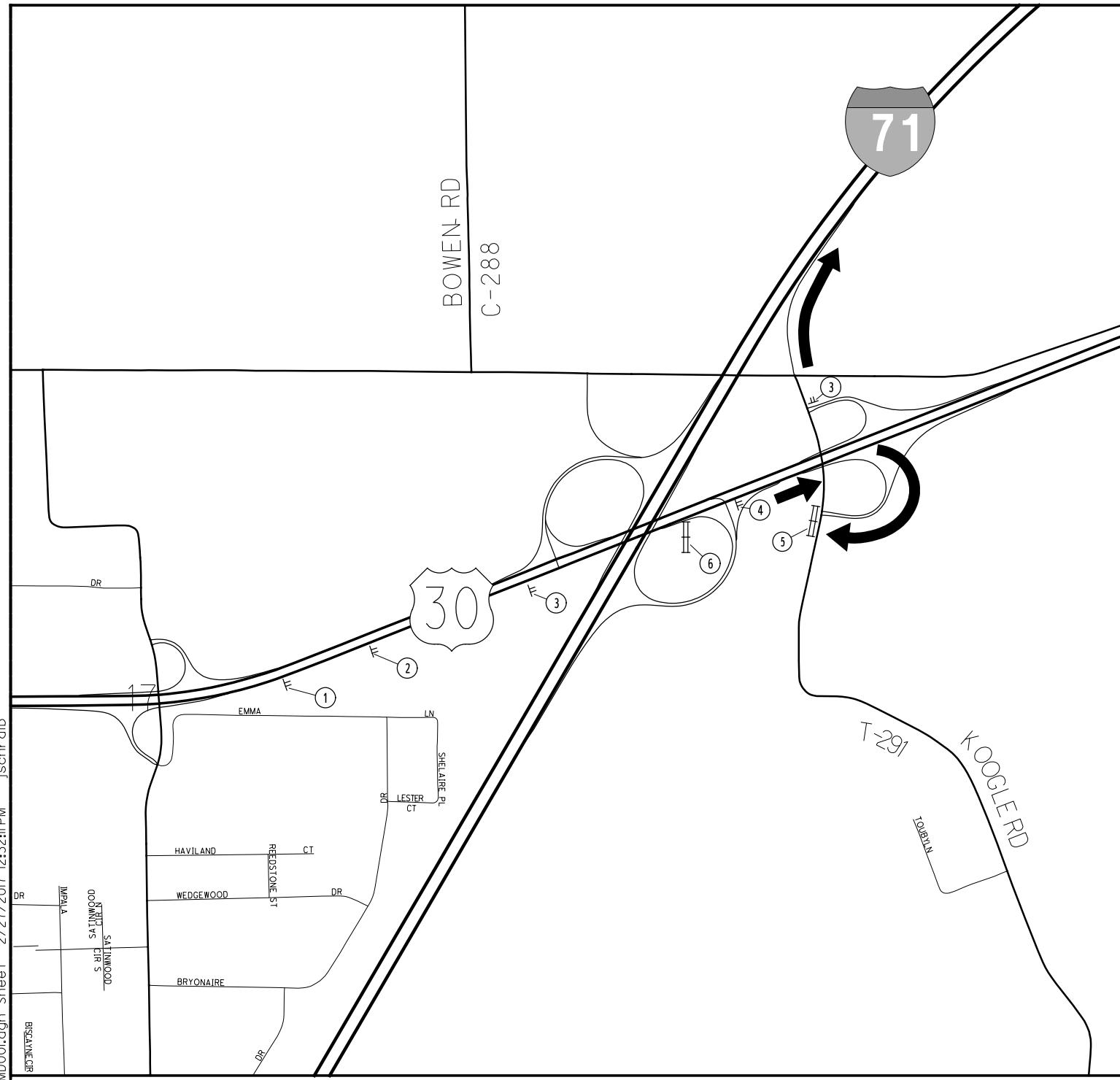
THE CONTRACTOR SHALL ALSO NOTIFY, IN WRITING, THE FOLLOWING AGENCIES AT LEAST 21 DAYS PRIOR TO THE TIME WHEN THE DETOUR WILL BE IMPLEMENTED:

