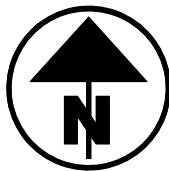


LOCATION MAP

LATITUDE: 41°8'55" LONGITUDE: -80°39'55"



PORTION TO BE IMPROVED	
INTERSTATE HIGHWAY	
FEDERAL ROUTES	
STATE ROUTES	
COUNTY & TOWNSHIP ROADS	
OTHER ROADS	

DESIGN DESIGNATION

CURRENT ADT (2024)	17,443
DESIGN FUNCTIONAL CLASSIFICATION: SR 193 (SLM 0.00 - 1.97) - URBAN MINOR ARTERIAL	
NHS PROJECT	NO

DESIGN EXCEPTIONS

NONE REQUIRED

ADA DESIGN WAIVERS

REQUIRED

UNDERGROUND UTILITIES

Contact Two Working Days
Before You Dig

Before You Dig

OHIO 811, 8-1-1, or 1-800-362-2764
(Non members must be called directly)

PLAN PREPARED BY:
ODOT DISTRICT 4 - CAPITAL PROGRAMS
2088 S. ARLINGTON RD
AKRON, OH 44306

STATE OF OHIO

DEPARTMENT OF TRANSPORTATION

TRU-193-0.00

LIBERTY TOWNSHIP

TRUMBULL COUNTY

INDEX OF SHEETS:

TITLE SHEET	P.1
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GENERAL NOTES	P.3
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PAVEMENT CALCULATIONS	P.7
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CURB RAMP DIMENSIONS AND DETAILS	P.9-11
RAISED PAVEMENT MARKERS	P.12
PAVEMENT MARKINGS SUBSUMMARY	P.13

FEDERAL PROJECT NUMBER

E240800

RAILROAD INVOLVEMENT

NONE

PROJECT DESCRIPTION

RESURFACING 1.97 MILES OF TRU SR 193

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA:	0.01 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA:	0.25 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA:	N/A (NOI NOT REQUIRED)* *ROUTINE MAINTENANCE PROJECT

2023 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS, CHANGES LISTED IN THE PROPOSAL, AND THE SUPPLEMENTAL SPECIFICATION 800 VERSION INDICATED ON THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

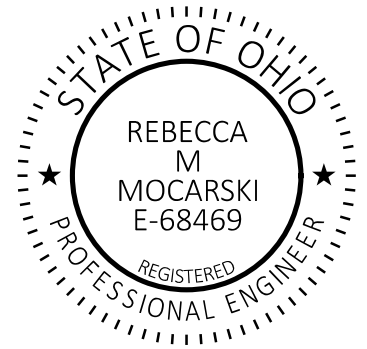
I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

Arthur G. Noiro Jr., P.E.
District 04 Deputy Director

Pamela Boratyn
Director, Department of Transportation

STANDARD CONSTRUCTION DRAWINGS						SUPPLEMENTAL SPECIFICATIONS	SPECIAL PROVISIONS
BP-3.1	1/19/24	MT-110.10	7/19/13			800-2023 1/17/25	
BP-3.2	1/18/19					821 4/20/12	
BP-7.1	1/17/25	TC-41.20	10/18/13			832 7/19/24	
BP-8.1	7/19/24	TC-42.10	10/18/13			897 1/16/15	
		TC-42.20	10/18/13			921 7/19/24	
CB-1	7/19/24	TC-52.10	10/18/13				
		TC-52.20	1/15/21				
DM-1.1	1/17/25	TC-61.30	7/19/24				
DM-4.3	1/15/16	TC-65.10	1/17/14				
DM-4.4	1/15/16	TC-65.11	1/17/25				
		TC-71.10	4/21/23				
MT-95.31	7/19/19						
MT-95.32	4/19/19						
MT-99.20	4/19/19						
MT-101.90	7/17/20						
MT-105.10	1/17/20						

ENGINEER'S SEAL



DESIGN AGENCY



DESIGNER

CAM

REVIEWER

RMM 2/19/25

PROJECT ID

119100

SHEET

TOTAL

P.1

13

LEGEND

- A

EXISTING ASPHALT
- B

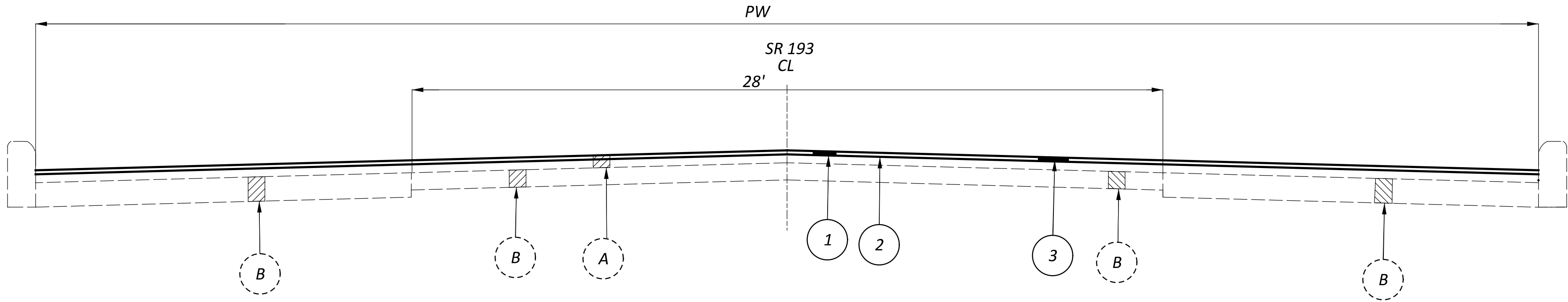
EXISTING CONCRETE BASE
- C

EXISTING CONCRETE ISLAND
- 1

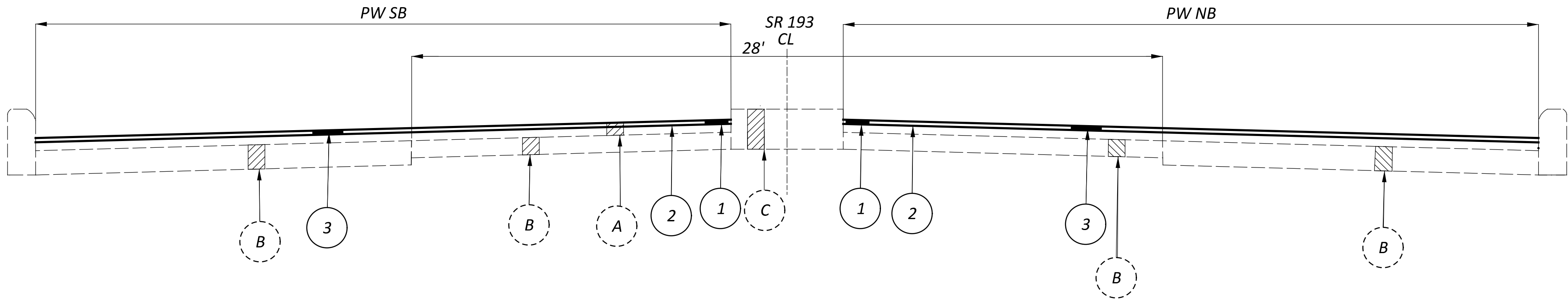
ITEM 897, PAVEMENT PLANNING, ASPHALT CONCRETE, CLASS A (T=1 1/4")
- 2

ITEM 407, NON-TRACKING TACK COAT @ 0.09 GAL/SY
- 3

ITEM 441, ASPHALT CONCRETE SURFACE COURSE, TYPE 1 (448), AS PER PLAN (PG70-22M) (T=1 1/4")



TYPICAL SECTETION 1			
SLM		PW (FT)	LENGTH (MILES)
FROM	TO		
0.00	0.83	56	0.83
0.83	0.92	69	0.09
0.92	0.97	56	0.05
0.97	1.03	69	0.06
1.03	1.36	56	0.33
1.73	1.78	56	0.05
1.78	1.9	68	0.12
1.9	1.97	80	0.07



TYPICAL SECTETION 2				
SLM		PW NB(FT)	PW SB(FT)	LENGTH (MILES)
FROM	TO			
1.36	1.53	24	32	0.17
1.53	1.68	50	29	0.15
1.68	1.73	22	33	0.05



UTILITIES

THE CONTRACTOR SHALL USE THE FOLLOWING PROCEDURE AT EACH LOCATION WHERE WORK IS PERFORMED, IN ACCORDANCE WITH SECTIONS 105.07 AND 107.16 IN THE CONSTRUCTION AND MATERIALS SPECIFICATIONS.

THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER, OHIO811, THE OHIO DEPARTMENT OF TRANSPORTATION DISTRICT 4 HEADQUARTERS (MICHELLE CHANEY AT 330-786-2267) AND ALL NON REGISTERED UTILITY OWNERS AT LEAST TWO (2) WORKING DAYS PRIOR TO COMMENCING CONSTRUCTION OPERATIONS IN ALL AREAS.

THE LOCATION OF EXISTING UNDERGROUND UTILITIES ARE NOT SHOWN ON THE PLANS, BUT CAN BE OBTAINED FROM THE OWNERS OF THE UTILITIES. THE CONTRACTOR IS RESPONSIBLE FOR ANY DAMAGE TO UTILITIES.

PAVEMENT MARKING DETAILS

THE PAVEMENT MARKING DETAIL SHEETS HAVE BEEN SUPPLIED AS REFERENCE DOCUMENTS FOR THIS PROJECT AND ARE AVAILABLE ON THE ODOT FTP SITE AT <https://ftp.dot.state.oh.us/pub/contracts/Attach/> FOR THIS PROJECT. FOR ANY LOCATIONS THAT PAVEMENT MARKING DETAILS HAVE NOT BEEN MADE AVAILABLE TO THE CONTRACTOR, IT WILL BE THE CONTRACTORS RESPONSIBILITY TO PUT BACK NEW PAVEMENT MARKINGS IN THE ORIGINAL LOCATIONS.

PROFILE AND ALIGNMENT

PLACE THE PROPOSED PAVEMENT TO FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT. PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY (AS SHOWN ON THE TYPICAL SECTIONS).

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

ADA DESIGN WAIVER

AN APPROVED ADA DESIGN WAIVER IS REQUIRED ON THIS PROJECT. THE FOLLOWING CURB RAMPS LISTED BELOW HAVE WAIVERS APPROVED.

