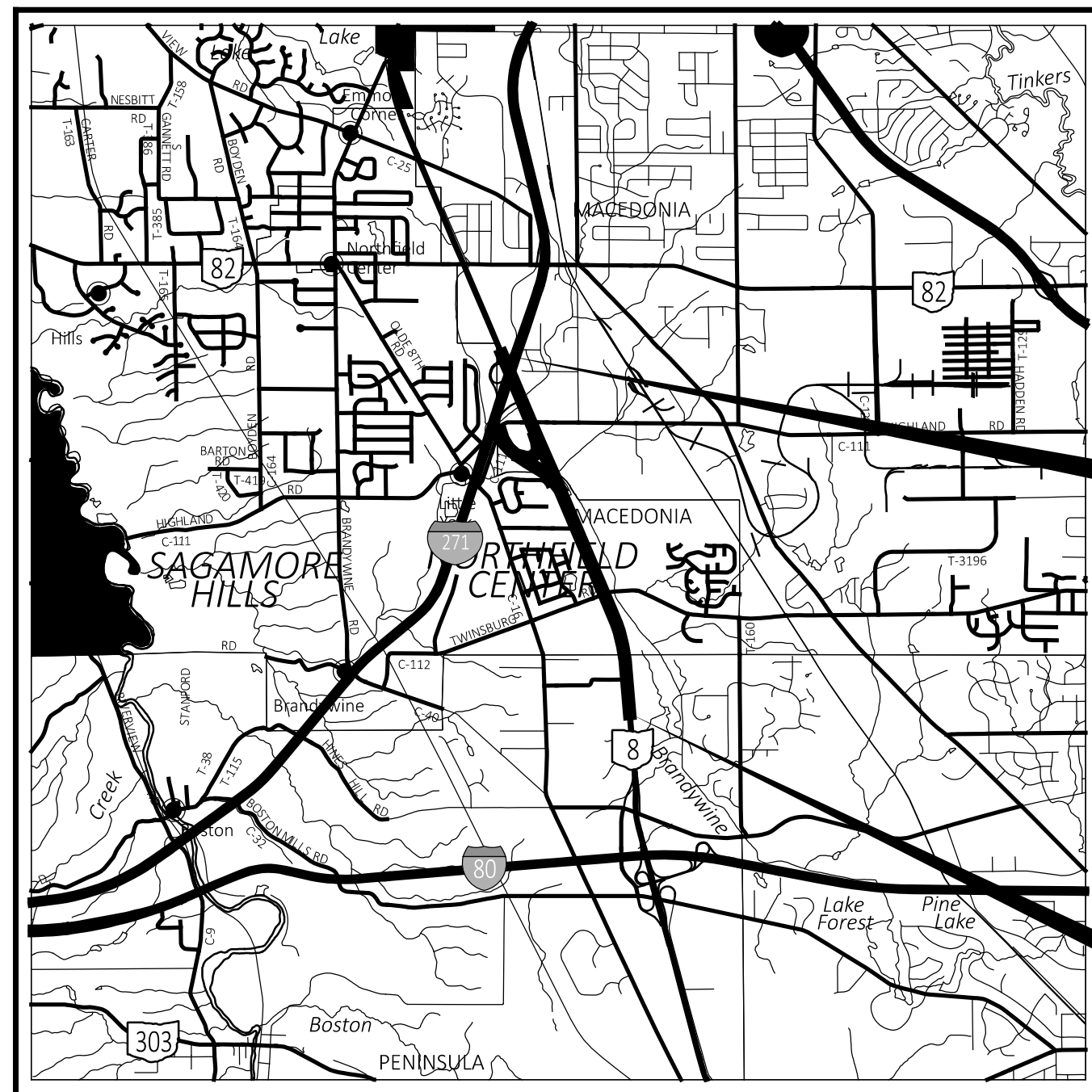


STATE OF OHIO DEPARTMENT OF TRANSPORTATION

SUM-8-15.75

CITY OF MACEDONIA
VILLAGE OF BOSTON HEIGHTS
NORTHFIELD CENTER TOWNSHIP
SUMMIT COUNTY



LOCATION MAP

LATITUDE: 41°17'25" LONGITUDE: 81°30'40"



END PROJECT
SR 8 SLM: 18.10

BEGIN PROJECT
SR 8 SLM: 15.75

| | |
|-------------------------|-------|
| PORTION TO BE IMPROVED | ————— |
| INTERSTATE HIGHWAY | ===== |
| FEDERAL ROUTES | ————— |
| STATE ROUTES | ————— |
| COUNTY & TOWNSHIP ROADS | ————— |
| OTHER ROADS | ————— |

DESIGN DESIGNATION (PER TMS)

| | SLM 15.75 TO 16.86 | | SLM 16.86 TO 18.10 | |
|--|--------------------|--------|--------------------|--------|
| | NB | SB | NB | SB |
| CURRENT ADT (2023) | 27,495 | 27,419 | 11,325 | 11,326 |
| DESIGN HOURLY VOLUME (2023) | 2,750 | 2,742 | 1,246 | 1,246 |
| TRUCKS (24 HOUR B&C) | 10% | 10% | 6% | 6% |
| DESIGN SPEED | 70 MPH | 70 MPH | 55 MPH | 55 MPH |
| LEGAL SPEED | 65 MPH | 65 MPH | 50 MPH | 50 MPH |
| DESIGN FUNCTIONAL CLASSIFICATION: | | | | |
| 02 OTHER FREEWAYS OR EXPRESSWAYS (URBAN) | | | | |
| NHS PROJECT | YES | | | |

INDEX OF SHEETS:

| | |
|------------------------|-----------|
| TITLE SHEET | P.1 |
| TYPICAL SECTIONS | P.2-P.5A |
| GENERAL NOTES | P.6-P.7 |
| MAINTENANCE OF TRAFFIC | P.8-P.13 |
| GENERAL SUMMARY | P.14-P.15 |
| PAVEMENT CALCULATIONS | P.16-P.17 |
| RPM SUBSUMMARY | P.18 |
| PAVEMENT MARKINGS | P.19 |
| LIGHTING | P.20-P.21 |
| STRUCTURES | P.22-P.33 |

Total number of sheets has increased to 34.

FEDERAL PROJECT NUMBER

E170(124)

RAILROAD INVOLVEMENT

NONE

PROJECT DESCRIPTION

RESURFACING OF SUM SR 8 SLM. 15.75 TO SLM. 18.10,
INCLUDES MINOR BRIDGE REHAB TO EIGHT STRUCTURES.

EARTH DISTURBED AREAS

| | |
|--|------------------------------|
| PROJECT EARTH DISTURBED AREA: | 0.63 ACRES |
| ESTIMATED CONTRACTOR EARTH DISTURBED AREA: | 0.25 ACRES |
| NOTICE OF INTENT EARTH DISTURBED AREA: | N/A (NOI NOT REQUIRED) |
| | *ROUTINE MAINTENANCE PROJECT |

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

2023 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS, CHANGES LISTED IN THE PROPOSAL, AND THE SUPPLEMENTAL SPECIFICATION 800 VERSION INDICATED ON THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY EXCEPT FOR THE RAMPS AS DESCRIBED ON SHEET P.10 AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

Arthur G. Noiro Jr.
Arthur G. Noiro Jr., P.E.
District 04 Deputy Director

Pamela Boratyn
Pamela Boratyn
Director, Department of Transportation

DESIGN EXCEPTIONS

NONE

ADA DESIGN WAIVERS

NONE

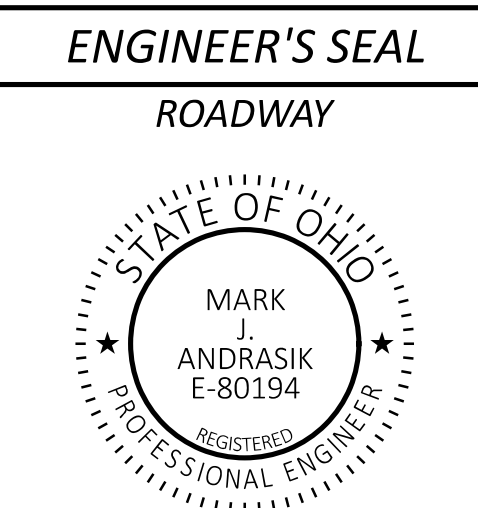
UNDERGROUND UTILITIES
Contact Two Working Days
Before You Dig

OHIO811.org
Before You Dig

OHIO811, 8-1-1, or 1-800-362-2764
(Non members must be called directly)

PLAN PREPARED BY:
ODOT --- DISTRICT 4, CAPITAL PROGRAMS
2088 S. ARLINGTON ROAD
AKRON, OHIO 44306

| STANDARD CONSTRUCTION DRAWINGS | | | | | | SUPPLEMENTAL SPECIFICATIONS | | SPECIAL PROVISIONS | |
|--------------------------------|---------|-----------|----------|----------|---------|-----------------------------|----------|--------------------|----------|
| BP-3.1 | 1/21/22 | MT-105.10 | 1/17/20 | HL-10.11 | 7/21/23 | 800-2023 | 7/21/23 | ASBESTOS | |
| | | | | HL-10.12 | 7/21/23 | 807 | 1/21/22 | REPORT | 11/11/24 |
| MT-95.30 | 7/19/19 | TC-41.20 | 10/18/13 | HL-10.13 | 1/20/23 | 808 | 7/19/24 | WPC | 11/22/24 |
| MT-95.40 | 7/21/23 | TC-42.10 | 10/18/13 | HL-20.11 | 7/21/23 | 813 | 7/21/23 | | |
| MT-95.50 | 7/21/17 | TC-42.20 | 10/18/13 | HL-20.14 | 4/17/20 | 821 | 4/20/12 | | |
| MT-97.10 | 4/19/19 | TC-52.10 | 10/18/13 | HL-60.11 | 7/21/17 | 832 | 7/19/24 | | |
| MT-97.12 | 1/20/17 | TC-61.30 | 7/19/19 | | | 842 | 7/15/11 | | |
| MT-98.10 | 1/17/20 | TC-65.10 | 1/17/14 | DM-4.3 | 1/15/16 | 843 | 10/18/19 | | |
| MT-98.11 | 1/17/20 | TC-65.11 | 7/15/22 | DM-4.4 | 1/15/16 | 850 | 7/21/23 | | |
| MT-98.20 | 4/19/19 | TC-71.10 | 4/21/23 | | | 905 | 4/17/20 | | |
| MT-98.22 | 1/17/20 | TC-72.20 | 7/21/23 | | | 908 | 10/20/17 | | |
| MT-98.28 | 1/17/20 | TC-73.20 | 7/21/23 | | | 913 | 4/16/21 | | |
| MT-98.29 | 1/17/20 | | | | | 921 | 7/19/24 | | |
| MT-99.20 | 4/19/19 | MT-101.90 | 7/17/20 | | | | | | |
| MT-101.60 | 4/21/23 | | | | | | | | |
| MT-104.10 | 1/19/24 | | | | | | | | |



TITLE SHEET

DESIGN AGENCY



DESIGNER

JF

REVIEWER

MJA 10-15-24

PROJECT ID

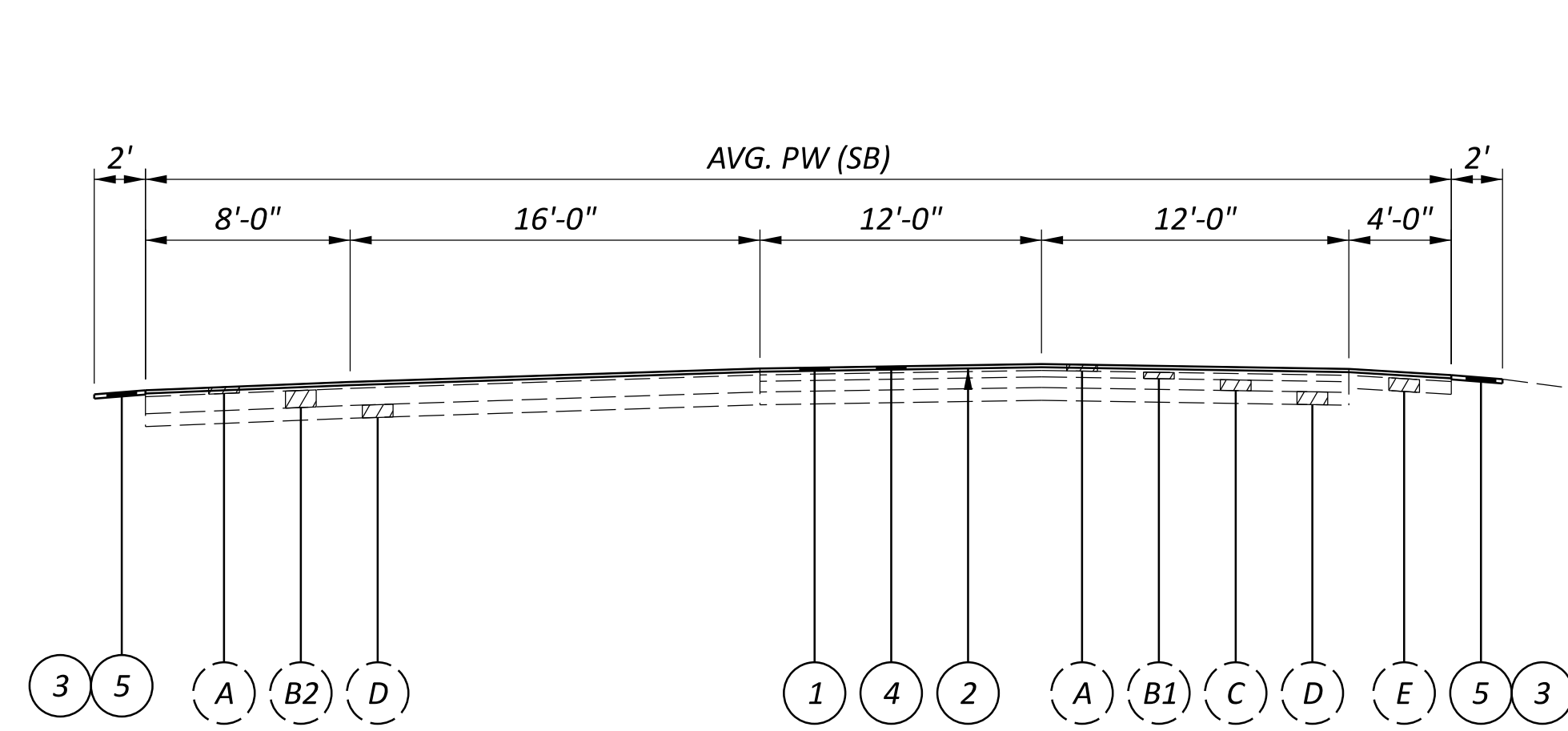
102330

SHEET TOTAL

P.1 33

SUM-8-15.75

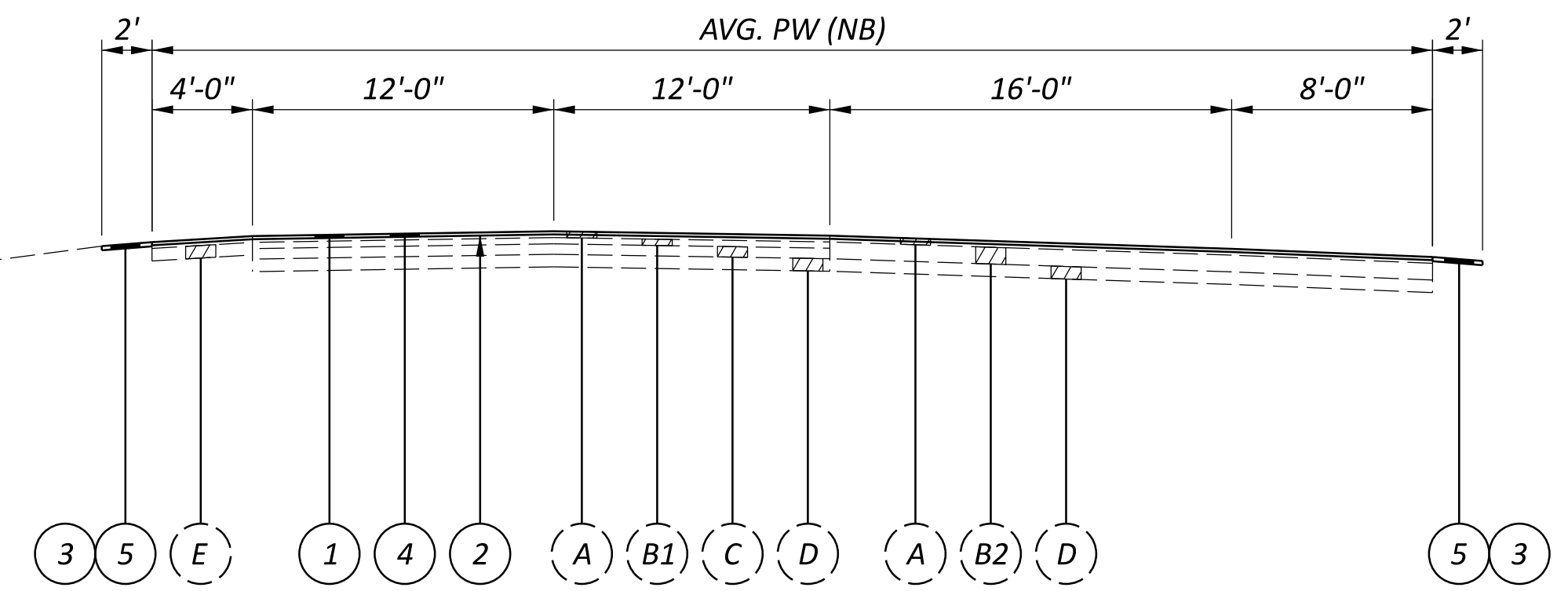
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TYPICAL SECTION 7

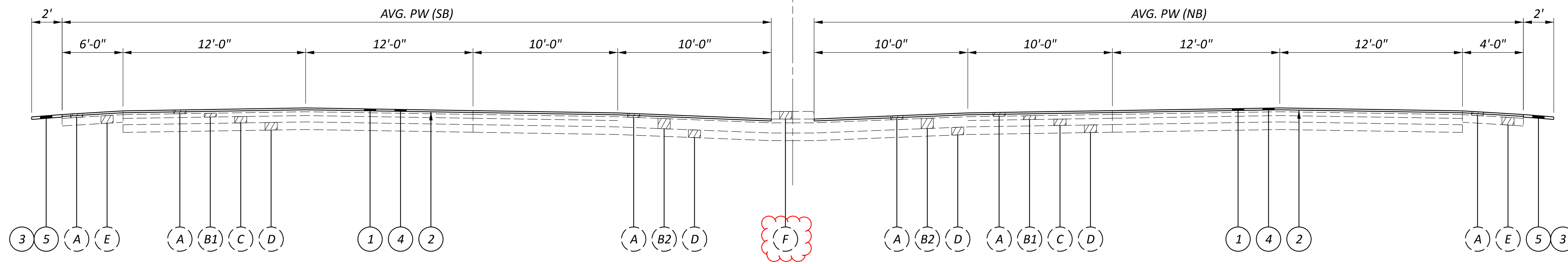
(TYPICAL SECTION 7) SR 8 SB:

| ROUTE | SLM | | AVG. PW (FEET) | LENGTH (MILES) |
|-------|-------|-------|----------------|----------------|
| | FROM | TO | | |
| SUM 8 | 17.66 | 17.70 | 36 | 0.04 |
| SUM 8 | 17.70 | 17.84 | 52 | 0.14 |
| SUM 8 | 17.84 | 17.86 | 32 | 0.02 |
| SUM 8 | 17.95 | 18.04 | 50 | 0.09 |
| SUM 8 | 18.04 | 18.10 | 49 | 0.06 |