- RICHLAND COUNTY ENGINEER
- LOCAL FIRE DEPARTMENT(S)
- LOCAL SCHOOL DISTRICT(S)
- RICHLAND COUNTY SHERIFF

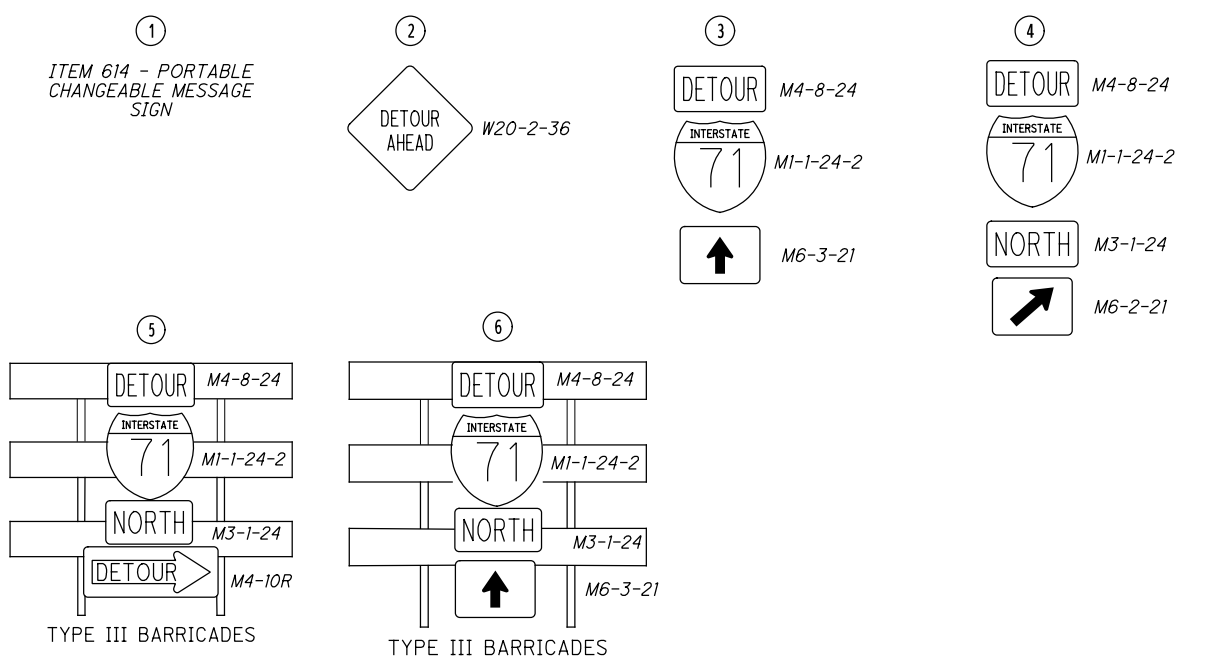
THE CONTRACTOR SHALL INSTALL THE PORTABLE CHANGEABLE MESSAGE SIGN A MINIMUM OF 7 DAYS IN ADVANCE OF THE DETOUR BEING PLACED TO NOTIFY MOTORISTS OF THE UPCOMING CLOSURE.

THE CONTRACTOR SHALL PROVIDE, ERECT, MAINTAIN AND SUBSEQUENTLY REMOVE THE DETOUR SIGNING AS DETAILED ON THIS SHEET.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATION, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.



