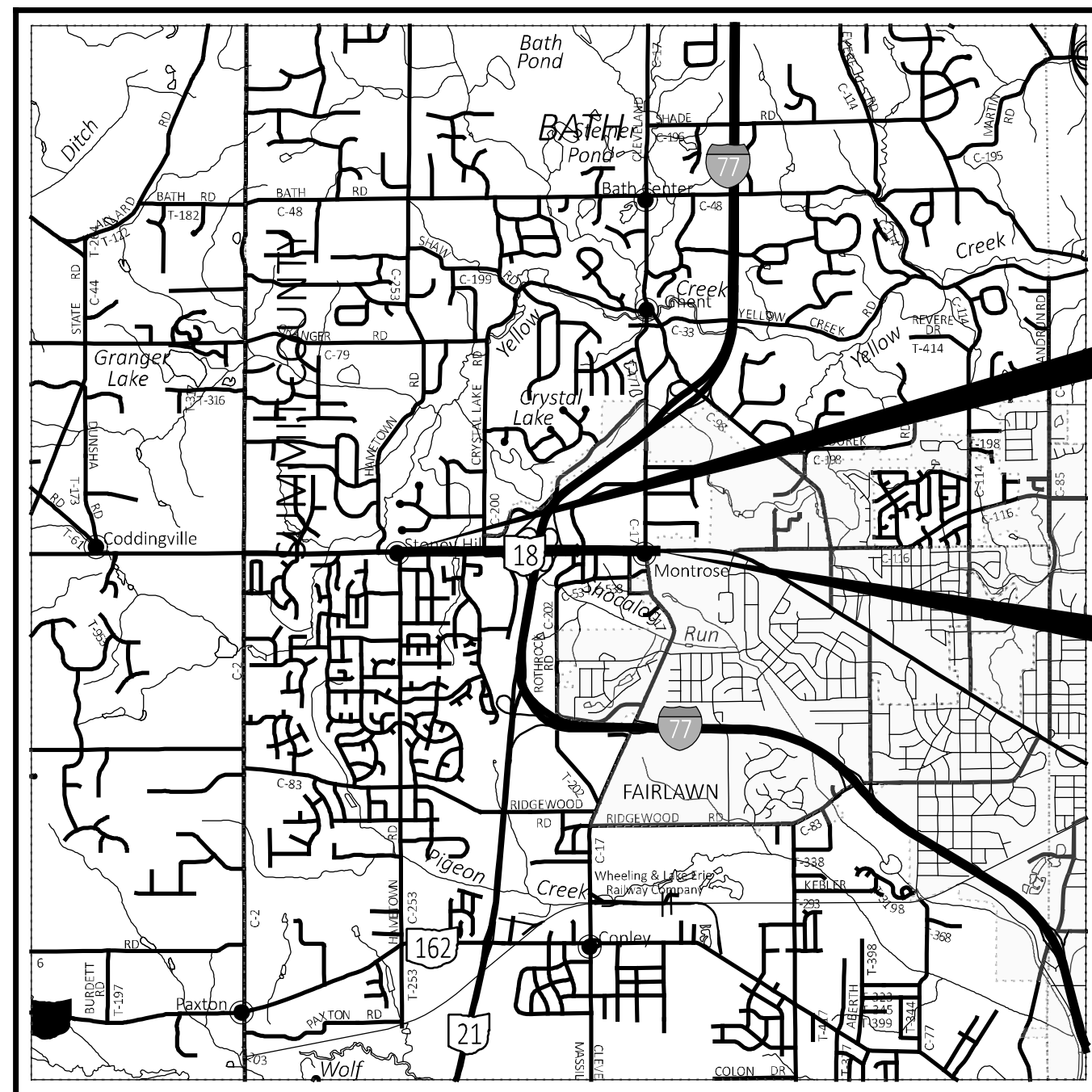


STATE OF OHIO DEPARTMENT OF TRANSPORTATION

SUM-18-1.01

BATH & COPLEY TOWNSHIPS



LOCATION MAP

LATITUDE: 41°8'9" LONGITUDE: -81°39'5"



PORTION TO BE IMPROVED	—————
INTERSTATE HIGHWAY	=====
FEDERAL ROUTES	=====
STATE ROUTES	=====
COUNTY & TOWNSHIP ROADS	=====
OTHER ROADS	—————

DESIGN DESIGNATION

CURRENT ADT (2022)	40685
DESIGN FUNCTIONAL CLASSIFICATION:	
RURAL PRINCIPAL ARTERIAL OTHER	
NHS PROJECT	YES

DESIGN EXCEPTIONS

NONE

ADA DESIGN WAIVERS

NONE

INDEX OF SHEETS:

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FEDERAL PROJECT NUMBER

E200(183)

RAILROAD INVOLVEMENT

NONE

PROJECT DESCRIPTION

RESURFACING OF SUM SR 18 FROM 1.01 TO 2.64.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA:	0.64 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA:	0.25 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA:	N/A (NOI NOT REQUIRED) *
* ROUTINE MAINTENANCE PROJECT	

2023 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS, CHANGES LISTED IN THE PROPOSAL, AND THE SUPPLEMENTAL SPECIFICATION 800 VERSION INDICATED ON THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

Arthur G. Noirot Jr.
Arthur G. Noirot Jr., P.E.
District 04 Deputy Director

Jack Marchbanks
Jack Marchbanks, PhD
Director, Department of Transportation

UNDERGROUND UTILITIES
Contact Two Working Days Before You Dig

OHIO811.org
Before You Dig

OHIO811, 8-1-1, or 1-800-362-2764
(Non members must be called directly)