(TYPICAL SECTION 7) SR 8 NB:

| ROUTE | SLM | | AVG. PW (FEET) | LENGTH (MILES) |
|-------|-------|-------|----------------|----------------|
| | FROM | TO | | |
| SUM 8 | 17.59 | 17.65 | 50 | 0.06 |
| SUM 8 | 17.65 | 17.71 | 62 | 0.06 |
| SUM 8 | 17.71 | 17.73 | 74 | 0.02 |
| SUM 8 | 17.88 | 17.96 | 47 | 0.08 |
| SUM 8 | 17.96 | 18.06 | 32 | 0.10 |



TYPICAL SECTION 8

(TYPICAL SECTION 8) SR 8 SB:

| ROUTE | SLM | | AVG. PW (FEET) | LENGTH (MILES) |
|-------|-------|-------|----------------|----------------|
| | FROM | TO | | |
| SUM 8 | 17.84 | 17.87 | 76 | 0.03 |
| SUM 8 | 17.87 | 17.92 | 68 | 0.05 |
| SUM 8 | 17.92 | 17.95 | 46 | 0.03 |

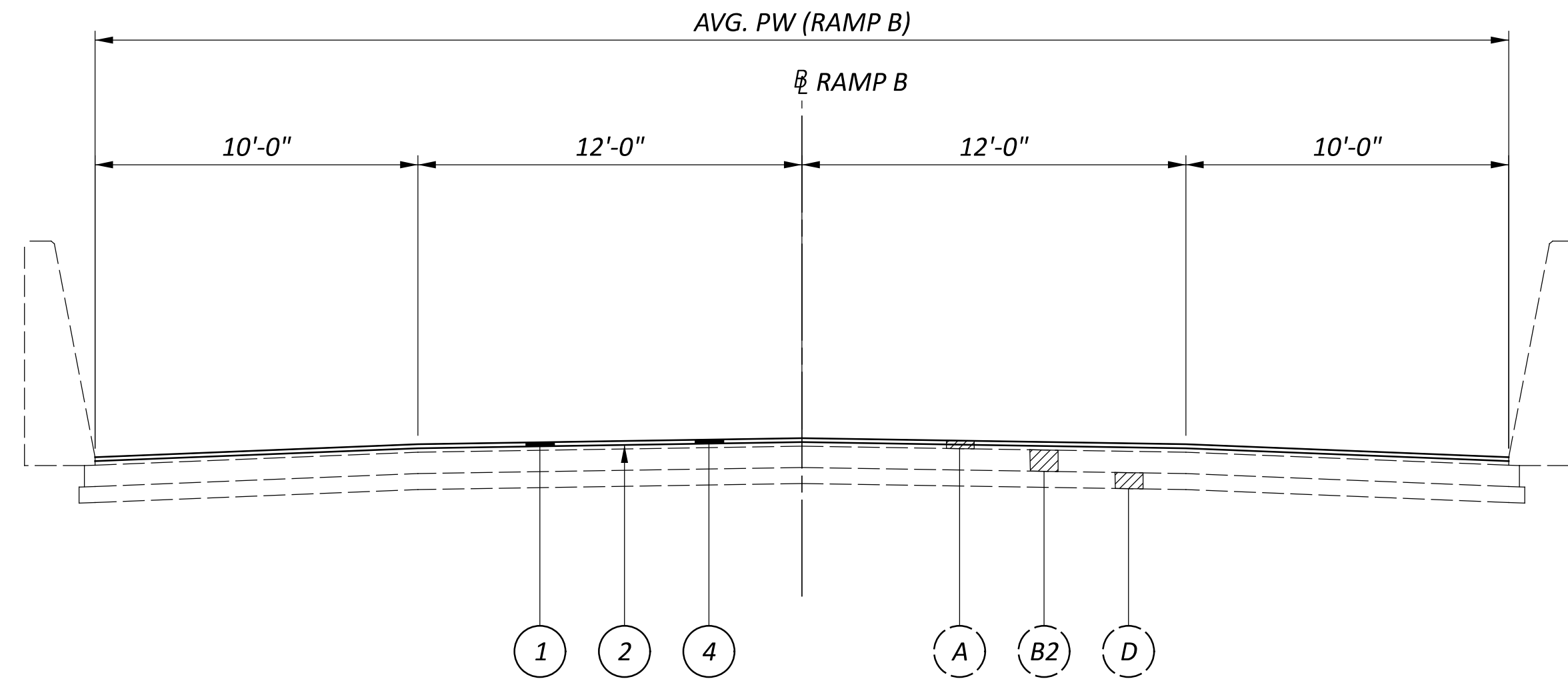
(TYPICAL SECTION 8) SR 8 NB:

| ROUTE | SLM | | AVG. PW (FEET) | LENGTH (MILES) |
|-------|-------|-------|----------------|----------------|
| | FROM | TO | | |
| SUM 8 | 17.73 | 17.88 | 44 | 0.15 |

LEGEND

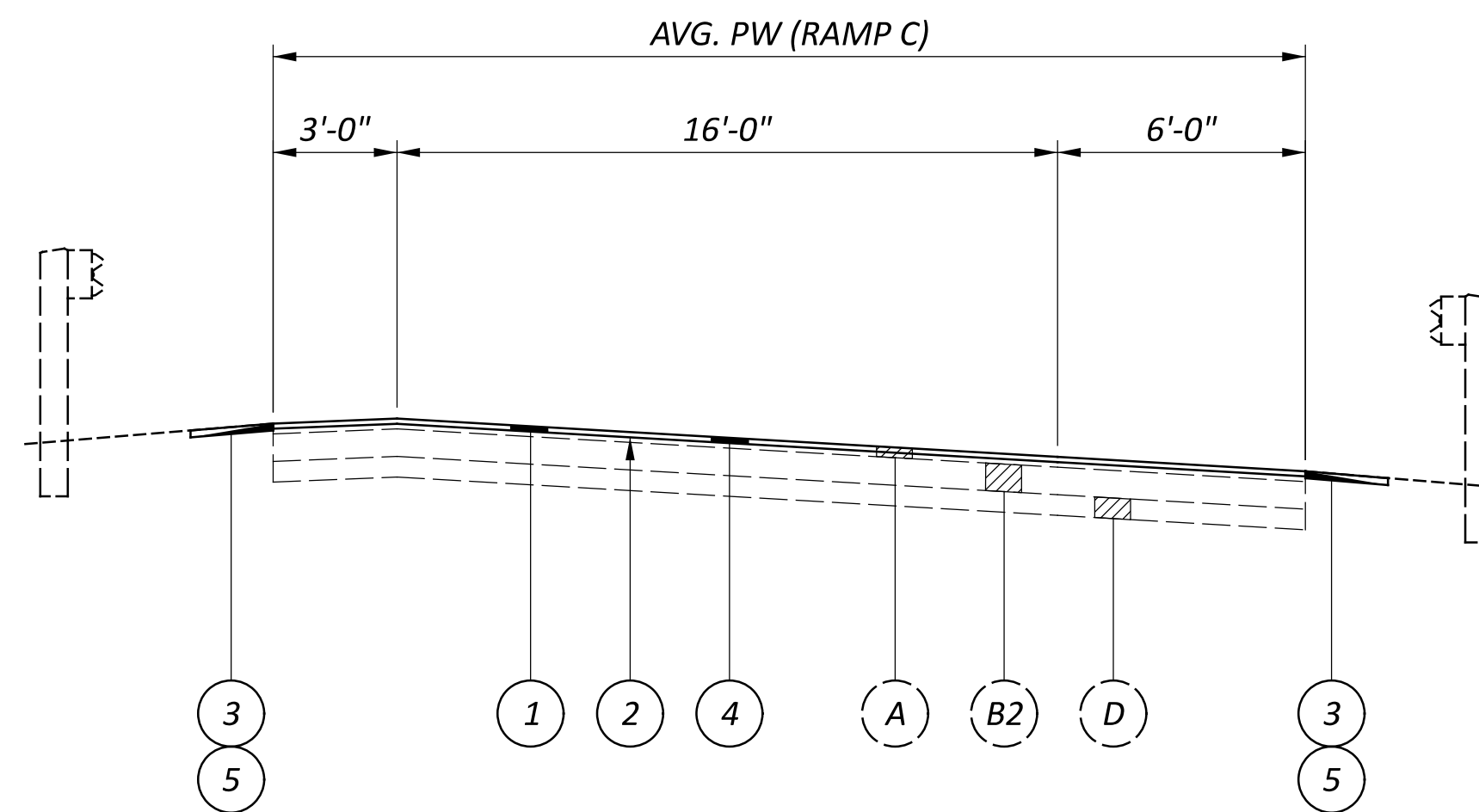
- (A) EXISTING ASPHALT (T=3"±)
- (B1) EXISTING ASPHALT CONCRETE (T=3"±)
- (B2) EXISTING ASPHALT CONCRETE (T=8"±)
- (C) EXISTING MACADAM BASE (T=5"±)
- (D) EXISTING AGGREGATE BASE (T=6"±)
- (E) EXISTING STABILIZED SHOULDER (ASPHALT)
- (F) EXISTING CONCRETE ISLAND
- (1) ITEM 254, PAVEMENT PLANING, ASPHALT CONCRETE (T=1.5")
- (2) ITEM 407, NON-TRACKING TACK COAT (0.09 GAL/SY)
- (3) ITEM 408, PRIME COAT, AS PER PLAN (0.40 GAL/SY)
- (4) ITEM 442, ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (447), PWL 2025, AS PER PLAN, PG88-22M (T=1.5")
- (5) ITEM 617, COMPACTED AGGREGATE, AS PER PLAN (T=1" AVG.)





TYPICAL SECTION 9

| (TYPICAL SECTION 9) RAMP B: | | |
|-----------------------------|----------------|---------------|
| ROUTE | AVG. PW (FEET) | LENGTH (FEET) |
| SUM 8 | 44 | 2880 |



TYPICAL SECTION 10

| (TYPICAL SECTION 10) RAMP C: | | |
|------------------------------|----------------|---------------|
| ROUTE | AVG. PW (FEET) | LENGTH (FEET) |
| SUM 8 | 25 | 1740 |

LEGEND

- | | |
|--|--|
| (A) EXISTING ASPHALT (T=3"±) | (1) ITEM 254, PAVEMENT PLANING, ASPHALT CONCRETE (T=1.5") |
| (B1) EXISTING ASPHALT CONCRETE (T=3"±) | (2) ITEM 407, NON-TRACKING TACK COAT (0.09 GAL/SY) |
| (B2) EXISTING ASPHALT CONCRETE (T=8"±) | (3) ITEM 408, PRIME COAT, AS PER PLAN (0.40 GAL/SY) |
| (C) EXISTING MACADAM BASE (T=5"±) | (4) ITEM 442, ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (447), PWL 2025, AS PER PLAN, PG88-22M (T=1.5") |
| (D) EXISTING AGGREGATE BASE (T=6"±) | (5) ITEM 617, COMPACTED AGGREGATE, AS PER PLAN (T=1" AVG.) |
| (E) EXISTING STABILIZED SHOULDER (ASPHALT) | |
| (F) EXISTING CONCRETE ISLAND | |



MAINTENANCE OF TRAFFIC

THIS ITEM SHALL CONSIST OF MAINTENANCE OF TRAFFIC ON EXISTING ROADWAYS AND RAMPS IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, CURRENT EDITION, LATEST REVISION, THE SPECIFICATIONS AND THE FOLLOWING:

1. A MINIMUM OF ONE ELEVEN FOOT LANE IN EACH DIRECTION SHALL BE MAINTAINED ON THE EXISTING PAVEMENT OR COMPLETED PAVEMENT DURING CONSTRUCTION OF THE WORK.
2. THE CONTRACTOR SHALL INFORM THE DISTRICT OFFICE (330) 786-2208, EIGHTEEN (18) DAYS PRIOR TO THE BEGINNING OF WORK.
3. LANE RESTRICTIONS OR LANE REDUCTIONS SHALL NOT BE PERMITTED AFTER NORMAL WORKING HOURS. NORMAL WORKING HOURS SHALL BE THOSE HOURS DURING WHICH THE CONTRACTOR HAS A FULL COMPLEMENT OF EMPLOYEES AND EQUIPMENT ACTIVELY REMOVING AND/OR PLACING PAVEMENT MATERIALS.
6. TRUCK MOUNTED ATTENUATORS (TMA'S) SHALL BE USED AS SHOWN IN THE STANDARD CONSTRUCTION DRAWINGS.
7. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE PERMITTED TO HAVE SUCCESSIVE WORK ZONES UNLESS THE DISTANCE BETWEEN THE DRUMS, BARRICADES OR CONES EXCEEDS TWO (2) MILES RURAL OR ONE [1] MILE URBAN.
8. FOR ROUTES NOT ON THE PERMITTED LANE CLOSURE CHART, ONLY DURING OFF-PEAK PERIODS (ie ANY PERIOD OTHER THAN 6-8AM AND 3-6PM) SHALL THE CONTRACTOR INSTALL AND SUBSEQUENTLY RESET ALL TRAFFIC CONTROL NECESSARY FOR THE WORK ZONE FOR EACH CONSTRUCTION PHASE.
9. IN ADDITION TO THE REQUIREMENTS OF 614.11 WORK ZONE PAVEMENT MARKINGS, AT THE END OF EACH DAY OF WORK, THE CONTRACTOR SHALL REPLACE (WITH WORK ZONE MARKINGS) ALL LANE, CENTER, STOP OR CHANNELIZING LINES THAT WERE REMOVED OR COVERED DURING THE PAVEMENT REMOVAL OR PLACEMENT OPERATIONS. QUANTITIES FOR SUCH PLACEMENT ARE CARRIED AS PART OF THE ITEMS LISTED UNDER 614 WORK ZONE PAVEMENT MARKINGS.
10. A QUANTITY OF 10 CU. YDS. OF ITEM 614 ASPHALT CONCRETE FOR MAINTAINING TRAFFIC SHALL BE PROVIDED FOR USE IN MAINTAINING PAVEMENT, SHOULDERS AND OTHER LOCATIONS AS DIRECTED BY THE ENGINEER.
11. PRIOR TO OPENING TO TRAFFIC EACH LANE SHALL BE IN A SAFE, PASSABLE CONDITION. ALL TRANSVERSE JOINTS SHALL EXTEND ACROSS THE FULL LANE AND SHOULDER WIDTH AND EACH LANE SHALL BE FREE FROM UNEVEN LONGITUDINAL JOINTS. THE CONTRACTOR SHALL PROVIDE ASPHALT WEDGES FOR TRANSVERSE JOINTS WHEREVER THERE ARE PAVEMENT ELEVATION DIFFERENCES.
12. THE CONTRACTOR SHALL INSTALL, MAINTAIN AND SUBSEQUENTLY REMOVE WORK ZONE MARKING SIGNS AND THEIR SUPPORTS WITHIN THE WORK LIMITS. THESE SIGNS INCLUDE "NO EDGE LINES", "DO NOT PASS" AND "PASS WITH CARE". ALL OTHER SIGNS WILL BE INCIDENTAL TO THE LUMP SUM PAY ITEM 614 MAINTAINING TRAFFIC UNLESS SEPARATELY ITEMIZED IN THE PLANS. A QUANTITY OF ITEM 614 WORK ZONE MARKING SIGNS HAS BEEN INCLUDED IN THE PLANS AS PER CMS 614.04.

Dropoffs note has been deleted.

MAINTENANCE OF TRAFFIC (CONT...)

13. MAINTENANCE OF TRAFFIC PLANS, NOTES, AND QUANTITIES HAVE BEEN DETAILED TO ADDRESS WORK AREA ACCESS AT SOUTHBOUND SR-8 AND WORK ZONE PROTECTION FOR REPAIRS ALONG THE FLYOVER RAMP FROM NORTHBOUND SR-8 TO NORTHBOUND I-271. IT IS EXPECTED THAT ALL OTHER WORK AREA PROTECTION MAY BE PERFORMED USING APPLICABLE STANDARD CONSTRUCTION DRAWINGS AND SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614, MAINTAINING TRAFFIC.

THE FOLLOWING QUANTITIES SHALL BE USED FOR THE MAINTENANCE OF TRAFFIC ON THIS PROJECT:

614, WORK ZONE LANE LINE, CLASS I, 6", 642 PAINT, 7.49 MILE
614, WORK ZONE CHANNELIZING LINE, CLASS I, 12", 642 PAINT, 4200 FT
614, WORK ZONE MARKING SIGN,(ALL PHASES) 6 EACH

614, WORK ZONE LANE LINE, CLASS III, 642 PAINT, 6", 7.49 MILE
614, WORK ZONE CHANNELIZING LINE, CLASS III, 642 PAINT, 12", 4200 FT

TO BE USED AS DIRECTED BY THE ENGINEER
614, WORK ZONE EDGE LINE, CLASS III, 20.24 MILE

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ADVANCED NOTICE TO PAVE

THE CONTRACTOR SHALL SUBMIT FOR APPROVAL TO THE DISTRICT CONSTRUCTION ENGINEER A DETAILED SCHEDULE 15 DAYS PRIOR TO THE PLACEMENT OF THE OVERLAY COURSES, ON HOW THEY PROPOSE TO PROSECUTE THE PAVING OPERATIONS. THE DETAILS SHALL SHOW THE ORDER OF PERFORMANCE OF EACH STAGE (START TO FINISH) OF THE WORK INCLUDING THE MAINTENANCE OF TRAFFIC THAT WILL BE USED.

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

| NOTIFICATION TIME TABLE | | |
|--|------------------------|--|
| ITEM | DURATION OF CLOSURE | NOTICE DUE TO PERMITS & PIO |
| ROAD & RAMP CLOSURES | >= 2WEEKS | 21 CALENDAR DAYS PRIOR TO CLOSURE |
| | > 12 HOURS & < 2 WEEKS | 14 CALENDAR DAYS PRIOR TO CLOSURE |
| | <12 HOURS | 4 BUSINESS DAYS PRIOR TO CLOSURE |
| LANE CLOSURES & RESTRICTIONS | >=2 WEEKS | 14 CALENDAR DAYS PRIOR TO CLOSURE |
| | < 2 WEEKS | 5 BUSINESS DAYS PRIOR TO CLOSURE |
| START OF CONSTRUCTION & TRAFFIC PATTERNS CHANGES | N/A | 14 CALENDAR DAYS PRIOR TO IMPLEMENTATION |

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

TRAFFIC CONTROL INSPECTOR

THE CONTRACTOR SHALL DESIGNATE AN INDIVIDUAL OTHER THAN THE SUPERINTENDENT AND SUBJECT TO THE APPROVAL OF THE ENGINEER, TO CONTINUOUSLY INSPECT ALL TRAFFIC CONTROL DEVICES WHENEVER CONSTRUCTION WORK IS BEING PERFORMED WITHIN THE WORK LIMITS OF THE PROJECT. THE DESIGNATED INDIVIDUAL SHALL ALSO INSPECT ALL TRAFFIC DEVICES AT THE BEGINNING AND AT THE END OF EACH WORK DAY. THE DESIGNATED INDIVIDUAL OR A QUALIFIED REPRESENTATIVE SHALL ALSO BE AVAILABLE ON AN AROUND THE CLOCK BASIS TO REPAIR AND/OR REPLACE DAMAGED OR MISSING TRAFFIC CONTROL DEVICES. THESE INDIVIDUALS SHALL BE EQUIPPED WITH CELLULAR PHONES AND THEIR NAMES AND PHONE NUMBERS SHALL BE GIVEN TO THE PROJECT ENGINEER AT THE PRE-CONSTRUCTION MEETING. THE DESIGNATED INDIVIDUAL MAY HAVE OTHER CONSTRUCTION RELATED DUTIES AS LONG AS IMMEDIATE ATTENTION IS GIVEN TO TRAFFIC CONTROL. PAYMENT FOR THE SERVICES OF THE TRAFFIC CONTROL INSPECTOR SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614 MAINTAINING TRAFFIC.

