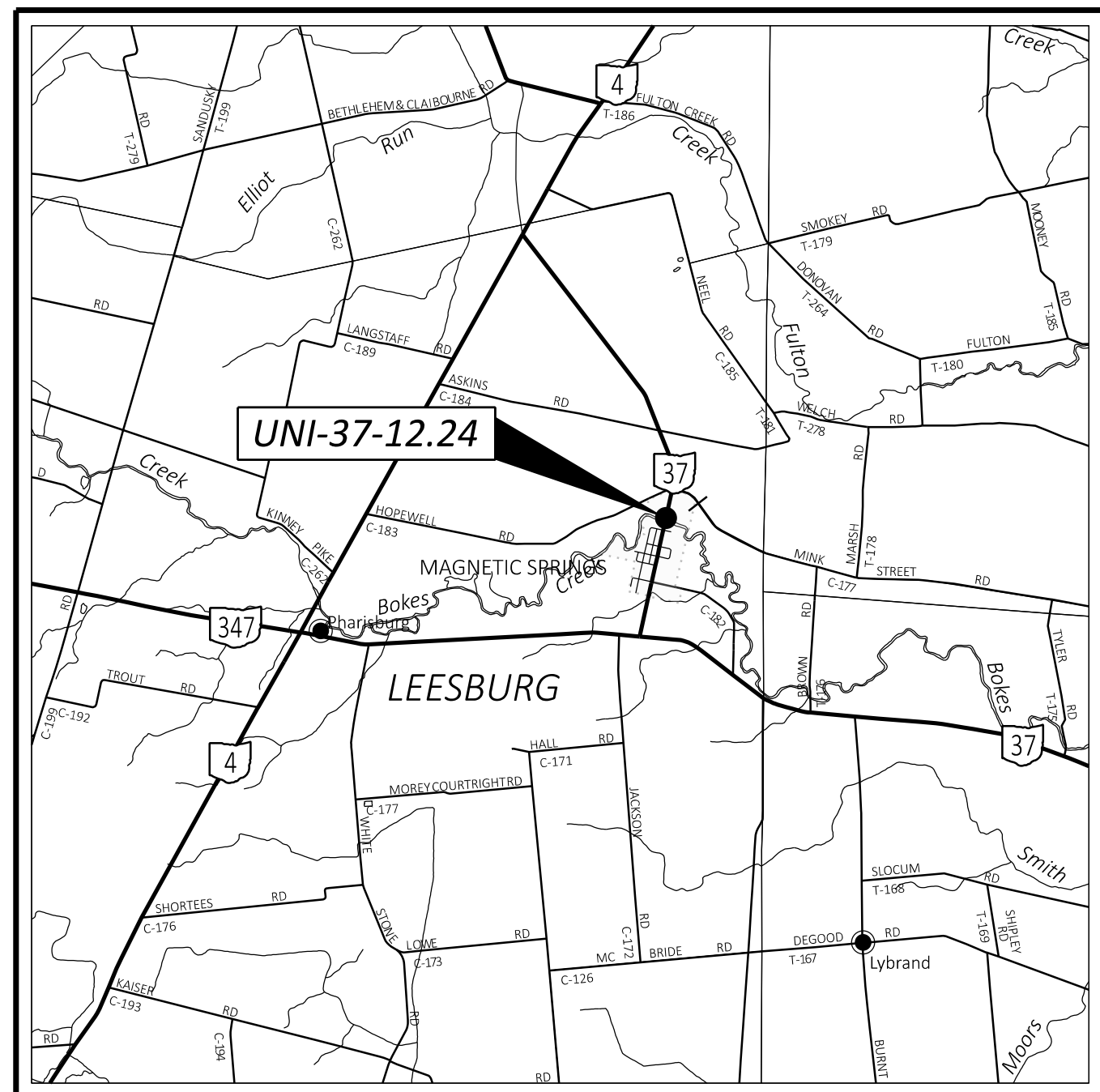


STATE OF OHIO DEPARTMENT OF TRANSPORTATION

UNI-37-12.24

VILLAGE OF MAGNETIC SPRINGS UNION COUNTY



LOCATION MAP

LATITUDE: 40°21'20" LONGITUDE: 83°15'43"



PORTION TO BE IMPROVED	.....	●
INTERSTATE HIGHWAY	=====	
FEDERAL ROUTES	=====	
STATE ROUTES	=====	
COUNTY & TOWNSHIP ROADS	=====	
OTHER ROADS	-----	

DESIGN DESIGNATION

CURRENT ADT (2025)	.....	650
DESIGN YEAR ADT (2045)	.....	700
DESIGN HOURLY VOLUME (2045)	.....	100
DIRECTIONAL DISTRIBUTION	.....	55%
TRUCKS (24 HOUR B&C)	.....	5%
DESIGN SPEED	.....	55 MPH (40 MPH)*
LEGAL SPEED	.....	50 MPH (35 MPH)*
DESIGN FUNCTIONAL CLASSIFICATION:	.....	* SPEED REDUCES AT SLM 12.25
05 MAJOR COLLECTOR (RURAL)	.....
NHS PROJECT	.....	NO

DESIGN EXCEPTIONS

NONE REQUIRED

ADA DESIGN WAIVERS

NONE REQUIRED

UNDERGROUND UTILITIES
Contact Two Working Days
Before You Dig

OHIO811.org
Before You Dig

OHIO811, 8-1-1, or 1-800-362-2764
(Non members must be called directly)



ENGINEER'S SEAL

JONATHAN M.
HUFFMAN
E-82368
REGISTERED
PROFESSIONAL ENGINEER

INDEX OF SHEETS:

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STANDARD CONSTRUCTION DRAWINGS						SUPPLEMENTAL SPECIFICATIONS		SPECIAL PROVISIONS	
BP-2.2	1/15/21	AS-1-15	1/20/23	TC-61.30	7/19/24	800-2023	7/19/24	WATERWAY	
BP-3.1	1/19/24	AS-2-15	7/21/23			832	7/19/24	PERMIT	
BP-5.1	7/15/22	DS-1-92	7/15/22			878	1/21/22	X/XX/XX	
BP-7.1	7/19/24	PSBD-2-07	7/20/18			902	7/19/19		
CB-3A	7/19/24								
		HL-50.21	7/15/22						
DM-1.1	7/17/20								
DM-4.2	7/20/12	MT-101.60	4/21/23						
DM-4.4	1/15/16	MT-101.90	7/17/20						
		MT-105.10	1/17/20						
MGS-1.1	7/16/21								
MGS-2.1	1/19/18	TC-42.10	10/18/13						
MGS-3.3	7/16/21	TC-42.20	10/18/13						
MGS-4.1	1/20/17	TC-52.10	10/18/13						
MGS-4.3	1/18/13	TC-52.20	1/15/21						

FEDERAL PROJECT NUMBER

E241 (105)

RAILROAD INVOLVEMENT

NONE

PROJECT DESCRIPTION

REHABILITATION OF STRUCTURE OVER BOKES CREEK IN MAGNETIC SPRINGS AT SLM 12.24 OF UNI-37. WORK TO CONSIST OF SUPERSTRUCTURE REPLACEMENT FROM NON-COMPOSITE TO COMPOSITE BOX BEAMS AND PIER RECONFIGURATION FROM 3-SPAN TO 2-SPAN. ADDITIONAL WORK TO INCLUDE FULL DEPTH PAVEMENT CORRECTION AND REPLACEMENT OF EXISTING CURB AND CURB INLETS.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA:	0.7 ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA:	0.2 ACRES
NOTICE OF INTENT EARTH DISTURBED AREA:	N/A (NOI NOT REQUIRED)

2023 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS AND CHANGES LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT DETOURS WILL BE PROVIDED AS INDICATED ON SHEET P.06.

Anthony C. Turowski, P.E.
District 06 Deputy Director

Pamela Boratyn
Director, Department of Transportation

UNI-37-12.24

MODEL: 122277_GT001 PAPER SIZE: 34x22 (in.) DATE: 2/20/2025 TIME: 3:08:49 PM USER: jprosnah pwc:\ohiodot-pw-bentley.com\ohiodot-pw-02\Documents\01 Active Projects\District 06\Union\122277\400-Engineering\Roadway\Sheets\122277_GT001.dgn

PID# 122277 UNI-37-12.24

DESIGN AGENCY



DESIGNER

JMB

REVIEWER

JPH 11/01/24

PROJECT ID

122277

SHEET TOTAL

P.01 | 34

NOTIFICATION OF CONSTRUCTION INITIATION:
AT LEAST FOURTEEN DAYS PRIOR TO ANY CONSTRUCTION ACTIVITIES, THE CONTRACTOR SHALL ADVISE THE DISTRICT OFFICE OF COMMUNICATIONS VIA EMAIL AT d06.pio@dot.ohio.gov AND THE DISTRICT WORK ZONE TRAFFIC MANAGER VIA EMAIL AT d06.mot@dot.ohio.gov OF THE ANTICIPATED START DATE OF ANY CONSTRUCTION ACTIVITIES, INCLUDING BUT NOT LIMITED TO THE PLACING OF WORK ZONE SIGNS. THE NOTIFICATION SHALL ALSO INCLUDE THE PROJECT NUMBER, PID, NAME AND PHONE NUMBER OF THE CONTRACTOR, A POINT OF CONTACT AND THE ANTICIPATED IMPACT ON TRAFFIC. THE CONTRACTOR WILL IMMEDIATELY INFORM THE DISTRICT OFFICE OF COMMUNICATIONS AND THE DISTRICT WORK ZONE TRAFFIC MANAGER OF ANY AND ALL DELAYS AND/OR CHANGES REGARDING THE CONSTRUCTION INITIATION DATE.

GENERAL:
THE CONTRACTOR SHALL SUBMIT IN WRITING A SCHEDULE OF OPERATIONS TO THE ENGINEER (SEE 108.02) AND RECEIVE APPROVAL IN WRITING BEFORE WORK IS STARTED ON THIS PROJECT. ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED, MAINTAINED, AND REMOVED BY THE CONTRACTOR IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.

CONTRACTORS EQUIPMENT - OPERATION AND STORAGE:
THE CONTRACTOR'S EQUIPMENT SHALL BE OPERATED IN THE DIRECTION OF TRAFFIC WHERE PRACTICAL. EQUIPMENT SHALL HAVE AT LEAST ONE AMBER FLASHING LIGHT. WHEN PARKED ALONG THE HIGHWAY, THE EQUIPMENT SHALL BE LOCATED EITHER A MINIMUM OF THIRTY FEET FROM THE EDGE OF PAVEMENT OR SIX FEET BEHIND GUARDRAIL WITH A MINIMUM OF 125 FEET OF GUARDRAIL PRECEDING THE EQUIPMENT. ALL OTHER EQUIPMENT, INCLUDING PRIVATE VEHICLES, SHALL BE STORED AT AN APPROVED CONTRACTOR'S STORAGE AREA.

CONTINGENCY QUANTITIES:
THE CONTRACTOR SHALL NOT ORDER MATERIALS OR PERFORM WORK FOR ITEMS DESIGNATED BY PLAN NOTE TO BE USED "AS DIRECTED BY THE ENGINEER" UNLESS AUTHORIZED BY THE ENGINEER. THE ACTUAL WORK LOCATIONS AND QUANTITIES USED FOR SUCH ITEMS SHALL BE INCORPORATED INTO THE FINAL CHANGE ORDER GOVERNING COMPLETION OF THIS PROJECT.

REMOVAL ITEMS:
GUARDRAIL, POSTS, ASPHALT AND MISCELLANEOUS HARDWARE DESIGNATED FOR REMOVAL BECOME PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OF. PAYMENT FOR THE ABOVE SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE REMOVED ITEM.

