

**WORK LIMITS**

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

**PROFILE AND ALIGNMENT**

PLACE THE PROPOSED PAVEMENT TO FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT. PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY AS SHOWN ON THE TYPICAL SECTIONS.

**UTILITIES**

THE CONTRACTOR SHALL USE THE FOLLOWING PROCEDURE AT EACH LOCATION WHERE WORK IS PERFORMED, IN ACCORDANCE WITH SECTIONS 105.07 AND 107.16 IN THE CONSTRUCTION AND MATERIALS SPECIFICATIONS.

THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER, OHIO811, THE OHIO DEPARTMENT OF TRANSPORTATION DISTRICT 4 HEADQUARTERS (MICHELLE CHANEY AT 330-786-2267) AND ALL NON REGISTERED UTILITY OWNERS AT LEAST TWO (2) WORKING DAYS PRIOR TO COMMENCING CONSTRUCTION OPERATIONS IN ALL AREAS.

THE LOCATION OF EXISTING UNDERGROUND UTILITIES ARE NOT SHOWN ON THE PLANS, BUT CAN BE OBTAINED FROM THE OWNERS OF THE UTILITIES. THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE TO UTILITIES.

AT&T  
ATTN: Chris Emrich  
50 W. Bowersy St.  
Akron, OH 44308  
330-384-3556  
330-319-5239 Cell  
CE3141@att.com  
  
Dominion Energy  
Kyle J. McCall  
Supervisor Gas Operations  
Distribution Design  
320 Springside Drive  
Akron, OH 44333  
330-801-8299 Cell  
Kyle.J.Mccall@dominionenergy.com

City of Akron Utility  
Coordinator  
Joseph Kunzler, PE  
AWR Program/AEB Utilities  
166 South High Street  
Akron, OH 44308  
330-375-2217  
330-690-0133 (cell)  
joseph.kunzler@akronohio.gov  
  
Charter  
ATTN: Jim Long  
1200 Brownstone Ave.  
Akron, OH 44310  
330-622-4106  
James.Long@charter.com

City of Cuyahoga Falls  
Water Department  
ATTN: Ruff Kring  
2560 Bailey Road  
Cuyahoga Falls, OH 44221  
330-971-8133  
kringrw@cityofcf.com

City of Cuyahoga Falls  
Electric Department  
ATTN: Ron Boorman  
2550 Bailey Road  
Cuyahoga Falls, OH 44221  
330-971-8049  
boormanrj@cityofcf.com

City of Cuyahoga Falls  
Technical Department  
ATTN: Carl Buser  
2511 Bailey Road  
Cuyahoga Falls, OH 44221  
330-971-8071  
busercg@cityofcf.com

Brightspeed/Lumen/CenturyLink  
ATTN: Alan Peters  
3801 Elm Road  
Warren, OH 44502  
330-841-1408  
330-219-3306 Cell  
Alan.I.peters@brightspeed.com

Verizon Business / MCI  
ATTN: Daniel Arz  
12300 Ridge Road  
North Royalton, OH 44133  
120 Ravine Street  
Akron, OH 44303  
440-457-4832  
216-570-9343 Cell  
Daniel.arz@verizon.com

Windstream Kinetic  
ATTN: Brian Vinson  
245 N. Main St.  
Hudson, OH 44236  
330-422-9128  
Brian.Vinson@windstream.com

Ohio Edison  
ATTN: David Miller  
1910 W. Market Street  
Building #1  
Akron, OH 44313  
330-436-4055  
330-715-4340 Cell  
millerdl@firstenergycorp.com

**ITEM 203 - EXCAVATION (FOR PAVEMENT REPAIR)**

THIS ITEM OF WORK SHALL CONSIST OF REMOVING AND DISPOSING OF ALL UNSUITABLE MATERIAL BY EXCAVATING THE EXISTING SUBGRADE AND SUBBASE TO AN AVERAGE DEPTH OF 6 INCHES OR AS DIRECTED BY THE ENGINEER. EXACT LIMITS OF REMOVAL SHALL BE DETERMINED BY THE ENGINEER. ALL EQUIPMENT, LABOR, TOOLS, AND INCIDENTALS NECESSARY TO COMPLETE THIS ITEM SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 203 EXCAVATION (FOR PAVEMENT REPAIR). THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:  
203, EXCAVATION (FOR PAVEMENT REPAIR) 84 CU YD

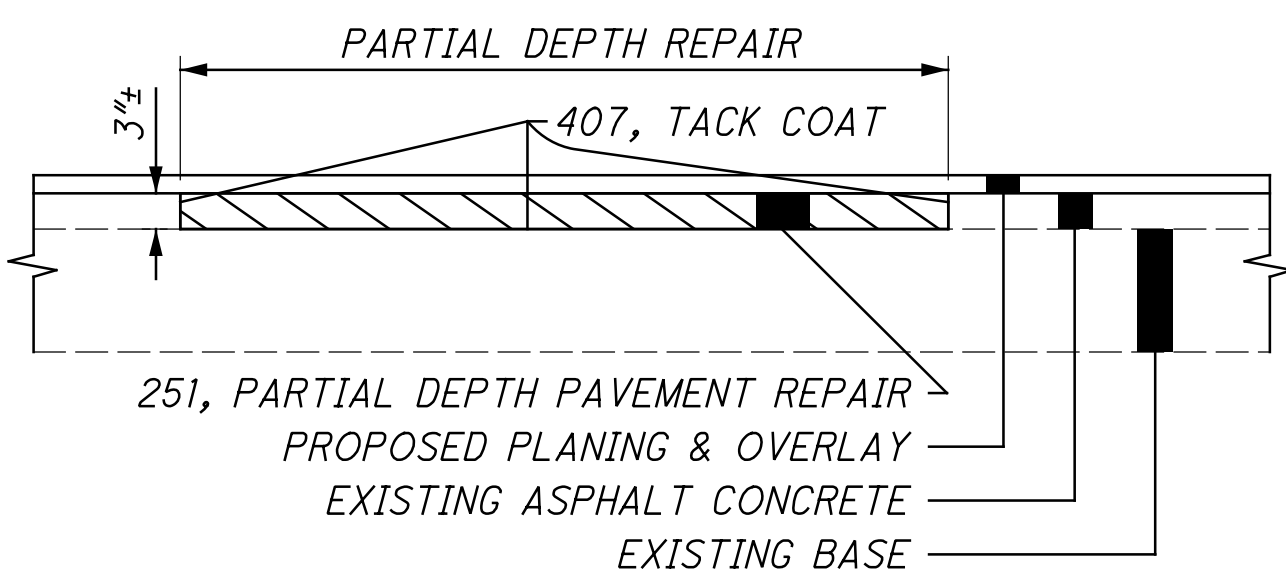
**ITEM 304 - AGGREGATE BASE (FOR PAVEMENT REPAIR)**

