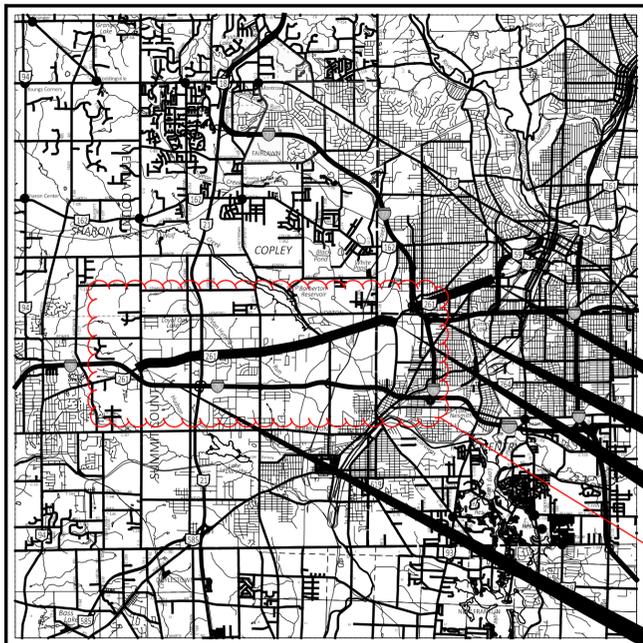


STATE OF OHIO DEPARTMENT OF TRANSPORTATION

SUM-SR-261-0.00/VAR

CITY OF AKRON AND NORTON SUMMIT COUNTY



LOCATION MAP

LATITUDE: 41°00'57" LONGITUDE: 81°29'30"



END PROJECT
SUM SR 261 SLM: 8.11
RESUME PROJECT
SUM SR 261 SLM: 6.25
SUSPEND PROJECT
SUM SR 261 SLM: 5.64
LIMITS ADJUSTED
BEGIN PROJECT
SUM SR 261 SLM: 0.00

PORTION TO BE IMPROVED	=====
INTERSTATE HIGHWAY	=====
FEDERAL ROUTES	=====
STATE ROUTES	=====
COUNTY & TOWNSHIP ROADS	=====
OTHER ROADS	=====

DESIGN DESIGNATION

CURRENT ADT (2025)	15432
DIRECTIONAL DISTRIBUTION	57
TRUCKS (24 HOUR B&C)	328
LEGAL SPEED	35 MPH
DESIGN FUNCTIONAL CLASSIFICATION:	URBAN MINOR ARTERIAL
NHS PROJECT	NO

DESIGN EXCEPTIONS

NONE REQUIRED

ADA DESIGN WAIVERS

NONE REQUIRED

UNDERGROUND UTILITIES
Contact Two Working Days
Before You Dig

OHIO811. 8-1-1. or 1-800-362-2764
(Non members must be called directly)

PLAN PREPARED BY:
ODOT DISTRICT 4 CAPITAL PROGRAMS
2088 SOUTH ARLINGTON ROAD
AKRON, OHIO 44306

INDEX OF SHEETS:

TITLE SHEET	P.1
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CURB RAMPS	P.14 - P.24
PAVEMENT MARKINGS	P.25 - P.32
STRUCTURES	P.33 - P.36

STANDARD CONSTRUCTION DRAWINGS				SUPPLEMENTAL SPECIFICATIONS	SPECIAL PROVISIONS
AS-1-15	1/20/23	MT-97.12	1/20/17	800-2023	7/18/25
		MT-99.20	4/19/19	821	4/20/12
BP-2.1	1/21/22	MT-101.90	7/17/20	832	7/18/25
BP-3.1	1/19/24	MT-105.10	1/17/20	921	7/19/24
BP-3.2	1/18/19	MT-110.10	1/19/23		
BP-4.1	7/19/13				
BP-5.1	1/17/25	TC-41.20	10/18/13		
BP-7.1	1/17/25	TC-52.10	10/18/13		
		TC-52.20	1/15/21		
MGS-4.3	1/18/13	TC-65.10	1/17/14		
		TC-65.11	1/17/25		
DS-1-92	7/15/22	TC-71.10	4/21/23		
		TC-72.20	1/17/25		
MT-95.31	7/19/19				
MT-95.32	4/19/19				
MT-97.10	4/19/19				

FEDERAL PROJECT NUMBER

E250879

RAILROAD INVOLVEMENT

NONE

PROJECT DESCRIPTION

RESURFACING OF SR 261 FROM SLM 0.00 - 5.29 IN THE CITY OF NORTON AND SLM 5.29 - 5.64 & 6.25 - 8.11 IN THE CITY OF AKRON. IN SUMMIT COUNTY. INCLUDES MINOR BRIDGE WORK TO 6 STRUCTURES.

SCOPE DESCRIPTION
ADJUSTED TO INCLUDE
SLM 2.64 - 3.73

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA:	0.97 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA:	0.25 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA:	N/A (NOI NOT REQUIRED)*
	* ROUTINE MAINTENANCE PROJECT

2023 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

Arthur G. Noiro Jr., P.E.
District 04 Deputy Director

Pamela Boratyn
Director, Department of Transportation

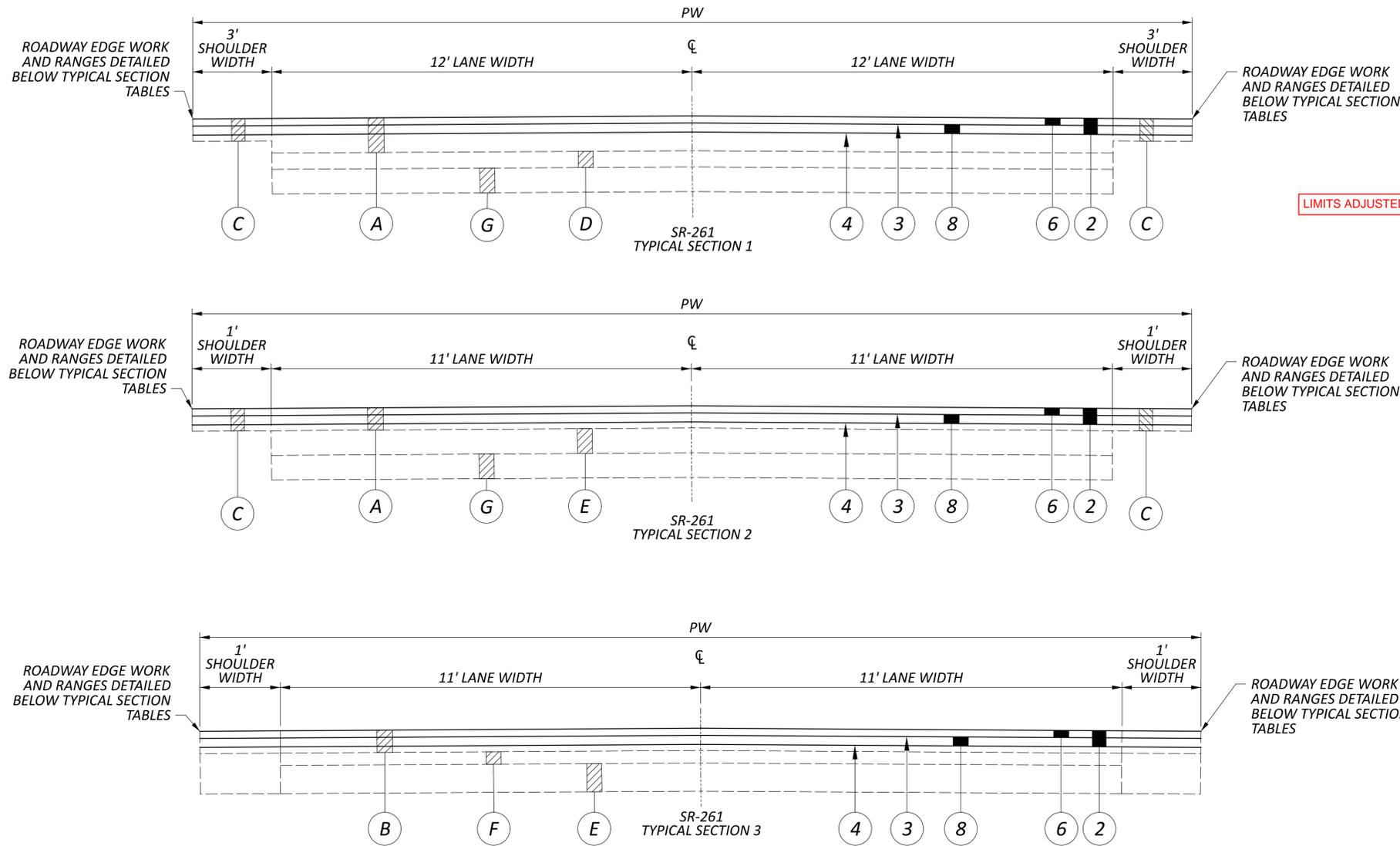
ENGINEER'S SEAL P.1 - P.13, P.25 - P.26	ENGINEER'S SEAL P.33 - P.36	ENGINEER'S SEAL P.14 - P.24, P.27 - P.32

