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FOR EACH ITEM 513E95030 – STRUCTURAL STEEL, MISC: STEEL PLATE CONFINEMENT ASSEMBLY INSTALLATION, 58 SQUARE FEET OF STRUCTURAL PNEUMATICALLY PLACED CONCRETE HAS BEEN INCLUDED IN THE ESTIMATED QUANTITY FOR ITEM SPECIAL STRUCTURES: STRUCTURAL PNEUMATICALLY PLACED CONCRETE (SHOTCRETE). THE SHOTCRETE IN THESE AREAS SHALL BE APPLIED TO THE EAST AND WEST COLUMN FACES AND TO THE END FACE RECEIVING THE STEEL PLATE CONFINEMENT ASSEMBLY INSTALLATION FOR THE ENTIRE HEIGHT OF THE SECOND COLUMN SEGMENT AS INDICATED IN THE PLANS. AND COVER ALL EXPOSED EXISTING REINFORCING AND THE OUTER THREADED BARS OF THE STEEL PLATE CONFINEMENT ASSEMBLY. THE SHOTCRETE SHALL ONLY BE PLACED IN THESE AREAS AFTER INSTALLATION OF THE STEEL PLATE CONFINEMENT ASSEMBLIES HAS BEEN ACCEPTED.

METHOD OF MEASUREMENT. THE DEPARTMENT WILL MEASURE THE STRUCTURAL PNEUMATICALLY APPLIED CONCRETE BY THE NUMBER OF SQUARE FEET OF PIER COLUMN REPAIRED. THE DEPARTMENT WILL MEASURE ALL COMPLETED, TESTED, AND APPROVED PATCHES, IRRESPECTIVE OF DEPTH OR THICKNESS OF THE PATCH. THE DEPARTMENT WILL NOT SEPARATELY MEASURE FURNISHING AND INSTALLING GALVANIC ANODES FOR PAYMENT

THE CONTRACTOR IS RESPONSIBLE FOR ALL TEST PANELS. CORING REPAIR OF CORE HOLES. INDEPENDENT LABORATORY TESTING OF THE CORES, REPLACEMENT OF REJECTED AREAS, AND ALL PREVIOUSLY MENTIONED WORK UNDER ITEM 520 AND THIS NOTE.

BASIS OF PAYMENT. THE DEPARTMENT WILL CONSIDER FURNISHING AND INSTALLING GALVANIC ANODES AS INCIDENTAL TO THIS WORK. THE DEPARTMENT WILL NOT PAY FOR ADDITIONAL REINFORCEMENT TO REPLACE THAT DAMAGED BY THE CONTRACTOR'S OPERATIONS. THE DEPARTMENT WILL NOT PAY FOR REMOVING. REPLACING. AND DETERMINED BY SOUNDING. VISIBLE CRACKS. OR UNACCEPTABLE CORES. THE DEPARTMENT WILL SEPARATELY PAY FOR FURNISHING AND INSTALLING REINFORCING STEEL THAT IS USED TO REPLACE EXISTING REINFORCING DEEMED BY THE ENGINEER TO BE

THE DEPARTMENT WILL PAY FOR ACCEPTED QUANTITIES AT THE CONTRACT PRICE AS FOLLOWS AND IN ACCORDANCE WITH CMS TABLE 520.16-1. SHOTCRETE 28 DAY COMPRESSIVE STRENGTH PAY

DESCRIPTION 530E0600 SPECIAL SQUARE FOOT STRUCTURES: STRUCTURAL

ITEM 513 – STRUCTURAL STEEL, MISC.: STEEL PLATE CONFINEMENT ASSEMBLY

IN ADDITION TO THE REQUIREMENTS OF ITEM 513, THIS WORK CONSISTS OF FURNISHING AND FABRICATING STEEL PLATES. FURNISHING AND INSTALLING THREADED RODS AND ASSOCIATED NUTS AND WASHERS, PREPARING THE EXISTING CONCRETE SURFACES, LOCATING THE EXISTING PIER COLUMN REINFORCING, DRILLING HOLES THROUGH THE EXISTING PIER COLUMNS, AND COMPLETING THE INSTALLATION OF ALL STEEL CONFINEMENT COMPONENTS. THIS WORK ALSO INCLUDES PROVIDING THE NECESSARY LABOR, MATERIALS, AND EQUIPMENT TO ACCESS THE

STEEL FABRICATION AND MATERIALS SHALL BE PER CMS 513 (LEVEL

THE EXISTING ARCHITECTURAL REVEAL/EXTENSION AT THE COLUMN CORNERS SHALL BE REMOVED TO PROVIDE A UNIFORM BEARING SURFACE FOR THE STEEL CONFINEMENT PLATE. ALL SOUND AND UNSOUND CONCRETE SHALL BE REMOVED TO PROVIDE A UNIFORM BEARING SURFACE FOR THE STEEL PLATES. REMOVAL SHALL BE INCLUDED IN THE COST OF THIS REPAIR AND SHALL BE PERFORMED AS PER CMS 520.08. AS DIRECTED BY THE ENGINEER, USE A BUSHHAMMER OR GRINDER FOLLOWED BY A THIN FILM OF PORTLAND CEMENT MORTAR OR PASTE TO PROVIDE A

FURNISH THREADED RODS, NUTS, AND WASHERS PER CMS 711.09. THREADED RODS, WITH A HEAVY DUTY WASHER AND TWO NUTS ON EACH END, MAY BE REPLACED BY ASTM F3125, GRADE A325, BOLTS

ALL STEEL PLATES, THREADED RODS, NUTS, WASHERS, AND OTHER STEEL COMPONENTS SHALL BE GALVANIZED PER CMS 711.02.

FABRICATE THE STEEL PLATES WITH OVERSIZED HOLES FOR THE

PRIOR TO FIELD DRILLING HOLES THROUGH THE PIER COLUMNS AND FABRICATING THE STEEL PLATES, LOCATE THE EXISTING REINFORCING STEEL IN THE VICINITY OF THE PROPOSED HOLES TO VERIFY THE DRILLING OPERATION WILL NOT DAMAGE THE EXISTING REINFORCING. IF EXISTING REINFORCING STEEL INTERFERES WITH A PROPOSED HOLE LOCATION AND FIELD-FABRICATING HORIZONTAL, LONG SLOTTED HOLES PER AISC TABLE J3.3 IN THE PLATE WILL NOT ACHIEVE A PROPER FIT, A NEW GALVANIZED STEEL PLATE SHALL BE FURNISHED TO FIT THE REVISED HOLE PATTERN AT NO COST TO THE DEPARTMENT. THE FIELD-DRILLED HOLES THROUGH THE PIER COLUMN SHALL NOT BE MORE THAN 1/8" LARGER IN DIAMETER THAN THE THREADED ROD.

AFTER INSTALLATION OF THE STEEL PLATE CONFINEMENT ASSEMBLIES HAS BEEN COMPLETED AND ACCEPTED, APPLY PNEUMATICALLY PLACED CONCRETE (SHOTCRETE) IN THESE AREAS TO COVER ALL EXPOSED EXISTING REINFORCING STEEL AND THE OUTER THREADED RODS OF THE STEEL PLATE CONFINEMENT ASSEMBLY. FIFTY-EIGHT (58) SQUARE FEET OF STRUCTURAL PNEUMATICALLY PLACED CONCRETE HAS BEEN INCLUDED IN THE ESTIMATED QUANTITY FOR ITEM SPECIAL – STRUCTURES: STRUCTURAL PNEUMATICALLY PLACED CONCRETE (SHOTCRETE) FOR EACH STEEL PLATE CONFINEMENT ASSEMBLY.