ASPHALT PAVING LIMITATION

THE CONTRACTOR SHALL NOT ANTICIPATE OR SCHEDULE PLACING ASPHALT (ASPHALT SURFACE COURSE, ASPHALT INTERMEDIATE COURSE, ASPHALT CONCRETE BASE, ETC.) BETWEEN NOVEMBER 1 AND APRIL 1 WHEN SUBMITTING THEIR INITIAL BAR CHART PROGRESS SCHEDULE TO THE DISTRICT CONSTRUCTION ENGINEER (DCE) AS SPECIFIED IN CMS SECTION 108.02A. THIS LIMITATION SHALL ALSO INCLUDE INITIAL BASE LINE SCHEDULES AND ALL UPDATES IF A CPM SCHEDULE IS REQUIRED.

PLACEMENT OF ASPHALT CONCRETE

TWO-WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT THAT ONE-WAY TRAFFIC WILL BE PERMITTED FOR MINIMUM PERIODS OF TIME CONSISTENT WITH THE REQUIREMENTS OF THE SPECIFICATIONS FOR PROTECTION OF COMPLETED ASPHALT CONCRETE COURSES.

ITEM 614, MAINTAINING TRAFFIC (NOTICE OF CLOSURE SIGN)

NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. [AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK.]

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD/RAMP FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.

| NOTICE OF CLOSURE SIGN TIME TABLE | | |
|-----------------------------------|------------------------|-----------------------------------|
| ITEM | DURATION OF CLOSURE | SIGN DISPLAYED TO PUBLIC |
| ROAD & RAMP CLOSURES | >= 2WEEKS | 14 CALENDAR DAYS PRIOR TO CLOSURE |
| | > 12 HOURS & < 2 WEEKS | 7 CALENDAR DAYS PRIOR TO CLOSURE |
| | <12 HOURS | 2 BUSINESS DAYS PRIOR TO CLOSURE |

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION. THIS IS TO BE A SPECIFIC OFFICE WITHIN THE DISTRICT RATHER THAN THE GENERAL SWITCHBOARD NUMBER.

TIME LIMITATION, TRAFFIC ON A MILLED SURFACE

THE MAXIMUM ALLOWABLE TIME FOR TRAFFIC TO BE PLACED ON A MILLED SURFACE SHALL BE 7 CONSECUTIVE CALENDAR DAYS. SHOULD THE CONTRACTOR FAIL TO MEET THIS REQUIREMENT, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF \$3,000 PER DAY THAT THE TRAFFIC IS PLACED ON A MILLED SURFACE BEYOND THE SPECIFIED LIMIT.



WORK ZONE SPEED ZONES (WZSZS)

THE FOLLOWING WORK ZONE SPEED ZONE (WZSZ) SPEED LIMIT REVISION(S) HAVE BEEN APPROVED FOR USE ON THIS PROJECT WHEN WORK ZONE CONDITIONS AND FACTORS ARE MET AS DESCRIBED BELOW:

| WZSZ REVISION NUMBER | COUNTY-ROUTE | SLM RANGE (DIRECTION) |
|----------------------|--------------|-----------------------------|
| WZ - 26222 | SUM-8 | SLM 15.37 TO SLM 17.21 (NB) |
| WZ - 26222 | SUM-8 | SLM 15.65 TO SLM 17.37 (SB) |

POTENTIAL WZSZ LOCATIONS SHALL HAVE AN ORIGINAL (PRE-CONSTRUCTION) POSTED SPEED LIMIT OF 55 MPH OR GREATER, A QUALIFYING WORK ZONE CONDITION OF AT LEAST 0.5 MILE IN LENGTH, AN EXPECTED WORK DURATION OF AT LEAST THREE HOURS, AND A WORK ZONE CONDITION IN PLACE THAT REDUCES THE EXISTING FUNCTIONALITY OF THE TRAVEL LANES OR SHOULDERS (I.E., LANE CLOSURE, LANE SHIFT, CROSSOVER, CONTRAFLOW AND/OR SHOULDER CLOSURE). THE LENGTH OF THE WORK ZONE CONDITION IS MEASURED FROM THE BEGINNING OF THE TAPER FOR THE SUBJECT WORK ZONE CONDITION IMPACTING THE TRAVEL LANES AND/OR SHOULDER TO THE END OF THE DOWNSTREAM TAPER, WHERE DRIVERS ARE RETURNED TO TYPICAL ALIGNMENT. AN EXPECTED WORK DURATION OF AT LEAST THREE HOURS IS REQUIRED TO BALANCE THE ADDITIONAL EXPOSURE CREATED BY INSTALLING AND REMOVING WZSZ SIGNING WITH THE TIME NEEDED TO COMPLETE THE WORK.

IF THE WORK ZONE MEETS THESE MINIMUM CRITERIA, IT SHALL BE ANALYZED FURTHER USING TABLE 1 BELOW TO DETERMINE IF AND WHEN IT QUALIFIES FOR A SPEED LIMIT REDUCTION. DEPENDING ON THE ORIGINAL POSTED SPEED LIMIT, THE TYPE OF TEMPORARY TRAFFIC CONTROL USED, AND WHETHER OR NOT WORKERS ARE PRESENT, A WARRANTED WZSZ WILL VARY IN THE APPROVED SPEED LIMIT TO BE POSTED OVER TIME.

C&MS ITEM 614, PARAGRAPH 614.02(B), INDICATES THAT TWO DIRECTIONS OF A DIVIDED HIGHWAY ARE CONSIDERED SEPARATE HIGHWAY SECTIONS. THEREFORE, IF THE WORK ON A MULTI-LANE DIVIDED HIGHWAY IS LIMITED TO ONLY ONE DIRECTION, A SPEED LIMIT REDUCTION IN THE DIRECTION OF THE WORK DOES NOT AUTOMATICALLY CONSTITUTE A SPEED LIMIT REDUCTION IN THE OPPOSITE DIRECTION. EACH DIRECTION SHALL BE ANALYZED INDEPENDENTLY FROM EACH OTHER.

ALL WZSZS FLUCTUATE BETWEEN TWO APPROVED REDUCED SPEED LIMITS OR BETWEEN AN APPROVED REDUCED SPEED LIMIT AND THE ORIGINAL POSTED SPEED LIMIT. ONLY ONE OF TWO SIGNING STRATEGIES SHALL BE USED TO IMPLEMENT A WZSZ.

[WZSZS USING DSL SIGN ASSEMBLIES SHALL BE IN ACCORDANCE WITH THIS NOTE, APPROVED LIST, SUPPLEMENTAL SPECIFICATIONS (SS) 808 AND 908, AND TRAFFIC SCD MT-104.10.]

[WZSZS USING TEMPORARY FLATSHEET SPEED LIMIT SIGNS SHALL BE IN ACCORDANCE WITH THIS NOTE AND SCD MT-104.10. ADDITIONALLY PAYMENT MAY BE REMOVED, OR A DISINCENTIVE APPLIED, FOR WZSZS USING TEMPORARY FLATSHEET SPEED LIMIT SIGNS THE SAME AS DESCRIBED IN THE MOST RECENT PUBLICATION OF SS 808 IN REGARDS TO WZSZS USING DSL SIGN ASSEMBLIES (SEE SS 808.06 PARAGRAPHS 4 THROUGH 7, INCLUDING TABLE 1).]

ONLY ONE WARRANTED SPEED LIMIT APPLIES AT ANY ONE TIME; SPEED LIMIT REDUCTIONS ARE NOT CUMULATIVE. WZSZS SHALL NOT BE USED FOR MOVING/MOBILE ACTIVITIES, AS DEFINED IN OMUTCD PART 6.

WORK ZONE SPEED ZONES (WZSZS) (CONT...)

WHEN LOOKING UP THE WARRANTED WORK ZONE SPEED LIMITS, ALWAYS USE THE ORIGINAL, PRECONSTRUCTION, POSTED SPEED LIMIT. DO NOT USE A PRIOR OR CURRENT WORK ZONE SPEED LIMIT AS A LOOK UP VALUE IN THE TABLE. POSITIVE PROTECTION IS GENERALLY REGARDED AS PORTABLE BARRIER OR OTHER RIGID BARRIER IN USE ALONG THE WORK AREA WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WITHOUT POSITIVE PROTECTION IS GENERALLY REGARDED AS USING DRUMS, CONES, SHADOW VEHICLE, ETC., ALONG THE WORK AREA WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WORKERS ARE CONSIDERED AS BEING PRESENT WHEN ON-SITE, WORKING WITHIN THE SUBJECT WARRANTED WORK ZONE CONDITION. WHEN THE WORK ZONE CONDITION REDUCING THE EXISTING FUNCTIONALITY OF THE TRAVEL LANES OR SHOULDERS IS REMOVED, THE SPEED LIMIT DISPLAYED SHALL RETURN TO THE ORIGINAL POSTED SPEED LIMIT.

TABLE 1: WARRANTED WORK ZONE SPEED LIMITS (MPH) FOR WORK ZONES ON HIGH-SPEED (55 MPH OR GREATER) MULTI-LANE HIGHWAYS

| ORIGINAL POSTED SPEED LIMIT | WITH POSITIVE PROTECTION | | WITHOUT POSITIVE PROTECTION | |
|-----------------------------|--------------------------|---------------------|-----------------------------|---------------------|
| | WORKERS PRESENT | WORKERS NOT PRESENT | WORKERS PRESENT | WORKERS NOT PRESENT |
| 70 | 60 | 65 | 55 | 65 |
| 65 | 55 | 60 | 50 | 60 |
| 60 | 55 | 60 | 50 | 60 |
| 55 | 50 | 55 | 45 | 55 |

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 808, DIGITAL SPEED LIMIT (DSL) SIGN ASSEMBLY, 12 SIGN MNTH [ASSUMING 4 DSL SIGN ASSEMBLIES FOR 3 MONTHS]

Work zone speed limit sign quantity has been removed.

ITEM 632 - DETECTOR LOOP, AS PER PLAN

THE CONTRACTOR SHALL CONTACT THE DISTRICT OFFICE (330-786-3146) THREE WORKING DAYS PRIOR TO ANY PLANING OR TRENCHING AT THE INTERSECTION OF I-271 NB OFF RAMP TO SR-8 LOOP DETECTORS DISTURBED BY PAVEMENT PLANING OR TRENCHING SHALL BE ABANDONED IN PLACE. THE LOOP DETECTOR WIRE WILL BE CUT INTO THE PAVEMENT AFTER THE PROPOSED SURFACE COURSE HAS BEEN PLACED. ALL STOP LINE INDUCTANCE DETECTOR LOOPS SHALL BE THE POWERHEAD CONFIGURATION SHOWN ON TC-82.10. THE WIDTH SHALL BE AS SPECIFIED ON TC-82.10 AND THE LENGTH SHALL BE AS SPECIFIED BELOW. THE LOCATION OF THESE LOOPS SHALL BE SUCH THAT THE POWERHEAD IS LOCATED AT THE STOP LINE, NOT PAST IT. ALL DILEMMA ZONE INDUCTANCE DETECTOR LOOPS CALLED FOR IN THE PLANS SHALL BE THE ANGULAR DESIGN DETECTION (ADD) LOOP AS SHOWN ON TC-82.10. DIMENSIONS SHALL BE AS SPECIFIED ON TC-82.10 AND THE LOOP SHALL BE PLACED AT THE SAME LOCATION AS THE EXISTING LOOPS.

THE QUANTITIES LISTED BELOW HAVE BEEN CARRIED TO THE GENERAL SUMMARY. THE NEW LOOP DETECTOR WIRES SHALL BE RUN INTO THE EXISTING CONTROL BOX OR THE EXISTING PULLBOX. INCLUDED IN THIS ITEM IS THE POURED EPOXY TYPE CABLE SPLICE KIT (CONFORMING TO 725.15E) THAT MUST BE USED IN MAKING THESE CONNECTIONS. ALL NECESSARY MATERIAL, LABOR, SPLICE KITS AND EQUIPMENT SHALL BE INCIDENTAL TO PAYMENT OF THESE ITEMS.

632 DETECTOR LOOP, 6'x25', AS PER PLAN, 2 EACH

PERMITTED LANE CLOSURE SCHEDULE (PLCS)

LANE CLOSURE(S) SHALL CONFORM TO THE PLCS. PUBLISHED PLCS INFORMATION CAN BE FOUND ON THE ODOT WEBSITE.

THE MONTHLY PUBLISHED SCHEDULES REQUIRED TO BE USED, FOR EACH PLCS SEGMENT WITHIN THE PROJECT AREA, ARE THOSE THAT COMPRISE THE CONSECUTIVE 12-MONTH PERIOD BEGINNING 15 MONTHS PRIOR TO THE MONTH AND YEAR OF SALE AND ENDING 4 MONTHS PRIOR TO THE MONTH AND YEAR OF SALE. THESE SAME 12 MONTHS APPLY FOR THE LIFE OF THE PROJECT AND SHALL BE APPLIED TO EACH RESPECTIVE MONTH OF CONSTRUCTION (MONTH OF LANE CLOSURE(S) SHALL MATCH MONTH OF PLCS USED). LANE CLOSURE(S) IN PLACE FOR MULTIPLE MONTHS SHALL ALWAYS COMPLY WITH THE CURRENT RESPECTIVE MONTH.

(FOR EXAMPLE: IF THE SALE DATE FOR THE PROJECT WAS MARCH OF 2021, THE MONTHLY PUBLISHED SCHEDULES FOR EACH APPLICABLE PLCS SEGMENT WOULD BE DECEMBER 2019 TO NOVEMBER 2020. IF THIS WAS A THREE-YEAR PROJECT, YEAR THREE WOULD STILL BE USING THE DECEMBER 2019 TO NOVEMBER 2020 MONTHLY SCHEDULES. IF THE PROJECT DESIRED TO

CLOSE TWO LANES IN JUNE 2021, REFERENCE WOULD BE MADE TO THE JUNE 2020 SCHEDULE(S) FOR THE RESPECTIVE PLCS SEGMENT(S). IF THE SAME TWO LANES WERE DESIRED TO BE CLOSED AGAIN IN JULY 2021, REFERENCE WOULD BE MADE TO THE JULY 2020 SCHEDULE(S) FOR THE RESPECTIVE PLCS SEGMENT(S).)

MORE RESTRICTIVE CHANGES TO THE ALLOWABLE LANE CLOSURE HOURS ARE AT THE DISCRETION OF THE ENGINEER IN ORDER TO COMPLY WITH THE TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)).

LESS RESTRICTIVE CHANGES TO THE ALLOWABLE LANE CLOSURE HOURS ARE SUBJECT TO THE TRAFFIC MANAGEMENT IN WORK ZONES POLICY (21-008(P)) AND STANDARD PROCEDURE (123-001(SP)) AND SHALL NOT BE IMPLEMENTED UNTIL, AND UNLESS, APPROVED BY THE PROPER ODOT AUTHORITY. [EXISTING MOT EXCEPTIONS THAT HAVE ALREADY BEEN APPROVED IN ACCORDANCE TO THE TRAFFIC MANAGEMENT IN WORK ZONES POLICY AND STANDARD PROCEDURE ARE DETAILED IN THE APPROVED MAINTENANCE OF TRAFFIC (MOT) POLICY EXCEPTION(S) PLAN NOTE.]

ALLOWABLE LANE CLOSURE HOURS FOR FACILITIES NOT COVERED BY THE PLCS, IF ANY, SHALL BE AS SPECIFIED ELSEWHERE IN THE PLANS.

DELINEATION OF PORTABLE AND PERMANENT BARRIER

BARRIER REFLECTORS AND OBJECT MARKERS SHALL BE INSTALLED ON ALL PORTABLE BARRIER (PB) USED FOR TRAFFIC CONTROL; AND, ON PERMANENT CONCRETE BARRIER (INCLUDING BRIDGE PARAPETS) LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE.

BARRIER REFLECTORS SHALL CONFORM TO C&MS 626, EXCEPT THAT BARRIER REFLECTOR SPACING SHALL BE AT APPROXIMATELY 25-FOOT INTERVALS. OBJECT MARKERS AND THEIR INSTALLATION SHALL CONFORM TO C&MS 614.03 AND SCD MT-101.70 WITH THE MODIFICATION THAT OBJECT MARKER SPACING SHALL BE AT APPROXIMATELY 25-FOOT INTERVALS. WHEN THE PB OR PERMANENT BARRIER (INCLUDING BRIDGE PARAPETS) CONTAINS GLARE SCREEN, ONE SET OF THREE VERTICAL STRIPES OF SHEETING SHALL BE CONSIDERED EQUIVALENT TO AN OBJECT MARKER, ONE-WAY.

INCREASED BARRIER DELINEATION, AS SPECIFIED HEREIN, SHALL BE INSTALLED ON ALL PB AND PERMANENT CONCRETE BARRIER LOCATED WITHIN 5 FEET OF THE EDGE OF THE TRAVELED LANE UNDER EITHER OF THE FOLLOWING CONDITIONS: ALONG TAPERS AND TRANSITION AREAS; OR ALONG CURVES (OUTSIDE ONLY) WITH DEGREE OF CURVATURE GREATER THAN OR EQUAL TO 3 DEGREES.

THE INCREASED BARRIER DELINEATION SHALL CONSIST OF EITHER DELINEATION PANELS OR THE TRIPLE STACKING OF WORK ZONE BARRIER REFLECTORS.

DELINEATION PANELS SHALL CONSIST OF PANELS OF DELINEATION, APPROXIMATELY 34 INCHES LONG AND 6 INCHES WIDE AND SHALL BE "CRIMPED." PANELS SHALL BE INSTALLED AND SPACED PER TRAFFIC SCD MT-101.70.