SIGN LEGEND



- OFFICIAL SIGNED DETOUR
- TYPE III BARRICADES,
- DETOUR SIGNS

**USR 30 EB TO I71 NB (RAMP E)
DETOUR MAP**

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SHEET NUM.						PART.				ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
9	10	11	13	14	15	01/NHS/PV	02/IMS/PV	03/NHS/BR							
													ROADWAY		
					104		104		202	98300	104	SY	REMOVAL MISC.: (MEDIAN)	7	
													DRAINAGE		
					850		850		605	06020	850	FT	4" BASE PIPE UNDERDRAINS WITH GEOTEXTILE FABRIC	7	
													PAVEMENT		
			106			60	46		251	01031	106	CY	PARTIAL DEPTH PAVEMENT REPAIR (442), AS PER PLAN	7	
			1,180			1,180			252	01001	1,180	SY	FULL DEPTH RIGID PAVEMENT REMOVAL AND FLEXIBLE REPLACEMENT, AS PER PLAN	7	
			7,080			7,080			252	01500	7,080	FT	FULL DEPTH PAVEMENT SAWING		
			100,556			72,698	27,858		254	01000	100,556	SY	PAVEMENT PLANING, ASPHALT CONCRETE (1.50")		
			100				100		255	10011	100	SY	FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, CLASS OC1, AS PER PLAN	7	
			805				805		255	20001	805	FT	FULL DEPTH PAVEMENT SAWING, AS PER PLAN	7	
			9,050			6,543	2,507		407	20000	9,050	GAL	NON-TRACKING TACK COAT		
			4,190			3,029	1,161		442	10000	4,190	CY	ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (446)		
				10			10		511	53012	10	CY	CLASS OC2 CONCRETE, MISC.: (MEDIAN)	7	
			144				144		618	40101	144	FT	RUMBLE STRIPS, (ASPHALT CONCRETE), AS PER PLAN	8	
													TRAFFIC CONTROL		
				386			210	176	621	00100	386	EACH	RPM		
				386			210	176	621	54000	386	EACH	RAISED PAVEMENT MARKER REMOVED		
				18.71			15.48	3.23	644	00104	18.71	MILE	EDGE LINE, 6"		
				7.74			7.74		644	00204	7.74	MILE	LANE LINE, 6"		
				370				370	644	00400	370	FT	CHANNELIZING LINE, 8"		
				53			53		644	00500	53	FT	STOP LINE		
				120			120		644	00720	120	FT	CHEVRON MARKING		
													STRUCTURE REPAIR (RIC-30-11.59 L&R)		
								1,088	512	73500	1,088	SY	TREATING CONCRETE BRIDGE DECKS WITH GRAVITY FED RESIN		
													MAINTENANCE OF TRAFFIC		
	160						90	70	614	11110	160	HOUR	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE		
	4						4		614	11500	4	MNTH	WORKSITE TRAFFIC SUPERVISOR		
14							14		614	12410	14	EACH	SPEED ZONE AHEAD SYMBOL SIGN		
		LS						LS	614	12421	LS		DETOUR SIGNING, AS PER PLAN	11	
30							30		614	12470	30	EACH	WORK ZONE SPEED LIMIT SIGN		
6							6		614	12484	6	EACH	WORK ZONE INCREASED PENALTIES SIGN		
	5						2	3	614	18601	5	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN	10	
12							12		614	18700	12	SNMT	DIGITAL SPEED LIMIT (DSL) SIGN ASSEMBLY		
				4.77			4.77		614	20550	4.77	MILE	WORK ZONE LANE LINE, CLASS III, 642 PAINT		
				12.77			9.53	3.24	614	22350	12.77	MILE	WORK ZONE EDGE LINE, CLASS III, 642 PAINT		
				53			53		614	26610	53	FT	WORK ZONE STOP LINE, CLASS III, 642 PAINT		
													INCIDENTALS		
						LS	LS	LS	614	11000	LS		MAINTAINING TRAFFIC		
						1	1		619	16010	2	MNTH	FIELD OFFICE, TYPE B		
						LS	LS	LS	623	10000	LS		CONSTRUCTION LAYOUT STAKES AND SURVEYING		
						LS	LS	LS	624	10000	LS		MOBILIZATION		