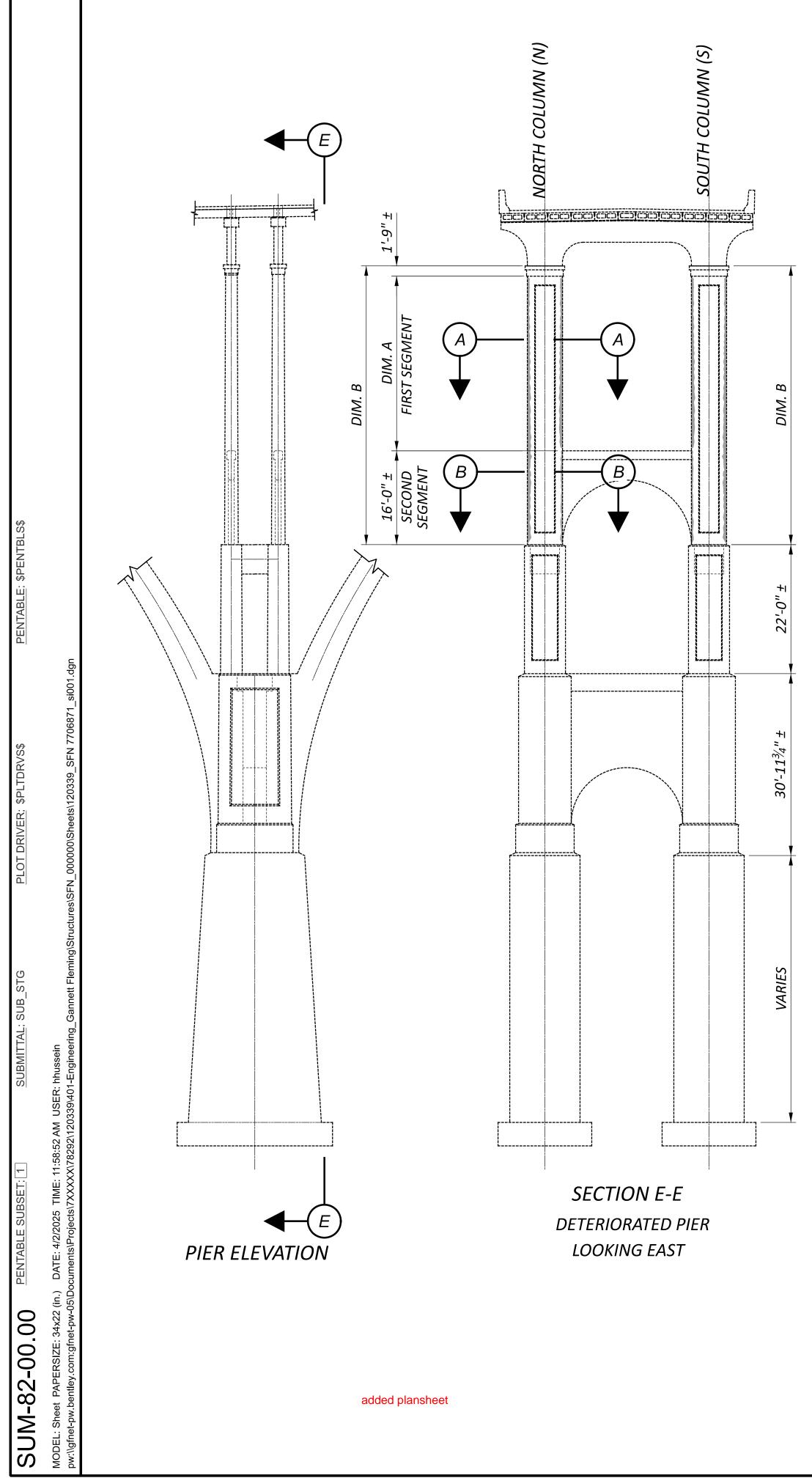
METHOD OF MEASUREMENT. THE DEPARTMENT WILL MEASURE THE STEEL PLATE CONFINEMENT ASSEMBLIES BY THE NUMBER OF EACH. EACH ASSEMBLY INCLUDES 2 STEEL PLATES. 4 THREADED RODS, 16 NUTS, AND 8 WASHERS.

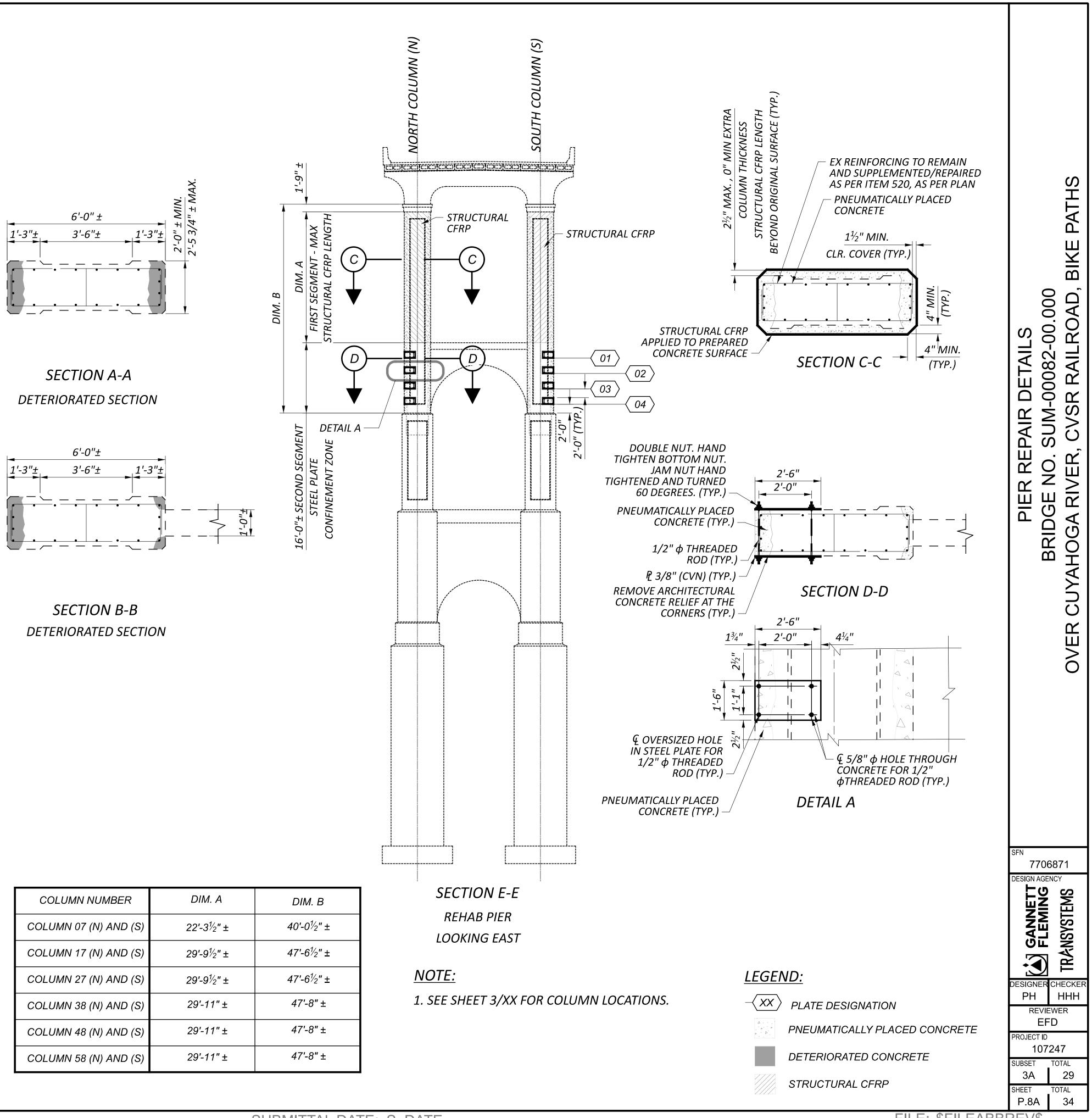
BASIS OF PAYMENT. THE DEPARTMENT WILL CONSIDER THE COSTS FOR REMOVING EXISTING CONCRETE AND PROVIDING A UNIFORM BEARING SURFACE FOR THE STEEL CONFINEMENT PLATE AS INCIDENTAL TO THIS WORK. THE DEPARTMENT WILL CONSIDER THE COSTS FOR DRILLING THREADED ROD HOLES THROUGH THE EXISTING CONCRETE AS INCIDENTAL TO THIS WORK. THE DEPARTMENT WILL NOT PAY FOR FURNISHING NEW STEEL PLATES SO THE THREADED ROD HOLE LOCATIONS DO NOT INTERFERE WITH EXISTING REINFORCING STEEL, IF THE EXISTING REINFORCING WAS NOT PROPERLY LOCATED PRIOR TO FABRICATING THE PLATES. THE DEPARTMENT WILL PAY FOR APPLYING PNEUMATICALLY PLACED CONCRETE (SHOTCRETE) SEPARATELY. THE DEPARTMENT WILL PAY FOR ACCEPTED QUANTITIES OF STEEL PLATE CONFINEMENT ASSEMBLIES AS FOLLOWS:

