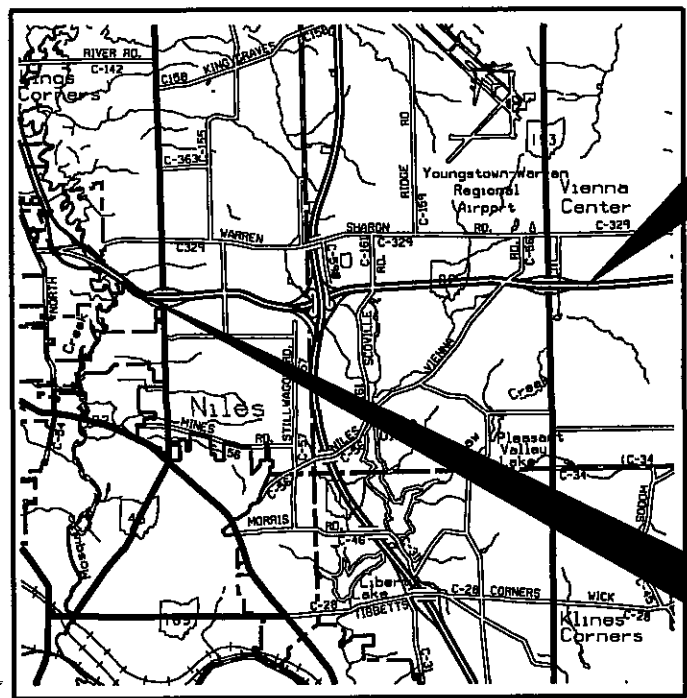


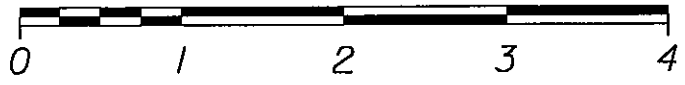
TRU - SR 82-17.49
050025 PID - 77758
Dist 4 1/12/2005



LOCATION MAP

LATITUDE 41° 14' 2" LONGITUDE -80° 45' 4"

SCALE IN MILES



PORTION TO BE IMPROVED: _____
INTERSTATE & DIVIDED HIGHWAY: _____
UNDIVIDED STATE & FEDERAL ROUTES: _____
OTHER ROADS: _____

STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

TRU-82-17.49

VIENNA TOWNSHIP
HOWLAND TOWNSHIP
TRUMBULL COUNTY

PROJECT DESCRIPTION

THIS IMPROVEMENT SHALL CONSIST OF REPAIR AND RESURFACING OF 3.54 MILES OF EXISTING PAVEMENT ON SR 82 IN TRUMBULL COUNTY.

EARTH DISTURBED AREAS

PROJECT EDA - N/A (MAINTENANCE PROJECT)
EST. CONTRACTOR EDA - N/A (MAINTENANCE PROJECT)
NOTICE OF INTENT EDA - N/A (MAINTENANCE PROJECT)

LIMITED ACCESS

THIS IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 OF THE OHIO REVISED CODE.

2002 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEARBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

INDEX OF SHEETS:

TITLE	1
TYPICAL SECTIONS	2-3
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PAVEMENT MARKING SUB-SUMMARY	12
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UNDERGROUND UTILITIES
TWO WORKING DAYS
BEFORE YOU DIG
CALL 1-800-362-2764 (TOLL FREE)
OHIO UTILITIES PROTECTION SERVICE
NON-MEMBERS
MUST BE CALLED DIRECTLY

PLAN PREPARED BY:

D4 PRODUCTION

ENGINEERS SEAL:



SIGNED: *[Signature]*
DATE: 10/15/04

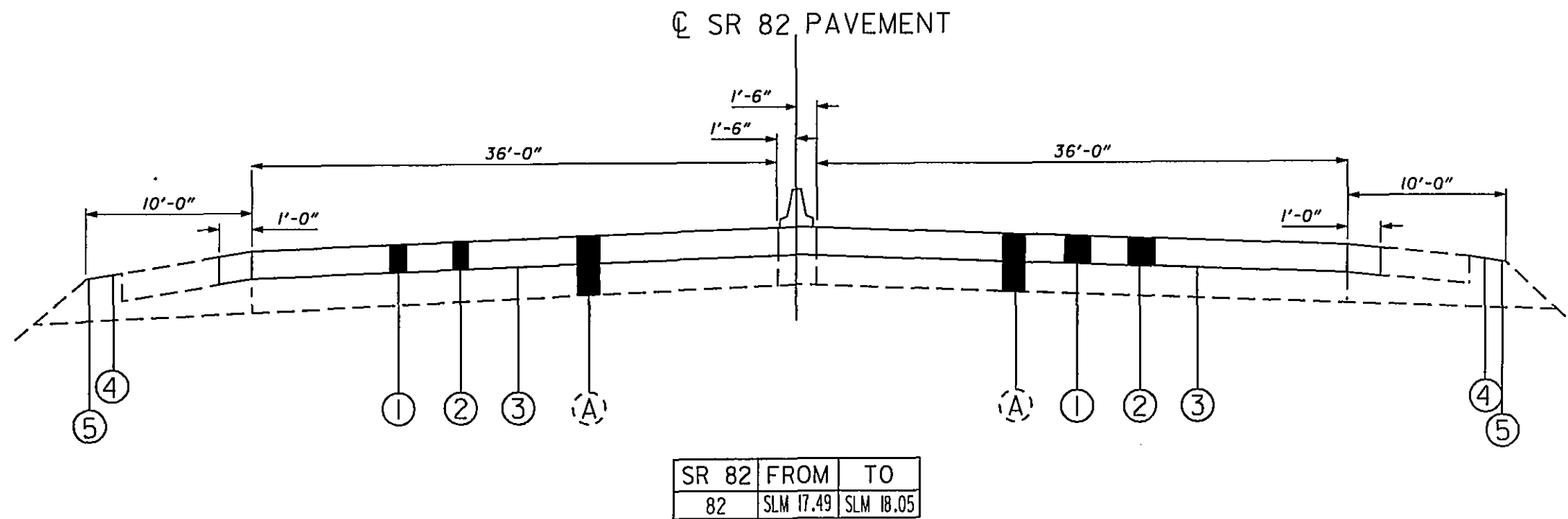
STANDARD CONSTRUCTION DRAWINGS										SUPPLEMENTAL SPECIFICATIONS
BP-3.1	7-28-00	TC-41.20	1-19-01	MT-35.10	4-20-01	PCB-91	7-19-02			832 4-17-04
BP-4.1	7-28-00	TC-42.20	7-16-04			RM-4.2	4-18-03			833 2-12-03
		TC-52.10	4-20-01							
DM-4.3	7-19-02	TC52.20	4-20-01	MT-95.31	7-16-04					
DM-4.4	7-19-02	TC65.10	10-19-01	MT-95.32	7-16-04					
		TC65.11	10-19-01	MT-98.12	4-19-02					
GR-1.1	4-18-03	TC65.12	10-19-02	MT-98.13	4-19-02					
GR-2.1	4-18-03	TC-71.10	4-19-02	MT-98.14	4-19-02					
GR-3.1	4-18-03	TC-73.10	1-19-01	MT-98.15	7-16-04					
GR-3.2	4-18-03	TC-82.10	4-19-02	MT-98.16	4-19-02					
GR-3.5	4-18-03			MT-98.17	10-18-02					
GR-4.2	10-17-03			MT-98.18	10-18-02					
GR-5.2	4-18-03			MT-99.20M	1-30-95					
GR-6.1	4-18-03			MT-105.10	10-18-02					
GR-6.2	4-18-03			MT-105.11	10-18-02					

APPROVED: *[Signature]*
DATE 10-15-04 DISTRICT DEPUTY DIRECTOR

APPROVED: *[Signature]*
DATE 11-1-04 DIRECTOR, DEPARTMENT OF TRANSPORTATION

FEDERAL PROJECT NO. E041(105)
PID NO. 77758
CONSTRUCTION PROJECT NO.
RAILROAD INVOLVEMENT NONE
TRU-82-17.49
17

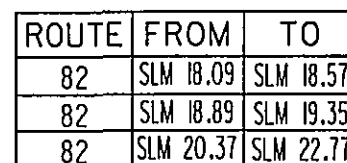
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LEGEND

- ① 448 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE IH, AS PER PLAN
- ② 254 1.5" PAVEMENT PLANING, ASPHALT CONCRETE
- ③ 407 TACK COAT
- ④ 617 COMPACTED AGGREGATE, TYPE A, AS PER PLAN
- ⑤ 408 PRIME COAT, AS PER PLAN

(A) EXISTING ASPHALT CONCRETE PAVEMENT



①	448	1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE IH, AS PER PLAN
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④	617	COMPACTED AGGREGATE, TYPE A, AS PER PLAN
⑤	408	PRIME COAT, AS PER PLAN

(A) EXISTING ASPHALT CONCRETE PAVEMENT

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. THE INSTALLATION AND OPERATION OF ALL TEMPORARY TRAFFIC CONTROL AND TEMPORARY TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS SHALL BE PROVIDED BY THE CONTRACTOR WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

CONVERSION OF STANDARD CONSTRUCTION DRAWINGS

THE METRIC STANDARD DRAWINGS REFERENCED IN THIS PLAN SHALL BE CONVERTED TO ENGLISH UNITS USING THE SI (METRIC) TO ENGLISH CONVERSION FACTORS PROVIDED IN SECTION 109.02 OF THE 2002 CONSTRUCTION AND MATERIALS SPECIFICATIONS. THE APPENDIX OF ASTM E 380 SHALL BE UTILIZED FOR ANY ADDITIONAL CONVERSION FACTORS REQUIRED. CONVERSIONS SHALL BE APPROPRIATELY PRECISE AND SHALL REFLECT STANDARD INDUSTRY ENGLISH VALUES WHERE SUITABLE.

PROFILE AND ALIGNMENT

THE PROPOSED PAVEMENT RESURFACING SHALL FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT EXCEPT WHERE OTHERWISE NOTED.

CONTINGENCY QUANTITIES

THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED "AS DIRECTED BY THE ENGINEER" UNLESS AUTHORIZED BY THE ENGINEER. THE ACTUAL WORK LOCATIONS AND QUANTITIES USED FOR SUCH ITEMS SHALL BE INCORPORATED INTO THE FINAL CHANGE ORDER GOVERNING COMPLETION OF THIS PROJECT.

UTILITIES

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER OR ADJACENT TO THE WORK AREA.

GRADING AND FILLING OPERATIONS

THE PLACEMENT OF COMPACTED AGGREGATE SHALL NOT EXTEND PAST THE EXISTING GRADED SHOULDERS. NO EXCAVATION, GRADING, OR FILLING OPERATION SHALL BE PERFORMED IN ANY WETLANDS OR STREAMS, UNLESS THE REQUIRED STATE AND/OR FEDERAL PERMITS HAVE BEEN OBTAINED IN ACCORDANCE WITH ALL APPLICABLE STATE AND/OR FEDERAL LAWS AND REGULATIONS. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR STORE EQUIPMENT AND/OR MATERIALS IN ANY WETLANDS OR STREAMS.

ITEM 407 - TACK COAT

THE RATE OF APPLICATION OF THE 407 TACK COAT SHALL BE SUBJECT TO ADJUSTMENT AS DIRECTED BY THE ENGINEER. FOR ESTIMATING PURPOSES ONLY, THE PLAN QUANTITIES INDICATE AN AVERAGE APPLICATION RATE OF:

407, TACK COAT 0.15 GAL. PER SQ. YARD FOR THE PLAINED SURFACE.