GENERAL SUMMARY

RIC-30-9.13 RM

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FUNDING SPLIT	COUNTY	ROUTE	STRAIGHT LINE MILEAGE		LENGTH	AVERAGE WIDTH	PAVED SHOULDER WIDTH		*TYPICAL	PAVEMENT AREA	251	252	252	254	255	255	407	442	618	
			MILE	FT			FT	FT			FT	SO YD	CY	SY	FT	SY	SY	FT	GAL	CY
BEGIN EASTBOUND PAVEMENT QUANTITIES																				
01/NHS/PV	RIC	30	9.13	10.00	0.87	4,594	24.0			12,251		5	145	872						
01/NHS/PV	RIC	30	10.00	11.00	1.00	5,280	24.0			14,080		9	193	1,160						
01/NHS/PV	RIC	30	11.00	12.00	1.00	5,280	24.0			14,080		8	157	944						
01/NHS/PV	RIC	30	12.00	13.00	1.00	5,280	24.0			14,080		7	63	376						
01/NHS/PV	RIC	30	9.13	10.77	1.64	8,659	26.0			25,015							2,251	1,042		
SUSPEND/RESUME AT CONCRETE STRUCTURE OVER SR 39																				
01/NHS/PV	RIC	30	10.81	11.38	0.57	2,983	26.0			8,618							776	359		
SUSPEND/RESUME AT CONCRETE STRUCTURE OVER BOWMAN ST																				
01/NHS/PV	RIC	30	11.41	11.59	0.18	940	26.0			2,716							244	113		
SUSPEND RESURFACING AT CONCRETE STRUCTURE OVER RAILROAD																				
BEGIN WESTBOUND PAVEMENT QUANTITIES																				
01/NHS/PV	RIC	30	9.13	10.00	0.87	4,594	24.0			12,251		4	116	696						
01/NHS/PV	RIC	30	10.00	11.00	1.00	5,280	24.0			14,080		11	212	1,272						
01/NHS/PV	RIC	30	11.00	12.00	1.00	5,280	24.0			14,080		9	205	1,232						
01/NHS/PV	RIC	30	12.00	13.00	1.00	5,280	24.0			14,080		7	88	528						
01/NHS/PV	RIC	30	9.13	10.77	1.64	8,659	26.0			25,015							2,251	1,042		
SUSPEND/RESUME AT CONCRETE STRUCTURE OVER SR 39																				
01/NHS/PV	RIC	30	10.81	11.38	0.57	2,983	26.0			8,618							776	359		
SUSPEND/RESUME AT CONCRETE STRUCTURE OVER BOWMAN ST																				
01/NHS/PV	RIC	30	11.41	11.59	0.18	940	26.0			2,716							244	113		
SUSPEND RESURFACING AT CONCRETE STRUCTURE OVER RAILROAD																				
BEGIN 71/30 INTERCHANGE RAMP QUANTITIES																				
02/IMS/PV	(RAMP A) - 71 SB EXIT RAMP TO CRIDER RD						27.0			8	2,217				2,217	8	64	200	92	
02/IMS/PV	(RAMP B) - 71 SB EXIT RAMP TO SR 30 WB						27.0			7	4,360				4,360	16	126	392	182	
02/IMS/PV	(RAMP AB) - 71 SB EXIT RAMP						27.0			8	2,684				2,684	10	78	242	112	48
02/IMS/PV	(RAMP C) - 71 SB ENTRANCE RAMP FROM SR 30 WB						27.0			8	834				834	3	24	75	35	
02/IMS/PV	(RAMP D) - 71 SB ENTRANCE RAMP FROM SR 30 EB						27.0			8	707		46		707	3	20	64	29	
02/IMS/PV	(RAMP CD) - 71 SB ENTRANCE RAMP FROM SR 30						27.0			7	4,528				4,528	16	131	408	189	48
02/IMS/PV	(RAMP E) - 71 NB ENTRANCE RAMP FROM SR 30 EB						27.0			7	5,302				5,302	19	153	477	221	48
02/IMS/PV	(RAMP F) - 71 NB EXIT RAMP TO SR 30 WB						27.0			8	616				616	2	18	55	26	
02/IMS/PV	(RAMP G) - 71 NB EXIT RAMP TO SR 30 EB						27.0			8	846				846	3	24	76	35	
02/IMS/PV	(RAMP FG) - 71 NB EXIT RAMP TO SR 30						27.0			7	5,764				5,764	21	167	519	240	
SUB-TOTAL FOR PLAN SPLIT 01/NHS/PV											60	1,180	7,080	72,698			6,543	3,029		
SUB-TOTAL FOR PLAN SPLIT 02/IMS/PV											46			27,858	100	805	2,507	1,161	144	
TOTAL CARRIED TO THE GENERAL SUMMARY											106	1,180	7,080	100,556	100	805	9,050	4,190	144	