ITEM UNIT DESCRIPTION 513E95030 EACH STRUCTURAL STEEL, MISC: STEEL PLATE CONFINEMENT ASSEMBLY

added plansheet

	STRUCTURE GENERAL NOTES BRIDGE NO. SUM-00082-00.000 OVER CUYAHOGA RIVER, CVSR RAILROAD, BIKE PATHS	
FILE: \$FILEABBF	SFN 7706871 DESIGN AGENCY	

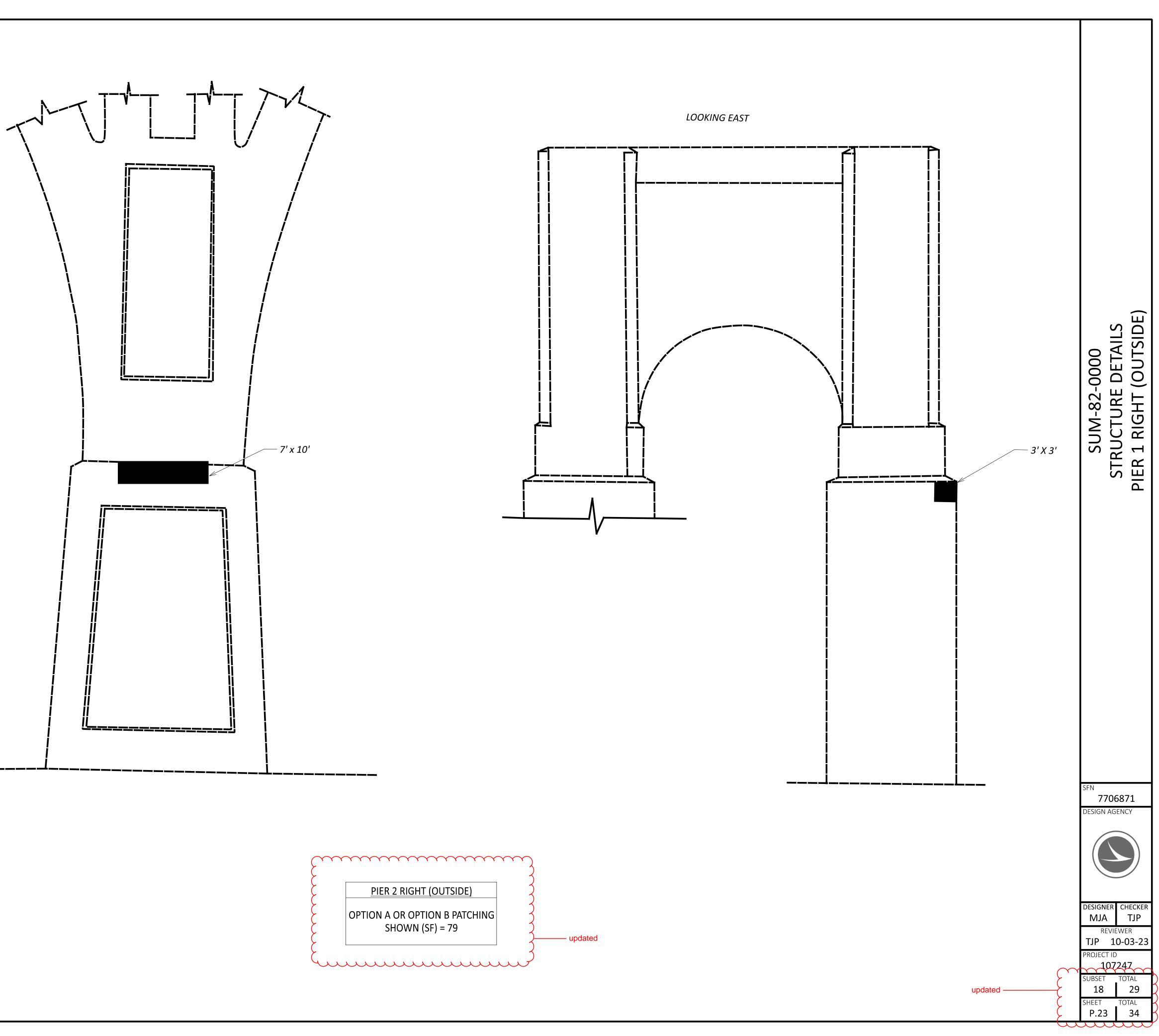


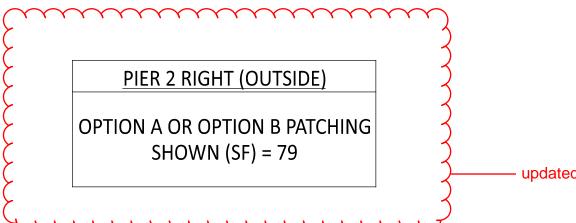


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SUM-82-0.00 MODEL: Sheet 16B - Pier 1 Right PA

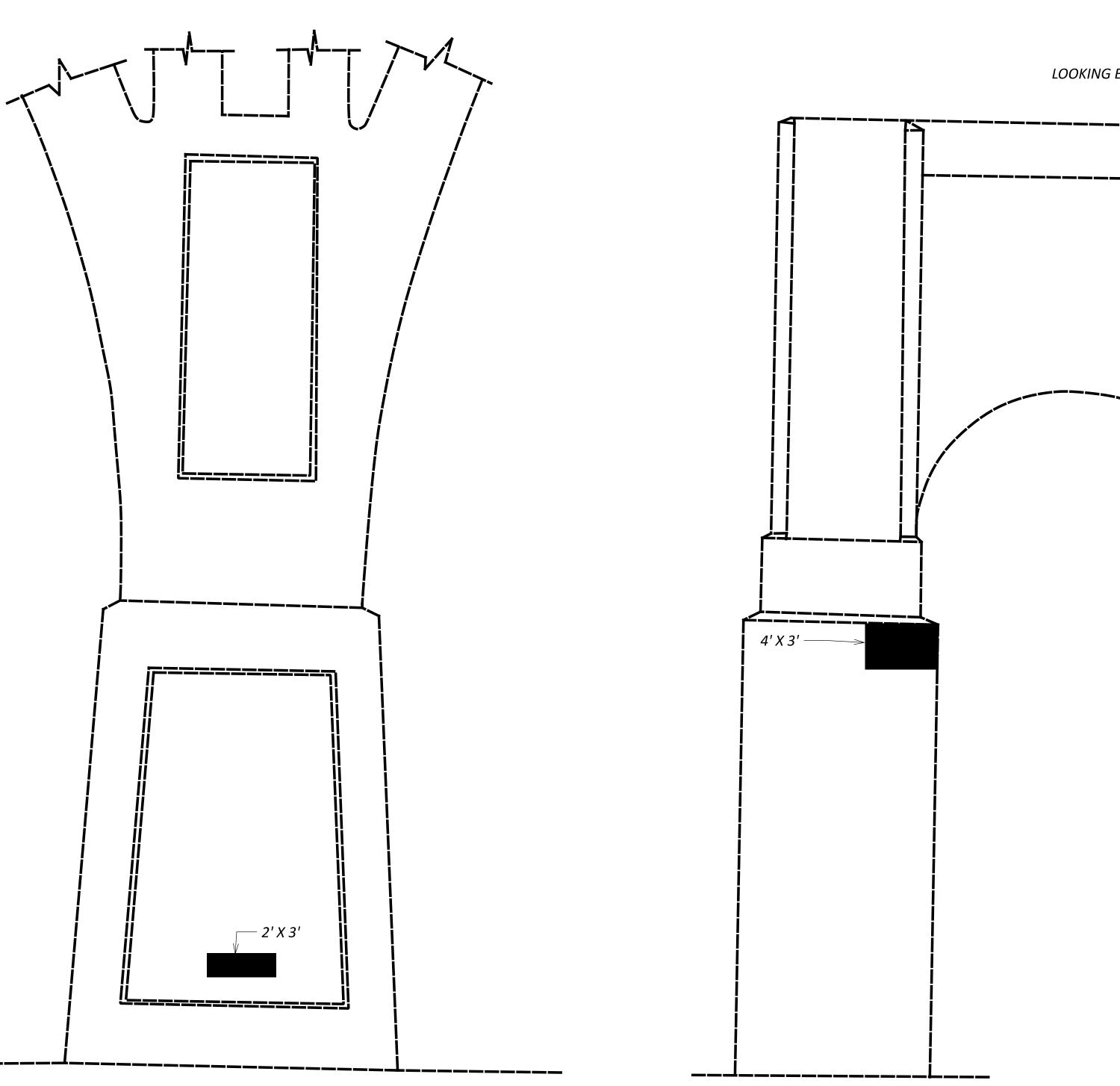
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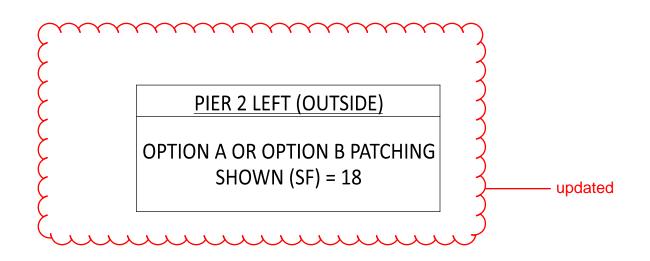