TITLE SHEET

DESIGN AGENCY



DESIGNER	SBD
REVIEWER	BC
PROJECT ID	113037
SHEET	P.1
TOTAL	36

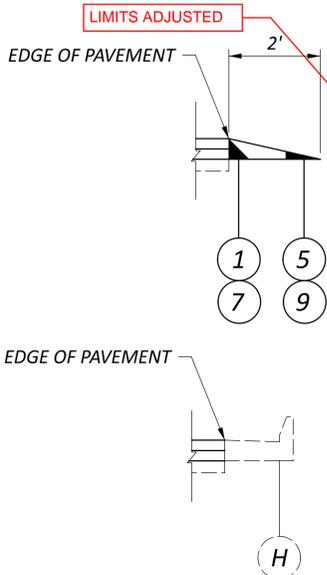


TYPICAL SECTION #1				
ROUTE	SLM		LENGTH (MILES)	PW (FEET)
	FROM	TO		
SR 261	0.00	0.06	0.06	29
SR 261	0.08	0.30	0.22	29

TYPICAL SECTION #2				
ROUTE	SLM		LENGTH (MILES)	PW (FEET)
	FROM	TO		
SR 261	0.30	2.20	1.90	28
SR 261	2.22	2.65	0.43	30
SR 261	2.65	2.69	0.04	35
SR 261	2.69	2.71	0.02	33
SR 261	2.71	2.73	0.02	30
SR 261	2.73	3.74	1.01	26
SR 261	3.74	4.09	0.35	25
SR 261	4.09	4.20	0.11	44
SR 261	4.22	4.56	0.34	28
SR 261	4.57	4.76	0.19	26
SR 261	4.77	4.94	0.17	26
SR 261	4.95	5.29	0.34	29
SR 261	6.25	6.43	0.18	70

TYPICAL SECTION #3				
ROUTE	SLM		LENGTH (MILES)	PW (FEET)
	FROM	TO		
SR 261	5.29	5.64	0.35	26
SR 261	6.43	7.22	0.79	39
SR 261	7.22	7.45	0.23	38
SR 261	7.45	7.67	0.22	37
SR 261	7.67	8.11	0.44	40

LIMITS ADJUSTED



NON-CURBED SECTIONS			
ROUTE	SLM		LENGTH (MILES)
	FROM	TO	
SR 261	0.00	5.29	5.29
SR 261	6.25	6.43	0.18

CURBED SECTIONS			
ROUTE	SLM		LENGTH (MILES)
	FROM	TO	
SR 261	6.43	8.11	1.68

LEGEND

- 1 209, PREPARING SUBGRADE FOR SHOULDER PAVING
- 2 254, PAVEMENT PLANNING, ASPHALT CONCRETE (T = 2")
- 3 407, NON-TRACKING TACK COAT @ 0.06 GAL/SY
- 4 407, NON-TRACKING TACK COAT @ 0.09 GAL/SY
- 5 408, PRIME COAT, AS PER PLAN @ 0.40 GAL/SY
- 6 424, FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, (448) (T = 1")
- 7 SAFETY EDGE, AS PER SCD BP-3.2
- 8 441, ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, (448) (T = 1")
- 9 617, COMPACTED AGGREGATE, AS PER PLAN (T = 1" AVG.)

- A EXISTING ASPHALT CONCRETE (T = 6")
- B EXISTING ASPHALT CONCRETE (T = 4")
- C EXISTING SHOULDER
- D EXISTING MACADAM BASE (T = 4")
- E EXISTING CONCRETE BASE (T = 6")
- F EXISTING RIGID BRICK BASE (T = 2")
- G EXISTING AGGREGATE LAYER (T = 6")
- H EXISTING CONCRETE CURB & GUTTER

UTILITIES

THE CONTRACTOR SHALL USE THE FOLLOWING PROCEDURE AT EACH LOCATION WHERE WORK IS PERFORMED, IN ACCORDANCE WITH SECTIONS 105.07 AND 107.16 IN THE CONSTRUCTION AND MATERIALS SPECIFICATIONS.

THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER, OHIO811 AND ALL NON REGISTERED UTILITY OWNERS AT LEAST TWO (2) WORKING DAYS PRIOR TO COMMENCING CONSTRUCTION OPERATIONS IN ALL AREAS.

THE LOCATION OF EXISTING UNDERGROUND UTILITIES ARE NOT SHOWN ON THE PLANS, BUT CAN BE OBTAINED FROM THE OWNERS OF THE UTILITIES. THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE TO UTILITIES.

PROFILE AND ALIGNMENT

PLACE THE PROPOSED PAVEMENT TO FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT. PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY AS SHOWN ON THE TYPICAL SECTIONS.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

PAVEMENT MARKING LANE WIDTHS

THE NORMAL LANE WIDTH FOR THE PAVEMENT MARKING ON THIS PROJECT SHALL BE AS FOLLOWS:

ROUTE	S.L.M. TO S.L.M.	LANE WIDTH
SR 261	0.00 TO 0.30	12'
SR 261	0.30 TO 5.29	11'
SR 261	6.25 TO 8.11	11'

(Note: Red circles and arrows labeled "LIMITS ADJUSTED" point to the 0.00-0.30 and 6.25-8.11 ranges.)

PAVEMENT MARKING DETAILS

THE PAVEMENT MARKING DETAIL SHEETS HAVE BEEN SUPPLIED AS REFERENCE DOCUMENTS FOR THIS PROJECT AND ARE AVAILABLE ON THE ODOT FTP SITE AT <https://ftp.dot.state.oh.us/pub/contracts/Attach/FOR THIS PROJECT>. FOR ANY LOCATIONS THAT PAVEMENT MARKING DETAILS HAVE NOT BEEN MADE AVAILABLE TO THE CONTRACTOR, IT WILL BE THE CONTRACTORS RESPONSIBILITY TO PUT BACK NEW PAVEMENT MARKINGS IN THE ORIGINAL LOCATIONS.