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN PROVIDED AND SHALL BE USED AS DIRECTED BY THE ENGINEER TO BACKFILL AREAS WHICH WERE EXCAVATED UNDER ITEM 203 EXCAVATION (FOR PAVEMENT REPAIR). THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:  
304, AGGREGATE BASE (FOR PAVEMENT REPAIR) 84 CU YD

**ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (441)**

A QUANTITY OF THIS ITEM SHALL BE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER. THE ITEM SHALL CONSIST OF REPAIRING EXISTING LOCATIONS EXHIBITING SURFACE DETERIORATION AND PLACING ITEM 441 ASPHALT CONCRETE, TYPE 2. IT IS NOT THE INTENT TO REPAIR EVERY DETERIORATED AREA WITHIN THE PROJECT. PAVEMENT REPAIRS WILL BE MARKED IN THE FIELD BY THE PROJECT ENGINEER ACCORDING TO CMS 251.02. MINIMUM WIDTH IS 2'. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THIS ITEM SHALL BE PERFORMED AFTER THE COMPLETION OF MAINLINE PAVEMENT PLANING AND PRIOR TO THE PLACEMENT OF ASPHALT ON THE MILLED SURFACE. PAYMENT SHALL BE BASED ON THE ACTUAL NUMBER OF SQUARE YARDS OF PAVEMENT REPAIR.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:  
251, PARTIAL DEPTH PAVEMENT REPAIR (441) (LONGITUDINAL), 2300 SY  
251, PARTIAL DEPTH PAVEMENT REPAIR (441) (TRANSVERSE), 700 SY



**ITEM 255 - FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, TYPE 1, CLASS RRCM**

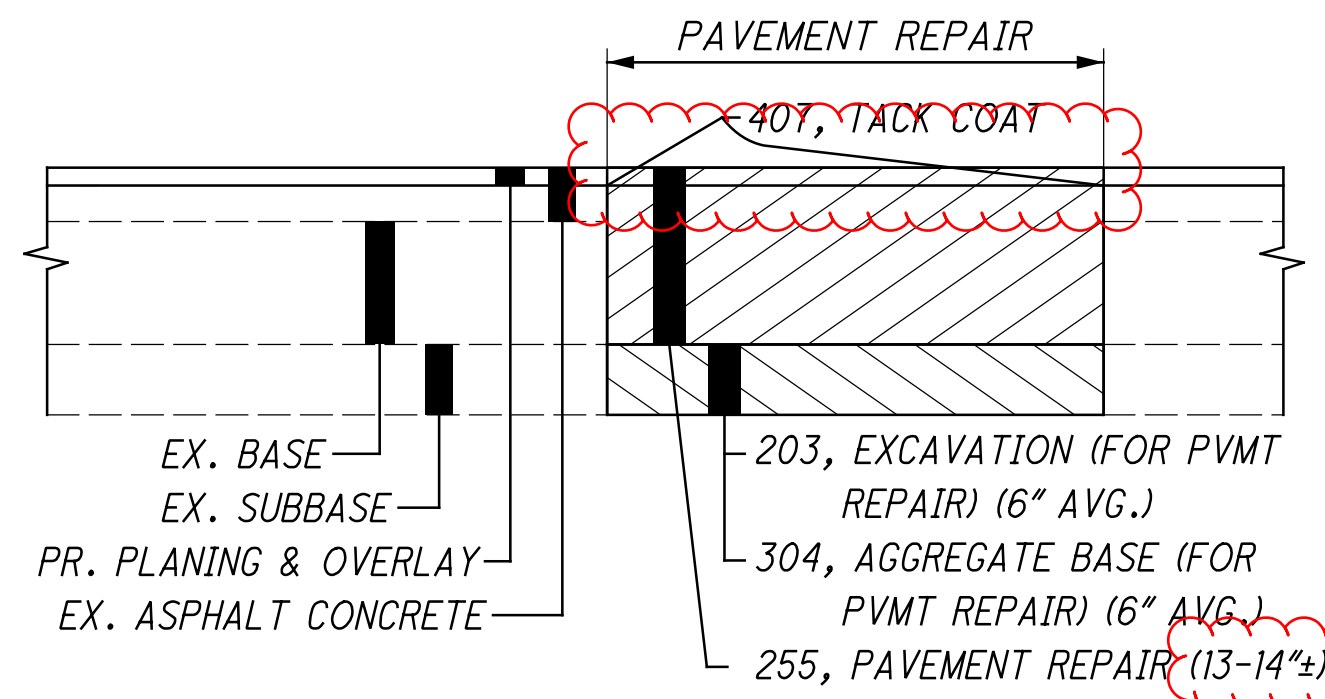
A QUANTITY OF THIS ITEM SHALL BE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER. THIS ITEM SHALL CONSIST OF CUTTING AND REMOVING DETERIORATED PAVEMENT FULL DEPTH AND PLACING 13-14" ± RRCM CONCRETE.

UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THE CONTRACTOR MAY PERFORM THIS WORK PRIOR TO THE MAINLINE PAVEMENT PLANING OPERATION.

IT IS NOT THE INTENT TO REPAIR EVERY DETERIORATED AREA WITHIN THE PROJECT. THE ENGINEER SHALL DETERMINE WHICH AREAS ARE TO BE REPAIRED. PAYMENT SHALL BE BASED ON THE ACTUAL NUMBER OF SQUARE YARDS OF PAVEMENT REMOVED AND REPLACED TO THE LIMITS DESIGNATED BY THE ENGINEER.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

- 252, FULL DEPTH SAW CUTTING, 2275 FT
- 255, FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, TYPE 1, CLASS RRCM, 500 SQ YD



**ITEM 442 - ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (447), PWL, 2025, AS PER PLAN, PG70-22M**

ALL REQUIREMENTS OF C&MS ITEM 442 APPLY EXCEPT AS SHOWN.

MAT DENSITY ACCEPTANCE - FOLLOW THE REQUIREMENTS OF 447 MAT DENSITY ACCEPTANCE, EXCEPT AS MODIFIED BELOW.

OBTAIN 6-INCH DIAMETER CORES FOR EACH LOT.

THE PWL CALCULATOR, LOCATED ON THE ODOT WEBSITE AT THE OFFICE OF CONSTRUCTION ADMINISTRATION, WILL BE USED TO DETERMINE THE LOT PWL AND THE LOT AASHTO PAY FACTORS.

THE DEPARTMENT WILL DETERMINE THE PAY FACTOR FOR EACH LOT CORED BY THE FOLLOWING TABLE.

LOWER SPECIFICATION LIMIT	PAY FACTOR CRITERIA	PAY FACTOR (PF)
92.60%	If AVE density is >= 93% and PWL >= 90	PF =1 or AASHTO PF whichever is greater
	If 90 > PWL > 50	AASHTO PF
	If PWL <= 50	REMOVE AND REPLACE

**PAVEMENT MARKING LANE WIDTHS**

THE NORMAL LANE WIDTH FOR THE PAVEMENT MARKINGS ON THIS PROJECT SHALL BE AS FOLLOWS:

ROUTE	S.L.M. TO S.L.M.	LANE WIDTH
SR 8 NB	2.91 TO 7.68	12'
SR 8 SB	2.99 TO 7.53	12'

**ITEM 408 - PRIME COAT, AS PER PLAN**

APPLY "MC-70" AT A RATE OF 0.4 GALLONS PER SQUARE YARD, OR AS DETERMINED BY THE ENGINEER, TO THE COMPLETED COMPACTED AGGREGATE SHOULDER.

**ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN**

IN LOW SHOULDER AREAS EXCEEDING 1", AND ADJACENT TO THE SAFETY EDGE, OR AS DIRECTED BY THE ENGINEER, RECYCLED ASPHALT PAVEMENT (RAP) SHALL BE USED IN AREAS ADJACENT TO THE PAVED BERM. THE RAP SHALL HAVE A MINIMUM PG CONTENT OF 4.5% AND MEET THE FOLLOWING GRADATION. ONCE THE STOCKPILE MEETS THE GRADATION, THE PG CONTENT OF THE RAP SHALL BE DETERMINED PER 441.03. THE RAP ANALYSIS MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL 2 WEEKS PRIOR TO USE. METHOD OF MEASUREMENT SHALL BE AS PER 617.06. PLACEMENT AND COMPACTION SHALL MEET THE REQUIREMENTS OF ITEM 617. ALL MATERIALS, LABOR, EQUIPMENT, TOOLS AND INCIDENTALS NECESSARY TO COMPLETE THE WORK SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 617 COMPACTED AGGREGATE, AS PER PLAN.

MODIFIED GRADATION SHALL APPLY:

SIEVE	TOTAL PERCENT PASSING
1- 1/2"	100
3/4"	50-100
NO. 4	35-70
NO. 30	9-33
NO. 200	0-13

**LINEAR GRADING**

AREAS WHERE THE SHOULDER IS HIGHER THAN THE EDGE OF PAVEMENT WILL BE GRADED TO PROVIDE POSITIVE DRAINAGE. THIS WORK WILL ONLY BE PERFORMED IN AREAS NECESSARY AND WILL NOT BE PERFORMED ON THE ENTIRE PROJECT. AREAS FOR THE WORK WILL BE MARKED BY THE PROJECT ENGINEER. UNDER NO CIRCUMSTANCES WILL THIS WORK BE PERFORMED CONCURRENTLY WITH ANY OTHER OPERATION.

GRADING WILL BE ACCOMPLISHED BY THE REMOVAL OF MATERIAL TO PROVIDE A 0.08 POSITIVE SLOPE. THE GRADED AREAS WILL BE COMPACTED TO A SUFFICIENT DENSITY TO PREVENT EROSION UNTIL SEEDING AND MULCHING IS PERFORMED. ALL EXCESS MATERIAL WILL BE REMOVED FROM THE BERMS AND WILL BE DISPOSED OF OFF THE PROJECT BY THE CONTRACTOR.

THE CONTRACTOR IS REQUIRED TO PLACE ITEM 617 WITHIN A PERIOD NOT TO EXCEED 7 DAYS. REFER TO THE AS PER PLAN NOTE FOR REQUIREMENTS.

EXPOSED EARTH OUTSIDE OF THE LIMITS OF ITEM 617 ARE REQUIRED TO BE SEEDED AND MULCHED WITHIN 7 DAYS OF PLACEMENT OF ITEM 617. PAYMENT FOR THIS WORK SHALL BE MADE UNDER ITEM 832.