PLAN PREPARED BY:
ODOT DISTRICT 4 - CAPITAL PROGRAMS
2088 S. ARLINGTON RD
AKRON, OH 44306

STANDARD CONSTRUCTION DRAWINGS						SUPPLEMENTAL SPECIFICATIONS		SPECIAL PROVISIONS	
BP-3.1	1/21/22	TC-41.20	10/18/13			800-2023	7/21/23		
BP-3.2	1/18/19	TC-42.10	10/18/13			809	4/21/23		
BP-5.1	7/15/22	TC-42.20	10/18/13			821	4/20/12		
BP-7.1	1/20/23	TC-52.10	10/18/13			831	4/21/23		
		TC-52.20	1/15/21			832	7/15/22		
MT-95.31	7/19/19	TC-65.10	1/17/14			921	4/20/12		
MT-95.32	4/19/19	TC-65.11	7/15/22			931	4/21/23		
MT-95.60	4/19/19	TC-71.10	4/21/23						
MT-97.10	4/19/19	TC-74.10	7/21/23						
MT-97.12	1/20/17	TC-82.10	7/19/19						
MT-98.10	1/17/20								
MT-98.20	4/19/19								
MT-99.20	4/19/19								
MT-101.90	7/17/20								
MT-105.10	1/17/20								

added MT-98.29 and MT-98.30

ENGINEER'S SEAL

REBECCA M. MOCARSKI
E-68469
REGISTERED PROFESSIONAL ENGINEER

Rebecca MocarSKI

2023.10.05 11:32:53-04'00"

SUM-18-1.01

MODEL: Sheet_SurfFI PAPER SIZE: 34x22 (in.) DATE: 10/4/2023 TIME: 11:23:44 AM USER: astrub pwc:\ohiodot-pw-bentley.com\ohiodot-pw-02\Documents\07 Active Projects\District 04\Summit\110644\400-Engineering\Roadway\Sheets\110644_GT001.dgn

TITLE SHEET

DESIGN AGENCY	
DESIGNER	JMW
REVIEWER	RMM 7/3/23
PROJECT ID	110644
SHEET	P.1
TOTAL	14

UTILITIES

THE CONTRACTOR SHALL USE THE FOLLOWING PROCEDURE AT EACH LOCATION WHERE WORK IS PERFORMED, IN ACCORDANCE WITH SECTIONS 105.07 AND 107.16 IN THE CONSTRUCTION AND MATERIALS SPECIFICATIONS.

THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER, OHIO811, THE OHIO DEPARTMENT OF TRANSPORTATION DISTRICT 4 HEADQUARTERS (MICHELLE CHANEY AT 330-786-2267) AND ALL NON REGISTERED UTILITY OWNERS AT LEAST TWO (2) WORKING DAYS PRIOR TO COMMENCING CONSTRUCTION OPERATIONS IN ALL AREAS.

THE LOCATION OF EXISTING UNDERGROUND UTILITIES ARE NOT SHOWN ON THE PLANS, BUT CAN BE OBTAINED FROM THE OWNERS OF THE UTILITIES. THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE TO UTILITIES.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

PAVEMENT MARKING LANE WIDTHS

THE NORMAL LANE WIDTH FOR THE PAVEMENT MARKINGS ON THIS PROJECT SHALL BE AS FOLLOWS:

ROUTE	S.L.M. TO S.L.M.	LANE WIDTH
SR 18	1.01 2.64	12'

PAVEMENT MARKING DETAILS

THE PAVEMENT MARKING DETAIL SHEETS HAVE BEEN SUPPLIED AS REFERENCE DOCUMENTS FOR THIS PROJECT AND ARE AVAILABLE ON THE ODOT FTP SITE AT <https://ftp.dot.state.oh.us/pub/contracts/Attach/> FOR THIS PROJECT. FOR ANY LOCATIONS THAT PAVEMENT MARKING DETAILS HAVE NOT BEEN MADE AVAILABLE TO THE CONTRACTOR, IT WILL BE THE CONTRACTORS RESPONSIBILITY TO PUT BACK NEW PAVEMENT MARKINGS IN THE ORIGINAL LOCATIONS.

PROFILE AND ALIGNMENT

PLACE THE PROPOSED PAVEMENT TO FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT. (PREVIOUS CONSTRUCTION PLANS, PROJECT NO. 150391, SHOWING THE ORIGINAL ALIGNMENT AND PROFILE, ARE AVAILABLE FOR INSPECTION AT THE ODOT DISTRICT 4 OFFICE). PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY (WITH A UNIFORM THICKNESS OF 1.5 INCHES).

PROPOSAL NOTE 420 DELETED

~~SMOOTHNESS PN 420 IS REQUIRED FOR SUM SR-18 SLM 1.01-2.12.~~

INTERSECTIONS

INTERSECTIONS WILL BE RESURFACED 10 FT. BEYOND THE EDGE LINE, UNLESS OTHERWISE DIRECTED BY THE ENGINEER OR INDICATED IN THE PLAN. INTERSECTIONS SHALL BE PAVED AFTER COMPLETION OF THE SURFACE COURSE. A BUTT JOINT, AS PER STANDARD CONSTRUCTION DRAWING BP-3.1, SHALL BE USED TO PROVIDE A SMOOTH TRANSITION TO THE EXISTING PAVEMENT. USE THE SAME ASPHALT CONCRETE AS THE MAINLINE PAVEMENT UNLESS SHOWN OTHERWISE ON THE ASPHALT CONCRETE CALCULATIONS SHEET. ANY GRADING OR PRIME NECESSARY TO ACCOMPLISH THIS WORK SHALL BE INCLUDED IN THE COST OF THE PERTINENT BID ITEM.

DRIVEWAYS

THE CONTRACTOR WILL NOT BE PERMITTED TO LEAVE A DIFFERENCE IN ELEVATION BETWEEN THE MAINLINE ASPHALT SURFACE COURSE AND THE EXISTING DRIVEWAYS. IF APPROVED BY THE ENGINEER, AN ASPHALT WEDGE WITH A MINIMUM WIDTH OF 2' MAY BE PLACED EITHER ON THE ROADWAY SHOULDER OR DRIVEWAY DEPENDENT UPON WHICH SIDE IS HIGH. A QUANTITY OF MAINLINE SURFACE COURSE ASPHALT HAS BEEN PROVIDED IN THE CALCULATIONS AND GENERAL SUMMARY TO PERFORM THIS ITEM OF WORK.