RAMP ID	APPROVAL DATE	LOCATION/DESCRIPTION
RMP 766	05/16/2025	TRUMBULL AVE / RAMP G
RMP 767	05/16/2025	TRUMBULL AVE / RAMP B
RMP 768	05/16/2025	TRUMBULL AVE / RAMP F
RMP 772	05/16/2025	COLONIAL DR / RAMP G
RMP 775	05/16/2025	TRUMBULL AVE / RAMP C
RMP 776	05/16/2025	TRUMBULL AVE / RAMP A
RMP 777	05/16/2025	TRUMBULL AVE / RAMP H
RMP 778	05/16/2025	EUCLID BLVD / RAMP C
RMP 783	05/16/2025	GOLDIE RD / RAMP B
RMP 4219	05/16/2025	BELGRADE AVE / RAMP D
RMP 4227	05/16/2025	AMHURST / RAMP D
RMP 29598	05/16/2025	CHURCHILL PARK / RAMP D

INTERSECTIONS

INTERSECTIONS WILL BE RESURFACED 10 FT. BEYOND THE EDGE LINE, UNLESS OTHERWISE DIRECTED BY THE ENGINEER OR INDICATED IN THE PLAN. INTERSECTIONS SHALL BE PAVED AFTER COMPLETION OF THE SURFACE COURSE. A BUTT JOINT, AS PER STANDARD CONSTRUCTION DRAWING BP-3.1, SHALL BE USED TO PROVIDE A SMOOTH TRANSITION TO THE EXISTING PAVEMENT. USE THE SAME ASPHALT CONCRETE AS THE MAINLINE PAVEMENT UNLESS SHOWN OTHERWISE ON THE ASPHALT CONCRETE CALCULATIONS SHEET. ANY GRADING OR PRIME NECESSARY TO ACCOMPLISH THIS WORK SHALL BE INCLUDED IN THE COST OF THE PERTINENT BID ITEM.

CURB RAMPS / DETECTABLE WARNINGS

UNLESS OTHERWISE DIRECTED BY THE ENGINEER, INSTALLATION OF THE CURB RAMPS / DETECTABLE WARNINGS WILL BE PERFORMED PRIOR TO MAINLINE RESURFACING.

ITEM SPECIAL - AS-BUILT CONSTRUCTION PLANS

PRIOR TO FINAL ACCEPTANCE OF THE WORK, THE CONTRACTOR SHALL FURNISH THE DEPARTMENT FORMAL AS-BUILT CONSTRUCTION PLANS. THE FORMAL AS-BUILT CONSTRUCTION PLANS SHALL INCLUDE ALL RED-LINED CHANGES. RED-LINE CHANGE SHALL BE DENOTED UTILIZING CLOUDING IN MICROSTATION (OR OTHER CAD SOFTWARE) OR CLOUDING IN PDF EDITING SOFTWARE. THE AS-BUILT CONSTRUCTION PLANS SHALL HAVE A SIGNED VERIFICATION ON THE TITLE SHEET FROM THE CONTRACTOR INDICATING THAT ALL RED-LINED AND FIELD CHANGES HAVE BEEN INCORPORATED INTO AS-BUILT CONSTRUCTION PLANS.

THE CONTRACTORS VERIFICATION STATEMENT INDICATES ALL KNOWN FIELD MODIFICATIONS MADE HAVE BEEN INCLUDED IN THE FORMAL AS-BUILT CONSTRUCTION PLANS. THE CONTRACTORS VERIFICATION STATEMENT SHALL BE SIGNED BY THE CONTRACTORS PROJECT MANAGER (OR ACCEPTABLE REPRESENTATIVE).

IN ADDITION TO THE INFORMATION SHOWN ON THE CONSTRUCTION PLANS, THE AS-BUILT CONSTRUCTION PLANS SHALL SHOW THE FOLLOWING:

1. ALL DEVIATIONS FROM THE ORIGINAL APPROVED CONSTRUCTION PLANS WHICH RESULT IN A CHANGE OF LOCATION, MATERIAL, TYPE OR SIZE OF WORK.
2. ANY UTILITIES, PIPES, WELLHEADS, ABANDONED PAVEMENTS, FOUNDATIONS OR OTHER MAJOR OBSTRUCTIONS DISCOVERED AND REMAINING IN PLACE WHICH ARE NOT SHOWN, OR DO NOT CONFORM TO LOCATIONS OR DEPTHS SHOWN IN THE PLANS. UNDERGROUND FEATURES SHALL BE SHOWN AND LABELED ON THE AS-BUILT CONSTRUCTION PLANS IN TERMS OF STATION, OFFSET AND ELEVATION.
3. THE FINAL OPTION AND SPECIFICATION NUMBER SELECTED FOR THOSE ITEMS WHICH ALLOW SEVERAL MATERIAL OPTIONS UNDER THE SPECIFICATION (E.G., CONDUIT).
4. CHANGES TO THE PAY ITEMS AND FINAL QUANTITIES AS PAID SHALL BE SHOWN ON THE GENERAL SUMMARY AND SUBSUMMARIES.
5. ADDITIONAL PLAN SHEETS MAY BE NEEDED IF NECESSARY TO SHOW WORK NOT INCLUDED IN THE CONSTRUCTION PLANS. IF ADDITIONAL PLAN SHEETS ARE NEEDED, THEY ARE REQUIRED TO BE PREPARED IN CONFORMANCE WITH THE LOCATION AND DESIGN MANUAL, VOLUME 3, SECTION 1200 - PLAN PREPARATION.

NOTATION SHALL ALSO BE MADE OF LOCATIONS AND THE EXTENT OF USE OF MATERIALS, OTHER THAN SOIL, FOR EMBANKMENT CONSTRUCTION (ROCK, BROKEN CONCRETE WITHOUT REINFORCING STEEL, ETC.).

THE PLAN INDEX SHALL SHOW THE PLAN SHEETS WHICH HAVE CHANGES APPEARING ON THEM.

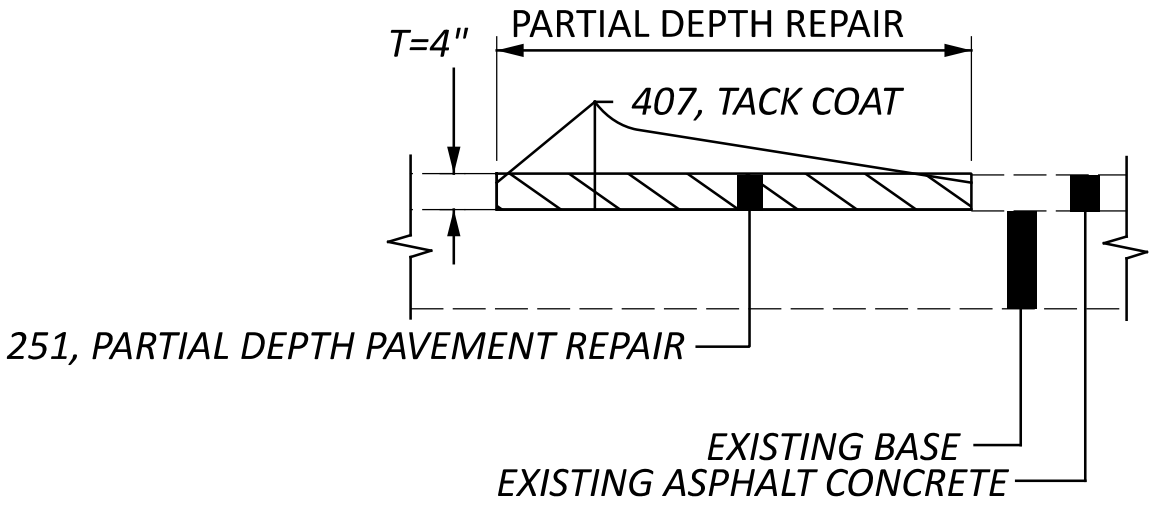
TWO COPIES OF THE AS-BUILT CONSTRUCTION PLANS SHALL BE DELIVERED TO THE PROJECT ENGINEER FOR APPROVAL UPON COMPLETION OF THE PHYSICAL WORK BUT PRIOR TO THE REQUEST FOR FINAL PAYMENT. AFTER THE DEPARTMENT HAS APPROVED THE AS-BUILT CONSTRUCTION PLANS, THE ASSOCIATED ELECTRONIC FILES SHALL BE DELIVERED TO THE DISTRICT CAPITAL PROGRAMS ADMINISTRATOR. ACCEPTANCE OF THESE PLANS AND DELIVERY OF THE ASSOCIATED ELECTRONIC FILES IS REQUIRED PRIOR TO THE WORK BEING ACCEPTED AND THE FINAL ESTIMATE APPROVED.

PAYMENT FOR ALL THE ABOVE SHALL BE LUMP SUM UPON PROPER EXECUTION OF ALL WORK OF THIS ITEM AS DETERMINED BY THE PROJECT ENGINEER.

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (441)

A QUANTITY OF THIS ITEM SHALL BE PROVIDED FOR USE AS DIRECTED BY THE ENGINEER. THE ITEM SHALL CONSIST OF REPAIRING EXISTING LOCATIONS EXHIBITING SURFACE DETERIORATION AND PLACING ITEM 441 ASPHALT CONCRETE, TYPE 2. IT IS NOT THE INTENT TO REPAIR EVERY DETERIORATED AREA WITHIN THE PROJECT. PAVEMENT REPAIRS WILL BE MARKED IN THE FIELD BY THE PROJECT ENGINEER ACCORDING TO CMS 251.02. MINIMUM WIDTH IS 2'. UNLESS OTHERWISE DIRECTED BY THE ENGINEER, THIS ITEM SHALL BE PERFORMED BEFORE THE COMPLETION OF MAINLINE PAVEMENT PLANNING. PAYMENT SHALL BE BASED ON THE ACTUAL NUMBER OF SQUARE YARDS OF PAVEMENT REPAIR. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

251, PARTIAL DEPTH PAVEMENT REPAIR (441), 600 SQ. YD.



ITEM 608 CURB RAMP, AS PER PLAN

UNLESS OTHERWISE DIRECTED BY THE ENGINEER, INSTALLATION OF THE CURB RAMPS / DETECTABLE WARNINGS WILL BE PERFORMED PRIOR TO MAINLINE RESURFACING.

IN ADDITION TO THE CMS REQUIREMENTS OF ITEM 608 CURB RAMP, THIS ITEM SHALL INCLUDE THE RESTORATION OF THE ADJACENT AREAS DISTURBED FOR THE INSTALLATION OF CURB RAMPS AND IMMEDIATELY ADJACENT CONCRETE WALK. RESTORATION SHALL INCLUDE PLACEMENT OF ITEM 659 TOPSOIL, ITEM 659 COMMERCIAL FERTILIZER, ITEM 659 SEEDING AND MULCHING, AND ITEM 659 WATER, ALL PER CMS.

PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO PERFORM THE WORK DESCRIBED ABOVE SHALL BE INCLUDED IN THE UNIT PID PRICE FOR ITEM 608 CURB RAMP, AS PER PLAN.

ITEM 611 – MANHOLE ADJUSTED TO GRADE, AS PER PLAN
ITEM 623 – MONUMENT ASSEMBLYADJUSTEDASSEMBLY ADJUSTED TO GRADE, AS PER PLAN

IN ADDITION TO THE REQUIREMENTS OF CMS 611.10.D FOR MANHOLES, 623.05 FOR MONUMENT ASSEMBLY, OR 638.18 FOR VALVE BOXES, THE CONTRACTOR WILL MAKE A CLEAN CIRCULAR CUT AROUND THE CASTING (48" DIAMETER FOR STORM AND SANITARY MANHOLE CASTINGS, 24"-28" FOR VALVE BOXES AND MONUMENT ASSEMBLIES, AND 2' IN DIAMETER LARGER THAN THE CASTING DIAMETER FOR ANY CASTINGS THAT ARE LARGER THAN STANDARD MANHOLES SUCH AS TELECOMMUNICATION MANHOLE CASTINGS) AND REMOVE AND DISCARD THE EXISTING CASTING. INSTALL A NEW CASTING TO GRADE (ACCORDING TO TOLERANCES AS SHOWN ON STANDARD CONSTRUCTION DRAWING BP-3.1) AFTER THE PAVEMENT SURFACE COURSE HAS BEEN REPLACED.