WORK LIMITS:
THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. THE INSTALLATION AND OPERATION OF ALL TEMPORARY TRAFFIC CONTROL AND TEMPORARY TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS SHALL BE PROVIDED BY THE CONTRACTOR WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

UTILITIES:
LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS:

COLUMBIA GAS OF OHIO DUBLIN ROAD EAST, COLUMBUS, OH 614.481.1057	OHIO EDISON 420 SOUTH YORK ST SPRINGFIELD, OH 45505 937.327.1272
TIME WARNER CABLE 3760 INTERCHANGE DR COLUMBUS, OH 43204 614.255.6349	CENTURYLINK 441 WEST BROAD ST PATASKALA, OH 43062 740.927.8282

THE LOCATION OF THE UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE AS OBTAINED FROM THE OWNERS AS REQUIRED BY SECTION 153.64 O.R.C.

SURVEYING PARAMETERS:
PRIMARY PROJECT CONTROL MONUMENTS GOVERN ALL POSITIONING ON ODOT PROJECTS. SEE BELOW FOR A TABLE CONTAINING PROJECT CONTROL INFORMATION.

USE THE FOLLOWING PROJECT CONTROL, VERTICAL POSITIONING, AND HORIZONTAL POSITIONING PARAMETERS FOR ALL SURVEYING:

PROJECT CONTROL

POSITIONING METHOD: V.R.S.
MONUMENT TYPE: IRON PIN AND MAG NAIL

VERTICAL POSITIONING

ORTHOMETRIC HEIGHT DATUM: NAVD88
GEOID: GEOID12B

HORIZONTAL POSITIONING

REFERENCE FRAME: NAD83 (2011)
ELLIPSOID: GRS80
MAP PROJECTION: TRANSVERSE MERCATOR
COORDINATE SYSTEM: UNION COUNTY LDP

UNITS ARE IN U.S. SURVEY FEET.

LABEL	EASTING	NORTHING	STATION	OFFSET	ELEV.	DESCRIPTION
BM#1	188697.579	494092.881	645+73.91	16.04' LT	935.693	IRON PIN
BM#2	188573.062	493585.033	650+96.56	0.00'	934.791	MON BOX
BM#3	188708.697	494222.296	644+45.03	0.09' RT	935.957	MAG NAIL

ITEM 201 - CLEARING AND GRUBBING, AS PER PLAN:
ALTHOUGH THERE ARE NO TREES OR STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE LIMITS OF THE PROJECT, A LUMP SUM QUANTITY IS INCLUDED IN THE GENERAL SUMMARY FOR ITEM 201, CLEARING AND GRUBBING, AS PER PLAN. THIS SHALL ALSO INCLUDE DEBRIS REMOVAL OF BUT NOT LIMITED TO LOGS, TRUNKS AND TREE BRANCHES. THE CONTRACTOR SHALL REVIEW THE SITE TO ACCURATELY BID THIS ITEM. ALL PROVISIONS AS SET FORTH IN THE SPECIFICATIONS UNDER THIS ITEM ARE INCLUDED IN THE LUMP SUM PRICE BID FOR ITEM 201, CLEARING AND GRUBBING, AS PER PLAN.

ENDANGERED BAT HABITAT REMOVAL:
THIS PROJECT IS LOCATED WITHIN THE KNOWN HABITAT RANGES OF THE FEDERALLY LISTED AND PROTECTED INDIANA BAT, AND NORTHERN LONG-EARED BAT. NO TREES SHALL BE REMOVED UN THIS PROJECT FROM APRIL 1 THROUGH SEPTEMBER 30. ALL NECESSARY TREE REMOVAL SHALL OCCUR FROM OCTOBER 1 THROUGH MARCH 31. THIS REQUIREMENT IS NECESSARY TO AVOID AND MINIMIZE IMPACTS TO THESE SPECIES AS REQUIRED BY THE ENDANGERED SPECIES ACT (ESA). FOR THE PURPOSED OF THIS NOTE, A TREE IS DEFINED AS: A LIVE, DYING OR DEAD WOODY PLANT, WITH A TRUNK 3 INCHES OR GREATER IN DIAMETER AT HEIGHT OF 4.5 FEET ABOVE THE GROUND SURFACE, AND WITH A MINIMUM HEIGHT OF 13 FEET.

CONNECTION BETWEEN EXISTING AND PROPOSED GUARDRAIL
WHEN IT IS NECESSARY TO SPLICE PROPOSED GUARDRAIL TO EXISTING GUARDRAIL, ONLY THE EXISTING GUARDRAIL SHALL BE CUT, DRILLED, OR PUNCHED. THE CONNECTION SHALL BE MADE USING A W-BEAM, BEAM SPLICE AS SHOWN IN AASHTO M 180-12, EXCEPT THE BEAM WASHERS ARE NOT TO BE USED. PAYMENT SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE RESPECTIVE GUARDRAIL ITEMS.

ITEM 202 - PAVEMENT REMOVED, AS PER PLAN (15")
PAVEMENT REMOVED, ASPHALT WILL BE AN AVERAGE OF 15.00" BUT MAY VARY IN DEPTH TO MATCH THE EXISTING ASPHALT SUBGRADE AS DETAILED IN THE TYPICAL SECTIONS. PAVEMENT REMOVED WILL INCLUDE THE REMOVAL OF EXISTING AGGREGATE BASE COURSE(S) OR MACADAM AS DETAILED IN THE TYPICAL SECTIONS. ITEM 202 - PAVEMENT REMOVED, ASPHALT INCLUDES SAWCUTTING AS DETAILED IN THE TYPICAL SECTIONS. IT IS THE INTENT OF THE SAWCUT TO PROVIDE A SMOOTH JOINT AT THE REMOVAL. ANY OTHER METHOD THAT THE CONTRACTOR PROPOSES MAY BE USED IF APPROVED BY THE ENGINEER.

ITEM 202 - WALK REMOVED, AS PER PLAN:
THIS ITEM HAS BEEN PROVIDED AND SHALL BE USED IN CONJUNCTION WITH THE CONSTRUCTION OF THE PROPOSED CURB RAMPS. THE CONTRACTOR SHALL PROVIDE ALL MATERIAL, LABOR, EQUIPMENT, AND HARDWARE NECESSARY TO REMOVE EXISTING SIDEWALK (INCLUDING ANY EXISTING CURB RAMPS). SPECIFIC LOCATIONS AND LIMITS OF THIS WORK CAN BE FOUND ON PLAN SHEET P.09.

ITEM 202 - GUARDRAIL REMOVED, AS PER PLAN:
ITEM 202 - ANCHOR ASSEMBLY REMOVED, TYPE A, AS PER PLAN:
IN ADDITION TO THE REQUIREMENTS OF ITEM 202, REMOVAL OF SPECIFIED GUARDRAIL ITEMS SHALL INCLUDE BUT NOT BE LIMITED TO ANY ATTACHED POSTS, SIGNS AND DELINEATORS (NOT OTHERWISE SPECIFIED). THIS REMOVAL WILL INCLUDE ALL POSTS, ANCHORS AND HARDWARE UNDER GROUND.

THE CONTRACTOR SHALL EXPECT TO REMOVE ALL CONCRETE FOUNDATIONS COMPLETELY AT ALL LOCATIONS UNLESS OTHERWISE INSTRUCTED OR APPROVED BY THE ENGINEER. REMOVING EXISTING CONCRETE FOUNDATION TO A MINIMUM OF 1 FOOT BELOW THE GRADE OF THE SURROUNDING AREA MAY ONLY BE PERMITTED IF THE EXISTING CONCRETE DOES NOT FALL WITHIN 6 FEET OF THE PROPOSED AS TO NOT COMPROMISE THE PERFORMANCE OF THE PROPOSED GUARDRAIL SYSTEM(S).

EXISTING TYPE T ANCHOR ASSEMBLIES (NOT IDENTIFIED IN THE PLANS) ENCOUNTERED WITHIN REMOVAL LIMITS SHALL BE REMOVED UNDER THE QUANTITIES PROVIDED FOR ITEM 202 - GUARDRAIL REMOVED, AS PER PLAN, AND BE PAID FOR BY "FEET".

ALL HOLES AND VOIDS REMAINING AFTER REMOVAL OF GUARDRAIL POSTS AND FOUNDATIONS SHALL BE FILLED WITH GRANULAR MATERIAL CONFORMING TO CMS 203.02R. FILL MATERIAL CONTAINING SOD SHALL NOT BE USED. ALL FILL MATERIAL SHALL BE APPROVED BY THE ENGINEER. MATERIAL PLACED IN HOLES SHALL BE THOROUGHLY COMPACTED AND LEVELED OFF AS DIRECTED BY THE ENGINEER. PAYMENT FOR THE ABOVE SHALL BE INCLUDED IN THE UNIT PRICE BID FOR THE APPLICABLE GUARDRAIL REMOVAL ITEM.

NO HAZARD SHALL BE LEFT UNPROTECTED EXCEPT FOR THE ACTUAL TIME NECESSARY TO REMOVE THE EXISTING GUARDRAIL, PREPARE THE SITE, AND INSTALL NEW GUARDRAIL IN A CONTINUOUS OPERATION. GUARDRAIL DESIGNATED FOR REMOVAL SHALL BECOME PROPERTY OF THE CONTRACTOR AND SHALL BE DISPOSED OF.

ITEM 203 - EMBANKMENT, AS PER PLAN:
QUANTITIES FOR ITEM 203 EMBANKMENT, AS PER PLAN HAVE BEEN PROVIDED TO BUILD UP FORESLOPES AND ENSURE PROPER GRADING FOR THE PROPOSED WINGWALLS, MGS GUARDRAIL, AND BRIDGE TERMINAL ASSEMBLIES. THIS ITEM OF WORK INCLUDES ANY CLEARING AND GRUBBING NECESSARY TO PLACE THE EMBANKMENT AT THE LOCATIONS SPECIFIED OR DIRECTED PER DETAIL ON P.02.

THE FOLLOWING QUANTITIY HAS BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER.

ITEM 203 - EMBANKMENT, AS PER PLAN = 15 CU. YD.

ITEM 606 - MGS BRIDGE TERMINAL ASSEMBLY, TYPE TST-2, AS PER PLAN:
IN ADDITION TO ITEM 606 - MGS BRIDGE TERMINAL ASSEMBLY, TYPE TST-2, 'POST #1' PER SCD MGS-3.3 SHALL BE CONCRETE ENCASED TO COINCIDE WITH 'POST A' OF SCD MGS-4.3 TO TRANSITION BETWEEN TST-2 BTA AND ANCHOR ASSEMBLY, MGS TYPE A.

ITEM 608 - 4" CONCRETE WALK, AS PER PLAN:
THIS ITEM SHALL BE USED TO PROVIDE ADEQUATE GRADE TRANSITIONS BETWEEN THE EXISTING SIDE WALK AND THE PROPOSED CURB RAMPS. THIS ITEM SHALL INCLUDE THE COST OF FURNISHING ALL MATERIALS, GRADING, FORMING AND FINISHING OF THE SIDEWALK AT THE PROPOSED GRADE OF THE TRANSITION. SPECIFIC LOCATIONS AND LIMITS OF THIS WORK CAN BE FOUND ON PLAN SHEET P.09.

GRADING AND EROSION CONTROL:
THE FOLLOWING QUANTITIES ARE PROVIDED TO PROMOTE GROWTH AND CARE OF PERMANENT SEEDED AREAS:

659 - TOPSOIL	200 CU. YD.
659 - SEEDING AND MULCHING	1,794 SQ. YD.
659 - REPAIR SEEDING AND MULCHING	90 SQ. YD.
659 - INTER-SEEDING	90 SQ. YD.
659 - COMMERCIAL FERTILIZER	0.24 TON
659 - LIME	0.37 ACRES
659 - WATER	10 M. GAL.
670 - SLOPE EROSION PROTECTION	1,794 SQ. YD.

SEEDING AND MULCHING SHALL BE APPLIED TO ALL AREAS OF EXPOSED SOIL BETWEEN THE RIGHT-OF-WAY LINES, AND WITHIN THE CONSTRUCTION LIMITS FOR AREAS OUTSIDE THE RIGHT-OF-WAY LINES COVERED BY WORK AGREEMENT OR SLOPE EASEMENT. QUANTITY CALCULATIONS FOR SEEDING AND MULCHING ARE BASED ON THESE LIMITS.