IN THE EVENT THAT THE ENGINEER DETERMINES ADDITIONAL WORK IS NECESSARY TO PROPERLY ADDRESS FIELD CONDITIONS, AN ITEM FOR WEARING COURSE REMOVED HAS BEEN PROVIDED. THE REMOVAL DEPTH IS DEPENDENT UPON THE ELEVATION DIFFERENCE AND ALLOW FOR 1"-2" OF COMPACTED ASPHALT MATERIAL TO BE PLACED.

LINEAR GRADING

AREAS WHERE THE SHOULDER IS HIGHER THAN THE EDGE OF PAVEMENT WILL BE GRADED TO PROVIDE POSITIVE DRAINAGE. THIS WORK WILL ONLY BE PERFORMED IN AREAS NECESSARY AND WILL NOT BE PERFORMED ON THE ENTIRE PROJECT. AREAS FOR THE WORK WILL BE MARKED BY THE PROJECT ENGINEER. UNDER NO CIRCUMSTANCES WILL THIS WORK BE PERFORMED CONCURRENTLY WITH ANY OTHER OPERATION.

GRADING WILL BE ACCOMPLISHED BY THE REMOVAL OF MATERIAL TO PROVIDE A 0.08 POSITIVE SLOPE. THE GRADED AREAS WILL BE COMPACTED TO A SUFFICIENT DENSITY TO PREVENT EROSION UNTIL SEEDING AND MULCHING IS PERFORMED. ALL EXCESS MATERIAL WILL BE REMOVED FROM THE BERMS AND WILL BE DISPOSED OF OFF THE PROJECT BY THE CONTRACTOR. SEEDING AND MULCHING, FERTILIZER AND LIME WILL BE PERFORMED WITHIN A PERIOD NOT TO EXCEED 10 DAYS AFTER THE LINEAR GRADING.

THE QUANTITY OF ITEM 209 IS NOT PERMITTED TO BE INCREASED. REDUCTIONS IN QUANTITIES ARE PERMITTED AS DETERMINED BY THE PROJECT ENGINEER.

ALL MATERIALS, LABOR, EQUIPMENT, TOOLS, AND INCIDENTALS NECESSARY TO COMPLETE THIS WORK WILL BE INCLUDED IN THE UNIT PRICE FOR THE PERTINENT BID ITEM. THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

- 209, LINEAR GRADING, 115 STA.
- 659, SEEDING AND MULCHING, 3,097 SQ YD
- 659, COMMERCIAL FERTILIZER, 0.417 TON
- 659, LIME, 0.639 ACRES
- 659, WATER, 16.7 M. GAL.

ITEM 203 - EXCAVATION (FOR PAVEMENT REPAIR)

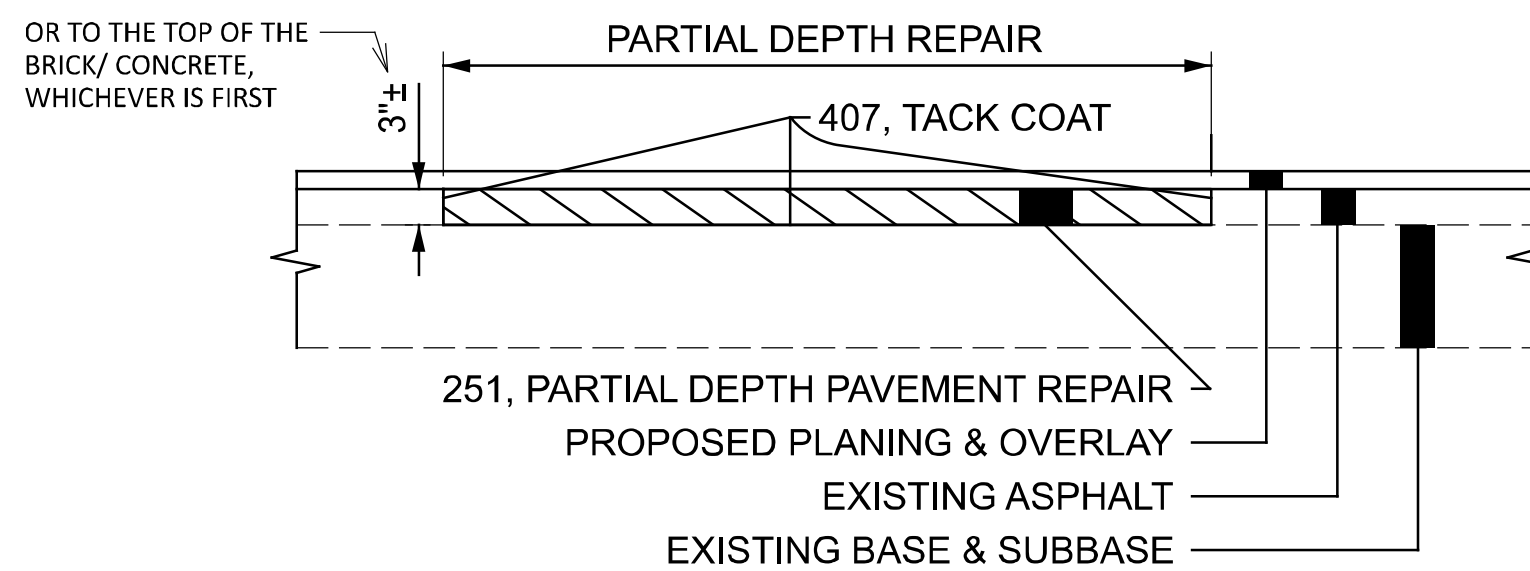
THIS ITEM OF WORK SHALL CONSIST OF REMOVING AND DISPOSING OF ALL UNSUITABLE MATERIAL BY EXCAVATING THE EXISTING SUBGRADE AND SUBBASE TO AN AVERAGE DEPTH OF 6 INCHES OR AS DIRECTED BY THE ENGINEER. EXACT LIMITS OF REMOVAL SHALL BE DETERMINED BY THE ENGINEER. ALL EQUIPMENT, LABOR, TOOLS, AND INCIDENTALS NECESSARY TO COMPLETE THIS ITEM SHALL BE INCLUDED IN THE UNIT PRICE BID FOR ITEM 203 EXCAVATION (FOR PAVEMENT REPAIR). THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