CMS 499 CLASS QCMS CONCRETE (DYE THE CONCRETE SUCH THAT ITS COLOR CLOSELY MATCHES THE COLOR OF THE SURROUNDING PAVEMENT) WILL BE USED FOR BACKFILLING THE FULL PAVEMENT SECTION AND THE JOINT BETWEEN THE ASPHALT AND CONCRETE WILL BE SEALED WITH CMS 702.01 PG BINDER. EPOXY COATED REBAR SHALL BE PLACED IN THE CONCRETE AT 6" MAXIMUM ON CENTER AND A MINIMUM OF 3.5" CLEARANCE FROM THE TOP, BOTTOM AND SIDES. THE CONCRETE WILL BE VIBRATED SUFFICIENTLY TO ELIMINATE AIR POCKETS UNDER THE FRAME.

PAYMENT WILL INCLUDE REMOVAL OF THE EXISTING MATERIAL, INSTALLATION AND FURNISHING OF A NEW CASTING, AND ALL LABOR AND MATERIALS REQUIRED TO COMPLETE THIS ITEM OF WORK AS DESCRIBED.

THE FOLLOWING HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 611 – MANHOLE ADJUSTED TO GRADE, AS PER PLAN 17 EACH
ITEM 623 – MONUMENT ASSEMBLY ADJUSTED TO GRADE, AS PER PLAN 7 EACH

REVIEW OF CURB RAMPS

PRIOR TO THE START OF WORK, PERFORM AN INSPECTION WITH REPRESENTATIVES OF THE DEPARTMENT AND CONTRACTOR OF ALL CURB RAMPS INCLUDED IN THE PROJECT LIMITS. PRIOR TO THE INSPECTION, THE CONTRACTOR SHALL MARK THE WORK LIMITS OF ALL CURB RAMPS AND ASSOCIATED WORK TO IDENTIFY ANY CONDITIONS THE CONTRACTOR CANNOT BRING INTO ADA COMPLIANCE PER BP-7.1. THE DEPARTMENT SHALL HAVE TEN (10) BUSINESS DAYS TO RESOLVE ANY SUCH ANTICIPATED NON-COMPLIANCE WITH AN ADA WAIVER, QUANTITY CHANGE OR DESIGN REVISION PRIOR TO THE START OF WORK.

IMMEDIATELY FOLLOWING INSTALLATION OF FORMWORK FOR CURB RAMP CONCRETE PLACEMENT AND PRIOR TO PLACEMENT OF CONCRETE, THE CONTRACTOR SHALL VERIFY ADA COMPLIANCE BASED UPON MEASUREMENT OF THE FORMS. THE ENGINEER SHALL BE IMMEDIATELY NOTIFIED OF ANY NON-COMPLIANT ADA CONDITIONS MEASURED BY THE CONTRACTOR WITHIN THE WORK LIMITS. THE ENGINEER SHALL HAVE THREE (3) BUSINESS DAYS TO RESOLVE ANY SUCH NON-COMPLIANCE WITH A CORRECTIVE FORM LAYOUT, ADA WAIVER, QUANTITY CHANGE OR DESIGN CHANGE PRIOR TO THE PLACEMENT OF CONCRETE.

THE CONTRACTOR IS RESPONSIBLE TO CORRECT ANY ADA NON-COMPLIANCE FOR ALL ISSUES NOT REPORTED TO THE ENGINEER IN THE INITIAL INSPECTION AND/OR THE FORM INSTALLATION MEASUREMENT. MODIFICATION OF FORMS INTO A COMPLIANT CONFIGURATION AND/OR REMOVAL AND RECONSTRUCTION OF FINISHED WORK SHALL BE AT THE EXPENSE OF THE CONTRACTOR.

PAYMENT FOR ALL OPERATIONS DESCRIBED ABOVE IS INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 608 CURB RAMP ITEMS.

DESIGN AGENCY



DESIGNER
CAM

REVIEWER
RMM 2/19/25

PROJECT ID
119100

SHEET	TOTAL
P.3	11

MAINTENANCE OF TRAFFIC

THIS ITEM SHALL CONSIST OF MAINTENANCE OF TRAFFIC ON EXISTING ROADWAYS AND RAMPS IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES FOR STREETS AND HIGHWAYS, CURRENT EDITION, LATEST REVISION, THE SPECIFICATIONS AND THE FOLLOWING:

1. A MINIMUM OF ONE TEN FOOT LANE IN EACH DIRECTION SHALL BE MAINTAINED ON THE EXISTING PAVEMENT OR COMPLETED PAVEMENT DURING CONSTRUCTION OF THE WORK.
2. THE CONTRACTOR SHALL INFORM THE DISTRICT OFFICE (330) 786-2208, EIGHTEEN (18) DAYS PRIOR TO THE BEGINNING OF WORK.
3. LANE RESTRICTIONS OR LANE REDUCTIONS SHALL NOT BE PERMITTED AFTER NORMAL WORKING HOURS. NORMAL WORKING HOURS SHALL BE THOSE HOURS DURING WHICH THE CONTRACTOR HAS A FULL COMPLEMENT OF EMPLOYEES AND EQUIPMENT ACTIVELY REMOVING AND/OR PLACING PAVEMENT MATERIALS.
4. ALL FULL DEPTH PAVEMENT REMOVAL AND REPLACEMENT OPERATIONS SHALL BE COMPLETED THE SAME DAY THE EXCAVATION IS MADE. IF THE CONTRACTOR CANNOT COMPLETE THE WORK, THE EXCAVATION SHALL BE BACKFILLED OR PROTECTED AS PER STANDARD CONSTRUCTION DRAWING MT-101.90.
5. TRUCK MOUNTED ATTENUATORS [TMA'S] SHALL BE USED AS SHOWN IN THE STANDARD CONSTRUCTION DRAWINGS.
6. UNDER NO CIRCUMSTANCES SHALL THE CONTRACTOR BE PERMITTED TO HAVE SUCCESSIVE WORK ZONES UNLESS THE DISTANCE BETWEEN THE DRUMS, BARRICADES OR CONES EXCEEDS TWO (2) MILES RURAL OR ONE [1] MILE URBAN.
7. FOR ROUTES NOT ON THE PERMITTED LANE CLOSURE CHART, ONLY DURING OFF-PEAK PERIODS (ie ANY PERIOD OTHER THAN 6-8AM AND 3-6PM) SHALL THE CONTRACTOR INSTALL AND SUBSEQUENTLY RESET ALL TRAFFIC CONTROL NECESSARY FOR THE WORK ZONE FOR EACH CONSTRUCTION PHASE.
8. IN ADDITION TO THE REQUIREMENTS OF 614.11 WORK ZONE PAVEMENT MARKINGS, AT THE END OF EACH DAY OF WORK, THE CONTRACTOR SHALL REPLACE (WITH WORK ZONE MARKINGS) ALL LANE, CENTER, STOP OR CHANNELIZING LINES THAT WERE REMOVED OR COVERED DURING THE PAVEMENT REMOVAL OR PLACEMENT OPERATIONS. QUANTITIES FOR SUCH PLACEMENT ARE CARRIED AS PART OF THE ITEMS LISTED UNDER 614 WORK ZONE PAVEMENT MARKINGS.
9. A QUANTITY OF 10 CU. YDS. OF ITEM 614 ASPHALT CONCRETE FOR MAINTAINING TRAFFIC SHALL BE PROVIDED FOR USE IN MAINTAINING PAVEMENT, SHOULDERS AND OTHER LOCATIONS AS DIRECTED BY THE ENGINEER.
10. PRIOR TO OPENING TO TRAFFIC EACH LANE SHALL BE IN A SAFE, PASSABLE CONDITION. ALL TRANSVERSE JOINTS SHALL EXTEND ACROSS THE FULL LANE AND SHOULDER WIDTH AND EACH LANE SHALL BE FREE FROM UNEVEN LONGITUDINAL JOINTS. THE CONTRACTOR SHALL PROVIDE ASPHALT WEDGES FOR TRANSVERSE JOINTS WHEREVER THERE ARE PAVEMENT ELEVATION DIFFERENCES.
11. THE CONTRACTOR SHALL INSTALL, MAINTAIN AND SUBSEQUENTLY REMOVE WORK ZONE MARKING SIGNS AND THEIR SUPPORTS WITHIN THE WORK LIMITS. THESE SIGNS INCLUDE NO EDGE LINES, DO NOT PASS AND PASS WITH CARE". ALL OTHER SIGNS WILL BE INCIDENTAL TO THE LUMP SUM PAY ITEM 614 MAINTAINING TRAFFIC UNLESS SEPARATELY ITEMIZED IN THE PLANS. A QUANTITY OF ITEM 614 WORK ZONE MARKING SIGNS HAS BEEN INCLUDED IN THE PLANS AS PER CMS 614.04.

12. THE CONTRACTOR SHALL SET A WORK ZONE AT THE REQUEST OF THE ENGINEER TO ALLOW THE LAYOUT OF THE PARTIAL/FULL DEPTH PAVEMENT REPAIR AREAS. THIS WORK IS INCIDENTAL TO ITEM 614 MAINTAINING TRAFFIC.
- THE FOLLOWING QUANTITIES SHALL BE USED FOR THE MAINTENANCE OF TRAFFIC ON THIS PROJECT:

PHASE 1 MILLED SURFACE

614, WORK ZONE CENTER LINE, CLASS I, 3.00 MILE

614, WORK ZONE LANE LINE, CLASS I, 3.94 MILE

614, WORK ZONE STOP LINE, CLASS 1, 544 FT

614, WORK ZONE CHANNELIZING LINE, CLASS 1, 3360 FT

614, WORK ZONE MARKING SIGN,(ALL PHASES) 4 EACH

PHASE 2 SURFACE COURSE

614, WORK ZONE CENTERLINE, CLASS III, 642 PAINT, 3.00 MILE

614, WORK ZONE LANE LINE, CLASS III, 642 PAINT, 3.94 MILE

614, WORK ZONE STOP LINE, CLASS III, 642 PAINT, 544 FT

614, WORK ZONE CHANNELIZING LINE, CLASS III, 642 PAINT, 3360 FT

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

TIME LIMITATION, CURB RAMP

THE MAXIMUM ALLOWABLE TIME FOR THE CONTRACTOR TO HAVE AN INDIVIDUAL CURB RAMP AND ASSOCIATED SIDEWALK LEADING INTO THE CURB RAMP OUT OF SERVICE FOR THE REMOVAL AND REPLACEMENT SHALL BE 14 CONSECUTIVE CALENDAR DAYS (THE TIME PERIOD INCLUDES ALL WORK LISTED IN THE BASIS OF PAYMENT PER CMS 608.09, INCLUDING ALL REMOVAL, REPLACEMENT, AND BACKFILL ASSOCIATED WITH THE CURB RAMPS, AND CURING TIME PERIOD).

