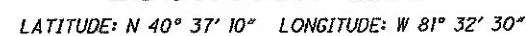


TUS-250/ VAR-0.00/ VAR
441 2-LANE RESURFACING
424 2-LANE RESURFACING

[illegible]

PROJECT EARTH DISTURBED AREA = N/A (MAINTENANCE PROJECT)
ESTIMATED CONTRACTOR EARTH DISTURBED AREA = N/A (MAINTENANCE PROJECT)
NOTICE OF INTENT EARTH DISTURBED AREA = N/A (MAINTENANCE PROJECT)

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THESE IMPROVEMENTS WILL NOT REQUIRE THE CLOSING OF THE HIGHWAYS TO TRAFFIC AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

Thomas D. Gray
DISTRICT DEPUTY DIRECTOR

DIRECTOR, DEPARTMENT OF TRANSPORTATION

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PLAN PREPARED BY:
ODOT DISTRICT II
ENGINEERING

[illegible]

SIGNED: Paul A. Brown
DATE: 6/18/20

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ITEM 614 - MAINTAINING TRAFFIC

A MINIMUM OF ONE LANE OF TRAFFIC SHALL BE MAINTAINED AT ALL TIMES PER ITEM 614 AND AS PER SCD MT-97.12. LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

THE PLANING AND RESURFACING WILL PROCEED CONTINUOUSLY A MINIMUM OF FIVE (5) DAYS PER WEEK, WEATHER PERMITTING, EXCEPT FOR THE HOLIDAYS AND EVENTS LISTED BELOW. ANY OPEN PAVEMENT TRENCH SHALL BE ADEQUATELY MAINTAINED AND PROTECTED WITH DRUMS OR PORTABLE BARRIER, PER SCD MT-101.90.

THE CONTRACTOR SHALL FURNISH, INSTALL AND MAINTAIN W8-15 "GROOVED PAVEMENT" SIGNS PER CMS 614.055.

THE CONTRACTOR SHALL FURNISH, INSTALL, AND MAINTAIN SIGNS W8-1 (48"x48") "BUMP" AND W8-2 (48"x48") "DIP" WITH W13-1P (24"x24") ADVISORY SPEED PLAQUE WITH SPEEDS APPROVED BY THE ENGINEER FOR ALL BUTT JOINT LOCATIONS, WHILE THE BUMP OR DIP CONDITION EXISTS.

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS OR EVENTS:
MEMORIAL DAY, FOURTH OF JULY, LABOR DAY
STRASBURG CORN FESTIVAL (JULY 2021)
STRASBURG OX ROAST (SEPTEMBER 2021)

ALL WORK ON U.S. 250, EXCEPT FROM SLM 0.00 TO SLM 2.22, MUST TAKE PLACE FROM 6:00PM TO 6:00AM SUNDAY NIGHT THROUGH FRIDAY MORNING.

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF THE WEEK	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM FRIDAY
THURSDAY (THANKSGIVING ONLY)	6:00 AM WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF \$5857 FOR EACH HOUR THE ABOVE DESCRIBED LANE CLOSURE RESTRICTIONS ARE VIOLATED.

WHEN RAISED PAVEMENT MARKERS ARE TO BE INSTALLED, THE REQUIRED LANE CLOSURE SHALL REMAIN IN EFFECT UNTIL THE EPOXY IS DRY AND ALL FOREIGN MATTER OR DEBRIS CREATED BY THE INSTALLATION OF THE RPM CASTING IS REMOVED FROM THE ROADWAY.

PEDESTRIAN WALKWAYS CONSTRUCTED BY THE CONTRACTOR SHALL BE KEPT FREE OF ANY OBSTRUCTIONS OR HAZARDS INCLUDING HOLES, DEBRIS AND MUD. OTHER WALKWAYS DAMAGED OR DIRTIED BY THE CONTRACTOR SHALL BE IMMEDIATELY REPAIRED OR CLEANED. THE CONTRACTOR MUST TAKE PRECAUTIONS TO PROTECT PEDESTRIANS FROM EXPOSURE TO HAZARDS RESULTING FROM THE CONSTRUCTION OPERATION BY INSTALLING CONSTRUCTION FENCE AND SIGNING.

TEMPORARY ORANGE PLASTIC CONSTRUCTION FENCE HAS BEEN PROVIDED HEREIN AS A TRAFFIC CONTROL DEVICE TO DIVERT AND GUIDE PEDESTRIANS WHOSE PATH WOULD OTHERWISE ENTER THE WORK AREA. THE TEMPORARY ORANGE PLASTIC CONSTRUCTION FENCE SHALL BE PLACED AROUND THE SIDEWALK WORK AREAS. SIDEWALK CLOSED SIGN (R9-9 (30 X 18)) MOUNTED ON A TYPE 3 BARRICADE WITH TYPE A FLASHING LIGHTS SHALL BE PLACED OUTSIDE THE FENCE ON EACH SIDEWALK APPROACH AS SHOWN ON SCD MT-101.60.

ITEM 614 - MAINTAINING TRAFFIC (CONTINUED)

FOR REPAIR OR RECONSTRUCTION WORK INVOLVING SIDEWALKS ON BOTH SIDES OF THE STREET, THE WORK SHALL BE STAGED SO THAT ONE SIDE IS COMPLETED AND OPEN TO PEDESTRIAN TRAFFIC BEFORE THE OTHER IS DISRUPTED.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

UTILITIES

THERE ARE NO UNDERGROUND UTILITIES SHOWN ON THIS PLAN. THE NATURE OF THE WORK REQUIRED BY THIS PROJECT WILL NOT AFFECT ANY KNOWN UNDERGROUND UTILITIES THAT EXIST UNDER OR ADJACENT TO THE WORK AREA.

SURFACE COURSE COMPLETION REQUIREMENTS

ANY GIVEN LENGTH OF WORK ON WHICH RESURFACING OPERATIONS HAVE BEEN STARTED IN A CONSTRUCTION SEASON SHALL HAVE THE SURFACE COURSE PLACED THAT SAME SEASON.

SHIELD

THE CONTRACTOR SHALL PROVIDE A SHIELD TO PREVENT THE SPRAYING OR DRIFTING OF LIQUID BITUMINOUS MATERIAL ONTO THE EDGE OF THE PAVEMENT OR EDGELINE. THE CONTRACTOR SHALL ADHERE TO THE PROVISIONS SET FORTH IN CMS 107.10 CONCERNING THE PROTECTION AND RESTORATION OF ALL PUBLIC AND PRIVATE PROPERTY IMPACTED BY CONSTRUCTION OPERATIONS.

PROFILE AND ALIGNMENT

PLACE THE PROPOSED PAVEMENT TO FOLLOW THE PROFILE AND ALIGNMENT OF THE EXISTING PAVEMENT. PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY AS SHOWN ON THE TYPICAL SECTIONS.

ITEM 646 - EPOXY PAVEMENT MARKINGS

THE CONTRACTOR SHALL REPLACE THE EXISTING PAVEMENT MARKINGS WITHIN THE PROJECT LIMITS WITH NEW PAVEMENT MARKINGS AT THE SAME LOCATIONS AS PER CMS 641.06. SEE STANDARD DRAWING TC-71.10 FOR PAVEMENT MARKING DETAILS.

ITEM 617 - SHOULDER PREPARATION

THIS ITEM OF WORK SHALL BE PERFORMED IN ACCORDANCE WITH CMS "ITEM 617 - RECONDITIONING SHOULDERS" WITH SPECIAL ATTENTION GIVEN TO SECTION 617.04. THE CONTRACTOR SHALL PREPARE THE SHOULDERS REASONABLY CLOSE IN CONFORMITY WITH THE DETAILS AND TYPICAL SECTIONS SHOWN IN THE PLANS, OR AS ESTABLISHED BY THE ENGINEER.

ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN

GRADED SHOULDERS SHALL BE RESHAPED AS PER THE REQUIREMENTS OF ITEM 617, COMPACTED AGGREGATE. GRINDINGS MAY BE USED IN LIEU OF ITEM 617, COMPACTED AGGREGATE. IF THE AMOUNT OF GRINDINGS ARE NOT SUFFICIENT TO COVER THE COMPACTED AGGREGATE QUANTITY REQUIRED FOR THIS PLAN, THEN ADDITIONAL MATERIAL MEETING SPECIFICATION 617 SHALL BE PROCURED AND USED BY THE CONTRACTOR. ALL GRINDINGS SHALL ADHERE TO THE SPECIFICATIONS STATED IN CMS 617 AND MEET GRADATIONS CONFORMING TO CMS 703.18. THE COST FOR STORING THE GRINDINGS ON THE PROJECT AND PLACING THE GRINDINGS SHALL ALSO BE INCLUDED IN THIS ITEM.

ITEM 614 - WORK ZONE PAVEMENT MARKINGS, CLASS II, 642 PAINT
ITEM 614 - WORK ZONE PAVEMENT MARKINGS, CLASS III, 642 PAINT
ITEM 614 - WORK ZONE MARKING SIGNS

THE CONTRACTOR SHALL INSTALL ITEM 614 - WORK ZONE CENTER LINE, CLASS II OR CLASS III, 642 PAINT PRIOR TO OPENING THE LANE TO TRAFFIC, OR WHEN THE EXISTING MARKINGS HAVE BEEN COVERED OR DAMAGED, AS PER CMS 614.11.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

WORK ZONE PAVEMENT MARKINGS TABLE						
PART	LOCATION	614				FUNDING
		WORK ZONE CENTER LINE, CLASS II, 642 PAINT	WORK ZONE CENTER LINE, CLASS III, 642 PAINT	WORK ZONE CHANNELIZING LINE, CLASS III, 8", 642 PAINT	WORK ZONE STOP LINE, CLASS III, 642 PAINT	
		MI	MI	FT.	FT.	
1	U.S. 250	12.84	12.84	2,597	562	01/NHS/PV
2	U.S. 250	0.96	0.96	90	162	01/NHS/PV
3	S.R. 21	1.47	1.47			01/NHS/PV
4	S.R. 93	0.24	0.24			02/STR/PV
5	S.R. 212	2.14	2.14		44	02/STR/PV
SUB-TOTALS PARTS 1, 2, & 3		15.27	15.27	2,687	724	01/NHS/PV
SUB-TOTALS PARTS 4&5		2.38	2.38		44	02/STR/PV
TOTALS (CARRIED TO GENERAL SUMMARY)		17.65	17.65	2,687	768	

THE CONTRACTOR SHALL ERECT "NO EDGE LINES" (W8-H12a) SIGNS IN ADVANCE OF ANY SECTION OF ROADWAY LACKING CMS STANDARD EDGE LINE MARKINGS, AS PER CMS 614.04.

IN THE EVENT THE CONTRACTOR CANNOT INSTALL THE WORK ZONE CENTER LINE, CLASS III, DUE TO CONDITIONS BEYOND HIS CONTROL, OR WHEN CLASS II PAVEMENT MARKINGS ARE USED, AN ESTIMATED CONTINGENCY QUANTITY FOR "DO NOT PASS" (R4-1) AND "PASS WITH CARE" (R4-2) SIGNS HAVE BEEN PROVIDED IN THE "WORK ZONE MARKING SIGN TABLE" WHICH SHALL BE ERECTED BY THE CONTRACTOR IN LIEU OF THE AFOREMENTIONED PAVEMENT MARKINGS. THE APPROPRIATE SIGNAGE SHALL BE PLACED AS PER OMUTCD SECTIONS 2B.28 AND 2B.29 PRIOR TO THE COVERING, OR REMOVAL OF EXISTING PAVEMENT MARKINGS.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER FOR WORK ZONE MARKING SIGNS PER THE REQUIREMENTS ABOVE AND ITEM 614 OF THE SPECIFICATIONS.