REVIEW OF DRAINAGE FACILITIES:
BEFORE ANY WORK IS STARTED ON THE PROJECT AND AGAIN BEFORE FINAL ACCEPTANCE OF THE PROJECT BY THE STATE, REPRESENTATIVES OF THE STATE AND THE CONTRACTOR, ALONG WITH LOCAL REPRESENTATIVES, SHALL INSPECT ALL EXISTING SEWERS WHICH ARE TO REMAIN IN SERVICE AND WHICH MAY BE AFFECTED BY THE WORK. THE CONDITION OF THE EXISTING CONDUITS AND THEIR APPURTENANCE SHALL BE DETERMINED FROM FIELD OBSERVATIONS. RECORDS OF THE INSPECTION SHALL BE KEPT IN WRITING ALONG WITH PHOTOS BY THE STATE.

ALL NEW CONDUITS, INLETS, CATCH BASINS, AND MANHOLES CONSTRUCTED, AS A PART OF THE PROJECT SHALL BE FREE OF ALL FOREIGN MATTER AND IN A CLEAN CONDITION BEFORE THE PROJECT WILL BE ACCEPTED BY THE STATE.

ALL EXISTING SEWERS INSPECTED INITIALLY BY THE ABOVE MENTIONED PARTIES SHALL BE MAINTAINED AND LEFT IN A CONDITION REASONABLY COMPARABLE TO THAT DETERMINED BY THE ORIGINAL INSPECTION. THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER SHALL CORRECT ANY CHANGE IN THE CONDITION RESULTING FROM THE CONTRACTOR'S OPERATIONS.

PAYMENT FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 611 CONDUIT ITEMS.



DESIGNER	JMB
REVIEWER	JPH 11/01/24
PROJECT ID	122277
SHEET	TOTAL
P.03	34

ITEM 617 - WATER:

THE FOLLOWING CONTINGENCY QUANTITY HAS BEEN PROVIDED AND CARRIED TO THE GENERAL SUMMARY:

ITEM 617 - WATER = 1 M. GAL.

ITEM 304 – AGGREGATE BASE, AS PER PLAN:

THIS ITEM SHALL CONFORM TO ITEM 304 OF THE OHIO DEPARTMENT OF TRANSPORTATION CONSTRUCTION NAMD MATERIAL SPECIFICATIONS. THE QUANTITY PROVIDED IN THE PAVEMENT CALCULATIONS HAS BEE ESTIMATED USING A 6" THICKNESS.

THE SUBGRADES OF THE EXISTING ABUTTING PAVEMENT AND PROPOSED PAVEMENT SHOULD MEET AT THE SAME ELEVATION IF NECESSARY, THE AGGREGATE BASE UNDER REPLACEMENT SHOULD BE THICKENED SO THE SUBGRADE ELEVATIONS WILL MATCH. IF THE AGGREGATE BASE UNDER THE REPLACEMENT IS THICKER THAN THE EXISTING AGGREGATE BASE, THE PROPOSED SUBGRADE SHOULD BE SLOPED AWAY FROM THE EXISTING AND DRAINAGE PROVIDED PER PDM 504.7.

IF THE ACTUAL QUANTITY USED IS LESS THAN THE AMOUNT BID, THE ENGINEER WILL DETERMINE THE CU. YDS. FOR NON-PAYMENT BY TAKING THE DIFFERENCE IN DEPTHS AND MULTIPLYING IT BY THE AREA OF PAVEMENT.

FARM DRAINS

PROVIDE UNOBSTRUCTED OUTLETS TO ALL FARM DRAINS ENCOUNTERED DURING CONSTRUCTION. REPLACE EXISTING COLLECTORS WHICH ARE LOCATED BELOW THE ROADWAY WITHIN THE ELEVATIONS, AND WHICH CROSS THE ROADWAY WITHIN THE (RIGHT OF WAY)(CONSTRUCTION) LIMITS WITH ITEM 611, CONDUIT, TYPE B, ONE COMMERCIAL SIZE LARGER THAN THE EXISTING CONDUIT.

OUTLET EXISTING COLLECTORS AND ISOLATED FARM DRAINS, WHICH ARE ENCOUNTERED ABOVE THE ELEVATION OF ROADWAY DITCHES INTO THE ROADWAY DITCH USING ITEM 611, TYPE F CONDUIT. THE OPTIMUM OUTLET ELEVATION IS ONE FOOT ABOVE THE FLOWLINE ELEVATION OF THE DITCH. INTERCEPT LATERAL FIELD TILES WHICH CROSS THE ROADWAY WITH ITEM 611, TYPE E CONDUIT, AND CARRY IN A LONGITUDINAL DIRECTION TO AN ADEQUATE OUTLET OR ROADWAY CROSSING.

THE LOCATION, TYPE, SIZE AND GRADE OF REPLACEMENTS IS DETERMINED BY THE ENGINEER AND PAYMENT MADE ON FINAL MEASUREMENTS.

PROVIDE EROSION CONTROL PADS AT THE OUTLET END OF ALL FARM DRAINS PER STANDARD CONSTRUCTION DRAWING DM-1.1, EXCEPT WHEN THEY OUTLET INTO A DRAINAGE STRUCTURE.

PAYMENT FOR THE EROSION CONTROL PADS AND ANY NECESSARY BENDS OR BRANCHES IS INCLUDED FOR PAYMENT IN THE PERTINENT CONDUIT ITEMS.

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR THE WORK NOTED ABOVE:

ITEM 611 - 4" CONDUIT, TYPE C = 20 FT
 ITEM 611 - 6" CONDUIT, TYPE C = 20 FT.
 ITEM 611 - 8" CONDUIT, TYPE C = 20 FT.

ENVIRONMENTAL COMMITMENTS:

1) ECOLOGICAL STUDIES IDENTIFIED SWALLOW NESTS ON THE UNI-37-12.24 (SFN 8001936) BRIDGE. IF CONSTRUCTION ACTIVITIES WILL OCCUR BETWEEN MAY 1 AND AUGUST 31 ON THIS STRUCTURE, INSPECT THE STRUCTURE FOR EVIDENCE OF AN ACTIVE BIRD NEST CONTAINING AN EGG OR CHICK PRIOR TO STARTING WORK. PROVIDE WRITTEN CONFIRMATION OF THE INSPECTION, INCLUDING A STATEMENT WHETHER AN ACTIVE NEST WAS FOUND, TO THE ENGINEER. IF NO NESTS ARE ENCOUNTERED DURING THE INSPECTION, OR IF ONLY INACTIVE NESTS THAT DO NOT CONTAIN AN EGG OR CHICK ARE ENCOUNTERED, PROCEED WITH CONSTRUCTION ACTIVITIES. THE CONTRACTOR MAY REMOVE AND DESTROY INACTIVE NESTS. THE CONTRACTOR MAY INSTALL EXCLUSION MEASURES BETWEEN AUGUST 31 AND MAY 1 TO PREVENT MIGRATORY BIRDS FROM NESTING ON THE STRUCTURE. PROJECTS PERFORMING CONSTRUCTION ACTIVITIES BETWEEN THE DATES OF SEPTEMBER 1 AND APRIL 30 DO NOT REQUIRE AN INSPECTION FOR MIGRATORY BIRDS OR AVOIDANCE MEASURES. IF AN ACTIVE NEST CONTAINING AN EGG OR CHICK IS ENCOUNTERED, AVOID IMPACTS TO THE NEST UNTIL ALL DEVELOPING BIRDS ARE ABLE TO INDEPENDENTLY FLY FROM THE NEST. IF AN ACTIVE NEST CONTAINING AN EGG OR CHICK CANNOT BE AVOIDED, CONTACT THE ENGINEER AT LEAST 4 WEEKS PRIOR DESTROYING AN ACTIVE NEST SO THE CONTRACTOR CAN OBTAIN A DEPREDATION PERMIT FROM THE U.S. FISH AND WILDLIFE SERVICE. THE CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COSTS AND COMPLETING ALL TASKS RELATED TO OBTAINING THE DEPREDATION PERMIT EXCEPT FOR DIRECT COORDINATION WITH THE MIGRATORY BIRD REGIONAL PERMIT OFFICE. DO NOT PROCEED WITH ACTIVITIES THAT WILL IMPACT AN ACTIVE NEST UNTIL THE ENGINEER CONFIRMS THE DEPREDATION PERMIT IS RECEIVED.

2) ODOT WILL OBTAIN WATERWAY PERMITS PRIOR TO CONSTRUCTION AND WILL PROVIDE SPECIAL PROVISIONS. ALL WATERWAY PERMIT SPECIAL PROVISIONS WILL BE ADHERED TO DURING CONSTRUCTION.

3) ANY AND ALL CONSTRUCTION DEBRIS, EARTHEN DEBRIS, EXCESS ASPHALT OR CONCRETE, WOOD DEBRIS FROM CLEARING, EXCESS FILL MATERIAL, AND TRASH SHOULD BE DISPOSED OF AT AN APPROVED UPLAND SITE OR LAND FILL ABOVE FEMA 100-YEAR FLOOD ELEVATIONS.

4) ODOT WILL ENSURE IMPACTS TO THE FEDERALLY LISTED AND PROTECTED INDIANA BAT AND NORTHERN LONG-EARED BAT AND THE STATE LISTED AND PROTECTED LITTLE BROWN BAT AND TRICOLORED BAT ARE AVOIDED AND MINIMIZED. NO TREE REMOVAL IS ALLOWED FROM APRIL 1 THROUGH SEPTEMBER 30. PERFORM ALL NECESSARY TREE REMOVAL FROM OCTOBER 1 THROUGH MARCH 31. DEMARCATÉ CLEARING LIMITS IN THE FIELD TO AVOID ANY UNAUTHORIZED TREE CLEARING. FOR THE PURPOSES OF THIS NOTE, A TREE IS DEFINED AS A LIVE, DYING, OR DEAD WOODY PLANT, WITH A TRUNK THREE INCHES OR GREATER IN DIAMETER AT A HEIGHT OF 4.5 FEET ABOVE THE GROUND SURFACE, AND WITH A MINIMUM HEIGHT OF 13 FEET.

CONSTRUCTION NOISE

ACTIVITIES AND LAND USE ADJACENT TO THIS PROJECT MAY BE AFFECTED BY CONSTRUCTION NOISE. IN ORDER TO MINIMIZE ANY ADVERSE CONSTRUCTION NOISE IMPACTS, DO NOT OPERATE POWER-OPERATED CONSTRUCTION-TYPE DEVICES BETWEEN THE HOURS OF 8 PM AND 7 AM. IN ADDITION, DO NOT OPERATE AT ANY TIME ANY DEVICE IN SUCH A MANNER THAT THE NOISE CREATED SUBSTANTIALLY EXCEEDS THE NOISE CUSTOMARILY AND NECESSARILY ATTENDANT TO THE REASONABLE AND EFFICIENT PERFORMANCE OF SUCH EQUIPMENT.

ITEM SPECIAL - WATER FILLER BLADDER:

THIS WORK SHALL CONSIST OF FURNISHING, INSTALLATION AND REMOVAL OF WATER FILLED BLADDERS IN ACCORDANCE WITH THE PLAN DETAILS.

WATER FILLED BLADDERS SHALL BE INSTALLED PER MANUFACTURER GUIDELINES AND MAINTAIN A MINIMUM TOP OF BLADDER ELEVATION OF 926.20 FT. USE ONE OF THE MANUFACTURERS BELOW, OR AN APPROVED EQUAL.

AQUA DAM, INC. P.O. BOX 144 121 MAIN ST. STE A SCOTIA, CA 95565 https://www.aquadam.net/	DAM-IT DAMS INC. 546 E REID ROAD GRAND BLANC, MI 48439 https://damitdams.com/
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WATER FILLED BLADDERS ARE PAID FOR AT THE UNIT PRICE BID FOR ITEM SPECIAL - WATER FILLED BLADDER. THIS PRICE INCLUDES THE COST FOR MATERIAL, EQUIPMENT, LABOR AND ALL INCIDENTALS REQUIRED TO INSTALL AND REMOVE BLADDERS FOR PURPOSES OF DEWATERING AND DEMOLITION OF STRUCTURE (UNI-37-1224).