AT THE CONCLUSION OF CONSTRUCTING OF THE CURB RAMP AND PRIOR TO OPENING TO PEDESTRIAN TRAFFIC THE CONTRACTOR SHALL ENSURE THAT THE REQUIREMENTS OF STANDARD CONSTRUCTION DRAWING BP-7.1 ARE MET. THE CONTRACTOR SHALL USE ASPHALT AS A WEDGE, OR SUBMIT ANOTHER METHOD APPROVED BY THE ENGINEER, TO ENSURE THE TRANSITION FROM THE CURB RAMP TO THE ROADWAY ARE PER STANDARD CONSTRUCTION DRAWING BP-7.1. ALL COSTS TO PERFORM THIS WORK SHALL BE INCIDENTAL TO THE ASSOCIATED PAY ITEMS FOR THE INSTALLATION OF THE CURB RAMP.

SHOULD THE CONTRACTOR FAIL TO MEET THIS REQUIREMENT, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF \$1000 PER DAY PER AFFECTED RAMP THAT THE AFFECTED CURB RAMP REMAINS OUT OF SERVICE BEYOND 14 CONSECUTIVE CALENDAR DAYS.

ADVANCED NOTICE TO PAVE

THE CONTRACTOR SHALL SUBMIT FOR APPROVAL TO THE DISTRICT CONSTRUCTION ENGINEER A DETAILED SCHEDULE 15 DAYS PRIOR TO THE PLACEMENT OF THE OVERLAY COURSES, ON HOW THEY PROPOSE TO PROSECUTE THE PAVING OPERATIONS. THE DETAILS SHALL SHOW THE ORDER OF PERFORMANCE OF EACH STAGE (START TO FINISH) OF THE WORK INCLUDING THE MAINTENANCE OF TRAFFIC THAT WILL BE USED.

TRAFFIC CONTROL INSPECTOR

THE CONTRACTOR SHALL DESIGNATE AN INDIVIDUAL OTHER THAN THE SUPERINTENDENT AND SUBJECT TO THE APPROVAL OF THE ENGINEER, TO CONTINUOUSLY INSPECT ALL TRAFFIC CONTROL DEVICES WHENEVER CONSTRUCTION WORK IS BEING PERFORMED WITHIN THE WORK LIMITS OF THE PROJECT. THE DESIGNATED INDIVIDUAL SHALL ALSO INSPECT ALL TRAFFIC DEVICES AT THE BEGINNING AND AT THE END OF EACH WORK DAY. THE DESIGNATED INDIVIDUAL OR A QUALIFIED REPRESENTATIVE SHALL ALSO BE AVAILABLE ON AN AROUND THE CLOCK BASIS TO REPAIR AND/OR REPLACE DAMAGED OR MISSING TRAFFIC CONTROL DEVICES. THESE INDIVIDUALS SHALL BE EQUIPPED WITH CELLULAR PHONES AND THEIR NAMES AND PHONE NUMBERS SHALL BE GIVEN TO THE PROJECT ENGINEER AT THE PRE-CONSTRUCTION MEETING. THE DESIGNATED INDIVIDUAL MAY HAVE OTHER CONSTRUCTION RELATED DUTIES AS LONG AS IMMEDIATE ATTENTION IS GIVEN TO TRAFFIC CONTROL. PAYMENT FOR THE SERVICES OF THE TRAFFIC CONTROL INSPECTOR SHALL BE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 614 MAINTAINING TRAFFIC.

ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW SHALL NOT BE PERMITTED AT PROJECT COST NOR TIME COMPENSATION. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC, OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

DURING PERIODS WHERE TRAFFIC NEEDS TO BE DIRECTED CONTRARY TO A TRAFFIC CONTROL DEVICE (FLAGGER, SIGN [E.G. STOP SIGN, STREET OR HIGHWAY SIGNS, ETC], SIGNAL OR OTHER DEVICE USED TO REGULATE, WARN OR GUIDE TRAFFIC). TRAFFIC IN THIS INSTANCE INCLUDES VEHICULAR, PEDESTRIAN AND/OR SHARED USE PATH USERS.

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

FOR LANE CLOSURES THAT MEET ALL OF THE CRITERIA LISTED BELOW: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

CRITERIA

 - . ON A MULTI-LANE DIVIDED INTERSTATE, OTHER FREEWAY OR EXPRESSWAY; AND
 - . AN AUTHORIZED SPEED LIMIT OF 45 MPH OR GREATER THAT IS IN EFFECT AT THE TIME OF THE OPERATION; AND,
 - . AADT OF 50,000 (OR AADT OF 30,000 WITH 25% OR HIGHER PERCENT TRUCKS)

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION (OR AT THE POINT OF ROAD CLOSURE), AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS AND/OR IN CONTRARY TO OTHER TRAFFIC CONTROL DEVICE IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE SHIFT DURATION SHALL NOT BE LESS THAN THE LEO'S MINIMUM SHOW-UP TIME REQUIRED BY THEIR LAW ENFORCEMENT AGENCY.THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE THAT SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 150 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF A LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

MAINTENANCE OF TRAFFIC NOTES

DESIGN AGENCY	
DESIGNER CAM	
REVIEWER RMM 2/19/25	
PROJECT ID 119100	
SHEET P.4	TOTAL 13

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 4 SIGN MONTHS ASSUMING 2 PCMS SIGN(S) FOR 2 MONTH(S)

TIME LIMITATION, TRAFFIC ON A MILLED SURFACE

THE MAXIMUM ALLOWABLE TIME FOR TRAFFIC TO BE PLACED ON A MILLED SURFACE SHALL BE 7 CONSECUTIVE CALENDAR DAYS. SHOULD THE CONTRACTOR FAIL TO MEET THIS REQUIREMENT, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF \$3000 PER DAY THAT THE TRAFFIC IS PLACED ON A MILLED SURFACE BEYOND THE SPECIFIED LIMIT.

ASPHALT PAVING LIMITATION

THE CONTRACTOR SHALL NOT ANTICIPATE OR SCHEDULE PLACING ASPHALT (ASPHALT SURFACE COURSE, ASPHALT INTERMEDIATE COURSE, ASPHALT CONCRETE BASE, ETC.) BETWEEN NOVEMBER 1 AND APRIL 1 WHEN SUBMITTING THEIR INITIAL BAR CHART PROGRESS SCHEDULE TO THE DISTRICT CONSTRUCTION ENGINEER (DCE) AS SPECIFIED IN CMS SECTION 108.02A. THIS LIMITATION SHALL ALSO INCLUDE INITIAL BASE LINE SCHEDULES AND ALL UPDATES IF A CPM SCHEDULE IS REQUIRED.

NOTIFICATION OF TRAFFIC RESTRICTIONS


THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS & PIO
ROAD & RAMP CLOSURES	>= 2WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	<12 HOURS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	>=2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERNS CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.