ITEM 614 - WORK ZONE MARKING SIGN - 77 EACH

WORK ZONE MARKING SIGN TABLE					
PART	LOCATION	614			FUNDING
		WORK ZONE MARKING SIGN			
		NO EDGE LINES	DO NOT PASS	PASS WITH CARE	
		EACH	EACH	EACH	
1/2	U.S. 250	16	12	8	01/NHS/PV
3	S.R. 21	4	6	4	01/NHS/PV
4	S.R. 93	2	2	1	02/STR/PV
5	S.R. 212	11	6	5	02/STR/PV
SUB-TOTALS PARTS 1, 2&3		50			01/NHS/PV
SUB-TOTALS PARTS 4&5		27			02/STR/PV
TOTALS (CARRIED TO GENERAL SUMMARY)		77			

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ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD) INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF CMS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT, SHORT DURATION CLOSURE OF A MULTI-LANE DIVIDED HIGHWAY AS INDICATED ON THE PLAN INSERT SHEET OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

WHEN CONSTRUCTION VEHICLES ARE ENTERING/EXITING THE ZONE DIRECTLY FROM/INTO AN OPEN LANE OF TRAFFIC. IF A LANE HAS BEEN CLOSED TO PROVIDE AN ACCELERATION/DECELERATION LANE FOR THE VEHICLE, THE LEO WILL NOT BE REQUIRED.

IN GENERAL, LEOS SHOULD BE POSITIONED AT THE POINT OF LANE RESTRICTION OR ROAD CLOSURE AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEO'S DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE -- 120 HOURS (01/STR/PV)

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF A LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

ITEM 408 - PRIME COAT, AS PER PLAN

THIS ITEM OF WORK SHALL BE PERFORMED IN ACCORDANCE WITH CMS "ITEM 408 - PRIME COAT," EXCEPT THE CONTRACTOR SHALL APPLY "MC-70" AT A RATE OF 0.4 GALLONS PER SQUARE YARD, OR AS DETERMINED BY THE ENGINEER, TO THE COMPLETED COMPACTED AGGREGATE SHOULDER, AS PER PLAN.

EXTRA FOR WIDENING (PAVEMENT AREA)

AN ADDITIONAL QUANTITY HAS BEEN ADDED TO THE PAVEMENT DATA SHEETS TO BE USED, AS DIRECTED BY THE ENGINEER, TO COVER AREAS THAT HAVE BEEN WIDENED EITHER ON CURVES, OR ON PREVIOUS MAINTENANCE ACTIVITIES BEYOND THE AVERAGE PAVEMENT WIDTH INDICATED IN THE PLANS.

COORDINATION OF RESURFACING AND PLANING OPERATIONS

ONCE THE PAVEMENT PLANING OPERATIONS HAVE COMMENCED, THE CONTRACTOR SHALL PLANE CONTINUOUSLY UNTIL ALL ELEMENTS OF WORK ASSOCIATED WITH THE PAVEMENT PLANING OPERATIONS ARE CONCLUDED. THE PAVEMENT PLANING OPERATIONS SHALL BE COMPLETED IN A TIMELY MANNER, OR AS DIRECTED BY THE ENGINEER.

THE CONTRACTOR MUST BEGIN PAVING OPERATIONS NO LATER THAN 4 DAYS AFTER THE START OF THE PAVEMENT PLANING.

IF ASPHALT CONCRETE IS TO BE APPLIED DIRECTLY ONTO PORTLAND CEMENT, CONCRETE, OR BRICK PAVEMENT, THE CONTRACTOR SHALL TACK THE EXISTING PAVEMENT WITH RUBBERIZED ASPHALT EMULSION CONFORMING TO CMS 702.13.

ALL GRINDINGS SHALL BECOME THE PROPERTY OF THE CONTRACTOR EXCEPT FOR WHAT IS REQUIRED TO BE USED FOR SHOULDER MATERIAL INDICATED IN ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN.

WINDOW CONTRACT TABLE

USE THE FOLLOWING TABLE AS REFERRED TO IN THE PROPOSAL:

DESCRIPTION OF CRITICAL WORK	CALENDER DAYS TO COMPLETE	DISINCENTIVE \$ PER PAY	WORK WINDOW	
			START	END
ALL WORK ON U.S. 250, S.R. 21, S.R. 93, AND S.R. 212	122 DAYS	PER C&MS 108.07	CONTRACT EXECUTION DATE	CONTRACT COMPLETION DATE

ITEM 607 - FENCE, MISC.: TEMPORARY ORANGE PLASTIC CONSTRUCTION FENCE

TEMPORARY ORANGE PLASTIC/NYLON CONSTRUCTION FENCE SHALL BE PLACED AROUND NEW CURB RAMPS AND WALK LOCATIONS FROM DEMOLITION, IF NOT IMMEDIATELY POURED, UNTIL ADEQUATELY CURED, OR AS DIRECTED BY THE ENGINEER TO PROTECT PEDESTRIAN TRAFFIC FROM CONSTRUCTION OPERATIONS. THE FENCING MATERIAL SHALL BE SECURELY FASTENED TO EITHER WOOD, OR METAL POSTS WITH A MAXIMUM SPACING NOT TO EXCEED 6 FEET. THE FENCING MATERIAL SHALL HAVE A NOMINAL HEIGHT OF 42 INCHES, AND THE TOP EDGE OF THE FENCING SHALL NOT BE PERMITTED TO SAG BELOW 30 INCHES. THE CONTRACTOR SHALL ENSURE THE FENCE IS IN GOOD CONDITION, PROPERLY PLACED, AND MAINTAINED AT ALL TIMES. PAYMENT FOR THE FENCING SHALL OCCUR ONCE PER LOCATION AND ANY REMOVAL OR REERECTION AT THE CONTRACTOR'S DISCRETION SHALL BE CONSIDERED INCIDENTAL AND PERFORMED AT THE CONTRACTOR'S EXPENSE.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 607 - FENCE, MISC.: TEMPORARY ORANGE PLASTIC CONSTRUCTION FENCE -680 FT (01/NHS/PV)

ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), (DRIVEWAYS), AS PER PLAN

THE PAVING OPERATION FOR INTERSECTING PUBLIC ROADS (NON-MAINLINE), AND DRIVEWAYS SHALL CONSIST OF AN AVERAGE THICKNESS EQUAL TO THE ADJACENT SURFACE COURSE FOR AN APPROXIMATE DISTANCE FROM THE EDGE OF PAVEMENT OR PAVED SHOULDERS, WHICHEVER IS APPLICABLE, UNLESS OTHERWISE DIRECTED BY THE ENGINEER AS FOLLOWS:

NORMAL OVERLAY OR UNEQUAL MILL/FILL (I.E. 1" MILL & 2" FILL)
PUBLIC ROADS: 20'
DRIVES: 10'

EQUAL MILL/FILL (I.E. 2" MILL & 2" FILL)
PUBLIC ROADS: 10' WITH 10' WIDE WEARING COURSE REMOVED
DRIVES: OMIT

FINE GRADED POLYMER OR THINLAY (WITH MILLING)
PUBLIC ROADS: 5' WITH 5' WIDE WEARING COURSE REMOVED (BUTT JOINT)
DRIVES: OMIT

FINE GRADED POLYMER OR THINLAY (WITHOUT MILLING)
PUBLIC ROADS: 5' WITH 3' WIDE WEARING COURSE REMOVED (BUTT JOINT)
DRIVES: 5'

UP-HILL DRIVEWAYS SHALL ONLY BE PAVED TO THE BEGINNING OF THE UPSLOPE OF THE DRIVEWAY, OR AS DIRECTED BY THE ENGINEER.

ALL GRADING, TACK COAT, PRIME COAT, TOOLS, EQUIPMENT, AND INCIDENTALS REQUIRED TO LAYOUT AND PAVE THE DRIVEWAYS AND INTERSECTING PUBLIC ROADS SHALL BE INCLUDED IN THE CU. YD. PRICE BID FOR ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), (DRIVEWAYS), AS PER PLAN.

THE CONTRACTOR SHALL ADHERE TO THE PROVISIONS SET FORTH IN CMS 107.10 CONCERNING THE PROTECTION AND RESTORATION OF ALL PUBLIC AND PRIVATE PROPERTY IMPACTED BY CONSTRUCTION OPERATIONS.

ALL DRIVEWAYS SHALL BE PAVED WITHIN (5) WORKING DAYS AFTER PLACING THE SURFACE COURSE ON THE MAINLINE PAVEMENT.

MATERIALS FURNISHED FOR FINE AND COARSE AGGREGATES USED IN THIS ITEM SHALL FOLLOW THE SPECIFICATIONS OF CMS 703.05, EXCEPT DO NOT USE COARSE AGGREGATE FROM A SOURCE DESIGNATED "SR" OR "SRH" AS DEFINED BY THE OFFICE OF MATERIALS MANAGEMENT (OMM) IN ANY JOB MIX FORMULA (JMF) FOR THIS ITEM OF WORK.

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ITEM 441 – ASPHALT CONCRETE SURFACE COURSE, TYPE 1,(446),
AS PER PLAN (PG70-22M)

THE CONTRACTOR SHALL FOLLOW THE SPECIFICATIONS OF CMS 703.05, EXCEPT DO NOT USE COARSE AGGREGATE FROM A SOURCE DESIGNATED “SR” OR “SRH” AS DEFINED BY THE OFFICE OF MATERIALS MANAGEMENT (OMM) IN ANY JOB MIX FORMULA (JMF) FOR THIS ITEM OF WORK.

446 DENSITY ACCEPTANCE WITH FLAGGER CLOSING OF A 2-LANE
HIGHWAY FOR PAVING OPERATIONS

THIS PLAN NOTE APPLIES ONLY TO A FLAGGER CLOSURE OF ONE LANE OF A 2-LANE HIGHWAY DURING PAVING OPERATIONS WHEN USING STANDARD CONSTRUCTION DRAWING MT-97.11 OR MT-97.12, AND ALLOWS A PAVING OPERATION TO PROCEED CONCURRENTLY WITH THE MARKING AND CUTTING OF CORES REQUIRED FOR 446 DENSITY ACCEPTANCE.

IN ALL CASES THE CONTRACTOR SHOULD LENGTHEN THEIR LANE CLOSURES TO THE MAXIMUM PERMISSIBLE LENGTH DETAILED IN THE ABOVE REFERENCED STANDARD CONSTRUCTION DRAWINGS TO ALLOW THE ENGINEER ADEQUATE TIME TO MARK THE REQUIRED CORE LOCATIONS AND FOR CORE CUTTING OPERATIONS.

THE CONTRACTOR WILL PROVIDE TO THE ENGINEER THE PLANNED QUANTITY THAT WILL BE PLACED FOR THE DAY’S PRODUCTION. EACH DAY’S PRODUCTION WILL BE CONSIDERED ONE LOT AND INCLUDES SHOULDERS. TEN CORES WILL BE OBTAINED BY THE CONTRACTOR FOR EACH LOT AT RANDOM LOCATIONS DETERMINED BY THE ENGINEER. THE ENGINEER WILL DIVIDE A LOT INTO FIVE EQUAL SUBLOTS AND CALCULATE TWO RANDOM CORE LOCATIONS IN EACH SUBLOT AS DESCRIBED IN C&MS 446.05.

