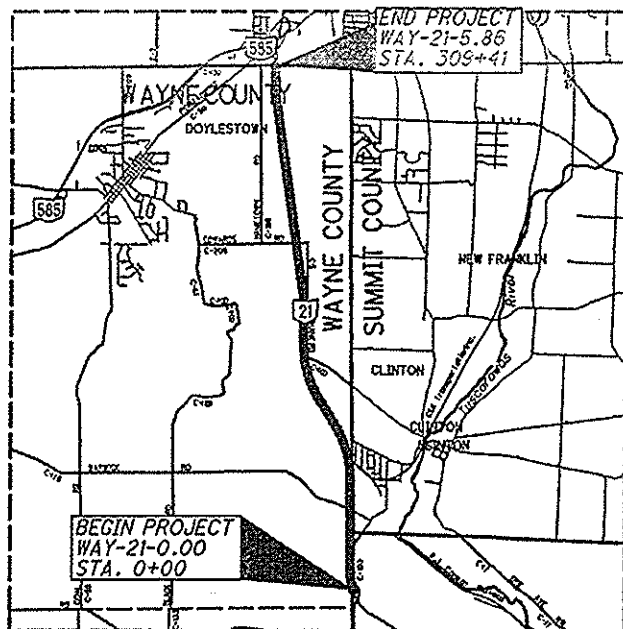


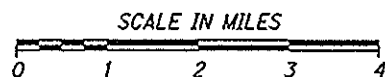
STATE OF OHIO
DEPARTMENT OF TRANSPORTATION

WAY-21-0.00

**CHIPPEWA TOWNSHIP
WAYNE COUNTY**



LATITUDE: N 40°56'47" LONGITUDE: W 81°39'28"



PORTION TO BE IMPROVED	-----
INTERSTATE & DIVIDED HIGHWAY	=====
UNDIVIDED STATE & FEDERAL ROUTES	=====
OTHER ROADS	-----

DESIGN DESIGNATION

SLM	0.00-4.66	4.66-5.86
CURRENT ADT (2016)	13,000	18,000
DESIGN YEAR ADT (2021)	14,000	19,000
DESIGN HOURLY VOLUME (2021)	1,300	1,900
DIRECTIONAL DISTRIBUTION	52%	63%
TRUCKS (24 HOUR B&C)	8%	6%
DESIGN SPEED/LEGAL SPEED	60 MPH	60 MPH

DESIGN FUNCTIONAL CLASSIFICATION:
OTHER PRINCIPAL ARTERIAL

NHS PROJECT - YES

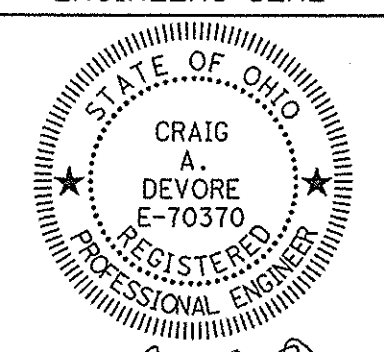
DESIGN EXCEPTIONS

NONE

INDEX OF SHEETS:

TITLE SHEET	1
SCHEMATIC PLAN	2-4
TYPICAL SECTIONS	5
ROCKCUT TYPICAL DRAINAGE SECTION	6
GENERAL NOTES/ MAINTENANCE OF TRAFFIC NOTES	7-9
GENERAL SUMMARY	10
FULL DEPTH PAVEMENT REPAIR WITH AGGREGATE DRAINS	11
PAVEMENT MARKING/RPM SUBSUMMARY	12
CULVERT DETAIL	13

ROADWAY/STRUCTURE
ENGINEERS SEAL:



SIGNED: *Craig A. Devore*
DATE: 7-27-2015

STANDARD CONSTRUCTION DRAWINGS

NO.	DATE	DESCRIPTION	DATE	NO.	DATE	DESCRIPTION	DATE
BP-3.1	7/18/14	MT-95.50	7/19/13				
		MT-95.30	7/18/14	PCB-91	1/18/13		
BP-7.1	7/18/14	MT-95.40	7/18/14				
		MT-97.10	7/18/14				
DM-1.1	1/18/13	MT-97.12	7/18/14				
DM-4.3	7/19/13	MT-99.20	7/19/13	TC-41.20	10/18/13		
DM-4.4	7/20/12	MT-101.70	1/17/14	TC-42.20	10/18/13		
		MT-101.90	7/18/14	TC-52.10	10/18/13		
		MT-103.10	1/16/15	TC-52.20	7/18/14		
RM-4.2	6/4/14	MT-105.10	7/19/13				
				TC-65.10	1/17/14		
CB-3.3	1/18/13			TC-65.11	7/18/14		
				TC-72.20	7/18/14		

SUPPLEMENTAL SPECIFICATIONS

800	7/17/15
821	4/20/12
832	1/17/14

PROJECT DESCRIPTION

THIS PROJECT IS 5.86 MILES IN LENGTH ALONG STATE ROUTE 21. IT WILL INCLUDE PARTIAL AND FULL DEPTH PAVEMENT REPAIRS, RESURFACING REPAIR LOCATIONS, REPLACING PAVEMENT MARKINGS, REPLACING A CATCH BASIN, CORRECTING A DRAINAGE DITCH, AND MAINTENANCE WORK.