CALCULATED JWS
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PAVEMENT & SHOULDER DATA
RIC-30-9.13 RM
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AUXILIARY & LONG LINE MARKINGS

FUNDING SPLIT	COUNTY	ROUTE	SLM / RAMP		HIGHWAY MILES	DESCRIPTION	614						644									
			FROM	TO			MILE	WORK ZONE LANE LINE, CLASS III, 642 PAINT	WORK ZONE EDGE LINE, CLASS III, 642 PAINT	WORK ZONE STOP LINE, CLASS III, 642 PAINT			EDGE LINE, 6" (WHITE)	EDGE LINE, 6" (YELLOW)	LANE LINE, 6"	AUXILIARY MARKINGS (740.04)						
																8"	24"	24"				
						MILE	MILE	FT							FT	FT						
01/NHS/PV	RIC	30	9.13	13.00	3.87	EASTBOUND LONG LINES MARKINGS	2.38	4.77														
01/NHS/PV	RIC	30	9.13	13.00	3.87	WESTBOUND LONG LINES MARKINGS	2.38	4.77														
02/IMS/PV	RIC	71/30	A, B, AB			71 (SB) EXIT RAMP AT SR 30		1.03	26						250	26	72					
02/IMS/PV	RIC	71/30	C, D, CD			71 (SB) ENTRANCE RAMP AT SR 30		0.71														
02/IMS/PV	RIC	71/30	E			71 (NB) ENTRANCE RAMP AT SR 30		0.66														
02/IMS/PV	RIC	71/30	F, G, FG			71 (NB) EXIT RAMP AT SR 30		0.84	27						120	27	48					
SUB-TOTAL FOR PLAN SPLIT 01/NHS/PV							4.77	9.53														
SUB-TOTAL FOR PLAN SPLIT 02/IMS/PV								3.24	53						370	53	120					
TOTALS TO GENERAL SUMMARY							4.77	12.77	53						370	53	120					

