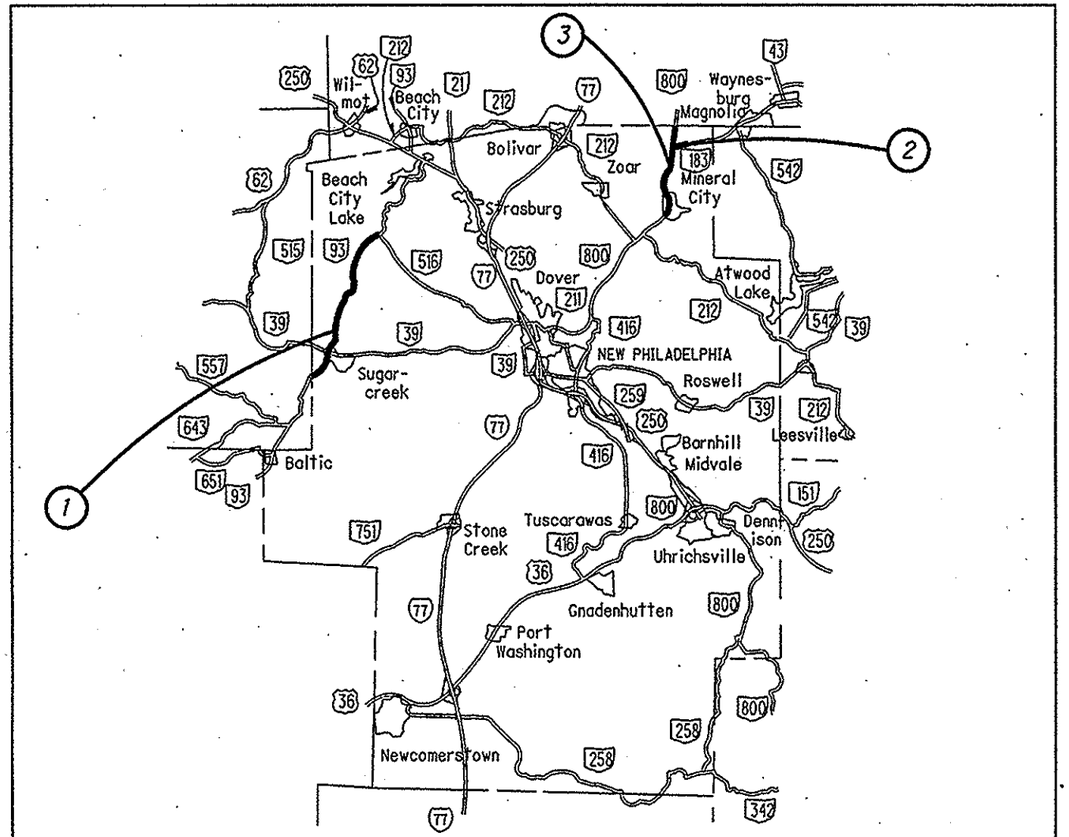
STATE OF OHIO
DEPARTMENT OF TRANSPORTATION



PORTIONS TO BE IMPROVED

LOCATION MAP

LATITUDE: N 40° 34' 50" LONGITUDE: W 81° 31' 20"

UNDERGROUND UTILITIES	STANDARD CONSTRUCTION DRAWINGS						SUPPLEMENTAL SPECIFICATIONS	
CONTACT BOTH SERVICES CALL TWO WORKING DAYS BEFORE YOU DIG	BP-3.1	10-19-07	MT-97.10	10-15-10	TC-41.20	1-19-01	800	11-8-10
CALL 1-800-362-2764 (TOLL FREE)	BP-4.1	7-16-04	MT-97.12	10-15-10	TC-42.20	7-16-04	832	5-5-09
OHIO UTILITIES PROTECTION SERVICE NON-MEMBERS MUST BE CALLED DIRECTLY	BP-5.1	7-28-00	MT-99.20	1-16-09	TC-52.10	1-19-07		
	BP-7.1	10-15-10	MT-101.60	4-17-09	TC-52.20	1-19-07		
			MT-101.90	1-16-09	TC-65.10	1-21-05		
	CB-2.3	7-15-05	MT-105.10	1-16-09	TC-65.11	1-21-05		
					TC-71.10	1-15-10		
OIL & GAS PRODUCERS PROTECTIVE SERVICE CALL: 1-800-925-0988	DM-1.1	4-21-06			TC-73.10	1-19-01		
	DM-1.4	4-21-06					SPECIAL PROVISIONS	
PLAN PREPARED BY: ODOT DISTRICT 11	DM-4.3	4-17-09						
	DM-4.4	4-17-09						

TUS-93-1.08
448 2-LANE RESURFACING

PART	COUNTY	ROUTE	SECTIONS	PROJECT TERMINI BEGIN	PROJECT TERMINI END	NET LENGTH MILES	CITY
1	TUS	SR 93	(1.08)	1.08	8.66	7.49	Sugarcreek
2	TUS	SR 183	(0.00)	0.00	0.04	0.04	
3	TUS	SR 800	(30.54)	30.54	34.91	4.32	Mineral City

PROJECT EARTH DISTURBED AREA = N/A (MAINTENANCE PROJECT)
ESTIMATED CONTRACTOR EARTH DISTURBED AREA = N/A (MAINTENANCE PROJECT)
NOTICE OF INTENT EARTH DISTURBED AREA = N/A (MAINTENANCE PROJECT)

THE 2010 STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THESE IMPROVEMENTS WILL NOT REQUIRE THE CLOSING OF THE HIGHWAYS TO TRAFFIC AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FOURTH ON THE PLANS AND ESTIMATES.

APPROVED DATE 11/5/10 — Richard A. Bibbee P.E.
DISTRICT DEPUTY DIRECTOR

APPROVED DATE 11-19-10 — Jolene M. Molitoris
DIRECTOR, DEPARTMENT OF TRANSPORTATION

ENGINEER'S SEAL:

STATE OF OHIO
SCOTT K. WARNER
E-65533
REGISTERED PROFESSIONAL ENGINEER

SIGNED: Scott K. W
DATE: 11-5-10

INDEX OF SHEETS

FEDERAL PROJECT NO. E070(302)
PID NO. 81691
CONSTRUCTION PROJECT NO.
RAILROAD INVOLVEMENT NONE
TUS-93-1.08
1/15

TUS - SR-93-1.08
C 110075 PID - 81691
Dist 11 2/10/2011

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MAINTAINING TRAFFIC

A MINIMUM OF ONE LANE OF TRAFFIC SHALL BE MAINTAINED AT ALL TIMES PER ITEM 614 AND AS PER SCD MT-97.12. THE LENGTH OF RESTRICTED TRAFFIC LANES SHALL BE KEPT TO A MINIMUM CONSISTENT WITH THE CMS REQUIREMENTS FOR THE PROTECTION OF WORK ITEMS, WHICH NECESSITATE THE RESTRICTION. THE LIMITS AND DURATION OF LANE CLOSURES SHALL BE SUBJECT TO THE APPROVAL OF THE ENGINEER.

THE PLANING AND RESURFACING WILL PROCEED CONTINUOUSLY A MINIMUM OF FIVE (5) DAYS PER WEEK, WEATHER PERMITTING, EXCEPT FOR THE HOLIDAYS AND EVENTS LISTED BELOW. ANY OPEN PAVEMENT TRENCH SHALL BE ADEQUATELY MAINTAINED AND PROTECTED WITH BARRICADES, DRUMS, VERTICAL PANELS OR PORTABLE CONCRETE BARRIER.

WHEN INSTALLING THE PIPE AND CATCH BASINS AT SLM 32.53, A MINIMUM OF ONE LANE OF TRAFFIC SHALL BE MAINTAINED AT ALL TIMES PER ITEM 614 AND SCD MT-97.12.

THE CONTRACTOR SHALL FURNISH, INSTALL AND MAINTAIN W-8-H15 "GROOVED PAVEMENT" SIGNS PER CMS 614.055.

THE CONTRACTOR SHALL FURNISH, INSTALL, AND MAINTAIN SIGNS W8-1 (48"x48") "BUMP" AND W8-2 (48"x48") "DIP" WITH W13-1 (24"x24") ADVISORY SPEED PLAQUE WITH SPEEDS APPROVED BY THE ENGINEER FOR ALL BUTT JOINT LOCATIONS, WHILE THE BUMP OR DIP CONDITION EXISTS.

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:

MEMORIAL DAY, FOURTH OF JULY, LABOR DAY

STATE ROUTE 93 - SUGARCREEK CAR SHOW JUNE 10-12, 2011.
STATE ROUTE 93 - SUGARCREEK SWISS FESTIVAL SEPT. 21-26, 2011.

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF THE WEEK	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 12:00N MONDAY
MONDAY	12:00N FRIDAY THROUGH 12:00N TUESDAY
TUESDAY	12:00N MONDAY THROUGH 12:00N WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 12:00N THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 12:00N FRIDAY
FRIDAY	12:00N THURSDAY THROUGH 12:00N MONDAY
SATURDAY	12:00N FRIDAY THROUGH 12:00N MONDAY

THE CONTRACTOR SHALL COMPLETE ALL GRINDING, RESURFACING, SHOULDER, AND TEMPORARY PAVEMENT MARKING WORK ON SR 93 INSIDE THE VILLAGE OF SUGARCREEK ON OR BEFORE MAY 25, 2011 TO AVOID CONFLICT WITH TOURISM TRAFFIC. IN ADDITION NO LANE RESTRICTIONS ARE PERMITTED ON SR 93 FRIDAY 10 AM THROUGH SUNDAY 7 AM DURING THE MONTH OF MAY. PERMANENT PAVEMENT MARKINGS MAY BE INSTALLED ON SR 93 SUNDAYS THROUGH THURSDAYS, MAY THROUGH NOVEMBER.

NO EXTENSIONS OF TIME SHALL BE GRANTED FOR DELAYS IN MATERIAL DELIVERIES, UNLESS SUCH DELAYS ARE INDUSTRY-WIDE, OR FOR LABOR STRIKES, UNLESS SUCH STRIKES ARE AREA-WIDE.

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED LIQUIDATED DAMAGES IN ACCORDANCE WITH CMS 108.07.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

WHEN RAISED PAVEMENT MARKERS ARE TO BE INSTALLED, THE REQUIRED LANE CLOSURE SHALL REMAIN IN EFFECT UNTIL THE EPOXY IS DRY AND ALL FOREIGN MATTER OR DEBRIS CREATED BY THE INSTALLATION OF THE RPM CASTING IS REMOVED FROM THE ROADWAY.