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA:	N/A ACRES
(MAINTENANCE PROJECT)	
ESTIMATED CONTRACTOR EARTH DISTURBED AREA:	N/A ACRES
(MAINTENANCE PROJECT)	
NOTICE OF INTENT EARTH DISTURBED AREA:	N/A ACRES
(MAINTENANCE PROJECT)	

2013 SPECIFICATIONS

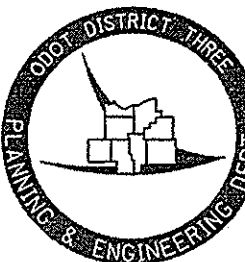
THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

APPROVED: *[Signature]*
DATE 7-27-2015 DISTRICT DEPUTY DIRECTOR

APPROVED: *[Signature]*
DATE 7-30-15 DIRECTOR, DEPARTMENT OF TRANSPORTATION

PLANS PREPARED BY:



UNDERGROUND UTILITIES

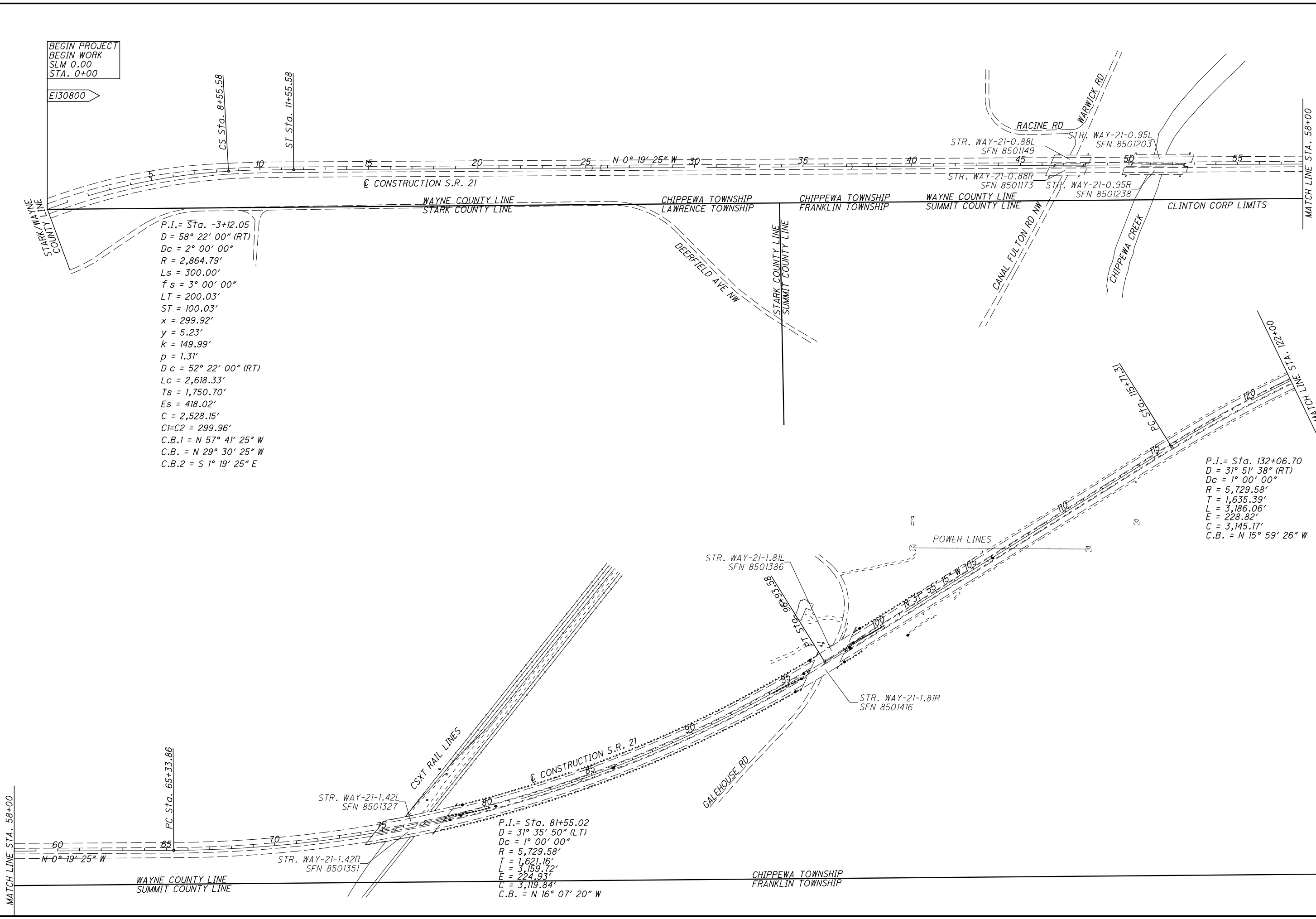
CONTACT BOTH SERVICES
CALL TWO WORKING DAYS
BEFORE YOU DIG
CALL 1-800-362-2764
(TOLL FREE)
OHIO UTILITIES PROTECTION SERVICE
NON-MEMBERS
MUST BE CALLED DIRECTLY
OIL & GAS PRODUCERS PROTECTIVE
SERVICE CALL: 1-800-925-0988

FEDERAL PROJECT NO. E130800
PID NO. 87710
CONSTRUCTION PROJECT NO.
RAILROAD INVOLVEMENT NONE
WAY-21-0.00

WAY - SR 21-0.00
150553 PID - 87710
Dist 3 10/22/2015

Contract Proposal Available @ www.
contracts.dot.state.oh.us/home

DESIGN FILE: projects\87710\WORKSTATION\devore



BEGIN PROJECT
 BEGIN WORK
 SLM 0.00
 STA. 0+00

E130800

P.I. = Sta. -3+12.05
 D = 58° 22' 00" (RT)
 Dc = 2° 00' 00"
 R = 2,864.79'
 Ls = 300.00'
 fs = 3° 00' 00"
 LT = 200.03'
 ST = 100.03'
 x = 299.92'
 y = 5.23'
 k = 149.99'
 p = 1.31'
 Dc = 52° 22' 00" (RT)
 Lc = 2,618.33'
 Ts = 1,750.70'
 Es = 418.02'
 C = 2,528.15'
 C1-C2 = 299.96'
 C.B.1 = N 57° 41' 25" W
 C.B.2 = N 29° 30' 25" W
 C.B.2 = S 1° 19' 25" E