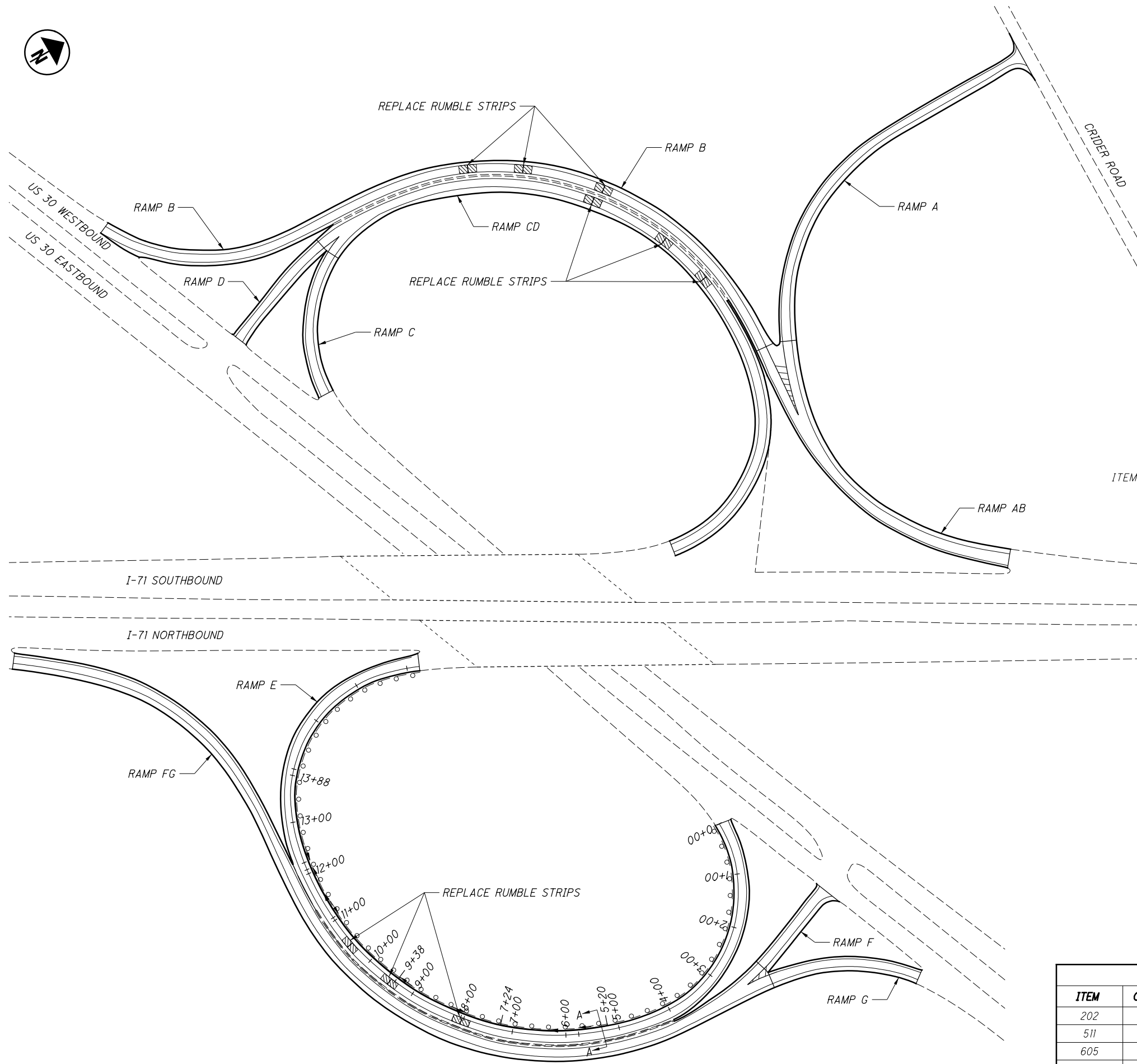
RAISED PAVEMENT MARKERS

FUNDING SPLIT	COUNTY	ROUTE	SLM / RAMP		DETAIL	621					REMARKS	DETAIL	DESCRIPTION	
			FROM	TO		RAISED PAVEMENT MARKER REMOVED	RPM	PRISMATIC RETRO-REFLECTOR TYPES						
								ONE-WAY	TWO-WAY					
						WHITE	YELLOW / YELLOW	WHITE / RED	YELLOW / RED	BLUE / BLUE				
						EACH	EACH	EACH						
01/NHS/PV	RIC	30	9.13	11.59		105	105	105					EASTBOUND CONTINUOUS ROUTE TREATMENT	
01/NHS/PV	RIC	30	9.13	11.59		105	105	105					WESTBOUND CONTINUOUS ROUTE TREATMENT	
02/IMS/PV	RIC	71/30	RAMPS			176	176		66	110			INTERCHANGE @ US 30	
01/NHS/PV SUB-TOTAL						210	210	210						
02/IMS/PV SUB-TOTAL						176	176		66	110				
TOTALS TO GENERAL SUMMARY						386	386							