INTERSECTIONS

INTERSECTIONS SHALL BE RESURFACED A MINIMUM OF 25 FT. BEYOND THE EDGE LINE, UNLESS OTHERWISE DIRECTED BY THE ENGINEER OR INDICATED IN THE PLAN. A BUTT JOINT, AS PER STANDARD CONSTRUCTION DRAWING BP-3.1, SHALL BE USED TO PROVIDE A SMOOTH TRANSITION TO THE EXISTING PAVEMENT. INTERSECTIONS SHALL BE RESURFACED WITH STANDARD MIX, AS SHOWN ON THE ASPHALT CONCRETE SHEET. ANY GRADING OR PRIME NECESSARY TO ACCOMPLISH THIS WORK SHALL BE INCLUDED IN THE UNIT BID PRICE OF ITEM 448.

ITEM 408 - PRIME COAT, AS PER PLAN

THE CONTRACTOR WILL APPLY "MC-70" AT A RATE OF 0.4 GALLONS PER SQUARE YARD, OR AS DETERMINED BY THE ENGINEER, TO THE COMPLETED COMPACTED AGGREGATE SHOULDER. THE CONTRACTOR SHALL PROVIDE A SHIELD TO PREVENT THE SPRAYING OR DRIFTING OF LIQUID PRIME COAT MATERIAL ON THE EDGE OF THE PAVEMENT OR EDGELINE. CARE ALSO SHALL BE TAKEN TO AVOID SPRAYING LIQUID PRIME COAT MATERIAL ONTO DRIVEWAY APRONS, MAILBOX APPROACHES OR ANY PEDESTRIAN AREAS. THE ATTENTION OF THE CONTRACTOR IS DIRECTED TO 107.10 OF THE SPECIFICATIONS. AN ESTIMATED QUANTITY OF 4600 GALLONS HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR THIS WORK.

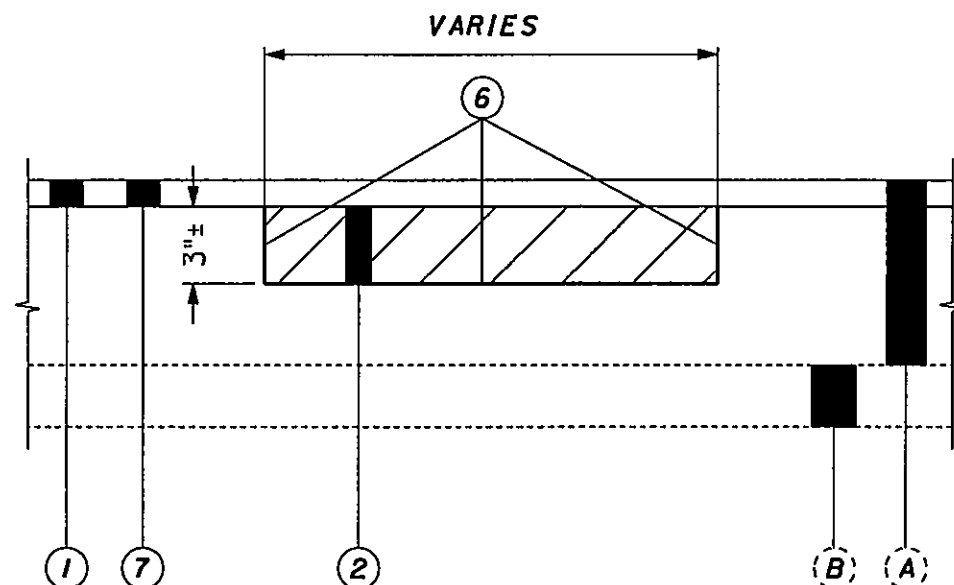
ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN

IN LOW SHOULDER AREAS EXCEEDING 1" OR AS DIRECTED BY THE ENGINEER, RECYCLED ASPHALT PAVEMENT (RAP) SHALL BE USED IN AREAS ADJACENT TO THE PAVED BERM. THE RAP SHALL HAVE A MINIMUM PG CONTENT OF 4.5% AND MEET THE FOLLOWING GRADATION. ONCE THE STOCKPILE MEETS THE GRADATION, THE PG CONTENT OF THE RAP SHALL BE DETERMINED PER 441.03. THE RAP ANALYSIS MUST BE SUBMITTED TO THE ENGINEER FOR APPROVAL 2 WEEKS PRIOR TO USE. METHOD OF MEASUREMENT SHALL BE AS PER 617.06. PLACEMENT OF COMPACTION SHALL MEET THE REQUIREMENTS OF ITEM 617. ALL MATERIALS, LABOR EQUIPMENT, TOOLS, AND INCIDENTALS NECESSARY TO COMPLETE THE WORK SHALL BE INCLUDED IN THE UNIT BID PRICE FOR ITEM 617 COMPACTED AGGREGATE, TYPE A, AS PER PLAN. AN

MODIFIED GRADATION SHALL APPLY:
SIEVE TOTAL PERCENT PASSING

1-1/2"	100
3/4"	50-90
NO. 4	35-70
NO. 30	9-33
NO.200	0-13

AN ESTIMATED QUANTITY OF 650 CU. YDS. HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR THIS WORK.



ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR

A QUANTITY OF THIS ITEM SHALL BE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER. THIS WORK SHALL NOT BEGIN UNTIL EXISTING PAVEMENT HAS BEEN PLANED AND SHALL CONSIST OF REPAIRING LOCATIONS, EXHIBITING SURFACE DETERIORATION AND PLACING 3"± OF ITEM 448 ASPHALT CONCRETE. IT IS NOT THE INTENT TO REPAIR EVERY DETERIORATED AREA WITHIN THE PROJECT. THE ASPHALT CONCRETE SHALL BE COMPACTED WITH A TYPE I PNEUMATIC TIRE ROLLER AND A STEEL WHEEL ROLLER AS PER 401.16. PAYMENT SHALL BE BASED ON THE ACTUAL NUMBER OF SQUARE YARDS OF PAVEMENT REPAIR.

ITEM 251 PARTIAL DEPTH PAVEMENT REPAIR 50 SQ.YD.

- ① 254 1.75" PAVEMENT PLANING, ASPHALT CONCRETE
- ② 251 3"± PARTIAL DEPTH PAVEMENT REPAIR
- ③ 253 12"± PAVEMENT REPAIR
- ④ 203 6" AVG. EXCAVATION
- ⑤ 304 AGGREGATE BASE

ITEM 448 - ASPHALT CONCRETE SURFACE COURSE, TYPE IH, AS PER PLAN

THE REQUIREMENTS OF 441, 446, AND SUPPLEMENTAL SPECIFICATIONS 908 SHALL APPLY; DEVIATIONS FROM THESE ARE AS FOLLOWS:

THE COMBINATION OF NEW AGGREGATES, NEW ASPHALT BINDER, AND RECLAIMED MATERIAL SHALL BE AS REQUIRED TO PRODUCE A COMPOSTION CONTAINING A MINIMUM OF 6.2% NEW ASPHALT BINDER RESULTING IN A MINIMUM TOTAL BINDER OF 6.9%.

ANY PERCENTAGE OF RECLAIMED MATERIAL PROPOSED FOR USE SHALL BE INCLUDED IN THE MIX DESIGN PROCESS TO ESTABLISH THE JOB MIX FORMULA (JMF) IN ACCORDANCE WITH 441.02

MATERIALS: THE MATERIALS SHALL BE
 AGGREGATES 703.05*
 ASPHALT BINDER SS-908

*THE VIRGIN COURSE AGGREGATE PORTION OF THE MIXTURE SHALL BE AIR COOLED BLAST FURNACE SLAG AND MEET THE REQUIREMENTS OF 703.05.

ONLY RECLAIMED PAVEMENT FROM THIS PROJECT OR AN APPROVED EQUAL DETERMINDED BY THE DISTRICT ENGINEER OF TESTS WILL BE PERMITTED FOR USE IN THIS ITEM.

PAVEMENT MARKING LANE WIDTHS

THE NORMAL LANE WIDTH FOR THE PAVEMENT MARKINGS ON THIS PROJECT SHALL BE AS FOLLOWS:

ROUTE	LANE WIDTH
S.R.82	12'
E.MARKET ST. ON RAMP	12'
SR 46 WB OFF RAMP	24'
SR 46 EB ON RAMP	24'
SR 46 EB OFF RAMP	12'

CALCULATED
LMB
CHECKED
DAY

GENERAL NOTES

TRU-82-17.49

5
17

MAINTENANCE OF TRAFFIC

THIS ITEM SHALL CONSIST OF MAINTENANCE OF TRAFFIC ON EXISTING ROADWAYS AND RAMPS IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, CURRENT EDITION, LATEST REVISION, THE SPECIFICATIONS AND THE FOLLOWING:

1. A MINIMUM OF ONE ELEVEN FOOT LANE IN EACH DIRECTION SHALL BE MAINTAINED ON THE EXISTING PAVEMENT OR COMPLETED PAVEMENT DURING CONSTRUCTION OF THE WORK.
2. THE CONTRACTOR SHALL INFORM THE DISTRICT OFFICE (330) 786-3148, EIGHTEEN (18) DAYS PRIOR TO THE BEGINNING OF WORK.
3. CONES SHALL NOT BE ACCEPTABLE TRAFFIC CONTROL DEVICES FOR LANE RESTRICTIONS OR LANE REDUCTIONS THAT ARE IN OPERATION ONE-HALF HOUR AFTER SUNSET OR ONE HALF-HOUR BEFORE SUNRISE. ALL NIGHTTIME LANE RESTRICTIONS SHALL REQUIRE DRUMS OR BARRICADES AT A MAXIMUM SPACING OF FIFTY (50) FEET. WEIGHTED CHANNELIZERS MAY BE USED IN ACCORDANCE WITH THE ADDITIONAL NOTE HEREIN.
4. LANE RESTRICTIONS OR LANE REDUCTIONS SHALL NOT BE PERMITTED AFTER NORMAL WORKING HOURS. NORMAL WORKING HOURS SHALL BE THOSE HOURS DURING WHICH THE CONTRACTOR HAS A FULL COMPLEMENT OF EMPLOYEES AND EQUIPMENT ACTIVELY REMOVING AND/OR PLACING PAVEMENT MATERIALS.
5. THE CONTRACTOR SHALL FURNISH, ERECT, MAINTAIN AND SUBSEQUENTLY REMOVE ALL FLAGS, BARRICADES, SIGNS, SIGN SUPPORTS AND FURNISH AND MAINTAIN ALL FLAGGERS, WATCHERS AND INCIDENTALS RELATED THERETO.
6. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE PERMITTED TO HAVE SUCCESSIVE WORK ZONES UNLESS THE DISTANCE BETWEEN THE DRUMS, BARRICADES OR CONES EXCEEDS TWO (2) MILES.
7. IN ADDITION TO THE REQUIREMENTS OF 614 WORK ZONE PAVEMENT MARKINGS (614.11), AT THE END OF EACH DAY OF WORK, THE CONTRACTOR SHALL REPLACE (WITH TEMPORARY MARKINGS) ALL LANE OR CHANNELIZING AND STOP LINES THAT WERE REMOVED OR COVERED DURING THE PAVEMENT REMOVAL OR PLACEMENT OPERATIONS. QUANTITIES FOR SUCH REPLACEMENT ARE CARRIED AS PART OF THE ITEMS LISTED UNDER 614 WORK ZONE PAVEMENT MARKINGS.
8. A QUANTITY OF 20 CU. YDS. OF 614 ASPHALT CONCRETE FOR MAINTAINING TRAFFIC SHALL BE PROVIDED FOR USE IN MAINTAINING PAVEMENT, SHOULDERS AND OTHER LOCATIONS PRIOR TO RESURFACING, AS DIRECTED BY THE ENGINEER.
9. PRIOR TO OPENING TO TRAFFIC, EACH LANE SHALL BE IN A SAFE, PASSABLE CONDITION. ALL TRANSVERSE JOINTS SHALL EXTEND ACROSS THE FULL LANE AND SHOULDER WIDTH AND EACH LANE SHALL BE FREE FROM UNEVEN LONGITUDINAL JOINTS. THE CONTRACTOR SHALL PROVIDE ASPHALT WEDGES FOR TRANSVERSE JOINTS WHEREVER THERE ARE PAVEMENT ELEVATION DIFFERENCES.
10. A QUANTITY OF ITEM 614 WORK ZONE MARKING SIGN HAS BEEN INCLUDED IN THE PLAN. THIS QUANTITY SHALL INCLUDE, BUT NOT BE LIMITED TO, THE FOLLOWING SIGNS: OW-62 [BUMP], OW-167 [NO EDGE LINES], OW-171 [UNEVEN LANES SYMBOL]. THESE QUANTITIES SHALL BE AS PER 614.04.
11. THE CONTRACTOR SHALL PROVIDE ADEQUATE DRAINAGE OF THE MILLED SURFACES BY MILLING SLOTS IN THE OUTSIDE SHOULDER AS NEEDED.