P.I. = Sta. 132+06.70
 D = 31° 51' 38" (RT)
 Dc = 1° 00' 00"
 R = 5,729.58'
 T = 1,635.39'
 L = 3,186.06'
 E = 228.82'
 C = 3,145.17'
 C.B. = N 15° 59' 26" W

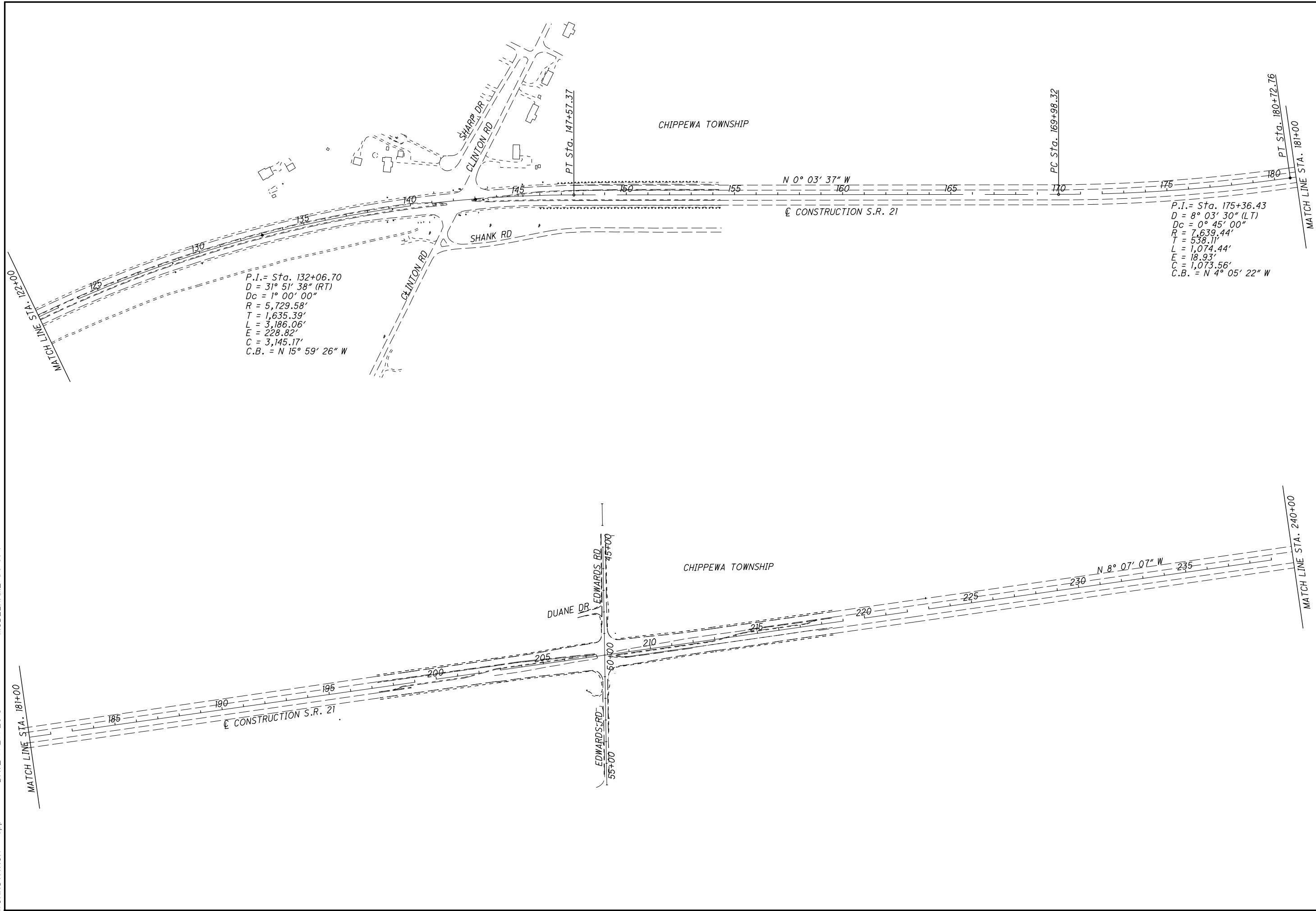
P.I. = Sta. 81+55.02
 D = 31° 35' 50" (LT)
 Dc = 1° 00' 00"
 R = 5,729.58'
 T = 1,621.16'
 L = 3,159.72'
 E = 224.93'
 C = 3,119.84'
 C.B. = N 16° 07' 20" W