GENERAL SUMMARY	
DESIGN AGENCY	
	
DESIGNER	
CAM	
REVIEWER	
RMM	2/26/25
PROJECT ID	
119100	
SHEET	TOTAL
P.6	13

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PAVEMENT CALUCLATIONS

DESIGN AGENCY

DESIGNER

CAM

REVIEWER

RMM 2/19/25

PROJECT ID

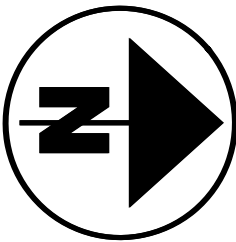
119100

SHEET

P.7

TOTAL

13



END PAVING
SR 193: SLM 1.97

BEGIN PROJECT
PAV'T JOINT JUST SOUTH
OF GYPSY LANE
SR 193 SLM 0.00



SR 304
SLM: 1.97

MOTOR INN DR
SLM: 1.57

1-80
SLM: 1.5

E LIBERTY ST
SLM: 1.35

LAUREL ST
SLM: 1.12

HAZELWOOD ST
SLM 0.94

GOLDIE RD
SLM: 0.83

TRUMBULL AVE
SLM: 0.51

COLONIAL DR
SLM 0.023

MAIN ROUTE	INTERSECTING ROUTE	DESIGN SHEET	QUADRANT RL=REAR LT, RR=REAR RT FL=FWD LT, FR=FWD RT (LOOKING UPSTATION)	SUGGESTED CURB RAMP TYPE (SCD BP-7.1, SHEET 2/3)	202	202	202	608	608		609	611	625	SPECIAL				COMMENTS
					WALK REMOVED	CURB REMOVED	CURB AND GUTTER REMOVED	4" CONCRETE WALK	CURB RAMP		CURB, TYPE 6	MANHOLE ADJUSTED TO GRADE	PULL BOX, MISC.: ADJUSTED TO GRADE	GAS VALVE BOX ADJUSTED TO GRADE				
					SF	FT	FT	SF	SF		FT	EACH	EACH	EACH				
SR 193	GYSKY LANE		FL	B2	112.00		16.00		112.00									AVOID PED POST
SR 193	GYSKY LANE		FR	B2	96.00		16.00		96.00									
SR 193	GYSKY LANE		RL	B2	126.00		21.00		126.00									
SR 193	GYSKY LANE		RR	B4	75.00		15.00		75.00									
																		COMPLIANT; ENSURE 5% MAX PAVE GRD. NO WORK; COMPLIANT NO WORK; COMPLIANT NO WORK; ADA WAIVER*
SR 193	BELGRADE		FL															
SR 193	FAIRLAWN		FR															
SR 193	FAIRLAWN		RR															
SR 193	BELGRADE		RL															NO WORK; COMPLIANT NO WORK; COMPLIANT NO WORK; COMPLIANT -N = FACING NORTH -W = FACING WEST; COMPLIANT ADA WAIVER*; ADD MISSING SIDEWALK
SR 193	COLONIAL DR		FL															
SR 193	COLONIAL DR		FR-S															
SR 193	COLONIAL DR *FACING COLONIAL		FR-W															
SR 193	COLONIAL DR *FACING COLONIAL		RR-N	A3	112.00	8.00			112.00		3.00			2.00				NO WORK; COMPLIANT NO WORK; COMPLIANT NO WORK; COMPLIANT -N = FACING NORTH -W = FACING WEST; COMPLIANT ADA WAIVER*; ADD MISSING SIDEWALK
SR 193	COLONIAL DR		RR-W															
SR 193	COLONIAL DR		RL	MISC				25.00										
SR 193	AMHURST DR		FL															NO WORK; COMPLIANT NO WORK; ADA WAIVER*
SR 193	AMHURST DR		RL															
SR 193	DARLINGTON AVE		FL															NO WORK; COMPLIANT NO WORK; COMPLIANT NO WORK; ADA WAIVER* ADA WAIVER FOR SLOPE; NEW LENGTH 4' FIX UPPER LANDING SLOPES (REM. WAIV.*) NO WORK; ADA WAIVER*
SR 193	DARLINGTON AVE		RL															
SR 193	TRUMBULL AVE		FL FL															NO WORK; ADA WAIVER* ADA WAIVER FOR SLOPE; NEW LENGTH 4' FIX UPPER LANDING SLOPES (REM. WAIV.*) NO WORK; ADA WAIVER*
SR 193	TRUMBULL AVE		FR	A1	112.00	14.00		56.00	56.00									
SR 193	TRUMBULL AVE		RR	A2	91.00	5.00		56.00	35.00					1.00				
SR 193	TRUMBULL AVE		RL RL															
																		NO WORK; COMPLIANT NO WORK; ADA WAIVER*
SR 193	EUCLID BLVD		FR															
SR 193	EUCLID BLVD		RR															
SR 193	MANSELL DR		FR															NO WORK; COMPLIANT NO WORK; COMPLIANT NO WORK; ADA WAIVER* MAKE RAMP 15' LONG PER DETAIL
SR 193	MANSELL DR		RR															
SR 193	GOLDIE AVE		FR															NO WORK; ADA WAIVER* MAKE RAMP 15' LONG PER DETAIL
SR 193	GOLDIE AVE		RR	A3	106.00	9.00		25.00	81.00									
SR 193	HAZELWOOD AVE		FL															NO WORK; COMPLIANT REQUIRED NEW RAMP NO WORK; DRIVEWAY NOT STREET NO WORK; DRIVEWAY NOT STREET NO WORK; DRIVEWAY NOT STREET NO WORK; DRIVEWAY NOT STREET NO WORK; DRIVEWAY NOT STREET
SR 193	HAZELWOOD AVE		RL	A3					65.00									
SR 193	LIBERTY PLAZA		FL															NO WORK; DRIVEWAY NOT STREET NO WORK; DRIVEWAY NOT STREET NO WORK; DRIVEWAY NOT STREET NO WORK; DRIVEWAY NOT STREET NO WORK; DRIVEWAY NOT STREET
SR 193	LIBERTY PLAZA		FR															
SR 193	LIBERTY PLAZA		FR															
SR 193	LIBERTY PLAZA		RR															
SR 193	LIBERTY PLAZA		RR															NO WORK; INC. IN PERMIT 25-12189 NO WORK; INC. IN PERMIT 25-12189 NO WORK; INC. IN PERMIT 25-12189
SR 193	E MONTROSE		RR	A3	85.00	6.00		25.00	60.00					1.00				
SR 193	E LIBERTY ST.		FL															NO WORK; FUTURE SAFETY PROJECT NO WORK; FUTURE SAFETY PROJECT NO WORK; FUTURE SAFETY PROJECT NO WORK; FUTURE SAFETY PROJECT
SR 193	E LIBERTY ST.		RL															
SR 193	E LIBERTY ST.		RR-W															
SR 193	E LIBERTY ST. *FACING LIBERTY ST		RR-N															
SR 193	E LIBERTY ST.		FR															NO WORK; INC. IN PERMIT 25-12189 NO WORK; INC. IN PERMIT 25-12189
SR 193	I-80 NW RAMP		RL															
SR 193	CHURCH HILL PARK ENTRANCE		FL															NO WORK; INC. IN PERMIT 25-12189 NO WORK; INC. IN PERMIT 25-12189
SR 193	CHURCH HILL PARK ENTRANCE		RL															
SUBTOTALS					915.00	42.00	68.00	187.00	818.00	0.00	3.00	0.00	0.00	4.00	0.00	0.00	0.00	
TOTALS CARRIED TO GENERAL SUMMARY					915	42	68	187	818	0	3	0	0	4	0	0	0	