THE ENGINEER WILL MARK THE CORE LOCATIONS AFTER THE PAVING OPERATION (INCLUDING THE FINISH ROLLER) HAS COMPLETELY PASSED THE RANDOMLY SELECTED CORE LOCATION. THE CORE DRILL OPERATION CAN BEGIN CUTTING CORES WHEN THE NEWLY PLACED SURFACE TEMPERATURE IS LESS THAN 140°F. IT IS THE CONTRACTOR’S RESPONSIBILITY TO MAINTAIN THE LANE CLOSURE DURING ALL PAVING, MARKING, AND CORING OPERATIONS PER THE REQUIREMENTS OF THE STANDARD CONSTRUCTION DRAWING USED FOR THE PAVING OPERATION.

ITEM 424 – FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B,
AS PER PLAN

THE CONTRACTOR SHALL FOLLOW THE SPECIFICATIONS OF CMS 703.05, EXCEPT DO NOT USE COARSE AGGREGATE FROM A SOURCE DESIGNATED “SR” OR “SRH” AS DEFINED BY THE OFFICE OF MATERIALS MANAGEMENT (OMM) IN ANY JOB MIX FORMULA (JMF) FOR THIS ITEM OF WORK.

ITEM 209 – PREPARING SUBGRADE FOR SHOULDER PAVING, AS PER PLAN

THIS ITEM OF WORK CONSISTS OF PREPARING THE SHOULDER TO ACCEPT THE SAFETY EDGE. THE PREPARATION SHALL BE CONSISTENT IN BOTH THICKNESS AND WIDTH.

PRIOR TO PAVING THE SAFETY EDGE, THE CONTRACTOR SHALL GRADE AN AREA 10 INCHES WIDE BEGINNING AT THE EDGE OF THE PAVED ROADWAY TO PROVIDE A LEVEL SURFACE FREE FROM VEGETATION FOR CONSTRUCTION OF THE SAFETY EDGE. THE CONTRACTOR SHALL EXCAVATE THE GRADED AREA TO A DEPTH NECESSARY TO CONSTRUCT THE SAFETY EDGE, AND COMPACT THE GRADED SHOULDER ACCORDING TO CMS 617.05, OR AS DIRECTED BY THE ENGINEER.

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (Hauling.Permits@dot.ohio.gov) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME TABLE		
ITEM	DURATION OF CLOSURE	NOTICE DUE TO PERMITS & PIO
RAMP & ROAD CLOSURES	>= 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	<= 12 HOURS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

AIRWAY/HIGHWAY CLEARANCE FOR AIRPORTS AND HELIPORTS

THIS PROJECT HAS BEEN IDENTIFIED AS BEING WITHIN THE INFLUENCE ARE OF A PUBLIC USE AIRPORT OR HELIPORT. NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT AT MAXIMUM OPERATING HEIGHT SHALL EXCEED A HEIGHT OF 25 FT. IF ANY TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT WILL EXCEED THIS HEIGHT, FURTHER COORDINATION WITH THE FEDERAL AVIATION ADMINISTRATION (FAA), AND THE ODOT OFFICE OF AVIATION, WILL BE NECESSARY PRIOR TO ERECTING SUCH TEMPORARY STRUCTURES OR OPERATING SUCH EQUIPMENT ON THE PROJECT. THE CONTRACTOR WILL BE REQUIRED TO FILE A NEW FAA 7460-1, ADVISING THE FAA THAT AERONAUTICAL STUDY NO. 2018-AGL-3905-OE IS BEING RESUBMITTED AND THAT AN ALTERATION TO THE ORIGINAL SUBMISSION IS REQUESTED.

NOTIFY THE ODOT OFFICE OF AVIATION WHEN RESUBMITTING FAA FORM 7460-1. NO TEMPORARY STRUCTURES OR CONSTRUCTION EQUIPMENT SHALL EXCEED THE PERMISSIBLE HEIGHT, UNTIL A COPY OF THE FAA APPROVAL AND THE ODOT OFFICE OF AVIATION PERMIT HAS BEEN FURNISHED TO THE PROJECT ENGINEER.

FAA APPROVAL MAY TAKE UP TO 45 DAYS. ALL SUBMISSIONS SHALL BE DIRECTED TO THESE OFFICES:

EXPRESS PROCESSING CENTER
THE FEDERAL AVIATION ADMINISTRATION
SOUTHWEST REGIONAL OFFICE
AIR TRAFFIC AIRSPACE BRANCH ASW-520
2601 MEACHAM BLVD.
FORT WORTH, TX 76137-4298

OHIO DEPARTMENT OF TRANSPORTATION
OFFICE OF AVIATION
2829 WEST DUBLIN-GRANVILLE ROAD
COLUMBUS, OHIO 43235
614-387-2346

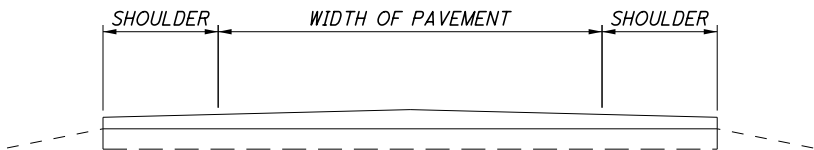
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SHEET NUM.											PART.			ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.	CALCULATED ADK	CHECKED ANS
2	3	8	9	10	11	12	14				01/NHS/PV	02/STR/PV	03/SAF/PV								
																		ROADWAY			
		7,167									5,884	1,283		202	23500	7,167	SY	WEARING COURSE REMOVED			
						746					746			202	30000	746	SF	WALK REMOVED			
						16					16			202	32000	16	FT	CURB REMOVED			
					60						60			203	10000	60	CY	EXCAVATION			
					149						149			204	10000	149	SY	SUBGRADE COMPACTION			
			4.24								4.24			209	60500	4.24	MILE	LINEAR GRADING			
			5.51								3.13	2.38		209	72051	5.51	MILE	PREPARING SUBGRADE FOR SHOULDER PAVING, AS PER PLAN	4		
	680										680			607	98000	680	FT	FENCE, MISC.:ORANGE PLASTIC CONTSTRUCTION FENCE	3		
						746					746			608	52000	746	SF	CURB RAMP			
						23					23			608	53021	23	SF	DETECTABLE WARNING, AS PER PLAN	4		
																		EROSION CONTROL			
						41					41			659	98000	41	SY	SEEDING, MISC.:CURB RAMP GRADING AND RESTORATION	4		
											850	150		832	30000	1,000	EACH	EROSION CONTROL			
																		PAVEMENT			
					200						160	40		251	01042	200	CY	PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE)			
		723									723			254	01000	723	SY	PAVEMENT PLANING, ASPHALT CONCRETE (1")			
		48,762	7,099	633							56,494			254	01000	56,494	SY	PAVEMENT PLANING, ASPHALT CONCRETE (3")			
					223						223			255	20000	223	FT	FULL DEPTH PAVEMENT SAWING			
					27						27			301	46000	27	CY	ASPHALT CONCRETE BASE, PG64-22			
					25						25			304	20000	25	CY	AGGREGATE BASE			
		18,512	3,259	124	21						19,399	2,517		407	20000	21,916	GAL	NON-TRACKING TACK COAT			
			6,090								4,973	1,117		408	10001	6,090	GAL	PRIME COAT, AS PER PLAN	3		
		599	139								738			424	12001	738	CY	FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN	4		
		3,982	704	113	6						4,160	645		441	10101	4,805	CY	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (446), AS PER PLAN (PG70-22M)	4		
		297	26									323		441	50200	323	CY	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, (448)			
		4,878	827								5,581	124		441	50300	5,705	CY	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)			
		304									217	87		441	50401	304	CY	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), (DRIVEWAYS), AS PER PLAN	3		
			1,061								828	233		617	10101	1,061	CY	COMPACTED AGGREGATE, AS PER PLAN	2		
			13,743								10,950	2,793		617	20000	13,743	SY	SHOULDER PREPARATION			
			4.24								4.24			618	41000	4.24	MILE	RUMBLE STRIPES, EDGE LINE (ASPHALT CONCRETE)			
		4.49											4.49	618	43000	4.49	MILE	RUMBLE STRIPES, CENTER LINE (ASPHALT CONCRETE)			
		23,706											23,706	874	20000	23,706	FT	LONGITUDINAL JOINT PREPARATION			
																		TRAFFIC CONTROL			
							775				616	159		621	00100	775	EACH	RPM			
							775				616	159		621	54000	775	EACH	RAISED PAVEMENT MARKER REMOVED			
							10.16				9.92	0.24		646	10010	10.16	MILE	EDGE LINE, 6"			
							8.27				7.08	1.19		646	10200	8.27	MILE	CENTER LINE			
							1,016				1,016			646	10300	1,016	FT	CHANNELIZING LINE, 8"			
							292				270	22		646	10400	292	FT	STOP LINE			
							431				431			646	10500	431	FT	CROSSWALK LINE			
							405				405			646	10600	405	FT	TRANSVERSE/DIAGONAL LINE			
							50				50			646	10800	50	SF	ISLAND MARKING			
							19				19			646	20300	19	EACH	LANE ARROW			
																		MAINTENANCE OF TRAFFIC			
	120										102	18		614	11110	120	hour	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE			
77											50	27		614	12460	77	EACH	WORK ZONE MARKING SIGN			
17.65											15.27	2.38		614	21500	17.65	MILE	WORK ZONE CENTER LINE, CLASS II, 642 PAINT			
17.65											15.27	2.38		614	21550	17.65	MILE	WORK ZONE CENTER LINE, CLASS III, 642 PAINT			
2,687											2,687			614	23680	2,687	FT	WORK ZONE CHANNELIZING LINE, CLASS III, 8", 642 PAINT			
768											724	44		614	26610	768	FT	WORK ZONE STOP LINE, CLASS III, 642 PAINT			
																		INCIDENTALS			
LS											LS	LS		614	11000	LS		MAINTAINING TRAFFIC			
											LS	LS		624	10000	LS		MOBILIZATION			

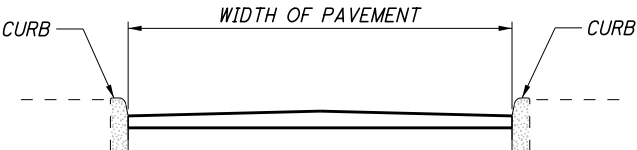
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BRIDGE TREATMENT