THE FOLLOWING QUANTITIES SHALL BE USED FOR THE MAINTENANCE OF TRAFFIC ON THIS PROJECT:

ITEM 614 WORK ZONE LANE LINE, CLASS I	15.32 MILE
ITEM 614 WORK ZONE CHANNELIZING LINE, CLASS I	7310 FT.
ITEM 614 WORK ZONE STOP LINE, CLASS I	272 FT.
ITEM 614 WORK ZONE MARKING SIGN	24 EACH

METHOD OF PAYMENT

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR 614 - MAINTAINING TRAFFIC, UNLESS SEPERATELY ITEMIZED IN THE PLAN.

WEIGHTED CHANNELIZERS

THE WEIGHTED CHANNELIZER SHALL BE PREDOMINATELY ORANGE IN COLOR AND SHALL BE MADE OF A LIGHTWEIGHT, FLEXIBLE, AND DEFORMABLE MATERIAL. THEY SHALL BE AT LEAST 42 INCHES IN HEIGHT WITH A WEIGHTED BASE. THEY MAY HAVE A "HANDLE" OR LIFTING DEVICE WHICH EXTENDS ABOVE THE 42" MINIMUM HEIGHT.

THE MARKINGS ON THE WEIGHTED CHANNELIZER SHALL BE HORIZONTAL, CIRCUMFERENTIAL, ALTERNATING ORANGE AND WHITE RETROREFLECTIVE STRIPES 6 INCHES WIDE. EACH WEIGHTED CHANNELIZER SHALL HAVE A MINIMUM OF TWO ORANGE AND TWO WHITE STRIPES. ANY NON-RETROREFLECTIVE SPACES BETWEEN THE HORIZONTAL ORANGE AND WHITE STRIPES SHALL NOT EXCEED 2 INCHES WIDE. THE WEIGHTED CHANNELIZER SHALL HAVE A 4-INCH MINIMUM WIDTH, REGARDLESS OF ORIENTATION.

ON FREEWAYS AND MULTILANE HIGHWAYS:

USE OF WEIGHTED CHANNELIZERS ON FREEWAYS AND MULTILANE HIGHWAYS SHALL BE LIMITED TO SHORT-TERM OPERATION, GENERALLY TWELVE HOURS OR LESS, FOR EITHER DAY OR NIGHT. UPON COMPLETION OF WORK WITHIN THE ABOVE NOTED TIME PERIOD, THE WEIGHTED CHANNELIZERS SHALL BE REMOVED. THE WEIGHTED CHANNELIZERS MAY AGAIN BE PLACED ON THE HIGHWAY WHEN THE WORK IS TO RESUME ON THE FOLLOWING DAY OR NIGHT. ANY LANE CLOSURE USING CHANNELIZATION DEVICES, EXPECTED TO REMAIN FOR MORE THAN TWELVE HOURS, SHALL REQUIRE THE USE OF DRUMS OR BARRIERS.

WHEN USED AT NIGHT, WEIGHTED CHANNELIZERS SHALL ONLY BE PLACED IN THE "TANGENT AREA". THE "TANGENT AREA" IS DEFINED AS THE AREA AFTER THE TRANSITION TAPER WHERE THE WORK TAKES PLACE. DRUMS SHALL BE USED IN THE TRANSITION TAPERS FOR NIGHT OPERATIONS.

MAXIMUM SPACING OF THE WEIGHTED CHANNELIZER SHALL BE 40 FEET.

STEPS SHOULD BE TAKEN TO ENSURE THAT THE WEIGHTED CHANNELIZERS WILL NOT BE BLOWN OVER OR DISPLACED BY WIND OR MOVING TRAFFIC. BALLASTS SHOULD NOT PRESENT A HAZARD IF THE WEIGHTED CHANNELIZERS ARE INADVERTENTLY STRUCK, NOR SHOULD THEY AFFECT THE VISIBILITY OF THE WEIGHTED CHANNELIZERS. ALL BALLASTS USED SHOULD BE IN ACCORDANCE WITH THE MANUFACTURER'S SPECIFICATIONS.

ADVANCE NOTICE TO PAVE

THE CONTRACTOR SHALL SUBMIT FOR APPROVAL TO THE DISTRICT CONSTRUCTION ENGINEER A DETAILED SCHEDULE 15 DAYS PRIOR TO THE PLACEMENT OF THE OVERLAY COURSES, ON HOW THEY PROPOSE TO PROSECUTE THE PAVING OPERATIONS. THE DETAILS SHALL SHOW THE ORDER OF PERFORMANCE OF EACH STAGE (START TO FINISH) OF THE WORK INCLUDING THE MAINTENANCE OF TRAFFIC THAT WILL BE USED.

LANE CLOSURES

DURATION OF LANE CLOSURES AND RESTRICTIONS BETWEEN THE BEGINNING OF THE PROJECT AND SR46 SHALL BE AS PER THE PERMITTED LANE CLOSURE CHART. THE PERMITTED LANE CLOSURE CHART USED FOR THIS PROJECT SHALL BE THE MOST CURRENT AVAILABLE WHEN THIS PROJECT SELLS. THE CHART CAN BE FOUND AT: https://dotaw100.dot.state.oh.us/plcm/plcm_web.jsp

ON SR82 BETWEEN SR46 AND SR11 NO LANE CLOSURES ARE PERMITTED; FROM 7AM TO 10AM AND FROM 2PM TO 6PM MONDAY THRU FRIDAY AND FROM 11AM TO 2PM ON SATURDAY AND SUNDAY

ON SR82 BETWEEN SR11 AND THE END OF THE PROJECT NO LANE CLOSURES ARE PERMITTED; FROM 4PM TO 6PM MONDAY THRU FRIDAY.

THE FAILURE OF THE CONTRACTOR TO MEET THE LIMITATION TIMES WILL CAUSE SEPARATE LIQUIDATED DAMAGES OF \$1,000 PER HOUR OF OVERRUN OF LIMITATION TIME TO BE ASSESSED. THE CONTRACTOR WILL COMPLY WITH ALL PROVISIONS OF CMS 108.07.

CONTRACTOR'S EQUIPMENT - OPERATION AND STORAGE

A QUALIFIED FLAGGER SHALL BE EMPLOYED WHERE THE CONTRACTOR'S EQUIPMENT MUST MERGE WITH THE TRAFFIC STREAM. THE CONTRACTOR'S EQUIPMENT SHALL BE EQUIPPED WITH AT LEAST ONE AMBER FLASHING LIGHT. PAVERS, ROLLERS AND OTHER EQUIPMENT MAY BE PARKED IN AREAS ALONG THE HIGHWAY WHEN PAVING OPERATIONS ARE SCHEDULED TO CONTINUE WITHIN THE NEXT WORKDAY. OTHERWISE, THE EQUIPMENT SHALL BE STORED AT A STORAGE AREA OUTSIDE THE R/W, THE LOCATION OF WHICH SHALL HAVE PRIOR APPROVAL OF THE ENGINEER. WHEN PARKING ALONG THE HIGHWAY, THE EQUIPMENT SHALL BE PLACED AND DELINEATED AS PER 614.03. NO EQUIPMENT SHALL BE PARKED IN THE MEDIAN OF THE HIGHWAY. ADEQUATE BARRICADES AND LIGHTS SHALL BE PLACED ON THE PAVEMENT SIDE OF THE EQUIPMENT TO IDENTIFY THE LIMITS OF THE EQUIPMENT. ALL OTHER EQUIPMENT, INCLUDING PRIVATE VEHICLES, SHALL BE STORED AT THE APPROVED CONTRACTOR'S STORAGE AREA. NO EQUIPMENT SHALL BE PARKED ON PRIVATE PROPERTY UNLESS PRIOR APPROVAL OF THE OWNER AND THE PROJECT ENGINEER/SUPERVISOR HAS BEEN GRANTED.

TRAFFIC CONTROL INSPECTOR

THE CONTRACTOR SHALL DESIGNATE AN INDIVIDUAL OTHER THAN THE SUPERINTENDENT AND SUBJECT TO THE APPROVAL OF THE ENGINEER, TO CONTINUOUSLY INSPECT ALL TRAFFIC CONTROL DEVICES WHENEVER CONSTRUCTION WORK IS BEING PERFORMED WITHIN THE WORK LIMITS OF THE PROJECT. THE DESIGNATED INDIVIDUAL SHALL ALSO INSPECT ALL TRAFFIC DEVICES AT THE BEGINNING AND AT THE END OF EACH WORK DAY. THE DESIGNATED INDIVIDUAL OR A QUALIFIED REPRESENTATIVE SHALL ALSO BE AVAILABLE ON AN AROUND THE CLOCK BASIS TO REPAIR AND/OR REPLACE DAMAGED OR MISSING TRAFFIC CONTROL DEVICES. THESE INDIVIDUALS SHALL BE EQUIPPED WITH CELLULAR PHONES AND THEIR NAMES AND PHONE NUMBERS SHALL BE GIVEN TO THE PROJECT ENGINEER AT THE PRE-CONSTRUCTION MEETING. THE DESIGNATED INDIVIDUAL MAY HAVE OTHER CONSTRUCTION RELATED DUTIES AS LONG AS IMMEDIATE ATTENTION IS GIVEN TO TRAFFIC CONTROL. PAYMENT FOR THE SERVICES OF THE TRAFFIC CONTROL INSPECTOR SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614 MAINTAINING TRAFFIC.