MAINTAINING TRAFFIC, CON'T.

PEDESTRIAN WALKWAYS CONSTRUCTED BY THE CONTRACTOR SHALL BE KEPT FREE OF ANY OBSTRUCTIONS OR HAZARDS INCLUDING HOLES, DEBRIS AND MUD. OTHER WALKWAYS DAMAGED OR DIRTIED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED OR CLEANED. THE CONTRACTOR MUST TAKE PRECAUTIONS TO PROTECT PEDESTRIANS OR RESIDENTS (INCLUDING CHILDREN) FROM EXPOSURE TO HAZARDS RESULTING FROM THE CONSTRUCTION OPERATION BY INSTALLING CONSTRUCTION FENCE AND SIGNING AS FOLLOWS:
TEMPORARY ORANGE PLASTIC CONSTRUCTION FENCE SHALL BE PLACED AROUND THE SIDEWALK WORK AREAS. SIDEWALK CLOSED SIGN (R9-9 (30 X 18)) MOUNTED ON A TYPE III BARRICADE WITH TYPE A FLASHING LIGHTS SHALL BE PLACED OUTSIDE THE FENCE ON EACH SIDEWALK APPROACH AS SHOWN ON SCD MT-101.60.

FOR REPAIR OR RECONSTRUCTION WORK INVOLVING SIDEWALKS ON BOTH SIDES OF THE STREET, THE WORK SHALL BE STAGED SO THAT ONE SIDE IS COMPLETED AND OPEN TO PEDESTRIAN TRAFFIC BEFORE THE OTHER IS DISRUPTED.

TEMPORARY ORANGE PLASTIC CONSTRUCTION FENCE HAS BEEN PROVIDED HEREIN AS A TRAFFIC CONTROL DEVICE TO DIVERT AND GUIDE PEDESTRIANS WHOSE PATH WOULD OTHERWISE ENTER THE WORK AREA. ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

EXTRA FOR WIDENING (PAVEMENT AREA)

AN ADDITIONAL QUANTITY HAS BEEN ADDED TO THE PAVEMENT DATA SHEETS TO BE USED AS DIRECTED BY THE ENGINEER, TO COVER AREAS THAT HAVE BEEN WIDENED ON CURVES OR ON PREVIOUS MAINTENANCE ACTIVITIES BEYOND THE AVERAGE PAVEMENT WIDTH SHOWN.

ITEM 448 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG70-22M, AS PER PLAN

MATERIALS FURNISHED FOR FINE AND COARSE AGGREGATES USED IN THIS ITEM SHALL EXCLUDE ALL STONE AND CRUSHED CARBONATE STONE.

ITEM 448 - ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, PG64-22 (SPOT LEVELING)

LONGITUDINAL AND TRANSVERSE IRREGULARITIES ARE INTERMITTENTLY PRESENT THROUGHOUT THE EXISTING PAVEMENT SURFACE, BUT THE PAVEMENT DOES NOT REQUIRE A FULL-WIDTH LEVELING COURSE. IRREGULARITIES SHALL BE FILLED WITH 448 IN A MANNER THAT WILL RESULT IN SURROUNDING PORTIONS OF THE EXISTING SURFACE REMAINING EXPOSED AFTER THE SPOT LEVELING COURSE IS PLACED. THE SPOT LEVELING COURSE SHALL BE A VARIABLE DEPTH COURSE WITH A MINIMUM THICKNESS OF 0". THE MATERIAL SHALL BE PLACED IN A SEPARATE OPERATION DIRECTED BY THE ENGINEER.

ITEM 448 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG64-22 (DRIVEWAYS), AS PER PLAN

THIS ITEM OF WORK SHALL CONSIST OF PAVING ALL EXISTING DRIVEWAYS AND INTERSECTING PUBLIC ROADS NOT OTHERWISE INDICATED. A 2 INCH AVERAGE THICKNESS SHALL BE PLACED ON EXISTING AGGREGATE DRIVES AND APPROACHES OR AN AVERAGE THICKNESS EQUAL TO THE SURFACE COURSE THICKNESS SHALL BE PLACED ON THE EXISTING PAVED DRIVES AND APPROACHES, FOR AN APPROXIMATE DISTANCE OF 10 FEET FOR DRIVEWAYS AND 20 FEET FOR PUBLIC ROADS FROM THE EDGE OF PAVEMENT OR PAVED SHOULDERS, WHICHEVER IS APPLICABLE, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. UP GRADE DRIVEWAY PAVING SHALL BE PLACED TO THE BEGINNING OF THE UPSLOPE OF THE DRIVEWAY, AS DIRECTED BY THE ENGINEER. ALL GRADING, TACK COAT, PRIME COAT, TOOLS, EQUIPMENT AND INCIDENTALS REQUIRED TO LAYOUT AND PAVE THE DRIVEWAYS AND INTERSECTING PUBLIC ROADS SHALL BE INCLUDED IN THE CU. YD. PRICE BID FOR ITEM 448 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, PG64-22 (DRIVEWAYS), AS PER PLAN. THE CONTRACTOR'S ATTENTION IS DIRECTED TO CMS 107.10. ALL DRIVEWAYS SHALL BE PAVED WITHIN (5) WORKING DAYS AFTER PLACING OF THE SURFACE COURSE ON THE MAINLINE PAVEMENT. MATERIALS FURNISHED FOR FINE AND COARSE AGGREGATES USED IN THIS ITEM SHALL EXCLUDE ALL STONE AND CRUSHED CARBONATE STONE.

ITEM 614 - WORK ZONE PAVEMENT MARKINGS AND SIGNS

THE CONTRACTOR SHALL INSTALL ITEM 614 - WORK ZONE CENTER LINE, CLASS 1, 642 PAINT PRIOR TO OPENING THE LANE TO TRAFFIC, OR WHEN THE EXISTING MARKINGS HAVE BEEN COVERED OR DAMAGED, AS PER CMS 614.11.

IN THE EVENT THE CONTRACTOR CANNOT INSTALL THE WORK ZONE CENTER LINE, CLASS 1, DUE TO CONDITIONS BEYOND HIS CONTROL, AN ESTIMATED QUANTITY OF "DO NOT PASS" (R4-1) AND "PASS WITH CARE" (R4-2) SIGNS HAVE BEEN PROVIDED BELOW.

WORK ZONE CENTER LINE, CLASS 1 MARKINGS SHALL BE PLACED, AND THE ABOVE SIGNS REMOVED BY THE END OF THE CONTRACTOR'S NEXT WORK DAY. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

(PART 1) 614, WORK ZONE CENTER LINE, CLASS 1, 642 PAINT - **17.33 MILE**
(PART 2) 614, WORK ZONE CENTER LINE, CLASS 1, 642 PAINT - **0.08 MILE**
(PART 3) 614, WORK ZONE CENTER LINE, CLASS 1, 642 PAINT - **10.73 MILE**
TOTAL 28.14 MILE

THE CONTRACTOR SHALL ERECT "NO EDGE LINES" (W8-H12a) SIGNS IN ADVANCE OF ANY SECTION OF ROADWAY LACKING CMS STANDARD EDGE LINE MARKINGS, AS PER CMS 614.04.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER FOR WORK ZONE MARKING SIGNS PER THE REQUIREMENTS ABOVE, AND ITEM 614 OF THE SPECIFICATIONS.

(PART 1) 614, WORK ZONE MARKING SIGN - - - **66 EACH**
(PART 2) 614, WORK ZONE MARKING SIGN - - - **4 EACH**
(PART 3) 614, WORK ZONE MARKING SIGN - - - **58 EACH**
TOTAL 128 EACH

WORK ZONE MARKING SIGN TABLE		
"NO EDGE LINES"	PART 1	46
"NO EDGE LINES"	PART 2	4
"NO EDGE LINES"	PART 3	38
"DO NOT PASS"	PART 1	10
"DO NOT PASS"	PART 2	0
"DO NOT PASS"	PART 3	10
"PASS WITH CARE"	PART 1	10
"PASS WITH CARE"	PART 2	0
"PASS WITH CARE"	PART 3	10
	TOTAL	128

TRAFFIC PAINT

THE CONTRACTOR SHALL REPLACE THE EXISTING PAVEMENT MARKINGS WITHIN THE PROJECT LIMITS WITH NEW PAVEMENT MARKINGS AT THE SAME LOCATIONS AS PER CMS 641.06. SEE STANDARD DRAWINGS TC-71.10 AND TC-73.10 FOR PAVEMENT MARKING DETAILS.

PROFILE AND ALIGNMENT

PLACE THE PROPOSED PAVEMENT TO FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT. PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY AS SHOWN ON THE TYPICAL SECTIONS.

TEMPORARY ORANGE PLASTIC CONSTRUCTION FENCE

TEMPORARY ORANGE PLASTIC/NYLON CONSTRUCTION FENCE SHALL BE PLACED FOR THE PROTECTION OF PEDESTRIAN TRAFFIC. IT SHALL BE SECURELY FASTENED TO WOOD OR METAL POSTS AT NOT MORE THAN 6' SPACING. IT SHALL BE NOMINALLY 42" HIGH AND THE TOP EDGE SHALL NOT SAG BELOW 30". IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO INSURE THAT THE FENCE IS IN GOOD CONDITION AND PROPERLY PLACED AND MAINTAINED.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 607 - FENCE MISC.: ORANGE PLASTIC CONSTRUCTION FENCE - **585 FT**

CALCULATED JAC
CHECKED SKW

GENERAL NOTES

TUS-93-1.08

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF CMS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DIRECTING TRAFFIC THROUGH SIGNALIZED INTERSECTIONS.

WHEN SPECIFIED BY THE ENGINEER.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

THE LEO SHOULD REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. ONCE THE LEO HAS COMPLETED THE DUTIES DESCRIBED ABOVE AND STILL HAS TIME REMAINING ON HIS/HER SHIFT, THE LEO MAY BE ASKED TO PATROL THROUGH THE WORK ZONE (WITH FLASHING LIGHTS OFF) OR BE PLACED AT A LOCATION TO DETER MOTORISTS FROM SPEEDING. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS WITH PATROL CAR REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFOREMENT OFFICER WITH PATROL CAR FOR ASSISTANCE ----- **40 HOURS**

THE HOURS PAID SHALL INCLUDE MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF AN LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

SHOULDER PREPARATION

THIS WORK WILL BE IN ACCORDANCE WITH CMS ITEM 617, WITH SPECIAL ATTENTION GIVEN TO SECTION 617.04. THE WORK DONE WILL BE IN REASONABLY CLOSE CONFORMITY WITH THE LINES AND TYPICAL SECTIONS SHOWN ON THE PLANS OR AS ESTABLISHED BY THE ENGINEER.