ITEM 611 - 72" CONDUIT, TYPE A, AS PER PLAN, 706.02

IN ADDITION TO THE REQUIREMENTS OF ITEM 611, WORK FOR THIS ITEM INCLUDES THE REMOVAL OF THE 72 INCH CONDUIT AFTER THE BRIDGE DEMOLITION PHASE AND TRANSPORTING TO DISTRICT 6 AND FINAL BACKFILL AFTER THE CONDUIT IS REMOVED. ALL BACKFILL SHALL MEET THE REQUIREMENTS SPECIFIED IN ITEM 203. THE CONTRACTOR SHALL NOTIFY AND COORDINATE WITH THE DISTRICT ROADWAY SERVICE ENGINEER, SAM MORRISON AT 740-833-8023, PRIOR TO TRANSPORT. A PERFORMANCE INSPECTION PER 611.12 IS WAIVED AND THE CONDUIT TO BE SALVAGED SHALL BE FREE OF DEFECTS AND CLEANED BEFORE TRANSPORTING.

DESIGN AGENCY



DESIGNER

JMB

REVIEWER

JPH 11/01/24

PROJECT ID

122277

SHEET

P.04

TOTAL

34

GENERAL:
ALL TRAFFIC CONTROL DEVICES SHALL BE FURNISHED, ERECTED, MAINTAINED, AND REMOVED BY THE CONTRACTOR IN ACCORDANCE WITH THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (CURRENT EDITION). COPIES ARE AVAILABLE FROM:

THE OHIO DEPARTMENT OF TRANSPORTATION
BUREAU OF TRAFFIC,
1980 WEST BROAD STREET
COLUMBUS, OHIO 43223.

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH CMS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

NOTIFICATION OF TRAFFIC RESTRICTIONS:
THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE BUT IS NOT LIMITED TO ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION TIME FRAME TABLE		
ITEM	DURATION OF CLOSURE	NOTIFICATION DUE TO DISTRICT 6 COMMUNICATIONS OFFICE
ROAD CLOSURES	>= 2 WEEKS	21 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	<= 12 HOURS	4 BUSINESS DAYS PRIOR TO CLOSURE
LANE CLOSURES & RESTRICTIONS	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	< 2 WEEKS	5 BUSINESS DAYS PRIOR TO CLOSURE
START OF CONSTRUCTION & TRAFFIC PATTERN CHANGES	N/A	14 CALENDAR DAYS PRIOR TO IMPLEMENTATION

NOTIFICATION OF TRAFFIC RESTRICTIONS (CONT...):
ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME FRAME TABLE.

PUBLIC OUTREACH AND NOTIFICATION (ROAD CLOSURE)
THE CONTRACTOR SHALL BE RESPONSIBLE FOR CONTACTING THE DISTRICT 6 PUBLIC INFORMATION OFFICE VIA EMAIL AT d06.pio@dot.ohio.gov TO COORDINATE EFFORTS TO NOTIFY ALL LOCAL COUNTY, STATE AND FEDERAL EMERGENCY SERVICES, SCHOOL DISTRICTS AND ADJACENT RESIDENTS AND BUSINESSES OF THE UPCOMING CLOSURE. ADVANCE NOTIFICATION SHALL OCCUR NO LATER THAN TWENTY-ONE (21) DAYS PRIOR TO CLOSING THE ROAD. IF, SUBSEQUENT TO THE ADVANCE NOTIFICATION, THE START DATE IS CHANGED, THEN A NEW SEVEN (7) DAY NOTIFICATION WILL BE REQUIRED. THE ROAD CANNOT BE CLOSED UNLESS PRIOR NOTIFICATION HAS BEEN ACCOMPLISHED. THE SAME PARTIES SHALL BE NOTIFIED WHEN THE CLOSURE HAS CONCLUDED AND THE ROAD IS BACK OPEN TO TRAFFIC. ALL NOTIFICATIONS SHALL BE MADE UTILIZING THE TEMPLATE PROVIDED BY THE DISTRICT 6 PUBLIC INFORMATION OFFICE.

ITEM 614, MAINTAINING TRAFFIC:
FULL CLOSURE OF SR-37 AT THE PROJECT WILL BE PERMITTED TO COMPLETE ALL WORK. THE SIGNED AND LOCAL DETOUR ROUTES ARE SHOWN ON SHEET P.06.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR 614, DETOUR SIGNING AND ITEM 614 MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

ACCESS TO PRIVATE PROPERTY:
MAINTAIN ACCESS TO COMMERCIAL PROPERTIES WITH ONLY ONE DRIVEWAY AT ALL TIMES BY USE OF PART WIDTH CONSTRUCTION. FOR COMMERCIAL PROPERTIES WITH MULTIPLE DRIVEWAYS, DO NOT CLOSE MORE THAN ONE DRIVEWAY AT A TIME.

MAINTAIN ACCESS TO RESIDENTIAL PROPERTIES AT ALL TIMES. WHEN A RESIDENTIAL DRIVE IS CLOSED FOR CONSTRUCTION, MAINTAIN ALTERNATE ACCESS TO THE PROPERTY. IT MAY BE REQUIRED FOR THE CONTRACTOR TO MAINTAIN ONE PASSABLE LANE WITHIN A CLOSURE IN ORDER FOR VEHICLES TO ACCESS RESIDENCY WITH A VEHICLE.

SUCCESSFULLY NOTIFY THE OCCUPANTS/OWNERS OF COMMERCIAL OR RESIDENTIAL DRIVES TO BE CLOSED AND COORDINATE THE CLOSURE AT LEAST 48 HOURS BEFORE THE CLOSURE BEGINS (SIMPLY LEAVING A WRITTEN NOTICE OR PHONE MESSAGE IS NOT SUFFICIENT). COORDINATE ALTERNATE ACCESS TO RESIDENTIAL PROPERTIES WITH THE OWNER/OCCUPANT.

A+B BIDDING CONTRACT TABLE (PN 124)
USE THE FOLLOWING INFORMATION IN COMBINATION WITH THE PROPOSAL NOTE A + B BIDDING: THE CONTRACTOR WILL BID THE NUMBER OF CALENDAR DAYS FROM JUNE 1, 2025 TO OPENING OF UNI-37 TO TRAFFIC. THE BEGINNING DATE FOR CHARGING CALENDAR DAYS SHALL BE JUNE 1, 2025. THE CONTRACTOR MAY BEGIN WORK PRIOR TO THIS DATE.

CONTRACT SEGMENT - LOCATON OF CRITICAL WORK	MINIMUM DAYS	MAXIMUM DAYS	INCENTIVE / DISINCENTIVE \$ PER DAY	MAXIMUM INCENTIVE \$
OPEN US-37 TO 2 LANES OF UNRESTRICTED TRAFFIC FROM STA. 645+20.89 TO STA. 648+54.75 ON FINAL SURFACE COURSE AND ALL SAFETY ITEMS IN PLACE.	40	65	\$6,000	\$30,000

DESIGNATED LOCAL DETOUR ROUTE:
IN ADDITION TO THE OFFICIAL, SIGNED DETOUR ROUTE, A LOCAL ROUTE HAS BEEN DETERMINED TO BE THE SECONDARY, UNSIGNED DETOUR ROUTE OR "DESIGNATED LOCAL DETOUR ROUTE." THIS ROUTE IS SHOWN ON SHEET P.05 DURING THE TIME THAT TRAFFIC IS DETOURED, THE CONTRACTOR SHALL MAINTAIN THIS ROUTE IN A CONDITION WHICH IS REASONABLY SMOOTH AND FREE FROM HOLES, RUTS, RIDGES, BUMPS, DUST AND STANDING WATER. ONCE THE DETOUR IS REMOVED AND TRAFFIC RETURNED TO ITS NORMAL PATTERN, THE DESIGNATED LOCAL DETOUR ROUTE SHALL BE RESTORED TO A CONDITION THAT IS EQUIVALENT TO THAT WHICH EXISTED PRIOR TO ITS USE FOR THIS PURPOSE. ALL SUCH WORK SHALL BE PERFORMED WHEN AND AS DETERMINED BY THE ENGINEER.

THE FOLLOWING ESTIMATED QUANTITIES ARE PROVIDED FOR USE AS DETERMINED BY THE ENGINEER TO MAINTAIN AND SUBSEQUENTLY RESTORE THE DESIGNATED LOCAL DETOUR ROUTE:

ITEM 614 - ASPHALT CONCRETE FOR MAINTAINING TRAFFIC	=	10 CU. YD.
ITEM 616 - WATER	=	1 M. GAL.
ITEM 642 - CENTER LINE, TYPE 1	=	0.25 MILE

MAINTAINING TRAFFIC (NOTICE OF CLOSURE SIGN):
NOTICE OF CLOSURE SIGNS, W20-H13, SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK.

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD/RAMP FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.

NOTICE OF CLOSURE SIGN TIME TABLE		
ITEM	DURATION OF CLOSURE	SIGN DISPLAYED TO PUBLIC
ROAD CLOSURES	>= 2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	> 12 HOURS & < 2 WEEKS	7 CALENDAR DAYS PRIOR TO CLOSURE
	<= 12 HOURS	2 BUSINESS DAYS PRIOR TO CLOSURE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MM-DD-YY FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN SHALL DISPLAY THE PHONE NUMBER OF THE DISTRICT 6 PUBLIC INFORMATION CONSTRUCTION LINE, (740)833-8268, WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION.

**SR-37 WILL BE
CLOSED MM/DD/YY
FOR XX DAYS
INFO: (740) 833-8268**

W20-H13-60

PAYMENT SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614 MAINTAINING TRAFFIC.

DETOUR SIGNING:
SIZE AND PLACEMENT OF DETOUR SIGNS (M4-9) SHOULD FOLLOW THE REQUIREMENTS OF THE ODOT SECTION 6F.03, SECTION 2A.11 AND TABLE 6F.01.

DETOUR SIGNING SHALL PROVIDE DRIVERS ADEQUATE TIME TO CLEARLY READ THE SIGNS AND MAKE THE PROPER DECISIONS AT EACH REQUIRED TURNING MOVEMENT. THE DESIGNATED DETOUR ROUTE SHALL BE SIGNED IN ACCORDANCE WITH THE REQUIREMENTS BELOW:

- APPROXIMATELY 1500 FEET PRIOR TO TIP OF THE PAINTED GORE AT AN INTERCHANGE WHEN EXITING A HIGH SPEED (45 MPH OR HIGHER) FACILITY.
- AT OR NEAR THE EXISTING SIGN IN THE GORE OF AN INTERCHANGE RAMP.
- AT OR NEAR THE FIRST EXISTING LANE ASSIGNMENT SIGN ON AN INTERCHANGE EXIT RAMP.
- AT OR NEAR THE EXISTING LANE ASSIGNMENT SIGN OR EXISTING ROUTE MARKER AT THE END OF AN EXIT RAMP.
- APPROXIMATELY 500 FEET PRIOR TO A REQUIRED TURN AT AN INTERSECTION NOT CONTROLLED BY A STOP SIGN (FOR 45 MPH OR HIGHER ONLY).
- AT OR NEAR THE EXISTING LANE ASSIGNMENT SIGN OR EXISTING ROUTE MARKER AT AN INTERSECTION.
- EVERY TWO MILES ALONG A TANGENT SECTION BETWEEN TURNING MOVEMENTS OUTSIDE A CITY.
- EVERY TWO BLOCKS ALONG A TANGENT SECTION BETWEEN TURNING MOVEMENTS WITHIN A CITY.
- AT ANY OTHER INTERSECTION OR DECISION POINT WHERE THE DETOUR ROUTE IS CONTRARY TO THE NORMAL, EXPECTED TURNING MANEUVER OR OTHERWISE UNCLEAR.