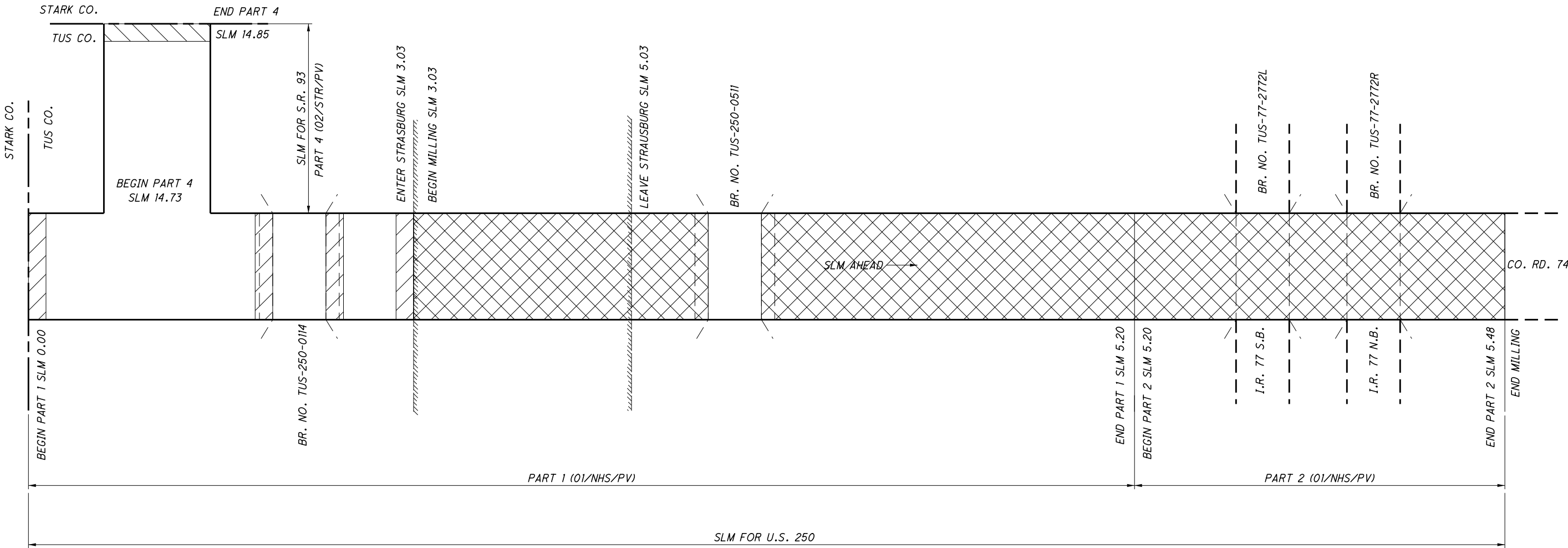
TUS-250-0114: (SFN 7904592): OMIT
TUS-250-0511: (SFN 7904622): OMIT DECK, SURFACE COURSE ON APPROACH SLABS
TUS-77-2772L: (SFN 7903510): MAINTAIN VERTICAL CLEARANCE
TUS-77-2772R: (SFN 7903545): MAINTAIN VERTICAL CLEARANCE



TYPICAL 1

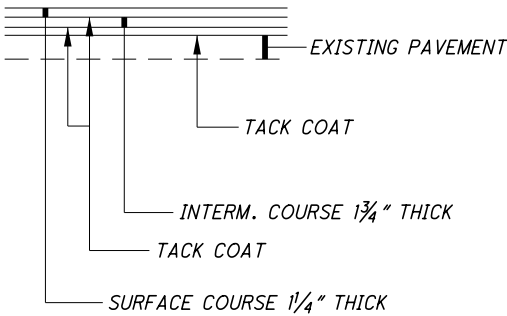


TYPICAL 2

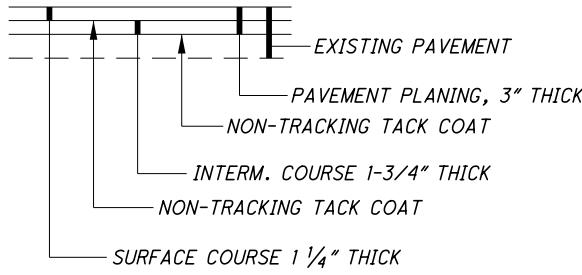


LEGEND

- ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN
- BUTT JOINT OR FEATHER AS PER STD. DWG. BP-3.1



PROPOSED WORK
PART 1,2&4



PROPOSED WORK
(PLANING AND RESURFACING)

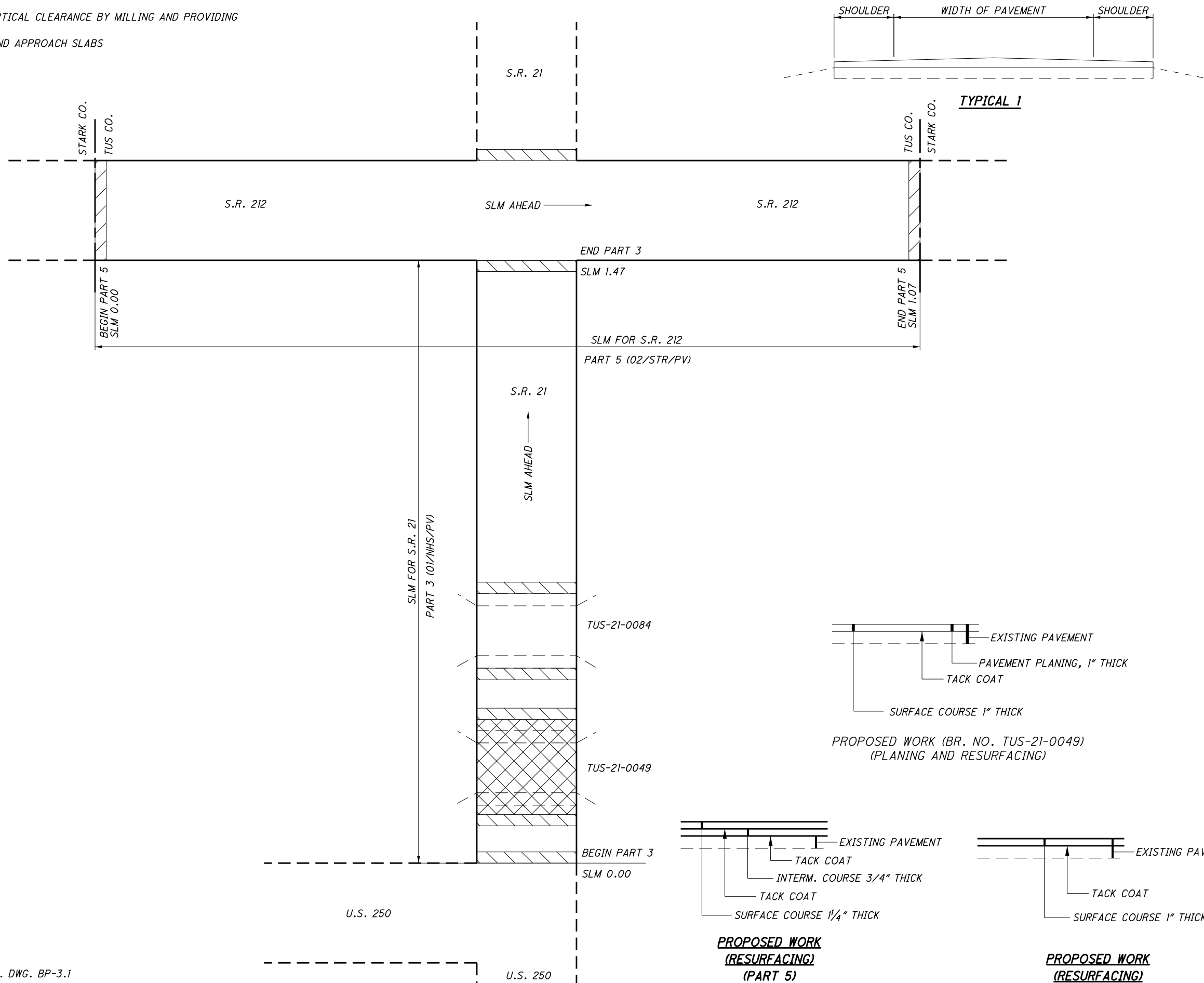
PAVEMENT DETAILS

TUS-250/ VAR -
0.00/ VAR

CALCULATED
ADK
CHECKED
ANS

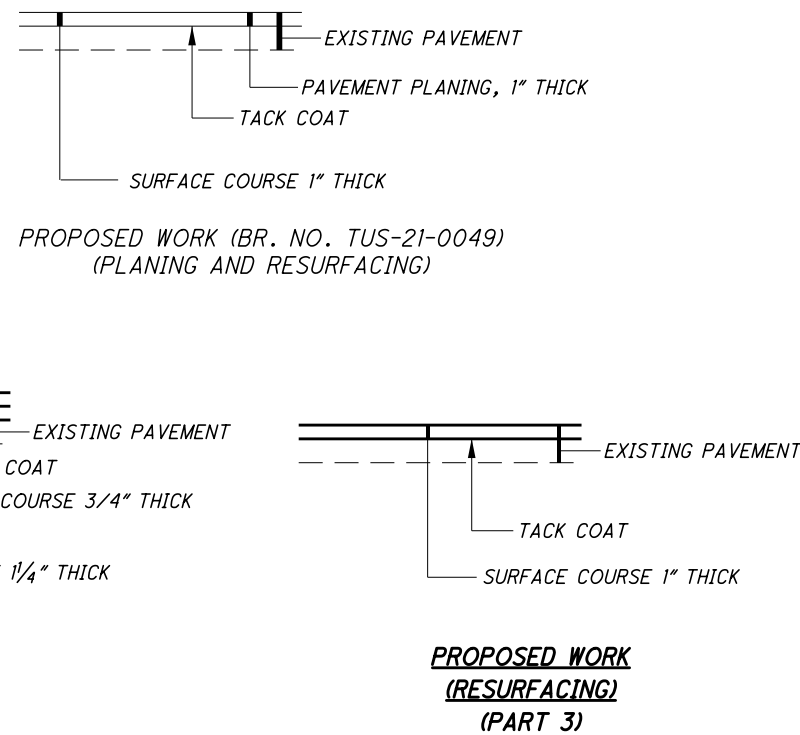
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BRIDGE TREATMENT
TUS-21-0049: (SFN 7900066): MAINTAIN VERTICAL CLEARANCE BY MILLING AND PROVIDING PROPOSED TREATMENT
TUS-21-0084: (SFN 7900090): OMIT DECK AND APPROACH SLABS



LEGEND

- ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN
- BUTT JOINT OR FEATHER AS PER STD. DWG. BP-3.1