RESURFACING PAVEMENT ADJACENT TO APPROACH SLABS

AT ALL LOCATIONS WHERE THE RESURFACING ABUTS TO THE END OF AN APPROACH SLAB THE CONTRACTOR SHALL APPLY A JOINT SEALER AS SHOWN IN STANDARD CONSTRUCTION DRAWING AS-15-15, SHEET 2, DETAIL C. THE COST FOR THIS WORK SHALL BE INCIDENTAL TO THE ASPHALT PAY ITEMS.

INTERSECTIONS

INTERSECTIONS WILL BE RESURFACED 10 FT. BEYOND THE EDGE LINE, UNLESS OTHERWISE DIRECTED BY THE ENGINEER OR INDICATED IN THE PLAN. INTERSECTIONS SHALL BE PAVED AFTER COMPLETION OF THE SURFACE COURSE. A BUTT JOINT, AS PER STANDARD CONSTRUCTION DRAWING BP-3.1, SHALL BE USED TO PROVIDE A SMOOTH TRANSITION TO THE EXISTING PAVEMENT. USE THE SAME ASPHALT CONCRETE AS THE MAINLINE PAVEMENT UNLESS SHOWN OTHERWISE ON THE ASPHALT CONCRETE CALCULATIONS SHEET. ANY GRADING OR PRIME NECESSARY TO ACCOMPLISH THIS WORK SHALL BE INCLUDED IN THE COST OF THE PERTINENT BID ITEM.

DRIVEWAYS

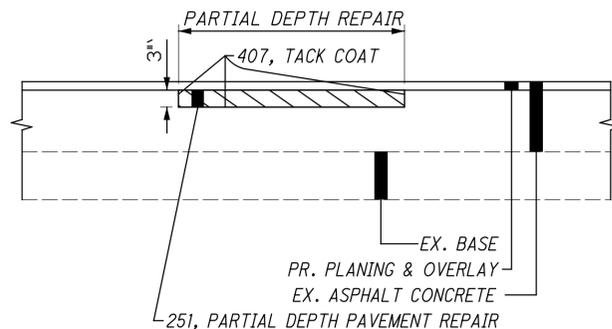
THE CONTRACTOR WILL NOT BE PERMITTED TO LEAVE A DIFFERENCE IN ELEVATION BETWEEN THE MAINLINE ASPHALT SURFACE COURSE AND THE EXISTING DRIVEWAYS. IF APPROVED BY THE ENGINEER, AN ASPHALT WEDGE WITH A MINIMUM WIDTH OF 2' MAY BE PLACED EITHER ON THE ROADWAY SHOULDER OR DRIVEWAY DEPENDENT UPON WHICH SIDE IS HIGH. A QUANTITY OF MAINLINE SURFACE COURSE ASPHALT HAS BEEN PROVIDED IN THE CALCULATIONS AND GENERAL SUMMARY TO PERFORM THIS ITEM OF WORK.

IN THE EVENT THAT THE ENGINEER DETERMINES ADDITIONAL WORK IS NECESSARY TO PROPERLY ADDRESS FIELD CONDITIONS, AN ITEM FOR WEARING COURSE REMOVED HAS BEEN PROVIDED. THE REMOVAL DEPTH IS DEPENDENT UPON THE ELEVATION DIFFERENCE AND ALLOW FOR 1-2 OF COMPACTED ASPHALT MATERIAL TO BE PLACED.

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (441)

A QUANTITY OF THIS ITEM SHALL BE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER. THE ITEM SHALL CONSIST OF REPAIRING EXISTING LOCATIONS EXHIBITING SURFACE DETERIORATION AND PLACING ITEM 441 ASPHALT CONCRETE, TYPE 2. IT IS NOT THE INTENT TO REPAIR EVERY DETERIORATED AREA WITHIN THE PROJECT. PAVEMENT REPAIRS WILL BE MARKED IN THE FIELD BY THE PROJECT ENGINEER ACCORDING TO CMS 251.02. MINIMUM WIDTH IS 2'. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THIS ITEM SHALL BE PERFORMED AFTER THE COMPLETION OF MAINLINE PAVEMENT PLANING AND PRIOR TO THE PLACEMENT OF ASPHALT ON THE MILLED SURFACE. PAYMENT SHALL BE BASED ON THE ACTUAL NUMBER OF SQUARE YARDS OF PAVEMENT REPAIR.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:
251, PARTIAL DEPTH PAVEMENT REPAIR (441)
SR-261, SLM 0.00 - 5.29: 1310 SY
SR-261, SLM 5.29 - 5.64: 110 SY
SR-261, SLM 6.25 - 8.11: 580 SY



**ITEM 611 – MANHOLE ADJUSTED TO GRADE, AS PER PLAN
ITEM 623 – MONUMENT ASSEMBLY ADJUSTED TO GRADE, AS PER PLAN**

IN ADDITION TO THE REQUIREMENTS OF CMS 611.10.D FOR MANHOLES, OR 623.05 FOR MONUMENT ASSEMBLY, THE CONTRACTOR WILL MAKE A CLEAN CIRCULAR CUT AROUND THE CASTING (48" DIAMETER FOR STORM AND SANITARY MANHOLE CASTINGS, 24"-28" FOR VALVE BOXES AND MONUMENT ASSEMBLIES, AND 2' IN DIAMETER LARGER THAN THE CASTING DIAMETER FOR ANY CASTINGS THAT ARE LARGER THAN STANDARD MANHOLES) AND REMOVE AND DISCARD THE EXISTING CASTING. INSTALL A NEW CASTING TO GRADE (ACCORDING TO TOLERANCES AS SHOWN ON STANDARD CONSTRUCTION DRAWING BP-3.1) AFTER THE PAVEMENT SURFACE COURSE HAS BEEN REPLACED.

CMS 499 CLASS QCMS CONCRETE (DYE THE CONCRETE SUCH THAT ITS COLOR CLOSELY MATCHES THE COLOR OF THE SURROUNDING PAVEMENT) WILL BE USED FOR BACKFILLING THE FULL PAVEMENT SECTION AND THE JOINT BETWEEN THE ASPHALT AND CONCRETE WILL BE SEALED WITH CMS 702.01 PG BINDER. EPOXY COATED REBAR SHALL BE PLACED IN THE CONCRETE AT 6" MAXIMUM ON CENTER AND A MINIMUM OF 3.5" CLEARANCE FROM THE TOP, BOTTOM AND SIDES. THE CONCRETE WILL BE VIBRATED SUFFICIENTLY TO ELIMINATE AIR POCKETS UNDER THE FRAME.

PAYMENT WILL INCLUDE REMOVAL OF THE EXISTING MATERIAL, INSTALLATION AND FURNISHING OF A NEW CASTING, AND ALL LABOR AND MATERIALS REQUIRED TO COMPLETE THIS ITEM OF WORK AS DESCRIBED.

611 - MANHOLE ADJUSTED TO GRADE, AS PER PLAN, 37 EACH
623 - MONUMENT ASSEMBLY ADJUSTED TO GRADE, AS PER PLAN, 27 EACH

CONNECTION BETWEEN EXISTING AND PROPOSED GUARDRAIL

WHEN IT IS NECESSARY TO SPLICE PROPOSED GUARDRAIL TO EXISTING GUARDRAIL, ONLY THE EXISTING GUARDRAIL SHALL BE CUT, DRILLED, OR PUNCHED. THE CONNECTION SHALL BE MADE USING A W-BEAM, BEAM SPLICE AS SHOWN IN AASHTO M 180-12, EXCEPT THE BEAM WASHERS ARE NOT TO BE USED. PAYMENT SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE RESPECTIVE GUARDRAIL ITEMS.