SUM-82-0.00 MODEL: Sheet 17B - Pier 1 Left PAPEL

Pier 1 Left PAPERSIZE: 34x22 (in.) DATE: 4/21/2025 TIME: 1:34:49 PM USER: mandrasi :ntley.com:ohiodot-pw-02\Documents\01 Active Projects\District 04\Summit\107247\400 eet 17B -lot-pw.be

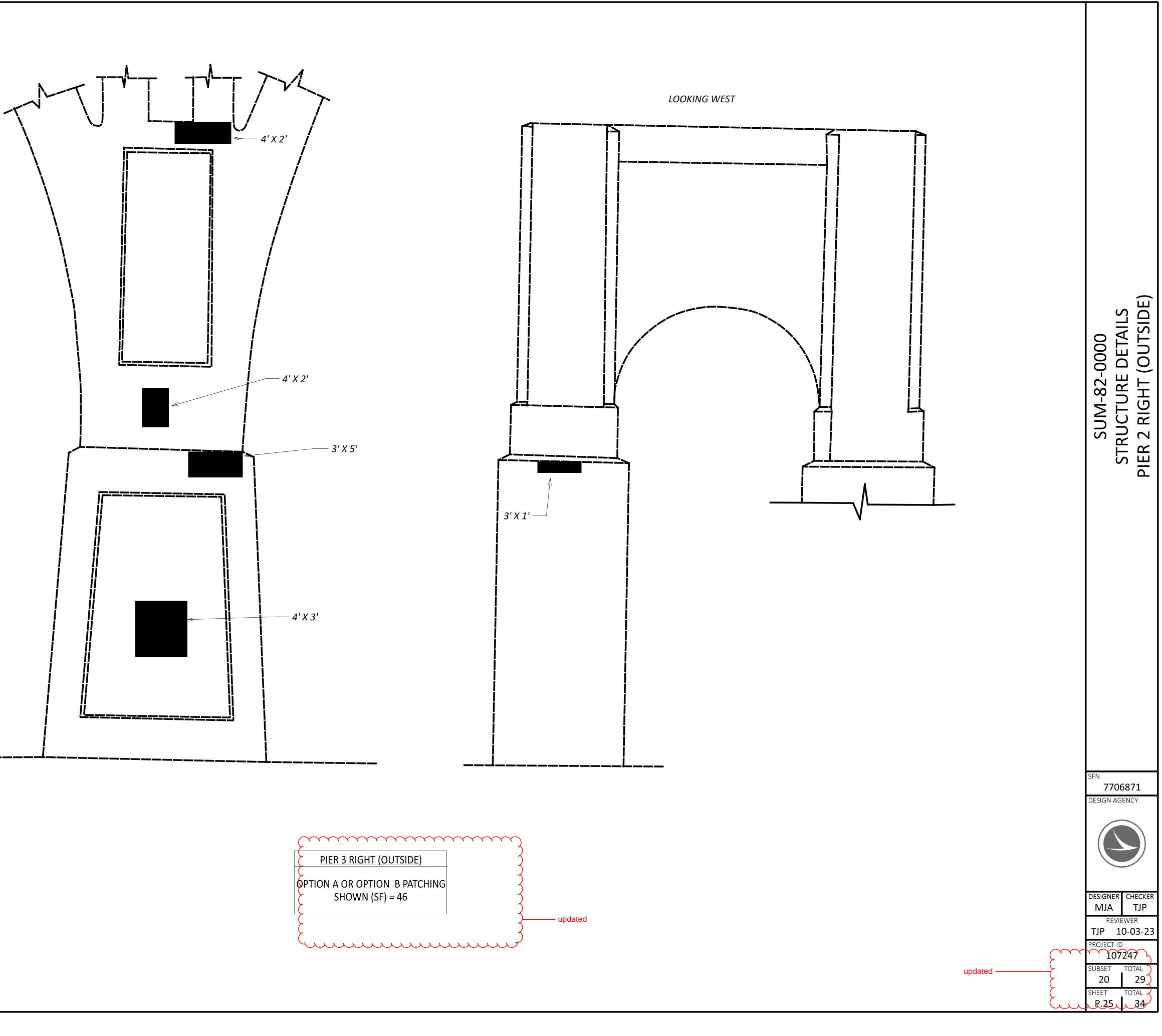


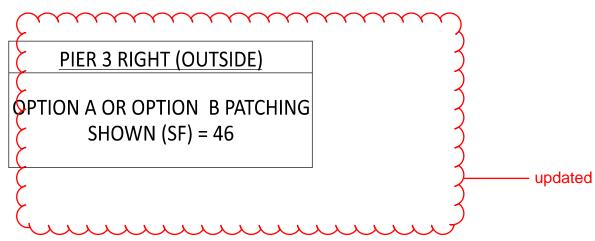


	STRUCTURE DETAILS PIER 1 LEFT (OUTSIDE)
updated	SFN 7706871 DESIGN AGENCY DESIGNER CHECKER MJA CHECKER MJA TJP REVIEWER TJP 10-03-23 PROJECT ID REVIEWER TJP 10-03-23 PROJECT ID 107247 SUBSET TOTAL 19 29 SHEET TOTAL 19 29 SHEET TOTAL 19 34