- 203, EXCAVATION (FOR PAVEMENT REPAIR) 100 CU YD

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (441)

A QUANTITY OF THIS ITEM SHALL BE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER. THE ITEM SHALL CONSIST OF REPAIRING EXISTING LOCATIONS EXHIBITING SURFACE DETERIORATION AND PLACING ITEM 441 ASPHALT CONCRETE, TYPE 2. THE ASPHALT CONCRETE SHALL BE COMPACTED WITH A TYPE I PNEUMATIC TIRE ROLLER AND A STEEL WHEEL ROLLER AS PER 401.13. IT IS NOT THE INTENT TO REPAIR EVERY DETERIORATED AREA WITHIN THE PROJECT. PAVEMENT REPAIRS WILL BE MARKED IN THE FIELD BY THE PROJECT ENGINEER ACCORDING TO CMS 251.02. MINIMUM WIDTH IS 2'. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THIS ITEM SHALL BE PERFORMED AFTER THE COMPLETION OF MAINLINE PAVEMENT PLANING AND PRIOR TO THE PLACEMENT OF ASPHALT ON THE MILLED SURFACE. PAYMENT SHALL BE BASED ON THE ACTUAL NUMBER OF SQUARE YARDS OF PAVEMENT REPAIR.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:
251, PARTIAL DEPTH PAVEMENT REPAIR (441), 1750 SQ. YD.



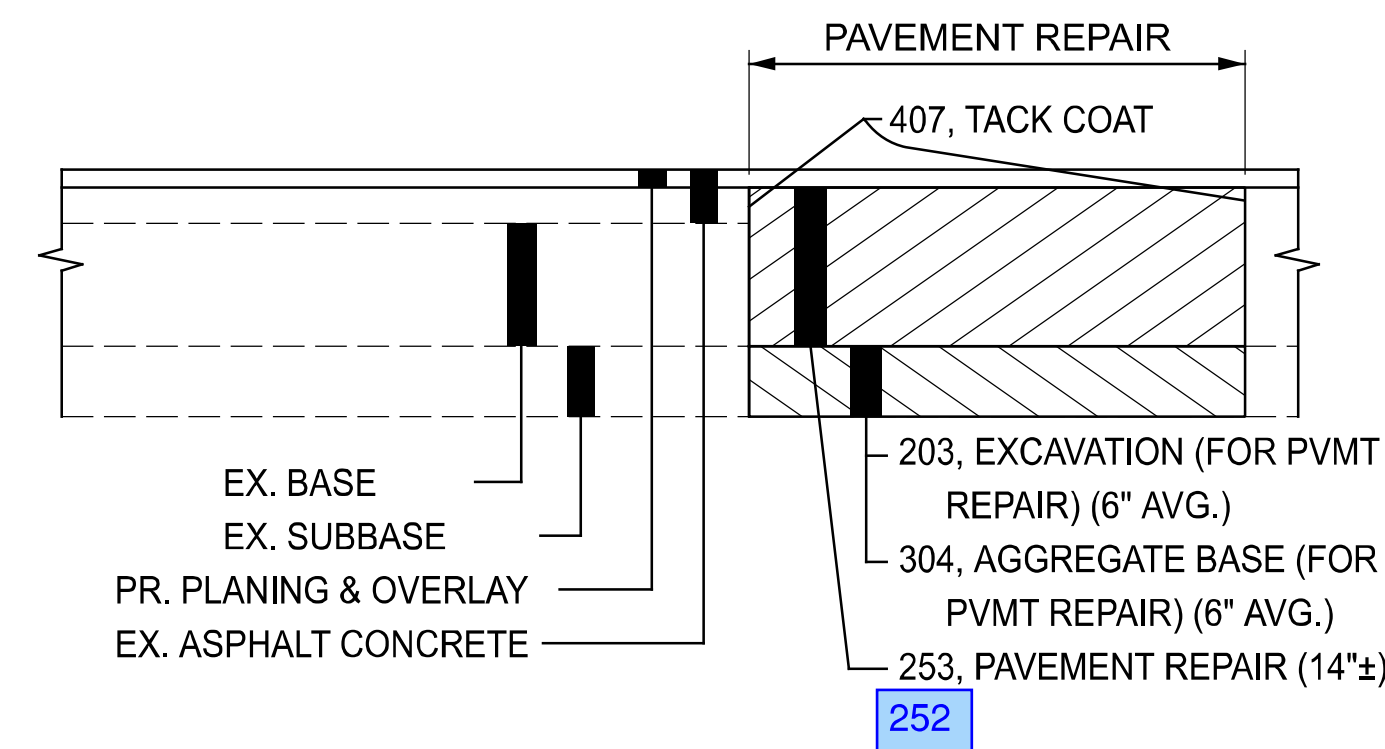
252 **ITEM 253 - PAVEMENT REPAIR**

A QUANTITY OF THIS ITEM SHALL BE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER. THIS ITEM SHALL CONSIST OF CUTTING AND REMOVING DETERIORATED PAVEMENT FULL DEPTH AND PLACING 14" 301 ASPHALT CONCRETE BASE, PG64-22. THE MAXIMUM COMPACTED DEPTH OF ANY ONE LAYER SHALL BE 6 INCHES. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THIS ITEM SHALL BE PERFORMED BEFORE THE COMPLETION OF MAINLINE PAVEMENT PLANING AND PRIOR TO THE PLACEMENT ~~OF ASPHALT ON THE MILLED SURFACE.~~

IT IS NOT THE INTENT TO REPAIR EVERY DETERIORATED AREA WITHIN THE PROJECT. THE ENGINEER SHALL DETERMINE WHICH AREAS ARE TO BE REPAIRED. PAYMENT SHALL BE BASED ON THE ACTUAL NUMBER OF SQUARE YARDS OF PAVEMENT REMOVED AND REPLACED TO THE LIMITS DESIGNATED BY THE ENGINEER.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