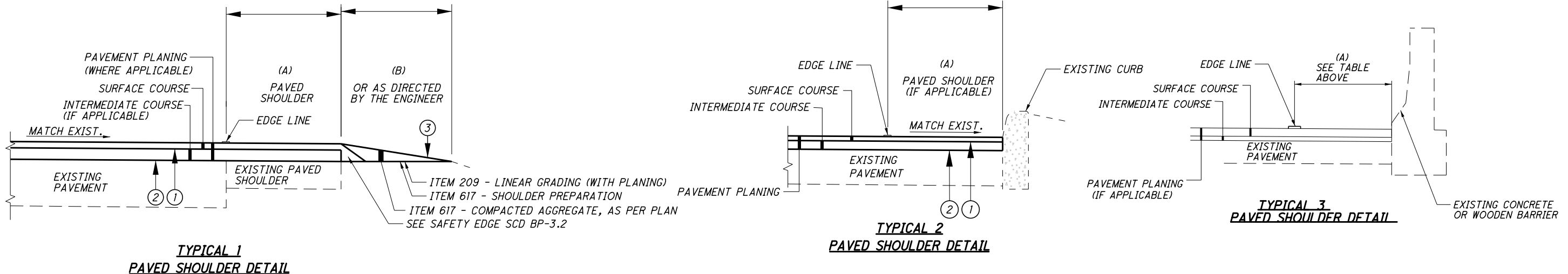


PAVEMENT DETAILS

TUS-250/ VAR -
0.00/ VAR

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SHOULDER QUANTITIES																																									
PART	COUNTY	ROUTE	LOG POINT TO LOG POINT (STRAIGHT LINE MILEAGE)		MILE	DEDUCTIONS FOR BRIDGES	FEET (WITH DEDUCTIONS)	TYPICAL		PROPOSED WIDTH (FEET)				SHOULDER AREA	202	209	209	254	407		408	424		441						617			618	FUNDING							
															WEARING COURSE REMOVED	PREPARING SUBGRADE FOR SHOULDER PAVING, AS PER PLAN	LINEAR GRADING	PAVEMENT PLANING, ASPHALT CONCRETE	NON-TRACKING TACK COAT		PRIME COAT, AS PER PLAN	IN.	FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, AS PER PLAN	IN.	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), AS PER PLAN, (PG70-22M)	SAFETY EDGE PAVT. < 3"	IN.	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1, (448),	IN.	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (448)	IN.	COMPACTED AGGREGATE, AS PER PLAN	SHOULDER PREPARATION		RUMBLE STRIPES, EDGE LINE (ASPHALT CONCRETE)						
																										ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (448), AS PER PLAN, (PG70-22M)															
			FROM	TO		FT.	LT.	RT.	A	B	A	B	SQ. YD.	SQ. YD.	MILE	MILE	SQ. YD.	0.055 GAL/SY GAL.	0.085 GAL/SY GAL.	0.4 GAL/SY GAL.	CU. YD.	CU. YD.	CU. YD.	CU. YD.	CU. YD.	CU. YD.	CU. YD.	CU. YD.	CU. YD.	CU. YD.	CU. YD.	SQ. YD.	MILE								
1	TUS	S.R.250	0.00	2.10	2.03	360	10,728	1	2.5		2.5		5,960					328	507	1,907			1 1/4	207	30			1 3/4	290												
									2.0		2.0		4,768		2.03				35	54			1 1/4	22	1			1 3/4	31	3	397	4,768	4.06								
1	TUS	S.R.250	2.10	2.19	0.09		475	1		6.0		6.0	633							84			1 1/4					1 3/4		3	18	211	0.18								
									2.0		2.0		211		0.09													1 3/4		3											
1	TUS	S.R.250	2.19	2.99	0.80		4,224	1	3.0		3.0		2,816					155	239			1 1/4	98	12				1 3/4	137		3	156	1,877								
									2.0		2.0		1,877		0.80					751			1 1/4					1 3/4		3											
1	TUS	S.R.250	2.99	3.03	0.04		211	1	3.0		6.0		211					12	18			1 1/4	7	1				1 3/4	10		3	8	94								
									2.0		2.0		94		0.04					38								1 3/4		3											
1	TUS	S.R.250	3.03	3.16	0.13		686	1	1.5		1.5		229					229	13	19			1 1/4	8	2			1 3/4	11												
									2.0		2.0		305							122								1 3/4		3	25										
1	TUS	S.R.250	3.16	3.62	0.46		2,429	1	8.0		8.0		4,318				0.26	4,318	237	367			1 1/4	150				1 3/4	210		3	90									
									2.0		2.0		1,080		0.92					432										3											
1	TUS	S.R.250	3.62	5.00	1.38		7,286	2	0.0		0.0		0																												
									0.0		0.0		0																												
1	TUS	S.R.250	5.00	5.03	0.03		158	1	3.0		3.0		105					105	6	9			1 1/4	4				1 3/4	5												
									0.0		0.0		0																												
1	TUS	S.R.250	5.03	5.16	0.08	249	437	1	8.0		3.0		534					534	29	45			1 1/4	19				1 3/4	26												
									0.0		2.0		97							39								1 3/4													
1	TUS	S.R.250	5.16	5.20	0.04		211	2	8.0		8.0		375					375	21	32			1 1/4	13				1 3/4	18												
								1	0.0		0.0		0																												
SUB-TOTALS: PART 1																2.96	1.18	5,561	2126	3,373				528	46				738		694	6,950	4.24	01/NHS/PV							
2	TUS	S.R.250I	5.20	5.30	0.10		528	2/3	8.0		8.0		939					939	52	80			1 1/4	33	1			1 3/4	46												
								1/3	0.0		0.0		0																												
2	TUS	S.R.250I	5.30	5.47	0.17		898	1	3.0		3.0		599					599	33	51			1 1/4	21	3			1 3/4	29												
									2.0		2.0		399		0.17				160											3	33	399									
SUB-TOTALS: PART 2																0.17		1,538	216	160				54	4				75		33	399		01/NHS/PV							
3	TUS	S.R. 21	0.00	0.15	0.15		792	1	3.0		3.0		528					29	45		1	15																			
									2.0		2.0		352		0.30					141										1	10	352									
3	TUS	S.R. 21	0.15	0.29	0.14		739	1	8.0		3.0		903					50	77		1	25																			
									2.0		2.0		328		0.28					131										1	9	328									
3	TUS	S.R. 21	0.29	0.33	0.04		211	1/3	3.0		8.0		258					14	22		1	7																			
									2.0		2.0		94		0.08					38										1	3	94									
3	TUS	S.R. 21	0.33	0.54	0.21		1,109		2.0		2.0		493					27	42		1	14																			
									2.0		2.0		493		0.42					197										1	14	493									
3	TUS	S.R. 22	0.33	0.54	0.21		1,109		6.0		2.0		986					54	84		1	27																			
									2.0		2.0		493		0.42					197										1	14	493									
3	TUS	S.R. 21	0.64	1.47	0.78	239	4,143	1	2.0		2.0		1,841					101	156		1	51								1	51	1,841									
									2.0		2.0		1,841		1.56					736																					
SUB-TOTALS: PART 3																	3.06		701	1,440	139												101	3,601		01/NHS/PV					
4	TUS	S.R. 93	14.73	14.85	0.12		634	6	2.0		2.0		282					16	24			1 1/4	10	2				1 3/4	14												
									2.0		2.0		282		0.24					113										3	24	282									
SUB-TOTALS: PART 4																0.24			40	113					10	2				14	24	282		02/STR/PV							
5	TUS	S.R. 212	0.00	1.07	1.07		5,650	6	1.0		1.0		1,256					69	107			1 1/4	44	16	3/4	26															
									2.0		2.0		2,511		2.14					1,004									3	209	2,511										
SUB-TOTALS: PART 5																2.14			176	1,004					44	16		26			209	2,511		02/STR/PV							
SAFETY FUNDING																																									
SUB-TOTALS: PARTS 1,2&3																3.13	4.24	7,099	3,043	4,973	139			632				813		828	10,950	4.24	03/SAF/PV								
SUB-TOTALS: PARTS 4&5																2.38	0.00		216	1,117					72			26		14	233	2,793		02/STR/PV							
TOTALS (CARRIED TO GENERAL SUMMARY)																	5.51	4.24	7,099	3,259	6,090			139			704			26		827	1,061	13,743	4.24						



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EXTENDED PAVED SHOULDER QUANTITIES															
PART	COUNTY	ROUTE	LOG POINT TO LOG POINT (STRAIGHT LINE MILEAGE)		SIDE	TYPICAL	LENGTH (FIELD MEASUREMENTS)	PROPOSED WIDTH	PAVEMENT AREA	254		441		FUNDING	REMARKS
										PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN	NON-TRACKING TACK COAT	AVERAGE THICKNESS	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (446), AS PER PLAN, (PG70-22M)		
FROM	TO	FT.	FT.	SQ. YD.	SQ. YD.	GAL.	IN.	CU. YD.							
1	TUS	S.R. 250	0.72	0.77	RT.	1	264	4	117		10	3	10		SHADYREST ARMY NAVY
1	TUS	S.R. 250	2.25	2.41	RT.	1	845	6	563		48	3	47		VACANT LOT TO WINFIELD CO.
1	TUS	S.R. 250	4.57	4.61	LT.	2	211	4	94	94	8	3	8		MIDLAND-BRIGHTWOOD
1	TUS	S.R. 250	4.62	4.65	LT.	2	158	4	70	70	6	3	6		KRUAS' PIZZA
1	TUS	S.R. 250	5.00	5.04	RT.	2	211	20	469	469	40	3	39		HARDEES
SUB-TOTALS: PART 1										633	112		110	01/NHS/PV	
3	TUS	S.R. 21	0.32	0.35	RT.	1	158	3	53		5	1	1		JENEI DRILLING CO.
3	TUS	S.R. 21	1.41	1.46	LT.	1	264	3	88		7	1	2		FEARON & FELLER DRILLING INC.
SUB-TOTALS: PART 3											12		3	01/NHS/PV	
TOTALS (CARRIED TO GENERAL SUMMARY)										633	124		113		

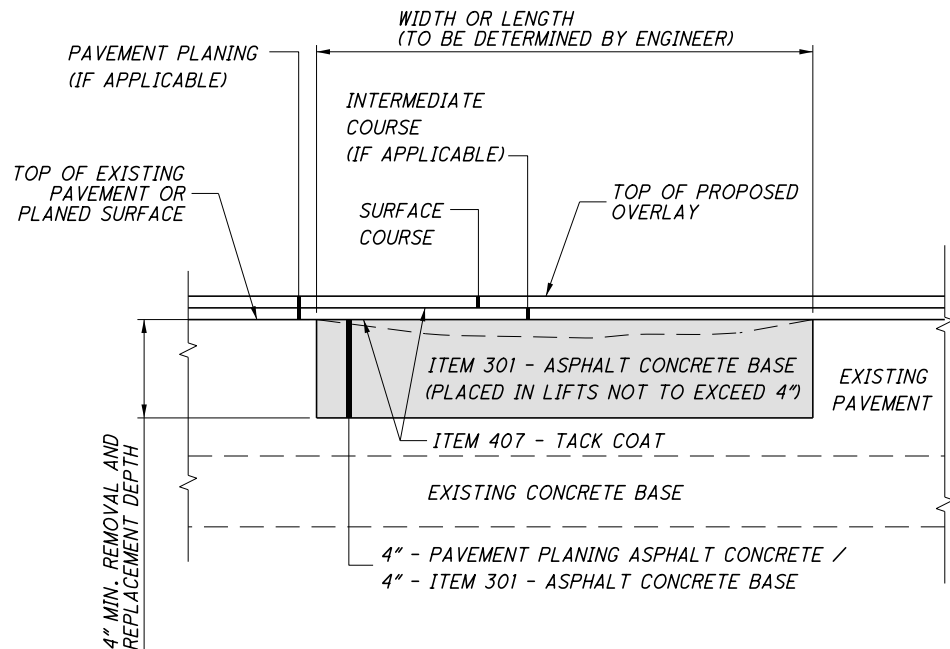
CALCULATED
ADK
CHECKED
ANS

EXTENDED PAVED SHOULDER DATA

TUS-250/ VAR -
0.00/ VAR

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21

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PARTIAL DEPTH PAVEMENT REPAIR TYPICAL

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE)

THIS ITEM OF WORK SHALL ADHERE TO THE REQUIREMENTS DETAILED IN CMS 251 EXCEPT IN LIEU OF PLACING MULTIPLE SURFACE COURSE LIFTS FOR THE REPAIR, THE CONTRACTOR SHALL PLACE AND COMPACT ONE FOUR (4) INCH LIFT OF ITEM 301, ASPHALT CONCRETE BASE, PG64-22. ODOT SHALL PERFORM A FINAL FIELD REVIEW PRIOR TO CONSTRUCTION AND AT THAT TIME, THE ENGINEER WILL DETERMINE AND MARK THE SIZE AND LOCATION FOR EACH REPAIR FOR THE CONTRACTOR. FINAL PAYMENT FOR THESE LOCATIONS SHALL BE FOR THE ACCEPTED QUANTITY COMPLETED IN PLACE.

ALL PARTIAL DEPTH REPAIRS SHALL BE COMPLETED PRIOR TO COMMENCING PAVING OPERATIONS.

PAYMENT FOR ALL LABOR, EQUIPMENT, AND MATERIALS SHALL BE INCLUDED IN THE CONTRACT PRICE FOR ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE). THE ESTIMATED QUANTITIES FROM THIS SHEET ARE TO BE USED AS DIRECTED BY THE ENGINEER.