SUM-82-0.00 MODEL: Sheet 18B - Pier 2 Right PA

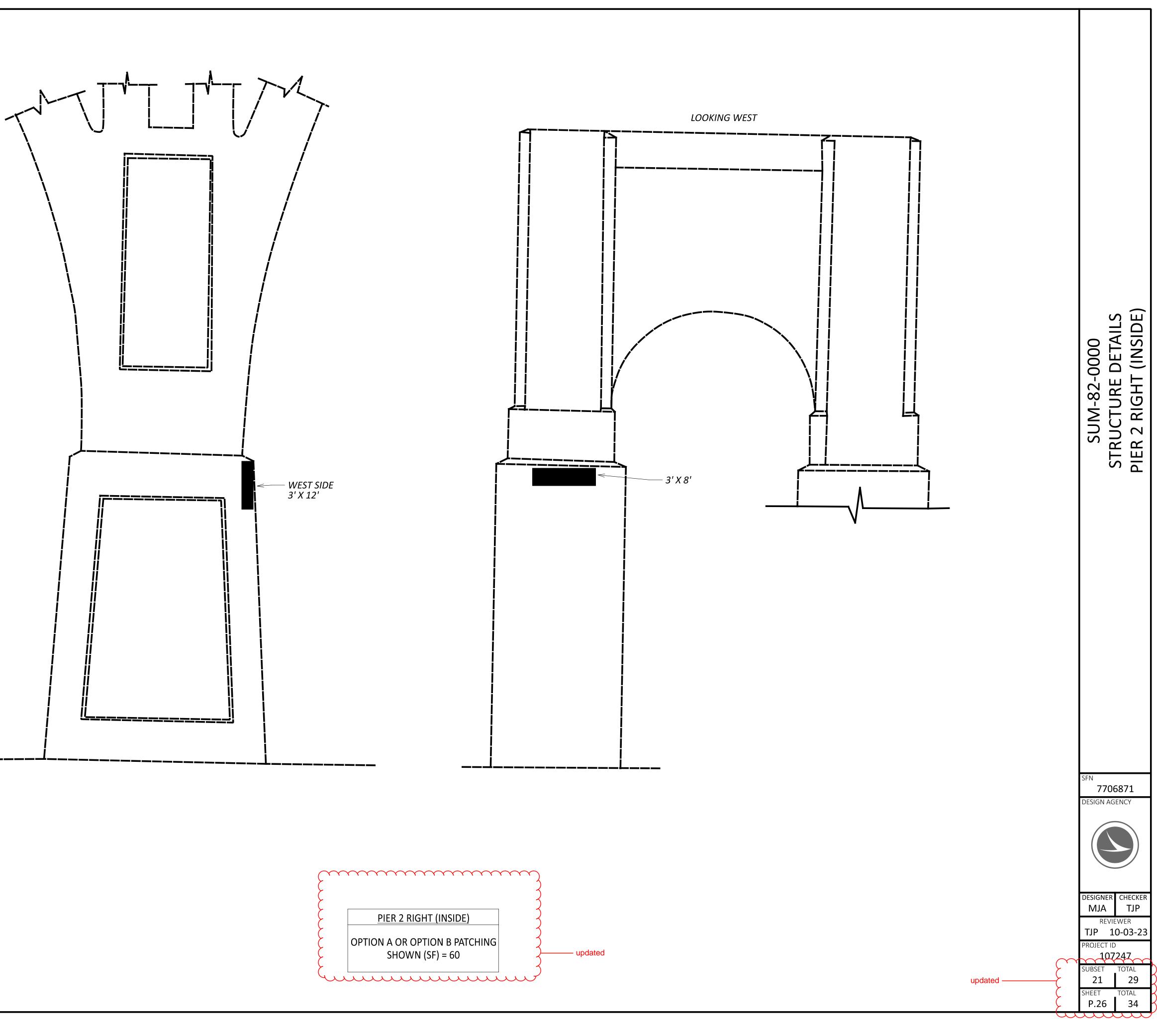
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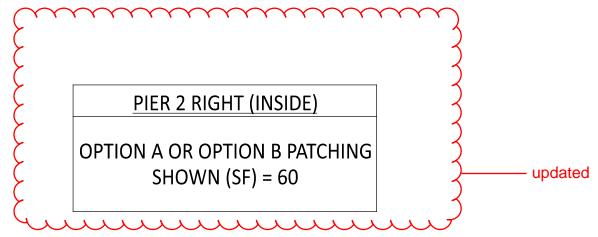




SUM-82-0.00 MODEL: Sheet 16 - Pier 2 Right PAP

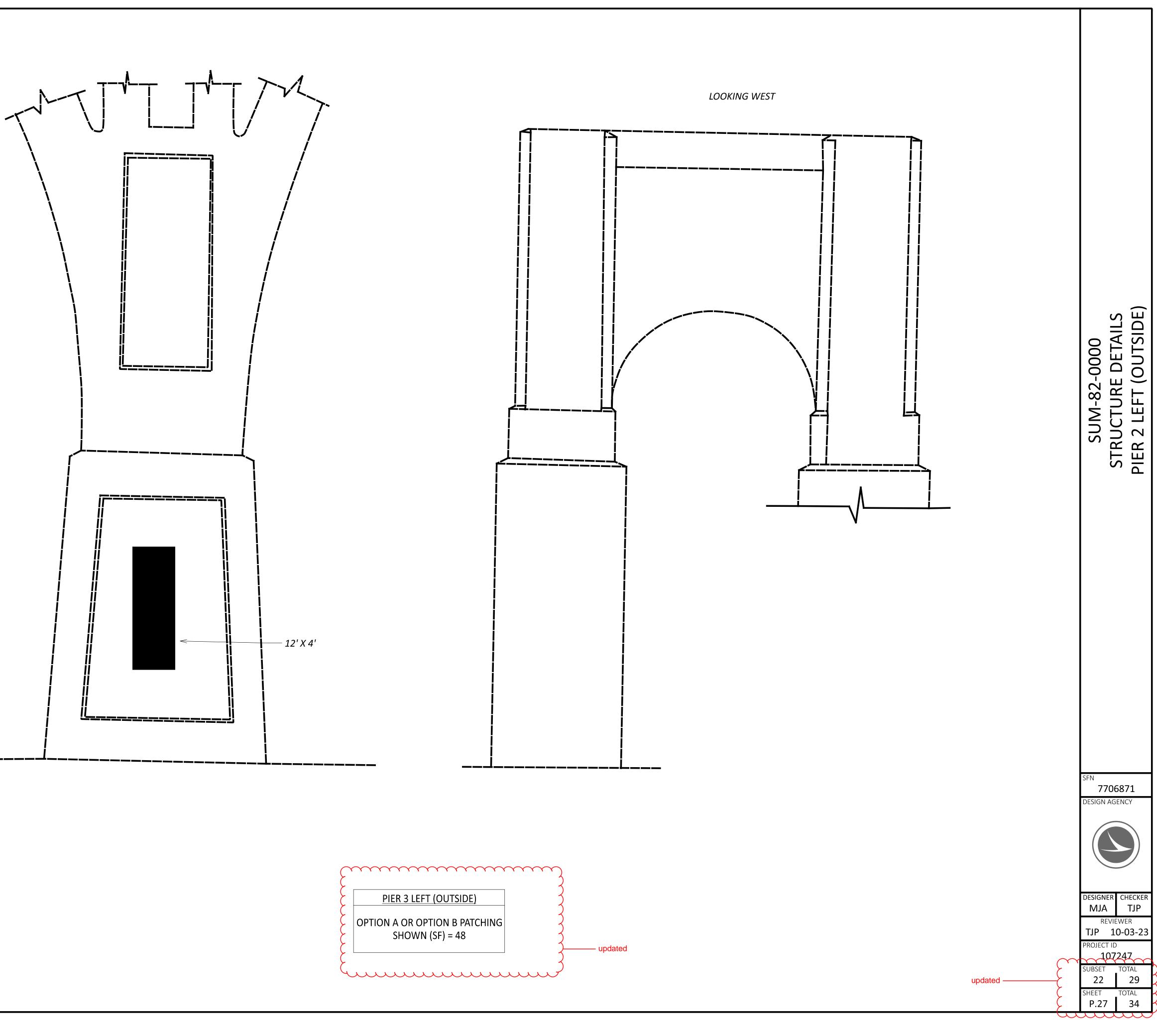
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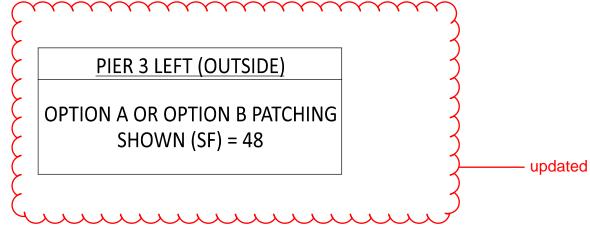