DETOUR SIGNS SHALL BE PLACED, WHEN POSSIBLE, NEXT TO BUT NOT BLOCKING EXISTING ROUTE MARKERS OR LANE ASSIGNMENT SIGNS. DETOUR SIGNS SHALL NOT OBSCURE OR BE OBSCURED BY OTHER EXISTING OR TEMPORARY SIGNS.

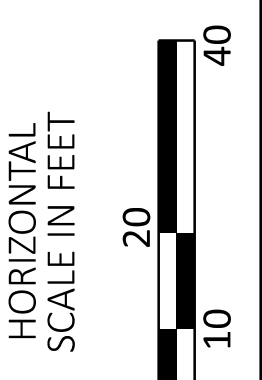
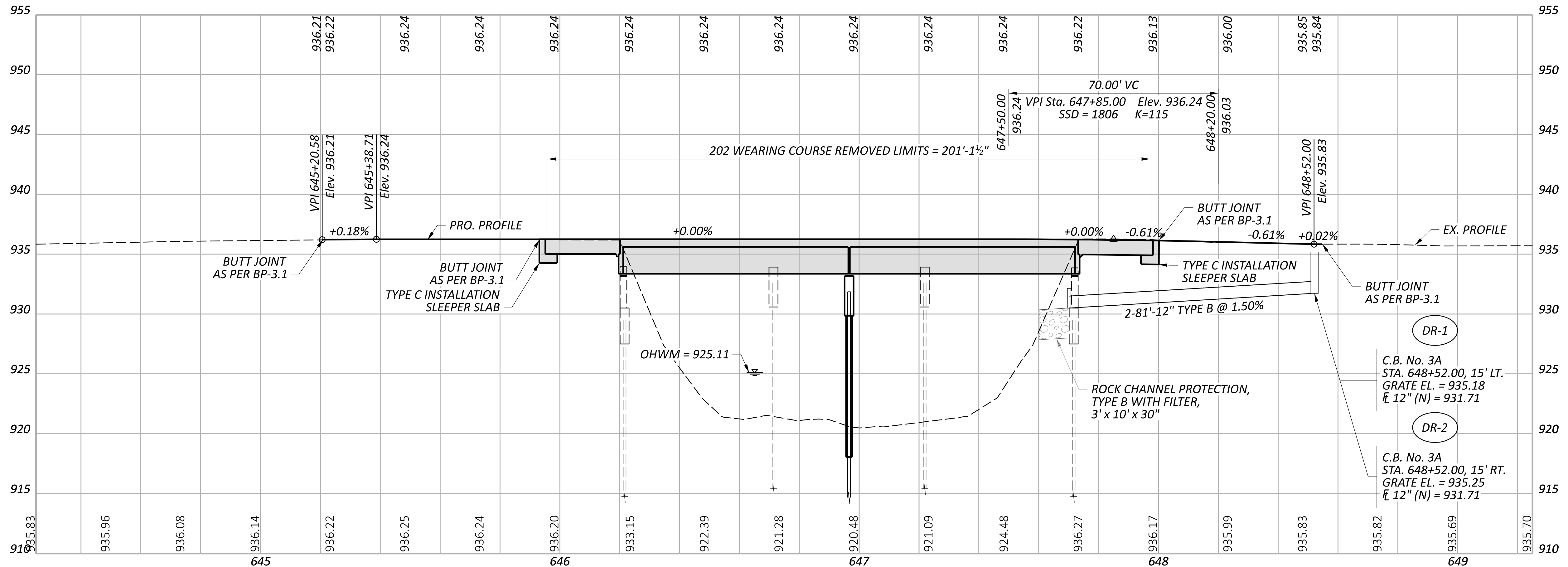
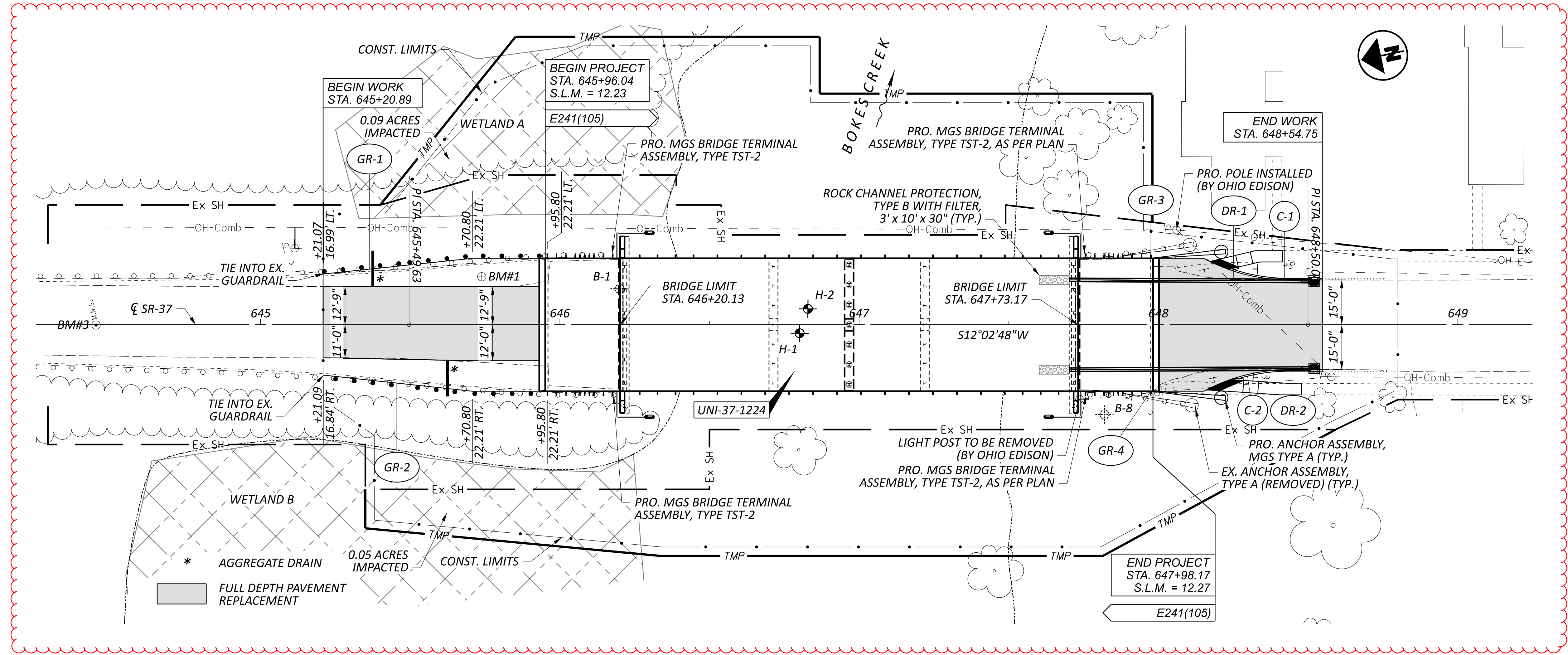
DETOUR SIGNS SHALL BE ERECTED AND/OR UNCOVERED PRIOR TO THE ROAD OR RAMP BEING CLOSED TO TRAFFIC BUT NO EARLIER THAN FOUR HOURS PRIOR TO THE CLOSURE. DETOUR SIGNS SHALL BE COVERED AND/OR REMOVED NO LATER THAN FOUR HOURS FOLLOWING THE ROAD OR RAMP RE-OPENING TO TRAFFIC.

THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN SIGNS, INCLUDING STANDARD 48 X 30 INCH ROAD CLOSED SIGNS, SIGN SUPPORTS, AND TYPE III BARRICADES AND LIGHTS AS DETAILED IN THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES AND IN SCD MT-101.60 AT THE LOCATIONS SHOWN ON SHEET P.05.

PAYMENT FOR ACCEPTED QUANTITIES WILL BE MADE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL BE FOR ALL MATERIALS, LABOR, INCIDENTALS AND EQUIPMENT FOR FURNISHING, PROPER SIGN PLACEMENT AND SIZING, TIMELY ERECTING AND/OR UNCOVERING OF SIGNS, MAINTAINING SIGNS, AND TIMELY COVERING AND/OR REMOVING SIGNS AND SUPPORTS.

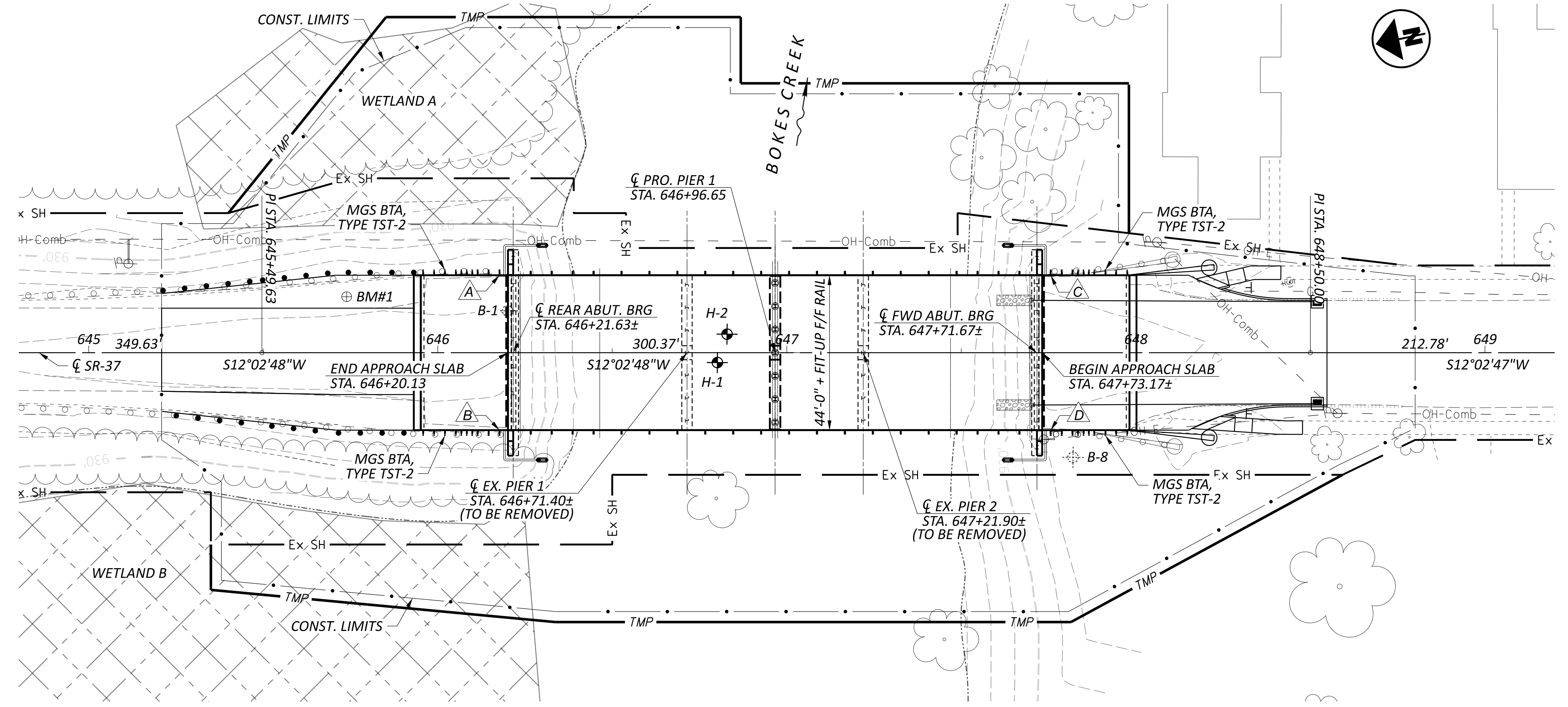
THE FOLLOWING ESTIMATED QUANTITY HAS BEEN PROVIDED AND CARRIED TO THE GENERAL SUMMARY:

ITEM 614 - DETOUR SIGNING = LS

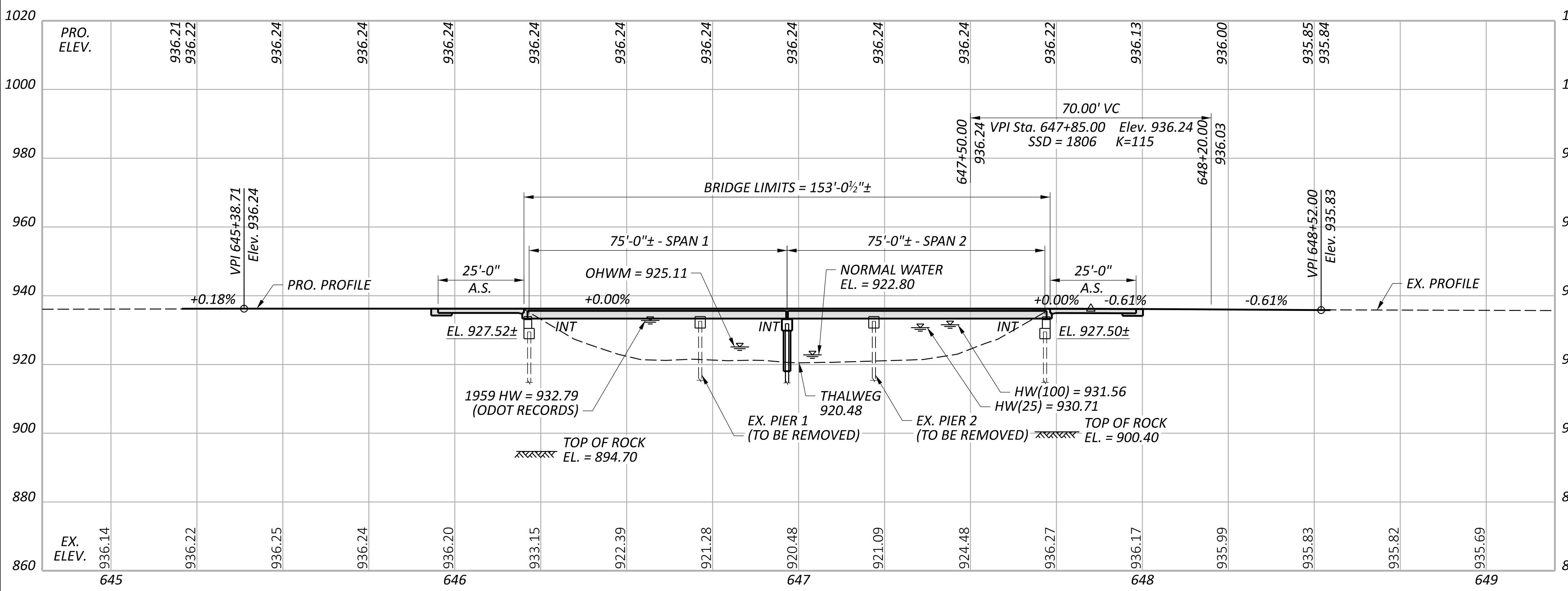


PLAN AND PROFILE
 STA. 644+25.00 TO STA. 649+25.00

DESIGN AGENCY	
DESIGNER	JMB
REVIEWER	JPH 11/01/24
PROJECT ID	122277
SHEET TOTAL	P.09 34



PLAN



PROFILE ALONG CENTERLINE SR-37

BENCHMARK DATA

BM #1 STA.	645+73.91	ELEV.	935.69	OFFSET	16.05'	LEFT
BM #2 STA.	650+96.56	ELEV.	934.79	OFFSET	0.00'	
BM #3 STA.	644+45.03	ELEV.	935.96	OFFSET	0.09'	RIGHT

NOTES

EARTHWORK LIMITS SHOWN ARE APPROXIMATE. ACTUAL SLOPES SHALL CONFORM TO PLAN CROSS SECTIONS.

DESIGN TRAFFIC:

2025 ADT = 650 2025 ADTT = 33
 2045 ADT = 700 2025 ADTT = 35
 DIRECTIONAL DISTRIBUTION = 55%

LEGEND

- ⊕ HISTORIC BORING LOCATION
- ⊙ HAND SOIL COLLECTION LOCATION
- A MGS BTA TYPE TST-2 LAST MOUNTED POST STA. 646+17.69
- B MGS BTA TYPE TST-2 LAST MOUNTED POST STA. 646+17.69
- C MGS BTA TYPE TST-2 LAST MOUNTED POST STA. 647+75.61
- D MGS BTA TYPE TST-2 LAST MOUNTED POST STA. 647+75.61

HYDRAULIC DATA

DRAINAGE AREA = 68.7 SQ. MILES
 Q (25) = 5330 CFS V (25) = 5.08 FT/S
 Q (100) = 7320 CFS V (100) = 6.26 FT/S
 STRUCTURE CLEARS THE 25 YEAR
 DESIGN HW BY 2.29 FEET.

EXISTING STRUCTURE

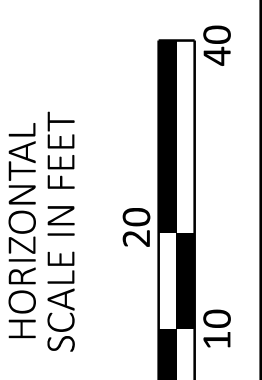
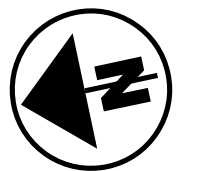
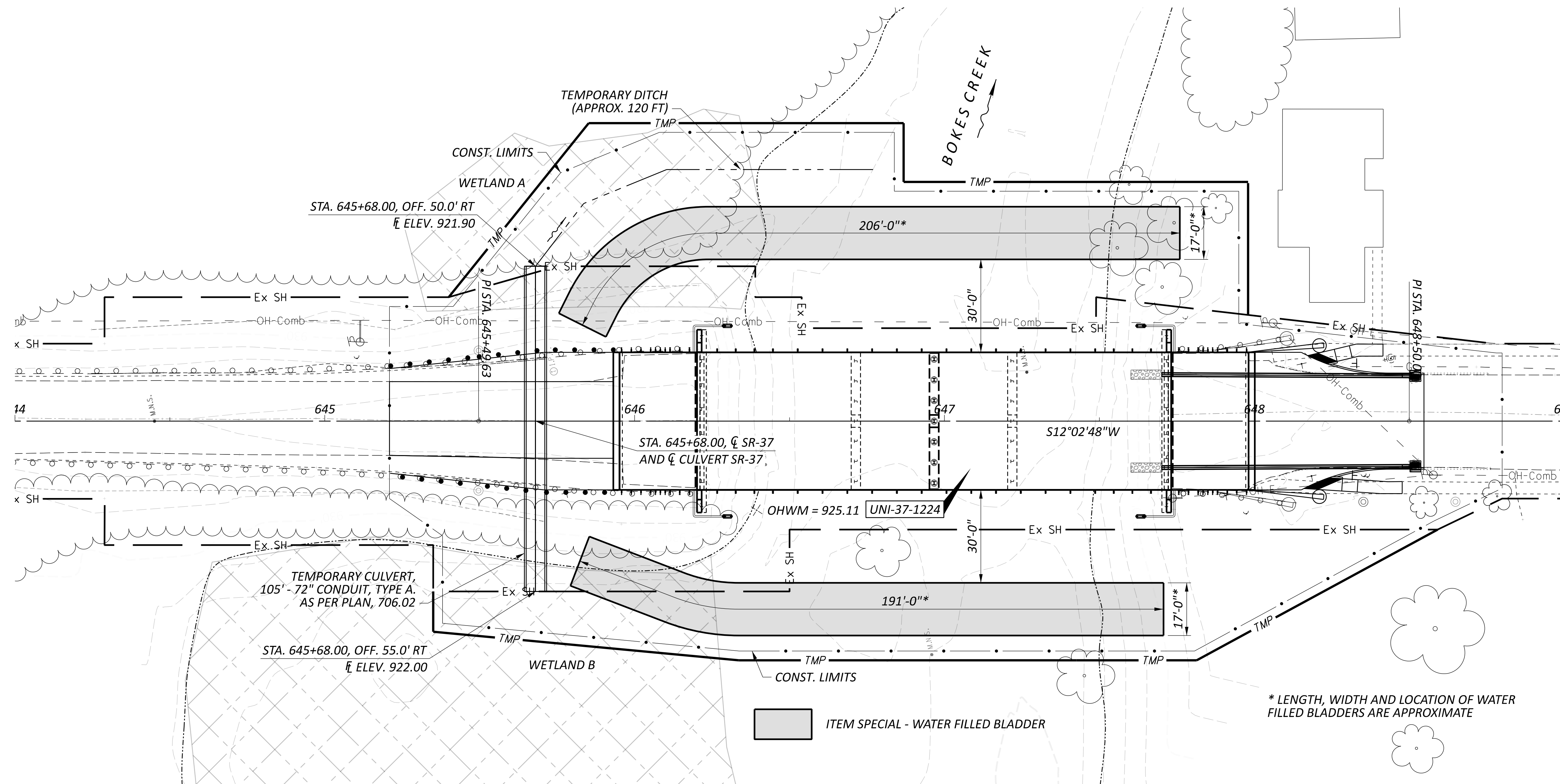
TYPE: PRESTRESSED CONCRETE BOX BEAMS WITH CAPPED PILE SUBSTRUCTURES
 SPANS: 3 @ 49'-0"± (49'-9" - 50'-6" - 49'-9" C/C SUBSTRUCTURES)
 ROADWAY: 44'-0" F/F GUARDRAIL
 LOADING: HS-20-44 & ALTERNATE MILITARY
 SKEW: 0°
 WEARING SURFACE: 2½" ASPHALT CONCRETE
 APPROACH SLABS: AS-1-72 (25' LONG)
 ALIGNMENT: TANGENT
 CROWN: NORMAL
 STRUCTURE FILE NUMBER: 8001936
 DATE BUILT: 1981
 DISPOSITION: REMOVE SUPERSTRUCTURE AND PIERS

PROPOSED STRUCTURE

TYPE: 2 SPAN PRESTRESSED COMPOSITE BOX BEAM (27") ON INTEGRAL ABUTMENTS & CAPPED PILE PIER
 SPANS: 75'-0", 75'-0" C/C BEARING
 ROADWAY: 44'-0" F/F RAIL
 LOADING: HL93 AND 60 PSF FUTURE WEARING SURFACE
 SKEW: 0°
 WEARING SURFACE: 1" MONOLITHIC CONCRETE
 APPROACH SLABS: 25'-0" LONG (AS-1-15, AS-2-15)
 ALIGNMENT: TANGENT
 CROWN: 0.016 FT/FT
 DECK AREA: 6796 SF
 COORDINATES: LATITUDE 40°21'19.78"N
 LONGITUDE 83°15'42.59"W

SITE PLAN
 BRIDGE NO. UNI-37-12.24
 OVER BOKES CREEK

SFN	8001937
DESIGN AGENCY	
DESIGNER	CHECKER
JMB	JMH
REVIEWER	
JPH	11/01/24
PROJECT ID	122277
SUBSET	TOTAL
1	19
SHEET	TOTAL
P.12	34