LIMITS ADJUSTED

ALL MATERIALS, LABOR, EQUIPMENT, TOOLS, AND INCIDENTALS NECESSARY TO COMPLETE THIS WORK WILL BE INCLUDED IN THE UNIT PRICE FOR THE PERTINENT BID ITEM. THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

SLM 0.00 - 5.29
209, LINEAR GRADING, 140 STA.
SLM 5.29 - 5.64, 6.25 - 6.43
209, LINEAR GRADING, 14 STA.

SURFACE SMOOTHNESS REQUIRMENTS FOR PAVEMENTS

REQUIREMENTS FOR PROPOSAL NOTE 420 - SURFACE SMOOTHNESS REQUIREMENTS FOR PAVEMENTS SHALL NOT APPLY TO THE PROJECT WITHIN THE FOLLOWING LIMITS:

SR-261 - SLM. 6.25 TO 8.11

REQUIREMENTS FOR PN 420 SHALL REMAIN UNCHANGED FOR ALL OTHER AREAS OF WORK WITHIN THE PROJECT LIMITS.

ITEM 638 – VALVE BOX ADJUSTED TO GRADE

THIS ITEM OF WORK INCLUDES ADJUSTMENT OF VALVE BOXES TO GRADE AS PER CMS 638.18.

PAYMENT WILL INCLUDE REMOVAL OF THE EXISTING MATERIAL, INSTALLATION AND FURNISHING OF A NEW CASTING, AND ALL LABOR AND MATERIALS REQUIRED TO COMPLETE THIS ITEM OF WORK AS DESCRIBED.

CITY OF NORTON:
638 - VALVE BOX ADJUSTED TO GRADE, AS PER PLAN, 1 EACH
CITY OF AKRON:
638 - VALVE BOX ADJUSTED TO GRADE, AS PER PLAN, 7 EACH

BIKE EXTENSION LINE DIMENSIONS

BIKE EXTENSION LINES PLACED WITHIN SLM 6.25 - 8.11 SHALL USE THE DIMENSIONS PROVIDED BY THE CITY OF AKRON, LOCATED ON SHEET P.25.

ITEM 408 - PRIME COAT, AS PER PLAN

APPLY "MC-70" AT A RATE OF 0.4 GALLONS PER SQUARE YARD, OR AS DETERMINED BY THE ENGINEER, TO THE COMPLETED COMPACTED AGGREGATE SHOULDER.



REVIEW OF CURB RAMPS

PRIOR TO THE START OF WORK, PERFORM AN INSPECTION WITH REPRESENTATIVES OF THE DEPARTMENT AND CONTRACTOR OF ALL CURB RAMPS INCLUDED IN THE PROJECT LIMITS. PRIOR TO THE INSPECTION, THE CONTRACTOR SHALL MARK THE WORK LIMITS OF ALL CURB RAMPS AND ASSOCIATED WORK TO IDENTIFY ANY CONDITIONS THE CONTRACTOR CANNOT BRING INTO ADA COMPLIANCE PER BP-7.1. THE DEPARTMENT SHALL HAVE TEN (10) BUSINESS DAYS TO RESOLVE ANY SUCH ANTICIPATED NON-COMPLIANCE WITH AN ADA WAIVER, QUANTITY CHANGE OR DESIGN REVISION PRIOR TO THE START OF WORK.

IMMEDIATELY FOLLOWING INSTALLATION OF FORMWORK FOR CURB RAMP CONCRETE PLACEMENT AND PRIOR TO PLACEMENT OF CONCRETE, THE CONTRACTOR SHALL VERIFY ADA COMPLIANCE BASED UPON MEASUREMENT OF THE FORMS. THE ENGINEER SHALL BE IMMEDIATELY NOTIFIED OF ANY NON-COMPLIANT ADA CONDITIONS MEASURED BY THE CONTRACTOR WITHIN THE WORK LIMITS. THE ENGINEER SHALL HAVE THREE (3) BUSINESS DAYS TO RESOLVE ANY SUCH NON-COMPLIANCE WITH A CORRECTIVE FORM LAYOUT, ADA WAIVER, QUANTITY CHANGE OR DESIGN CHANGE PRIOR TO THE PLACEMENT OF CONCRETE.

THE CONTRACTOR IS RESPONSIBLE TO CORRECT ANY ADA NON-COMPLIANCE FOR ALL ISSUES NOT REPORTED TO THE ENGINEER IN THE INITIAL INSPECTION AND/OR THE FORM INSTALLATION MEASUREMENT. MODIFICATION OF FORMS INTO A COMPLIANT CONFIGURATION AND/OR REMOVAL AND RECONSTRUCTION OF FINISHED WORK SHALL BE AT THE EXPENSE OF THE CONTRACTOR.

PAYMENT FOR ALL OPERATIONS DESCRIBED ABOVE IS INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 608 CURB RAMP ITEMS.

ITEM 608 - CURB RAMP, AS PER PLAN

UNLESS OTHERWISE DIRECTED BY THE ENGINEER, INSTALLATION OF THE CURB RAMPS / DETECTABLE WARNINGS WILL BE PERFORMED PRIOR TO MAINLINE RESURFACING.

IN ADDITION TO THE CMS REQUIREMENTS OF ITEM 608 CURB RAMP, THIS ITEM SHALL INCLUDE THE RESTORATION OF THE ADJACENT AREAS DISTURBED FOR THE INSTALLATION OF CURB RAMPS AND IMMEDIATELY ADJACENT CONCRETE WALK. RESTORATION SHALL INCLUDE PLACEMENT OF ITEM 659 TOPSOIL, ITEM 659 COMMERCIAL FERTILIZER, ITEM 659 SEEDING AND MULCHING, AND ITEM 659 WATER, ALL PER CMS.

CURB RAMP DIMENSIONS AND PLACEMENT DETAILS PROVIDED BY THE CITY OF AKRON CAN BE FOUND ON SHEETS P.15 - P.24.

PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO PERFORM THE WORK DESCRIBED ABOVE SHALL BE INCLUDED IN THE UNIT PID PRICE FOR ITEM 608 - CURB RAMP, AS PER PLAN.

BARRIER REFLECTORS

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS DIRECTED BY THE ENGINEER FOR INSTALLING/REPLACING BARRIER REFLECTORS ON ALL EXISTING BARRIER RUNS WITHIN THE PROJECT LIMITS.

- 202, REMOVAL MISC.: BARRIER REFLECTOR, 28 EACH
- 626, BARRIER REFLECTOR, TYPE 1, 4 EACH
- 626, BARRIER REFLECTOR, TYPE 2, 14 EACH

ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN

THIS ITEM OF WORK SHALL BE PERFORMED IN CONFORMANCE WITH ITEM 254 IN THE CMS EXCEPT THE DEPTH SHALL VARY FROM 2" TO THE TOP OF THE BRICK WHICHEVER IS FIRST. THIS WORK SHALL BE PERFORMED SO THAT THE BRICK BASE IS NOT DISTURBED. ALL EQUIPMENT, LABOR, TOOLS, AND OTHER INCIDENTALS REQUIRED TO PERFORM THIS WORK SHALL BE INCLUDED IN THE UNIT BID PRICE FOR ITEM 254 PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN.