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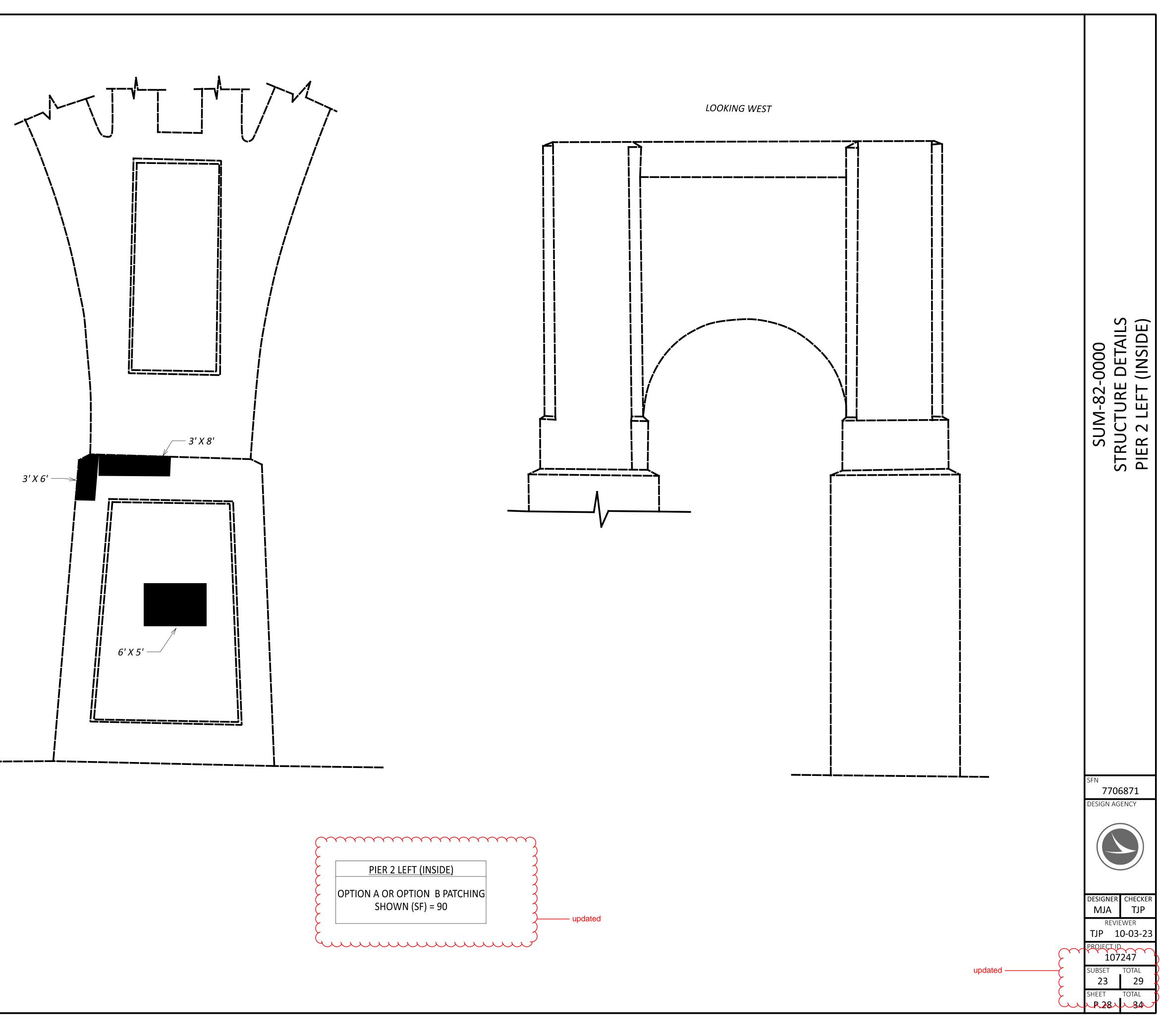
2 Left PAPERSIZE: 34x22 (in.) DATE: 4/21/2025 TIME: 1:34:50 PM USER: mandrasi y.com:ohiodot-pw-02\Documents\01 Active Projects\District 04\Summit\107247\400 et 19B -ot-pw.bei

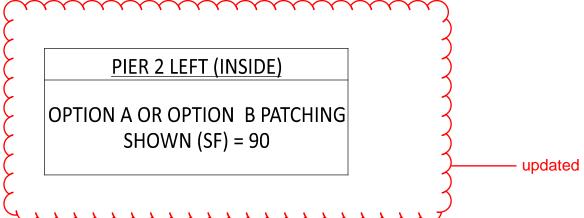




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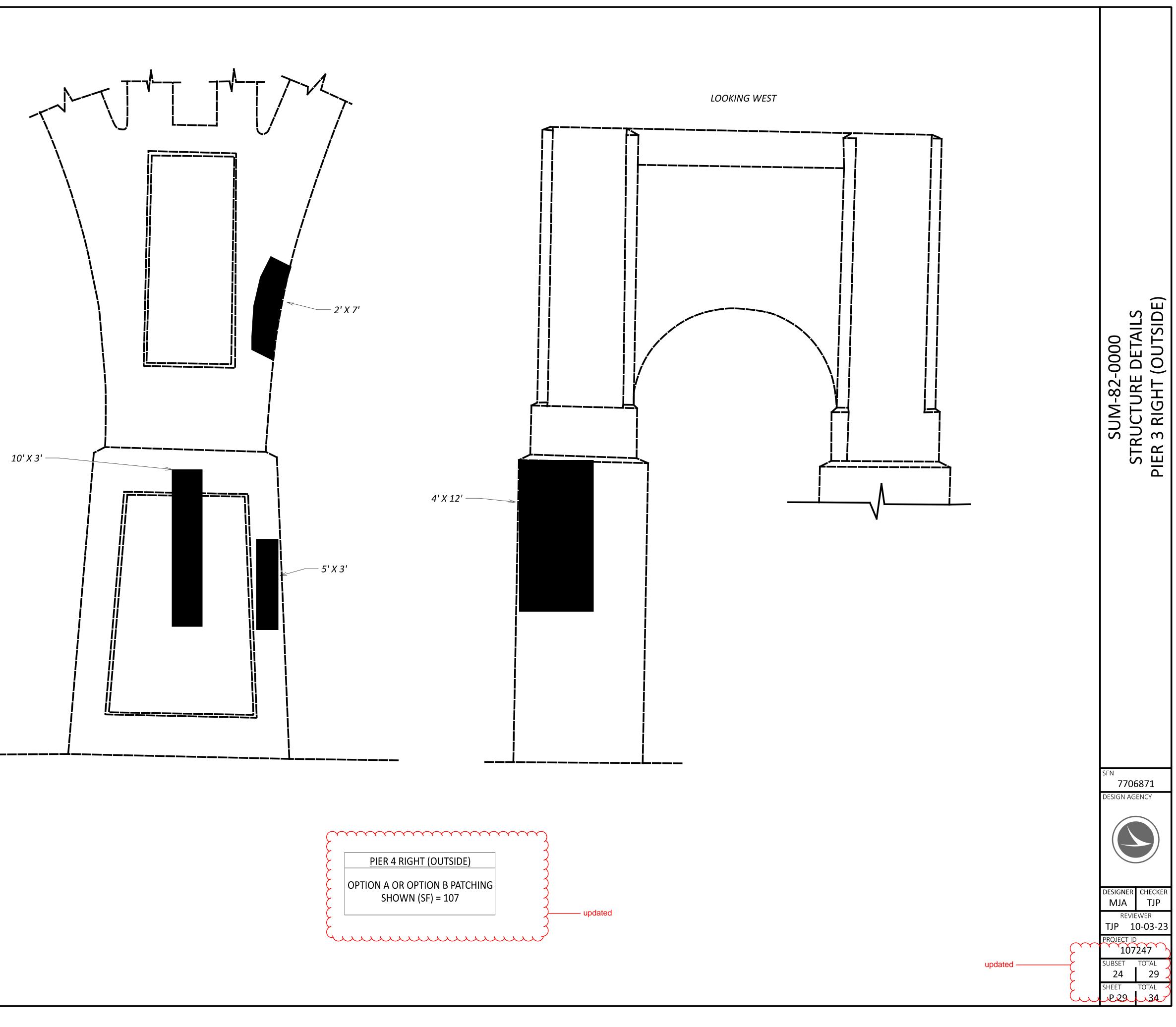
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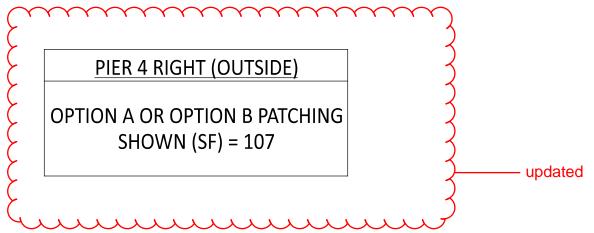