THE QUANTITY OF ITEM 209 IS NOT PERMITTED TO BE INCREASED. REDUCTIONS IN QUANTITIES ARE PERMITTED AS DETERMINED BY THE PROJECT ENGINEER.

ALL MATERIALS, LABOR, EQUIPMENT, TOOLS, AND INCIDENTALS NECESSARY TO COMPLETE THIS WORK WILL BE INCLUDED IN THE UNIT PRICE FOR THE PERTINENT BID ITEM. THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:  
209, LINEAR GRADING, 246 STA.

DESIGN AGENCY



DESIGNER  
JRF

REVIEWER  
MA 01-19-24

PROJECT ID  
100936

SHEET TOTAL  
P.4 22

**MAINTENANCE OF TRAFFIC**

THIS ITEM SHALL CONSIST OF MAINTENANCE OF TRAFFIC ON EXISTING ROADWAYS AND RAMPS IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, CURRENT EDITION, LATEST REVISION, THE SPECIFICATIONS AND THE FOLLOWING:

1. A MINIMUM OF ONE TEN FOOT LANE IN EACH DIRECTION SHALL BE MAINTAINED ON THE EXISTING PAVEMENT OR COMPLETED PAVEMENT DURING CONSTRUCTION OF THE WORK.
2. THE CONTRACTOR SHALL INFORM THE DISTRICT OFFICE (330) 786-2208, EIGHTEEN (18) DAYS PRIOR TO THE BEGINNING OF WORK.
3. LANE RESTRICTIONS OR LANE REDUCTIONS SHALL NOT BE PERMITTED AFTER NORMAL WORKING HOURS. NORMAL WORKING HOURS SHALL BE THOSE HOURS DURING WHICH THE CONTRACTOR HAS A FULL COMPLEMENT OF EMPLOYEES AND EQUIPMENT ACTIVELY REMOVING AND/OR PLACING PAVEMENT MATERIALS.
4. ALL FULL DEPTH PAVEMENT REMOVAL AND REPLACEMENT OPERATIONS SHALL BE COMPLETED THE SAME DAY THE EXCAVATION IS MADE. IF THE CONTRACTOR CANNOT COMPLETE THE WORK, THE EXCAVATION SHALL BE BACKFILLED OR PROTECTED AS PER STANDARD CONSTRUCTION DRAWING MT-101.90.
5. TRUCK MOUNTED ATTENUATORS [TMA'S] SHALL BE USED AS SHOWN IN THE STANDARD CONSTRUCTION DRAWINGS.
6. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE PERMITTED TO HAVE SUCCESSIVE WORK ZONES UNLESS THE DISTANCE BETWEEN THE DRUMS, BARRICADES OR CONES EXCEEDS TWO (2) MILES RURAL OR ONE [1] MILE URBAN.
7. FOR ROUTES NOT ON THE PERMITTED LANE CLOSURE CHART, ONLY DURING OFF-PEAK PERIODS (ie ANY PERIOD OTHER THAN 6-8AM AND 3-6PM) SHALL THE CONTRACTOR INSTALL AND SUBSEQUENTLY RESET ALL TRAFFIC CONTROL NECESSARY FOR THE WORK ZONE FOR EACH CONSTRUCTION PHASE.
8. IN ADDITION TO THE REQUIREMENTS OF 614.11 WORK ZONE PAVEMENT MARKINGS, AT THE END OF EACH DAY OF WORK, THE CONTRACTOR SHALL REPLACE (WITH WORK ZONE MARKINGS) ALL LANE, CENTER, STOP OR CHANNELIZING LINES THAT WERE REMOVED OR COVERED DURING THE PAVEMENT REMOVAL OR PLACEMENT OPERATIONS. QUANTITIES FOR SUCH PLACEMENT ARE CARRIED AS PART OF THE ITEMS LISTED UNDER 614 WORK ZONE PAVEMENT MARKINGS.
9. A QUANTITY OF 20 CU. YDS. OF ITEM 614 ASPHALT CONCRETE FOR MAINTAINING TRAFFIC SHALL BE PROVIDED FOR USE IN MAINTAINING PAVEMENT, SHOULDERS AND OTHER LOCATIONS AS DIRECTED BY THE ENGINEER.
10. PRIOR TO OPENING TO TRAFFIC EACH LANE SHALL BE IN A SAFE, PASSABLE CONDITION. ALL TRANSVERSE JOINTS SHALL EXTEND ACROSS THE FULL LANE AND SHOULDER WIDTH AND EACH LANE SHALL BE FREE FROM UNEVEN LONGITUDINAL JOINTS. THE CONTRACTOR SHALL PROVIDE ASPHALT WEDGES FOR TRANSVERSE JOINTS WHEREVER THERE ARE PAVEMENT ELEVATION DIFFERENCES.

11. THE CONTRACTOR SHALL INSTALL, MAINTAIN AND SUBSEQUENTLY REMOVE WORK ZONE MARKING SIGNS AND THEIR SUPPORTS WITHIN THE WORK LIMITS. THESE SIGNS INCLUDE "NO EDGE LINES", "DO NOT PASS" AND "PASS WITH CARE". ALL OTHER SIGNS WILL BE INCIDENTAL TO THE LUMP SUM PAY ITEM 614 MAINTAINING TRAFFIC UNLESS SEPARATELY ITEMIZED IN THE PLANS. A QUANTITY OF ITEM 614 WORK ZONE MARKING SIGNS HAS BEEN INCLUDED IN THE PLANS AS PER CMS 614.04.

12. THE CONTRACTOR SHALL SET A WORK ZONE AT THE REQUEST OF THE ENGINEER TO ALLOW THE LAYOUT OF THE PARTIAL/FULL DEPTH PAVEMENT REPAIR AREAS. THIS WORK IS INCIDENTAL TO ITEM 614 MAINTAINING TRAFFIC.