CALCULATED
 CAG
 CHECKED
 CAD

SCHEMATIC PLAN
WAY -21-0.00
STA. 0+00 TO STA. 122+00

WAY -21-0.00



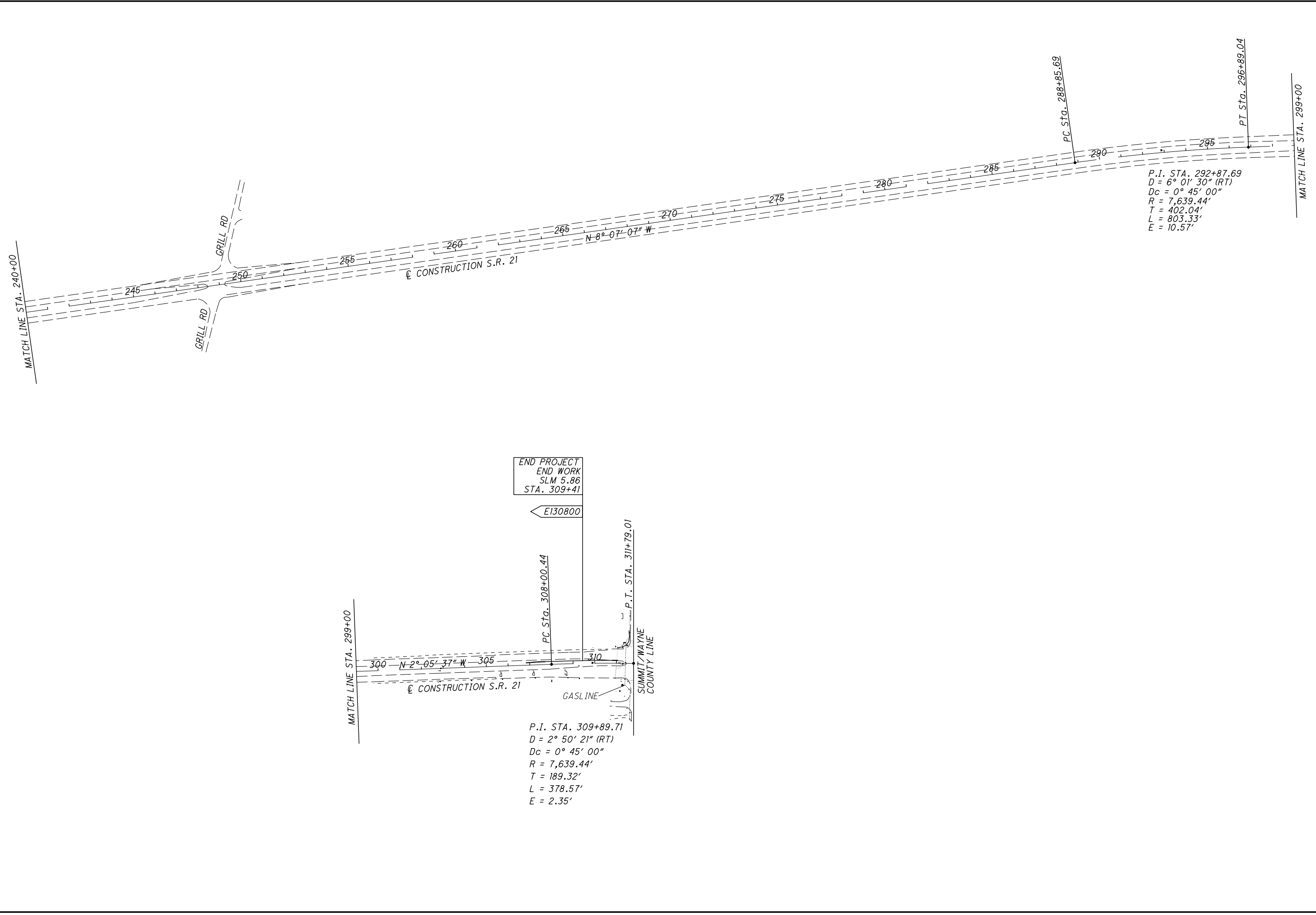
CALCULATED
 CAG
 CHECKED
 CAD

0 200 400
 HORIZONTAL
 SCALE IN FEET

N

SCHEMATIC PLAN
STA. 122+00 TO 240+00

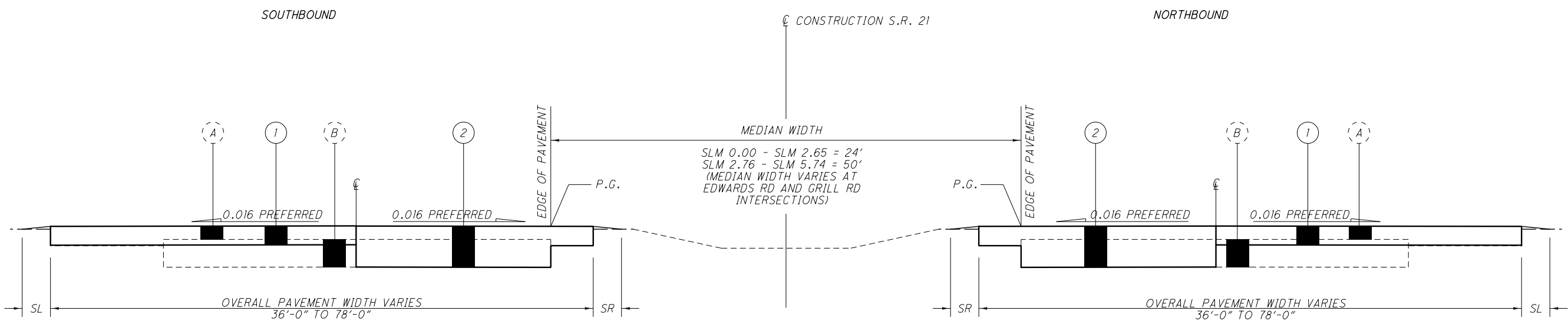
WAY - 21 - 0.00



CALCULATED	
CAG	
CHECKED	
CAD	

SCHEMATIC PLAN
STA. 240+00 TO STA. 311+79

WAY - 21 - 0.00



SOUTHBOUND ONLY
 STA. 0+0.00 TO STA. 139+92.00 = 13,992.0'
 STA. 145+72.80 TO STA. 309+41.00 = 16,368.2'