WINTER TRAFFIC LIMITATIONS

ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC BETWEEN NOVEMBER 15 AND APRIL 1. NOVEMBER 14 SHALL BE CONSIDERED TO CONSTITUTE AN INTERIM COMPLETION DATE AND LIQUIDATED DAMAGES AS SPECIFIED IN 108.07 SHALL BE ASSESSED FOR EACH CALENDAR DAY THAT THE ROADWAY REMAINS CLOSED TO TRAFFIC BEYOND THE SPECIFIED LIMIT. THE CONTRACTOR MAY CLOSE LANES PRIOR TO APRIL 1 WITH WRITTEN APPROVAL FROM THE DISTRICT CONSTRUCTION ENGINEER.

FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHT TIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NO CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS. VEHICLE HEADLIGHTS ARE NO ACCEPTABLE LIGHTING FOR ILLUMINATING THE WORK AREA.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR MAINTAINING TRAFFIC.

ITEM 614 - MAINTAINING TRAFFIC

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

CHRISTMAS	NEW YEARS
MEMORIAL DAY	FOURTH OF JULY
LABOR DAY	THANKSGIVING

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF THE WEEK	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 12:00N MONDAY
MONDAY	12:00N FRIDAY THROUGH 12:00N TUESDAY
TUESDAY	12:00N MONDAY THROUGH 12:00N WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 12:00N THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 12:00N MONDAY
FRIDAY	12:00N THURSDAY THROUGH 12:00N MONDAY
SATURDAY	12:00N FRIDAY THROUGH 12:00N MONDAY

NO EXTENSIONS OF TIME SHALL BE GRANTED FOR DELAYS IN MATERIAL DELIVERIES, UNLESS SUCH DELAYS ARE INDUSTRY-WIDE, OR FOR LABOR STRIKES, UNLESS SUCH STRIKES ARE AREA-WIDE.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED LIQUIDATED DAMAGES IN ACCORDANCE WITH 108.07.

ITEM 614 PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, 2 PORTABLE CHANGEABLE MESSAGE SIGNS [PCMS], ON SITE, FOR THE DURATION OF TIME SPECIFIED IN THIS NOTE. EACH SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS MAINTAINED BY THE DIRECTOR. THE CLASS 1 UNITS SHALL HAVE A MINIMUM LEGIBILITY DISTANCE OF 1250 FEET.

EACH SIGN SHALL BE TRAILER MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM TO DIM THE SIGN DURING DARKNESS AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY.

PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE PCMS SHOULD NOT BE LOCATED IN THE MEDIAN OF THE HIGHWAY UNLESS IT IS PROTECTED FROM BOTH DIRECTIONS OF TRAFFIC. THE PCMS SHOULD BE LOCATED BEHIND GUARDRAIL WHEREVER POSSIBLE. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE THE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS WILL BE OFF, FACING AWAY FROM ALL TRAFFIC AND SHALL DISPLAY ONE OR MORE HIGH INTENSITY YELLOW REFLECTIVE SHEETING SURFACES OF 9-INCH BY 15-INCH MINIMUM SIZE FACING TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT AND TO REVISE SIGN MESSAGES, IF NECESSARY.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE CONTRACTOR. A LIST OF ALL PROPOSED PREPROGRAMMED MESSAGES WILL BE GIVEN TO THE ENGINEER PRIOR TO CONSTRUCTION. THE SIGN SHALL HAVE TWO DIFFERENT MEMORIES [PROM AND RAM] AND CAPABILITY TO STORE UP TO 99 MESSAGES IN EACH MEMORY. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. IN ORDER TO CONVEY A MAXIMUM OF INFORMATION AT A SINGLE GLANCE, ONLY THREE LINE PRESENTATION FORMATS WITH A MAXIMUM OF SIX MESSAGE PHASES WILL BE PERMITTED. NORMALLY, ONLY A MAXIMUM OF THREE MESSAGE PHASES SHOULD BE EMPLOYED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST ONCE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DE-ACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL [IN ACTIVE CELLULAR AREAS] ALLOW REMOTE SIGN ACTIVATION, DEACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS AND REVISIONS TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES.

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF 614. THE CONTRACTOR SHALL PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC AND THE ENTIRE COST TO CONTROL TRAFFIC ACCRUED BY THE DEPARTMENT WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24 HOURS PER DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES. THE REQUIREMENT TO FURNISH, INSTALL, MAINTAIN AND REMOVE A PCMS UNIT ON THIS PROJECT SHALL NOT IN ANY WAY RELIEVE THE CONTRACTOR OF HIS RESPONSIBILITIES AS OUTLINED IN 614.02.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE BID FOR EACH SIGN MONTH OF ITEM 614 PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN AND SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614 PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN
6 SIGN MONTH

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR)

IN ADDITION TO THE REQUIREMENTS OF 614 AND THE LATEST EDITION OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD), A UNIFORMED LAW ENFORCEMENT OFFICER (AND OFFICIAL PATROL CAR WITH WORKING TOP MOUNTED EMERGENCY FLASHING LIGHTS) SHALL BE PROVIDED FOR CONTROLLING TRAFFIC FOR THE FOLLOWING TASKS:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR-DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT, OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED.

LAW ENFORCEMENT OFFICERS (LEO'S) SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED. THE LEO'S ARE CONSIDERED TO BE EMPLOYED BY THE CONTRACTOR AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THEIR ACTIONS. ALTHOUGH THEY ARE EMPLOYED BY THE CONTRACTOR, THE PROJECT ENGINEER SHALL HAVE CONTROL OVER THEIR PLACEMENT. THE OFFICIAL CAR SHALL BE A PUBLIC SAFETY VEHICLE AS REQUIRED BY THE OHIO REVISED CODE.

THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR THESE SERVICES WITH:

Trumbull County
Warren Patrol Post
3424 State Route 422
Southington, OH 44470
phone: (330) 898-2311

IF AFTER CONTACTING THE OHIO HIGHWAY PATROL, IT IS DETERMINED THAT THEY CANNOT SUPPLY THE LEO, THEN AN AUTHORIZED MUNICIPAL OR COUNTY POLICE OFFICER WITH A MARKED AND FLASHER-LIGHT EQUIPPED OFFICIAL POLICE OR PATROL CAR SHALL BE PROVIDED.

LAW ENFORCEMENT OFFICERS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR). THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR 400 HOURS

THE HOURS PAID SHALL INCLUDE MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

IF THE CONTRACTOR WISHES TO UTILIZE LEO'S FOR FLAGGING AND TRAFFIC CONTROL OTHER THAN FOR THAT REQUIRED IN THESE PLANS, HE MAY DO SO AT HIS OWN EXPENSE. PAYMENT FOR THE EXCESS ABOVE CONTRACT REQUIREMENTS WILL BE INCLUDED UNDER ITEM 614 MAINTAINING TRAFFIC.

GENERAL NOTES

1. It is intended that this drawing be used for treatment of drop-offs that develop during construction operations, and that are not otherwise provided for in the construction plans. Where the plans do not provide specific items for labor, equipment, or materials to implement the drop-off treatments specified herein, they shall be included for payment in the lump sum bid for Item 614 - Maintaining Traffic.

2. While the need for certain advisory signing is noted herein, it is not intended that this be indicative of all signing that may be required to advise or warn motorist, and all requirements of the Ohio Manual of Uniform Traffic Control Devices (OMUTCD) must be fulfilled.

3. In urban or otherwise heavily developed areas where pedestrians and/or bicyclists may be present in significant numbers, additional signing and protective measures other than those shown herein may be required.

4. The drop-off treatment selected for use at any given location shall be appropriate for the prevailing conditions at the site.

5. Where concrete barrier is specified, it shall be in accordance with Standard Construction Drawing PCB - 9.1, RM-4.2 and Item 622.

6. When drums are specified for a drop-off condition, a minimum number of four drums shall be used. Spacing shall be as indicated in the plans or as specified in the OMUTCD.

7. When W8-9 (Low Shoulder) signs or W8-11 (Uneven Lanes) and R4-9 (Stay in Lane) signs are required, they shall be placed 750' in advance of the condition, on all intersecting entrance ramps within the limits of the condition and immediately beyond all intersecting roadways within the limits of the condition. When the drop-off condition extends more than one-half mile, additional signs shall be erected at intervals of a maximum of one mile.

8. For locations, such as: at ramps, lane shifts, lane closures, etc., where traffic is required to negotiate any difference in elevation between pavements, the Optional Wedge Treatment shall be provided.

9. Portable concrete barrier shall be placed on the same level as the traffic surface and shall not encroach on lane width(s) designated as the minimum required for traffic use. Where drums are used, and their presence would reduce traveled lane widths to less than 10', drums may be placed on the opposite level from that of traffic provided the drop-off depth does not exceed 5" and approval is granted by the Project Engineer.

10. Pavement Repairs (or similar work):

a. Lengths greater than 60 feet - utilize appropriate treatment from Condition I.

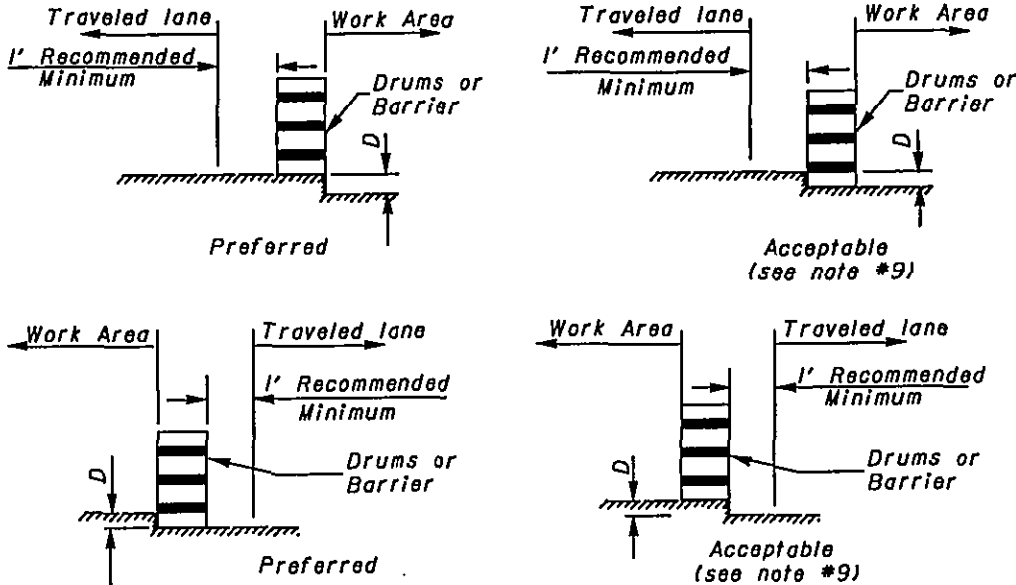
b. Lengths of 60 feet or less - repairs shall be affected in accordance with Item 255.08. Drums may be used as a separator adjacent to the traveled lane.

CONDITION I

1. These treatments are to be used for resurfacing, pavement planing excavation, etc., between, beside or within traveled lanes.

Distance From Traveled Lane	D (in)	Treatment
IFT-12FT	$\leq 1\frac{1}{2}$	Erect W8-11.
IFT-12FT	$1\frac{1}{2}$ -3	1. Lane closure utilizing drums* as shown below. (use only on 3 or more lanes) - or - 2. Optional Wedge Treatment.
IFT-12FT	3 - 5	Lane closure utilizing drums as shown below
IFT-12FT	5 - 12	Lane closure utilizing portable concrete barrier as shown below.
>12FT-20FT	12 - 24	Lane closure utilizing drums as shown below
>12FT-20FT	>24	Lane closure utilizing portable concrete barrier as shown below.