TRIPLE-STACKED BARRIER REFLECTORS SHALL CONSIST OF ALIGNING THREE BARRIER REFLECTORS VERTICALLY, AT LOCATIONS WHERE A SINGLE BARRIER REFLECTOR WOULD BE OTHERWISE ATTACHED. THERE SHALL BE NO OPEN SPACE BETWEEN THE ADJACENT BARRIER REFLECTORS. THE TRIPLE-STACKED BARRIER REFLECTORS SHALL CONFORM TO C&MS 626, EXCEPT THAT THEY SHALL BE SPACED AT APPROXIMATELY 25-FOOT INTERVALS AND ALIGNED PER TRAFFIC SCD MT-101.70.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE PLANS FOR THE SR-8 SOUTHBOUND LANE CLOSURE (SEE SHEET P.12) AND HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

- ITEM 614, WORK ZONE EDGE LINE, CLASS III, 6", 642 PAINT, 0.27 MILES
- ITEM 614, WORK ZONE DOTTED LINE, CLASS III, 6", 642 PAINT, 600 FEET
- ITEM 614, WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS, (UNIDIRECTIONAL), 1 EACH
- ITEM 614, BARRIER REFLECTOR, TYPE 1 (ONE-WAY OR BI-DIRECTIONAL), 20 EACH
- ITEM 614, OBJECT MARKER, ONE-WAY, 20 EACH
- ITEM 614, INCREASED BARRIER DELINEATION, 50 FEET
- ITEM 622, PORTABLE BARRIER, UNANCHORED, 490 FEET

PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIAL, LABOR, INCIDENTALS AND EQUIPMENT NECESSARY FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING EACH OF THE ABOVE ITEMS.

ALONG RUNS OF INCREASED BARRIER DELINEATION WHERE THIS ITEM IS PROVIDED, THE QUANTITY SHALL BE MEASURED AS THE ENTIRE LENGTH OF THE RUN OF INCREASED BARRIER DELINEATION, INCLUDING THE SPACES BETWEEN THE INDIVIDUAL DELINEATION PANELS OR STACKS OF BARRIER REFLECTORS.

| SR - 8 INTERCHANGE RAMP CLOSURES | | | | | |
|----------------------------------|---------------|----------------------------------|----------|--|--------------------------------------|
| RAMP | PROPOSED WORK | PERMITTED CLOSURE TIMES | DURATION | DETOUR ROUTE | APPROX. NUMBER OF PCMS (SIGN MONTHS) |
| RAMP C | RAMP PAVING | 6:30 PM FRIDAY TO 6:30 AM MONDAY | 3 DAYS | CONTINUE ON SR 8 SB TO E HINES HILL RD. TAKE EXIT TO E HINES HILL RD. TURN LEFT ONTO E HINES HILL RD. TURN LEFT ONTO SR 8 NB. CONTINUE ON SR 8 NB TO RAMP A. | 2 |

PARAPET REPAIRS ON STRUCTURE SUM-8-17.090A

WHEN PERFORMING THE REMOVAL OF A PORTION OF THE PARAPET AND THE INSTALLATION OF THE PILASTER IN THE REPAIRS TO THE OUTSIDE PARAPET, THE CONTRACTOR IS PERMITTED TO REDUCE TRAFFIC ON THE NORTHBOUND STRUCTURE TO A SINGLE LANE FOR SEVEN DAYS AS PER THE DETAIL SHOWN ON SHEETS P.12-P.13. ALL PARAPET REPAIRS REQUIRING THE PLACEMENT OF PORTABLE BARRIER SHALL BE COMPLETED WITHIN THE PERMITTED SEVEN-DAY CLOSURE PERIOD. A QUANTITY OF WORK ZONE PAVEMENT MARKINGS, PORTABLE BARRIER, OBJECT MARKERS, BARRIER REFLECTORS, INCREASED DELINEATION, AND IMPACT ATTENUATOR HAS BEEN PROVIDED FOR REPAIRS TO THE OUTSIDE PARAPET OF THE RAMP FROM NORTHBOUND SR-8 TO NORTHBOUND I-271.

AT THE END OF THE PERMITTED 7-DAY LANE CLOSURE PERIOD, THE CONTRACTOR SHALL REOPEN THE CLOSED LANE AND SHIFT THE PORTABLE BARRIER TO THE OUTSIDE SHOULDER, MAINTAINING A 2' MINIMUM CLEARANCE BETWEEN THE OUTSIDE EDGE LINE AND THE TOE OF THE PORTABLE BARRIER. ALL COSTS ASSOCIATED WITH SHIFTING THE PORTABLE BARRIER TO THE OUTSIDE SHOULDER SHALL BE CONSIDERED INCIDENTAL TO THE PRICE BID FOR ITEM 622, PORTABLE BARRIER, UNANCHORED. PORTABLE BARRIER SHIFTED TO THE SHOULDER SHALL BE PERMITTED TO REMAIN ON THE SHOULDER UNTIL THE PARAPET REPAIRS HAVE CURED. ONCE THE PARAPET REPAIRS HAVE CURED, THE BARRIER SHALL BE REMOVED AND THE SHOULDER OPENED.

WHEN REMOVING THE PORTABLE CONCRETE BARRIER AND ATTENUATOR USED FOR PARAPET REPAIR WORK, THE CONTRACTOR SHALL GROUT ALL HOLES PLACED IN THE BRIDGE DECK IN ACCORDANCE WITH SCD PCB-91. ALL HOLES PLACED IN THE BRIDGE DECK SHALL BE SEALED IN ACCORDANCE WITH SPECIFICATION 512.04. ALL COSTS ASSOCIATED WITH GROUTING AND SEALING THE ANCHOR HOLES NECESSITATED BY THIS WORK SHALL BE CONSIDERED INCIDENTAL TO THE MAINTENANCE OF TRAFFIC ITEMS LISTED BELOW.

LANE CLOSURES FOR WORK OUTSIDE OF THE PERMITTED SEVEN-DAY CLOSURE PERIOD MAY BE PERFORMED IN ACCORDANCE WITH SCD MT-95.30 WITHIN THE HOURS DEFINED IN THE PERMITTED LANE CLOSURE SCHEDULE.

ITEM 614, WORK ZONE EDGE LINE, CLASS III, 6", 642 PAINT, 0.37 MILES
 ITEM 614, WORK ZONE DOTTED LINE, CLASS III, 6", 642 PAINT, 840 FEET
 ITEM 614, BARRIER REFLECTOR, TYPE 1 (ONE-WAY OR BI-DIRECTIONAL), 16 EACH

ITEM 614, WORK ZONE IMPACT ATTENUATOR, 24" WIDE HAZARDS, (UNDIRECTIONAL), 1 EACH

ITEM 614, OBJECT MARKER, ONE-WAY, 16 EACH
 ITEM 614, INCREASED BARRIER DELINEATION, 60 FEET
 ITEM 622, PORTABLE BARRIER, 390 FEET

ITEM 614, MAINTAINING TRAFFIC (LANES OPEN DURING HOLIDAYS OR SPECIAL EVENTS)

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR SPECIAL EVENTS:

NEW YEAR'S (OBSERVED) GENERAL/REGULAR ELECTION DAY (NOV)
 THANKSGIVING CHRISTMAS (OBSERVED)
 MEMORIAL DAY FOURTH OF JULY (OBSERVED)
 LABOR DAY TWIN'S DAY

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR SPECIAL EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY TIME ALL LANES
 OR SPECIAL EVENT MUST BE OPEN TO TRAFFIC

SUNDAY 12:00N FRIDAY THROUGH 6:00 AM MONDAY
 MONDAY 12:00N FRIDAY THROUGH 6:00 AM TUESDAY
 MONDAY (TOTAL SOLAR ECLIPSE)
 12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
 TUESDAY 12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
 TUESDAY (GEN./REG. ELECTION)
 5:00 AM TUESDAY THROUGH 12:00 AM WEDNESDAY
 WEDNESDAY 12:00N TUESDAY THROUGH 6:00 AM THURSDAY
 THURSDAY 12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY
 THURSDAY (THANKSGIVING ONLY)
 6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
 FRIDAY 12:00N THURSDAY THROUGH 6:00 AM MONDAY
 SATURDAY 12:00N FRIDAY THROUGH 6:00 AM MONDAY

DURING THE SAME PERIODS, MAINTAIN PEDESTRIAN ACCESS IF PEDESTRIAN ACCESS WAS PRESENT PRIOR TO CONSTRUCTION.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT (PN 127).

| LANE VALUE CONTRACT | | | |
|---|--|----------------------|---------------------------------|
| DESCRIPTION OF CRITICAL LANE/ RAMP TO BE MAINTAINED | RESTRICTED TIME PERIOD | TIME UNIT | DISINCENTIVE \$ PER TIME PERIOD |
| SR 8 | PER MAINTAINING TRAFFIC NOTE 3 SHEET 8 | PER LANE/ PER MINUTE | \$20 |

DESIGN AGENCY

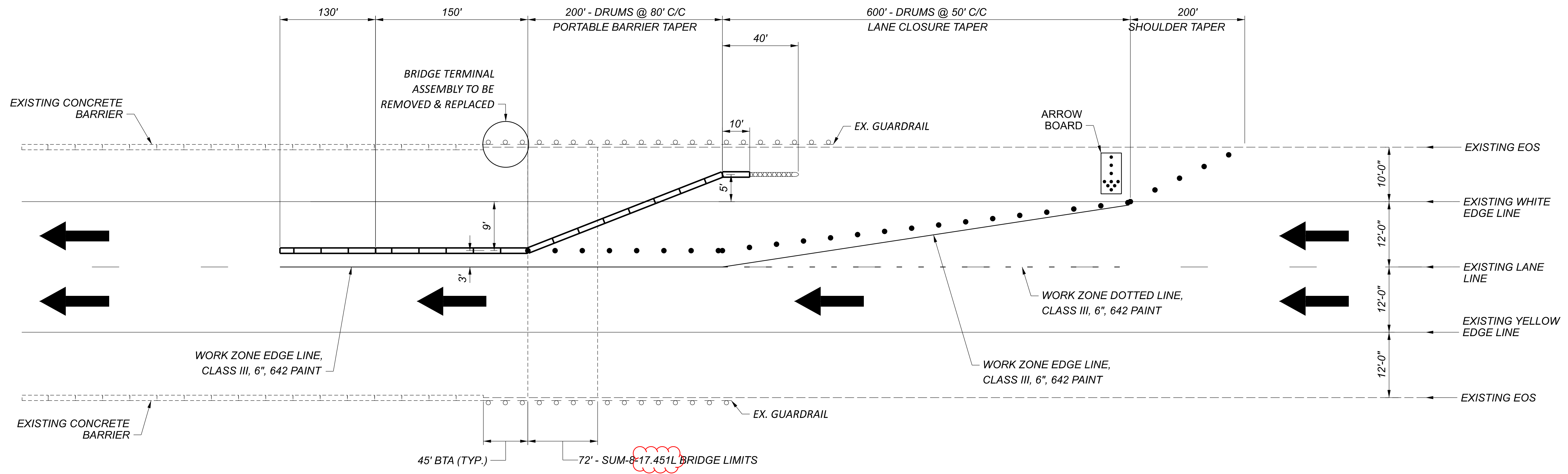


DESIGNER
JF

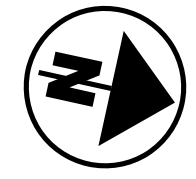
REVIEWER
MJA 10-15-24

PROJECT ID
102330

SHEET TOTAL
P.11 | 33



*SIGN IN ACCORDANCE WITH SCD MT-95.40



HORIZONTAL SCALE IN FEET
 0 NOT TO SCALE

MAINTENANCE OF TRAFFIC SCHEMATIC PLAN -- SR 8 SOUTHBOUND

DESIGN AGENCY



DESIGNER
 JF

REVIEWER
 LB


PROJECT ID
 10-15-24

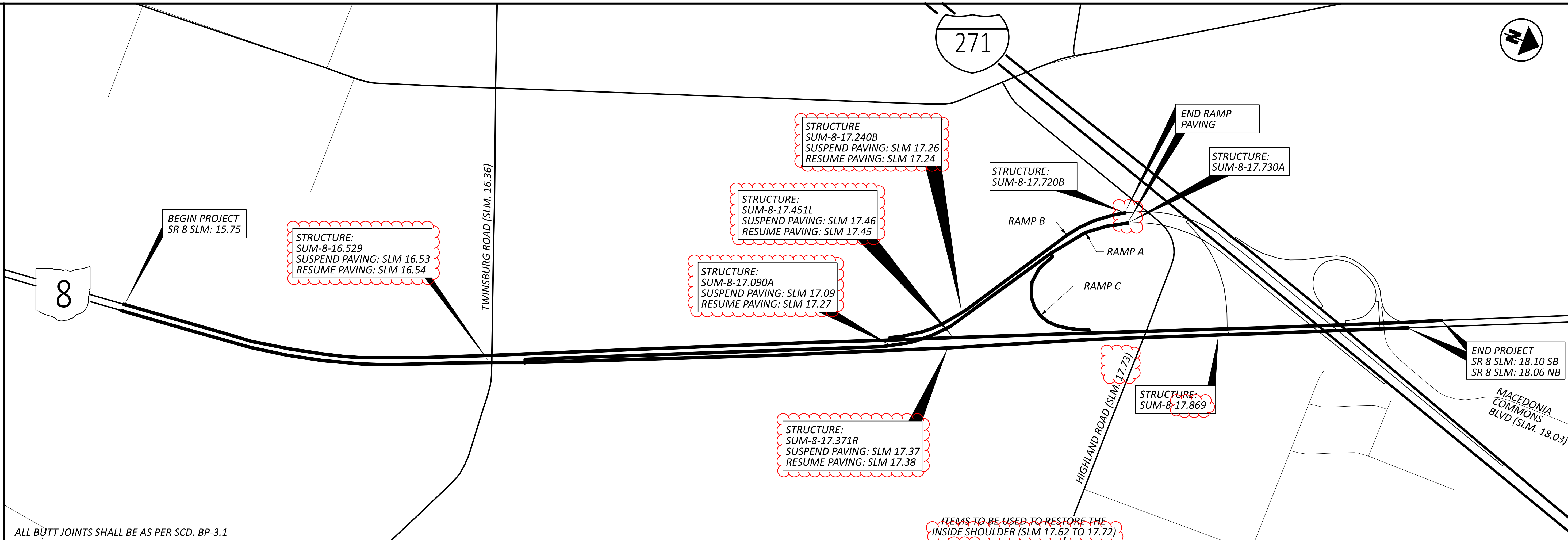
102330

SHEET TOTAL
 P.12 33

| SHEET NUM. | | | | | | | | | | PART. | | | | ITEM | ITEM EXT | GRAND TOTAL | UNIT | DESCRIPTION | SEE SHEET NO. |
|------------------------|----|---|---|----|----|---------|--------|-----|----|-----------|-----------|-----------|-----------|---------|----------|-------------|------|--|---------------|
| 6 | 7 | 8 | 9 | 10 | 11 | 16 | 17 | 21 | 22 | 01/NHS/05 | 02/IMS/47 | 03/NHS/47 | 04/NFP/47 | | | | | | |
| ROADWAY | | | | | | | | | | | | | | | | | | | |
| 41 | | | | | | | | | | 41 | | | | 202 | 98100 | 41 | EACH | REMOVAL MISC: BARRIER REFLECTOR | 6 |
| | | | | | | | 212 | | | 212 | | | | 203 | 10000 | 212 | CY | EXCAVATION | |
| 12 | | | | | | | | | | 12 | | | | 203 | 10000 | 12 | CY | EXCAVATION (FOR PAVEMENT REPAIR) | |
| 110 | | | | | | | | | | 110 | | | | 209 | 60200 | 110 | STA | LINEAR GRADING | |
| | 10 | | | | | | | | | 10 | | | | 623 | 39501 | 10 | EACH | MONUMENT ASSEMBLY ADJUSTED TO GRADE, AS PER PLAN | 7 |
| | LS | | | | | | | | | LS | | | | SPECIAL | 69091000 | LS | | AS-BUILT CONSTRUCTION PLANS | 7 |
| 2 | | | | | | | | | | 2 | | | | SPECIAL | 69098000 | 2 | EACH | VERTICAL CLEARANCE | 6 |
| EROSION CONTROL | | | | | | | | | | | | | | | | | | | |
| 334 | | | | | | | | | | 334 | | | | 659 | 10000 | 334 | SY | SEEDING AND MULCHING | |
| 0.04 | | | | | | | | | | 0.04 | | | | 659 | 20000 | 0.04 | TON | COMMERCIAL FERTILIZER | |
| 0.07 | | | | | | | | | | 0.07 | | | | 659 | 31000 | 0.07 | ACRE | LIME | |
| 1.8 | | | | | | | | | | 1.8 | | | | 659 | 35000 | 1.8 | MGAL | WATER | |
| | | | | | | | | | | 3,000 | | | | 832 | 30000 | 3,000 | EACH | EROSION CONTROL | |
| DRAINAGE | | | | | | | | | | | | | | | | | | | |
| | 10 | | | | | | | | | 10 | | | | 611 | 99655 | 10 | EACH | MANHOLE ADJUSTED TO GRADE, AS PER PLAN | 7 |
| PAVEMENT | | | | | | | | | | | | | | | | | | | |
| 1,800 | | | | | | | | | | 1,800 | | | | 251 | 01000 | 1,800 | SY | PARTIAL DEPTH PAVEMENT REPAIR (441) (LONGITUDINAL) | |
| 200 | | | | | | | | | | 200 | | | | 251 | 01000 | 200 | SY | PARTIAL DEPTH PAVEMENT REPAIR (441) (TRANSVERSE) | |
| 200 | | | | | | | | | | 200 | | | | 253 | 01000 | 200 | SY | PAVEMENT REPAIR | |
| | | | | | | 117,781 | 78,769 | | | 196,550 | | | | 254 | 01000 | 196,550 | SY | PAVEMENT PLANING, ASPHALT CONCRETE (T=1.5") | |
| | | | | | | | 163 | | | 163 | | | | 301 | 56000 | 163 | CY | ASPHALT CONCRETE BASE, PG64-22, (449) | |
| 12 | | | | | | | | | | 12 | | | | 304 | 20000 | 12 | CY | AGGREGATE BASE (FOR PAVEMENT REPAIR) | |
| | | | | | | 10,601 | 7,090 | | | 17,691 | | | | 407 | 20000 | 17,691 | GAL | NON-TRACKING TACK COAT | |
| | | | | | | 2,763 | 1,094 | | | 3,857 | | | | 408 | 10001 | 3,857 | GAL | PRIME COAT, AS PER PLAN @ 0.40 GAL/SY | 6 |
| | | | | | | | 49 | | | 49 | | | | 441 | 70000 | 49 | CY | ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (449), PG64-22 | |
| | | | | | | 2,271 | 1,840 | | | 4,111 | | | | 442 | 00100 | 4,111 | CY | ANTI-SEGREGATION EQUIPMENT | |
| | | | | | | 4,908 | 3,283 | | | 8,191 | | | | 442 | 10331 | 8,191 | CY | ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (447), PWL 2025, AS PER PLAN, PG88-22M (T=1.5") | 6 |
| | | | | | | 206 | 76 | | | 282 | | | | 617 | 10101 | 282 | CY | COMPACTED AGGREGATE, AS PER PLAN (T=1") | 6 |
| WATER WORK | | | | | | | | | | | | | | | | | | | |
| | 2 | | | | | | | | | 2 | | | | 638 | 10801 | 2 | EACH | VALVE BOX ADJUSTED TO GRADE, AS PER PLAN | 7 |
| LIGHTING | | | | | | | | | | | | | | | | | | | |
| | | | | | | | | 4 | | | | | 4 | 625 | 00450 | 4 | EACH | CONNECTION, FUSED PULL APART | |
| | | | | | | | | 3 | | | | | 3 | 625 | 00480 | 3 | EACH | CONNECTION, UNFUSED PERMANENT | |
| | | | | | | | | 1 | | | | | 1 | 625 | 10490 | 1 | EACH | LIGHT POLE, CONVENTIONAL, AT15B41.7 | |
| | | | | | | | | 1 | | | | | 1 | 625 | 10490 | 1 | EACH | LIGHT POLE, CONVENTIONAL, ST15B40 | |
| | | | | | | | | 4 | | | | | 4 | 625 | 10614 | 4 | EACH | LIGHT POLE ANCHOR BOLTS ON STRUCTURE | |
| | | | | | | | | 1 | | | | | 1 | 625 | 14100 | 1 | EACH | LIGHT POLE FOUNDATION, 24" X 8' DEEP | |
| | | | | | | | | 645 | | | | | 645 | 625 | 23200 | 645 | FT | NO. 4 AWG 2400 VOLT DISTRIBUTION CABLE | |
| | | | | | | | | 336 | | | | | 336 | 625 | 23400 | 336 | FT | NO. 10 AWG POLE AND BRACKET CABLE | |
| | | | | | | | | 400 | | | | | 400 | 625 | 24320 | 400 | FT | 1-1/2" DUCT CABLE WITH THREE NO. 4 AWG 2400 VOLT CABLES | |
| | | | | | | | | 8 | | | | | 8 | 625 | 25400 | 8 | FT | CONDUIT, 2", 725.04 | |
| | | | | | | | | 2 | | | | | 2 | 625 | 26252 | 2 | EACH | LUMINAIRE, CONVENTIONAL, SOLID STATE (LED), 115-133 WATT (8500-12000 LUMENS), TYPE III | |
| | | | | | | | | 2 | | | | | 2 | 625 | 29920 | 2 | EACH | STRUCTURE JUNCTION BOX | |
| | | | | | | | | 1 | | | | | 1 | 625 | 32000 | 1 | EACH | GROUND ROD | |