STRUCTURE IDENTIFICATION SIGNS

STRUCTURE IDENTIFICATION SIGNS (I-H25b) WILL BE PLACED ON EACH APPROACH OFF THE RIGHT SHOULDER, FACING TRAFFIC, AND BEHIND THE GUARDRAIL IF APPLICABLE. A QUANTITY OF ONE SIGN PER APPROACH WILL BE INSTALLED. THE SIGNS WILL HAVE A NON-REFLECTIVE WHITE SHEETING BACKGROUND.

THE SIGNS WILL BE MOUNTED ON NEW NO. 2 POSTS AND WILL BE INSTALLED AS PER STANDARD CONSTRUCTION DRAWING TC-41.20, MOST CURRENT REVISION. EACH POST WILL BE 7.5' IN LENGTH.

INSTALL SIGNS FOR THE FOLLOWING STRUCTURES:

- SUM-261-0.070
- SUM-261-2.205
- SUM-261-4.185
- SUM-261-4.567
- SUM-261-4.756
- SUM-261-4.922

THE FOLLOWING QUANTITIES HAVE BEEN INCLUDED FOR EACH APPROACH:

- ITEM 630 - SIGN, FLAT SHEET, 730.20, 1 SQ FT
- ITEM 630 - GROUND MOUNTED SUPPORT, NO. 2 POST, 7.5 FT
- ITEM 630 - REMOVAL OF GROUND MOUNTED SIGN AND DISPOSAL, 1 EACH
- ITEM 630 - REMOVAL OF GROUND MOUNTED POST SUPPORT AND DISPOSAL, 1 EACH

PAVED MAILBOX APPROACHES

ALL EXISTING MAIL BOX APPROACHES WILL BE PAVED WITH ASPHALT CONCRETE. THE BUILDUP OF THE ASPHALT PAVEMENT SHALL MATCH THE MAINLINE PAVING. THE LIMITS OF THE PAVING SHALL MATCH THE EXISTING MAILBOX APPROACH LIMITS. PAYMENT FOR THE WORK SHALL BE INCLUDED IN THE MAINLINE PAVING QUANTITIES, SEPARATE QUANTITIES FOR THE MAILBOX APPROACHES ARE NOT PROVIDED.

CATCH BASIN ADJUSTED TO GRADE

AN ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR ADJUSTING CATCH BASINS TO GRADE.

EXISTING CASTINGS MAY PROVE TO BE UNSUITABLE FOR REUSE, AS DETERMINED BY THE ENGINEER. IT IS THE CONTRACTOR'S RESPONSIBILITY TO PROVIDE THE CASTINGS OF REQUIRED TYPE, SIZE AND STRENGTH. ENSURE ALL MATERIAL MEETS CMS ITEM 611 AND HAS PRIOR APPROVAL OF THE ENGINEER.

- ITEM 611 - CATCH BASIN ADJUSTED TO GRADE, 46 EACH
- ITEM SPECIAL - MISCELLANEOUS METAL, 2000 LB

ITEM 304 - AGGREGATE BASE (FOR PAVEMENT REPAIR)

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN PROVIDED AND SHALL BE USED AS DIRECTED BY THE ENGINEER TO BACKFILL AREAS WHICH WERE EXCAVATED UNDER ITEM 203 (FOR PAVEMENT REPAIR). THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

- 304, AGGREGATE BASE (FOR PAVEMENT REPAIR), 28 CU YD
- SR-261, SLM 0.00 - 5.29: 18 CY
- SR-261, SLM 5.29 - 5.64: 2 CY
- SR-261, SLM 6.25 - 8.11: 8 CY

ITEM 203 - EXCAVATION (FOR PAVEMENT REPAIR)

THIS ITEM OF WORK SHALL CONSIST OF REMOVING AND DISPOSING OF ALL UNSUITABLE MATERIAL BY EXCAVATING THE EXISTING SUBGRADE AND SUBBASE TO AN AVERAGE DEPTH OF 6 INCHES OR AS DIRECTED BY THE ENGINEER. EXACT LIMITS OF REMOVAL SHALL BE DETERMINED BY THE ENGINEER. ALL EQUIPMENT, LABOR, TOOLS, AND INCIDENTALS NECESSARY TO COMPLETE THIS ITEM SHALL BE INCLUDED IN THE UNIT PRICE BID FOR EXCAVATION (FOR PAVEMENT REPAIR). THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

- 203, EXCAVATION (FOR PAVEMENT REPAIR), 28 CU YD
- SR-261, SLM 0.00 - 5.29: 18 CY
- SR-261, SLM 5.29 - 5.64: 2 CY
- SR-261, SLM 6.25 - 8.11: 8 CY

ITEM 253 - PAVEMENT REPAIR

A QUANTITY OF THIS ITEM SHALL BE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER. THIS ITEM SHALL CONSIST OF CUTTING AND REMOVING DETERIORATED PAVEMENT FULL DEPTH AND PLACING 10" (SLM 0.00 - 0.30), OR 12" (SLM 0.30 - 5.64, 6.25-8.11) 301 ASPHALT CONCRETE BASE, PG64-22. THE MAXIMUM COMPACTED DEPTH OF ANY ONE LAYER SHALL BE 6 INCHES. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THIS ITEM SHALL BE PERFORMED AFTER THE COMPLETION OF MAINLINE PAVEMENT PLANING AND PRIOR TO THE PLACEMENT OF ASPHALT ON THE MILLED SURFACE.

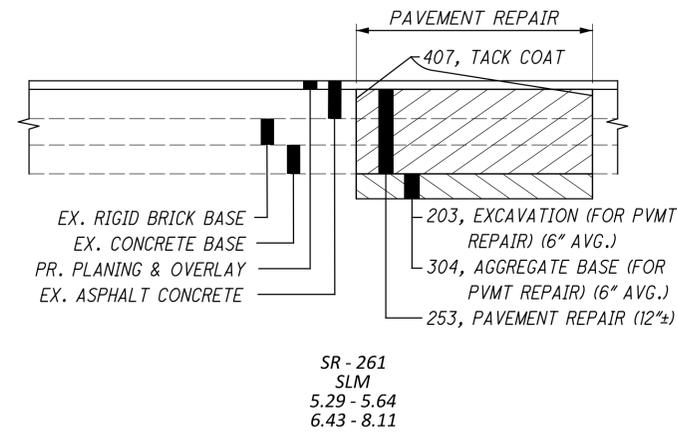
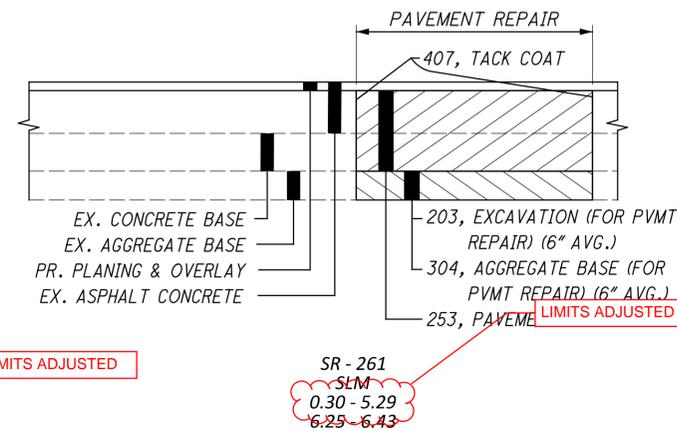
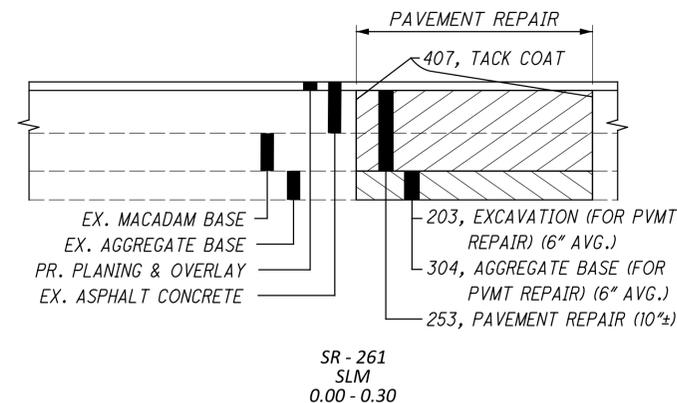
IT IS NOT THE INTENT TO REPAIR EVERY DETERIORATED AREA WITHIN THE PROJECT. THE ENGINEER SHALL DETERMINE WHICH AREAS ARE TO BE REPAIRED. PAYMENT SHALL BE BASED ON THE ACTUAL NUMBER OF SQUARE YARDS OF PAVEMENT REMOVED AND REPLACED TO THE LIMITS DESIGNATED BY THE ENGINEER.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