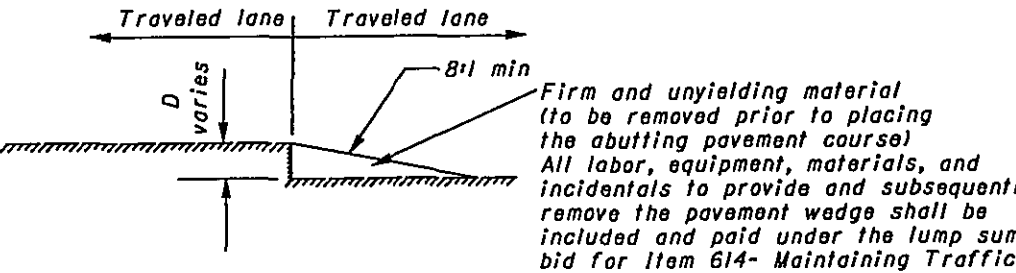
*Cones may be used for daytime only conditions.



OPTIONAL WEDGE TREATMENT

(MILLING OR RESURFACING)

1. This treatment may be used when permitted for Condition I only.
2. W8-11 and R4-9 signs required.



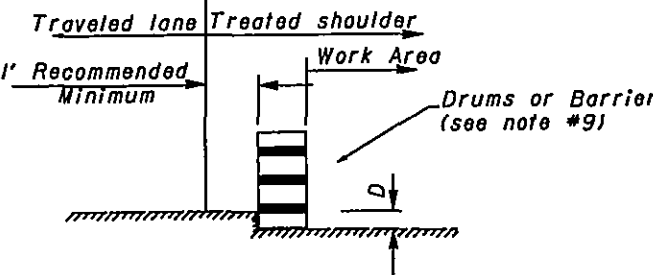
CONDITION II

DROPOFFS WITHIN GRADED SHOULDER AREA
[except for linear grading areas]

The treatments indicated below are for use in conjunction with resurfacing, planing, or excavation within the graded shoulder area.

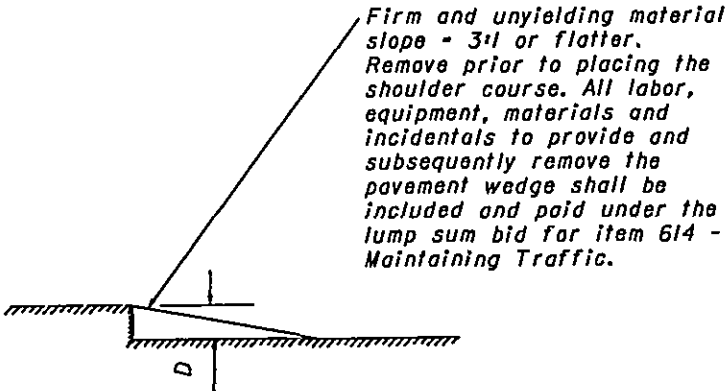
Distance From Traveled Lane	D (in)	Treatment
IFT-12FT	$\leq 1\frac{1}{2}$	1.) If edgelines are present, no treatment necessary. or 2.) Erect W8-11 and R4-9 signs
IFT-12FT	$1\frac{1}{2}$ - 5	1) If min. lane widths* requirements can be met, maintain lanes utilizing drums as shown below. - or - 2) If min. lane width* requirements cannot be met, close adjacent lane utilizing drums. (use only on 3 or more lanes) - or - 3) Optional shoulder treatment
>12FT-30FT	<- 24	Shoulder closure utilizing drums as shown below
>12FT-30FT	>24	Shoulder closure utilizing portable concrete barrier as shown below.

*Minimum lane widths shall be 10' unless otherwise specified in the plans.



OPTIONAL SHOULDER TREATMENT

1. This treatment shall not be used within a bituminous shoulder where a hot longitudinal joint per 401.17 is required.
2. W8-9 signs required.

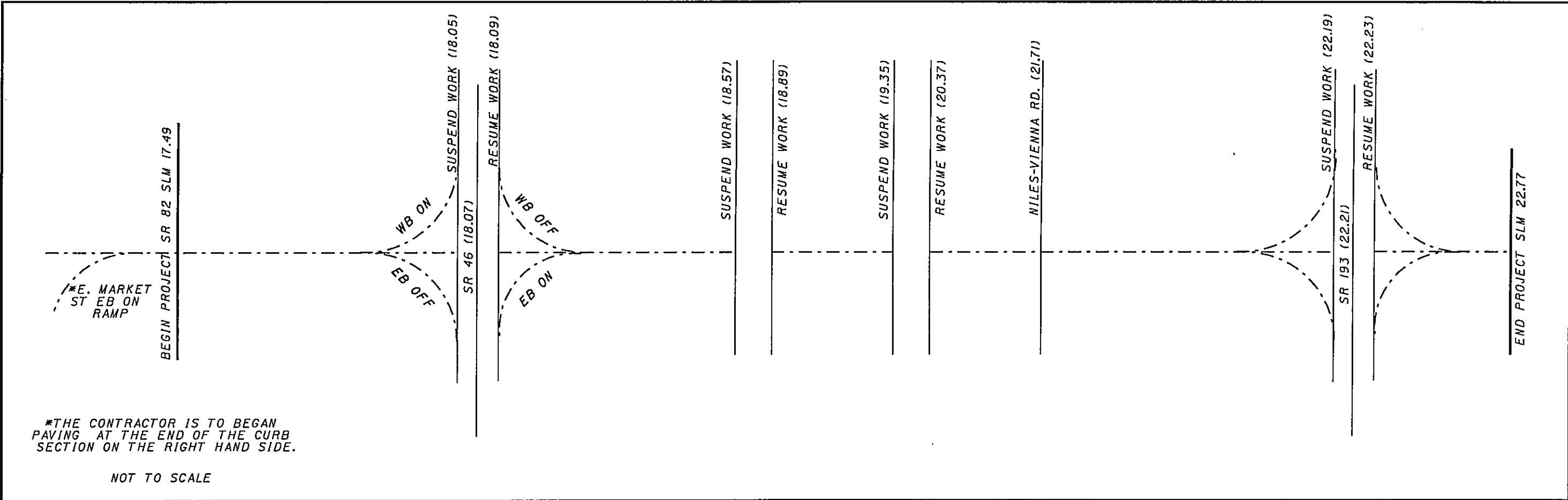


SHEET NUMBER								PARTICIPATION			ITEM	ITEM EXT.	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
4	5	6	7	10	11	12										
														ROADWAY		
					565					202	54000	565	EACH	RPM REMOVED AND DISPOSED		
														PAVEMENT		
	50									251	01000	50	SQ YD	PARTIAL DEPTH PAVEMENT REPAIR		
				166681						254	01000	166681	SQ YD	PAVEMENT PLANING, ASPHALT CONCRETE		
				25005						407	10000	25005	GALLON	TACK COAT FOR PLAINED SURFACES 0.15 GAL/SY		
4600										408	10001	4600	GALLON	PRIME COAT, AS PER PLAN	4	
				6949						448	50001	6949	CU YD	ASPHALT CONCRETE SURFACE COURSE, TYPE IH, AS PER PLAN	10	
650										617	10101	650	CU YD	COMPACTED AGGREGATE, TYPE A, AS PER PLAN	4	
														TRAFFIC CONTROL		
					134					621	10010	134	EACH	RPM, LOW PROFILE, WHITE		
					514					621	10000	514	EACH	RPM, LOW PROFILE WHITE/RED		
						822				646	10600	822	FT	TRANSVERSE LINE		
						19.75				646	10000	19.75	MILE	EDGE LINE		
						7.66				646	10100	7.66	MILE	LANE LINE		
						3655				646	10300	3655	FT	CHANNELIZING LINE		
						136				646	10400	136	FT	STOP LINE		
														MAINTENANCE OF TRAFFIC		
			400							614	11100	400	HOURL	LAW ENFORCEMENT OFFICER WITH PATROL CAR		
		24								614	12460	24	EACH	WORK ZONE MARKING SIGN		
		20								614	13000	20	CU YD	ASPHALT CONCRETE FOR MAINTAINING TRAFFIC		
			6							614	18601	6	SIGNMNTH	PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN	6	
		15.32								614	20000	15.32	MILE	WORK ZONE LANE LINE, CLASS I		
		7310								614	23000	7310	FT	WORK ZONE CHANNELIZING LINE, CLASS I		
		272								614	26000	272	FT	WORK ZONE STOP LINE, CLASS I		
										614	11000	LUMP		MAINTAINING TRAFFIC		
										619	16010	4	MONTH	FIELD OFFICE, TYPE B		
										623	10000	LUMP		CONSTRUCTION LAYOUT STAKES		
										624	10000	LUMP		MOBILIZATION		

GENERAL SUMMARY

TRU-82-17.49

9
17



PAVEMENT DATA												
ROUTE	LOG POINT TO LOG POINT	LENGTH		WP FEET AVG.	PAVEMENT AREA SQ. YDS.	254	407	448				
		MILES	LIN.FT.			PAVMENT PLANING, ASPHALT CONCRETE, 1 1/2" SQ. YD.	TACK COAT FOR PLANED SURFACES @0.15 GAL./SY. GALLON	THICK INCHES	CU. YD.			
SR 82	17.49-18.05	0.56	2957	74	24312	24312	3647	1 1/2"	1013			
SR 82	18.09-18.57	0.48	2534	52	14644	14644	2197	1 1/2"	611			
SR 82	18.89-19.35	0.46	2428	58	15653	15653	2348	1 1/2"	653			
SR 82	20.37-22.77	2.40	12672	58	81664	81664	12250	1 1/2"	3403			
E. Market St on Ramp	16.89-17.49	0.60	3168	27	9504	9504	1426	1 1/2"	396			
SR 46 EB Off Ramp			500	26	1445	1445	217	1 1/2"	61			
SR 46 WB Off Ramp			700	30	2334	2334	251	1 1/2"	98			
SR 46 WB On Ramp			500	44	7125	7125	1069	1 1/2"	297			
INTERSECTIONS, ACCELERATION, AND DECELERATION LANES					10000	10000	1500		417			
CARRIED TO THE GENERAL SUMMARY					166681	166681	25005		6949			

RPM REPLACEMENT / REMOVAL	
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STANDARD CONSTRUCTION DWG.