GENERAL SUMMARY

DESIGN AGENCY

 DESIGNER
 JF
 REVIEWER
 MJA 10-15-24
 PROJECT ID
 102330
 SHEET TOTAL
 P.14 33




ALL BUTT JOINTS SHALL BE AS PER SCD. BP-3.1

| SLM RANGE | TYPICAL SECTION | SIDE | DISTANCE (D) | AVERAGE WIDTH (W) | SURFACE AREA (A) A=DxW/9 | CADD GENERATED AREA | 254 | SPECIAL | 408 | 442 | 442 | 617 | | 203 | 301 | 441 | | | | | | | | | |
|--|-----------------|-------|--------------|-------------------|--------------------------|---------------------|---|-----------------------------|---------------------------------------|----------------------------|--|---|------------|---|---|-------|------|------|------|------|------|------|------|------|---|
| | | | | | | | PAVEMENT PLANING, ASPHALT CONCRETE (T=1.5') | ING TACK COAT @ 0.09 GAL/SY | PRIME COAT, AS PER PLAN @ 0.40 GAL/SY | ANTI-SEGREGATION EQUIPMENT | ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (447), PWL 2025, AS PER PLAN, PG88-22M (T=1.5') | COMPACTED AGGREGATE, AS PER PLAN (T=1') | EXCAVATION | ASPHALT CONCRETE BASE, PG64-22, (449) (T=10') | ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (449), PG64-22 (T=3", PLACED IN 2 LIFTS) | | | | | | | | | | |
| FT | FT | SY | SY | SY | GAL | GAL | CY | CY | CY | CY | CY | CY | CY | CY | CY | CY | | | | | | | | | |
| SUM-8-SB | | | | | | | | | | | | | | | | | | | | | | | | | |
| 15.75 | TO | 15.96 | 1 | LT | 1108.80 | 54.00 | 6652.80 | 6652.80 | 598.75 | 98.56 | 123.20 | 277.20 | 6.84 | | | | | | | | | | | | |
| 15.96 | TO | 16.00 | 2 | LT | 211.20 | 54.00 | 1267.20 | 1267.20 | 114.05 | 18.77 | 23.47 | 52.80 | 1.30 | | | | | | | | | | | | |
| 16.00 | TO | 16.53 | 3 | LT | 2798.40 | 54.00 | 16790.40 | 16790.40 | 1511.14 | 248.75 | 310.93 | 699.60 | 17.27 | | | | | | | | | | | | |
| 16.54 | TO | 16.84 | 3 | LT | 1584.00 | 67.00 | 11792.00 | 11792.00 | 1061.28 | 140.80 | 264.00 | 491.33 | 9.78 | | | | | | | | | | | | |
| 16.84 | TO | 17.29 | 4 | LT | 2376.00 | 68.00 | 17952.00 | 17952.00 | 1615.68 | 211.20 | 396.00 | 748.00 | 14.67 | | | | | | | | | | | | |
| 17.29 | TO | 17.32 | 5 | LT | 158.40 | 68.00 | 1196.80 | 1196.80 | 107.71 | 14.08 | 35.20 | 49.87 | 0.98 | | | | | | | | | | | | |
| 17.32 | TO | 17.45 | 6 | LT | 686.40 | 44.00 | 3355.73 | 3355.73 | 302.02 | 61.01 | 152.53 | 139.82 | 4.24 | | | | | | | | | | | | |
| 17.46 | TO | 17.66 | 6 | LT | 1056.00 | 44.00 | 5162.67 | 5162.67 | 464.64 | 93.87 | 234.67 | 215.11 | 6.52 | | | | | | | | | | | | |
| 17.66 | TO | 17.70 | 7 | LT | 211.20 | 36.00 | 844.80 | 844.80 | 76.03 | 18.77 | 46.93 | 35.20 | 1.30 | 211.85 | 162.96 | 48.89 | | | | | | | | | |
| 17.70 | TO | 17.84 | 7 | LT | 739.20 | 52.00 | 4270.93 | 4270.93 | 384.38 | 65.71 | 82.13 | 177.96 | 4.56 | | | | | | | | | | | | |
| 17.84 | TO | 17.87 | 8 | LT | 158.40 | 76.00 | 1337.60 | 1337.60 | 120.38 | 14.08 | 17.60 | 55.73 | 0.98 | | | | | | | | | | | | |
| 17.87 | TO | 17.92 | 8 | LT | 264.00 | 68.00 | 1994.67 | 1994.67 | 179.52 | 23.47 | 29.33 | 83.11 | 1.63 | | | | | | | | | | | | |
| 17.92 | TO | 17.95 | 8 | LT | 158.40 | 46.00 | 809.60 | 809.60 | 72.86 | 14.08 | 17.60 | 33.73 | 0.98 | | | | | | | | | | | | |
| 17.95 | TO | 18.04 | 7 | LT | 475.20 | 50.00 | 2640.00 | 2640.00 | 237.60 | 42.24 | 52.80 | 110.00 | 2.93 | | | | | | | | | | | | |
| 18.04 | TO | 18.10 | 7 | LT | 316.80 | 49.00 | 1724.80 | 1724.80 | 155.23 | 28.16 | 52.80 | 71.87 | 1.96 | | | | | | | | | | | | |
| TURNAROUND @ SLM 17.45 | | | | | | | 108.89 | 108.89 | 9.80 | | | 4.54 | | | | | | | | | | | | | |
| INTERSECTIONS | | | | | 10.00 | VARIABLES | | 868.00 | 868.00 | 78.12 | | 36.17 | | | | | | | | | | | | | |
| SUBTOTALS | | | | | | | 78768.89 | 7089.20 | 1093.55 | 1839.20 | 3282.04 | 75.94 | 0.00 | 211.85 | 162.96 | 48.89 | 0.00 | 0.00 | 0.00 | 0.00 | 0.00 | 0.00 | 0.00 | 0.00 | |
| TOTALS CARRIED TO GENERAL SUMMARY | | | | | | | 78769 | 7090 | 1094 | 1840 | 3283 | 76 | 0 | 212 | 163 | 49 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 | 0 |

DESIGN AGENCY

 DESIGNER
 JF
 REVIEWER
 MJA 10-15-24
 PROJECT ID
 102330
 SHEET TOTAL
 P.17 33

| COUNTY | ROUTE | LOCATION | | 621 RPM (YELLOW/YELLOW) | 621 RPM (WHITE/RED) | 621 RPM (WHITE) | 621 RPM (YELLOW/RED) | 621 RAISED PAVEMENT MARKER REMOVED | REMARKS |
|-----------------------------------|--------|----------|-------|----------------------------|------------------------|--------------------|-------------------------|--|---------|
| | | FROM | TO | | | | | | |
| SUM | 8 NB | 15.75 | 16.68 | | | 82 | | 66 | |
| SUM | 8 NB | 16.68 | 17.57 | | | 40 | | 32 | |
| SUM | 8 NB | 17.61 | 18.06 | | | 20 | | 16 | |
| SUM | 8 SB | 15.75 | 16.12 | | | 17 | | 14 | |
| SUM | 8 SB | 16.12 | 16.61 | | | 44 | | 36 | |
| SUM | 8 SB | 16.61 | 17.07 | | 80 | 63 | | 115 | |
| SUM | 8 SB | 17.07 | 17.56 | | | 22 | | 18 | |
| SUM | 8 SB | 17.59 | 17.71 | | | 12 | | 10 | |
| SUM | 8 SB | 17.71 | 17.88 | | | 8 | | 7 | |
| SUM | 8 SB | 17.88 | 17.92 | | | 4 | | 4 | |
| SUM | 8 SB | 17.92 | 18.10 | | | 8 | | 7 | |
| SUM | RAMP A | 16.86 | 17.32 | | 25 | 21 | 31 | 62 | |
| SUM | RAMP A | 17.50 | 17.85 | | 8 | 16 | | 20 | |
| SUM | RAMP B | | | | | 16 | | 13 | |
| SUM | RAMP C | | | | 8 | | | 7 | |
| TOTALS CARRIED TO GENERAL SUMMARY | | | | | 121 | 373 | 31 | 427 | |

RPM SUBSUMMARY

DESIGN AGENCY

 DESIGNER
 JF
 REVIEWER
 MJA 10-15-24
 PROJECT ID
 102330
 SHEET TOTAL
 P.18 | 33

| STRUCTURE FILE NO. (SFN) | EXPRESSWAY / FREEWAY STRUCTURE ID INFO | INTERSECTING ROADWAY STRUCTURE ID INFO | APPROACH DIRECTION (NB, SB, EB, WB) | SIDE OF ROADWAY (LT, RT) | GENERAL | | MAINLINE FREEWAY/EXPRESSWAY | | | | ROADWAY OVER EXPRESSWAY/FREEWAY | | | | ROADWAY UNDER EXPRESSWAY/FREEWAY | | | | |
|--|--|--|-------------------------------------|--------------------------|---|---|-----------------------------|------------------|------------------|------------------------------------|---------------------------------|--------------------------|------------------|------------------|------------------------------------|--------------------------|------------------|------------------|------------------------------------|
| | | | | | 630 | 630 | 630 | 630 | 630 | 630 | 630 | 630 | 630 | 630 | 630 | 630 | 630 | 630 | 630 |
| | | | | | REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL | REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL | SIGN, FLAT SHEET, 730.20 | SIGN, FLAT SHEET | SIGN, FLAT SHEET | GROUND MOUNTED SUPPORT, NO. 2 POST | SIGN, FLAT SHEET, 730.20 | SIGN, FLAT SHEET, 730.20 | SIGN, FLAT SHEET | SIGN, FLAT SHEET | GROUND MOUNTED SUPPORT, NO. 2 POST | SIGN, FLAT SHEET, 730.20 | SIGN, FLAT SHEET | SIGN, FLAT SHEET | GROUND MOUNTED SUPPORT, NO. 2 POST |
| EACH | EACH | SF | SF | SF | FT | SF | SF | SF | SF | FT | SF | SF | SF | FT | | | | | |
| 7700792 | SUM-8-17.720B | SUM-CR-111-2.783 | SB | RT | 1 | 1 | | | | | | | | | | | | | |
| 7700792 | SUM-8-17.720B | SUM-CR-111-2.783 | EB | LT | 1 | 1 | | | | | | | | | | 3 | | 11 | |
| 7700792 | SUM-8-17.720B | SUM-CR-111-2.783 | EB | RT | 1 | 1 | | | | | | | | | 1 | | 3 | 11 | |
| 7700792 | SUM-8-17.720B | SUM-CR-111-2.783 | WB | LT | 1 | 1 | | | | | | | | | | 3 | | 11 | |
| 7700792 | SUM-8-17.720B | SUM-CR-111-2.783 | WB | RT | 1 | 1 | | | | | | | | | 1 | | 3 | 11 | |
| 7700717 | SUM-8-16.529 | SUM-CR-112-1.700 | NB | RT | 1 | 1 | 1 | 3 | | | | | | | | | | | |
| 7700717 | SUM-8-16.529 | SUM-CR-112-1.700 | SB | RT | 1 | 1 | 1 | | 3 | | | | | | | | | | |
| 7700717 | SUM-8-16.529 | SUM-CR-112-1.700 | EB | LT | 1 | 1 | | | | | | | | | | 3 | | 11 | |
| 7700717 | SUM-8-16.529 | SUM-CR-112-1.700 | EB | RT | 1 | 1 | | | | | | | | | 1 | | 3 | 11 | |
| 7700717 | SUM-8-16.529 | SUM-CR-112-1.700 | WB | LT | 1 | 1 | | | | | | | | | | 3 | | 11 | |
| 7700717 | SUM-8-16.529 | SUM-CR-112-1.700 | WB | RT | 1 | 1 | | | | | | | | | 1 | | 3 | 11 | |
| 7700709 | SUM-8-17.371R | | NB | LT | 1 | 1 | 1 | 3 | | | | | | | | | | | |
| 7700709 | SUM-8-17.371R | | NB | RT | 1 | 1 | 1 | | 3 | | | | | | | | | | |
| 7700806 | SUM-8-17.451L | | SB | LT | 1 | 1 | 1 | 3 | | | | | | | | | | | |
| 7700806 | SUM-8-17.451L | | SB | RT | 1 | 1 | 1 | | 3 | | | | | | | | | | |
| 7700725 | SUM-8-17.869 | | NB | LT | 1 | 1 | | | | | | | | | | | | | |
| 7700725 | SUM-8-17.869 | | NB | RT | 1 | 1 | 1 | 3 | | | | | | | | | | | |
| 7700725 | SUM-8-17.869 | | SB | LT | 1 | 1 | | | | | | | | | | | | | |
| 7700725 | SUM-8-17.869 | | SB | RT | 1 | 1 | 1 | | 3 | | | | | | | | | | |
| 7700784 | *SUM-8-17.240B | | SB | RT | | | 1 | | | | | | | | | | | | |
| 7700768 | *SUM-8-17.090A | | NB | RT | | | 1 | | | | | | | | | | | | |
| 7700776 | SUM-8-17.730A | SUM-CR-111-2.783 | NB | RT | 1 | 1 | 1 | | | | | | | | | | | | |
| 7700776 | SUM-8-17.730A | SUM-CR-111-2.783 | EB | LT | 1 | 1 | | | | | | | | | | 3 | | 11 | |
| 7700776 | SUM-8-17.730A | SUM-CR-111-2.783 | EB | RT | 1 | 1 | | | | | | | | | 1 | | 3 | 11 | |
| 7700776 | SUM-8-17.730A | SUM-CR-111-2.783 | WB | LT | 1 | 1 | | | | | | | | | | 3 | | 11 | |
| 7700776 | SUM-8-17.730A | SUM-CR-111-2.783 | WB | RT | 1 | 1 | | | | | | | | | 1 | | 3 | 11 | |
| TOTALS CARRIED TO GENERAL SUMMARY | | | | | 24 | 24 | 12 | 12 | 12 | 107 | | | | | | 6 | 18 | 18 | 132 |
| | | | | | NOTE 1 | NOTE 2 | NOTE 3 | | | | | NOTE 1 | NOTE 4 | NOTE 2 | NOTE 3 | NOTE 1 | NOTE 2 | NOTE 3 | |

*SIGNS FOR STRUCTURES SUM-8-19.090A AND SUM-8-17.240B SHALL BE INSTALLED ON THE PARAPET AT THE EXPANSION JOINT.

| | |
|---------------|---|
| NOTE 1 | I-h25b, MOUNTED UNDER OM-3R IF SPECIFIED, USE EXPRESSWAY / FREEWAY STRUCTURE INFO |
| NOTE 2 | OM-3L |
| NOTE 3 | OM-3R |
| NOTE 4 | I-h25b, MOUNTED UNDER MAINLINE STRUCTURE ID SIGN, USE INTERSECTING ROADWAY STRUCTURE INFO |

STRUCTURE ID SIGNS

| PROPOSED WORK | BRIDGE NUMBER / STRUCTURE FILE NUMBER | | | | | | | |
|--|---------------------------------------|--------------------------|--------------------------|-------------------------|--------------------------|--------------------------|--------------------------|--------------------------|
| | SUM-8-16.529 7700717 | SUM-8-17.371R 7700709 | SUM-8-17.451L 7700806 | SUM-8-17.869 7700725 | SUM-8-17.720B 7700792 | SUM-8-17.240B 7700784 | SUM-8-17.090A 7700768 | SUM-8-17.730A 7700776 |
| DECK SEALING WITH GRAVITY FED RESIN -SEAL EXISTING WEARING SURFACE & APPROACH SLABS | X | X | X | | X | X | X | X |
| CONCRETE APPROACH SLAB PATCHING -PATCH AREAS OF THE CONCRETE WEARING SURFACE THAT ARE VISIBLY UNSOUND | | | | | X | | X | |
| ASPHALT OVERLAY -PAVE OVER STRUCTURE WITH MAINLINE PAVING OPERATION, SEE P.20-P.21 | | | | X | | | | |
| SUBSTRUCTURE PATCHING (OUTSIDE LIMITS OF ACCIDENT REPAIR) -PATCH ALL UNSOUND AREAS OF THE SUBSTRUCTURE / SEAL PATCHES WITH EPOXY-URETHANE | | X | X | | | | | |
| PATCHING CONCRETE RAILING (OUTSIDE LIMITS OF ACCIDENT REPAIR) -PATCH ALL SPALLED OR UNSOUND AREAS / SEAL CONCRETE PATCHES WITH EPOXY-URETHANE | | | | | | | X | |
| SEALING OF CONCRETE RAILING (OUTSIDE LIMITS OF ACCIDENT REPAIR) -REMOVE EXISTING SEALER / SEAL THE CONCRETE RAILING WITH EPOXY-URETHANE | X | | | | X | X | X | X |
| CHANNEL CLEANOUT -REMOVE ALL VEGETATION, BUILT-UP SILT, & DEBRIS WITHIN THE CHANNEL | | X | | X | | | | |
| SCOUR REPAIR -REPAIR SCOUR AT THE INLET AND OUTLET WITH 601 - ROCK CHANNEL PROTECTION, TYPE B | | | | X | | | | |
| CLEARING & GRUBBING, AS PER PLAN, AROUND BRIDGES/STRUCTURES/CULVERTS | X | X | X | X | X | X | | X |

DESIGN SPECIFICATIONS

THIS STRUCTURE CONFORMS TO THE "LRFD BRIDGE DESIGN SPECIFICATIONS" ADOPTED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS, 17TH EDITION, INCLUDING THE 2012 INTERIM SPECIFICATIONS. AND THE ODOT BRIDGE DESIGN MANUAL, 2020.