ESTIMATED QUANTITIES

(PART 1)

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE) - 40 CU YD

(PART 3)

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE) - 120 CU YD

TOTAL USE: 160 CU YD (01/NHS/PV)

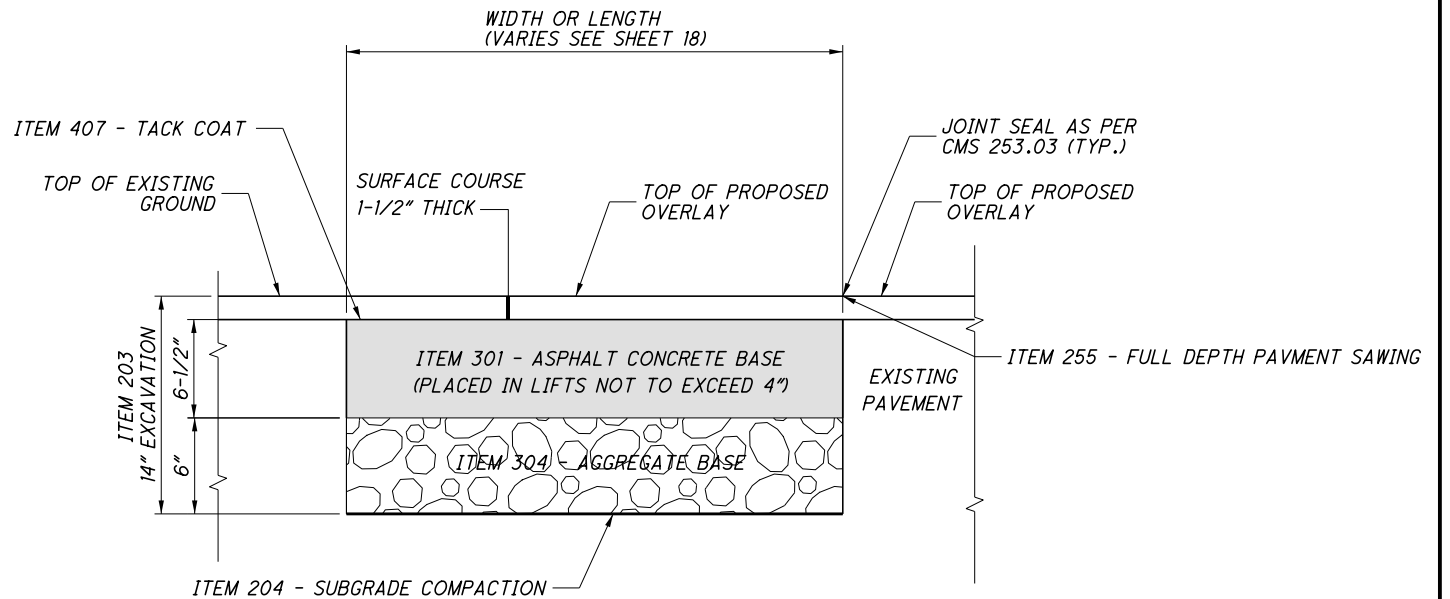
(PART 4)

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE) - 15 CU YD

(PART 5)

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE) - 25 CU YD

TOTAL USE: 40 CU YD (02/STR/PV)



PAVEMENT WIDENING

PAVEMENT WIDENING IS TO BE USED AT THE INTERSECTION OF U.S. 250 AND 9TH ST. SEE SHEET 18 FOR DETAILS.

WIDENING WORK SHALL MEET THE REQUIREMENTS OF C&MS 2019 AND THE ABOVE TYPICAL SECTION.

ALL ITEMS (EXCLUDING THE ITEM 441 - ASPHALT CONCRETE SURFACE COURSE) SHALL BE COMPLETED PRIOR TO THE PAVING OPERATIONS.

THE ESTIMATED QUANTITIES FROM THIS SHEET HAVE BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER. FINAL PAYMENT FOR THESE ITEMS SHALL BE FOR THE ACCEPTED QUANTITY COMPLETED IN PLACE.

ESTIMATED QUANTITIES

PART 1 (01/NHS/PV)

ITEM 203 - EXCAVATION - 60 CU YD

ITEM 204 - SUBGRADE COMPACTION - 149 SQ YD

ITEM 255 - FULL DEPTH PAVEMENT SAWING - 223 FT

ITEM 301 - ASPHALT CONCRETE BASE, PG 64-22 - 27 CU YD

ITEM 304 - AGGREGATE BASE - 25 CU YD

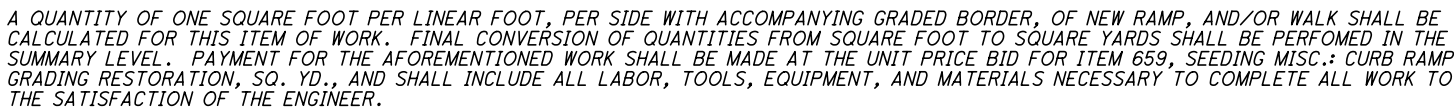
ITEM 407 - NON-TRACKING TACK COAT - 21 GAL

ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (446), AS PER PLAN (PG 70-22M) - 6 CU YD

(TOTALS CARRIED TO GENERAL SUMMARY)

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REF. NO.	PART	COUNTY	ROUTE	SLM	INTERSECTING PUBLIC ROAD	LOCATION (SEE LEGEND)	CURB RAMP TYPE	DIMENSION A, OR LANDING DEPTH	DIMENSION B	AVG. WIDTH OF SIDEWALK OR LANDING	202	202	608		659	FUNDING	REMARKS
											WALK REMOVED	CURB REMOVED	CURB RAMP	DETECTABLE WARNING, AS PER PLAN	SEEDING MISC.: CURB RAMP GRADING RESTORATION		
											SQ FT	FT	SQ FT	SQ FT	SQ FT		
VILLAGE OF STRASBURG																	
CR-1	1	TUS	U.S. 250	3.86	7TH ST NW	D1	D11-A	6		5	30		30		15		
CR-2	1	TUS	U.S. 250	3.95	ALLEY	A1	D11-A	5		5	25		25		12.5		BETWEEN 6TH ST NE & 7TH ST NE
CR-3	1	TUS	U.S. 250	3.95	ALLEY	C1	D11-A	5		5	25		25		12.5		BETWEEN 6TH ST NE & 7TH ST NE
CR-4	1	TUS	U.S. 250	4.09	ALLEY	A1	D11-A	5		5	25		25		12.5		BETWEEN CHERRY ST & 6TH ST NE
CR-5	1	TUS	U.S. 250	4.09	ALLEY	C1	D11-A	5		6	30		30		15		BETWEEN CHERRY ST & 6TH ST NE
CR-6	1	TUS	U.S. 250	4.12	ALLEY	A1	D11-A	8		6	48		48		24		BETWEEN VINE ST SE & CHERRY ST
CR-7	1	TUS	U.S. 250	4.12	ALLEY	C1	D11-A	12		6	72		72		36		BETWEEN VINE ST SE & CHERRY ST
CR-8	1	TUS	U.S. 250	4.20	ALLEY	A1	D11-A	6		5	30		30		15		BETWEEN VINE ST SE & HIGH ST
CR-9	1	TUS	U.S. 250	4.20	ALLEY	C1	D11-A	9		6	54	6	54		27		BETWEEN VINE ST SE & HIGH ST
CR-10	1	TUS	U.S. 250	4.20	ALLEY	B1	D11-A	5		5	25		25		12.5		BETWEEN VINE ST SE & HIGH ST
CR-11	1	TUS	U.S. 250	4.20	ALLEY	D1	D11-G	2		4	11.25		11.25	11.25	4		BETWEEN VINE ST SE & HIGH ST
CR-12	1	TUS	U.S. 250	4.30	ALLEY	A1	D11-A	5		6	30		30		15		BETWEEN HIGHST & 2ND ST NE
CR-13	1	TUS	U.S. 250	4.30	ALLEY	C1	D11-A	6		5	30		30		15		BETWEEN HIGHST & 2ND ST NE
CR-14	1	TUS	U.S. 250	4.30	ALLEY	B1	D11-G	2		4	11.25		11.25	11.25	4		BETWEEN HIGHST & 2ND ST NE
CR-15	1	TUS	U.S. 250	4.30	ALLEY	D1	D11-A	6		6	36		36		18		BETWEEN HIGHST & 2ND ST NE
CR-16	1	TUS	U.S. 250	4.34	2ND ST NE	A1	D11-A	7		7	49		49		24.5		
CR-17	1	TUS	U.S. 250	4.48	ALLEY	A1	D11-A	8		5	40		40		20		BETWEEN 1ST ST W TO 2ND ST SW
CR-18	1	TUS	U.S. 250	4.48	ALLEY	C1	D11-A	9		5	45		45		22.5		BETWEEN 1ST ST W TO 2ND ST SW
CR-19	1	TUS	U.S. 250	4.64	4TH ST	A1	D11-C	5	4	4	20		20		10		
CR-20	1	TUS	U.S. 250	4.64	4TH ST	D1	D11-A	5		6	30		30		15		
CR-21	1	TUS	U.S. 250	4.64	5TH ST	D1	TUS-1	9	5	6	54		54		27		SEE SHEET 13 FOR DETAILS
CR-22	1	TUS	U.S. 250	4.64	5TH ST	C1	D11-C	5	4	5	25	10	25		12.5		REMOVE CURB AND ADD LANDING ZONE
CONVERT SQ. FT. TO SQ. YD.															369.5		
															SQ YD		
SUB-TOTALS: PART 1											746	16	746	23	41	01/NHS/PV	
(TOTALS CARRIED TO GENERAL SUMMARY)											746	16	746	23	41		



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TRAFFIC CONTROL SUBSUMMARY																				
PART	SHEET NO.	COUNTY	ROUTE	LOG POINT TO LOG POINT (STRAIGHT LINE MILEAGE)		621				646										FUNDING
						RPM			RAISED PAVEMENT MARKER REMOVED	EDGE LINE, 6" (WHITE)	EDGE LINE 6" (YELLOW)	CENTER LINE	CHANNELIZING LINE, 8"	STOP LINE	CROSSWALK LINE	TRANSVERSE/DIAGONAL LINE		ISLAND MARKING (YELLOW)	LANE ARROW	
						SPACING	YELLOW/YELLOW	WHITE/RED								YELLOW	WHITE			
				FROM	TO															
1		TUS	U.S. 250	0.00	3.03	80	201		201	6.06		3.03								
1		TUS	U.S. 250	3.03	5.03	80	133		133			2.00								
1		TUS	U.S. 250	5.03	5.20	80	12		12	0.34		0.17								
1	15	TUS	U.S. 250	0.20									205	50						
1	16	TUS	U.S. 250	2.22		40		4	4		0.02		156	36						
1	17	TUS	U.S. 250	4.34										41	275					
1	17	TUS	U.S. 250	4.43										40	156					
1	18	TUS	U.S. 250	5.08		40		6	6			0.05	195			120		50	3	
1	19	TUS	U.S. 250	5.13	5.22	40/80	6	13	19			0.04	430	49		165			8	
SUB-TOTALS PART 1							375		375	6.42		5.29	986	216	431	285		50	11	01/NHS/PV
2		TUS	U.S. 250 I	5.20	5.48	80	38		38	0.56		0.28								
2	20	TUS	U.S. 250 I	5.22	5.29	80/40	5	3	8			0.04	30	24		120			8	
2	20	TUS	U.S. 250 I	5.45	5.48									30						
SUB-TOTALS PART 2							46		46	0.56		0.32	30	54		120			8	01/NHS/PV
3		TUS	S.R. 21	0.00	1.47	80	195		195	2.94		1.47								
SUB-TOTALS PART 3							195		195	2.94		1.47								01/NHS/PV
4		TUS	S.R. 93	14.73	14.85	40	17		17	0.24		0.12								
SUB-TOTALS PART 4							17		17	0.24		0.12								02/STR/PV
5		TUS	S.R. 212	0.00	1.07	40	142		142			1.07								
5	21	TUS	S.R. 212	0.50										22						
SUB-TOTALS PART 5							142		142			1.07		22						02/STR/PV
SUB-TOTALS PARTS 1,2&3							616		616	9.92		7.08	1,016	270	431	405		50	19	01/NHS/PV
SUB-TOTALS PART 4&5							159		159	0.24		1.19		22						02/STR/PV
TOTALS (CARRIED TO GENERAL SUMMARY)							775		775	10.16		8.27	1,016	292	431	405		50	19	