ITEM 408 - PRIME COAT, AS PER PLAN

THE CONTRACTOR WILL APPLY "MC-70" AT A RATE OF 0.4 GALLONS PER SQUARE YARD, OR AS DETERMINED BY THE ENGINEER, TO THE COMPLETED SHOULDER THAT IS MADE UP OF GRINDINGS.

SHIELD

THE CONTRACTOR SHALL PROVIDE A SHIELD TO PREVENT THE SPRAYING OR DRIFTING OF LIQUID BITUMINOUS MATERIAL ONTO THE EDGE OF THE PAVEMENT OR EDGELINE. THE ATTENTION OF THE CONTRACTOR IS DIRECTED TO 107.10 OF THE SPECIFICATIONS.

ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN

THE CONTRACTOR WILL UTILIZE MATERIAL (I.E. GRINDINGS) OBTAINED FROM THE PAVEMENT PLANING, ASPHALT CONCRETE, OPERATION. THIS MATERIAL WILL BE PLACED IN LIEU OF THE COMPACTED AGGREGATE. ALL SPECIFICATIONS FOR ITEM 617 APPLY.

UTILITIES

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER OR ADJACENT TO THE WORK AREA.

SIDEWALK, CURB RAMPS, AND DETECTABLE WARNINGS

ALL CURB RAMP AND SIDEWALK WORK SHALL BE ACCOMPLISHED WHEN SCHOOL IS NOT IN SESSION

NOTIFICATION OF WORK ZONE LANE RESTRICTIONS

THE CONTRACTOR SHALL NOTIFY THE ENGINEER AT LEAST EIGHTEEN (18) DAYS PRIOR TO IMPLEMENTING ANY WORK ZONE RESTRICTIONS THAT WILL REDUCE THE WIDTH OR VERTICAL CLEARANCE OF ANY LANE ON WHICH TRAFFIC WILL BE MAINTAINED DURING CONSTRUCTION.

THE ENGINEER SHALL IMMEDIATELY NOTIFY THE DISTRICT ROADWAY SERVICES MANAGER TO ADVISE THE OFFICE OF HIGHWAY MANAGEMENT OF THE RESTRICTIONS.

COORDINATION OF RESURFACING AND PLANING OPERATIONS

ONCE THE PAVEMENT PLANING OPERATIONS HAVE BEGUN, IT SHALL PROCEED CONTINUOUSLY UNTIL ALL ELEMENTS OF THE WORK ASSOCIATED WITH THE PAVEMENT PLANING OPERATIONS ARE COMPLETED. THE PAVEMENT PLANING OPERATION SHALL BE COMPLETED IN A TIMELY MANNER AS DIRECTED BY THE ENGINEER. THE RESURFACING OPERATION SHALL BEGIN NO LATER THAN THREE (3) DAYS AFTER THE START OF THE PAVEMENT PLANING OPERATION.
IF PAVING THE ASPHALT CONCRETE DIRECTLY ONTO PORTLAND CEMENT, CONCRETE OR BRICK PAVEMENT, TACK THE PAVEMENT WITH RUBBERIZED ASPHALT EMULSION CONFORMING TO CMS 702.13.

THE CONTRACTOR WILL UTILIZE GRINDINGS OBTAINED FROM PAVEMENT PLANING TO BE PLACED IN LIEU OF COMPACTED AGGREGATE ON PROPOSED SHOULDERS.

GRINDINGS NOT USED FOR SHOULDER MATERIAL SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND DISPOSED OF AT HIS EXPENSE OUTSIDE THE LIMITS OF RIGHT OF WAY.

CALCULATED JAS
CHECKED SKW

GENERAL NOTES

TUS-93-1.08

3
15

SHEET NUMBER													ITEM	ITEM EXT.	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
2	3	5	6	7	8	9	10	11	14	15								
																	ROADWAY	
										17			202	23000	17	SQ YD	PAVEMENT REMOVED	
		2,932	1,443										202	23500	4,375	SQ YD	WEARING COURSE REMOVED	
							473	800					202	30000	1273	SQ FT	WALK REMOVED	
							6	67					202	32000	73	FT	CURB REMOVED	
										33			202	35100	33	FT	PIPE REMOVED, 24" AND UNDER	
										2			202	58100	2	EACH	CATCH BASIN REMOVED	
585													607	98000	585	FT	FENCE, MISC.: ORANGE PLASTIC CONSTRUCTION FENCE	2
							268	225					608	13000	493	SQ FT	6" CONCRETE WALK	
							75	585					608	52000	660	SQ FT	CURB RAMP	
							146	92					608	53020	238	SQ FT	DETECTABLE WARNING	
								7					609	26000	7	FT	CURB, TYPE 6	
																	EROSION CONTROL	
													832	30000	1000	EACH	EROSION CONTROL	
																	DRAINAGE	
										28			603	04400	28	FT	12" CONDUIT, TYPE B	
										5			603	04600	5	FT	12" CONDUIT, TYPE C	
										2			604	02000	2	EACH	CATCH BASIN, NO. 6	
						838							605	31101	838	FT	AGGREGATE DRAINS, AS PER PLAN	9
																	PAVEMENT	
						419							253	02000	419	CU YD	PAVEMENT REPAIR	9
		16,756	57,954	1,549	4,646								254	01000	80,905	SQ YD	PAVEMENT PLANING, ASPHALT CONCRETE	
										4			301	46000	4	CU YD	ASPHALT CONCRETE BASE, PG 64-42	
										3			304	20000	3	CU YD	AGGREGATE BASE	
		8,160	5,627	1,314	605								407	10000	15,706	GALLON	TACK COAT	
		4,353	3,003	697	322								407	14000	8,375	GALLON	TACK COAT FOR INTERMEDIATE COURSE	
				6,974	2,487								408	10001	9,461	GALLON	PRIME COAT, AS PER PLAN	3
		460	761	47	64								448	46020	1,332	CU YD	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE I, PG64-22 (SPOT LEVELING)	2
		3,675	985	682	165								448	46050	5,507	CU YD	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, PG64-22	
		3,778	2,606	609	280								448	46905	7,273	CU YD	ASPHALT CONCRETE SURFACE COURSE, TYPE I, PG70-22M, AS PER PLAN	2
		635	252										448	48021	887	CU YD	ASPHALT CONCRETE SURFACE COURSE, TYPE I, PG64-22 (DRIVEWAYS), AS PER PLAN	2
				1,212	433								617	10101	1,645	CU YD	COMPACTED AGGREGATE, AS PER PLAN	3
				17,435	6,218								617	20000	23,653	SQ YD	SHOULDER PREPARATION	
																	TRAFFIC CONTROL	
									1586				621	00100	1,586	EACH	RPM	
		689	318										621	54000	1,007	EACH	RAISED PAVEMENT MARKER REMOVED	
									23.98				642	00100	23.98	MILE	EDGE LINE, TYPE I	
									11.99				642	00300	11.99	MILE	CENTER LINE, TYPE I	
									194				642	00500	194	FT	STOP LINE, TYPE I	
									612				642	00600	612	FT	CROSSWALK LINE, TYPE I	
									2				642	01100	2	EACH	SCHOOL SYMBOL MARKING, 72", TYPE I	
									1				642	01110	1	EACH	SCHOOL SYMBOL MARKING, 96", TYPE I	
																	MAINTENANCE OF TRAFFIC	
	40												614	11110	40	HOUR	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	
128													614	12460	128	EACH	WORK ZONE MARKING SIGN	
28.14													614	21100	28.14	MILE	WORK ZONE CENTER LINE, CLASS I, 642 PAINT	
LUMP													614	11000	LUMP		MAINTAINING TRAFFIC	
													624	10000	LUMP		MOBILIZATION	

CALCULATED JAC
CHECKED SKW

GENERAL SUMMARY

TUS-93-1.08

BRIDGE TREATMENT

STR. NO. TUS-93-0156 - SAME AS MAINLINE TREATMENT

STR. NO. TUS-93-0205 - SAME AS MAINLINE TREATMENT

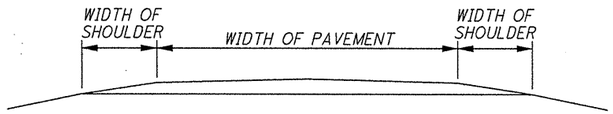
BR. NO. TUS-93-0336 - PLANE 1.25 IN, REPLACE 1.25 IN SURFACE COURSE