TC-65.10 10-19-01

TC-65.11	10-19-01
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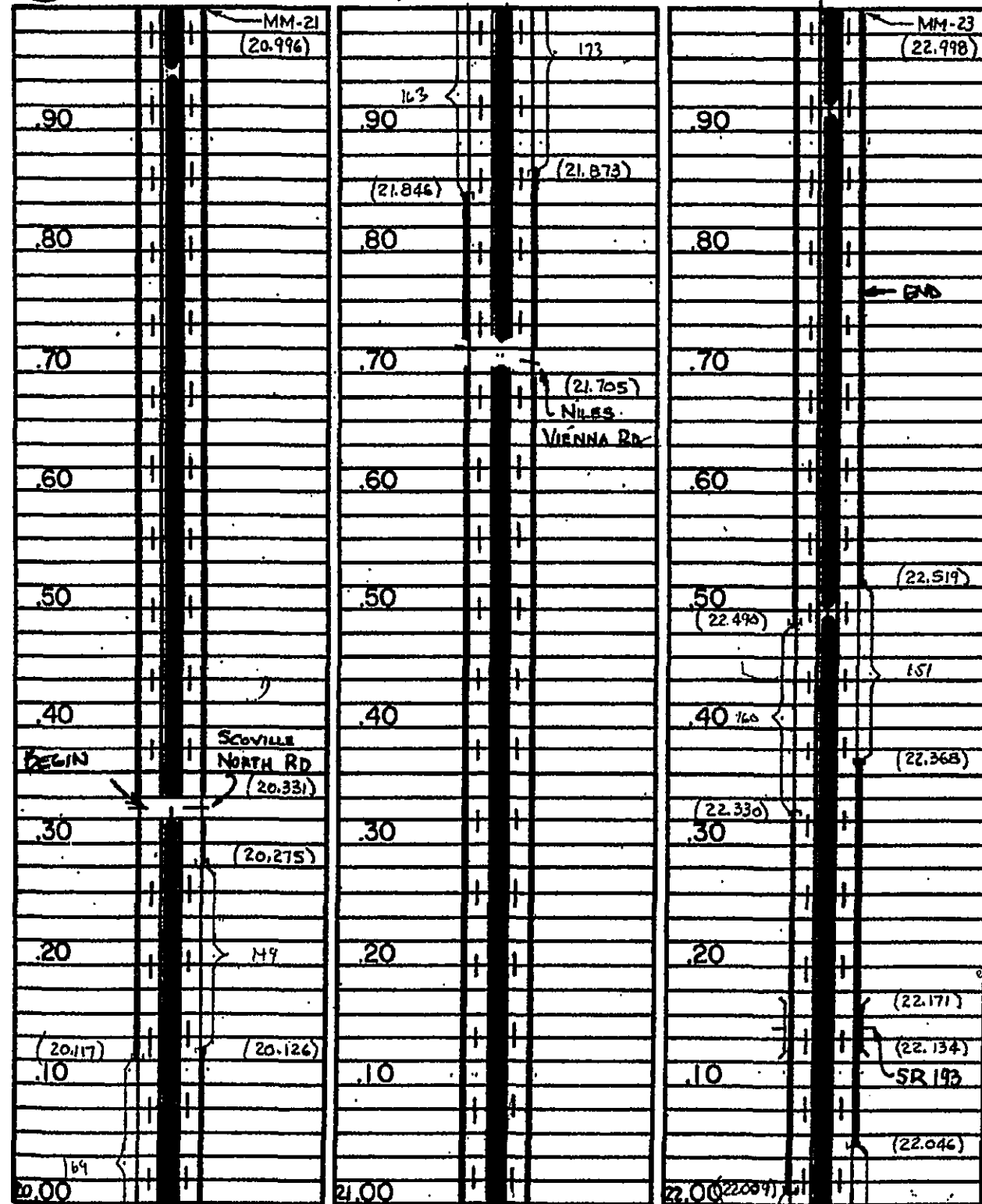
[illegible]

RAISED PAVEMENT MARKER SUB-SUMMARY

TRU-82-17.49

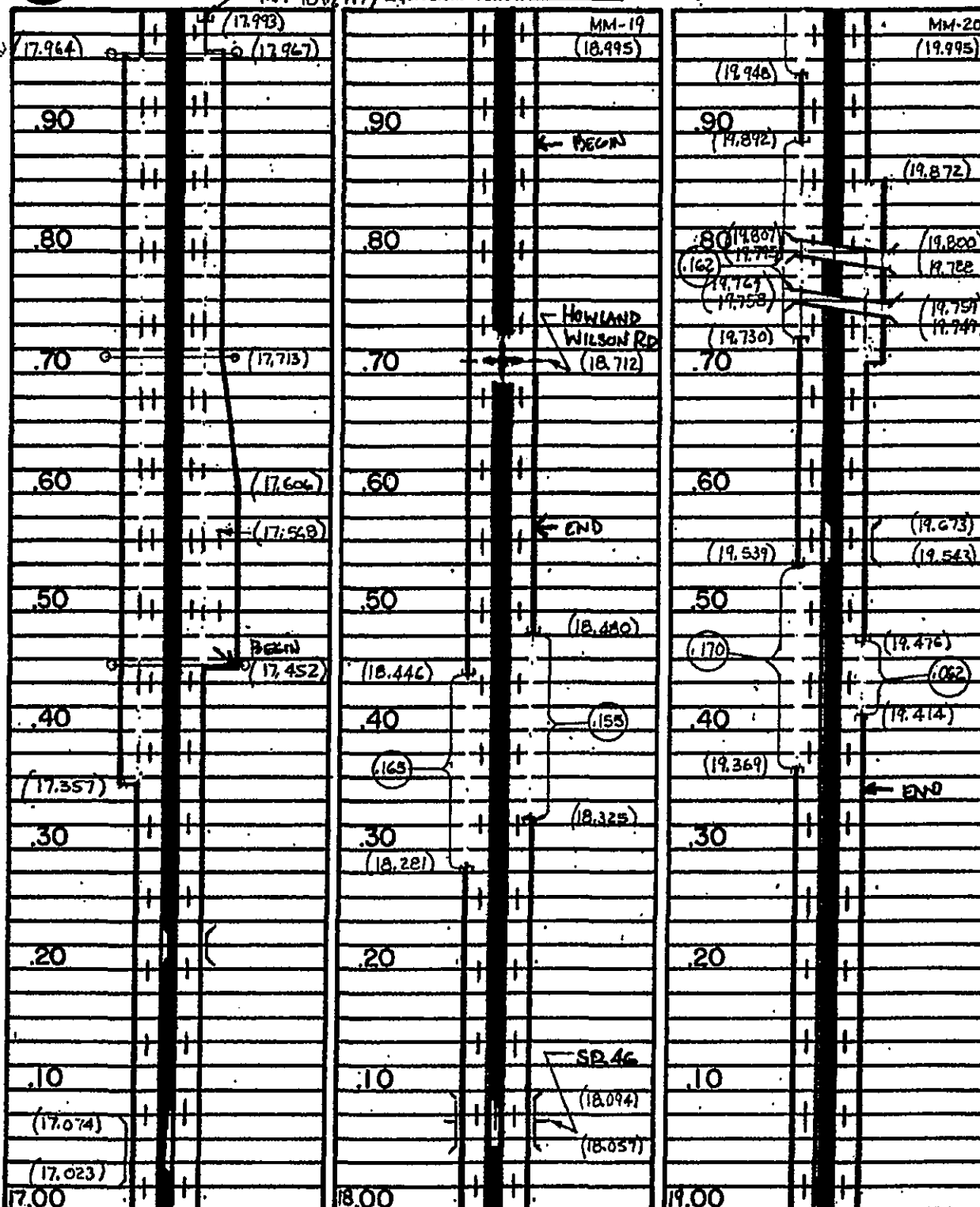


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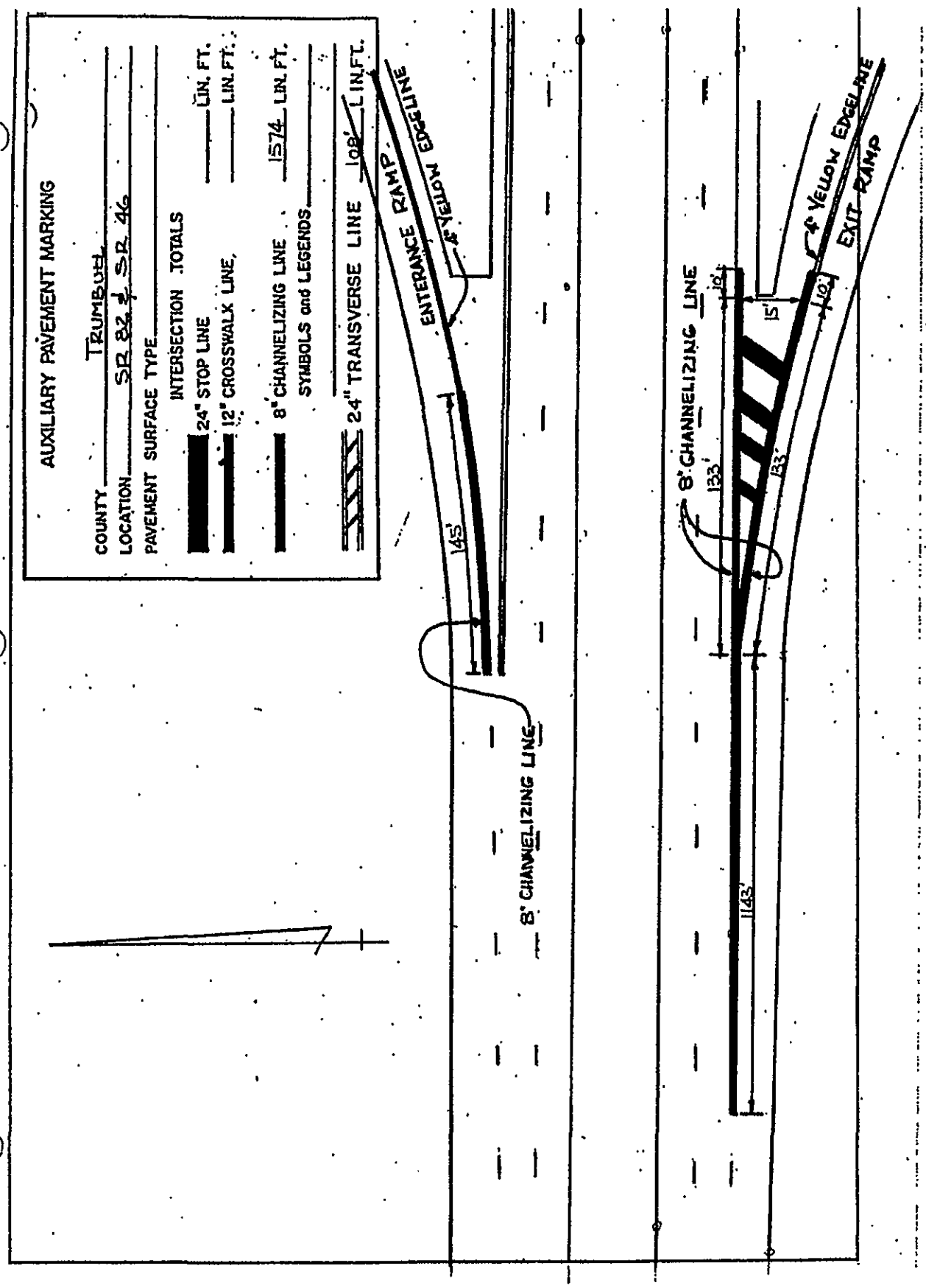
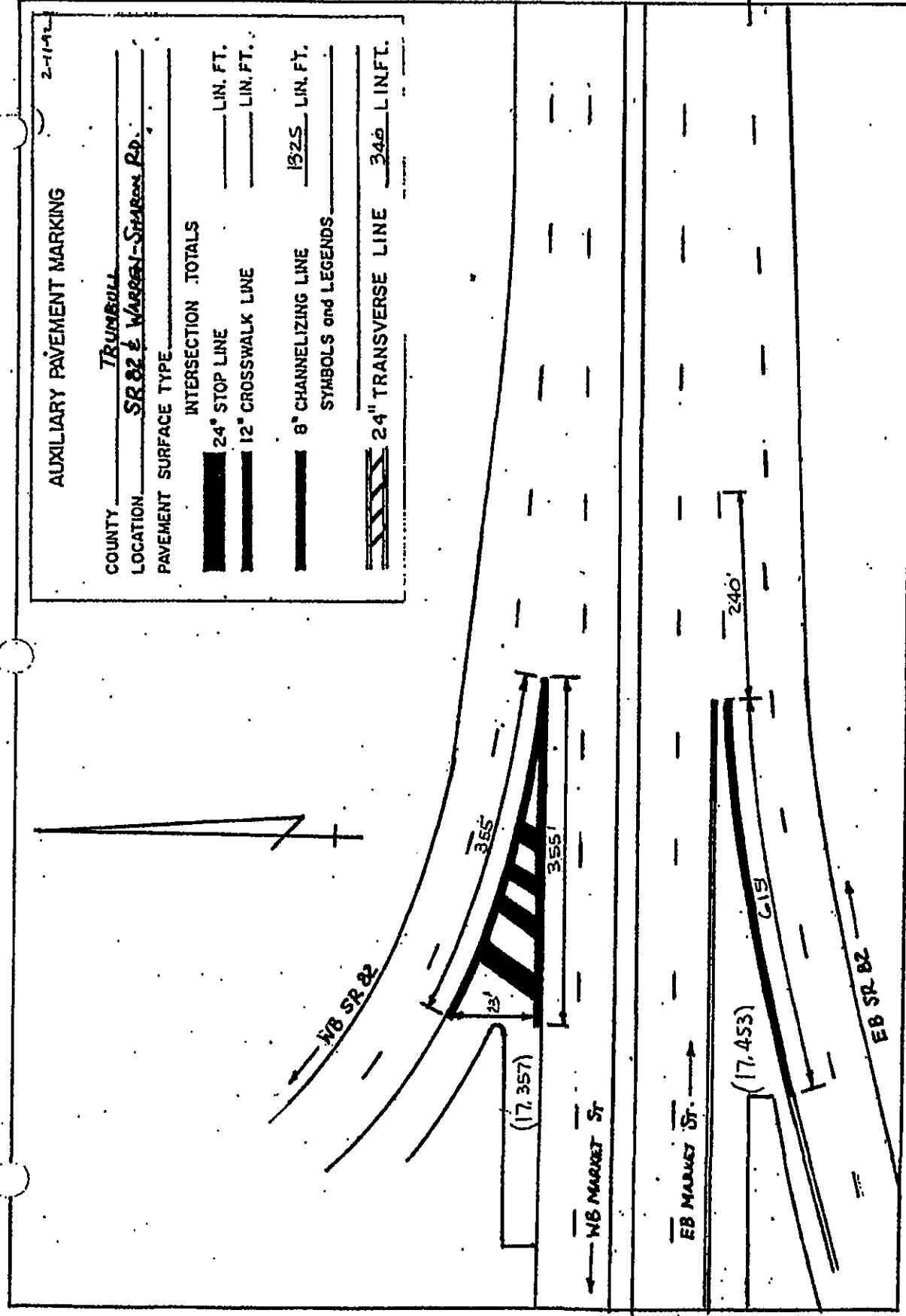
Yellow: Solid _____ Dash _____ Yellow: Solid _____ Dash _____ Yellow: Solid _____ Dash _____