EXISTING STRUCTURE VERIFICATION

DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURE HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURE AND FROM FIELD OBSERVATIONS AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURE AND THE PROPOSED WORK BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO C&MS, SECTIONS 102.05, 105.02, AND 513.04*. BASE CONTRACT BID PRICES UPON A RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PREBID EXAMINATION OF THE EXISTING STRUCTURE. HOWEVER, THE DEPARTMENT WILL PAY FOR ALL PROJECT WORK BASED UPON ACTUAL DETAILS AND DIMENSIONS THAT HAVE BEEN VERIFIED IN THE FIELD.

ITEM 201 - CLEARING AND GRUBBING, AS PER PLAN, AROUND BRIDGES/STRUCTURES/CULVERTS

ALTHOUGH NO TREES OR STUMPS ARE SPECIFICALLY MARKED FOR REMOVAL WITHIN THE PLANS, A LUMP SUM QUANTITY IS INCLUDED IN THE STRUCTURE GENERAL SUMMARY FOR ITEM 201 – CLEARING AND GRUBBING, AS PER PLAN, AROUND BRIDGES/STRUCTURES/CULVERTS. SCALPING IS NOT REQUIRED FOR THIS ITEM OF WORK. ALL VEGETATION SHALL BE REMOVED WITHIN 15 FEET (OR TO THE R/W LIMITS, WHICHEVER IS CLOSER) OF THE HEADWALLS, ABUTMENTS AND/OR PIERS.

ALL OTHER PROVISIONS AS SET FORTH IN THE CMS UNDER THIS ITEM ARE INCLUDED IN THE LUMP SUM BID PRICE FOR ITEM 201 – CLEARING AND GRUBBING, AS PER PLAN, AROUND BRIDGES/STRUCTURES/CULVERTS.

ITEM 202 - REMOVAL MISC.: CHANNEL CLEANOUT

THIS WORK WILL CONSIST OF RE-ESTABLISHING THE ORIGINAL CHANNEL PROFILE BY REMOVING SEDIMENT BUILDUP, VEGETATION, AND DEBRIS FROM THE EXISTING CHANNEL WITHIN STATE RIGHT-OF-WAY LIMITS, INCLUDING UNDER THE BRIDGE OR THE INSIDE OF THE CIRCULAR, ELLIPTICAL, OR BOX CULVERT, AS SPECIFIED IN THE PLANS FOR STRUCTURE(S) SUM-8-17.869. ANY TREES LOCATED WITHIN CHANNEL OR BANK LIMITS WILL BE INCLUDED UNDER ITEM 201, CLEARING AND GRUBBING. ALL MATERIALS REMOVED SHALL BE DISPOSED OF IN ACCORDANCE WITH 105.16 AND 105.17 OF THE CMS WITH THE APPROVAL OF THE ENGINEER. NO AREAS OF EXISTING CHANNEL PROTECTION SHALL BE REMOVED IN ORDER TO RESTORE THE ORIGINAL CHANNEL PROFILE. AFFECTED CHANNEL AREAS SHALL BE CLEANED OUT TO THE SATISFACTION OF THE ENGINEER.

CHANNEL CLEANOUT WILL BE PAID FOR AT THE UNIT PRICE BID FOR ITEM 202 REMOVAL MISC.: CHANNEL CLEANOUT. THIS PRICE WILL INCLUDE THE COST FOR LABOR, EQUIPMENT, AND ALL INCIDENTALS REQUIRED TO COMPLETE THE CHANNEL CLEANOUT.

EROSION AND SCOUR REPAIR

THE QUANTITIES BELOW SHALL BE USED TO REPAIR SCOUR/EROSION AT THE SPECIFIED LOCATIONS:

SUM-8-17.869 (SCOUR AT INLET AND OUTLET)
 601, ROCK CHANNEL PROTECTION, TYPE B WITHOUT FILTER, 2 CY

ITEM 519 - PATCHING CONCRETE STRUCTURES, AS PER PLAN

PRIOR TO THE SURFACE CLEANING SPECIFIED IN C&MS 519.04 AND WITHIN 24 HOURS OF PLACING PATCHING MATERIAL, BLAST CLEAN ALL SURFACES TO BE PATCHED INCLUDING THE EXPOSED REINFORCING STEEL. ACCEPTABLE METHODS INCLUDE: HIGH-PRESSURE WATER BLASTING WITH, OR WITHOUT, ABRASIVES IN THE WATER, ABRASIVE BLASTING WITH CONTAINMENT OR VACUUM ABRASIVE BLASTING.

LISTED BELOW ARE THE LOCATIONS AND QUANTITIES TO BE USED AT EACH STRUCTURE:

SUM-8-17.371R (ABUTMENTS)
 -ITEM 519, PATCHING CONCRETE STRUCTURES, AS PER PLAN, 50 SF

SUM-8-17.451L (ABUTMENTS & PIER CAPS)
 -ITEM 519, PATCHING CONCRETE STRUCTURES, AS PER PLAN, 50 SF

SUM-8-17.090A (PARAPETS AT REAR RIGHT APPROACH)
 -ITEM 519, PATCHING CONCRETE STRUCTURES, AS PER PLAN, 25 SF

STRUCTURE NOTES

SUM-8-16.529, SUM-8-17.371R, SUM-8-17.451L, SUM-8-17.869
 SUM-8-17.720B, SUM-8-17.240B, SUM-8-17.090A, SUM-8-17.730A

SFN
 VARIOUS
 DESIGN AGENCY



DESIGNER: JF
 CHECKER: MJA

REVIEWER: TJP 10-15-24

PROJECT ID: 102330

SUBSET TOTAL
 1 5


SHEET TOTAL
 P.23 33

CALC: JF DATE: 3/27/2024
 CHECKED: MJA DATE: 7/15/2024

| ESTIMATED QUANTITIES | | | | | | | | | | | | |
|---------------------------------------|---------------------------------------|--|--|---------------------------------------|---------------------------------------|---------------------------------------|---------------------------------------|------|-----------|------|--|-----------|
| BRIDGE NO. / STRUCTURE FILE NO. | | | | | | | | ITEM | EXTENSION | UNIT | DESCRIPTION | SEE SHEET |
| SUM-8-17.720B 7700792 02/IMS/47 | SUM-8-16.529 7700717 03/INHS/47 | SUM-8-17.371R 7700769 03/INHS/47 | SUM-8-17.451L 7700806 03/INHS/47 | SUM-8-17.869 7700725 03/INHS/47 | SUM-8-17.090A 7700768 02/IMS/47 | SUM-8-17.240B 7700784 02/IMS/47 | SUM-8-17.730A 7700776 02/IMS/47 | | | | | |
| LS | | | | LS | | | | 201 | 11001 | | CLEARING AND GRUBBING, AS PER PLAN, AROUND BRIDGES/STRUCTURES/CULVERTS | 1 / 5 |
| | | | | LS | | | | 202 | 98000 | | REMOVAL MISC.: CHANNEL CLEANOUT | 1 / 5 |
| 939 | 211 | | | | 1145 | 172 | 967 | 512 | 10100 | SY | SEALING OF CONCRETE SURFACES (EPOXY-URETHANE) | |
| 4194 | 1689 | 683 | 683 | | 5114 | 679 | 4066 | 512 | 73500 | SY | TREATING CONCRETE BRIDGE DECKS WITH GRAVITY FED RESIN | |
| 939 | 211 | | | | 1145 | 172 | 967 | 512 | 74000 | SY | REMOVAL OF EXISTING COATINGS FROM CONCRETE SURFACES | |
| 2462 | 961 | 369 | 369 | | 3003 | 550 | 2387 | 512 | 74500 | FT | REMOVAL OF EXISTING PAVEMENT MARKING | |
| | | 50 | 50 | | 25 | | | 519 | 11101 | SF | PATCHING CONCRETE STRUCTURE, AS PER PLAN | 1 / 5 |
| 2 | | | | | 2 | | | 519 | 12304 | SY | PATCHING CONCRETE BRIDGE DECK - TYPE C | |
| | | | | 2 | | | | 601 | 34100 | CY | ROCK CHANNEL PROTECTION, TYPE B WITHOUT FILTER | |

ESTIMATED QUANTITIES
 SUM-8-16.529, SUM-8-17.371R, SUM-8-17.451L, SUM-8-17.869
 SUM-8-17.720B, SUM-8-17.240B, SUM-8-17.090A, SUM-8-17.730A


SFN
 VARIOUS
 DESIGN AGENCY

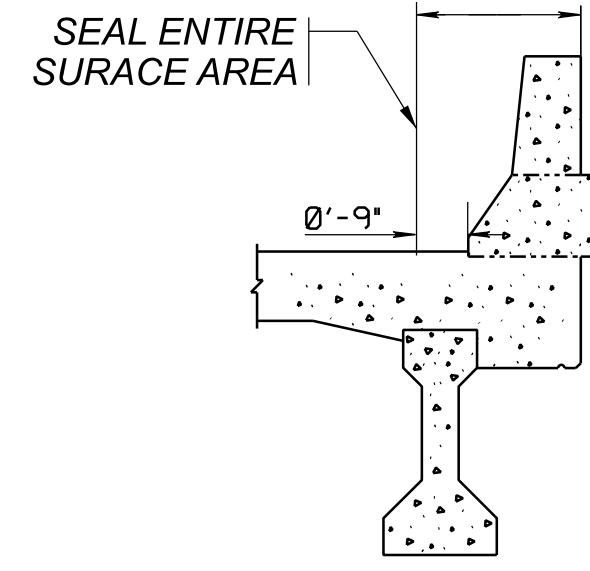


DESIGNER: JF CHECKER: MJA
 REVIEWER: TJP 10-15-24
 PROJECT ID: 102330
 SUBSET: 2 TOTAL: 5
 SHEET: P.24 TOTAL: 33

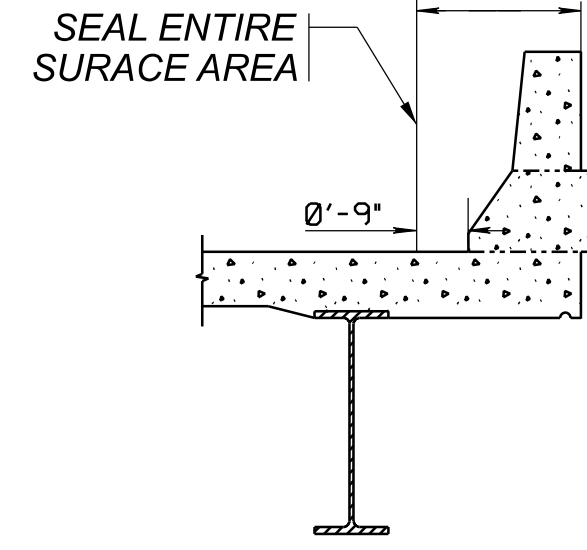
| BRIDGE NUMBER | BRIDGE DECK | | | | | | | | | | APPROACH SLABS | | | | | | | | | | | | | |
|---------------|------------------------|--------------|-----------|---|--------------------------------------|---|--------------------------------------|----|----|-------|---------------------------|----|----|----|----|---|--------------------------------------|--|--|---------|--|--|--|--|
| | LENGTH (BRIDGE LIMITS) | BRIDGE WIDTH | DECK AREA | 512 | | 512 | | | | | | | | | | 512 | | | | SPECIAL | | | | |
| | | | | TREATING CONCRETE BRIDGE DECKS WITH GRAVITY FED RESIN | REMOVAL OF EXISTING PAVEMENT MARKING | TREATING CONCRETE BRIDGE DECKS WITH GRAVITY FED RESIN | REMOVAL OF EXISTING PAVEMENT MARKING | | | | | | | | | TREATING CONCRETE BRIDGE DECKS WITH GRAVITY FED RESIN | REMOVAL OF EXISTING PAVEMENT MARKING | | | | | | | PATCHING CONCRETE BRIDGE DECK - TYPE C |
| FT | FT | SQ YD | SY | FT | SY | FT | SY | FT | FT | SQ YD | APPROACH (FORWARD / REAR) | SY | FT | SY | FT | SY | | | | | | | | |
| SUM-8-17.720B | 770.50 | 46.00 | 3938.11 | 3938.11 | 2312 | | | | | | | | | | | | | | | | | | | |
| SUM-8-16.529 | 70.08 | 126.55 | 985.38 | 985.38 | 561 | | | | | | | | | | | | | | | | | | | |
| SUM-8-17.371R | 72.84 | 50.00 | 404.67 | 404.67 | 219 | | | | | | | | | | | | | | | | | | | |
| SUM-8-17.451L | 72.84 | 50.00 | 404.67 | 404.67 | 219 | | | | | | | | | | | | | | | | | | | |
| SUM-8-17.090A | 950.50 | 46.00 | 4858.11 | 4858.11 | 2852 | | | | | | | | | | | | | | | | | | | |
| SUM-8-17.240B | 123.43 | 48.00 | 658.29 | 658.29 | 370 | | | | | | | | | | | | | | | | | | | |
| SUM-8-17.730A | 795.50 | 46.00 | 4065.89 | 4065.89 | 2387 | | | | | | | | | | | | | | | | | | | |

STRUCTURE DETAILS
 SUM-8-16.529, SUM-8-17.371R, SUM-8-17.451L
 SUM-8-17.720B, SUM-8-17.240B, SUM-8-17.090A, SUM-8-17.730A

SFN
 VARIOUS
 DESIGN AGENCY

 DESIGNER: JF CHECKER: MJA
 REVIEWER: TJP 10-15-24
 PROJECT ID: 102330
 SUBSET: 3 TOTAL: 5
 SHEET: P.25 TOTAL: 33



DETAIL A
 CONCRETE DECKS WITH
 DEFLECTOR PARAPET



DETAIL B
 CONCRETE DECK WITH
 DEFLECTOR PARAPET

| BRIDGE NUMBER | SEALING PAY ITEM | STRUCTURE TYPE | PROPOSED SEALING | FEDERAL COLOR NUMBER | ESTIMATED QUANTITIES | | | | |
|---------------|--|---------------------------------|---|----------------------|----------------------|--------------|---------------|-----------------|---------------|
| | | | | | ABUT (SQ YD) | PIER (SQ YD) | SUPER (SQ YD) | GENERAL (SQ YD) | TOTAL (SQ YD) |
| SUM-8-16.529 | ITEM 512 - SEALING OF CONCRETE SURFACES (EPOXY-URETHANE) | PRESTRESSED CONCRETE MULTI-BEAM | SEAL THE CONCRETE RAILING AS PER DETAIL A | MATCH EXISTING | | | 211 | | 211 |
| SUM-8-17.720B | ITEM 512 - SEALING OF CONCRETE SURFACES (EPOXY-URETHANE) | STEEL CONTINUOUS MULTI-BEAM | SEAL THE CONCRETE RAILING AS PER DETAIL B | MATCH EXISTING | | | 939 | | 939 |
| SUM-8-17.090A | ITEM 512 - SEALING OF CONCRETE SURFACES (EPOXY-URETHANE) | STEEL CONTINUOUS MULTI-BEAM | SEAL THE CONCRETE RAILING AS PER DETAIL B | MATCH EXISTING | | | 1145 | | 1145 |
| SUM-8-17.240B | ITEM 512 - SEALING OF CONCRETE SURFACES (EPOXY-URETHANE) | STEEL CONTINUOUS MULTI-BEAM | SEAL THE CONCRETE RAILING AS PER DETAIL B | MATCH EXISTING | | | 172 | | 172 |
| SUM-8-17.730A | ITEM 512 - SEALING OF CONCRETE SURFACES (EPOXY-URETHANE) | STEEL CONTINUOUS MULTI-BEAM | SEAL THE CONCRETE RAILING AS PER DETAIL B | MATCH EXISTING | | | 967 | | 967 |
| | | | | | | | | | |
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STRUCTURE SEALING DETAILS
 SUM-8-16.529, SUM-8-17.720B, SUM-8-17.090A,
 SUM-8-17.240B & SUM-8-17.730A

SFN
 VARIOUS
 DESIGN AGENCY

DESIGNER: JF
 CHECKER: MJA
 REVIEWER: TJP
 DATE: 10-15-24
 PROJECT ID: 102330
 SUBSET: 4 | TOTAL: 5
 SHEET: P.26 | TOTAL: 33

 CHANNEL CLEANOUT

FOR REFERENCE ONLY

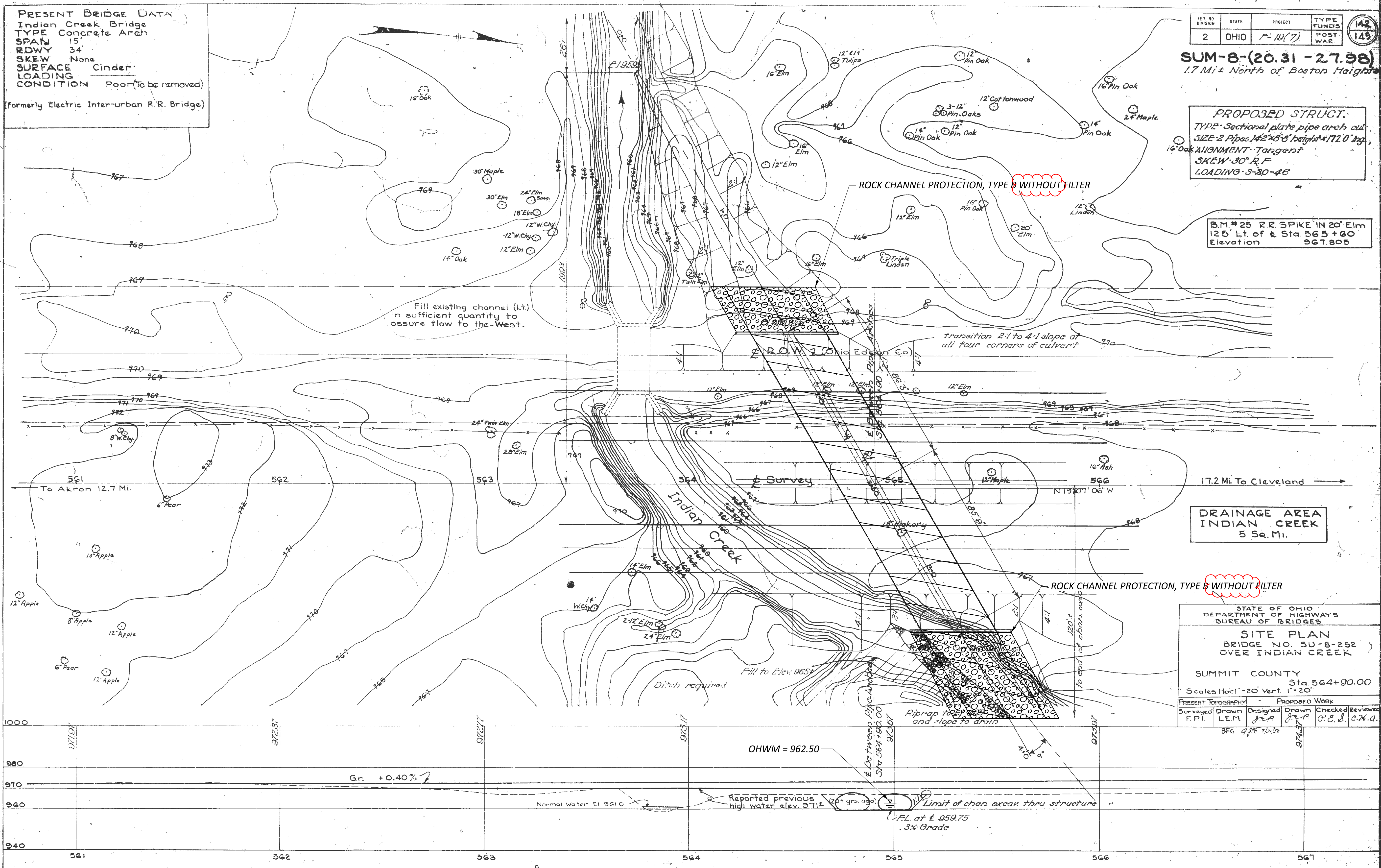
PRESENT BRIDGE DATA
 Indian Creek Bridge
 TYPE Concrete Arch
 SPAN 15'
 RDWY 34'
 SKEW None
 SURFACE Cinder
 LOADING
 CONDITION Poor (to be removed)
 (formerly Electric Inter-urban R.R. Bridge)

| | | | |
|-------------------|-------|---------|------------|
| FED. NO. DIVISION | STATE | PROJECT | TYPE FUNDS |
| 2 | OHIO | F-19(7) | POST WAR |

SUM-8-(20.31 - 27.98)
 1.7 Mi ± North of Boston Heights

PROPOSED STRUCT.
 TYPE Sectional plate pipe arch culvert
 SIZE 2 Pipes 142"x8'8" height x 172.0' long
 ALIGNMENT Tangent
 SKEW 30° R.F.
 LOADING S-20-46

B.M. #25 RR SPIKE IN 20' Elm
 125' Lt. of Sta. 565 + 60
 Elevation 967.805



DRAINAGE AREA
 INDIAN CREEK
 5 Sq. Mi.