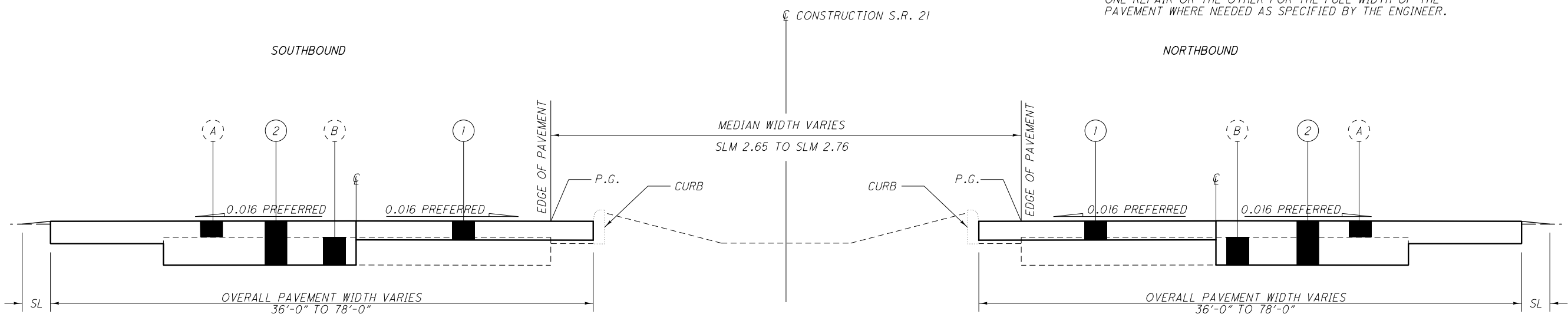
TYPICAL 1

NORTHBOUND ONLY
 * SR 21 - STA. 0+00.00 TO STA. 142+03.20 = 14,203.2'
 * SR 21 - STA. 144+14.40 TO STA. 309+41.00 = 16,526.6'

NOTE:
 1) ++ - SLOPES VARY, SEE EXISTING PLANS.

- | | | | |
|---------------------------------|--|-------------------------------------|--------------------------------|
| <u>EXISTING LEGEND</u> | | <u>EXISTING LEGEND</u> | |
| (A) VAR. DEPTH ASPHALT CONCRETE | (1) ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR | (B) 9" REINFORCED CONCRETE PAVEMENT | (2) ITEM 253 - PAVEMENT REPAIR |

NOTE: ITEM 251 AND ITEM 253 ARE SHOWN ON EACH SECTION FOR INFORMATIONAL PURPOSES ONLY. THE INTENT IS TO PERFORM ONE REPAIR OR THE OTHER FOR THE FULL WIDTH OF THE PAVEMENT WHERE NEEDED AS SPECIFIED BY THE ENGINEER.

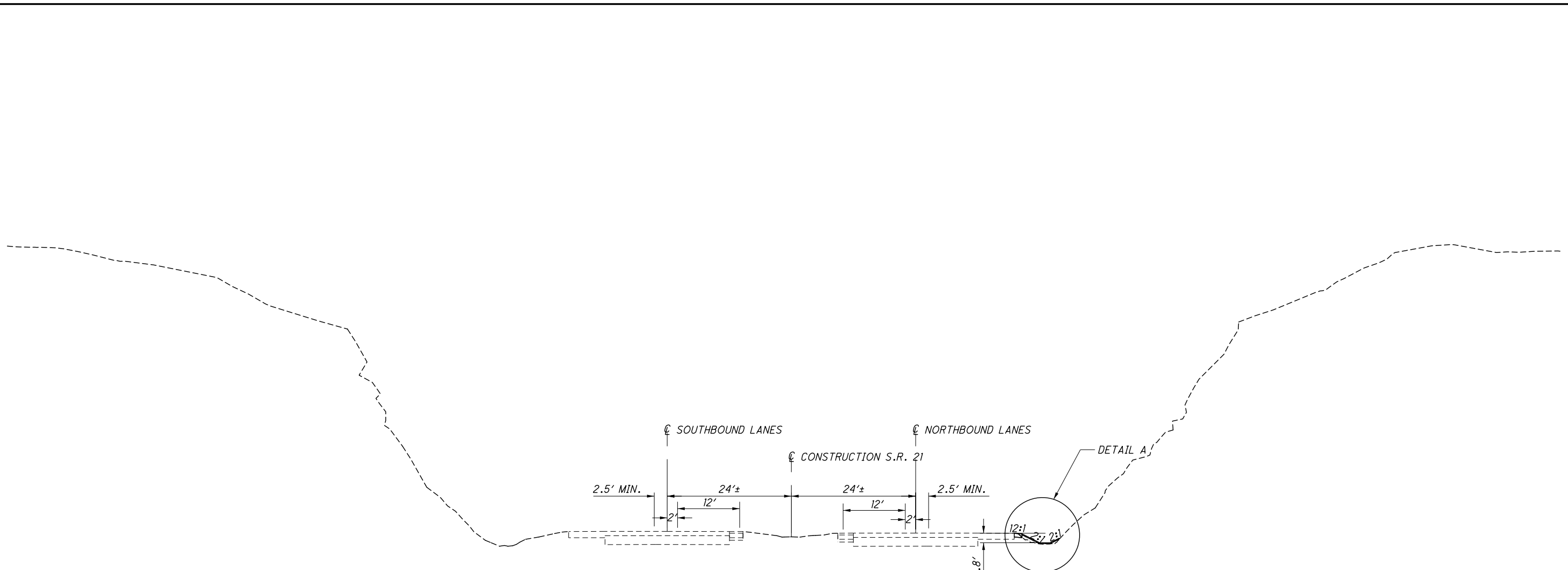


SOUTHBOUND ONLY
 ++ SR 21 - STA. 139+92.00 TO STA. 145+72.80 = 580.8'

TYPICAL 2

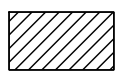
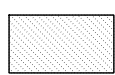

NORTHBOUND ONLY
 * SR 21 - STA. 142+03.20 TO STA. 144+14.40 = 211.2'