CURB RAMP SUBSUMMARY

DESIGN AGENCY




DESIGNER
CAM

REVIEWER
RMM 7/31/24

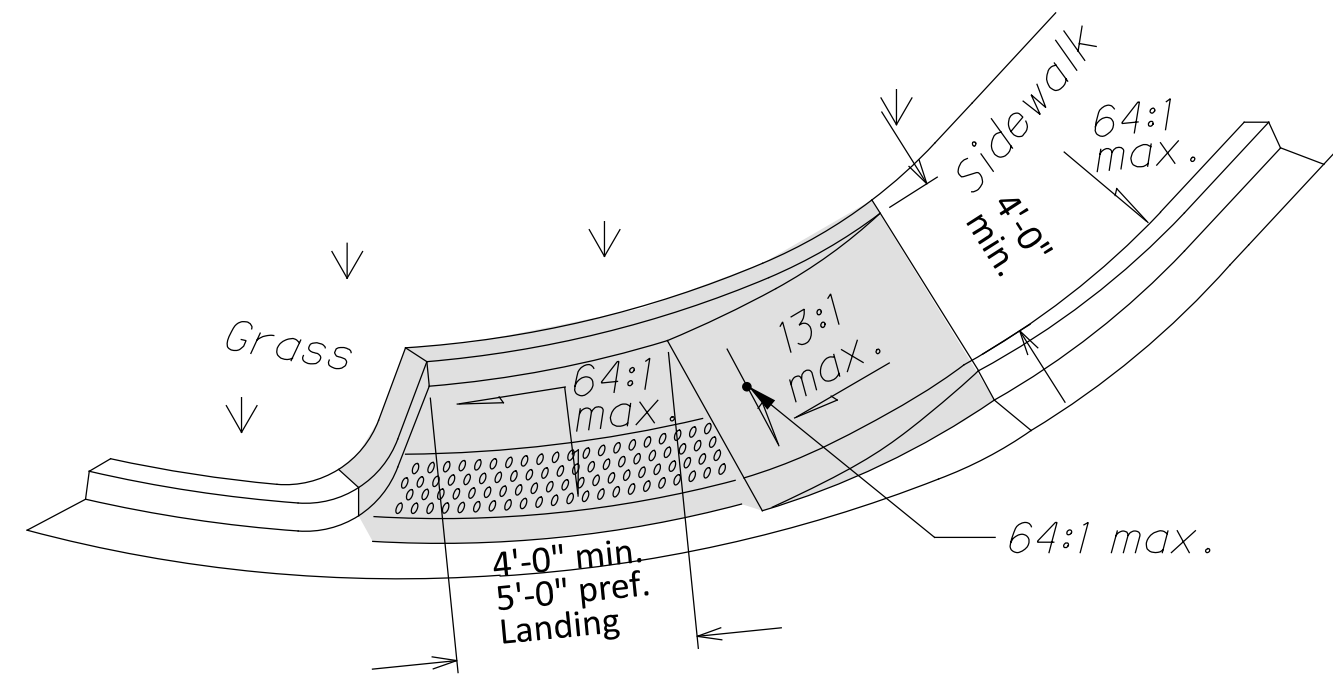
PROJECT ID
119100

SHEET
P.8

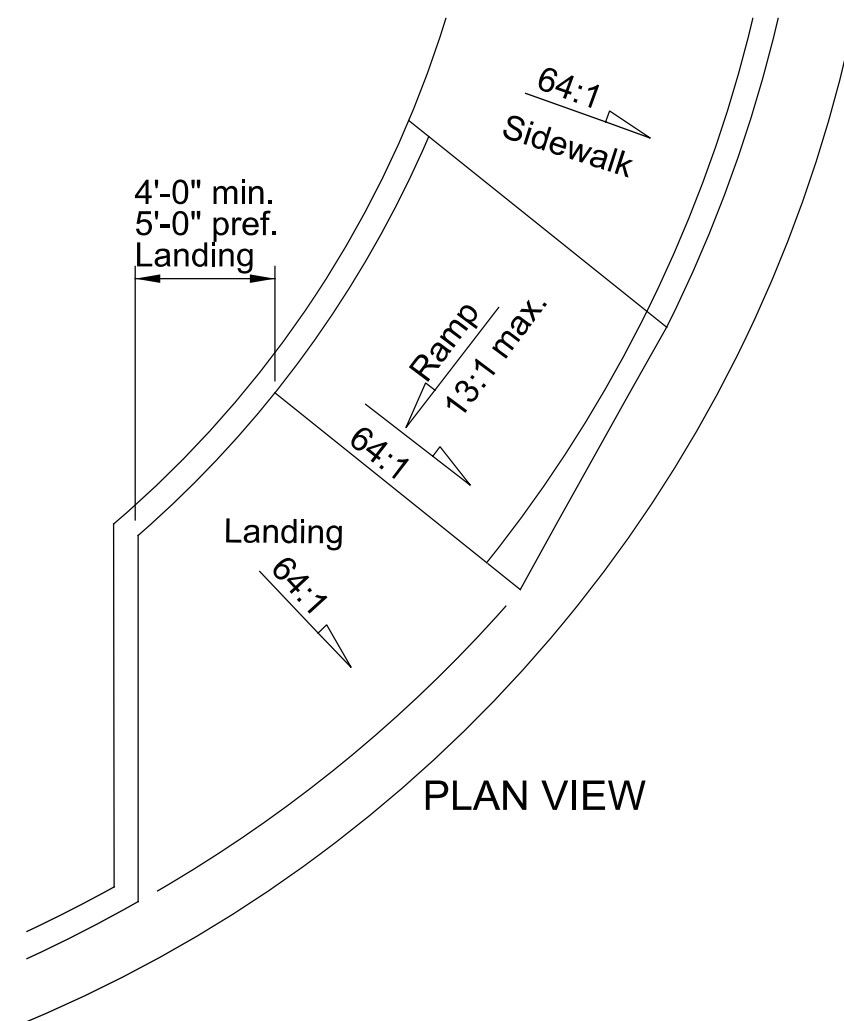
TOTAL
13

DESIGN AGENCY	
	
DESIGNER	
CAM	
REVIEWER	
RMM 7/31/24	
PROJECT ID	
119100	
SHEET	TOTAL
P.9	13

For Types A1, A2, B1, and B2, see SCD BP-7.1

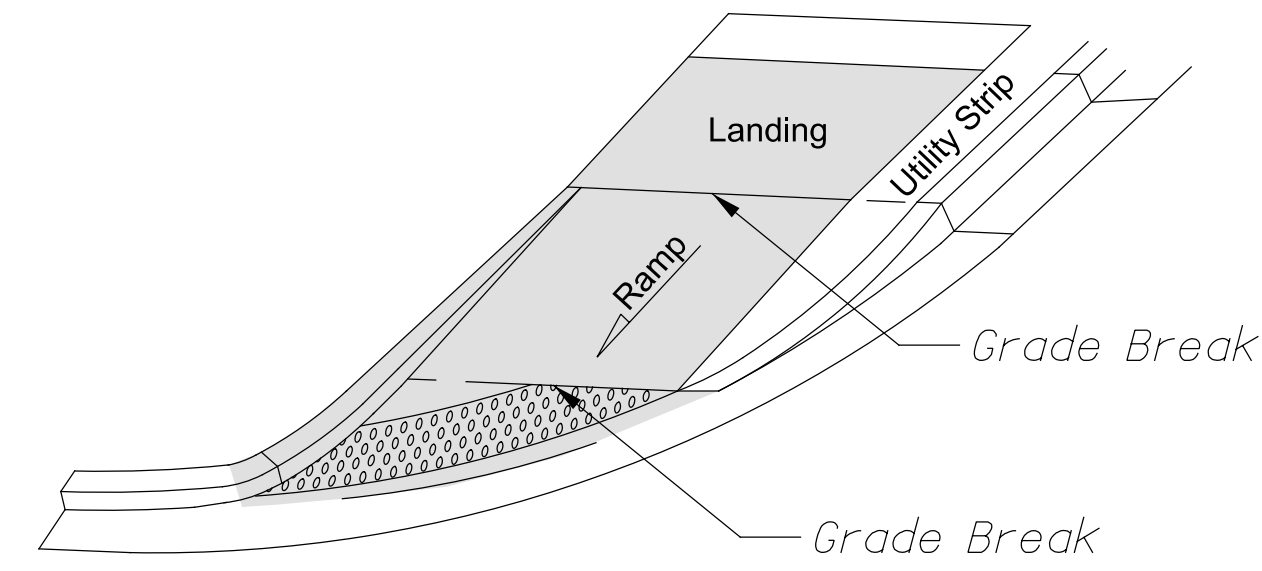


ISOMETRIC VIEW

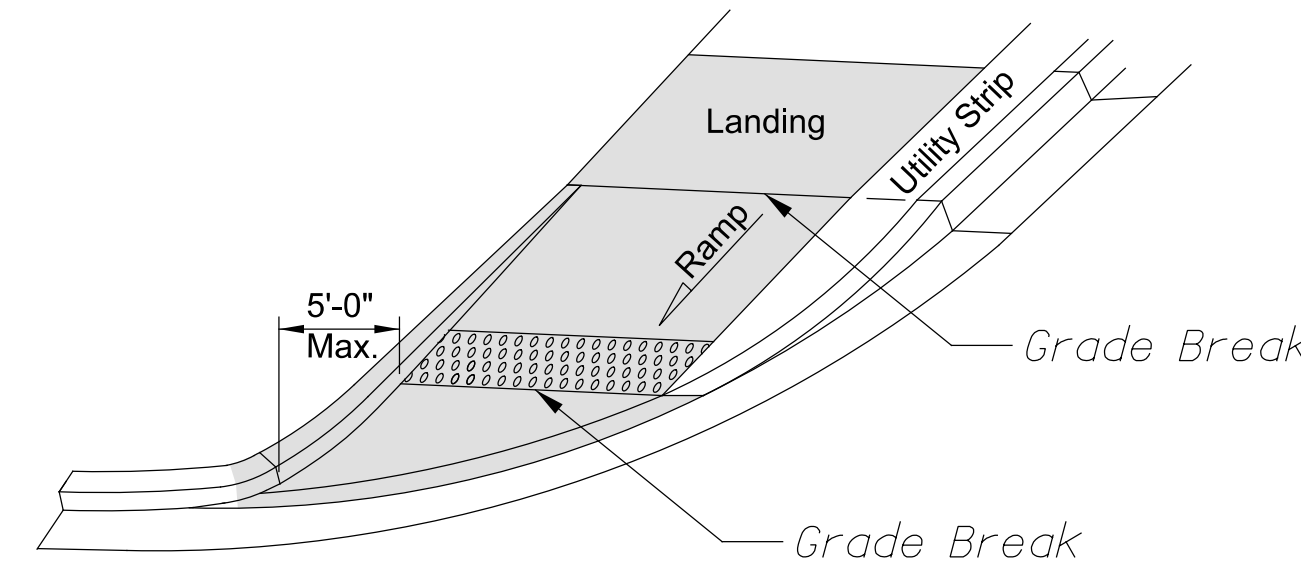


PLAN VIEW

Type B4

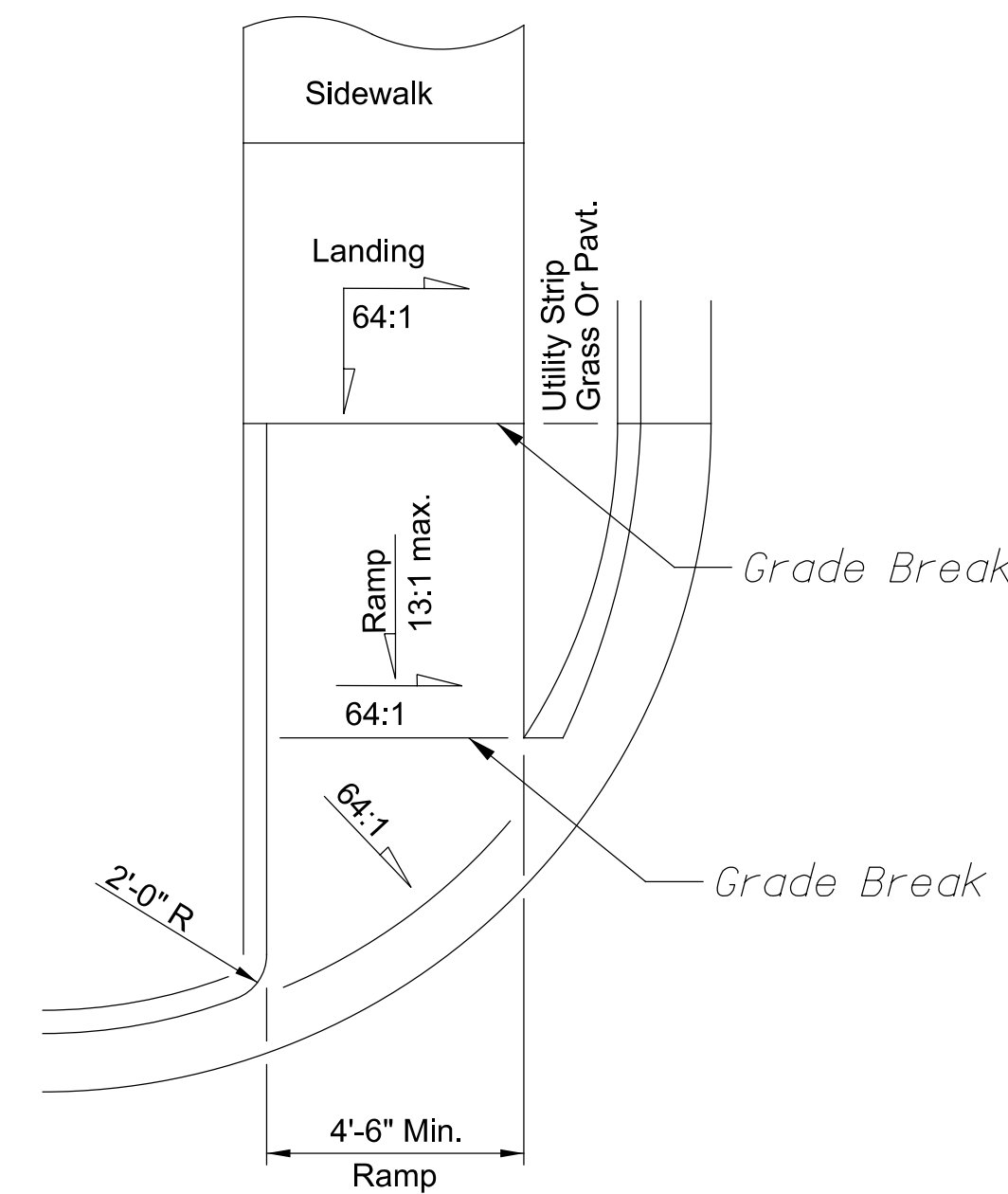


OPTION A



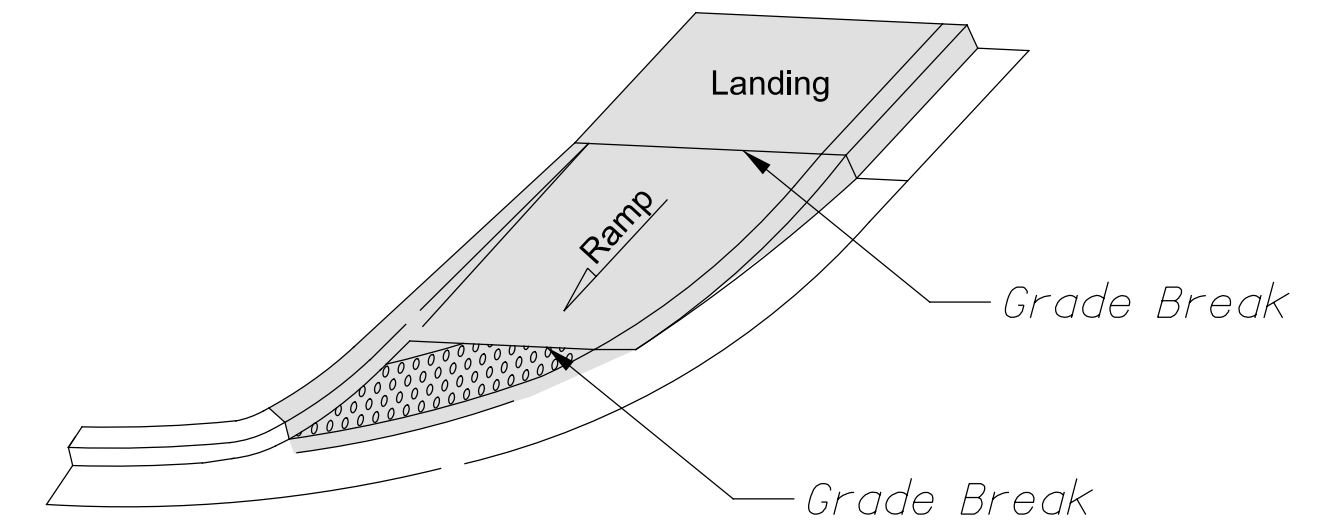
OPTION B

ISOMETRIC VIEW

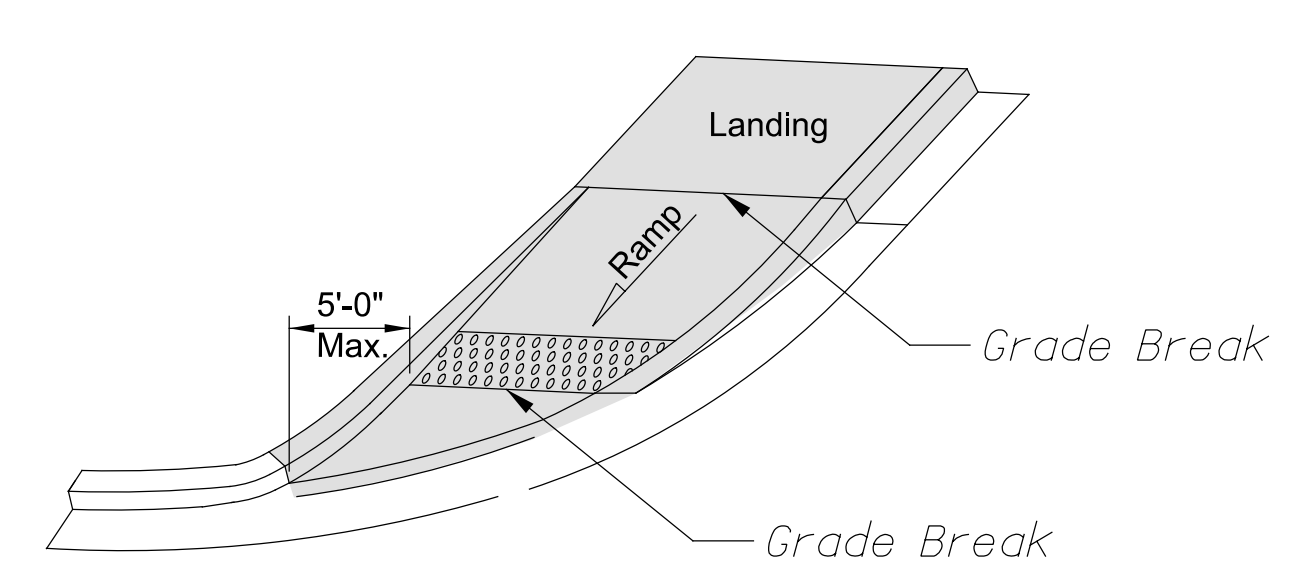


PLAN VIEW

Type A3

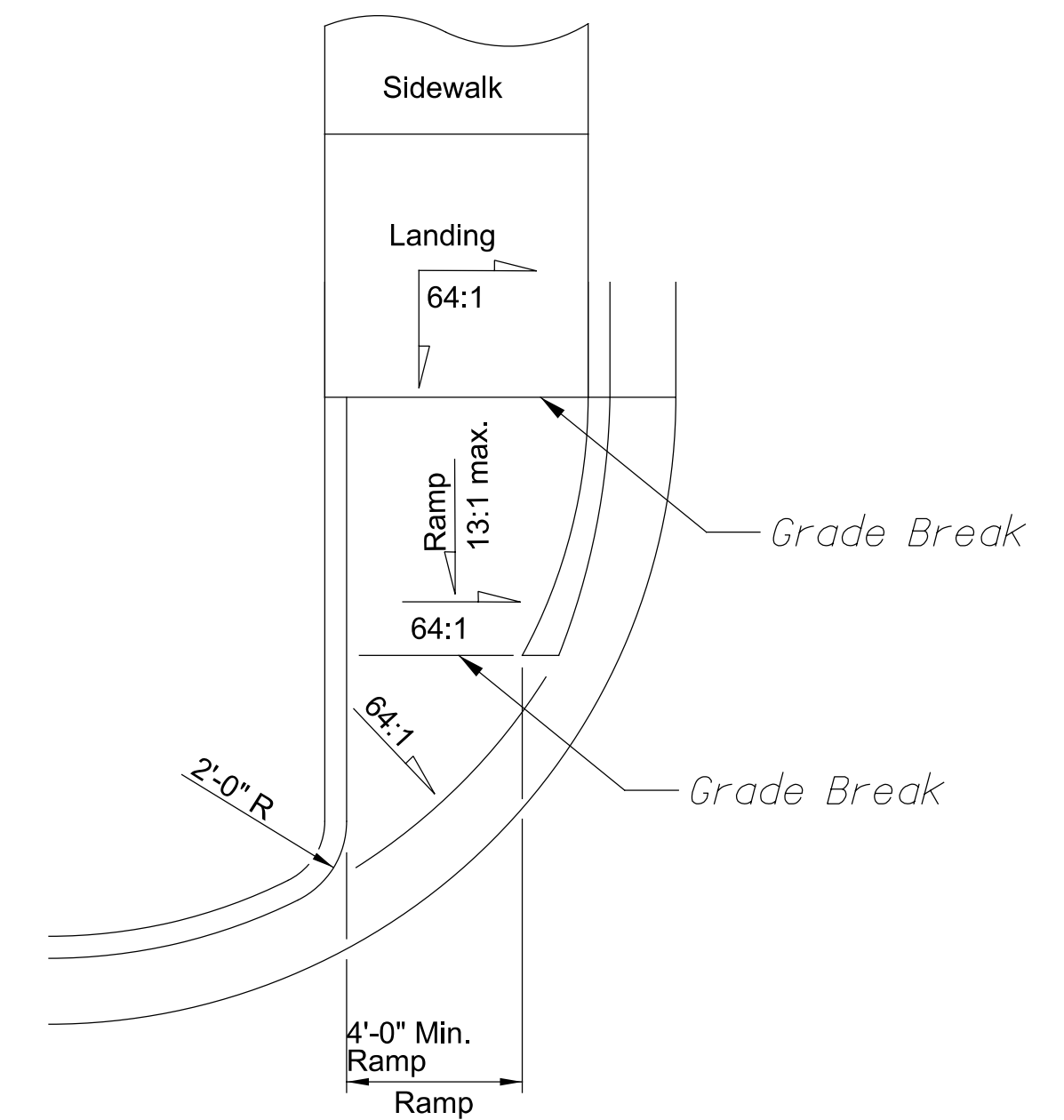


OPTION A



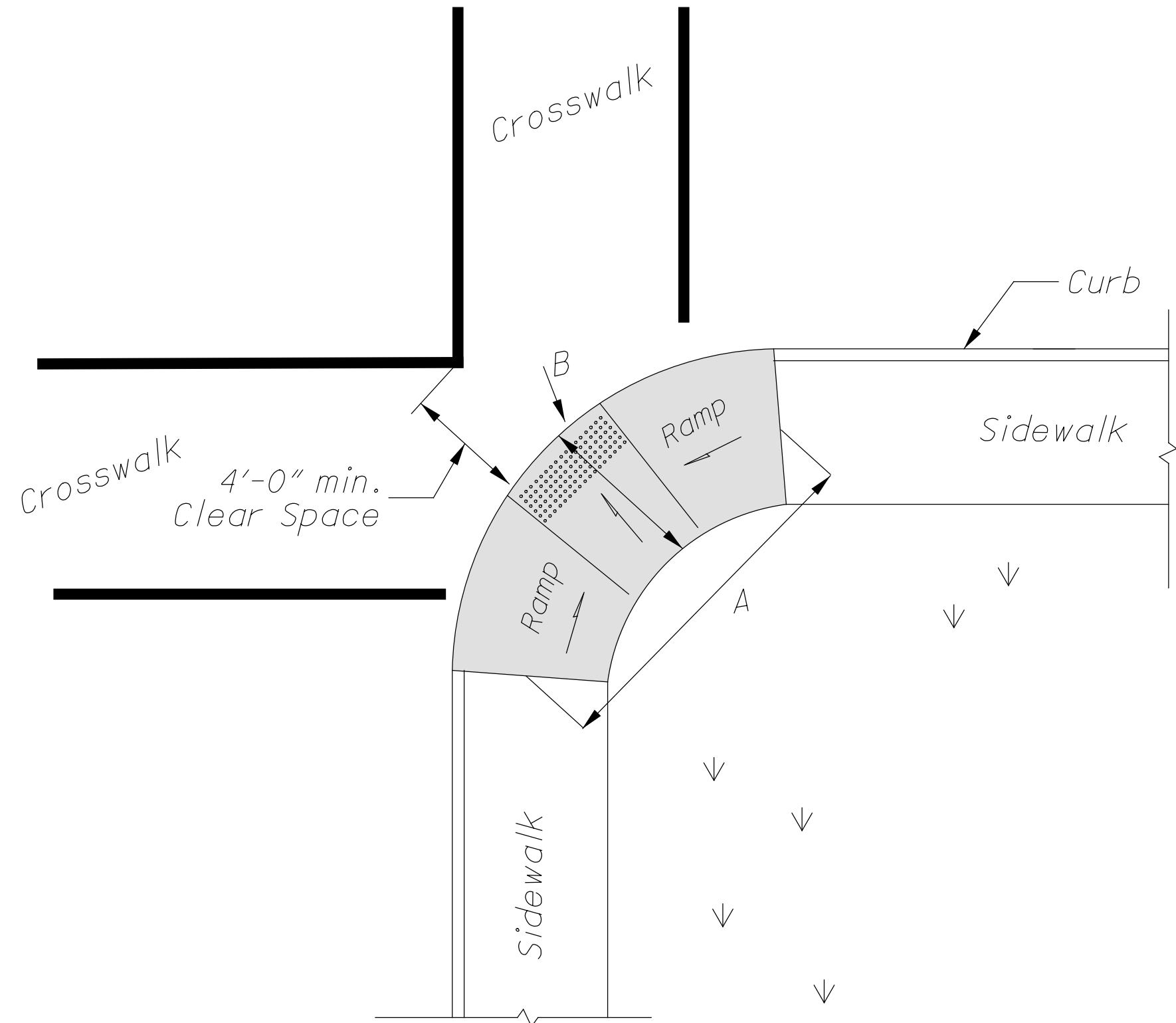
OPTION B

ISOMETRIC VIEW

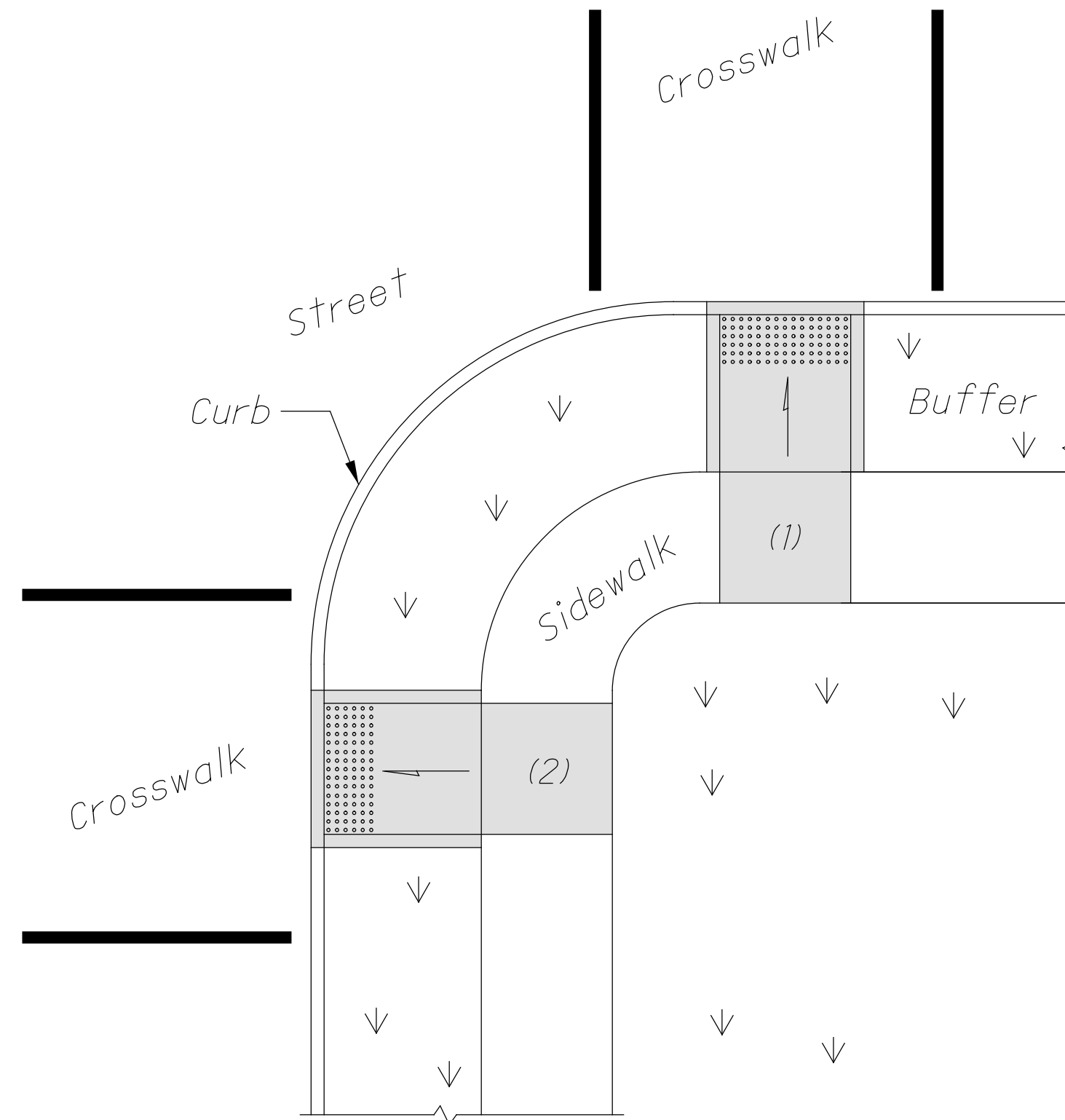


PLAN VIEW

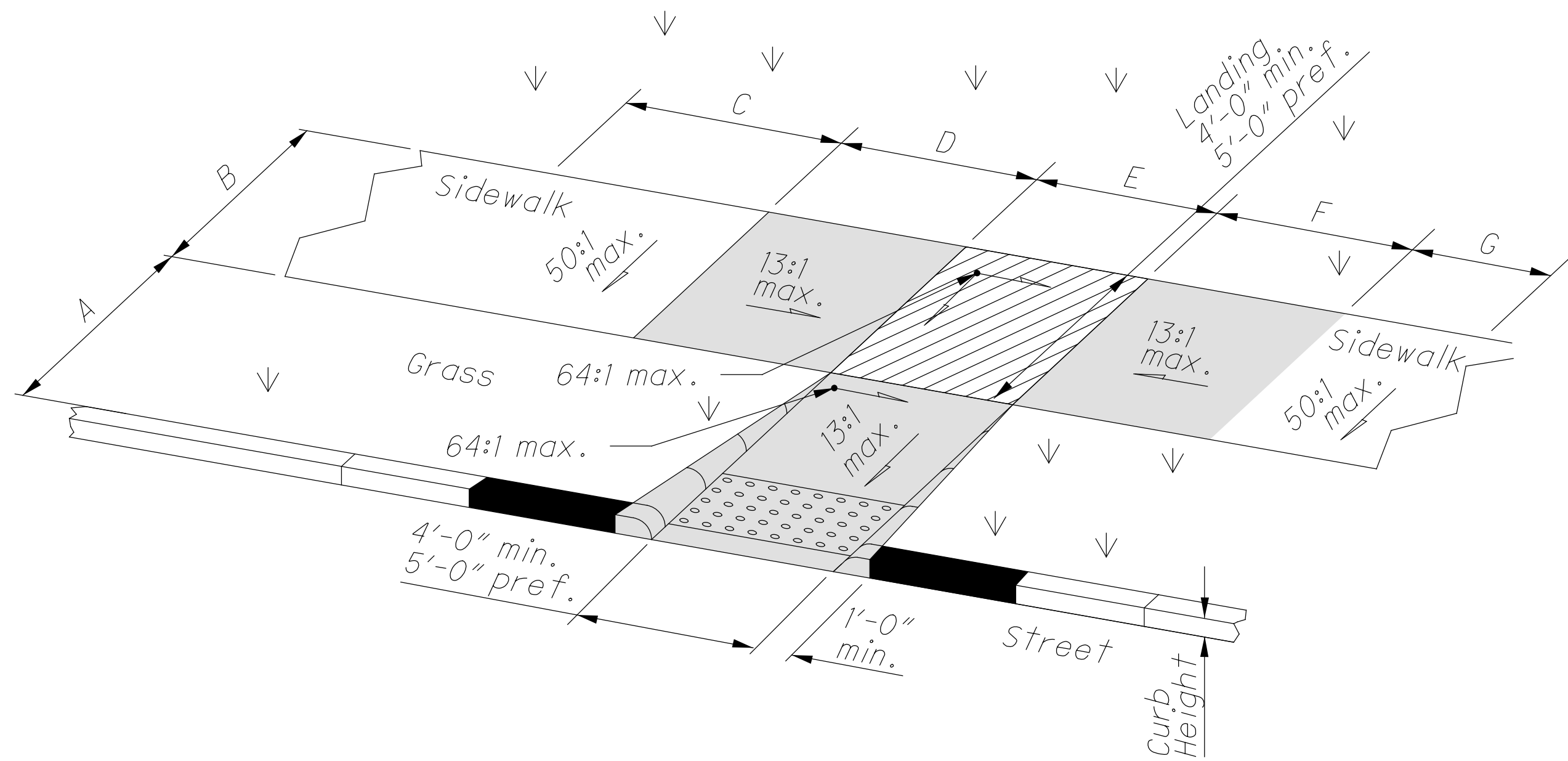
Type A4



Type D - PARALLEL CURB RAMP (TYPE B2)



TYPE A2/C2 - PERPENDICULAR CURB RAMPS



Type A2/C2



TRU-193-0.00

MODEL: Sheet PAPER SIZE: 34x22 (in.) DATE: 5/22/2025 TIME: 4:34:07 PM USER: cmarcum
pw:\chiodo-pw.bentley.com\chiodo-pw\02\Documents\01 Active Projects\District 04\Trumbull\119100\400-Engineering\Roadway\Sheets\119100_GSO02.dgn

[illegible]

RAISED PAVEMENT MARKER SUB SUMMARY

DESIGN AGENCY



DESIGNER

CAM

REVIEWER

RMM 2/19/25

PROJECT ID	119100
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SHEET	TOTAL
P.12	1

TRU-193-0.00