- 252 253, PAVEMENT REPAIR, 600 SQ YD
- 252, FULL DEPTH PAVEMENT SAWING, 3600 FT



ITEM 304 - AGGREGATE BASE (FOR PAVEMENT REPAIR)

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN PROVIDED AND SHALL BE USED AS DIRECTED BY THE ENGINEER TO BACKFILL AREAS WHICH WERE EXCAVATED UNDER ITEM 203 EXCAVATION (FOR PAVEMENT REPAIR). THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:
304, AGGREGATE BASE (FOR PAVEMENT REPAIR) 100 CU YD

ITEM 408 - PRIME COAT, AS PER PLAN

APPLY "MC-70" AT A RATE OF 0.4 GALLONS PER SQUARE YARD, OR AS DETERMINED BY THE ENGINEER, TO THE COMPLETED COMPACTED AGGREGATE SHOULDER.

CONSTRUCTION NOISE DELETED

~~ACTIVITIES AND LAND USE ADJACENT TO THIS PROJECT MAY BE AFFECTED BY CONSTRUCTION NOISE. IN ORDER TO MINIMIZE ANY ADVERSE CONSTRUCTION NOISE IMPACTS, DO NOT OPERATE POWER-OPERATED CONSTRUCTION-TYPE DEVICES AT ANY TIME IN SUCH A MANNER THAT THE NOISE CREATED SUBSTANTIALLY AND NECESSARILY EXCEEDS THE NOISE CUSTOMARILY ATTENDANT TO THE REASONABLE AND EFFICIENT PERFORMANCE OF SUCH EQUIPMENT. USE OF SUCH EQUIPMENT SHOULD BE LIMITED AS MUCH AS POSSIBLE TO DAYLIGHT HOURS. EQUIPMENT AND VEHICLES STAGING AREAS SHOULD BE LOCATED AS FAR FROM NOISE-SENSITIVE AREAS (I.E. RESIDENCES AND PARKLANDS) AS PRACTICABLE.~~

~~HOURS OF OPERATION SHALL CONFORM WITH ANY LOCAL ORDINANCE. ANY DEVIATION FROM THIS REQUIRES PRIOR APPROVAL FROM THE LOCAL JURISDICTIONS.~~



MAINTENANCE OF TRAFFIC

THIS ITEM SHALL CONSIST OF MAINTENANCE OF TRAFFIC ON EXISTING ROADWAYS AND RAMPS IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, CURRENT EDITION, LATEST REVISION, THE SPECIFICATIONS AND THE FOLLOWING:

1. A MINIMUM OF ONE TEN FOOT LANE IN EACH DIRECTION SHALL BE MAINTAINED ON THE EXISTING PAVEMENT OR COMPLETED PAVEMENT DURING CONSTRUCTION OF THE WORK.
2. THE CONTRACTOR SHALL INFORM THE DISTRICT OFFICE (330) 786-2208, EIGHTEEN (18) DAYS PRIOR TO THE BEGINNING OF WORK.
3. NO LANE CLOSURES SHALL BE IMPLEMENTED BETWEEN THE HOURS OF 7:00AM AND 8:00PM.
4. LANE RESTRICTIONS OR LANE REDUCTIONS SHALL NOT BE PERMITTED AFTER NORMAL WORKING HOURS. NORMAL WORKING HOURS SHALL BE THOSE HOURS DURING WHICH THE CONTRACTOR HAS A FULL COMPLEMENT OF EMPLOYEES AND EQUIPMENT ACTIVELY REMOVING AND/OR PLACING PAVEMENT MATERIALS.
5. ALL FULL DEPTH PAVEMENT REMOVAL AND REPLACEMENT OPERATIONS SHALL BE COMPLETED THE SAME DAY THE EXCAVATION IS MADE. IF THE CONTRACTOR CANNOT COMPLETE THE WORK, THE EXCAVATION SHALL BE BACKFILLED OR PROTECTED AS PER STANDARD CONSTRUCTION DRAWING MT-101.90.
6. TRUCK MOUNTED ATTENUATORS [TMA'S] SHALL BE USED AS SHOWN IN THE STANDARD CONSTRUCTION DRAWINGS.
7. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE PERMITTED TO HAVE SUCCESSIVE WORK ZONES UNLESS THE DISTANCE BETWEEN THE DRUMS, BARRICADES OR CONES EXCEEDS TWO (2) MILES RURAL OR ONE [1] MILE URBAN.
8. FOR ROUTES NOT ON THE PERMITTED LANE CLOSURE CHART, ONLY DURING OFF-PEAK PERIODS (ie ANY PERIOD OTHER THAN 6-8AM AND 3-6PM) SHALL THE CONTRACTOR INSTALL AND SUBSEQUENTLY RESET ALL TRAFFIC CONTROL NECESSARY FOR THE WORK ZONE FOR EACH CONSTRUCTION PHASE. DELETED
9. IN ADDITION TO THE REQUIREMENTS OF 614.11 WORK ZONE PAVEMENT MARKINGS, AT THE END OF EACH DAY OF WORK, THE CONTRACTOR SHALL REPLACE (WITH WORK ZONE MARKINGS) ALL LANE, CENTER, STOP OR CHANNELIZING LINES THAT WERE REMOVED OR COVERED DURING THE PAVEMENT REMOVAL OR PLACEMENT OPERATIONS. QUANTITIES FOR SUCH PLACEMENT ARE CARRIED AS PART OF THE ITEMS LISTED UNDER 614 WORK ZONE PAVEMENT MARKINGS. RENUMBERED NUMBERS 8 thru 13
10. A QUANTITY OF 10 CU. YDS. OF ITEM 614 ASPHALT CONCRETE FOR MAINTAINING TRAFFIC SHALL BE PROVIDED FOR USE IN MAINTAINING PAVEMENT, SHOULDERS AND OTHER LOCATIONS AS DIRECTED BY THE ENGINEER.
11. PRIOR TO OPENING TO TRAFFIC EACH LANE SHALL BE IN A SAFE, PASSABLE CONDITION. ALL TRANSVERSE JOINTS SHALL EXTEND ACROSS THE FULL LANE AND SHOULDER WIDTH AND EACH LANE SHALL BE FREE FROM UNEVEN LONGITUDINAL JOINTS. THE CONTRACTOR SHALL PROVIDE ASPHALT WEDGES FOR TRANSVERSE JOINTS WHEREVER THERE ARE PAVEMENT ELEVATION DIFFERENCES.