STATE OF OHIO
 DEPARTMENT OF HIGHWAYS
 BUREAU OF BRIDGES
SITE PLAN
 BRIDGE NO. SU-8-252
 OVER INDIAN CREEK

SUMMIT COUNTY
 Sta 564+90.00
 Scales Hor: 1"=20' Vert: 1"=20'

| | | | |
|--------------------|---------------|----------|------------------|
| PRESENT TOPOGRAPHY | PROPOSED WORK | | |
| Surveyed | Drawn | Designed | Checked/Reviewed |
| F.P.I. | LEM | JEP | P.E. & C.H.A. |


BFG 9/14/59

SUM-8-15.75

MODEL: Sheet_SurvF13 PAPER: SIZE: 34x22 (in.) DATE: 1/24/2025 TIME: 7:06:50 AM USER: fitzsim
 pw:\ohiodot-pw\entire\com\ohiodot-pw-02\Documents\01_Active Projects\District 04\Summit\102330\400_Engineering\Structures\SFN_7700717_Sheets\102330_SFN_7700717_SM001.dgn

CHANNEL CLEANOUT DETAIL
 SUM-8-17.869
 INDIAN CREEK

SFN 7700725

| | |
|---------------|---|
| DESIGN AGENCY |  |
| DESIGNER | CHECKER |
| JF | MJA |
| REVIEWER | |
| TJP 10-15-24 | |
| PROJECT ID | |
| 102330 | |
| SUBSET | TOTAL |
| 5 | 5 |
| SHEET | TOTAL |
| P.27 | 33 |

STANDARD DRAWINGS AND SUPPLEMENTAL SPECIFICATIONS

REFER TO THE FOLLOWING STANDARD BRIDGE DRAWING(S):

SBR-1-20 DATED (REVISED) 7/21/2023
HL-20.14 DATED (REVISED) 4/17/2020

AND TO THE FOLLOWING SUPPLEMENTAL SPECIFICATION(S):

843 DATED 1/19/2024

PROPOSED WORK

SUM-8-17.451L

-REMOVE ALL CONTAMINATION FROM THE REAR ABUTMENT, PIERS, DECK UNDERSIDE, AND LEFT DECK EDGE IN ACCORDANCE WITH ITEM 512 – REMOVAL OF EXISTING COATINGS FROM CONCRETE SURFACES

-PATCH THE REAR ABUTMENT, LEFT DECK EDGE, AND WINGWALL AT THE REAR LEFT CORNER OF THE BRIDGE USING ITEM 519 – PATCHING CONCRETE STRUCTURE, AS PER PLAN AND SEAL ALL PATCHES WITH ITEM 512 – SEALING OF CONCRETE SURFACES (EPOXY URETHANE)

SUM-8-17.090A

-REMOVE ALL CONTAMINATION FROM PIER 4 (INCLUDING THE PIER) TO THE FORWARD ABUTMENT (INCLUDING MSE WALL), PIERS, DECK UNDERSIDE, AND OUTSIDE OF BOTH PARAPETS IN ACCORDANCE WITH ITEM 512 – REMOVAL OF EXISTING COATINGS FROM CONCRETE SURFACES

-PERFORM ITEM 514 - SURFACE PREPARATION OF EXISTING STRUCTURAL STEEL TO ALL THE STRUCTURAL STEEL BETWEEN PIER 4 AND THE FORWARD ABUTMENT

-PERFORM ITEM 514 - SURFACE PREPARATION OF GIRDER F BETWEEN PIER 2 AND PIER 4

-PAINT THE STRUCTURAL STEEL (INCLUDING PRIME, INTERMEDIATE AND FINISH COATS) AT THE FOLLOWING LOCATIONS:
-GIRDER A FROM PIER 4 TO FORWARD ABUTMENT, EXTERIOR FACE AND BOTTOM FLANGE (SEE DETAIL)
-GIRDER F FROM PIER 2 TO FORWARD ABUTMENT, EXTERIOR FACE AND BOTTOM FLANGE (SEE DETAIL)
-ALL STRUCTURAL STEEL ON EITHER SIDE OF THE INTERMEDIATE JOINT (PIER 5), 10 FEET ON EITHER SIDE OF THE JOINT (TOTAL OF 20 FEET)

-PATCH THE PARAPET (OUTSIDE FACE OF BOTH PARAPETS FROM PIER 4 TO THE FORWARD ABUTMENT), PIER 4, PIER 5, FORWARD ABUTMENT MSE WALL, AND FORWARD ABUTMENT WITH ITEM 843 - PATCHING CONCRETE STRUCTURES WITH TROWELABLE MORTAR.

-INSTALL A NEW LIGHT POLE ON THE RIGHT PARAPET 25 FEET NORTH OF PREVIOUS LIGHT POLE

-PERFORM CONCRETE SEALING WITH ITEM 512 - SEALING OF CONCRETE SURFACES (EPOXY-URETHANE) TO THE FOLLOWING AREAS:
-PIER 4
-PIER 5
-FORWARD ABUTMENT MSE WALL
-FORWARD ABUTMENT
-OUTSIDE FACE OF BOTH PARAPETS FROM PIER 4 TO THE FORWARD ABUTMENT
-PATCHES ON THE FORWARD ABUTMENT MSE WALL

-REPLACE DAMAGED PANELS OF THE NOISE WALL

PROPOSED WORK (CONT...)

SUM-8-17.240B

-REMOVE ALL CONTAMINATION FROM THE ABUTMENTS, DECK UNDERSIDE, AND OUTSIDE OF PARAPETS IN ACCORDANCE WITH ITEM 512 – REMOVAL OF EXISTING COATINGS FROM CONCRETE SURFACES

-PERFORM ITEM 514 - SURFACE PREPARATION OF EXISTING STRUCTURAL STEEL TO ALL THE STRUCTURAL STEEL

-PAINT THE STRUCTURAL STEEL (INCLUDING PRIME, INTERMEDIATE AND FINISH COATS) TO THE STRUCTURAL STEEL AT THE FOLLOWING LOCATIONS:

-LEFT EXTERIOR GIRDER, EXTERIOR FACE AND BOTTOM FLANGE (SEE DETAIL)
-RIGHT EXTERIOR GIRDER, EXTERIOR FACE AND BOTTOM FLANGE (SEE DETAIL)

-PATCH THE PARAPETS (OUTSIDE FACE) AND ABUTMENTS, WITH ITEM 843 - PATCHING CONCRETE STRUCTURES WITH TROWELABLE MORTAR.

-PERFORM CONCRETE SEALING WITH ITEM 512 – SEALING OF CONCRETE SURFACES (EPOXY-URETHANE) TO THE FOLLOWING AREAS: OUTSIDE OF BOTH PARAPETS

-REPLACE DAMAGED PANELS OF THE NOISE WALL

ITEM 202, PORTIONS OF STRUCTURE REMOVED, AS PER PLAN

THIS ITEM SHALL INCLUDE THE ELEMENTS INDICATED IN THE PLANS AND GENERAL NOTES AND THAT ARE NOT SEPARATELY LISTED FOR PAYMENT, EXCEPT FOR WEARING COURSE REMOVAL. ITEMS TO BE REMOVED INCLUDE ALL EXISTING MATERIALS BEING REPLACED BY NEW CONSTRUCTION AND MISCELLANEOUS ITEMS THAT ARE NOT SHOWN TO BE INCORPORATED INTO THE FINAL CONSTRUCTION AND ARE DIRECTED TO BE REMOVED BY THE ENGINEER. THE USE OF EXPLOSIVES, HEADACHE BALLS AND/OR HOE-RAMS WILL NOT BE PERMITTED. THE METHOD OF REMOVAL AND THE WEIGHT OF HAMMER SHALL BE APPROVED BY THE ENGINEER. PERFORM ALL WORK IN A MANNER THAT WILL NOT CUT, ELONGATE OR DAMAGE THE EXISTING REINFORCING STEEL TO BE PRESERVED. CHIPPING HAMMERS SHALL NOT BE HEAVIER THAN THE NOMINAL 90-POUND CLASS. PNEUMATIC HAMMERS SHALL NOT BE PLACED IN DIRECT CONTACT WITH REINFORCING STEEL THAT IS TO BE RETAINED IN THE REBUILT STRUCTURE. SUBMIT CONSTRUCTION PLANS ACCORDING TO C&MS 501.05.

CUT LINE CONSTRUCTION JOINT PREPARATION

SAW CUT BOUNDARIES OF PROPOSED CONCRETE REMOVALS 1 INCH DEEP. REMOVE CONCRETE TO A ROUGH SURFACE. LEAVE THE EXISTING REINFORCING STEEL, IF REQUIRED IN THE PLANS, IN PLACE. INSTALL DOWEL BARS IF SPECIFIED. PRIOR TO CONCRETE PLACEMENT ABRASIVELY CLEAN JOINT SURFACES AND EXISTING EXPOSED REINFORCEMENT TO REMOVE LOOSE AND DISINTEGRATED CONCRETE AND LOOSE RUST. THOROUGHLY CLEAN THE JOINT SURFACE AND EXPOSED REINFORCEMENT OF ALL DIRT, DUST, RUST OR OTHER FOREIGN MATERIAL BY THE USE OF WATER, AIR UNDER PRESSURE, OR OTHER METHODS THAT PRODUCE SATISFACTORY RESULTS. EXISTING REINFORCING STEEL DOES NOT HAVE TO HAVE A BRIGHT STEEL FINISH BUT REMOVE ALL PACK AND LOOSE RUST. THOROUGHLY DRENCH EXISTING CONCRETE SURFACES WITH CLEAN WATER AND ALLOW TO DRY TO A DAMP CONDITION BEFORE PLACING CONCRETE.

ITEM 509 - EPOXY COATED REINFORCING STEEL, AS PER PLAN

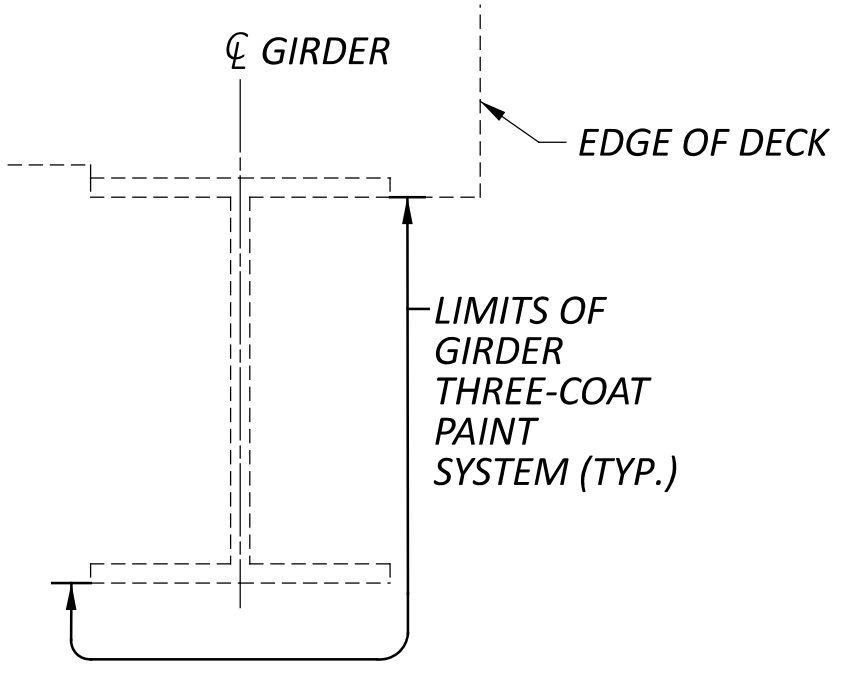
IN ADDITION TO THE PROVISIONS OF ITEM 509, FIELD BEND AND/OR FIELD CUT THE REINFORCING STEEL DESIGNATED IN THE PLANS, AS NECESSARY, IN ORDER TO MAINTAIN THE REQUIRED CLEARANCES AND BAR SPACINGS. REPAIR ALL DAMAGE TO THE EPOXY COATING, AS A RESULT OF THIS WORK, ACCORDING TO C&MS 709.00.

ITEM 509 - REINFORCING STEEL, REPLACEMENT OF EXISTING REINFORCING STEEL, AS PER PLAN

REPLACE ALL EXISTING REINFORCING BARS DEEMED BY THE ENGINEER TO BE UNUSABLE BECAUSE OF CORROSION. THE DEPARTMENT WILL MEASURE THE REPLACEMENT REINFORCING STEEL BY THE NUMBER OF POUNDS ACCEPTED IN PLACE. REPLACE ALL EXISTING REINFORCING STEEL BARS WHICH ARE TO BE INCORPORATED INTO THE NEW WORK AND ARE DEEMED BY THE ENGINEER TO BE MADE UNUSABLE BY CONCRETE REMOVAL OPERATIONS WITH NEW REINFORCING STEEL OF THE SAME SIZE AND COATING AT NO COST TO THE DEPARTMENT.

ITEM 514 - PAINTING OF STRUCTURAL STEEL

THE COLOR FOR THE FINISHED COAT OF STRUCTURES SUM-8-17.090A AND SUM-8-17.240B WILL CONFORM TO FEDERAL COLOR NUMBER 10324 (BROWN).



ITEM 519 - PATCHING CONCRETE STRUCTURES, AS PER PLAN

PRIOR TO THE SURFACE CLEANING SPECIFIED IN C&MS 519.04 AND WITHIN 24 HOURS OF PLACING PATCHING MATERIAL, BLAST CLEAN ALL SURFACES TO BE PATCHED INCLUDING THE EXPOSED REINFORCING STEEL. ACCEPTABLE METHODS INCLUDE: HIGH-PRESSURE WATER BLASTING WITH, OR WITHOUT, ABRASIVES IN THE WATER, ABRASIVE BLASTING WITH CONTAINMENT OR VACUUM ABRASIVE BLASTING.

BRIDGE TERMINAL ASSEMBLY REMOVED FOR REUSE

IN ORDER TO ACCESS THE WORK AREA THE CONTRACTOR SHALL REMOVE AND STORE THE TWO BRIDGE TERMINAL ASSEMBLIES LOCATED AT THE REAR LEFT CORNER OF STRUCTURE SUM-8-17.451L ONCE ALL WORK IS COMPLETE AND BEFORE THE PORTABLE BARRIER IS REMOVED THE CONTRACTOR SHALL REINSTALL THE BRIDGE TERMINAL ASSEMBLIES TO THEIR PREVIOUS CONDITION. THE DEPARTMENT WILL NOT PAY FOR ANY DAMAGE CAUSED BY THE CONTRACTOR TO THE BRIDGE TERMINAL ASSEMBLIES.

ITEM SPECIAL - NOISE BARRIER (STRUCTURE MOUNTED PANEL REPLACEMENT)

THIS WORK SHALL CONSIST OF THE REPLACEMENT OF THE DAMAGED NOISE WALLS PANELS LOCATED ON STRUCTURES SUM-8-17.090A AND SUM-8-17.240B. THE PROJECT ENGINEER WILL MARK THE PANELS TO BE REPLACED. THE CONTRACTOR SHALL CAREFULLY REMOVE AND REPLACE THE DAMAGED PANEL. THE CONTRACTOR IS RESPONSIBLE FOR THE DISPOSAL OF THE EXISTING PANELS.

NOISE PANELS SHALL BE REFLECTIVE FIBERGLASS AND SHALL NOT EXCEED A WEIGHT OF 50 PSF. EACH END OF EVERY PANEL SHALL BE FASTENED TO POSTS. THE COLOR OF THE HIGHWAY SIDE OF THIS NOISE BARRIER IS TO BE BRICK RED (FEDERAL COLOR NUMBER 20109). THE COLOR OF THE RESIDENTIAL SIDE OF THE NOISE BARRIER WILL BE LIGHT GRAY (FEDERAL COLOR NUMBER 16515 LIGHT GRAY). THE REPLACEMENT PANELS SHALL MATCH THE TEXTURE OF THE EXISTING PANELS. ADDITIONAL DETAILS ON THE PANELS IS AVAILABLE IN THE EXISTING PLAN SET SUM-8-16.66 (PID 93445, PROJECT 13-3023).

THE CONTRACTOR IS REQUIRED TO PERFORM FIELD MEASUREMENTS TO DETERMINE THE REQUIRED SIZE AND OTHER DETAILS REQUIRED TO MANUFACTURE THE REPLACEMENT PANEL.

PAYMENT FOR THIS WORK SHALL INCLUDE ALL WORK REQUIRED TO REPLACE THE EXISTING DAMAGED PANEL. THE DEPARTMENT WILL NOT PAY FOR ANY DAMAGE CAUSED BY THE CONTRACTOR TO THE NOISE WALL OR THE PANELS ABOVE THE DAMAGE PANEL.

THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE ESTIMATED QUANTITIES:

SUM-8-17.090A (27 BAYS)
ITEM SPECIAL - NOISE BARRIER (STRUCTURE MOUNTED PANEL REPLACEMENT), 2430 SF

SUM-8-17.240B (5 BAYS)
ITEM SPECIAL - NOISE BARRIER (STRUCTURE MOUNTED PANEL REPLACEMENT), 650 SF

STRUCTURE NOTES (ACCIDENT DAMAGE REPAIRS)
SUM-8-17.451L, SUM-8-17.090A, SUM-8-17.240B
OVER SR 8 & BRANDYWINE CREEK

SFN
VARIOUS
DESIGN AGENCY



DESIGNER: JF
CHECKER: MJA

REVIEWER: TJP
10-15-24

PROJECT ID: 102330

SUBSET: 1 | TOTAL: 6

SHEET: P.28 | TOTAL: 33

CALC: JF DATE: 3/27/2024
 CHECKED: MJA DATE: 7/15/2024

| ESTIMATED QUANTITIES | | | | | | | | | |
|---------------------------------|--|---------------------------------------|---------------------------------------|---------|-----------|------|---|-----------|-------|
| BRIDGE NO. / STRUCTURE FILE NO. | | | | ITEM | EXTENSION | UNIT | DESCRIPTION | SEE SHEET | |
| 2 | SUM-8-17.451L 7700988 04/NFP/47 | SUM-8-17.090A 7700768 04/NFP/47 | SUM-8-17.240B 7700784 04/NFP/47 | 202 | 11201 | | PORTIONS OF STRUCTURE REMOVED, AS PER PLAN | | 1 / 6 |
| | | | | 202 | 47200 | EACH | BRIDGE TERMINAL ASSEMBLY REMOVED FOR REUSE | | 1 / 6 |
| 20 | 117 4 | 642 | 353 | 509 | 10000 | LB | EPOXY COATED STEEL REINFORCEMENT | | |
| | | | | 511 | 34448 | CY | CLASS QC2 CONCRETE, BRIDGE DECK (PARAPET) | | |
| | | | | 512 | 10100 | SY | SEALING OF CONCRETE SURFACES (EPOXY-URETHANE) | | |
| 238 | 1855 49307 11469 11469 11469 | 927 9998 1573 1573 1573 | | 512 | 74000 | SY | REMOVAL OF EXISTING COATINGS FROM CONCRETE SURFACES | | |
| | | | | 514 | 00050 | SF | SURFACE PREPARATION OF EXISTING STRUCTURAL STEEL | | |
| | | | | 514 | 00056 | SF | FIELD PAINTING OF EXISTING STRUCTURAL STEEL, PRIME COAT | | |
| | | | | 514 | 00060 | SF | FIELD PAINTING STRUCTURAL STEEL, INTERMEDIATE COAT | | |
| | | | | 514 | 00066 | SF | FIELD PAINTING STRUCTURAL STEEL, FINISH COAT | | |
| 20 | 17 7 | 5 2 | | 514 | 00504 | MNHR | GRINDING FINS, TEARS, SLIVERS ON EXISTING STRUCTURAL STEEL | | |
| | | | | 514 | 10000 | EACH | FINAL INSPECTION REPAIR | | |
| | | | | 519 | 11101 | SF | PATCHING CONCRETE STRUCTURE, AS PER PLAN | | 1 / 6 |
| 130 | 2430 1550 | 650 230 | | SPECIAL | 60610920 | SF | NOISE BARRIER (STRUCTURE MOUNTED) (DAMAGED PANEL REPLACEMENT) | | 1 / 6 |
| | | | | 843 | 50000 | SF | PATCHING CONCRETE STRUCTURES WITH TROWELABLE MORTAR | | |
| 25 8 | | | | 509 | 20001 | LB | CONCRETE REINFORCEMENT, REPLACEMENT OF EXISTING CONCRETE REINFORCEMENT, AS PER PLAN | | 1 / 6 |
| | | | | 510 | 10000 | EACH | DOWEL HOLES WITH NONSHRINK, NONMETALLIC GROUT | | |

ESTIMATED QUANTITIES (ACCIDENT DAMAGE REPAIRS)
 SUM-8-17.451L, SUM-8-17.090A, SUM-8-17.240B
 OVER SR 8 & BRANDYWINE CREEK

SFN
 VARIOUS
 DESIGN AGENCY



DESIGNER: JF
 CHECKER: MJA

REVIEWER: TJP
 10-15-24

PROJECT ID: 102330

SUBSET: 2 | TOTAL: 6

SHEET: P.29 | TOTAL: 33

| MARK | NUMBER | | | | LENGTH | WEIGHT (LBS) | TYPE | DIMENSIONS | | | | |
|--------------------------|--------------|-------------|-------|-------|--------|-----------------|------|------------|-------|--------|-------|---|
| | REAR ABUT | FWD ABUT | SUPER | TOTAL | | | | A | B | C | D | E |
| SUM-8-17.090A | | | | | | | | | | | | |
| S516 | | | 4 | 4 | 3'-2" | 14 | 2 | 1'-3" | 2'-1" | | | |
| *S517 | | | 4 | 4 | 6'-1" | 26 | 1 | 2'-5" | 4'-4" | | | |
| S518 | | | 4 | 4 | 4'-4" | 19 | ST | | | | | |
| S519 | | | 5 | 5 | 7'-3" | 38 | 21 | 1'-10" | 1'-4" | 1'-10" | 0'-6" | |
| *S520 | | | 2 | 2 | 9'-3" | 20 | 22 | 1'-10" | 1'-4" | 1'-10" | 1'-6" | |
| SUPERSTRUCTURE SUB-TOTAL | | | | | | 117 | | | | | | |
| ABUTMENT SUB-TOTAL | | | | | | 0 | | | | | | |
| GRAND TOTAL | | | | | | 117 | | | | | | |

THE BAR SIZE NUMBER IS SPECIFIED ON THE PLANS IN THE BAR MARK COLUMN. THE FIRST DIGIT WHERE THREE DIGITS ARE USED, THE FIRST TWO DIGITS WHERE FOUR ARE USED, INDICATES THE BAR SIZE NUMBER. FOR EXAMPLE, P601 IS A NO. 6 BAR. BAR DIMENSIONS SHOWN ARE OUT TO OUT UNLESS OTHERWISE INDICATED. R INDICATES INSIDE RADIUS, UNLESS OTHERWISE NOTED. "STD." WRITTEN IN PLACE OF A DIMENSION INDICATES A STANDARD BEND AT THE END OF THE BAR.

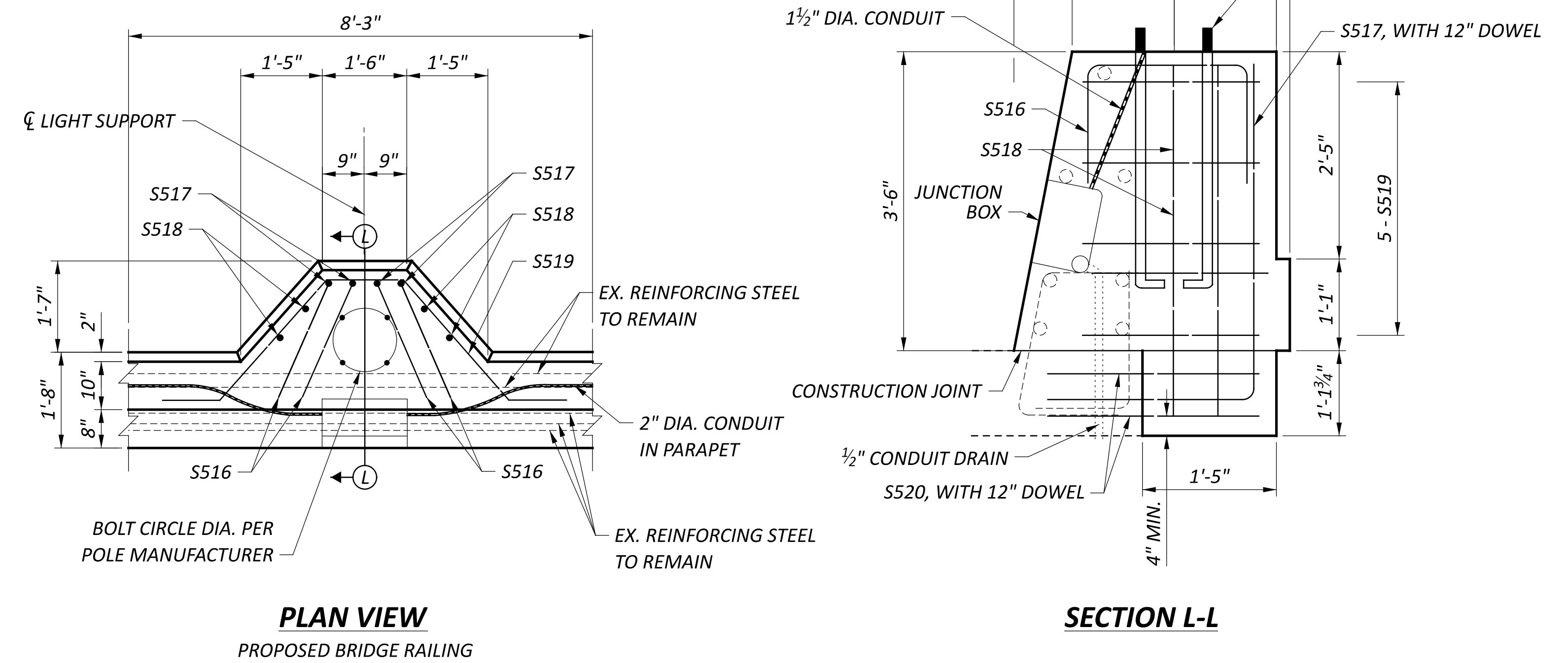
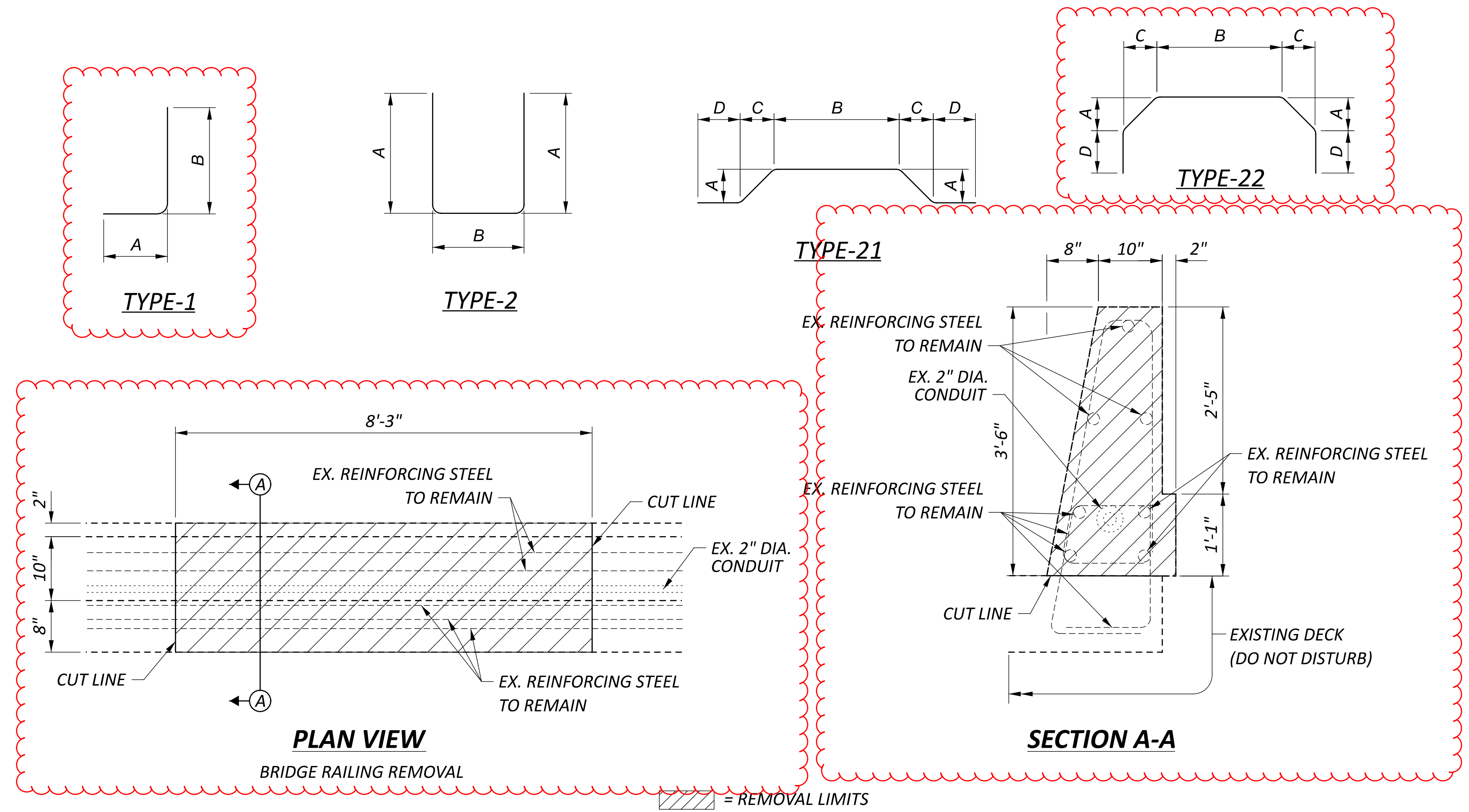
ALL REINFORCING STEEL TO BE EPOXY COATED

*BARS TO BE DOWELED 12", DOWELS FOR S520 BARS TO BE A MINIMUM OF 4" FROM THE BOTTOM OF THE DECK

SEE SCD HL-20.14 FOR ADDITIONAL DETAILS

NOTES:

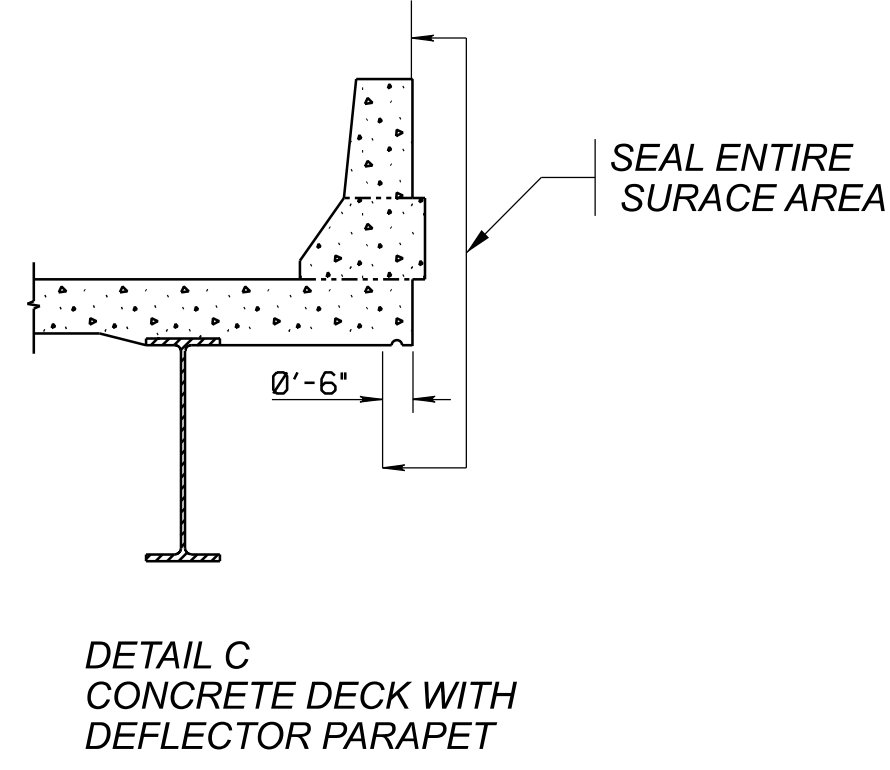
1. THE LOCATION OF THE LIGHT SUPPORT SHALL BE FIELD LOCATED AND BE OVER THE CENTER OF THE SR 8 SB SHOULDER, APPROXIMATELY 25 FEET TO THE NORTH OF THE EXISTING LIGHT SUPPORT ON THE RIGHT PARAPET OF STRUCTURE SUM-8-17.090A AND APPROVED BY THE PROJECT ENGINEER.



LIGHT SUPPORT DETAIL

SEE SHEET P.21 FOR LIGHTING QUANTITIES

| | |
|---------------|----------|
| SFN | 7700768 |
| DESIGN AGENCY | |
| DESIGNER | CHECKER |
| JF | MJA |
| REVIEWER | |
| TJP | 10-15-24 |
| PROJECT ID | 102330 |
| SUBSET | TOTAL |
| 5 | 6 |
| SHEET | TOTAL |
| P.32 | 33 |



DETAIL C
 CONCRETE DECK WITH
 DEFLECTOR PARAPET

| BRIDGE NUMBER | SEALING PAY ITEM | STRUCTURE TYPE | PROPOSED SEALING | FEDERAL COLOR NUMBER | ESTIMATED QUANTITIES | | | | |
|---------------|--|-----------------------------|--|----------------------|----------------------|--------------|---------------|-----------------|---------------|
| | | | | | ABUT (SQ YD) | PIER (SQ YD) | SUPER (SQ YD) | GENERAL (SQ YD) | TOTAL (SQ YD) |
| SUM-8-17.090A | ITEM 512 - SEALING OF CONCRETE SURFACES (EPOXY-URETHANE) | STEEL CONTINUOUS MULTI-BEAM | SEAL THE RIGHT CONCRETE RAILING AS PER DETAIL C PIER CAPS SEAL THE FORWARD ABUTMENT, INCLUDING MSE WALL | MATCH EXISTING | 184 | 347 | 111 | | 642 |
| | ITEM 512 - REMOVAL OF EXISTING COATINGS FROM CONCRETE SURFACES | STEEL CONTINUOUS MULTI-BEAM | REMOVE ALL CONTAMINATION FROM THE FORWARD ABUTMENT, INCLUDING MSE WALL, PIERS 4 AND 5, DECK UNDERSIDE FROM PIER 4 TO THE FORWARD ABUTMENT, AND THE OUTSIDE OF THE CONCRETE RAILING | | 184 | 347 | 1324 | | 1855 |
| SUM-8-17.240B | ITEM 512 - SEALING OF CONCRETE SURFACES (EPOXY-URETHANE) | STEEL CONTINUOUS MULTI-BEAM | SEAL THE CONCRETE RAILING AS PER DETAIL C FORWARD AND REAR ABUTMENTS | MATCH EXISTING | 211 | | 142 | | 353 |
| | ITEM 512 - REMOVAL OF EXISTING COATINGS FROM CONCRETE SURFACES | STEEL CONTINUOUS MULTI-BEAM | REMOVE ALL CONTAMINATION FROM THE FORWARD AND REAR ABUTMENT, DECK UNDERSIDE, AND THE OUTSIDE OF THE CONCRETE RAILING | | 211 | | 716 | | 927 |
| SUM-8-17.451L | ITEM 512 - SEALING OF CONCRETE SURFACES (EPOXY-URETHANE) | CONCRETE CONTINUOUS SLAB | SEAL ALL PATCHES TO THE REAR WINGWALLS, REAR ABUTMENT, AND REAR LEFT DECK EDGE | MATCH EXISTING | 17 | | 3 | | 20 |
| | ITEM 512 - REMOVAL OF EXISTING COATINGS FROM CONCRETE SURFACES | CONCRETE CONTINUOUS SLAB | REMOVE ALL CONTAMINATION FROM THE REAR ABUTMENT, REAR WINGWALLS, PIER CAPS, DECK UNDERSIDE, AND LEFT DECK EDGE | | 36 | 77 | 125 | | 238 |
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STRUCTURE SEALING DETAILS (ACCIDENT DAMAGE REPAIRS)
 SUM-8-17.451L, SUM-8-17.090A, SUM-8-17.240B
 OVER SR 8 & BRANDYWINE CREEK

SFN
 VARIOUS
 DESIGN AGENCY

DESIGNER: JF
 CHECKER: MJA
 REVIEWER: TJP
 DATE: 10-15-24
 PROJECT ID: 102330
 SUBSET: 6 | TOTAL: 6
 SHEET: P.33 | TOTAL: 33