BR. NO. TUS-93-0483 - OMIT

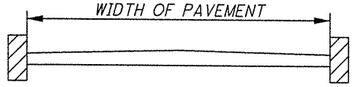
BR. NO. TUS-93-0802 - OMIT

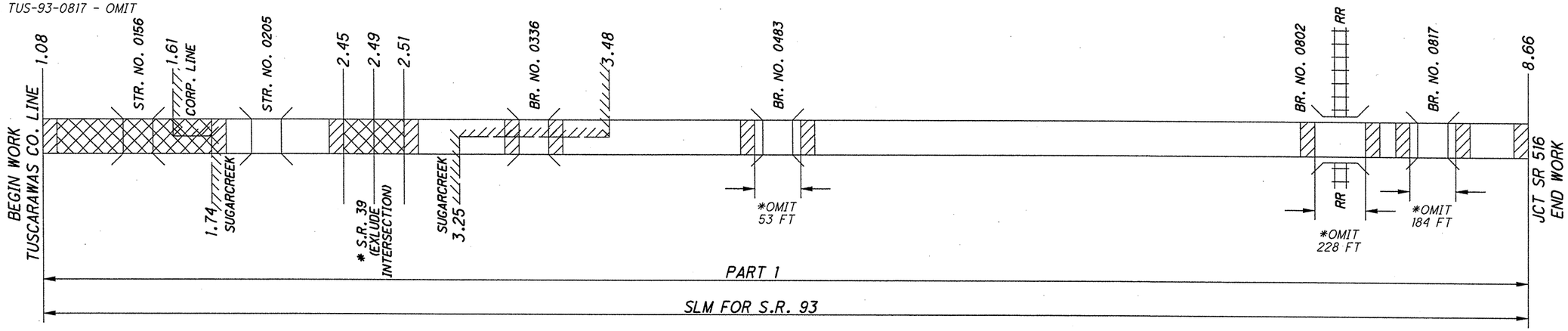
BR. NO. TUS-93-0817 - OMIT

TYPICAL 1



TYPICAL 2





ITEM 202 - WEARING COURSE REMOVED PER BP-3.1

ITEM 254 - Pavement Planing Asphalt Concrete

PAVEMENT DATA

										PROPOSED PAVEMENT														
										407	448									621	202	254		
Part	Route	Log Point to Log Point (Straight Line Mileage)		Mile	Feet	Width of Pavement	Typical	Existing Type Pavement	Pavement Area	Tack Coat @ 0.075 gal/s.y.	Tack Coat For Intermediate Course @ 0.04 gal/s.y.	In.	Asphalt Concrete Surface Course Type 1, PG70-22M, As Per Plan	In.	Asphalt Concrete Intermediate Course, Type 2, PG 64-22	In.	Asphalt Concrete Intermediate Course, Type 1, PG 64-22 (Spot Leveling)	In.	Asphalt Concrete Surface Course Type 1, PG64-22 (Driveways), As Per Plan	Raised Pavement Marker Removed	Wearing Course Removed	Pavement Planing Asphalt Concrete 1 1/2"	Pavement Planing Asphalt Concrete 1 3/4"	REMARKS
		From	To			Feet			Sq. Yards	Gal	Gal		Cu. Yards		Cu. Yards		Cu. Yards		Cu. Yards	Each	Sq. Yards	Sq. Yards	Sq. Yards	
1	SR 93	1.08 *	1.74 *	0.66	3,485	22	1	448	8,358	627	334	1 1/4	290	1 3/4	406							8,358		
		1.74	2.45	0.71	3,749	22	1	448	9,164	687	367	1 1/4	318			1/2	127				253			
		2.45	2.51	0.06	317	48	2	448	1,691	127	68	1 1/4	59			1/2	23						1,691	
		2.51	3.36	0.85	4,488	22	1	448	10,971	823	439	1 1/4	381			1/2	152				253			
		3.36 *	8.66 *	5.21	27,509	22	1	448	67,244	5,043	2,690	1 1/4	2,335	1 3/4	3,269					689	2,427			
		Extra for Aggregate Drives			2,340	10			2,600									2	144					
		Extra for Paved Drives			7,975	10			8,861									1 1/4	308			3,983		
		Extra for Paved Public Roads			2,375	20			5,278									1 1/4	183			1,644		
		Extra for Mailbox Turnouts		90 Ea x 20 Sq. Yd.					1,800	135	72	1 1/4	63			1/2	25					171		
		Extra for Widening (10%)							9,574	718	383	1 1/4	332			1/2	133					909		
TOTAL PART 1 (CARRIED TO GENERAL SUMMARY)										8,160	4,353		3,778		3,675		460		635	689	2,932	16,756		

CALCULATED JAC CHECKED SKW

PAVEMENT DATA

TUS-93-1.08

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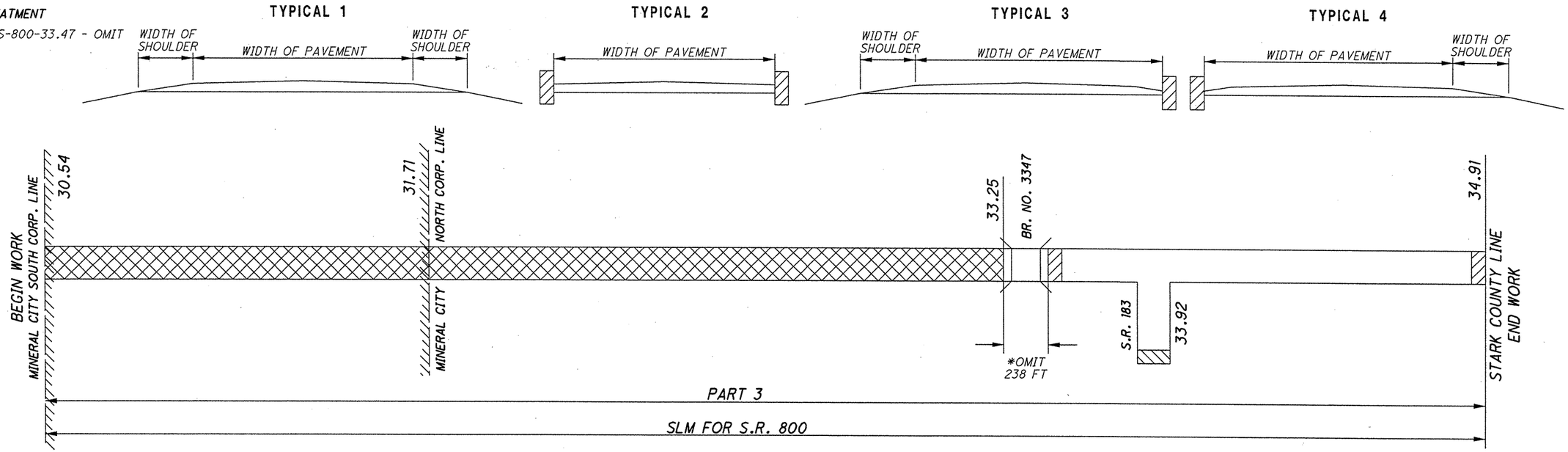
BRIDGE TREATMENT

BR. NO. TUS-800-33.47 - OMIT



ITEM 202 - WEARING COURSE REMOVED PER BP-3.1

ITEM 254 - Pavement Planing Asphalt Concrete

PAVEMENT DATA

										PROPOSED PAVEMENT															
										407		448								621		202	254	
Part	Route	Log Point to Log Point (Straight Line Mileage)		Mile	Feet	Width of Pavement	Typical	Existing Type Pavement	Pavement Area	Tack Coat @ 0.075 gal/s.y.	Tack Coat For Intermediate Course @ 0.04 gal/s.y.	In.	Asphalt Concrete Surface Course Type 1, PG70-22M, As Per Plan	In.	Asphalt Concrete Intermediate Course, Type 2, PG 64-22	In.	Asphalt Concrete Intermediate Course, Type 1, PG 64-22 (Spot Leveling)	In.	Asphalt Concrete Surface Course Type 1, PG64-22 (Driveways), As Per Plan	Raised Pavement Marker Removed		Wearing Course Removed	Pavement Planing Asphalt Concrete 1 3/4"	REMARKS
		From	To			Feet			Sq. Yards	Gal	Gal		Cu. Yards		Cu. Yards		Cu. Yards		Cu. Yards	Each		Sq. Yards	Sq. Yards	
2	SR 183	0.00	0.04	0.04	211	24	1	448	563	42	23	1 1/4	20	1 3/4	27					6		327		
3	SR 800	30.54	30.76	0.22	1,162	24	1	448	3,099	232	124	1 1/4	108			1/2	43						3,099	
		30.76	31.71	0.95	5,016	24	2	448	13,376	1,003	535	1 1/4	464			1/2	186						13,376	
		31.71	32.53	0.82	4,330	40	3	448	19,244	1,443	770	1 1/4	668			1/2	267						19,244	
		32.53	32.75	0.22	1,162	24	1	448	3,099	232	124	1 1/4	108			1/2	43			30		136	3,099	
		32.75	33.47	0.72	3,802	24	4	448	10,139	760	406	1 1/4	352			1/2	141			96			10,139	
		33.47 *	34.91 *	1.40	7,392	24	1	448	19,712	1,478	788	1 1/4	684	1 3/4	958					186		980		
		Extra for Aggregate Drives			1,245	10			1,383									2	77					
		Extra for Paved Drives			1,360	10			1,511									1 1/4	52				1,405	
		Extra for Paved Public Roads			1,355	20			3,011									1 1/4	105				2,244	
		Extra for Aggregate Public Roads			145	20			322									2	18					
		Extra for Mailbox Turnouts 12 Ea x 20 Sq. Yd.							240	18	10	1 1/4	8			1/2	3						161	
		Extra for Widening (10%)							5,586	419	223	1 1/4	194			1/2	78						3,745	
TOTAL PART 2 AND 3 (CARRIED TO GENERAL SUMMARY)										5,627	3,003		2,606		985		761		252	318		1,443	57,954	

CALCULATED JAC CHECKED SKW

PAVEMENT DATA

TUS-93-1.08

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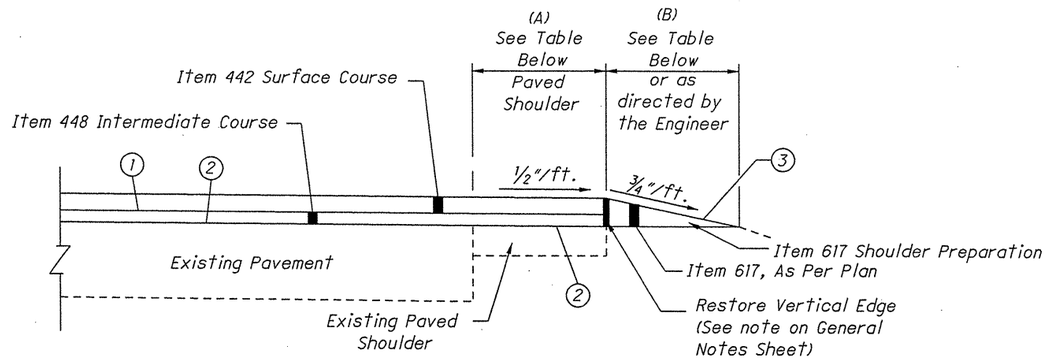
* OMIT Br. No. TUS-93-0483 (See Sheet 5)
* OMIT Br. No. TUS-93-0802 (See Sheet 5)
* OMIT Br. No. TUS-93-0817 (See Sheet 5)



1. TYPICAL PAVED SHOULDER DETAIL

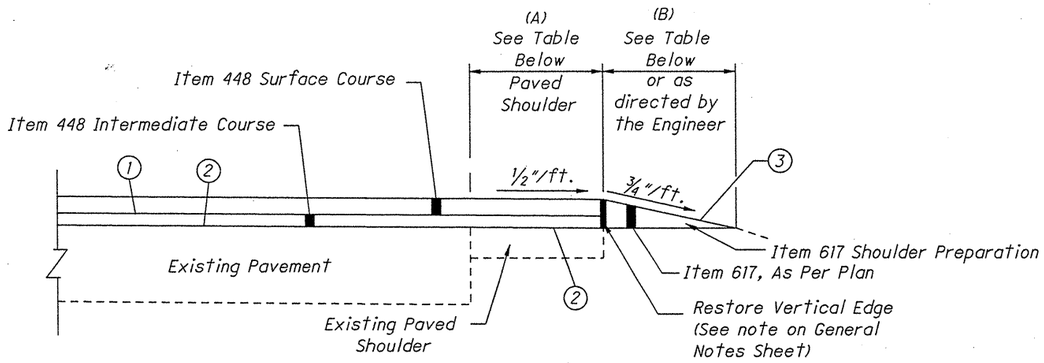
ITEM 407
① Tack Coat for Intermediate Course
② Tack Coat
ITEM 408
③ Prime Coat

For additional information regarding shoulder construction see the General Notes.