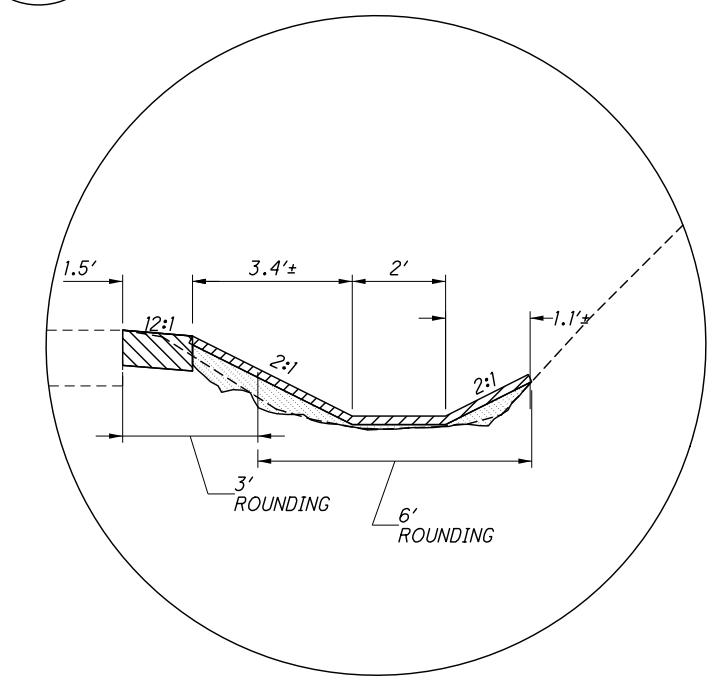
DESIGN FILE: \\projects\87710\roadway\sheets\87710GY002.dgn
 WORKSTATION: knopp DATE: 7/27/2015 MODELNAME: Sheet



EROSION PROTECTION TYPICAL SECTION
 STA. 105+60 TO STA. 120+60 RT.

LEGEND

-  - ITEM 601 - TIED CONCRETE BLOCK MAT, TYPE 1
-  - ITEM 203 - EMBANKMENT
-  - ITEM 617 - COMPACTED AGGREGATE



DETAIL A
 TIED CONCRETE BLOCK MAT, TYPE 1

ITEM	QUANTITY	UNIT	DESCRIPTION
203	125	CY	EMBANKMENT
601	1608	SY	TIED CONCRETE BLOCK MAT, TYPE 1
617	63	CY	COMPACTED AGGREGATE

ALL QUANTITIES CARRIED TO GENERAL SUMMARY

GENERAL

UTILITIES

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS.

THE LOCATION OF THE UNDERGROUND UTILITIES SHOWN ON THE PLANS ARE AS OBTAINED FROM THE OWNERS AS REQUIRED BY SECTION 153.64 O.R.C.

SPELLMAN PIPELINE HOLDINGS, LLC
9081 STATE ROUTE 250
STRASBURG, OHIO
1-800-848-5589

ODOT D3 TRAFFIC
906 CLARK AVENUE
ASHLAND, OHIO 44805
(419) 281-0513

SUNOCO PIPELINE L.P. (INLAND CORP.)
525 FRITZTOWN ROAD
SINKING SPRING, PENNSYLVANIA 19608
610-670-3279

TIME WARNER CABLE
1575 LEXINGTON AVENUE
MANSFIELD, OHIO 44901
419-756-6091 EXT. 419-555-5109

OHIO EDISON COMPANY
2600 SOUTH ERIE STREET
MASSILLON, OHIO 44646
330-830-7083

SPRINT COMMUNICATIONS
11370 ENTERPRISE PARK DR.
SHARONVILLE, OH 45241
513-612-4204

DOYLESTOWN TELEPHONE CO.
28 EAST MARION STREET
DOYLESTOWN, OHIO 44230
330-658-6666

AT&T OF OHIO
50 WEST BOWERY STREET, 4th FLOOR
AKRON, OHIO 44308
330-384-8057

CENTURYLINK
175 ASHLAND ROAD
P.O. BOX 3555
MANSFIELD, OHIO 44907
419-755-7956 OFFICE

MASSILLON CABLE TV
P.O. BOX 1000
MASSILLON, OHIO 44648-1000
330-833-4134 OR 330-833-5449

DOMINION EAST OHIO
320 SPRINGSIDE DRIVE, SUITE 320
AKRON, OH 44333
1-800-362-7557

VERIZON BUSINESS
120 RAVINE STREET
AKRON, OHIO 44303
330-253-8267

THE AFOREMENTIONED UTILITY COMPANIES AND AGENCIES HAVE VARIOUS FACILITIES IN THE AREA THAT WILL REMAIN IN PLACE DURING CONSTRUCTION.

EXTREME CAUTION SHOULD BE EXERCISED IN AREAS WITH UTILITIES. SECTIONS 105.07 AND 107.16 OF THE DEPARTMENT OF TRANSPORTATION CONSTRUCTION AND MATERIALS SPECIFICATIONS REQUIRE, AMONG OTHER THINGS, THAT THE CONTRACTOR COOPERATE WITH ALL UTILITIES LOCATED WITHIN THE LIMITS OF THIS CONSTRUCTION PROJECT AND TAKE RESPONSIBILITY FOR THE PROTECTION OF THE UTILITY PROPERTY AND SERVICES.

CONSTRUCTION NOTIFICATION

THE CONTRACTOR SHALL ADVISE THE PROJECT ENGINEER A MINIMUM OF FOURTEEN (14) DAYS PRIOR TO THE FOLLOWING: THE START OF CONSTRUCTION ACTIVITIES, LANE RESTRICTIONS, LANE CLOSURES, AND OR ROAD CLOSURES. THE PROJECT ENGINEER WILL FORWARD THIS INFORMATION TO THE FOLLOWING:

DISTRICT PUBLIC INFORMATION OFFICER (PIO) BY FAX AT (614) 887-4305 OR EMAIL AT D03.PIO@DOT.STATE.OH.US

DISTRICT PERMIT SECTION BY FAX AT (614) 887-4318 OR EMAIL AT LOUIS.TUMBLIN@DOT.STATE.OH.US

CENTRAL OFFICE SPECIAL HAUL PERMITS SECTION BY FAX AT (614) 728-4099 OR EMAIL AT HAULING.PERMITS@DOT.STATE.OH.US

THE PIO WILL, IN TURN, NOTIFY THE PUBLIC, THE LOCAL EMERGENCY SERVICES, AFFECTED SCHOOLS AND BUSINESSES, AND ANY OTHER IMPACTED LOCAL PUBLIC AGENCY OF ANY OF THE ABOVE MENTIONED ITEMS, VIA MEDIA SOURCES.