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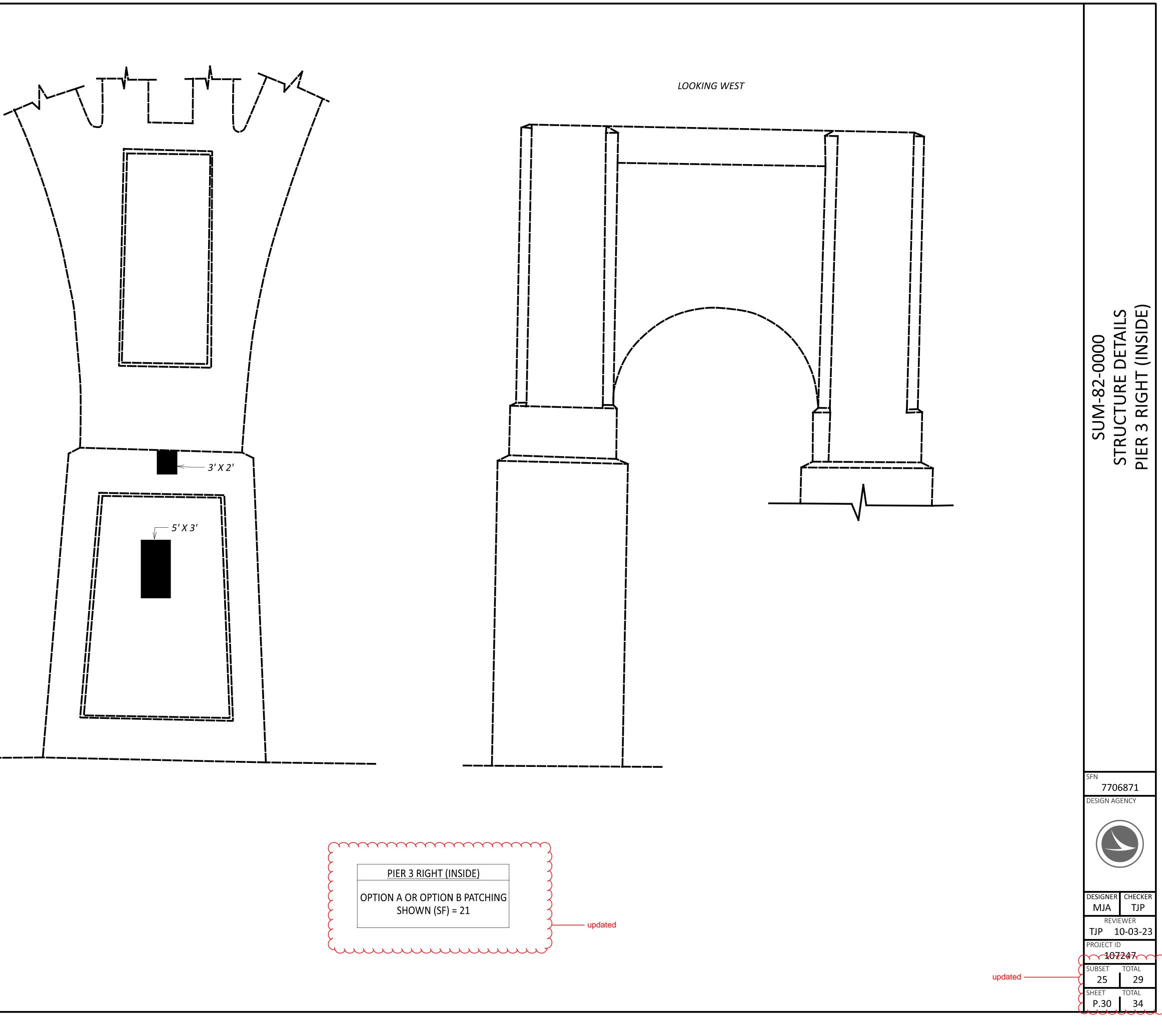
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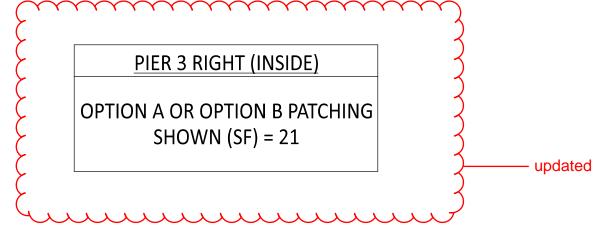




SUM-82-0.00 MODEL: Sheet 18 - Pier 3 Right PAP

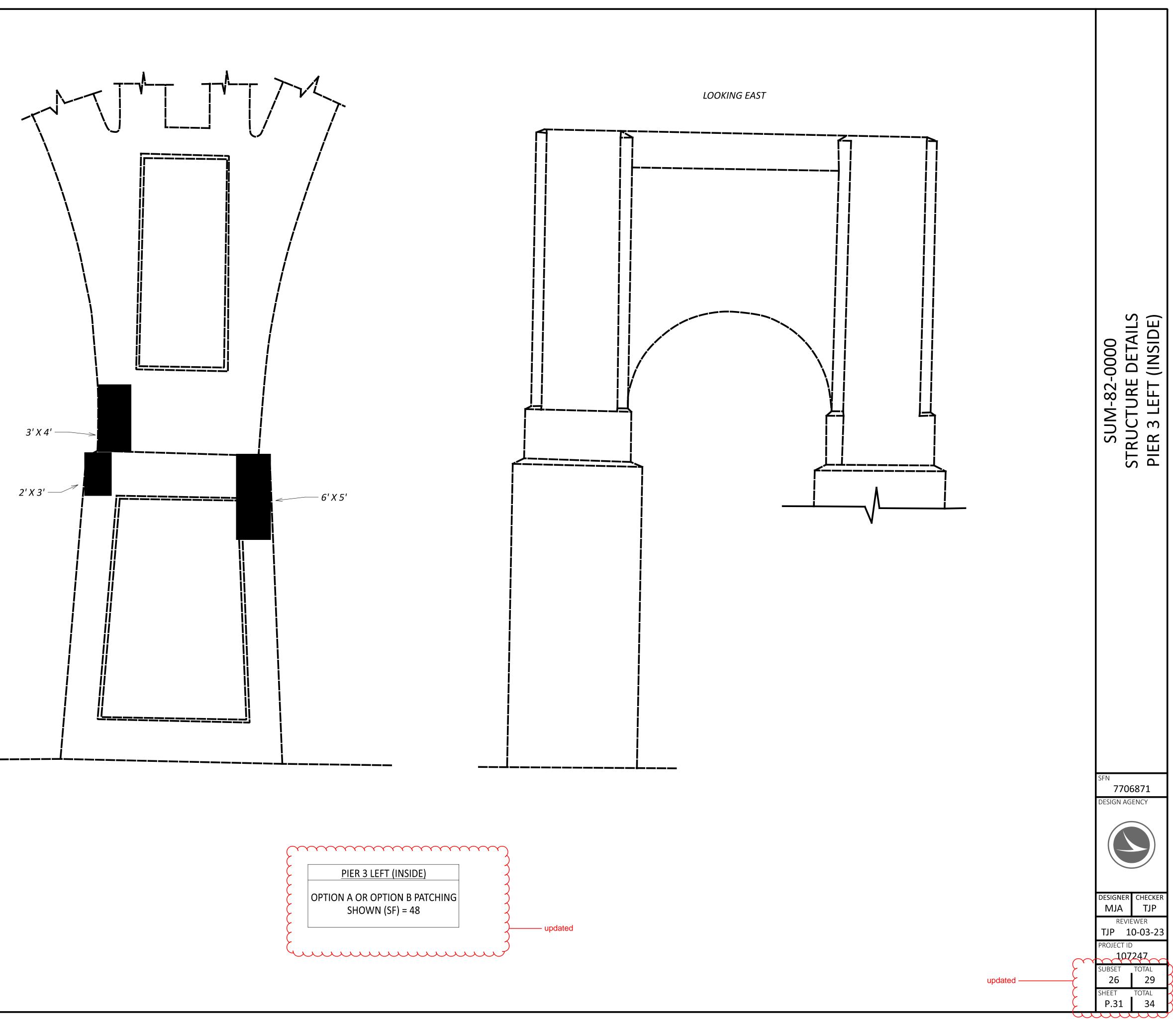
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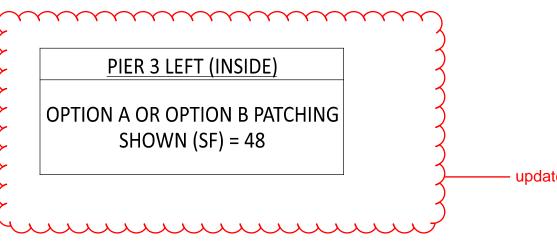