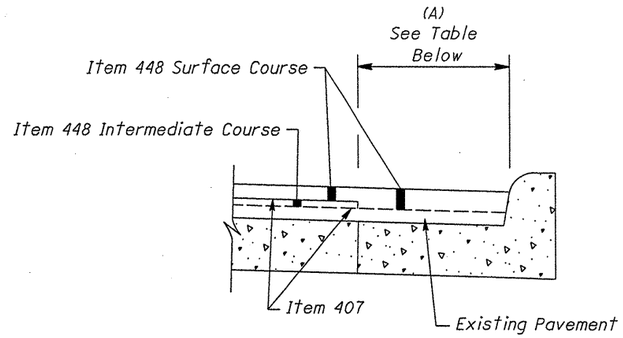
SHOULDER QUANTITIES

Part	Route	Log Point to Log Point (Straight Line Mileage)		Mile	Feet	TYPICAL	Proposed Width Feet				Shoulder Area	448						407		408	617		254		REMARKS
							Left		Right			In.	Asphalt Concrete Intermediate Course, Type 1, PG 64-22 (Spot Leveling)	In.	Asphalt Concrete Intermediate Course, Type 2, PG 64-22	In.	Asphalt Concrete Surface Course, Type 1, PG70-22M As Per Plan	Tack Coat @ 0.075 gal/s.y.	Tack Coat For Intermediate Course @ 0.04 gal/s.y.	Prime Coat, As Per Plan @ 0.40 gal/s.y.	Compacted Aggregate, As Per Plan 2 1/2"	Shoulder Preparation	Pavement Planing Asphalt Concrete 1 1/2"	Pavement Planing Asphalt Concrete 1 3/4"	
		From	To				A	B	A	B	Sq. Yards		Cu. Yards				Cu. Yards	Gal.	Gal.	Gal.	Cu. Yards.	Sq. Yards	Sq. Yards	Sq. Yards	
1	SR 93	1.08	1.74	0.66	3,485	1	2		2		1,549			1 3/4	75	1 1/4	54	116	62				1549		
								2		2	1,549									620	108	1,549			
		1.74	2.45	0.71	3,749	1	2		2		1,666	1/2	23			1 1/4	58	125	67						
								2		2	1,666									666	116	1,666			
		2.51	3.25	0.74	3,907	1	2		2		1,736	1/2	24			1 1/4	60	130	69						
								2		2	1,736									694	121	1,736			
		3.25 *	8.66 *	5.32	28,090	1	2		2		12,484			1 3/4	607	1 1/4	433	936	499						
								2		2	12,484									4,994	867	12,484			
	Extra for Extended Paved Shoulder				264		3				88					1 1/2	4	7							
TOTAL PART 1 (CARRIED TO GENERAL SUMMARY)													47		682		609	1,314	697	6,974	1,212	17,435	1,549		

* OMIT Br. No. TUS-800-3347 (See Sheet 6)



1. TYPICAL PAVED SHOULDER DETAIL



2. PAVED GUTTER DETAIL

ITEM 407
① Tack Coat for Intermediate Course
② Tack Coat
ITEM 408
③ Prime Coat

For additional information regarding shoulder construction see the General Notes.

SHOULDER QUANTITIES

Part	Route	Log Point to Log Point (Straight Line Mileage)		Mile	Feet	TYPICAL	Proposed Width Feet				Shoulder Area	448						407		408	617			254	
												In.	Asphalt Concrete Intermediate Course, Type 1, PG 64-22 (Spot Leveling)	In.	Asphalt Concrete Intermediate Course, Type 2, PG 64-22	In.	Asphalt Concrete Surface Course, Type 1, PG70-22M As Per Plan	Tack Coat @ 0.075 gal/s.y.	Tack Coat For Intermediate Course @ 0.04 gal/s.y.	Prime Coat, As Per Plan @ 0.40 gal/s.y.	Compacted Aggregate, As Per Plan 2 1/2"	Shoulder Preparation		Pavement Planing Asphalt Concrete 1 3/4"	REMARKS
		From	To				A	B	A	B	Sq. Yards		Cu. Yards				Cu. Yards	Gal.	Gal.	Gal.	Cu. Yards.	Sq. Yards		Sq. Yards	
2	SR 183	0.00	0.04	0.04	211	1	2		2		94			1 3/4	5	1 1/4	3	7	4						
								2		2	94									38	7	94			
3	SR 800	30.54	30.76	0.22	1,162	1	2		2		516	1/2	7			1 1/4	18	39	21					516	
								2		2	516									206	36	516			
		31.71	32.53	0.82	4,330	1 & 2	2		2		1,924	1/2	27			1 1/4	67	144	77					1,924	
								2		0	962									385	67	962			
		32.53	32.75	0.22	1,162	1	2		2		516	1/2	7			1 1/4	18	39	21					516	
								2		2	516									206	36	516			
		32.75	33.47	0.72	3,802	2 & 1	2		2		1,690	1/2	23			1 1/4	59	127	68					1,690	
								0		2	845									338	59	845			
		33.47 *	34.91 *	1.40	7,392	1	2		2		3,285			1 3/4	160	1 1/4	114	246	131						
								2		2	3,285									1,314	228	3,285			
	Extra for Extended Paved Shoulder				106			3			35					1 1/2	1	3							
TOTAL PART 2 AND 3 (CARRIED TO GENERAL SUMMARY)													64		165		280	605	322	2,487	433	6,218		4,646	

CALCULATED JAC CHECKED SKW

PAVED SHOULDER DATA

TUS-93-1.08

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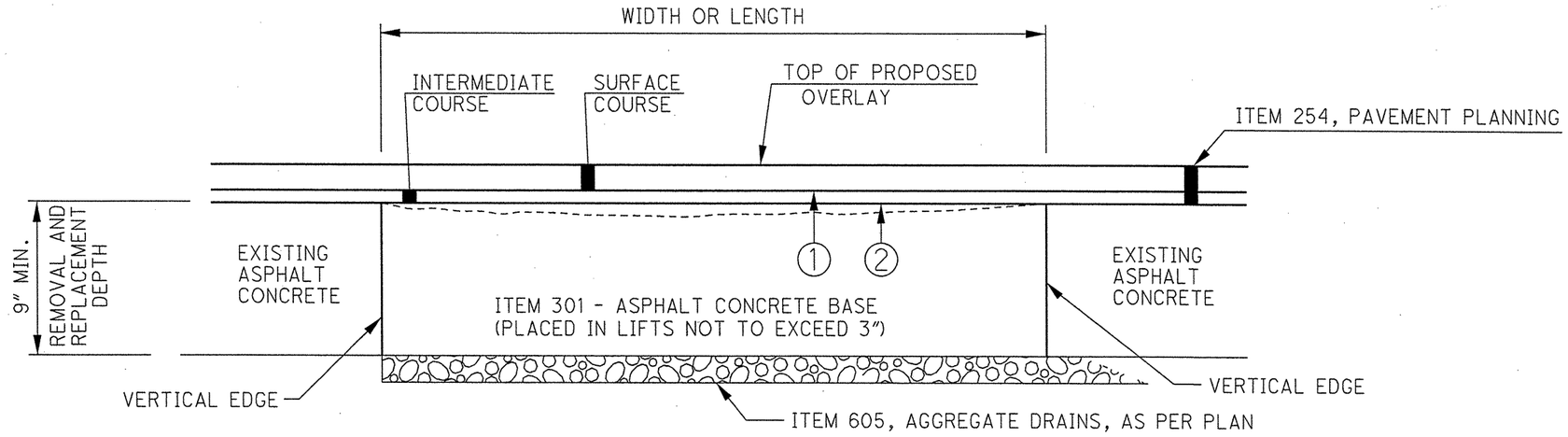
LEGEND

① ITEM 407, TACK COAT FOR INTERMEDIATE COURSE

② ITEM 407, TACK COAT



PAVEMENT REPAIR TYPICAL

ITEM 253 - PAVEMENT REPAIR

THE PAVEMENT REPAIR LOCATIONS AND ESTIMATED QUANTITIES WERE OBTAINED BY PRELIMINARY FIELD REVIEW AND SHALL BE CONSIDERED APPROXIMATE. A FINAL FIELD REVIEW WILL BE PERFORMED BY ODOT PRIOR TO CONSTRUCTION AND FINAL LOCATIONS WILL BE GIVEN TO THE CONTRACTOR AT THE PRE-CONSTRUCTION CONFERENCE.

THIS WORK CONSISTS OF REMOVING EXISTING ASPHALT CONCRETE, BRICK, PORTLAND CEMENT CONCRETE, OR AGGREGATE PAVEMENT COURSES; SHAPING AND COMPACTING THE EXPOSED MATERIAL; AND PLACING NEW ASPHALT CONCRETE PAVEMENT OR AGGREGATE AND ASPHALT CONCRETE PAVEMENT COURSES.

AN AGGREGATE DRAIN, AS PER PLAN SHALL BE INSTALLED IN ACCORDANCE WITH CMS 605.07.

THE ABOVE ESTIMATED QUANTITY IS TO BE USED AS DIRECTED BY THE ENGINEER. FINAL PAYMENT FOR THE ABOVE ITEMS SHALL BE FOR THE ACCEPTED QUANTITY COMPLETE IN PLACE.

ITEM 605 - AGGREGATE DRAINS, AS PER PLAN

THIS ITEM SHALL BE USED IN ACCORDANCE WITH ITEM 253, PAVEMENT REPAIR AND ITEM 605.07 OF THE CMS. THE AGGREGATE SHALL BE NO. 57 SIZE GRAVEL, UNLESS OTHERWISE DIRECTED BY THE ENGINEER.

THE ABOVE ESTIMATED QUANTITY IS TO BE USED AS DIRECTED BY THE ENGINEER. FINAL PAYMENT FOR THE ABOVE ITEMS SHALL BE FOR THE ACCEPTED QUANTITY COMPLETED IN PLACE.