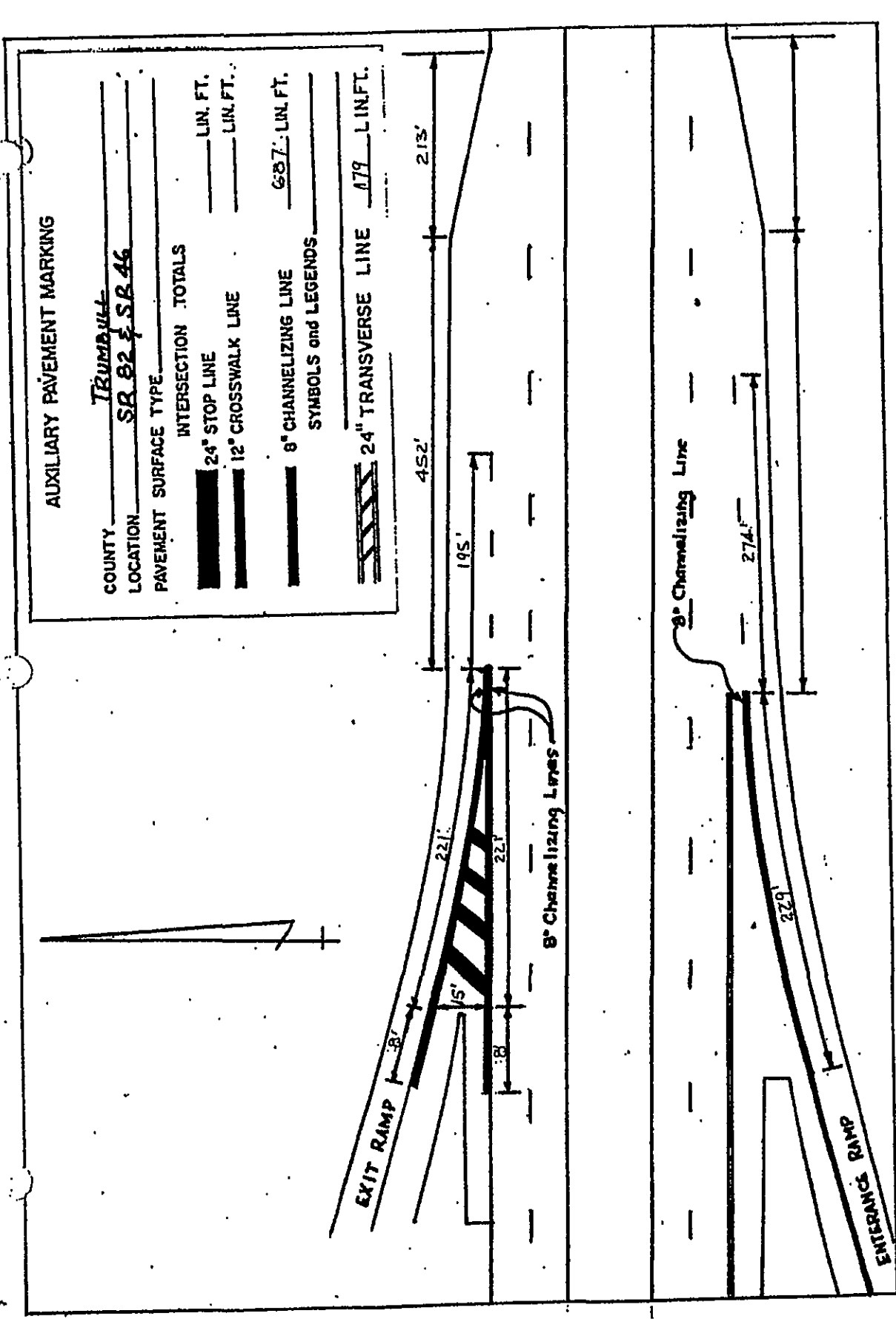
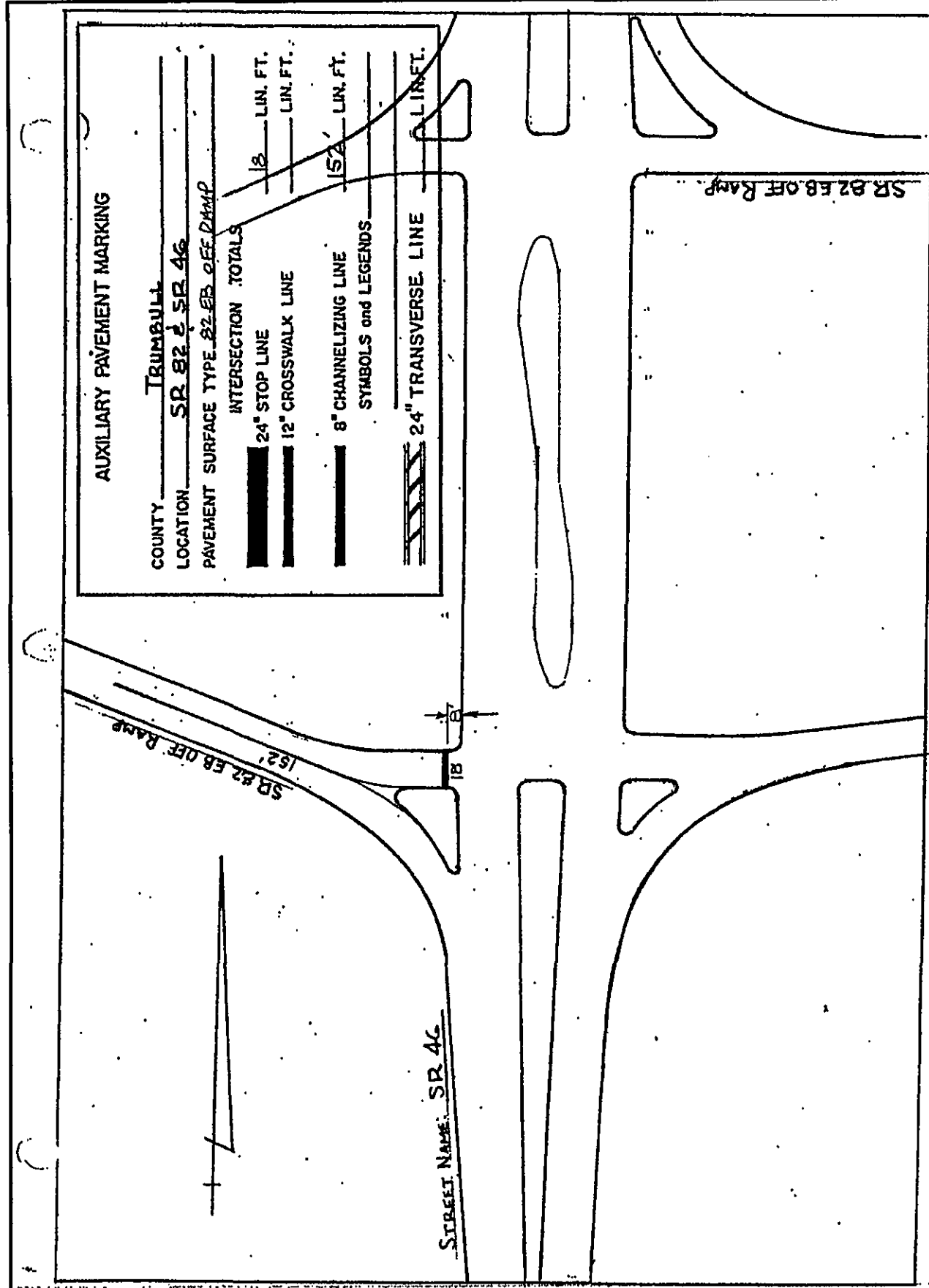
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Yellow: Solid _____ Dash _____ Yellow: Solid _____ Dash _____ Yellow: Solid _____ Dash _____

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AUXILIARY PAVEMENT MARKING
TAPERED