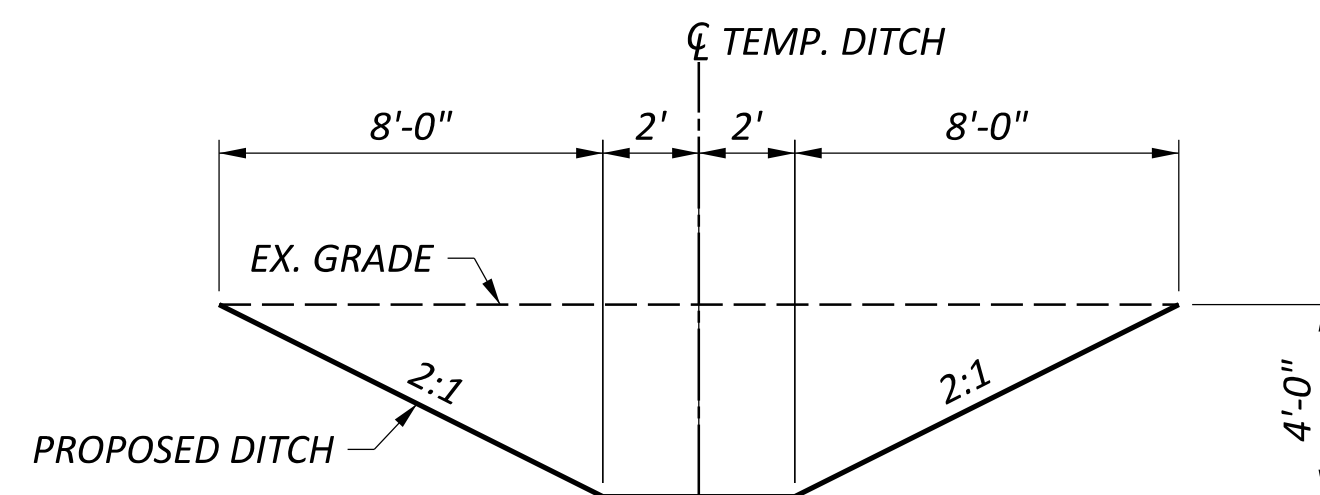
SUPPLEMENTAL DEMOLITION PLAN
BRIDGE NO. UNI-37-12.24
OVER BOKES CREEK

SEQUENCE OF REMOVAL

1. INSTALL TEMPORARY 72" CULVERT AND CUT TEMPORARY DITCH TO ALLOW CREEK TO FLOW AROUND STRUCTURE (UNI-37-1224)
2. INSTALL WATER FILLED BLADDERS TO DIVERT CREEK THROUGH 72" CULVERT.
3. REMOVE STRUCTURE AND SUBSEQUENT DEBRIS FROM THE CREEK BED. ALL DEBRIS (INCLUDING BUT NOT LIMITED TO CONCRETE, ASPHALT AND STEEL) MUST BE CONTAINED WITHIN THE DEWATERED AREA AND REMOVED PRIOR TO REMOVAL OF WATER FILLED BLADDERS.
4. REMOVE WATER FILLED BLADDERS.
5. REMOVE TEMPORARY 72" CULVERT AND RESTORE TEMPORARY DITCH.

TEMPORARY DITCH

TEMPORARY DITCH SHALL BE CUT TO DISCHARGE CULVERT BACK TO CREEK. UPON REMOVAL OF TEMPORARY 72" CULVERT, THE DITCH SHALL BE RETURNED TO ORIGINAL GRADE USING BORROW TO FILL.



TEMPORARY DITCH TYPICAL

* LENGTH, WIDTH AND LOCATION OF WATER FILLED BLADDERS ARE APPROXIMATE

ITEM SPECIAL - WATER FILLED BLADDER

NOTES

SEE SHEET P.04 FOR MORE INFORMATION ABOUT ITEM SPECIAL - WATER FILLED BLADDER AND ITEM 611 - 72" CONDUIT, TYPE A, AS PER PLAN, 706.02.

ESTIMATED QUANTITIES

ITEM	QUANTITY	UNIT	DESCRIPTION
203	213	CY	EXCAVATION
203	213	CY	EMBANKMENT
611	105	FT	72" CONDUIT, TYPE A, AS PER PLAN, 706.02
690	LS	LS	WATER FILLED BLADDER

QUANTITIES CARRIED TO GENERAL SUMMARY

SFN 8001937

DESIGN AGENCY



DESIGNER: JMB CHECKER: JRA

REVIEWER

JRA 02/20/25

PROJECT ID: 122277

SUBSET TOTAL: 1A 19

SHEET TOTAL: P.12A 34

STANDARD DRAWINGS AND SUPPLEMENTAL SPECIFICATIONS
 REFER TO THE FOLLOWING STANDARD BRIDGE DRAWING(S):
 AS-1-15 DATED (REVISED) 1/20/23
 AS-2-15 DATED (REVISED) 7/21/23
 BD-1-11 DATED (REVISED) 7/20/18
 DS-1-92 DATED (REVISED) 7/15/22
 PSBD-2-07 DATED (REVISED) 7/20/18
 TST-2-21 DATED (REVISED) 7/19/24

AND TO THE FOLLOWING SUPPLEMENTAL SPECIFICATION(S):
 NONE

DESIGN SPECIFICATIONS
 THIS STRUCTURE CONFORMS TO THE 9th EDITION OF THE "LRFD BRIDGE DESIGN SPECIFICATIONS" ADOPTED BY THE AMERICAN ASSOCIATION OF STATE HIGHWAY AND TRANSPORTATION OFFICIALS, 2020 AND THE ODOT BRIDGE DESIGN MANUAL, 2020.

OPERATIONAL IMPORTANCE
 A LOAD MODIFIER OF 1.00 HAS BEEN ASSUMED FOR THE DESIGN OF THIS STRUCTURE IN ACCORDANCE WITH THE AASHTO LRFD BRIDGE DESIGN SPECIFICATIONS, ARTICLE 1.3.5 AND THE ODOT BRIDGE DESIGN MANUAL, 2007.

DESIGN LOADING
 DESIGN LOADING INCLUDES:
 VEHICULAR LIVE LOAD: HL-93
 FUTURE WEARING SURFACE (FWS) OF 0.060 KIPS/SQ.FT

DESIGN DATA
 CONCRETE CLASS (QC2):
 COMPRESSIVE STRENGTH 4.5 KSI (SUPERSTRUCTURE, PIER CAP)
 CONCRETE CLASS (QC1):
 COMPRESSIVE STRENGTH 4.0 KSI (SUBSTRUCTURE)

CONCRETE REINFORCEMENT:
 GALVANIZED STEEL REINFORCEMENT, PER C&MS 709.16
 MINIMUM YIELD STRENGTH 60-KSI (DECK, PIER, ABUTMENT)

STEEL H-PILES - ASTM A572 - YIELD STRENGTH 50 KSI

CONCRETE FOR PRESTRESSED BEAMS:
 COMPRESSIVE STRENGTH (FINAL) - 7.0 KSI
 COMPRESSIVE STRENGTH (RELEASE) - 5.0 KSI

PRESTRESSING STRAND:
 AREA = 0.167 SQ.IN.
 ULTIMATE STRENGTH = 270 KSI
 INITIAL STRESS = 202.5 KSI (LOW RELAXATION STRANDS)

MONOLITHIC WEARING SURFACE
 MONOLITHIC WEARING SURFACE IS ASSUMED, FOR DESIGN THE DESIGN FLOOD AND CHECK FLOOD SCOUR ELEVATIONS ARE PROVIDED BELOW:

SCOUR ELEVATIONS
 PURPOSES, TO BE 1 INCH THICK.

	PIER 1
DESIGN FLOOD	918.00
CHECK FLOOD	918.00

DECK POURING LIMITATIONS
 POURING OF APPROACH SLABS CONCURRENTLY WITH THE DECK IS PROHIBITED. THE CONTRACTOR SHALL CONSTRUCT THE DECK AND APPROACH SLABS USING TWO SEPARATE POURS. FOLLOWING THE APPROACH SLAB POURS THE CONTRACTOR SHALL SAW AND SEAL THE BRIDGE JOINT AS PER ITEM 516 - 2" DEEP JOINT SEALER, AS PER PLAN.

ITEM 202, PORTIONS OF STRUCTURE REMOVED, AS PER PLAN (SUBSTRUCTURE)
 THIS WORK CONSISTS OF THE REMOVAL OF THE EXISTING SUBSTRUCTURE (ABUTMENTS) AS INDICATED IN THE PLANS AND GENERAL NOTES AND THAT ARE NOT SEPARATELY LISTED FOR PAYMENT. SUBMIT CONSTRUCTION PLANS ACCORDING TO C&MS 501.05

ALL CONCRETE SHALL BE REMOVE BY MEANS OF APPROVED PNEUMATIC HAMMERS EMPLOYING POINTED AND BLUNT CHISEL TOOLS. THE DEPARTMENT WILL NOT PERMIT HYDRAULIC HOE RAM TYPE HAMMERS. THE WEIGHT OF THE HAMMER SHALL NOT BE MORE THAN 35 POUNDS FOR REMOVAL WITHIN 18 INCHES OF PORTIONS TO BE PRESERVED. OUTSIDE THE 18-IN LIMIT, THE CONTRACTOR MAY USE HAMMERS NOT EXCEEDING 90 POUNDS UPON THE APPROVAL OF THE ENGINEER. DO NOT PLACE PNEUMATIC HAMMERS IN DIRECT CONTACT WITH CONCRETE REINFORCEMENT THAT IS TO BE RETAINED IN THE REBUILT STRUCTURE.

EXISTING STRUCTURE VERIFICATION
 DETAILS AND DIMENSIONS SHOWN ON THESE PLANS PERTAINING TO THE EXISTING STRUCTURE HAVE BEEN OBTAINED FROM PLANS OF THE EXISTING STRUCTURE AND FROM FIELD OBSERVATIONS AND MEASUREMENTS. CONSEQUENTLY, THEY ARE INDICATIVE OF THE EXISTING STRUCTURE AND THE PROPOSED WORK BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO C&MS, SECTIONS 102.05 AND 105.02. BASE CONTRACT BID PRICES UPON A RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PREBID EXAMINATION OF THE EXISTING STRUCTURE. HOWEVER, THE DEPARTMENT WILL PAY FOR ALL PROJECT WORK BASED UPON ACTUAL DETAILS AND DIMENSIONS THAT HAVE BEEN VERIFIED IN THE FIELD.

BRIDGE SEAT REINFORCING, SETTING ANCHORS
 ACCURATELY PLACE CONCRETE REINFORCEMENT IN THE VICINITY OF THE BRIDGE SEAT TO AVOID INTERFERENCE WITH THE DRILLING OF ANCHOR BAR HOLES.

DECK SLAB THICKNESS FOR CONCRETE QUANTITY
 THE ESTIMATED QUANTITY OF DECK CONCRETE IS MEASURED ACCORDING TO C&MS 511. IN ADDITION TO THE DESIGN SLAB THICKNESS, THE QUANTITY INCLUDES A VARIABLE HAUNCH THICKNESS THAT PROVIDES AN ALLOWANCE FOR BEAM CAMBER.

TEMPORARY FILLS
 FOR THIS PROJECT, PERMITS FOR SECTIONS 401 AND 404 OF THE CLEAN WATER ACT, ARE BASED ON THE LIMITS OF TEMPORARY CONSTRUCTION FILL PLACED IN "WATERS OF THE UNITED STATES" AS SHOWN BELOW. IF EITHER OF THE LIMITS PROVIDED ARE EXCEEDED, THEN A 404/401 PERMIT MODIFICATION WILL BE REQUIRED. IF A PERMIT MODIFICATION IS REQUIRED, REFER TO SUPPLEMENTAL SPECIFICATION 832.09 FOR THE APPLICATION REQUIREMENTS.

PLAN AREA OF TEMPORARY FILL MATERIAL = 0.271 ACRES

ITEM 526 - REINFORCED CONCRETE APPROACH SLABS (T = 15") & ITEM 526 - TYPE C INSTALLATION, AS PER PLAN
 IN LIEU OF USING EPOXY COATED REINFORCING AS SPECIFIED IN C&MS 526.02 AND BRIDGE STANDARD DRAWING AS-1-15 AND AS-2-15, CONSTRUCT APPROACH SLABS, SLEEPER SLAB, AND ARMORLESS JOINT HEADER WITH GALVANIZED REINFORCEMENT PER C&MS 709.16.

PILES TO BEDROCK
 DRIVE PILES TO REFUSAL ON BEDROCK. THE DEPARTMENT WILL CONSIDER REFUSAL TO BE OBTAINED WHEN THE PILE PENETRATION IS AN INCH OR LESS AFTER RECEIVING AT LEAST 20 BLOWS FROM THE PILE HAMMER. SELECT THE HAMMER SIZE TO ACHIEVE THE REQUIRED DEPTH TO BEDROCK AND REFUSAL.