ESTIMATED QUANTITIES

PART 1, ITEM 253 . . . PAVEMENT REPAIR - 303 CU YD
PART 3, ITEM 253 . . . PAVEMENT REPAIR - 116 CU YD
419 CU YD TOTAL

PART 1, ITEM 605 . . . AGGREGATE DRAINS, AS PER PLAN - 606 FT
PART 3, ITEM 605 . . . AGGREGATE DRAINS, AS PER PLAN - 232 FT
838 FT TOTAL

(TOTALS CARRIED TO GENERAL SUMMARY)

CALCULATED JAS CHECKED SKW

PAVEMENT REPAIR

TUS-93-1.08

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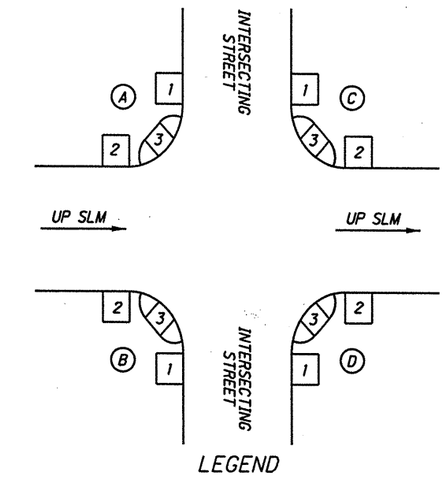
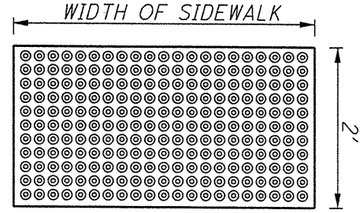
PART	ROUTE	INTERSECTION	LOCATION (SEE LEGEND)	202			608				609	
				CURB REMOVED	WALK REMOVED	CURB RAMP	TYPE * SEE BP 7.1	DIMENSION A	DETECTABLE WARNING	6" CONCRETE WALK	CURB, TYPE 6	WIDTH OF SIDEWALK
				FT	SQ FT	SQ FT		FT	SQ FT	SQ FT	FT	(FEET)
1	SR 93	FREDERICK ST.	B-1		16				8	8		4
↓	↓	FREDERICK ST.	D-1		16				8	8		4
		SHANE ST.	A-1		16				8	8		4
		SHANE ST.	C-1	6	44				8	36		4
		SHANE ST	B-1		16				8	8		4
		SHANE ST	D-1		25				10	15		5
		WALNUT DR.	D-1		24				8	16		4
		WALNUT DR.	A-1		16				8	8		4
		WALNUT DR.	C-1		16				8	8		4
		CHESTNUT DR.	C-1		16				8	8		4
		CHESTNUT DR.	A-1		20				8	12		4
		MAPLE ST.	B-2		132	75	A1*			57		4
		MAPLE ST.	A-2		28				8	20		4
		CHURCH ST.	A-1		24				8	16		4
		CHURCH ST.	C-1		16				8	8		4
		FENDER ST. (AFTER PARKING LOT)	A-1		16				8	8		4
		FENDER ST. (AFTER PARKING LOT)	C-1						8	8		4
↓	↓	CHERRY RUN	A-1		16				8	8		4
1	SR 93	CHERRY RUN	C-1		16				8	8		4
		TOTALS CARRIED TO GENERAL SUMMARY		6	473	75			146	268		

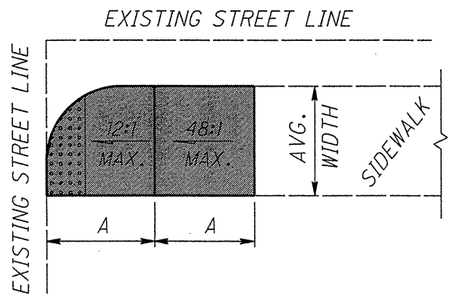
FOR DETAILS SEE SCD BP-7.1



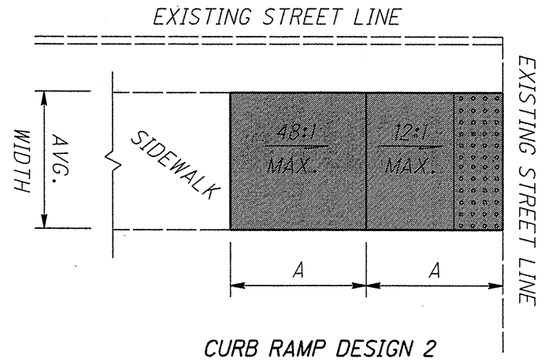
LEGEND



DETAIL
DETECTABLE WARNING



CURB RAMP DESIGN 1



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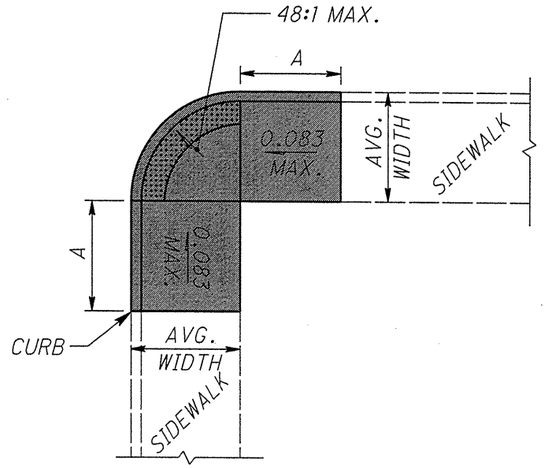

CURB RAMP DESIGN 3

CALCULATED JAC
CHECKED SKW

CURB RAMP SUB-SUMMARY

TUS-93-1.08

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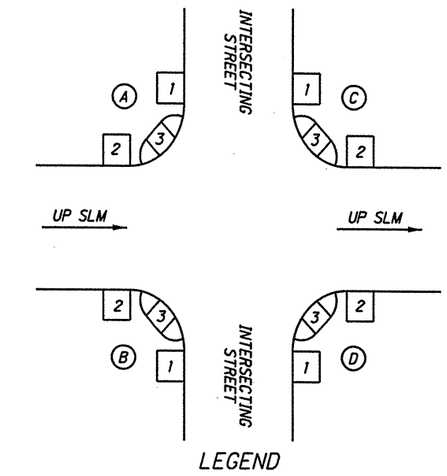
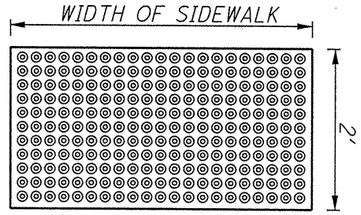
PART	ROUTE	INTERSECTION	LOCATION (SEE LEGEND)	202		608					609	WIDTH OF SIDEWALK
				CURB REMOVED	WALK REMOVED	CURB RAMP	TYPE * SEE BP 7.1	DIMENSION A	DETECTABLE WARNING	6" CONCRETE WALK	CURB, TYPE 6	
				FT	SQ FT	SQ FT		FT	SQ FT	SQ FT	FT	(FEET)
3	SR 800	RAILROAD ST.	B-1		42				12	30		6
↑	↑	RAILROAD ST.	D-1		25				10	15		5
		MINERS ST.	B-1		33				10	20		5
		MINERS ST.	D-1	12	90				10	80	7	5
		SHORT ST.	D-1	9	52	52	B3*					4
		SHORT ST.	B-1	8	75	75	B3*					5
		CENTER ST.	A-3	8	100	100	3	6				5
		CENTER ST.	C-1	10	78	78	B3*					5
		DAVIS ST.	B-1	7	50	50	2	5				5
		DAVIS ST.	D-1	7	50	50	2	5				5
		DIVISION ST.	B-1		25				10	15		5
		DIVISION ST.	D-1		25				10	15		5
		AT SLM 31.21 (CROSSWALK)	A-2			55	A2*					5
		AT SLM 31.21 (CROSSWALK)	B-2	6		50	A2*					5
		GRANT ST.	B-1		35				10	25		5
		GRANT ST.	D-1		20				10	10		5
↓	↓	GRANT ST.	A-1		25				10	15		5
3	SR 800	GRANT ST.	C-1		75	75	2	7.5				5
		TOTALS CARRIED TO GENERAL SUMMARY		67	800	585			92	225	7	

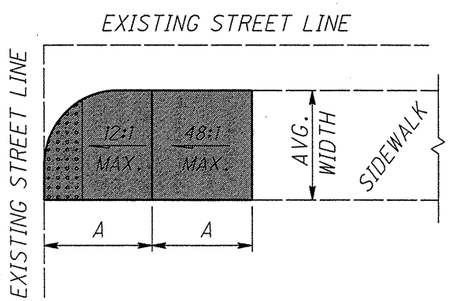
FOR DETAILS NOT SHOWN SEE SCD BP-7.1.