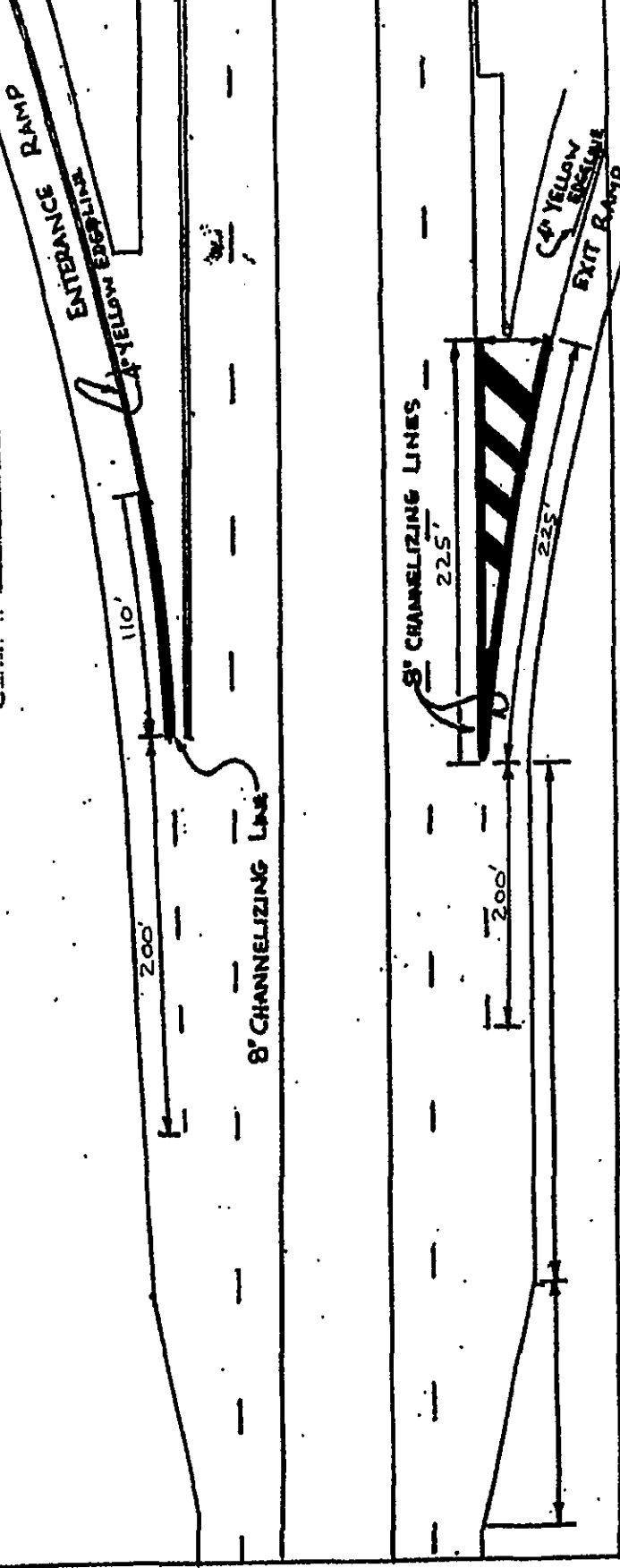
COUNTY Trumbull
LOCATION SR 82 & SR 193

PAVEMENT SURFACE TYPE _____

INTERSECTION TOTALS
 24" STOP LINE _____ LIN. FT.
 12" CROSSWALK LINE _____ LIN. FT.

8" CHANNELIZING LINE _____ 560 LIN. FT.
 SYMBOLS and LEGENDS _____

24" TRANSVERSE LINE _____ 82 LIN. FT.



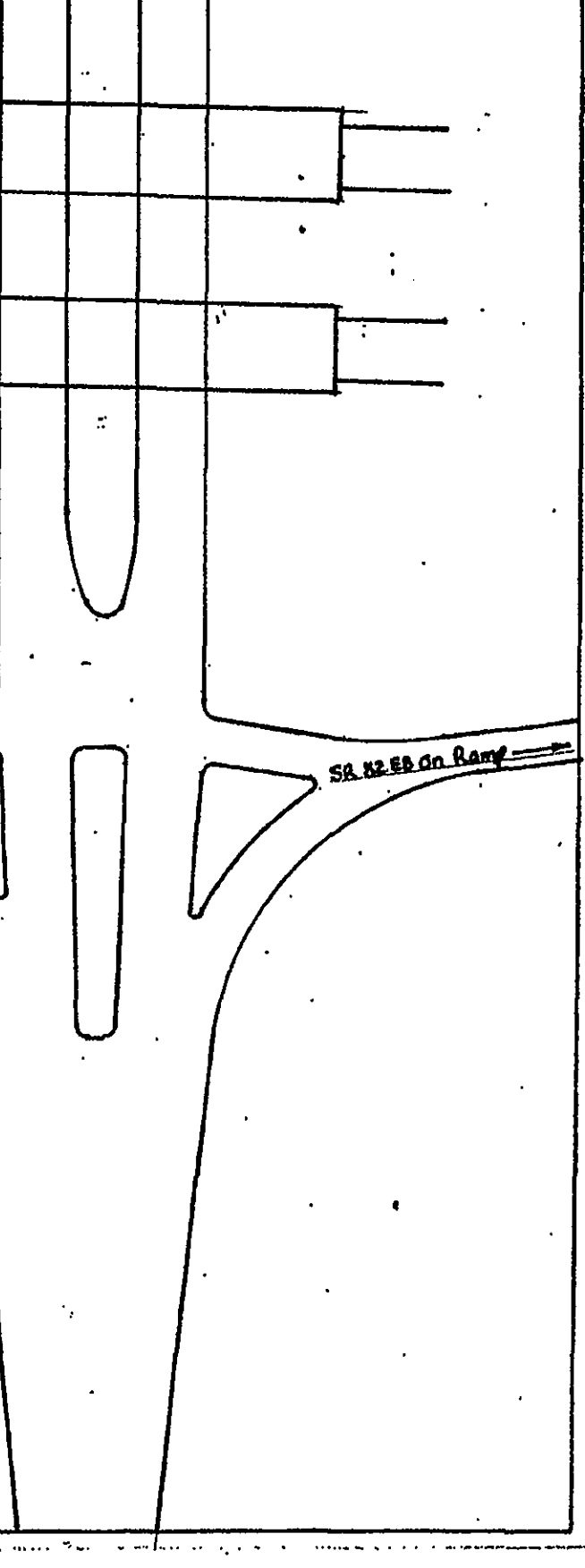
AUXILIARY PAVEMENT MARKING
5-6-72

COUNTY Trumbull
LOCATION SR 82 EB Ramps & SR 193

PAVEMENT SURFACE TYPE _____

INTERSECTION TOTALS
 24" STOP LINE _____ 20 LIN. FT.
 12" CROSSWALK LINE _____ LIN. FT.

8" CHANNELIZING LINE _____ LIN. FT.
 SYMBOLS and LEGENDS _____



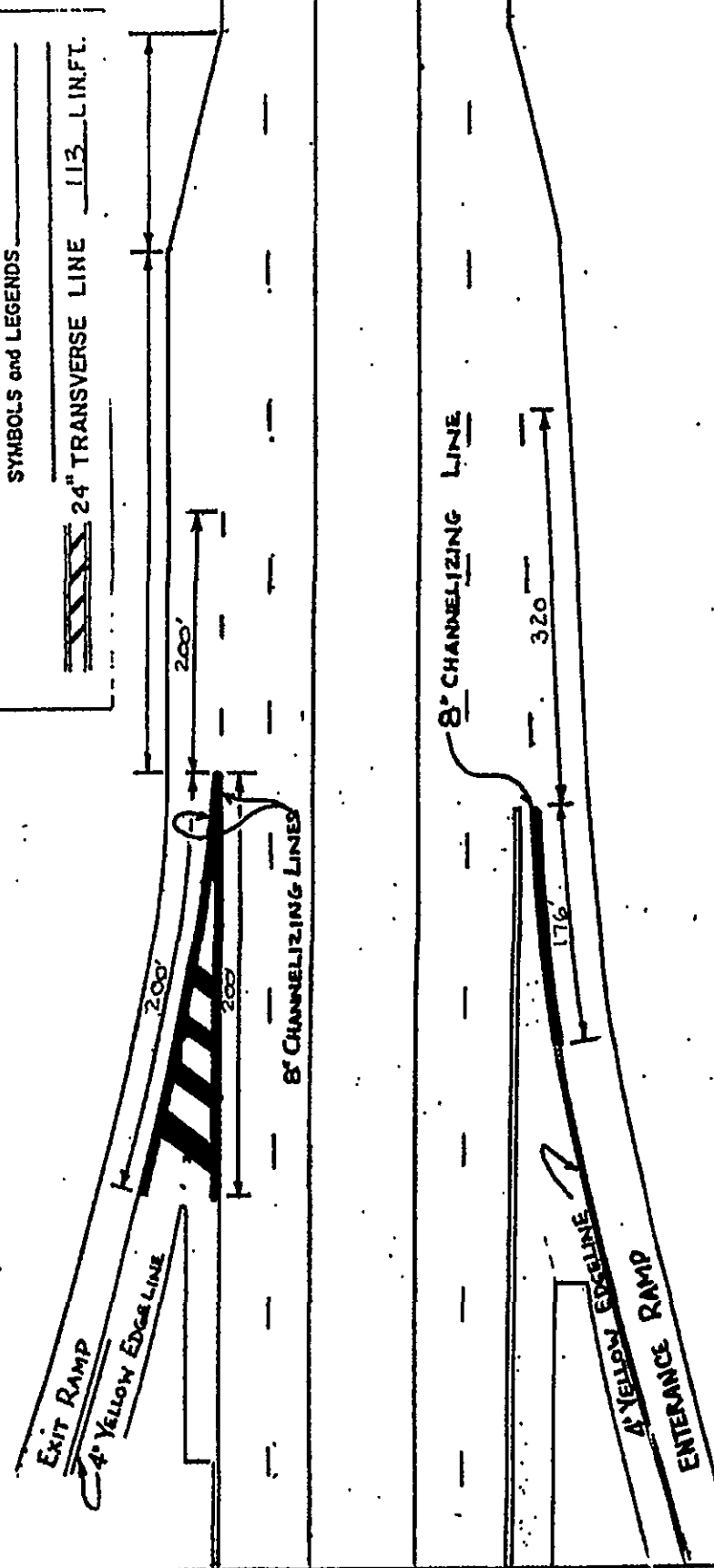
AUXILIARY PAVEMENT MARKING 5-8-92
TAPERED

COUNTY Trumbull
LOCATION SR 82 & SR 193
PAVEMENT SURFACE TYPE _____

INTERSECTION TOTALS
24" STOP LINE _____ LIN. FT.
12" CROSSWALK LINE _____ LIN. FT.

8" CHANNELIZING LINE _____ 576 LIN. FT.
SYMBOLS and LEGENDS _____

24" TRANSVERSE LINE _____ 113 LIN. FT.



AUXILIARY PAVEMENT MARKING 5-6-92

COUNTY Trumbull
LOCATION SR 82 WB Ramps & SR 193
PAVEMENT SURFACE TYPE _____

INTERSECTION TOTALS
24" STOP LINE _____ 18 LIN. FT.
12" CROSSWALK LINE _____ LIN. FT.

8" CHANNELIZING LINE _____ LIN. FT.
SYMBOLS and LEGENDS _____