- 253, PAVEMENT REPAIR, 500 SQ YD
- SR-261, SLM 0.00 - 5.29: 330 SY
- SR-261, SLM 5.29 - 5.64: 25 SY
- SR-261, SLM 6.25 - 8.11: 145 SY
- 252, SAW CUTTING, 2250 FT
- SR-261, SLM 0.30 - 5.29: 1440 FT
- SR-261, SLM 5.29 - 5.64: 130 FT
- SR-261, SLM 6.25 - 8.11: 680 FT

SPECIAL - BICYCLE LANE BIKE SYMBOL, 647 PAINT
SPECIAL - BICYCLE LANE ARROW MARKING, 647 PAINT

BICYCLE LANE BIKE SYMBOLS AND ARROW MARKINGS SHALL BE PLACED AS PER MUTCD-11, FIGURE 9E-1A USING TYPE A90 PAINT. MARKING COLOR SHALL BE WHITE.



GENERAL NOTES

DESIGN AGENCY	
DESIGNER	SBD
REVIEWER	BC 02/02/26
PROJECT ID	113037
SHEET	P.4
TOTAL	36

MAINTENANCE OF TRAFFIC

THIS ITEM SHALL CONSIST OF MAINTENANCE OF TRAFFIC ON EXISTING ROADWAYS AND RAMPS IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, CURRENT EDITION, LATEST REVISION, THE SPECIFICATIONS AND THE FOLLOWING:

1. A MINIMUM OF ONE TEN FOOT BIDIRECTIONAL LANE SHALL BE MAINTAINED ON THE EXISTING PAVEMENT OR COMPLETED PAVEMENT DURING CONSTRUCTION OF THE WORK.
2. THE CONTRACTOR SHALL INFORM THE DISTRICT OFFICE (330) 786-2208, EIGHTEEN (18) DAYS PRIOR TO THE BEGINNING OF WORK.
3. LANE RESTRICTIONS OR LANE REDUCTIONS SHALL NOT BE PERMITTED AFTER NORMAL WORKING HOURS. NORMAL WORKING HOURS SHALL BE THOSE HOURS DURING WHICH THE CONTRACTOR HAS A FULL COMPLEMENT OF EMPLOYEES AND EQUIPMENT ACTIVELY REMOVING AND/OR PLACING PAVEMENT MATERIALS.
4. ALL FULL DEPTH PAVEMENT REMOVAL AND REPLACEMENT OPERATIONS SHALL BE COMPLETED THE SAME DAY THE EXCAVATION IS MADE. IF THE CONTRACTOR CANNOT COMPLETE THE WORK, THE EXCAVATION SHALL BE BACKFILLED OR PROTECTED AS PER STANDARD CONSTRUCTION DRAWING MT-101.90.
5. TRUCK MOUNTED ATTENUATORS [TMA'S] SHALL BE USED AS SHOWN IN THE STANDARD CONSTRUCTION DRAWINGS.
6. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE PERMITTED TO HAVE SUCCESSIVE WORK ZONES UNLESS THE DISTANCE BETWEEN THE DRUMS, BARRICADES OR CONES EXCEEDS TWO (2) MILES RURAL OR ONE [1] MILE URBAN.
7. FOR ROUTES NOT ON THE PERMITTED LANE CLOSURE CHART, ONLY DURING OFF-PEAK PERIODS (ie ANY PERIOD OTHER THAN 6-8AM AND 3-6PM) SHALL THE CONTRACTOR INSTALL AND SUBSEQUENTLY RESET ALL TRAFFIC CONTROL NECESSARY FOR THE WORK ZONE FOR EACH CONSTRUCTION PHASE.
8. IN ADDITION TO THE REQUIREMENTS OF 614.11 WORK ZONE PAVEMENT MARKINGS, AT THE END OF EACH DAY OF WORK, THE CONTRACTOR SHALL REPLACE (WITH WORK ZONE MARKINGS) ALL LANE, CENTER, STOP OR CHANNELIZING LINES THAT WERE REMOVED OR COVERED DURING THE PAVEMENT REMOVAL OR PLACEMENT OPERATIONS. QUANTITIES FOR SUCH PLACEMENT ARE CARRIED AS PART OF THE ITEMS LISTED UNDER 614 WORK ZONE PAVEMENT MARKINGS.
9. A QUANTITY OF 20 CU. YDS. OF ITEM 614 ASPHALT CONCRETE FOR MAINTAINING TRAFFIC SHALL BE PROVIDED FOR USE IN MAINTAINING PAVEMENT, SHOULDERS AND OTHER LOCATIONS AS DIRECTED BY THE ENGINEER.
10. PRIOR TO OPENING TO TRAFFIC EACH LANE SHALL BE IN A SAFE, PASSABLE CONDITION. ALL TRANSVERSE JOINTS SHALL EXTEND ACROSS THE FULL LANE AND SHOULDER WIDTH AND EACH LANE SHALL BE FREE FROM UNEVEN LONGITUDINAL JOINTS. THE CONTRACTOR SHALL PROVIDE ASPHALT WEDGES FOR TRANSVERSE JOINTS WHEREVER THERE ARE PAVEMENT ELEVATION DIFFERENCES.
11. THE CONTRACTOR SHALL INSTALL, MAINTAIN AND SUBSEQUENTLY REMOVE WORK ZONE MARKING SIGNS AND THEIR SUPPORTS WITHIN THE WORK LIMITS. THESE SIGNS INCLUDE NO EDGE LINES, DO NOT PASS AND PASS WITH CARE". ALL OTHER SIGNS WILL BE INCIDENTAL TO THE LUMP SUM PAY ITEM 614 MAINTAINING TRAFFIC UNLESS SEPARATELY ITEMIZED IN THE PLANS. A QUANTITY OF ITEM 614 WORK ZONE MARKING SIGNS HAS BEEN INCLUDED IN THE PLANS AS PER CMS 614.04.

12. THE CONTRACTOR SHALL SET A WORK ZONE AT THE REQUEST OF THE ENGINEER TO ALLOW THE LAYOUT OF THE PARTIAL/FULL DEPTH PAVEMENT REPAIR AREAS. THIS WORK IS INCIDENTAL TO ITEM 614 MAINTAINING TRAFFIC.