CALCULATED
ADK

CHECKED
ANS

TRAFFIC CONTROL SUB-SUMMARY

TUS-250/ VAR -
0.00/ VAR

14
21

COUNTY TUS
U.S. 250
SECTION SLM 0.20
PART 1

0

20

40

HORIZONTAL
SCALE IN FEET

CALCULATED

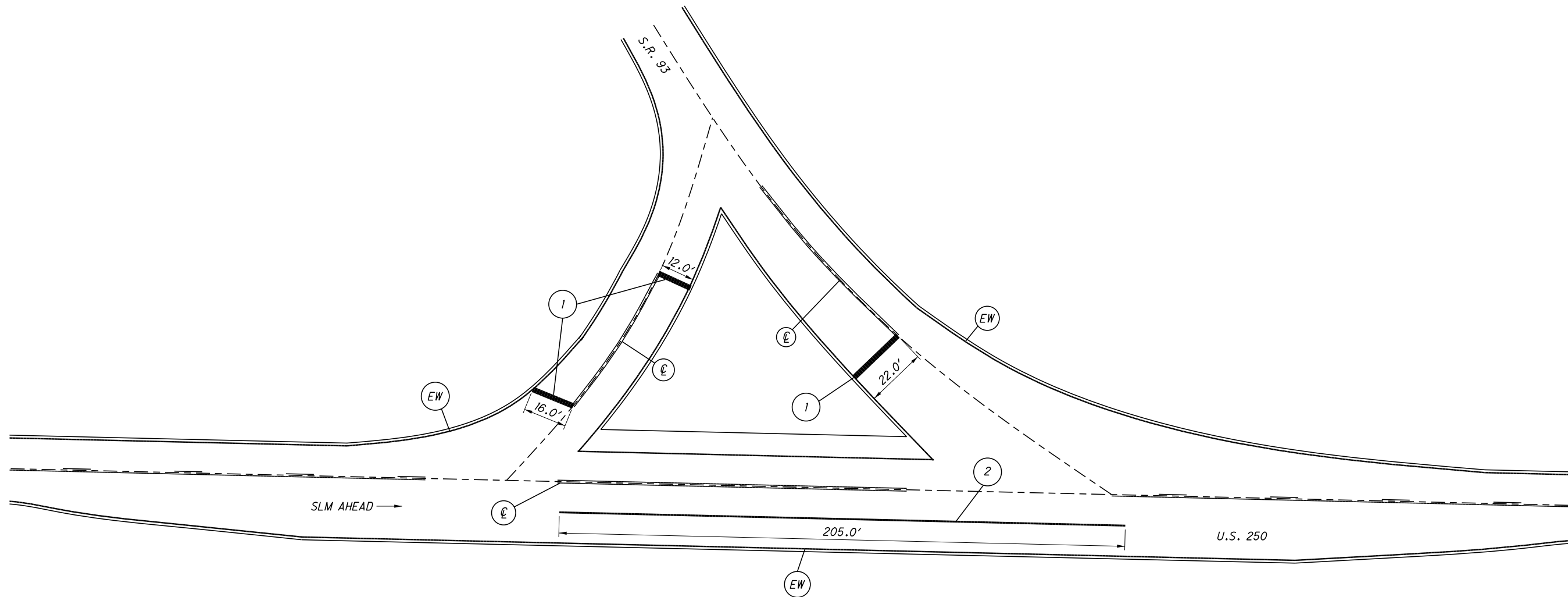
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CHECKED

ANS

U.S. 250
TRAFFIC CONTROL PLAN

TUS-250/ VAR -
0.00/ VAR



ADDITIONAL PAVEMENT MARKINGS (01/NHS/PV)

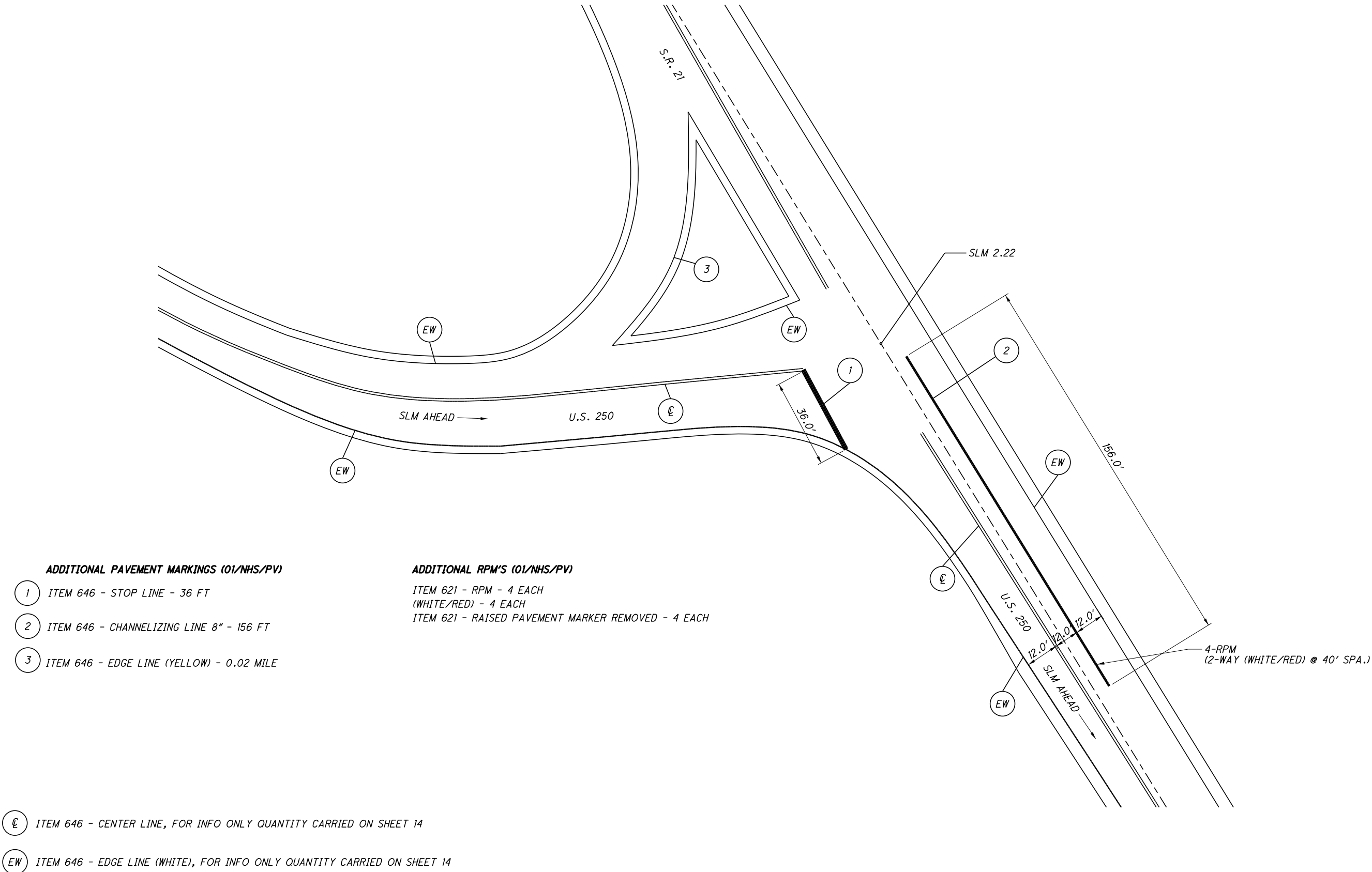
- 1 ITEM 646 - STOP LINE - 50 FT
- 2 ITEM 646 - CHANNELIZING LINE 8" - 205 FT

℄ ITEM 646 - CENTER LINE, FOR INFO ONLY QUANTITY CARRIED ON SHEET 14

EW ITEM 646 - EDGE LINE (WHITE), FOR INFO ONLY QUANTITY CARRIED ON SHEET 14

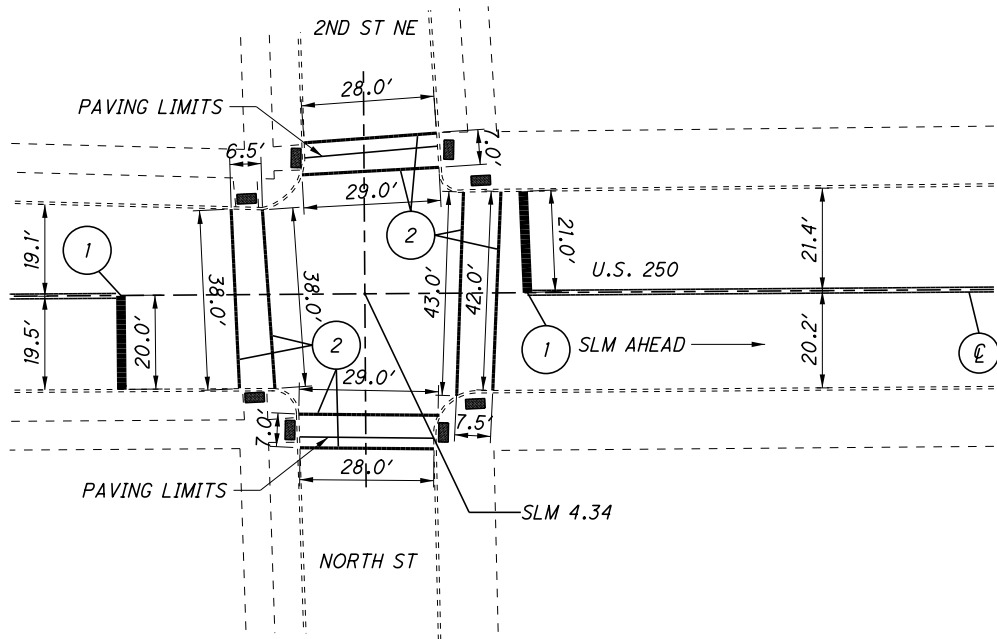
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COUNTY TUS
U.S. 250
SECTION SLM 4.34
PART 1