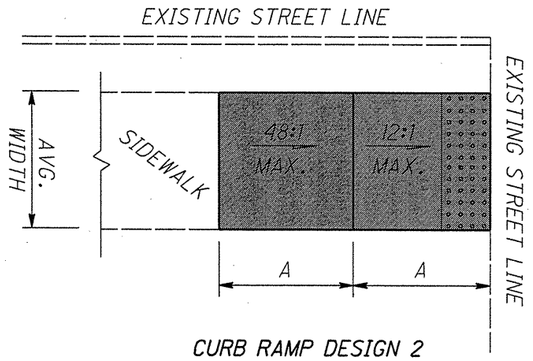
LEGEND



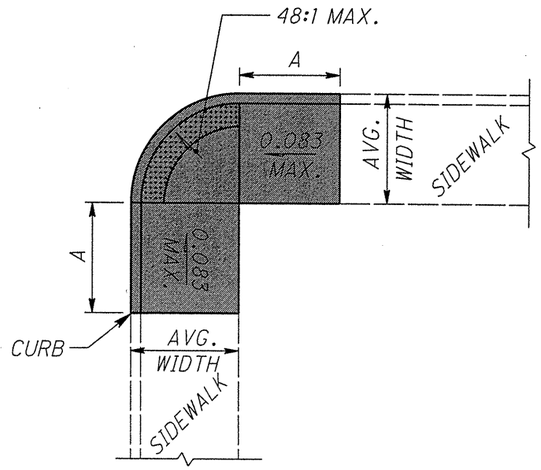
DETAIL
DETECTABLE WARNING



CURB RAMP DESIGN 1



CURB RAMP DESIGN 2



CURB RAMP DESIGN 3

CALCULATED JAC
CHECKED SKW

CURB RAMP SUB-SUMMARY

TUS-93-1.08

11
15

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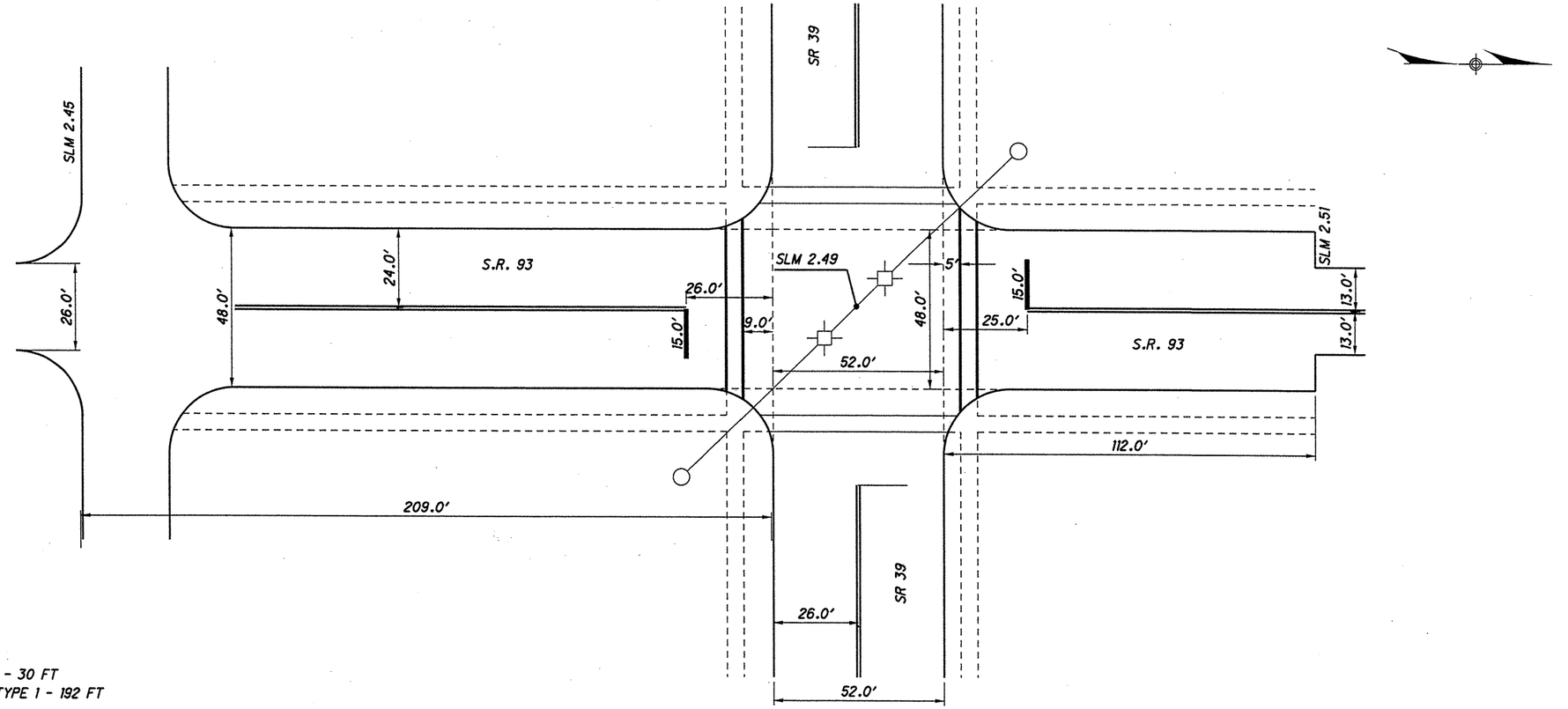
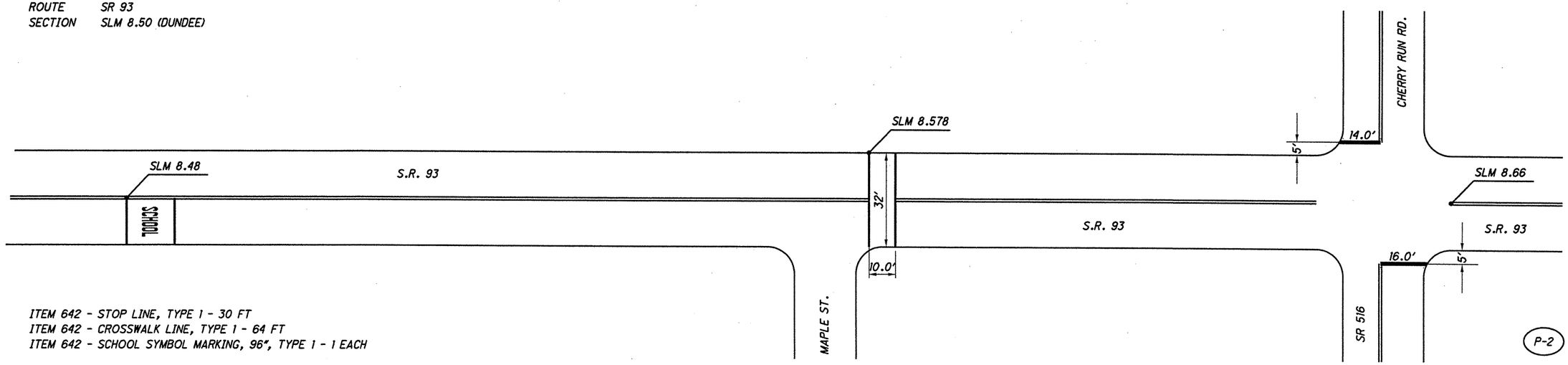
COUNTY TUS
ROUTE SR 93
SECTION SLM 2.49



ITEM 642 - STOP LINE, TYPE 1 - 30 FT
ITEM 642 - CROSSWALK LINE, TYPE 1 - 192 FT

P-1

COUNTY TUS
ROUTE SR 93
SECTION SLM 8.50 (DUNDEE)



ITEM 642 - STOP LINE, TYPE 1 - 30 FT
ITEM 642 - CROSSWALK LINE, TYPE 1 - 64 FT
ITEM 642 - SCHOOL SYMBOL MARKING, 96", TYPE 1 - 1 EACH

P-2

Notes: 1. For pavement marking details not shown, see Standard Construction Drawing TC-71.10.

2. All quantities carried to Pavement Marking Sub-Summary, Sheet 14.

CALCULATED JAC CHECKED SKW

AUXILIARY PAVEMENT MARKING

TUS-93-1.08

12/15

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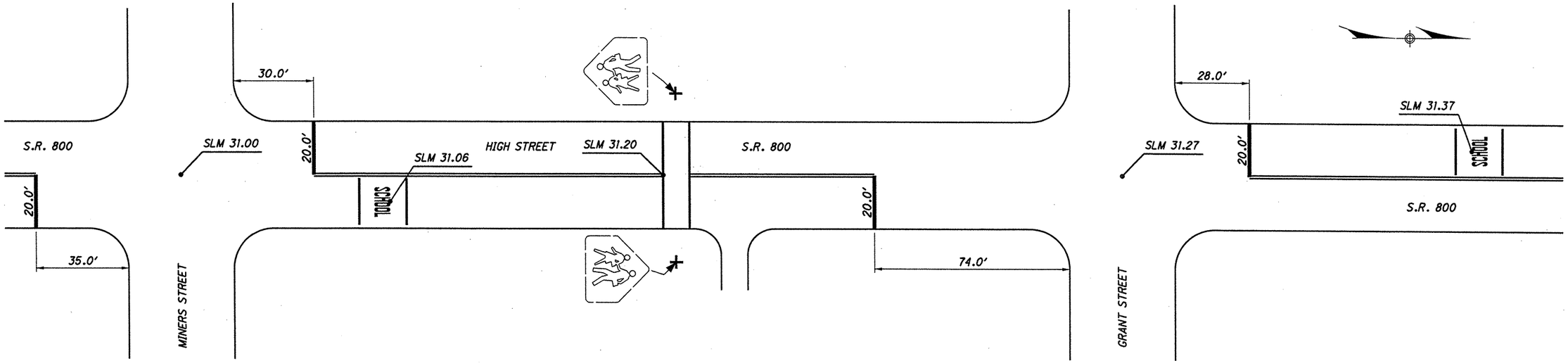
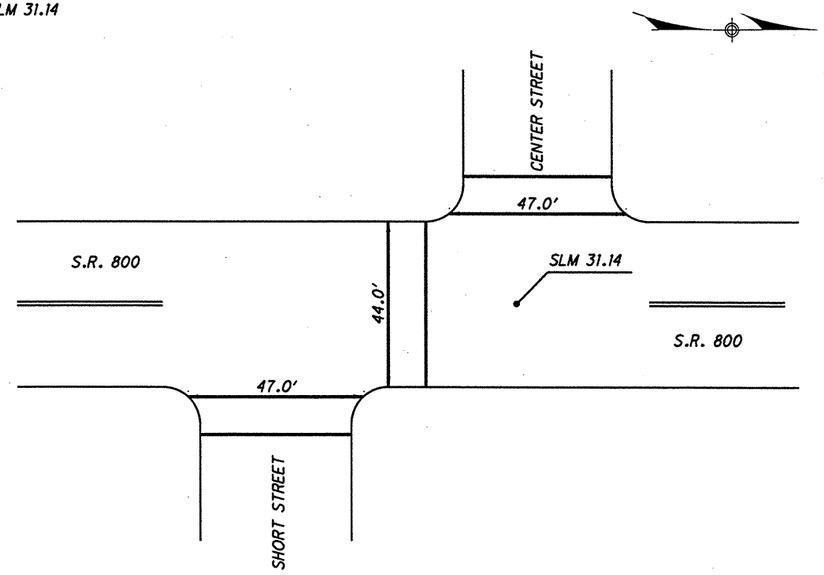
COUNTY TUS
ROUTE SR 800
SECTION SLM 31.00 (MINERAL CITY)



ITEM 642 - STOP LINE, TYPE 1 - 80 FT
ITEM 642 - CROSSWALK LINE, TYPE 1 - 80 FT
ITEM 642 - SCHOOL SYMBOL MARKING, 72", TYPE 1 - 2 EACH