13. TO ENSURE THAT WEIGHTED CHANNELIZERS WILL NOT BE BLOWN OVER OR DISPLACED BY WIND AND MOVING TRAFFIC, ALL WEIGHTED CHANNELIZERS UTILIZED ON INTERSTATES AND FREEWAYS SHALL BE FROM MANUFACTURERS ON THE OHIO DEPARTMENT OF TRANSPORTATION, OFFICE OF MATERIAL MANAGEMENT'S QUALIFIED PRODUCTS LIST (QPL) WHICH UTILIZE A MINIMUM OF A 30 POUND BALLAST.

14. DRUMS UTILIZED ON THE HIGH SIDE OF A SUPERELEVATED INTERSTATE OR FREEWAYS SHALL BE FROM MANUFACTURERS ON THE OFFICE OF MATERIAL MANAGEMENT'S QUALIFIED PRODUCTS LIST (QPL) WITH A MINIMUM BALLAST WEIGHT OF 30 POUNDS. ALL BALLASTS USED SHALL BE IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

THE FOLLOWING QUANTITIES SHALL BE USED FOR THE MAINTENANCE OF TRAFFIC ON THIS PROJECT:

- PHASE I (MILLED SURFACE):
- 614, WORK ZONE LANE LINE, CLASS I, 6", 18.62 MILE
  - 614, WORK ZONE STOP LINE, CLASS I, 250 FT
  - 614, WORK ZONE CHANNELIZING LINE, CLASS I, 12", 10300 FT
  - 614, WORK ZONE MARKING SIGN,(ALL PHASES) 10 EACH

TO BE USED AS DIRECTED BY THE ENGINEER  
614, WORK ZONE EDGE LINE, CLASS I, 6", 24.36 MILE

- PHASE II (SURFACE COURSE):
- 614, WORK ZONE LANE LINE, CLASS III, 642 PAINT, 6", 18.62 MILE
  - 614, WORK ZONE STOP LINE, CLASS III, 642 PAINT, 250 FT
  - 614, WORK ZONE CHANNELIZING LINE, CLASS III, 642 PAINT, 12", 10300 FT

TO BE USED AS DIRECTED BY THE ENGINEER  
614, WORK ZONE EDGE LINE, CLASS III, 6", 24.36 MILE

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

**ADVANCED NOTICE TO PAVE**

THE CONTRACTOR SHALL SUBMIT FOR APPROVAL TO THE DISTRICT CONSTRUCTION ENGINEER A DETAILED SCHEDULE 15 DAYS PRIOR TO THE PLACEMENT OF THE OVERLAY COURSES, ON HOW THEY PROPOSE TO PROSECUTE THE PAVING OPERATIONS. THE DETAILS SHALL SHOW THE ORDER OF PERFORMANCE OF EACH STAGE (START TO FINISH) OF THE WORK INCLUDING THE MAINTENANCE OF TRAFFIC THAT WILL BE USED.

**TRAFFIC CONTROL INSPECTOR**

THE CONTRACTOR SHALL DESIGNATE AN INDIVIDUAL OTHER THAN THE SUPERINTENDENT AND SUBJECT TO THE APPROVAL OF THE ENGINEER, TO CONTINUOUSLY INSPECT ALL TRAFFIC CONTROL DEVICES WHENEVER CONSTRUCTION WORK IS BEING PERFORMED WITHIN THE WORK LIMITS OF THE PROJECT. THE DESIGNATED INDIVIDUAL SHALL ALSO INSPECT ALL TRAFFIC DEVICES AT THE BEGINNING AND AT THE END OF EACH WORK DAY. THE DESIGNATED INDIVIDUAL OR A QUALIFIED REPRESENTATIVE SHALL ALSO BE AVAILABLE ON AN AROUND THE CLOCK BASIS TO REPAIR AND/OR REPLACE DAMAGED OR MISSING TRAFFIC CONTROL DEVICES. THESE INDIVIDUALS SHALL BE EQUIPPED WITH CELLULAR PHONES AND THEIR NAMES AND PHONE NUMBERS SHALL BE GIVEN TO THE PROJECT ENGINEER AT THE PRE-CONSTRUCTION MEETING. THE DESIGNATED INDIVIDUAL MAY HAVE OTHER CONSTRUCTION RELATED DUTIES AS LONG AS IMMEDIATE ATTENTION IS GIVEN TO TRAFFIC CONTROL. PAYMENT FOR THE SERVICES OF THE TRAFFIC CONTROL INSPECTOR SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614 MAINTAINING TRAFFIC.

**DROPOFFS AT SIDE STREETS AND DRIVEWAYS**

THE CONTRACTOR WILL NOT BE PERMITTED TO LEAVE A DIFFERENCE IN ELEVATION BETWEEN THE MAINLINE MILLED SURFACES AND THE SURFACE COURSE OF SIDE STREET APPROACHES/DRIVEWAYS GREATER THAN 1.25 INCH. THE CONTRACTOR SHALL PLACE A 12:1 ASPHALT WEDGE FOR ALL RESULTING ELEVATION DIFFERENCES GREATER THAN 1.25 INCH PRIOR TO OPENING TO TRAFFIC. THE PAVING OF INTERSECTION APPROACHES AND DRIVEWAYS, PER THE NOTES ON SHEET 5, SHALL BE PERFORMED WITHIN 7 DAYS OF MAINLINE SURFACE COURSE BEING APPLIED AND A DROPOFF BEING CREATED BETWEEN THE NEW SURFACE COURSE AND THE MILLED/EXISTING SIDE ROAD OR DRIVEWAY SURFACE. THE CONTRACTOR MAY ELECT TO PLACE A 12:1 ASPHALT WEDGE IN LIEU OF COMPLETING THE PAVING, HOWEVER THE ASPHALT CONCRETE USED FOR THE WEDGE SHALL BE CONSIDERED INCIDENTAL TO ITEM 614 – MAINTAINING TRAFFIC AND SHALL INCLUDE THE REMOVAL OF THE WEDGE BEFORE THE INTERSECTION/DRIVEWAY IS PAVED.