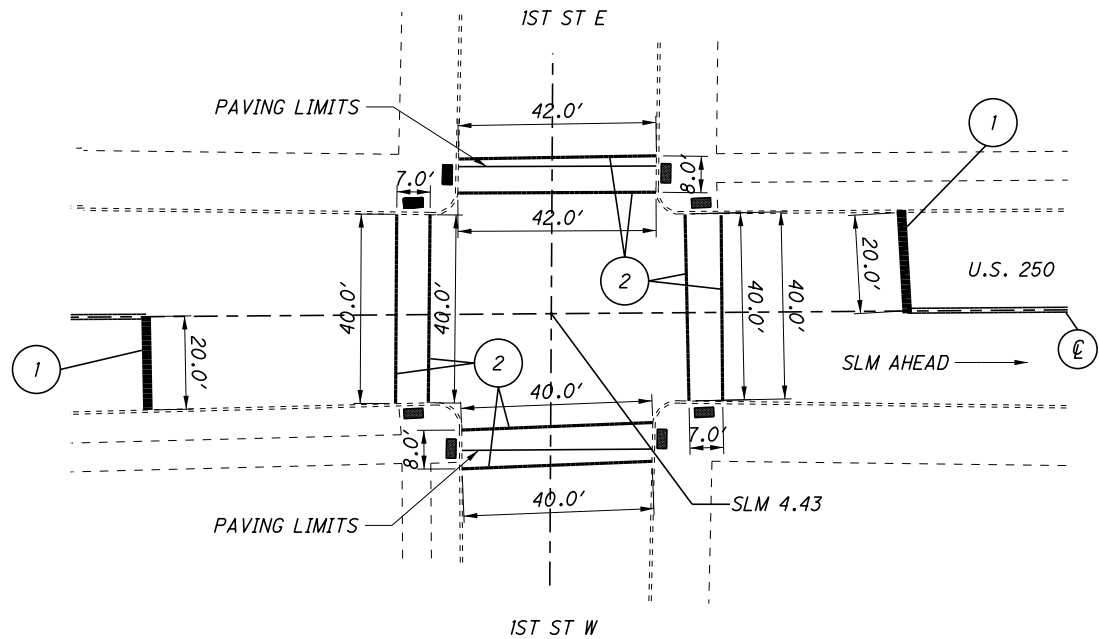


ADDITIONAL PAVEMENT MARKINGS (01/NHS/PV)

- 1 ITEM 646 - STOP LINE - 41 FT
- 2 ITEM 646 - CROSSWALK LINE - 275 FT

℄ ITEM 646 - CENTER LINE, FOR INFO ONLY QUANTITY CARRIED ON SHEET 14

COUNTY TUS
U.S. 250
SECTION SLM 4.43
PART 1



ADDITIONAL PAVEMENT MARKINGS (01/NHS/PV)

- 1 ITEM 646 - STOP LINE - 40 FT
- 2 ITEM 646 - CROSSWALK LINE - 156 FT

℄ ITEM 646 - CENTER LINE, FOR INFO ONLY QUANTITY CARRIED ON SHEET 14



ADDITIONAL RPM'S (01/NHS/PV)

- ITEM 646 - CENTER LINE, FOR INFO ONLY QUANTITY CARRIED ON SHEET 14
- ITEM 646 - EDGE LINE (WHITE), FOR INFO ONLY QUANTITY CARRIED ON SHEET 14

1. ALL QUANTITIES REQUIRED FOR WIDENING SHALL BE USED AS DIRECTED BY THE ENGINEER.

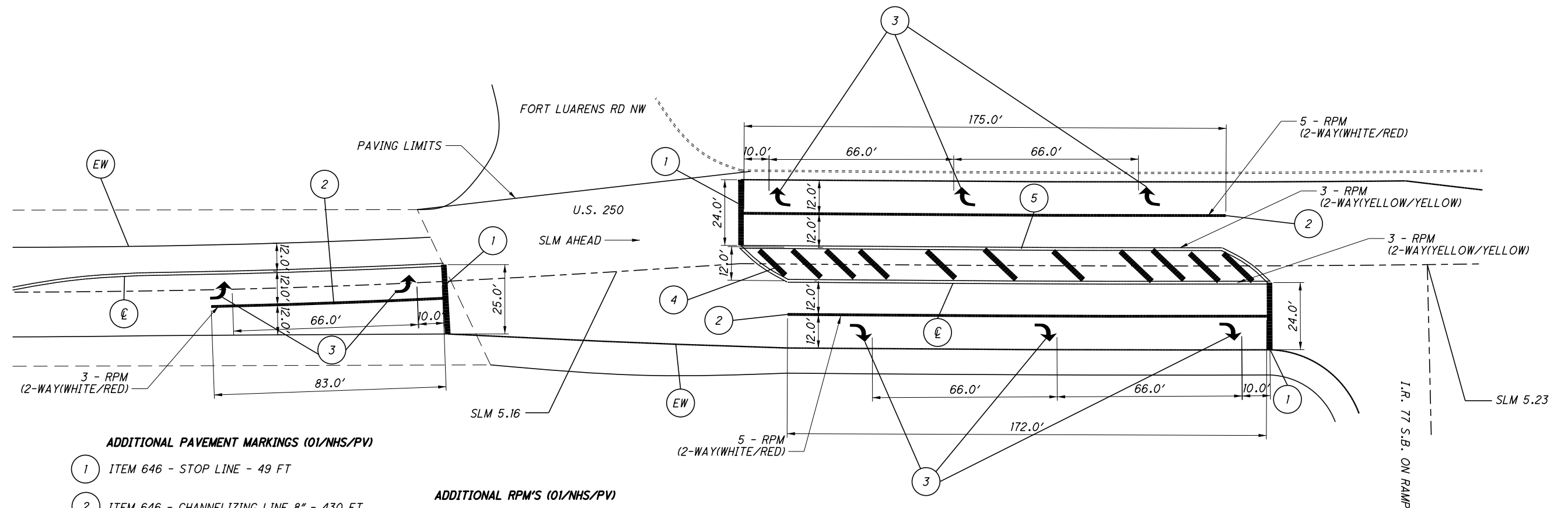


CALCULATED
ADK
CHECKED
ANS

U.S. 250
TRAFFIC CONTROL PLAN

TUS-250/ VAR -
0.00/ VAR

19
21



ADDITIONAL PAVEMENT MARKINGS (01/NHS/PV)

- 1 ITEM 646 - STOP LINE - 49 FT
- 2 ITEM 646 - CHANNELIZING LINE 8" - 430 FT
- 3 ITEM 646 - LANE ARROW - 8 EACH
- 4 ITEM 646 - TRANSVERSE/DIAGONAL LINE (YELLOW) - 165 FT
- 5 ITEM 646 - CENTER LINE - 0.04 MILE

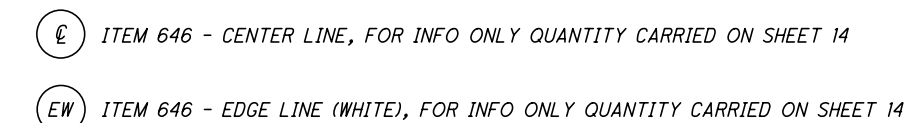
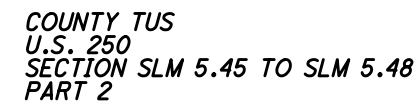
ADDITIONAL RPM'S (01/NHS/PV)

- ITEM 621 - RPM - 19 EACH
- (WHITE/RED) - 13 EACH
- (YELLOW/YELLOW) - 6 EACH
- ITEM 621 - RAISED PAVEMENT MARKER REMOVED - 19 EACH

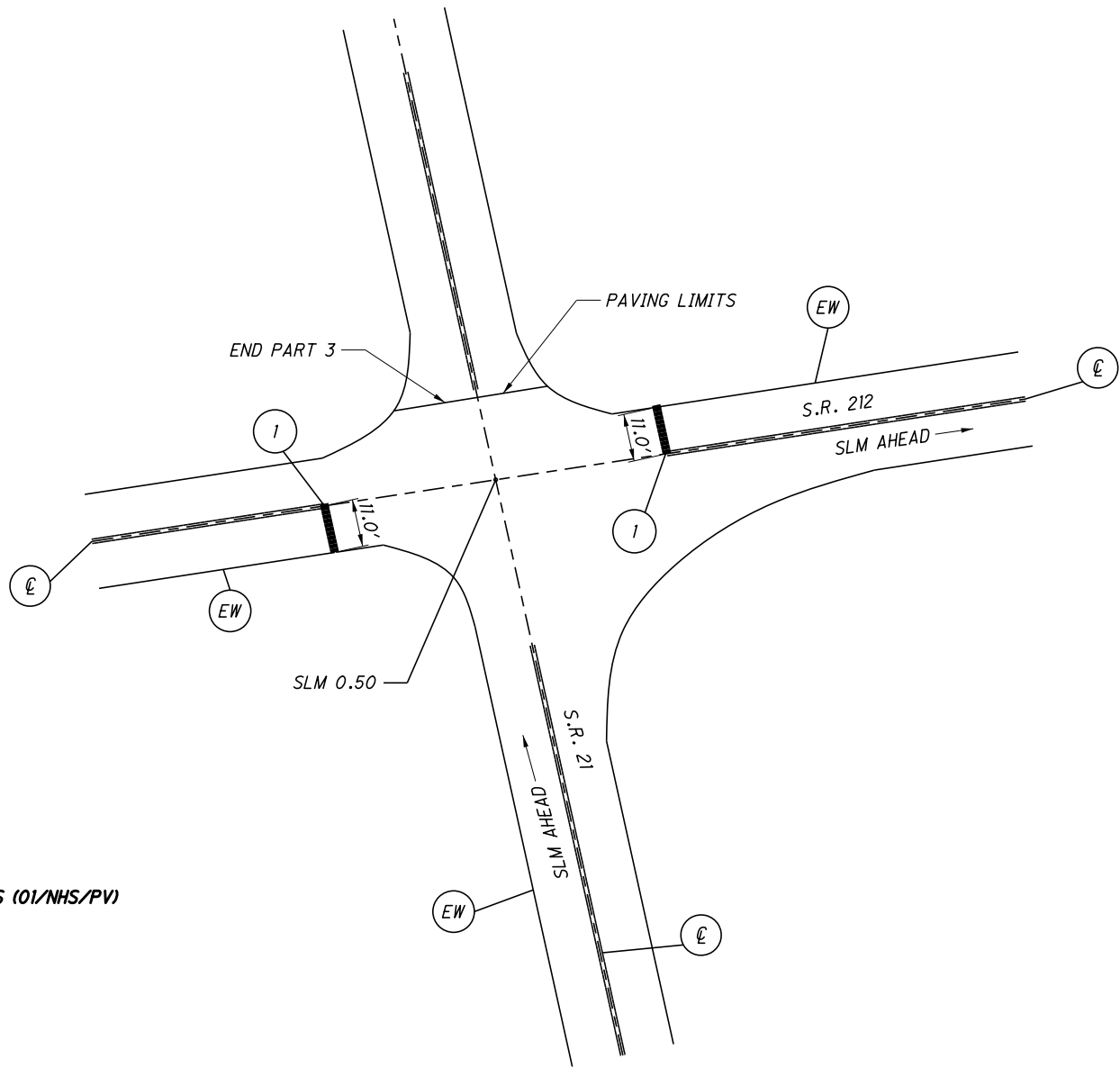
⊕ ITEM 646 - CENTER LINE, FOR INFO ONLY QUANTITY CARRIED ON SHEET 14

EW ITEM 646 - EDGE LINE (WHITE), FOR INFO ONLY QUANTITY CARRIED ON SHEET 14

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COUNTY TUS
S.R. 212
SECTION SLM 0.50
PART 5



ADDITIONAL PAVEMENT MARKINGS (01/NHS/PV)

1 ITEM 646 - STOP LINE - 22 FT

2 ITEM 646 - CENTER LINE, FOR INFO ONLY QUANTITY CARRIED ON SHEET 14

EW ITEM 646 - EDGE LINE (WHITE), FOR INFO ONLY QUANTITY CARRIED ON SHEET 14



CALCULATED	ADK	CHECKED	ANS
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S.R. 212
TRAFFIC CONTROL PLAN

TUS-250/ VAR -
0.00/ VAR