P-3

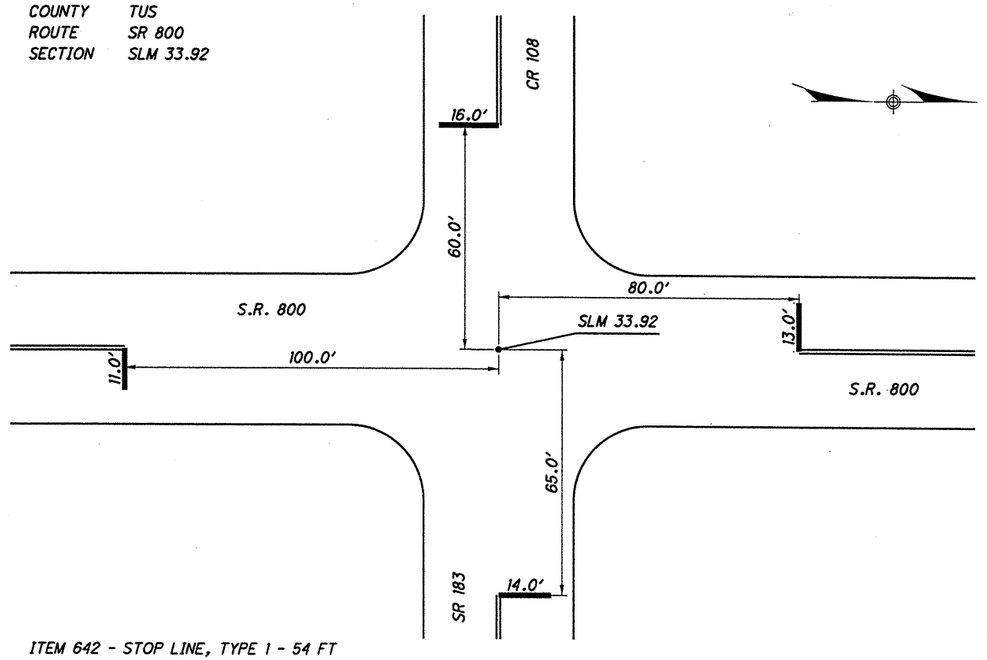
COUNTY TUS
ROUTE SR 800
SECTION SLM 31.14



ITEM 642 - CROSSWALK LINE, TYPE 1 - 276 FT

P-4

COUNTY TUS
ROUTE SR 800
SECTION SLM 33.92



ITEM 642 - STOP LINE, TYPE 1 - 54 FT

P-5

Notes: 1. For pavement marking details not shown, see Standard Construction Drawing TC-71.10.

2. All quantities carried to Pavement Marking Sub-Summary, Sheet 14.

CALCULATED JAC
CHECKED SKW

AUXILIARY PAVEMENT MARKING

TUS-93-1.08

13
15

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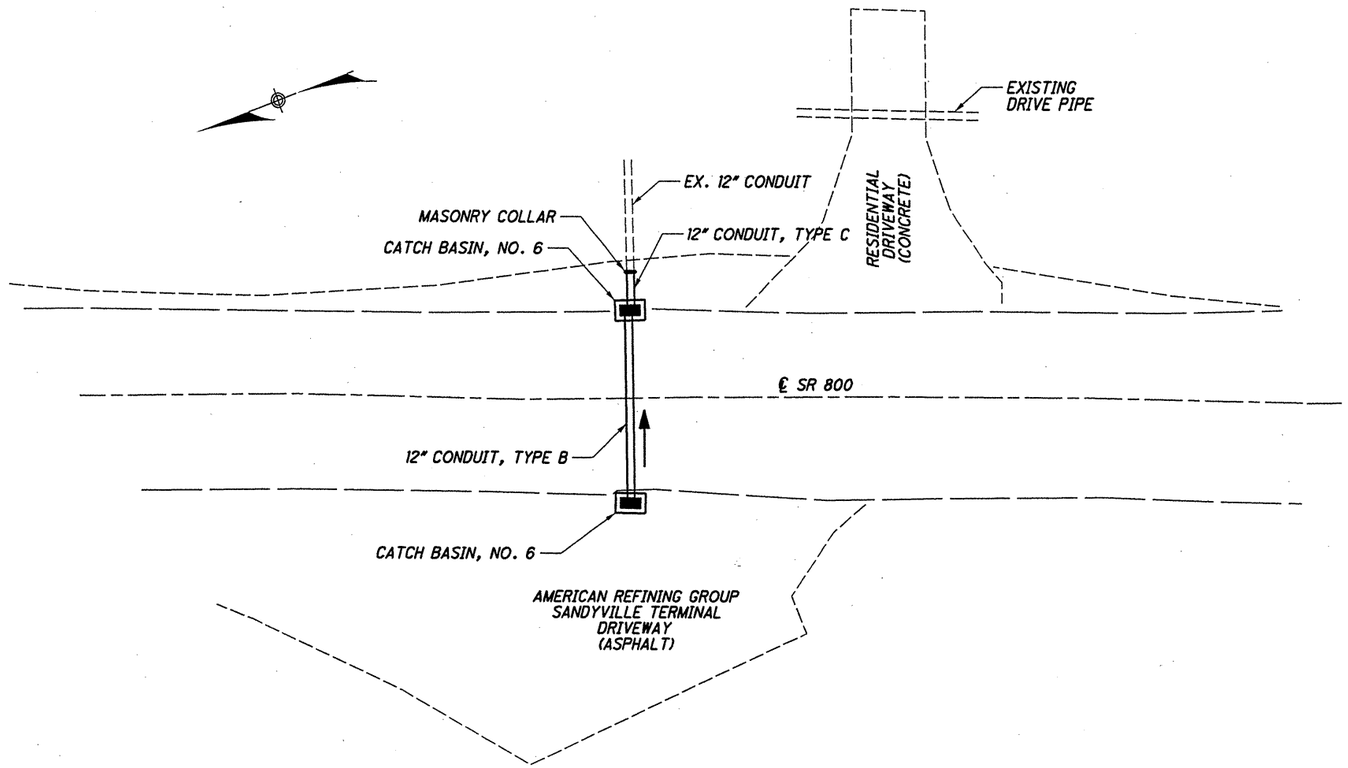
SHEET NO.	REFERENCE NO.	SLM FROM	SLM TO	PART	SPACING FT.	621 RPM YELLOW/YELLOW EACH	621 RPM WHITE EACH	621 RPM WHITE/RED EACH			642 EDGE LINE, TYPE 1 (WHITE) MILE	642 CENTERLINE, TYPE 1 MILE	642 EQUIVALENT SINGLE SOLID LINE LENGTH (FOR ESTIMATING PURPOSES ONLY) MILE			642 STOP LINE, TYPE 1 FT	642 CROSSWALK LINE, TYPE 1 FT	642 SCHOOL SYMBOL MARKING, 96", TYPE 1 EACH	642 SCHOOL SYMBOL MARKING, 72", TYPE 1 EACH			REMARKS
		1.08	8.66	1	40	1002					15.16	7.58										
12	P-1	2.49		1												30	192					
12	P-2	2.49		1												30	64	1				
SUB-TOTAL PART 1						**1002**					**15.16**	**7.58**				**60**	**256**	**1**				
		0	0.04	2	40	6					0.08	0.04										
SUB-TOTAL PART 2						**6**					**0.08**	**0.04**										
		30.54	34.91	3	40	578					8.74	4.37										
13	P-3	31.00		3												80	80		2			
13	P-4	31.14		3													276					
13	P-5	33.92		3												54						
SUB-TOTAL PART 3						**578**					**8.74**	**4.37**				**134**	**356**		**2**			
TOTALS (CARRIED TO GENERAL SUMMARY)						**1586**					**23.98**	**11.99**				**194**	**612**	**1**	**2**			

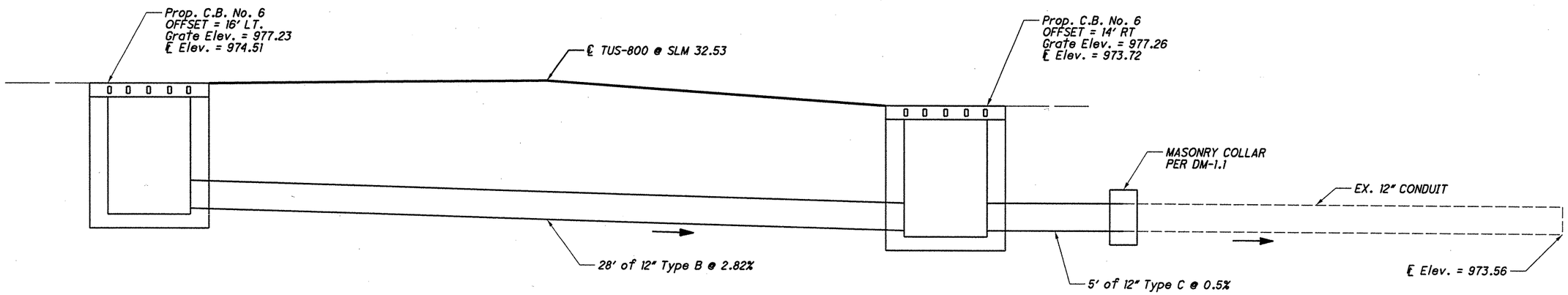
CALCULATED JAC
CHECKED SKW

RPM AND PAVEMENT MARKING SUB-SUMMARY

TUS-93-1.08



PLAN



PROFILE

LOCATION	202 PIPE REMOVED, 24" AND UNDER	202 CATCH BASIN REMOVED	202 PAVEMENT REMOVED	603 12" CONDUIT, TYPE B	603 12" CONDUIT, TYPE C	604 CATCH BASIN, NO. 6
	FT	EACH	SQ YD	FT	FT	EACH
SLM 32.53	33	2	17	28	5	2
TOTALS CARRIED TO GENERAL SUMMARY						

PAVEMENT RESTORATION FOR PIPE INSTALLATIONS AND/OR REMOVALS

THE FOLLOWING QUANTITIES, CARRIED TO THE GENERAL SUMMARY, HAVE BEEN PROVIDED FOR PAVEMENT RESTORATION FOLLOWING INSTALLATION AND/OR REMOVAL OF PIPES

ITEM 301 ASPHALT CONCRETE BASE, PG64-22 4 CU. YDS.

ITEM 304 AGGREGATE BASE, 3 CU. YDS.

THE ABOVE QUANTITY IS BASED ON A 301 THICKNESS OF 8 INCHES A 304 THICKNESS OF 6 INCHES, AND A PAVEMENT RESTORATION WIDTH THAT INCLUDES THE TRENCH WIDTH PLUS TWO FEET ON EASH SIDE OF THE TRENCH. SEE STANDARD CONSTRUCTION DRAWING DM-1.4 FOR TRENCH WIDTH FORMULA AND CALCULATION.

PROVIDE ANY MATERIALS USED OUTSIDE THE LIMITS STATED ABOVE AT NO ADDITIONAL COST.

INTERMEDIATE AND SURFACE COURSES TO BE PLACED DURING NORMAL PAVEMENT PLACING ACTIVITIES.

CALCULATED JAC
CHECKED SKW

DRAINAGE DETAILS
SLM 32.53

TUS-93-1.08

15
15