THE FOLLOWING QUANTITIES SHALL BE USED FOR THE MAINTENANCE OF TRAFFIC ON THIS PROJECT:

- ALL PHASES
614, WORK ZONE MARKING SIGN, 26 EACH
- PHASE I (MILLED SURFACE)
614, WORK ZONE CENTER LINE, CLASS I, 8.41 MILE
614, WORK ZONE LANE LINE, CLASS I, 0.13 MILE
614, WORK ZONE STOP LINE, CLASS I, 512 FT
614, WORK ZONE CHANNELIZING LINE, CLASS I, 3041 FT
- PHASE II (INTERMEDIATE COURSE)
614, WORK ZONE CENTERLINE, CLASS I, 8.41 MILE
614, WORK ZONE LANE LINE, CLASS I, 0.13 MILE
614, WORK ZONE STOP LINE, CLASS I, 512 FT
614, WORK ZONE CHANNELIZING LINE, CLASS I, 642 PAINT 3041 FT
- PHASE III (SURFACE COURSE)
614, WORK ZONE CENTERLINE, CLASS III, 642 PAINT, 8.41 MILE
614, WORK ZONE LANE LINE, CLASS III, 642 PAINT, 0.13 MILE
614, WORK ZONE STOP LINE, CLASS III, 642 PAINT, 512 FT
614, WORK ZONE CHANNELIZING LINE, CLASS III, 642 PAINT, 3041 FT
- TO BE USED AS DIRECTED BY THE ENGINEER
614, WORK ZONE EDGE LINE, CLASS III, 14.45 MILE

QUANTITIES ADJUSTED TO ACCOMMODATE NEW LIMITS

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

TRAFFIC CONTROL INSPECTOR

THE CONTRACTOR SHALL DESIGNATE AN INDIVIDUAL OTHER THAN THE SUPERINTENDENT AND SUBJECT TO THE APPROVAL OF THE ENGINEER, TO CONTINUOUSLY INSPECT ALL TRAFFIC CONTROL DEVICES WHENEVER CONSTRUCTION WORK IS BEING PERFORMED WITHIN THE WORK LIMITS OF THE PROJECT. THE DESIGNATED INDIVIDUAL SHALL ALSO INSPECT ALL TRAFFIC DEVICES AT THE BEGINNING AND AT THE END OF EACH WORK DAY. THE DESIGNATED INDIVIDUAL OR A QUALIFIED REPRESENTATIVE SHALL ALSO BE AVAILABLE ON AN AROUND THE CLOCK BASIS TO REPAIR AND/OR REPLACE DAMAGED OR MISSING TRAFFIC CONTROL DEVICES. THESE INDIVIDUALS SHALL BE EQUIPPED WITH CELLULAR PHONES AND THEIR NAMES AND PHONE NUMBERS SHALL BE GIVEN TO THE PROJECT ENGINEER AT THE PRE-CONSTRUCTION MEETING. THE DESIGNATED INDIVIDUAL MAY HAVE OTHER CONSTRUCTION RELATED DUTIES AS LONG AS IMMEDIATE ATTENTION IS GIVEN TO TRAFFIC CONTROL. PAYMENT FOR THE SERVICES OF THE TRAFFIC CONTROL INSPECTOR SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614 MAINTAINING TRAFFIC.

ADVANCED NOTICE TO PAVE

THE CONTRACTOR SHALL SUBMIT FOR APPROVAL TO THE DISTRICT CONSTRUCTION ENGINEER A DETAILED SCHEDULE 15 DAYS PRIOR TO THE PLACEMENT OF THE OVERLAY COURSES, ON HOW THEY PROPOSE TO PROSECUTE THE PAVING OPERATIONS. THE DETAILS SHALL SHOW THE ORDER OF PERFORMANCE OF EACH STAGE (START TO FINISH) OF THE WORK INCLUDING THE MAINTENANCE OF TRAFFIC THAT WILL BE USED.

TIME LIMITATION, CURB RAMP

THE MAXIMUM ALLOWABLE TIME FOR THE CONTRACTOR TO HAVE AN INDIVIDUAL CURB RAMP AND ASSOCIATED SIDEWALK LEADING INTO THE CURB RAMP OUT OF SERVICE FOR THE REMOVAL AND REPLACEMENT SHALL BE 14 CONSECUTIVE CALENDAR DAYS (THE TIME PERIOD INCLUDES ALL WORK LISTED IN THE BASIS OF PAYMENT PER CMS 608.09, INCLUDING ALL REMOVAL, REPLACEMENT, AND BACKFILL ASSOCIATED WITH THE CURB RAMPS, AND CURING TIME PERIOD).

AT THE CONCLUSION OF CONSTRUCTING OF THE CURB RAMP AND PRIOR TO OPENING TO PEDESTRIAN TRAFFIC THE CONTRACTOR SHALL ENSURE THAT THE REQUIREMENTS OF STANDARD CONSTRUCTION DRAWING BP-7.1 ARE MET. THE CONTRACTOR SHALL USE ASPHALT AS A WEDGE, OR SUBMIT ANOTHER METHOD APPROVED BY THE ENGINEER, TO ENSURE THE TRANSITION FROM THE CURB RAMP TO THE ROADWAY ARE PER STANDARD CONSTRUCTION DRAWING BP-7.1. ALL COSTS TO PERFORM THIS WORK SHALL BE INCIDENTAL TO THE ASSOCIATED PAY ITEMS FOR THE INSTALLATION OF THE CURB RAMP.

SHOULD THE CONTRACTOR FAIL TO MEET THIS REQUIREMENT, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF \$1000 PER DAY PER AFFECTED RAMP THAT THE AFFECTED CURB RAMP REMAINS OUT OF SERVICE BEYOND 14 CONSECUTIVE CALENDAR DAYS.

TIME LIMITATION, TRAFFIC ON A MILLED SURFACE

THE MAXIMUM ALLOWABLE TIME FOR TRAFFIC TO BE PLACED ON A MILLED SURFACE SHALL BE 7 CONSECUTIVE CALENDAR DAYS. SHOULD THE CONTRACTOR FAIL TO MEET THIS REQUIREMENT, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF \$2,000 PER DAY THAT THE TRAFFIC IS PLACED ON A MILLED SURFACE BEYOND THE SPECIFIED LIMIT.

LIMITS ADJUSTED

ITEM 614, MAINTAINING TRAFFIC (LANES OPEN DURING HOLIDAYS OR SPECIAL EVENTS)

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR SPECIAL EVENTS:

- | | |
|---------------------------|------------------------------------|
| NEW YEAR'S (OBSERVED) | GENERAL/REGULAR ELECTION DAY (NOV) |
| THANKSGIVING | AKRON MARATHON |
| MEMORIAL DAY | CHRISTMAS (OBSERVED) |
| FOURTH OF JULY (OBSERVED) | BRIDGESTONE INVITATIONAL LABOR DAY |

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR SPECIAL EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF HOLIDAY OR SPECIAL EVENT	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00 N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00 N FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00 N MONDAY THROUGH 6:00 AM WEDNESDAY
TUESDAY (GEN./ REG. ELECTION)	5:00 AM TUESDAY THROUGH 12:00 AM WEDNESDAY
WEDNESDAY	12:00 N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00 N WEDNESDAY THROUGH 6:00 AM FRIDAY
THURSDAY (THANKSGIVING)	6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00 N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00 N FRIDAY THROUGH 6:00 AM MONDAY

DURING THE SAME PERIODS, MAINTAIN PEDESTRIAN ACCESS IF PEDESTRIAN ACCESS WAS PRESENT PRIOR TO CONSTRUCTION.