SUM-82-0.00 MODEL: Sheet 19 - Pier 3 Left PAPE

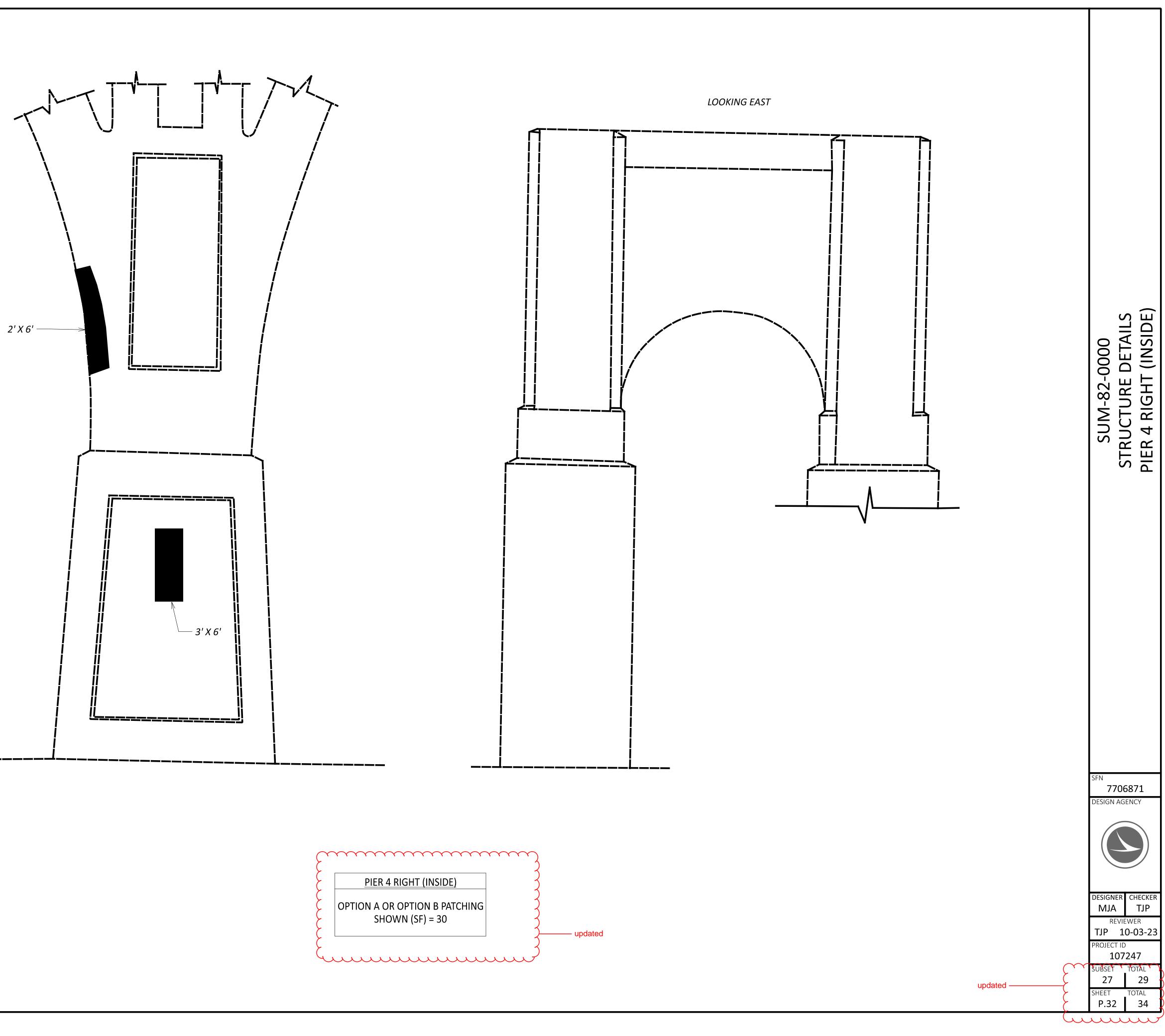
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SUM-82-0.00 MODEL: Sheet 20 - Pier 4 Right PAP

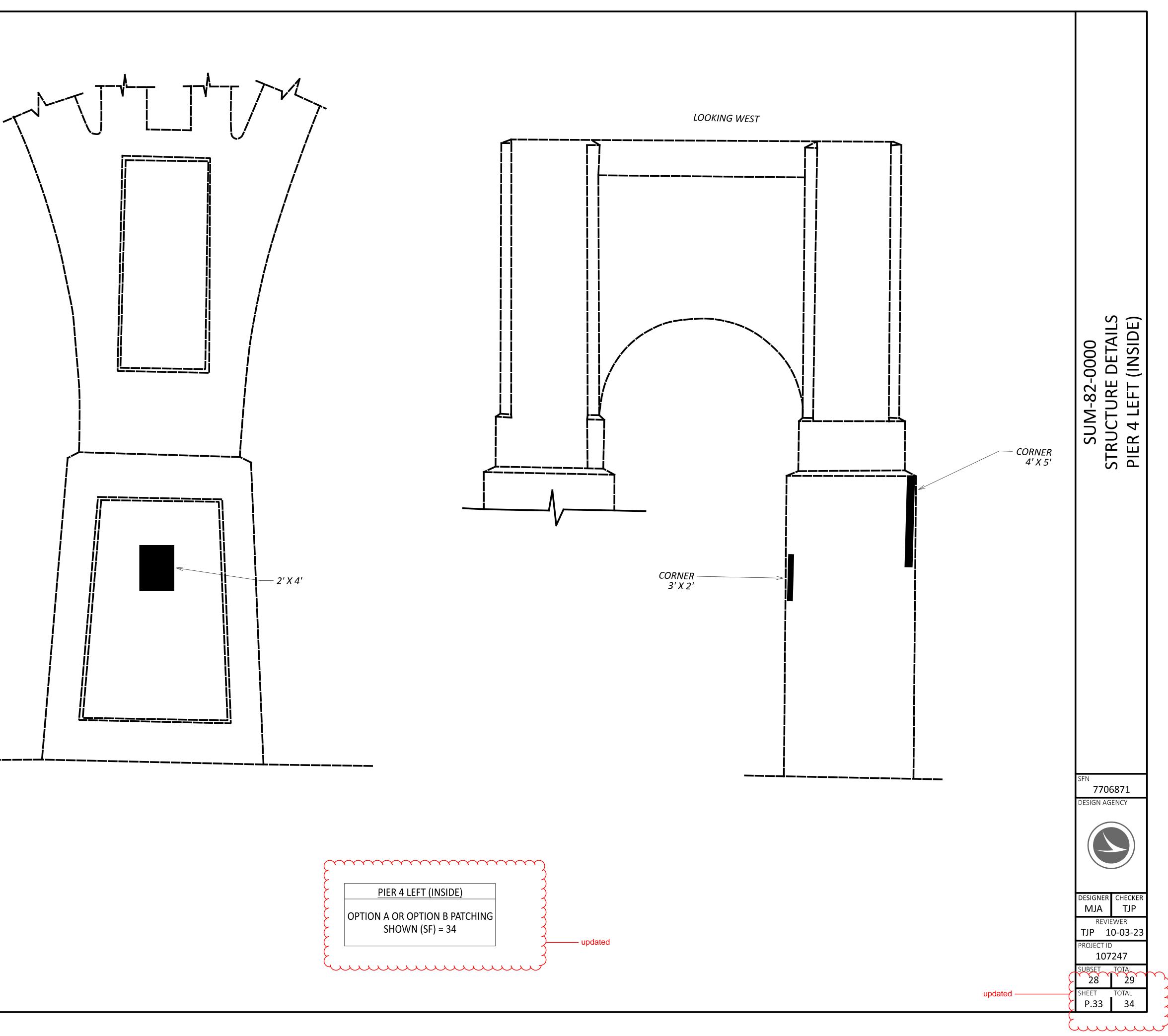
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SUM-82-0.00 MODEL: Sheet 21 - Pier 4 Left PAPE

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SUM-82-0.00 MODEL: Sheet 22 - Pier 5 Right PAP

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