**ASPHALT PAVING LIMITATION**

THE CONTRACTOR SHALL NOT ANTICIPATE OR SCHEDULE PLACING ASPHALT (ASPHALT SURFACE COURSE, ASPHALT INTERMEDIATE COURSE, ASPHALT CONCRETE BASE, ETC.) BETWEEN NOVEMBER 1 AND APRIL 1 WHEN SUBMITTING THEIR INITIAL BAR CHART TPROGRESS SCHEDULE TO THE DISTRICT CONSTRUCTION ENGINEER (DCE) AS SPECIFIED IN CMS SECTION 108.02A. THIS LIMITATION SHALL ALSO INCLUDE INITIAL BASE LINE SCHEDULES AND ALL UPDATES IF A CPM SCHEDULE IS REQUIRED.

**TIME LIMITATION, TRAFFIC ON A MILLED SURFACE**

THE MAXIMUM ALLOWABLE TIME FOR TRAFFIC TO BE PLACED ON A MILLED SURFACE SHALL BE 7 CONSECUTIVE CALENDAR DAYS. SHOULD THE CONTRACTOR FAIL TO MEET THIS REQUIREMENT, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF \$3,000 PER DAY THAT THE TRAFFIC IS PLACED ON A MILLED SURFACE BEYOND THE SPECIFIED LIMIT.

**NOTIFICATION OF TRAFFIC RESTRICTIONS**

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS & PIO
ROAD & RAMP CLOSURES	>= 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	<12 HOURS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	>=2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERNS CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

**FLOODLIGHTING**

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.



**ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN**

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

(THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL (IN ACTIVE CELLULAR PHONE AREAS) ALLOW REMOTE SIGN ACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS AND REVISIONS TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES. ONE REMOTE DATA INPUT DEVICE (LAPTOP COMPUTER PLUS MODEM OR EQUIVALENT) SHALL BE FURNISHED FOR USE BY THE DISTRICT TRAFFIC ENGINEER, OR EQUIVALENT, AND SHALL BE INSURED AGAINST THEFT.) THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 24 SIGN MONTH ASSUMING 8 PCMS SIGN(S) FOR 3 MONTH(S)

**SEQUENCE OF PAVEMENT OPERATIONS**

THE CONTRACTOR SHALL PERFORM ALL PAVING AND PAVEMENT REPAIR OPERATIONS ONE LANE AT A TIME (CONTRACTOR IS PERMITTED TO WORK ON BOTH SIDES OF FREEWAY AT THE SAME TIME). THIS WORK INCLUDES MILLING, PAVEMENT REPAIRS, AND PLACEMENT OF NEW ASPHALT.

ALL PAVING AND PAVEMENT REPAIR OPERATION WORK ON SHOULDERS SHALL BE DONE CONCURRENTLY WITH THE CLOSEST LANE.

RAMP PAVING AND PAVEMENT REPAIR OPERATIONS SHALL BE COMPLETED AFTER ALL MAINLINE PAVING IS COMPLETE.

**ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS**

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF CMS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

IN GENERAL LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION OR AT THE POINT OF ROAD CLOSURE, AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONE.

THE LEOS WORK AT THE DIRECTION OF THE ENGINEER. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. ONCE THE LEO HAS COMPLETED THE DUTIES DESCRIBED ABOVE AND STILL HAS TIME REMAINING ON HIS/HER SHIFT, THE LEO MAY BE ASKED TO PATROL THROUGH THE WORK ZONE (WITH FLASHING LIGHTS OFF) OR BE PLACED AT A LOCATION TO DETER MOTORISTS FROM SPEEDING. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 300 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

**PERMITTED LANE CLOSURE SCHEDULE (PLCS)**

LANE CLOSURE(S) SHALL CONFORM TO THE PLCS. PUBLISHED PLCS INFORMATION CAN BE FOUND ON THE ODOT WEBSITE AT: [HTTPS://WWW.TRANSPORTATION.OHIO.GOV/WPS/PORTAL/GOV/ODOT/WORKING/DATA-TOOLS/RESOURCES/PERMITTED-LANE-CLOSURE](https://www.transportation.ohio.gov/wps/portal/gov/odot/working/data-tools/resources/permited-lane-closure)

THE MONTHLY PUBLISHED SCHEDULES REQUIRED TO BE USED, FOR EACH PLCS SEGMENT WITHIN THE PROJECT AREA, ARE THOSE THAT COMPRISE THE CONSECUTIVE 12-MONTH PERIOD BEGINNING 15 MONTHS PRIOR TO THE MONTH AND YEAR OF SALE AND ENDING 4 MONTHS PRIOR TO THE MONTH AND YEAR OF SALE. THESE SAME 12 MONTHS APPLY FOR THE LIFE OF THE PROJECT AND SHALL BE APPLIED TO EACH RESPECTIVE MONTH OF CONSTRUCTION (MONTH OF LANE CLOSURE(S) SHALL MATCH MONTH OF PLCS USED). LANE CLOSURE(S) IN PLACE FOR MULTIPLE MONTHS SHALL ALWAYS COMPLY WITH THE CURRENT RESPECTIVE MONTH.

(FOR EXAMPLE: IF THE SALE DATE FOR THE PROJECT WAS MARCH OF 2021, THE MONTHLY PUBLISHED SCHEDULES FOR EACH APPLICABLE PLCS SEGMENT WOULD BE DECEMBER 2019 TO NOVEMBER 2020. IF THIS WAS A THREE-YEAR PROJECT, YEAR THREE WOULD STILL BE USING THE DECEMBER 2019 TO NOVEMBER 2020 MONTHLY SCHEDULES. IF THE PROJECT DESIRED TO CLOSE TWO LANES IN JUNE 2021, REFERENCE WOULD BE MADE TO THE JUNE 2020 SCHEDULE(S) FOR THE RESPECTIVE PLCS SEGMENT(S). IF THE SAME TWO LANES WERE DESIRED TO BE CLOSED AGAIN IN JULY 2021, REFERENCE WOULD BE MADE TO THE JULY 2020 SCHEDULE(S) FOR THE RESPECTIVE PLCS SEGMENT(S).)