12. THE CONTRACTOR SHALL INSTALL, MAINTAIN AND SUBSEQUENTLY REMOVE WORK ZONE MARKING SIGNS AND THEIR SUPPORTS WITHIN THE WORK LIMITS. THESE SIGNS INCLUDE "NO EDGE LINES", "DO NOT PASS" AND "PASS WITH CARE". ALL OTHER SIGNS WILL BE INCIDENTAL TO THE LUMP SUM PAY ITEM 614 MAINTAINING TRAFFIC UNLESS SEPARATELY ITEMIZED IN THE PLANS. A QUANTITY OF ITEM 614 WORK ZONE MARKING SIGNS HAS BEEN INCLUDED IN THE PLANS AS PER CMS 614.04.

13. THE CONTRACTOR SHALL SET A WORK ZONE AT THE REQUEST OF THE ENGINEER TO ALLOW THE LAYOUT OF THE PARTIAL/FULL DEPTH PAVEMENT REPAIR AREAS. THIS WORK IS INCIDENTAL TO ITEM 614 MAINTAINING TRAFFIC.

THE FOLLOWING QUANTITIES SHALL BE USED FOR THE MAINTENANCE OF TRAFFIC ON THIS PROJECT:

PHASE I - PLANED SURFACE
 614, WORK ZONE CENTER LINE, CLASS I, 1.66 MILE
 614, WORK ZONE LANE LINE, CLASS I, 6", 5.46 MILE
 614, WORK ZONE STOP LINE, CLASS 1, 548 FT
 614, WORK ZONE CHANNELIZING LINE, CLASS 1, 8" , 5825 FT
 614, WORK ZONE MARKING SIGN,(ALL PHASES) 7 EACH

PHASE II - SURFACE COURSE
 614, WORK ZONE CENTERLINE, CLASS III, 642 PAINT 1.66 MILE
 614, WORK ZONE LANE LINE, CLASS III, 6", 642 PAINT 5.46 MILE
 614, WORK ZONE STOP LINE, CLASS III, 642 PAINT 548 FT
 614, WORK ZONE CHANNELIZING LINE, CLASS III, 8", 642 PAINT 5825 FT

TO BE USED AS DIRECTED BY THE ENGINEER
 614, WORK ZONE EDGE LINE, CLASS III, 6", 2.30 MILE

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ADVANCED NOTICE TO PAVE

THE CONTRACTOR SHALL SUBMIT FOR APPROVAL TO THE DISTRICT CONSTRUCTION ENGINEER A DETAILED SCHEDULE 15 DAYS PRIOR TO THE PLACEMENT OF THE OVERLAY COURSES, ON HOW THEY PROPOSE TO PROSECUTE THE PAVING OPERATIONS. THE DETAILS SHALL SHOW THE ORDER OF PERFORMANCE OF EACH STAGE (START TO FINISH) OF THE WORK INCLUDING THE MAINTENANCE OF TRAFFIC THAT WILL BE USED.

DROPOFFS

THE CONTRACTOR WILL NOT BE PERMITTED TO LEAVE A DIFFERENCE IN ELEVATION BETWEEN THE MAINLINE MILLED SURFACES, AND ASPHALT SURFACE COURSE AND SIDE STREET APPROACHES/DRIVEWAYS GREATER THAN 1.25 INCH. THE CONTRACTOR SHALL PLACE A 12:1 ASPHALT WEDGE FOR ALL RESULTING ELEVATION DIFFERENCES GREATER THAN 1.25 INCH PRIOR TO OPENING TO TRAFFIC. THE PAVING OF INTERSECTION APPROACHES AND DRIVEWAYS, PER THE NOTES ON SHEET XX, SHALL BE PERFORMED WITHIN 7 DAYS OF MAINLINE SURFACE COURSE BEING APPLIED AND A DROPOFF BEING CREATED BETWEEN THE NEW SURFACE COURSE AND THE MILLED/EXISTING SIDE ROAD OR DRIVEWAY SURFACE. THE CONTRACTOR MAY ELECT TO PLACE A 12:1 ASPHALT WEDGE IN LIEU OF COMPLETING THE PAVING, HOWEVER THE ASPHALT CONCRETE USED FOR THE WEDGE SHALL BE CONSIDERED INCIDENTAL TO ITEM 614 – MAINTAINING TRAFFIC AND SHALL INCLUDE THE REMOVAL OF THE WEDGE BEFORE THE INTERSECTION/DRIVEWAY IS PAVED.

TRAFFIC CONTROL INSPECTOR

THE CONTRACTOR SHALL DESIGNATE AN INDIVIDUAL OTHER THAN THE SUPERINTENDENT AND SUBJECT TO THE APPROVAL OF THE ENGINEER, TO CONTINUOUSLY INSPECT ALL TRAFFIC CONTROL DEVICES WHENEVER CONSTRUCTION WORK IS BEING PERFORMED WITHIN THE WORK LIMITS OF THE PROJECT. THE DESIGNATED INDIVIDUAL SHALL ALSO INSPECT ALL TRAFFIC DEVICES AT THE BEGINNING AND AT THE END OF EACH WORK DAY. THE DESIGNATED INDIVIDUAL OR A QUALIFIED REPRESENTATIVE SHALL ALSO BE AVAILABLE ON AN AROUND THE CLOCK BASIS TO REPAIR AND/OR REPLACE DAMAGED OR MISSING TRAFFIC CONTROL DEVICES. THESE INDIVIDUALS SHALL BE EQUIPPED WITH CELLULAR PHONES AND THEIR NAMES AND PHONE NUMBERS SHALL BE GIVEN TO THE PROJECT ENGINEER AT THE PRE-CONSTRUCTION MEETING. THE DESIGNATED INDIVIDUAL MAY HAVE OTHER CONSTRUCTION RELATED DUTIES AS LONG AS IMMEDIATE ATTENTION IS GIVEN TO TRAFFIC CONTROL. PAYMENT FOR THE SERVICES OF THE TRAFFIC CONTROL INSPECTOR SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614 MAINTAINING TRAFFIC.