EXISTING PLANS

EXISTING PLANS ENTITLED STA-21-17.80; WAY-21-0.00; SUM-21-0.00 MAY BE INSPECTED IN THE ODOT DISTRICT 3 OFFICE IN ASHLAND.

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

ROUTINE MAINTENANCE

BETWEEN THE TIME THAT BIDS ARE TAKEN AND THE START OF CONSTRUCTION, THE MAINTAINING AGENCY MAY ENTER UPON THE PROJECT AND PERFORM ROUTINE MAINTENANCE SUCH AS CRACK SEALING, PATCHING, AND BERM AND SHOULDER REPAIR. THE EFFECTS, IF ANY, OF THE PERFORMANCE OF ROUTINE MAINTENANCE SHALL BE CONSIDERED AS INHERENT IN WORK OF THE CHARACTER PROVIDED FOR IN THE PLAN AND THE RESULTING CONDITIONS SHALL NOT BE CONSIDERED AS DIFFERING MATERIALLY FROM THOSE EXISTING AT THE TIME BIDS WERE TAKEN.

PAVEMENT

**ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR
ITEM 253 - PAVEMENT REPAIR**

THESE ITEMS OF WORK SHALL CONSIST OF THE REMOVAL OF THE EXISTING PAVEMENT OR PAVED BERM WHICH MAY BE ASPHALT, BRICK, CONCRETE, OR A COMBINATION OF EACH, IN AREAS OF EXISTING PAVEMENT FAILURE. CORING HAS BEEN PERFORMED TO HELP DETERMINE THE COMPONENTS THAT MAY BE ENCOUNTERED DURING THIS ITEM OF WORK. THE PAVEMENT CORING INFORMATION IS SHOWN ON THIS SHEET.

PAVEMENT REPAIR SHALL BE PERFORMED AFTER PAVEMENT PLANING AND BEFORE PLACEMENT OF THE INTERMEDIATE AND/OR SURFACE COURSE. THE DEPTH OF REMOVAL SHALL BE SUFFICIENT TO REMOVE ALL DETERIORATED PAVEMENT WITH A MAXIMUM DEPTH OF 16", BASED ON THE PAVEMENT DESIGN AND AN AVERAGE DEPTH OF 14" AND AN AVERAGE WIDTH OF 12 FT FOR ESTIMATING PURPOSES.

THE CONTRACTOR SHALL BE CAPABLE OF PERFORMING PAVEMENT REPAIRS 2 FEET WIDE.

REPLACEMENT MATERIAL SHALL BE ITEM 301 PLACED IN THE FIRST LIFT AND 1.50" OF ITEM 442 ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A (448) IN THE SECOND LIFT. THE SURFACE COURSE SHALL BE PLACED AND COMPACTED TO FINISH FLUSH WITH THE ADJACENT PAVEMENT SURFACE PRIOR TO OPENING THE HIGHWAY TO TRAFFIC. ALL EXISTING PAVEMENT AREAS WHICH WILL BE IN CONTACT WITH THE PAVEMENT REPAIR SHALL BE CLEANED AND COATED PER CMS 401.14 USING AN ASPHALT MATERIAL COMPLYING WITH 407.02. ALL COMPACTION SHALL BE ACHIEVED BY MECHANICAL METHODS TO THE SATISFACTION OF THE ENGINEER. ITEM 301 SHALL USE PG64-22 ASPHALT BINDER AND ITEM 442 12.5MM SHALL USE PG64-28 BINDER.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT REPAIR. FOR PAYMENT PURPOSES ITEM 251 PARTIAL DEPTH PAVEMENT REPAIR IS TO BE A MAXIMUM OF 4" DEEP AND ITEM 253 PAVEMENT REPAIR IS FOR DEPTHS GREATER THAN 4". PAYMENT WILL BE MADE AT THE UNIT BID PRICE PER CUBIC YARD, (BY TICKET WEIGHT CONVERSION), OF ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR OR ITEM 253 - PAVEMENT REPAIR. THE FOLLOWING ESTIMATED QUANTITIES ARE PROVIDED IN THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER:

TOTALS AS FOUND CURRENTLY IN THE FIELD:	
SR 21 ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR	100 CY
SR 21 ITEM 253 - PAVEMENT REPAIR	2868 CY
CONTINGENCY FOR FURTHER DAMAGE PRIOR TO CONSTRUCTION:	
SR 21 ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR	175 CY
SR 21 ITEM 253 - PAVEMENT REPAIR	707 CY
TOTAL CARRIED TO THE GENERAL SUMMARY:	
SR 21 ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR	275 CY
SR 21 ITEM 253 - PAVEMENT REPAIR	3575 CY

SLM	DIRECTION	FULL DEPTH REPAIRS		PARTIAL DEPTH REPAIRS		
		PASSING	DRIVING	QUANTITY	DIMENSIONS	LANE
0.00	1.00	NB	35	35	-	-
1.00	2.00	NB	25	32	1	4' X 4' VAR
2.00	3.00	NB	44	22	1	20' X 20' VAR
					1	6' X 6' VAR
3.00	4.00	NB	42	41	1	8' X 8' VAR
					4	5' X 5' VAR
					2	6' X 5' VAR
4.00	5.00	NB	57	47	1	4' X 4' VAR
					1	10' X 10' VAR
5.00	5.86	NB	41	41	3	4' X 4' VAR
					1	4' X 5' VAR
0.00	1.00	SB	23	20	1	5' X 5' VAR
					6	4' X 4' VAR
1.00	2.00	SB	26	27	1	200' X 5' SHOULDER
					1	800' X 6' SHOULDER
2.00	3.00	SB	35	37	-	-
					-	-
3.00	4.00	SB	32	47	-	-
					-	-
4.00	5.00	SB	71	56	1	5' X 5' VAR
					1	1 LONG. 15' X 5'
5.00	5.86	SB	39	39	3	4' X 4' VAR
					1	700' X 2' X 3" PASSING

NOTES:
1.) ALL FULL DEPTH REPAIRS ARE ASSUMED TO BE 6 FEET LONG, 12 FEET WIDE, AND APPROXIMATELY 14 INCHES DEEP, UNLESS NOTED OTHERWISE ABOVE.
2.) ALL PARTIAL DEPTH REPAIRS ARE TO FOLLOW THE DEPTH REQUIREMENTS SET FORTH IN THE C&MS, ITEM 251, UNLESS NOTED OTHERWISE ABOVE.