MODEL: Pavement Marking PAPERSIZE: 34x22 (in.) DATE: 5/22/2025 TIME: 4:34:17 PM USER: cmarcum
pw:\hplodot-pw.bentley.com:hplodot-pw-02\Documents\01 Active Projects\District 04\Trumbull\119100\400-Eng-

EDGE LINE													GENERAL SPEC:	640
													MATERIAL TYPE:	646
CTY	ROUTE	TRUE LOG	FROM	TRUE LOG	TO	WHITE EDGE LINE, 6"			YELLOW EDGE LINE, 6"			COMMENTS		
		TOTAL		HIGHWAY		RAMP	TOTAL	HIGHWAY	RAMP					
TOTAL						0			0					

LANE LINE									
CTY	ROUTE	TRUE LOG	FROM	TRUE LOG	TO	TOTAL MILES	6" LANE LINE		COMMENTS
		DASHED		SOLID					
TRU	193	0.00	GYPSY LANE, COUNTY LINE	1.97	SR 304	3.94			
TOTAL						3.94			

CENTER LINE								
CTY	ROUTE	TRUE LOG	FROM	TRUE LOG	TO	TOTAL MILES	EQUIVALENT SOLID LINE	COMMENTS
		0.00		GYPSY LANE, COUNTY LINE				
TRU	193	0.05	NORTH OF GYPSY LANE	0.23	COLONIAL DR	0.36	0.45	
		0.23	COLONIAL DR	0.26	NORTH OF COLONIAL DR	0.03	0.06	
		0.26	NORTH OF COLONIAL DR	0.48	SOUTH OF TRUMBULL AVE	0.44	0.55	
		0.48	SOUTH OF TRUMBULL AVE	0.52	NORTH OF TRUMBULL AVE	0.04	0.08	
		0.52	NORTH OF TRUMBULL AVE	0.80	SOUTH OF GOLDIE RD	0.56	0.70	
		0.80	SOUTH OF GOLDIE AVE	0.86	NORTH OF GOLDIE RD	0.06	0.11	
		0.86	NORTH OF GOLDIE RD	0.99	SOUTH OF LIBERTY PLAZA	0.26	0.33	
		0.99	SOUTH OF LIBERTY PLAZA	1.06	NORTH OF LIBERTY PLAZA	0.07	0.14	
		1.06	NORTH OF LIBERTY PLAZA	1.31	SOUTH OF E LIBERTY ST	0.50	0.63	
		1.31	SOUTH OF E LIBERTY ST	1.35	NORTH OF EAST LIBERTY ST	0.04	0.08	