PAVEMENT MARKING / RPM SUB-SUMMARY
 CALCULATED JWS
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RIC-30-9.13 RM
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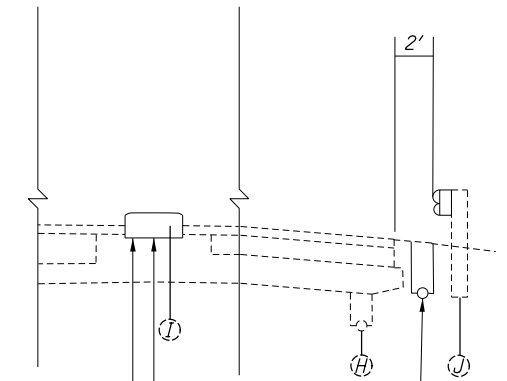
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NOTES:
 1) STATIONING ON RAMP E BEGINS AT THE GORE OF USR 30 EASTBOUND AND CONTINUES ALONG THE RIGHT EDGE LINE OF THE ENTRANCE RAMP TO I-71 NORTH.

SECTION A



ITEM 511 - CLASS OC2 CONCRETE, MISC.

ITEM 202 - REMOVAL, MISC.

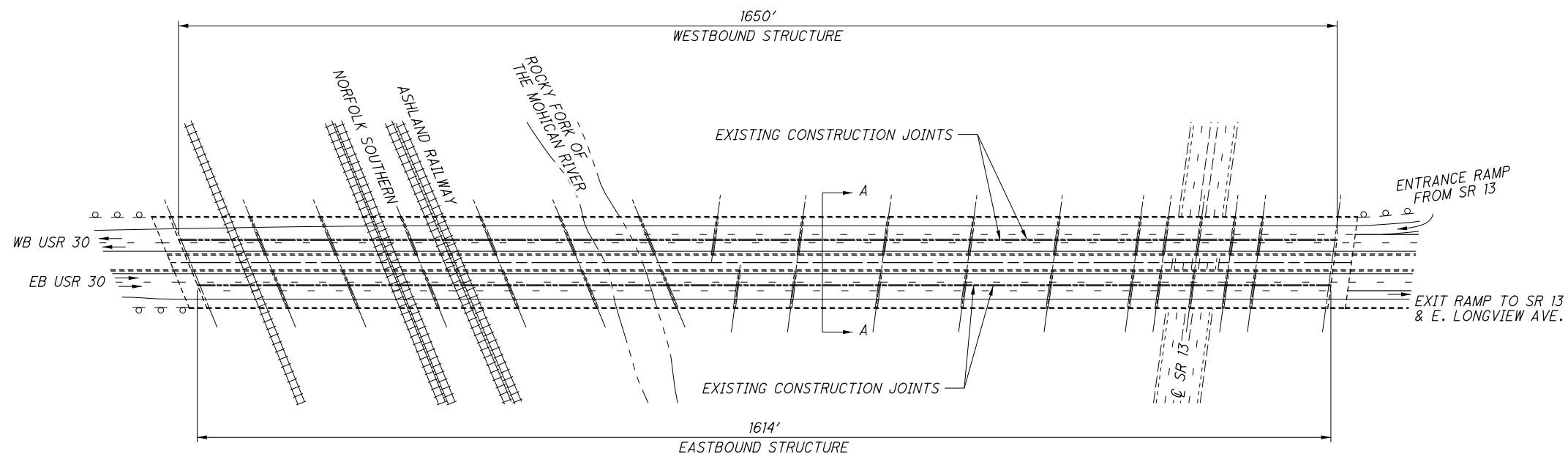
ITEM 605 - 4" BASE UNDERDRAIN WITH GEOTEXTILE FABRIC

99487 - (02/IMS/PV)			
ITEM	QUANTITY	UNIT	DESCRIPTION
202	104	SY	REMOVAL MISC.: (MEDIAN)
511	10	CY	CLASS OC2 CONCRETE, MISC.: (MEDIAN)
605	850	FT	4" BASE PIPE UNDERDRAIN W/ GEOTEXTILE FABRIC