TIME LIMITATION, TRAFFIC ON A MILLED SURFACE

THE MAXIMUM ALLOWABLE TIME FOR TRAFFIC TO BE PLACED ON A MILLED SURFACE SHALL BE 7 CONSECUTIVE CALENDAR DAYS. SHOULD THE CONTRACTOR FAIL TO MEET THIS REQUIREMENT, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF \$5000 PER DAY THAT THE TRAFFIC IS PLACED ON A MILLED SURFACE BEYOND THE SPECIFIED LIMIT.

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS & PIO
ROAD & RAMP CLOSURES	>= 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	4 CALENDAR DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	>= 2 WEEKS	14 CALENDAR DAYS
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERNS CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

PLACEMENT OF ASPHALT CONCRETE

TWO-WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT THAT ONE-WAY TRAFFIC WILL BE PERMITTED FOR MINIMUM PERIODS OF TIME CONSISTENT WITH THE REQUIREMENTS OF THE SPECIFICATIONS FOR PROTECTION OF COMPLETED ASPHALT CONCRETE COURSES.

ASPHALT PAVING LIMITATION

THE CONTRACTOR SHALL NOT ANTICIPATE OR SCHEDULE PLACING ASPHALT (ASPHALT SURFACE COURSE, ASPHALT INTERMEDIATE COURSE, ASPHALT CONCRETE BASE, ETC.) BETWEEN NOVEMBER 1 AND APRIL 1 WHEN SUBMITTING THEIR INITIAL BAR CHART PROGRESS SCHEDULE TO THE DISTRICT CONSTRUCTION ENGINEER (DCE) AS SPECIFIED IN CMS SECTION 108.02A. THIS LIMITATION SHALL ALSO INCLUDE INITIAL BASE LINE SCHEDULES AND ALL UPDATES IF A CPM SCHEDULE IS REQUIRED.

DESIGN AGENCY



DESIGNER
JMW

REVIEWER
RMM 7/3/23

PROJECT ID
110644

SHEET TOTAL
P.5 | 14

ITEM 614, MAINTAINING TRAFFIC (LANES OPEN DURING HOLIDAYS OR SPECIAL EVENTS)

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR SPECIAL EVENTS:

- NEW YEAR'S (OBSERVED) GENERAL/REGULAR ELECTION DAY ((NOV)
- TOTAL SOLAR ECLIPSE (4/8/24) THANKSGIVING
- MEMORIAL DAY CHRISTMAS (OBSERVED)
- FOURTH OF JULY (OBSERVED)
- LABOR DAY

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR SPECIAL EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY OR SPECIAL EVENT	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00 NOON FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00 NOON FRIDAY THROUGH 6:00 AM TUESDAY
MONDAY (TOTAL SOLAR ECLIPSE)	12:00 NOON FRIDAY THROUGH 6:00 AM WEDNESDAY
TUESDAY	12:00 NOON MONDAY THROUGH 6:00 AM WEDNESDAY
TUESDAY (GEN. /REG. ELECTION)	5:00 AM TUESDAY THROUGH 12:00 AM WEDNESDAY
WEDNESDAY	12:00 NOON TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00 NOON WEDNESDAY THROUGH 6:00 AM FRIDAY
THANKSGIVING	6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00 NOON THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00 NOON FRIDAY THROUGH 6:00 AM MONDAY

DURING THE SAME PERIODS, MAINTAIN PEDESTRAIN ACCESS IF PEDESTRIAN ACCESS WAS PRESENT PRIOR TO CONSTRUCTION.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT (PN 127).

FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

(THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL (IN ACTIVE CELLULAR PHONE AREAS) ALLOW REMOTE SIGN ACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS AND REVISIONS TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES. ONE REMOTE DATA INPUT DEVICE (LAPTOP COMPUTER PLUS MODEM OR EQUIVALENT) SHALL BE FURNISHED FOR USE BY THE DISTRICT TRAFFIC ENGINEER, OR EQUIVALENT, AND SHALL BE INSURED AGAINST THEFT.) THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 8 SIGN MONTH ~~ASSUMING 2 PCMS~~ **SIGNS FOR 4 MONTHS**

ADDED BELOW NOTE to Sheet 5 as # 13

ITEM 614 - MAINTAINING TRAFFIC - TIME LIMITATION FOR RAMPS
 Ramp closures shall be limited to between the hours of 10PM and 5AM. The closure of any ramp shall only be permitted while the contractor is actively working in the immediate vicinity of the ramp and shall require the approval of the project Engineer. The duration of any ramp closure shall be kept to a minimum and the ramp shall be reopened at the direction of the project Engineer. Ramp traffic shall be detoured using Portable Changeable Message Signs to communicate the detour route.