[NEWLY CONSTRUCTED LANE ADDITIONS, ONCE COMPLETED AND INITIALLY OPENED TO TRAFFIC, SHALL BE OPEN TO TRAFFIC DURING ALL SUBSEQUENT DESIGNATED HOLIDAYS AND SPECIAL EVENTS, AND RELATED PERIODS OF TIME, SPECIFIED ABOVE.]

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE PER THE LANE VALUE CONTRACT (PN 127).

LANE VALUE CONTRACT			
DESCRIPTION OF CRITICAL LANE/ RAMP TO BE MAINTAINED	RESTRICTED TIME PERIOD	TIME UNIT	DISINCENTIVE \$ PER TIME UNIT
SR-261 SLM 0.00 - 5.64	AS PER MAINTAINING TRAFFIC (LANES OPEN DURING HOLIDAYS OR SPECIAL EVENTS) NOTE ABOVE AND THE MAINTENANCE OF TRAFFIC RESTRICTIONS NOTE	PER LANE/PER MINUTE	\$40
SR-261 SLM 6.25 - 8.11	AS PER MAINTAINING TRAFFIC (LANES OPEN DURING HOLIDAYS OR SPECIAL EVENTS) NOTE ABOVE AND THE MAINTENANCE OF TRAFFIC RESTRICTIONS NOTE	PER LANE/PER MINUTE	\$50



ITEM 632 - DETECTOR LOOP, AS PER PLAN

THE CONTRACTOR SHALL CONTACT THE DISTRICT OFFICE (330-786-3146) AND SCOTT ELEKES (330-217-9032) THREE WORKING DAYS PRIOR TO ANY PLANING OR TRENCHING AT THE INTERSECTIONS LISTED BELOW. THE LOOP DETECTOR WIRE WILL BE CUT INTO THE PAVEMENT AFTER THE PROPOSED SURFACE COURSE HAS BEEN PLACED. ALL STOP LINE INDUCTANCE DETECTOR LOOPS SHALL BE THE POWERHEAD CONFIGURATION SHOWN ON TC-82.10. THE WIDTH SHALL BE AS SPECIFIED ON TC-82.10 AND THE LENGTH SHALL BE AS SPECIFIED BELOW. THE LOCATION OF THESE LOOPS SHALL BE SUCH THAT THE POWERHEAD IS LOCATED AT THE STOP LINE, NOT PAST IT. ALL DILEMMA ZONE INDUCTANCE DETECTOR LOOPS CALLED FOR IN THE PLANS SHALL BE THE ANGULAR DESIGN DETECTION (ADD) LOOP AS SHOWN ON TC-82.10. DIMENSIONS SHALL BE AS SPECIFIED ON TC-82.10 AND THE LOOP SHALL BE PLACED AT THE SAME LOCATION AS THE EXISTING LOOPS.

THE QUANTITIES LISTED BELOW HAVE BEEN CARRIED TO THE GENERAL SUMMARY. THE NEW LOOP DETECTOR WIRES SHALL BE RUN INTO THE EXISTING CONTROL BOX OR THE EXISTING PULLBOX. INCLUDED IN THIS ITEM IS THE POURED EPOXY TYPE CABLE SPLICE KIT (CONFORMING TO 725.15E) THAT MUST BE USED IN MAKING THESE CONNECTIONS. ALL NECESSARY MATERIAL, LABOR, SPLICE KITS AND EQUIPMENT SHALL BE INCIDENTAL TO PAYMENT OF THESE ITEMS.

INTERSECTION	632- DETECTOR LOOP, AS PER PLAN	POWERHEAD, 30' LENGTH	POWERHEAD, 20' LENGTH
SUM SR 261 & CLE/MASS RD.	2	2 (SR 261, 1 EAST, 1 WEST)	N/A
SUM SR 261 & EAST AVE.	6	2 (SR 261, 1 EAST/1 WEST)	4 (EAST AVE, 2 NORTH/2 SOUTH)
SUM SR 261 & FREDERICK BLVD.	4	2 (SR 261, 1 EAST/1 WEST)	2 (FREDERICK, 1 NORTH/1 SOUTH)
SUM SR 261 & 5 HAWKINS AVE.	4	2 (SR 261, 1 EAST/1 WEST)	2 (HAWKINS, 1 NORTH/1 SOUTH)
SUM SR 261 & MOON ST.	2	N/A	2 (MOON ST, 1 NORTH/1 SOUTH)
SUM SR 261 & RAYMOND ST.	2	N/A	2 (RAYMOND, 1 NORTH/1 SOUTH)

CARDINAL DIRECTION IS IN RESPECT TO DIRECTION TRAVELLED.
 EXAMPLE: 1 POWERHEAD, WEST = LOCATED ON WESTBOUND DIRECTION.

QUANTITY ADJUSTED TO ACCOMMODATE NEW LIMITS



EDGE LINE											GENERAL SPEC:	640
											MATERIAL TYPE:	646
CTY	ROUTE	TRUE LOG	FROM	TRUE LOG	TO	WHITE EDGE LINE, 4"			YELLOW EDGE LINE, 4"			COMMENTS
						TOTAL	HIGHWAY	RAMP	TOTAL	HIGHWAY	RAMP	
SUM	SR 261	0.00	MEDINA LINE RD	5.29	COLLER RD	10.58	10.58					
TOTAL						10.58	10.58	0				

QUANTITIES ADJUSTED TO ACCOMMODATE NEW LIMITS

LANE LINE										
CTY	ROUTE	TRUE LOG	FROM	TRUE LOG	TO	TOTAL MILES	4" LANE LINE		COMMENTS	
							DASHED	SOLID		
TOTAL										

CENTER LINE										
CTY	ROUTE	TRUE LOG	FROM	TRUE LOG	TO	TOTAL MILES	EQUIVALENT SOLID LINE		COMMENTS	
SUM	SR 261	0.00	MEDINA LINE RD	5.29	COLLER RD	5.29	8.66			
TOTAL						5.29	8.66			

QUANTITIES ADJUSTED TO ACCOMMODATE NEW LIMITS

AUXILIARY																					
CTY	ROUTE LOCATION	TRUE LOG	CHANNEL LINE, 8" FT	CHANNEL LINE, 12" FT	STOP LINE FT	CROSS WALK LINES FT	TRANSVERSE DIAGONAL LINES		ISLAND MARKING SF	SYMBOL MARKINGS			LANE ARROWS				REDUCT. ARROW EACH	WORD ON PVMT ONLY		DOTTED LINES, 6" FT	COMMENTS
							WHITE FT	YELLOW FT		Rxx	SCHOOL		TURN LEFT EACH	TURN RIGHT EACH	THRU EACH	COMB. EACH		72" EACH	96" EACH		
											72"	96"									
SUM	MEDINA LINE RD	0.00			12																
SUM	CLEVELAND MASSILLON RD	2.65	175		48	100															4
SUM	SUMMIT RD	4.17	339		48		160						3	2		4					
TOTAL			514		108	100		160					7	2		4					

QUANTITIES ADJUSTED TO ACCOMMODATE NEW LIMITS

DESIGN AGENCY



DESIGNER
SBD

REVIEWER
BC 02/02/26

PROJECT ID
113037

SHEET TOTAL
P.25 | 36