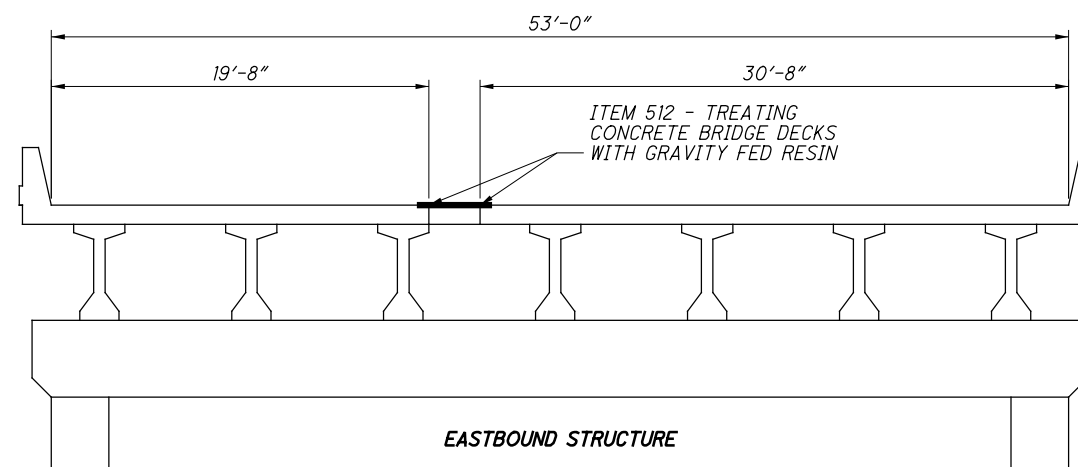
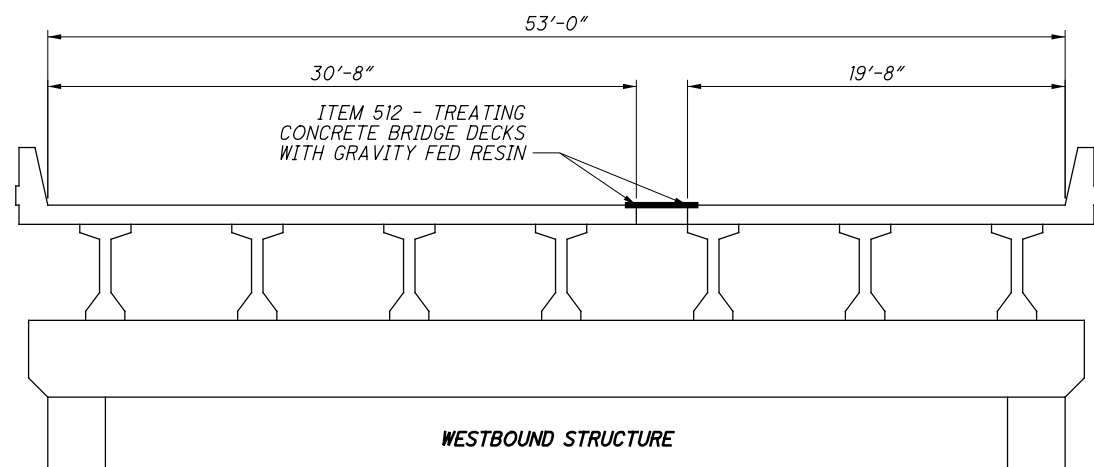
**RIC-71/30 INTERCHANGE
 RAMP DETAILS**

RIC-30-9.13 RM

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PLAN VIEW



SECTION VIEW A-A

SCALE 5:1

NOTES:

1. SEAL CENTER JOINT ON DECK WITH GRAVITY FED RESIN
2. SINGLE LANE CLOSURE WITH DRUMS
3. FOLLOW PLCM FOR PERMITTED LANE CLOSURE TIMES
4. CONTACT DISTRICT WORK ZONE TRAFFIC MANAGER PRIOR TO DESIGN TO DISCUSS PLCM
5. CONTRACTOR TO REMOVE ZONE WHEN NOT WORKING

RIC-30-11.58 (L&R) - (03/NHS/BR)			
ITEM	QUANTITY	UNIT	DESCRIPTION
512	1088	SY	TREATING CONCRETE BRIDGE DECKS WITH GRAVITY FED RESIN