		1.70	NORTH OF I-80 WB RAMP	1.97	SR 304	0.59	0.78	
						3.00	4.00	

*24" HIGH VISIBILITY																					
CTY	ROUTE LOCATION	TRUE LOG	CHANNEL LINE, 8"	CHANNEL LINE, 12"	STOP LINE	*CROSS WALK LINES	TRANSVERSE DIAGONAL LINES		ISLAND MARKING	SYMBOL MARKINGS			LANE ARROWS				REDUCT. ARROW	WORD ON PVMT ONLY		DOTTED LINES, 6"	COMMENTS
							WHITE	YELLOW		RxR	72"	96"	TURN LEFT	TURN RIGHT	THRU	TWO WAY LT ARROW		72"	96"		
TRU	GYPSY LANE	0.00		240	40	72							2			1					
TRU	TWO WAY LEFT N OF BEL. AVE.	0.10											1			1					
TRU	COLONIAL DR.	0.23		140	66	132							2								
TRU	TWO WAY LEFT N OF DARL. RD															1					
TRU	TRUMBULL AVE.	0.51		155	68	132							3			1					
TRU	TWO WAY LEFT N OF MANSELL	0.77														1					
TRU	GOLDIE RD.	0.82		490	80								3	2							
TRU	LIBERTY PLAZA	1.03		445	81	144							4	2		1					
TRU	TWO WAY LEFT N OF MONTROS.	1.20											1	1		1					
TRU	LIBERTY STREET	1.35		460	70								5								
TRU	I. 80 EB RAMP	1.50		330									3								
TRU	I. 80 WB RAMP	1.65		520	35								3							215	
TRU	I. 80 WB RAMP	1.65		150	36								2			1					
TRU	TWO WAY LEFT N OF I-80	1.70/1.90														2					
TRU	CHURCHILL RD	1.97		430	68								2	2							
TOTAL				3360	544	480							31	7		10				215	

PAVEMENT MARKING SUB SUMMARY

DESIGN AGENCY



DESIGNER

CAM

REVIEWER

BMM 7/31

11/5/2011

PROJECT ID

119100

SHEET	TOTAL
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