THE TOTAL FACTORED LOAD IS 342 KIPS PER PILE FOR PIER PILES (1-7).

PIER PILES:
 HP12X53 PILES 40 FEET LONG, ORDER LENGTH.
 USE STEEL PILE POINTS TO PROTECT THE TIPS OF THE PROPOSED STEEL H-PILES (1-7) AT PRO. PIER 1.

ITEM SPECIAL - PILE ENCASEMENT
 ENCASE ALL STEEL H-PILES FOR THE CAPPED PILE PIERS IN CONCRETE CONFORMING TO C&MS 511 (F'C = 4.0 KSI). PROVIDE A CONCRETE SLUMP BETWEEN 6 TO 8 INCHES WITH THE USE OF A SUPERPLASTICIZER. PLACE THE CONCRETE WITHIN A FORM THAT CONSISTS OF POLYETHYLENE PIPE (707.33), OR PVC PIPE (707.42). THE ENCASEMENT SHALL EXTEND FROM ELEVATION 918.00 UP TO THE CONCRETE PIER CAP. POSITION THE PIPE SO THAT AT LEAST 3 INCHES OF CONCRETE COVER IS PROVIDED AROUND THE EXTERIOR OF THE PILE. ALL REINFORCING STEEL USED IN THE PILE ENCASEMENT SHALL BE GALVANIZED.

THE DEPARTMENT WILL MEASURE PILE ENCASEMENT BY THE NUMBER OF FEET. THE DEPARTMENT WILL DETERMINE THE SUM AS THE LENGTH MEASURED ALONG THE AXIS OF EACH PILE FROM THE BOTTOM OF THE ENCASEMENT TO THE BOTTOM OF THE PIER CAP. THE DEPARTMENT WILL PAY FOR ACCEPTED QUANTITIES AT THE CONTRACT PRICE FOR ITEM - SPECIAL, PILE ENCASEMENT.

STRUCTURE INSTABILITY
 THE PILES OF PIER 2 BUCKLED DURING A PREVIOUS CONSTRUCTION OPERATION AND THE PILES OF PIER 1 ARE SEVERLY CORRODED. THEREFORE, THE CAPACITY OF THESE PILES IS UNKNOWN IN THEIR CURRENT STATE. ALL DEMOLITION PROCEDURES SHALL WORK UNDER THE ASSUMPTION THAT NO ADDITIONAL LOAD SHALL BE PLACED ON THE BRIDGE WITHOUT SHORING BEING PROVIDED PRIOR TO LOADING.



PILE SPLICES
 PILE SPLICES: IN LIEU OF USING THE FULL PENETRATION BUTT WELDS SPECIFIED IN CMS 507.09 TO SPlice STEEL H-PILES, THE CONTRACTOR MAY USE A MANUFACTURED H-PILE SPLICER. FURNISH SPLICERS FROM THE FOLLOWING MANUFACTURER:

ASSOCIATED PILE AND FITTING CORPORATION
 8 WOOD HOLLOW RD. PLAZA 1
 PARSIPPANY, NJ 07054

INSTALL AND WELD THE SPLICER TO THE PILE SECTIONS IN ACCORDANCE WITH THE MANUFACTURER'S WRITTEN ASSEMBLY PROCEDURE SUPPLIED TO THE ENGINEER BEFORE THE WELDING IS PERFORMED.

ABUTMENT CONCRETE
 DO NOT PLACE THE ABUTMENT CONCRETE ABOVE THE BRIDGE SEAT CONSTRUCTION JOINT UNTIL THE PRESTRESSED CONCRETE BOX BEAMS HAVE BEEN ERECTED.

CAMBER
 ESTIMATED CAMBER AT DAY 0 (D0) IS 1.375 INCHES.

ESTIMATED CAMBER AT DAY 30 (D30) IS 2.375 INCHES.

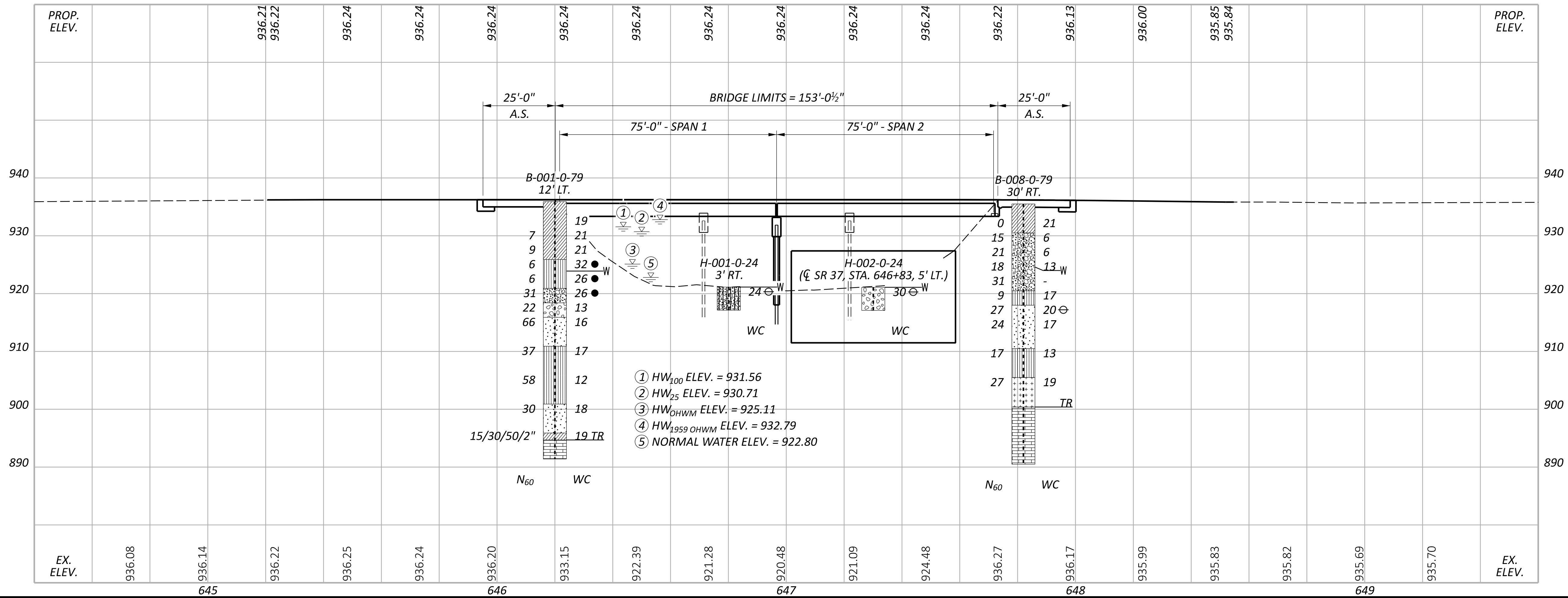
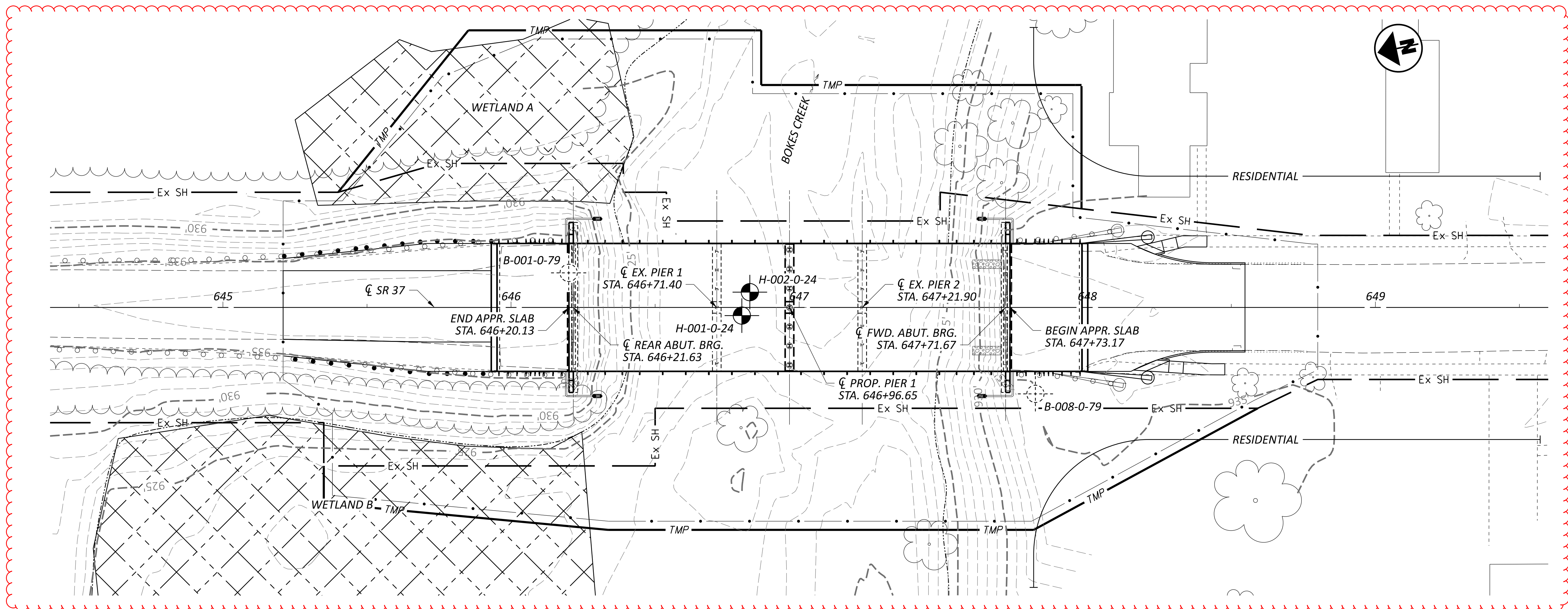
DEFLECTION DUE TO REMAINING DEAD LOAD (E.G. CONCRETE DECK, DIAPHRAGMS, BARRIERS, UTILITIES, ETC.) IS 0.75 INCHES.

THE BEAM SEAT ELEVATIONS ASSUME ESTIMATED CAMBER

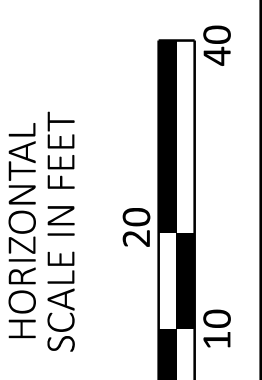
ABBREVIATIONS

E.S.	-	EACH SIDE
PROP.	-	PROPOSED
EXIST.	-	EXISTING
P.C.P.P.	-	PERFORATED CORRUGATED PLASTIC PIPE
N.P.C.P.P.	-	NON-PERFORATED CORRUGATED PLASTIC PIPE
L.T.	-	LEFT
R.T.	-	RIGHT
R.A.	-	REAR ABUTMENT
F.A.	-	FORWARD ABUTMENT
FRWD.	-	FORWARD
ABUT.	-	ABUTMENT
EL.	-	ELEVATION
CL BRG.	-	CENTERLINE OF BEARING
EMBED.	-	EMBEDMENT

SFN	8001937
DESIGN AGENCY	
DESIGNER	CHECKER
JMB	JMH
REVIEWER	
JPH	11/01/24
PROJECT ID	122277
SUBSET	TOTAL
2	19
SHEET	TOTAL
P.13	34



- ① HW₁₀₀ ELEV. = 931.56
- ② HW₂₅ ELEV. = 930.71
- ③ HW_{OHWM} ELEV. = 925.11
- ④ HW_{1959 OHWM} ELEV. = 932.79
- ⑤ NORMAL WATER ELEV. = 922.80



GEOTECHNICAL PROFILE - BRIDGE
 BRIDGE NO. UNI-37-1224 OVER BOKES CREEK

DESIGN AGENCY



DESIGNER

ARR

REVIEWER

SAT 11/01/24

PROJECT ID

122277

SUBSET TOTAL

2 4

SHEET TOTAL

P.32 34