MORE RESTRICTIVE CHANGES TO THE ALLOWABLE LANE CLOSURE HOURS ARE AT THE DISCRETION OF THE ENGINEER IN ORDER TO COMPLY WITH THE TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)).


LESS RESTRICTIVE CHANGES TO THE ALLOWABLE LANE CLOSURE HOURS ARE SUBJECT TO THE TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)) AND SHALL NOT BE IMPLEMENTED UNTIL, AND UNLESS, APPROVED BY THE PROPER ODOT AUTHORITY. [EXISTING MOT EXCEPTIONS THAT HAVE ALREADY BEEN APPROVED IN ACCORDANCE TO THE TRAFFIC MANAGEMENT IN WORK ZONES POLICY AND STANDARD PROCEDURE ARE DETAILED IN THE APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTION(S) PLAN NOTE.]

ALLOWABLE LANE CLOSURE HOURS FOR FACILITIES NOT COVERED BY THE PLCS, IF ANY, SHALL BE AS SPECIFIED ELSEWHERE IN THE PLANS.



SHEET NUM.											PART.		ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
4	5	6	7	8	9	13	14	15	16	20	01/NHS/05	02/NHS/47						
<b>ROADWAY</b>																		
	132										132		202	98100	132	EACH	REMOVAL MISC.: BARRIER REFLECTOR	5
84											84		203	10000	84	CY	EXCAVATION	
246											246		209	60200	246	STA	LINEAR GRADING	
	36										36		623	39501	36	EACH	MONUMENT ASSEMBLY ADJUSTED TO GRADE, AS PER PLAN	5
	LS										LS		SPECIAL	69091000	LS		AS-BUILT CONSTRUCTION PLANS	5
	6										6		SPECIAL	69098000	6	EACH	VERTICAL CLEARANCE	5
<b>EROSION CONTROL</b>																		
											3,000		832	30000	3,000	EACH	EROSION CONTROL	
<b>DRAINAGE</b>																		
	2										2		611	98630	2	EACH	CATCH BASIN ADJUSTED TO GRADE	
	5										5		611	99150	5	EACH	INLET ADJUSTED TO GRADE	
	11										11		611	99654	11	EACH	MANHOLE ADJUSTED TO GRADE	
	10										10		611	99660	10	EACH	MANHOLE RECONSTRUCTED TO GRADE	
	4,650										4,650		SPECIAL	61199820	4,650	LB	MISCELLANEOUS METAL	5
<b>PAVEMENT</b>																		
2,300											2,300		251	01000	2,300	SY	PARTIAL DEPTH PAVEMENT REPAIR (441) (LONGITUDINAL)	
700											700		251	01000	700	SY	PARTIAL DEPTH PAVEMENT REPAIR (441) (TRANSVERSE)	
2,275											2,275		252	01500	2,275	FT	FULL DEPTH PAVEMENT SAWING	
						308,216	56,400				364,616		254	01000	364,616	SY	PAVEMENT PLANING, ASPHALT CONCRETE (T=1 1/2")	
500											500		255	17000	500	SY	FULL DEPTH PAVEMENT REMOVAL AND RIGID REPLACEMENT, TYPE 1, CLASS RRCM	
84											84		304	20000	84	CY	AGGREGATE BASE	
						27,740	5,076				32,816		407	20000	32,816	GAL	NON-TRACKING TACK COAT	
						4,757	1,325				6,082		408	10001	6,082	GAL	PRIME COAT, AS PER PLAN @ 0.40 GAL/SY	4
						8,633					8,633		442	00100	8,633	CY	ANTI-SEGREGATION EQUIPMENT	
						12,843	2,350				15,193		442	10331	15,193	CY	ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (447), PWL 2025, AS PER PLAN (T=1 1/2")	4
						661	184				845		617	10101	845	CY	COMPACTED AGGREGATE, AS PER PLAN (T=2")	4
						19					19		618	40600	19	MILE	RUMBLE STRIPS, SHOULDER (ASPHALT CONCRETE)	
						42.98					42.98		850	10010	42.98	MILE	GROOVING FOR 6" RECESSED PAVEMENT MARKING, (ASPHALT)	
						11,175					11,175		850	10130	11,175	FT	GROOVING FOR 12" RECESSED PAVEMENT MARKING, (ASPHALT)	
<b>WATER WORK</b>																		
	4										4		638	10800	4	EACH	VALVE BOX ADJUSTED TO GRADE	
<b>TRAFFIC CONTROL</b>																		
	535										535		626	00102	535	EACH	BARRIER REFLECTOR, TYPE 1 (ONE-WAY)	
	117										117		626	00110	117	EACH	BARRIER REFLECTOR, TYPE 2 (ONE-WAY)	
								589			589		630	02100	589	FT	GROUND MOUNTED SUPPORT, NO. 2 POST	
								132			132		630	80100	132	SF	SIGN, FLAT SHEET	
								36			36		630	80100	36	SF	SIGN, FLAT SHEET, 730.20	
								58			58		630	84900	58	EACH	REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL	
								58			58		630	86002	58	EACH	REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL	
								250			250		646	10400	250	FT	STOP LINE	
								108			108		646	10510	108	FT	CROSSWALK LINE, 12"	
		82									82		646	10511	82	FT	CROSSWALK LINE, 12", AS PER PLAN	6
								31			31		646	20300	31	EACH	LANE ARROW	
								23			23		646	20320	23	EACH	WRONG WAY ARROW	
								24.36			24.36		807	12010	24.36	MILE	WET REFLECTIVE EPOXY PAVEMENT MARKING, EDGE LINE, 6"	
								18.62			18.62		807	12110	18.62	MILE	WET REFLECTIVE EPOXY PAVEMENT MARKING, LANE LINE, 6"	
								11,175			11,175		807	12310	11,175	FT	WET REFLECTIVE EPOXY PAVEMENT MARKING, CHANNELIZING LINE, 12"	