ADDED BELOW TABLE


RAMP DESCRIPTION	DETOUR ROUTE
NB SR-21/ I-77 TO SR 18 WB	I-77 NORTH/ GHENT RD / I-77 SOUTH
NB SR-21/ I-77 TO SR 18 EB	I-77 NORTH/ GHENT RD / I-77 SOUTH
EB SR 18 TO NB SR 21/I-77	SR-18 EAST / GHENT RD
EB SR 18 TO SB SR 21/I-77	SR-18 EAST / RIGHT ON CLEVELAND MASSILLON RD / 77S
SB SR21/ I-77 TO EB SR 18	I-77 SOUTH / RIDGEWOOD RD / I-77 NORTH
SB SR21/ I-77 TO WB SR 18	I-77 SOUTH / RIDGEWOOD RD / I-77 NORTH
WB SR 18 TO NB SR 21/ I-77	CRYSTAL LAKE RD / EMBASSY PARKWAY / CLEVELAND MASSILLON RD / GHENT RD
WB SR 18 TO SB SR 21/ I-77	CRYSTAL LAKE RD / EMBASSY PARKWAY / CLEVELAND MASSILLON RD / GHENT RD

USE PORTABLE CHANGEABLE MESSAGE SIGNS (PCMS) TO POST DETOUR ROUTES

SHEET NUM.												PART.	ITEM	ITEM	GRAND	UNIT	DESCRIPTION	SEE SHEET NO.
3	4	5	6	7	10	11	12	13	14			01/NHS/05	EXT	TOTAL				
							15					15	202	23500	15	SY	WEARING COURSE REMOVED	
					514							514	202	30000	514	SF	WALK REMOVED	
					50							50	202	32500	50	FT	CURB AND GUTTER REMOVED	
100	19											19	202	98100	19	EACH	REMOVAL MISC.: BARRIER REFLECTOR	4
												100	203	10000	100	CY	EXCAVATION (FOR PAVEMENT REPAIR)	
115												115	209	60200	115	STA	LINEAR GRADING	
					122							122	608	10000	122	SF	4" CONCRETE WALK	
					380							380	608	52000	380	SF	CURB RAMP	
	2											2	623	39501	2	EACH	MONUMENT ASSEMBLY ADJUSTED TO GRADE, AS PER PLAN	4
												LS	SPECIAL	69021000	LS		AS-BUILT CONSTRUCTION RECORD DRAWINGS	4
	205											205	831	00100	205	FT	LONGITUDINAL CHANNELIZING DEVICE	
	205											205	831	00500	205	FT	REMOVAL OF LONGITUDINAL CHANNELIZING DEVICE	
																	EROSION CONTROL	
3,097												3,097	659	10000	3,097	SY	SEEDING AND MULCHING	
0.42												0.42	659	20000	0.42	TON	COMMERCIAL FERTILIZER	
0.64												0.64	659	31000	0.64	ACRE	LIME	
16.7												16.7	659	35000	16.7	MGAL	WATER	
																	DRAINAGE	
	1											1	611	98630	1	EACH	CATCH BASIN ADJUSTED TO GRADE	
	7											7	611	99655	7	EACH	MANHOLE ADJUSTED TO GRADE, AS PER PLAN	4
	1											1	611	99660	1	EACH	MANHOLE RECONSTRUCTED TO GRADE	
	50											50	SPECIAL	61199820	50	LB	MISCELLANEOUS METAL	4
																	PAVEMENT	
1,750												1,750	251	01000	1,750	SY	PARTIAL DEPTH PAVEMENT REPAIR (441)	
3,600												3,600	252	01500	3,600	FT	FULL DEPTH PAVEMENT SAWING	
600												600	253	01000	600	SY	PAVEMENT REPAIR FULL DEPTH RIGID PAV'T REMOVAL & FLEXIBLE REPLACEMENT	
					57,984	29,887						87,871	254	01000	87,871	SY	PAVEMENT PLANING, ASPHALT CONCRETE (T = 1 1/2")	
100												100	304	20000	100	CY	AGGREGATE BASE (FOR PAVEMENT REPAIR)	
						5,219	2,703					7,922	407	20000	7,922	GAL	NON-TRACKING TACK COAT AT 0.09 GAL/SY	
						995	38					1,033	408	10001	1,033	GAL	PRIME COAT, AS PER PLAN AT 0.4 GAL/SY	3
						1,852	1,192					3,044	441	00100	3,044	CY	ANTI-SEGREGATION EQUIPMENT	
						2,416	1,246					3,662	442	10001	3,662	CY	ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (446), AS PER PLAN, PG70-22M	4
					12							12	609	12000	12	FT	COMBINATION CURB AND GUTTER, TYPE 2	
						139	3					142	617	10101	142	CY	COMPACTED AGGREGATE, AS PER PLAN	4
																	WATER WORK	
	1											1	638	10801	1	EACH	VALVE BOX ADJUSTED TO GRADE, AS PER PLAN	4
																	TRAFFIC CONTROL	
								506				506	621	00100	506	EACH	RPM (WHITE/RED)	
								109				109	621	00100	109	EACH	RPM (YELLOW/YELLOW)	
								497				497	621	54000	497	EACH	RAISED PAVEMENT MARKER REMOVED	
	51											51	626	00102	51	EACH	BARRIER REFLECTOR, TYPE 1, ONE WAY	
	24											24	626	00110	24	EACH	BARRIER REFLECTOR, TYPE 2, ONE WAY	
									3.04			3.04	646	10010	3.04	MILE	EDGE LINE, 6"	
									5.46			5.46	646	10110	5.46	MILE	LANE LINE, 6"	
									1.66			1.66	646	10200	1.66	MILE	CENTER LINE	
									5,825			5,825	646	10310	5,825	FT	CHANNELIZING LINE, 12"	
									548			548	646	10400	548	FT	STOP LINE	
										839		839	646	10520	839	FT	CROSSWALK LINE, 24"	
										64		64	646	20300	64	EACH	LANE ARROW	
										4		4	646	20370	4	EACH	TWO WAY LEFT TURN ARROW	
										1,675		1,675	646	20504	1,675	FT	DOTTED LINE, 6"	
																	TRAFFIC SIGNALS	
					18							18	632	26501	18	EACH	DETECTOR LOOP, AS PER PLAN	7
					2							2	809	69001	2	EACH	ADVANCE RADAR DETECTION, AS PER PLAN	7
					7							7	809	69101	7	EACH	STOP LINE RADAR DETECTION, AS PER PLAN	7

GENERAL SUMMARY

DESIGN AGENCY



DESIGNER: JMW
 REVIEWER: JMW
 PROJECT ID: 110644
 SHEET: P.8 TOTAL: 14