GENERAL SUMMARY

DESIGN AGENCY  
  
 DESIGNER: JRF  
 REVIEWER: JRF  
 PROJECT ID: MA 01-19-24  
 PROJECT ID: 100936  
 SHEET: P.11 | TOTAL: 22

SUM-8-2.91

MODEL: Sheet\_SurvFl\_PAPER SIZE: 34x22 (in.) DATE: 11/26/2024 TIME: 12:35:57 PM USER: fitzsim  
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EDGE LINE												GENERAL SPEC: 640
												MATERIAL TYPE: 646 / 807
CTY	ROUTE	TRUE LOG	FROM	TRUE LOG	TO	WHITE EDGE LINE, 6"			YELLOW EDGE LINE, 6"			COMMENTS
						TOTAL	HIGHWAY	RAMP	TOTAL	HIGHWAY	RAMP	
SUM	8 NB	2.91	GORGE BLVD	7.68	GRAHAM ROAD	6.20	4.77	1.43	6.20	4.77	1.43	807 - WET REFLECTIVE EPOXY PAVEMENT MARKING
SUM	8 SB	2.99	GORGE BLVD	7.53	GRAHAM ROAD	5.98	4.54	1.44	5.98	4.54	1.44	807 - WET REFLECTIVE EPOXY PAVEMENT MARKING
TOTAL						12.18	9.31	2.87	12.18	9.31	2.87	


LANE LINE											
CTY	ROUTE	TRUE LOG	FROM	TRUE LOG	TO	TOTAL MILES	6" LANE LINE		COMMENTS		
							DASHED	SOLID			
SUM	8 NB	2.91	GORGE BLVD	7.68	GRAHAM ROAD	9.54	9.54		807 - WET REFLECTIVE EPOXY PAVEMENT MARKING		
SUM	8 SB	2.99	GORGE BLVD	7.53	GRAHAM ROAD	9.08	9.08		807 - WET REFLECTIVE EPOXY PAVEMENT MARKING		
TOTAL						18.62	18.62				

CENTER LINE											
CTY	ROUTE	TRUE LOG	FROM	TRUE LOG	TO	TOTAL MILES	EQUIVALENT SOLID LINE		COMMENTS		
TOTAL											

AUXILIARY																										
CTY	ROUTE LOCATION	CHANNEL LINE, 12" FT	CHANNEL LINE, 12" FT	STOP LINE FT	CROSS WALK LINES FT	TRANSVERSE DIAGONAL LINES		ISLAND MARKING SF	SYMBOL MARKINGS				LANE ARROWS				WRONG WAY ARROWS EACH	WORD ON PVMT ONLY		DOTTED LINES, 6" FT	GROOVING RECESSED PAVEMENT MARKINGS					
						WHITE FT	YELLOW FT		RxR EACH	SCHOOL		TURN LEFT EACH	TURN RIGHT EACH	THRU EACH	COMB. EACH	ONLY		6" ASPHALT MILE	6" ASPHALT FEET		12" ASPHALT FEET	6" CONCRETE MILE	6" CONCRETE FEET	12" CONCRETE FEET		
										72"	96"					72"									96"	
SUM	SR 8 NB (SLM 2.91 TO 7.68)																									
SUM	SR 8 SB (SLM 2.99 TO 7.53)																									
SUM	RAMP C - SR 8 NB TO SR 261		230																							
SUM	RAMP D - SR 261 TO SR 8 NB		135																							
SUM	RAMP E - SR 8 SB TO SR 261	100	590	30																						
SUM	RAMP F - SR 261 TO SR 8 SB		150																							
SUM	RAMP A - CUYAHOGA FALLS TO SR 8 SB		160																							
SUM	RAMP B - SR 8 NB TO CUYAHOGA FALLS		720	22																						
SUM	RAMP C - SR 8 NB TO HOWE AVE	150	700	45																						
SUM	RAMP D - HOWE AVE TO SR 8 NB		580																							
SUM	RAMP E - HOWE AVE TO SR 8 SB		350																							
SUM	RAMP F - SR 8 SB TO HOWE AVE	70	440	34																						
SUM	RAMP G - BROAD BLVD TO SR 8 SB		365																							
SUM	RAMP H - SR 8 NB TO BROAD BLVD	295	560	45	56																					
SUM	RAMP J - SR 8 NB TO PORTAGE TRAIL		590																							
SUM	RAMP K - PORTAGE TRAIL TO SR 8 SB		510																							
SUM	RAMP J-I - BROAD BLVD TO SR 8 NB		390																							
SUM	RAMP K-I - SR 8 SB TO BROAD BLVD	280	560	34	52																					
SUM	RAMP L - FRONT ST TO SR 8 SB		250																							
SUM	RAMP M - SR 8 NB TO FRONT ST		420																							
SUM	RAMP N - FRONT ST TO SR 8 NB		250																							
SUM	RAMP P - SR 8 SB TO FRONT ST		480																							
SUM	RAMP O - HUDSON DR TO SR 8 NB		280																							
SUM	RAMP C - SR 8 NB TO GRAHAM RD	690	480	40																						
SUM	RAMP E - GRAHAM RD TO SR 8 SB		400																							
TOTAL		1585	9590	250	108																					

PAVEMENT MARKINGS

DESIGN AGENCY



DESIGNER: JRF  
 REVIEWER: MA  
 PROJECT ID: 100936  
 SHEET: P.15 TOTAL: 22