PAVEMENT CORING INFORMATION

BELOW IS PAVEMENT CORING INFORMATION TO HELP DETERMINE THE EXISTING PAVEMENT COMPOSITION.

COUNTY	ROUTE	SLM	ASPHALT DEPTH (IN.)	CONCRETE DEPTH (IN.)	WHEEL TRACK / SHOULDER	DIRECTION	YEAR CORED
WAY	21	0.00	9.0	0.0	SHOULDER	NB	2015
WAY	21	0.00	6.5	9.0	RIGHT WHEEL PATH	NB	2015
WAY	21	0.00	6.5	9.0	LEFT WHEEL PATH	NB	2015
WAY	21	0.50	5.5	0.0	SHOULDER	NB	2015
WAY	21	0.50	5.0	5.0	RIGHT WHEEL PATH	NB	2015
WAY	21	0.50	2.0	9.0	LEFT WHEEL PATH	NB	2015
WAY	21	1.00	6.0	0.0	SHOULDER	NB	2015
WAY	21	1.00	5.0	2.0	RIGHT WHEEL PATH	NB	2015
WAY	21	1.00	5.0	9.0	LEFT WHEEL PATH	NB	2015
WAY	21	1.50	4.5	0.0	SHOULDER	NB	2015
WAY	21	1.50	2.5	9.0	RIGHT WHEEL PATH	NB	2015
WAY	21	1.50	6.5	9.0	LEFT WHEEL PATH	NB	2015
WAY	21	2.00	4.5	0.0	SHOULDER	NB	2015
WAY	21	2.00	2.5	9.0	RIGHT WHEEL PATH	NB	2015
WAY	21	2.00	3.0	9.0	LEFT WHEEL PATH	NB	2015
WAY	21	2.50	6.0	0.0	SHOULDER	NB	2015
WAY	21	2.50	3.0	9.0	RIGHT WHEEL PATH	NB	2015
WAY	21	2.50	5.0	9.0	LEFT WHEEL PATH	NB	2015
WAY	21	3.00	4.5	0.0	SHOULDER	NB	2015
WAY	21	3.00	6.5	0.0	RIGHT WHEEL PATH	NB	2015
WAY	21	3.00	5.0	0.0	LEFT WHEEL PATH	NB	2015
WAY	21	3.50	4.5	0.0	SHOULDER	NB	2015
WAY	21	3.50	4.0	2.0	RIGHT WHEEL PATH	NB	2015
WAY	21	3.50	3.0	10.0	LEFT WHEEL PATH	NB	2015
WAY	21	4.00	4.0	0.0	SHOULDER	NB	2015
WAY	21	4.00	4.0	0.0	RIGHT WHEEL PATH	NB	2015
WAY	21	4.00	3.0	9.5	LEFT WHEEL PATH	NB	2015
WAY	21	4.50	3.0	0.0	SHOULDER	NB	2015
WAY	21	4.50	3.5	0.0	RIGHT WHEEL PATH	NB	2015
WAY	21	5.00	4.0	0.0	SHOULDER	NB	2015

PART-WIDTH CONSTRUCTION

BECAUSE OF THE NECESSITY TO BUILD THIS PROJECT UNDER TRAFFIC AND TO CONSTRUCT THE FULL PAVEMENT WIDTH IN STAGES, EXTREME CARE SHALL BE TAKEN TO PREVENT THE CONSTRUCTION OF A BUTT JOINT IN THE BASE COURSES. LONGITUDINAL JOINTS SHALL BE LAPPED AS SHOWN ON STANDARD CONSTRUCTION DRAWING BP-3.1.

ITEM 301 - ASPHALT CONCRETE BASE, PG64-22, AS PER PLAN

ITEM 301 ASPHALT CONCRETE BASE, PG64-22, AS PER PLAN SHALL BE USED FOR ITEM 253 AND ITEM 251 REPAIRS AT DEPTHS AS DETAILED IN THE PLANS.

ON THIS PROJECT ITEM 301 COARSE AGGREGATE SHALL HAVE A TWO FACE CRUSH COUNT OF 75% PER ASTM D 5821. MAXIMUM RECLAIMED ASPHALT CONCRETE PAVEMENT SHALL BE 30%. ENSURE THAT A MINIMUM OF 50% OF THE VIRGIN FINE AGGREGATE USED IN THE ITEM 301 IS SAND MANUFACTURED FROM STONE OR AIR COOLED SLAG. THE IN-PLACE BINDER SHALL BE PG64-22.

ALL EXISTING PAVEMENT AREAS WHICH WILL BE IN CONTACT WITH ITEM 301 SHALL BE CLEANED AND COATED PER CMS 401.14, USING AN ASPHALT MATERIAL COMPLYING WITH 407.02. ALL COMPACTION SHALL BE ACHIEVED BY MECHANICAL METHODS TO THE SATISFACTION OF THE ENGINEER.

ALL OF THE ABOVE COSTS SHALL BE INCLUDED IN ITEM 301 ASPHALT CONCRETE BASE, PG64-22, AS PER PLAN.

ITEM 605 - AGGREGATE DRAINS, AS PER PLAN

AGGREGATE DRAINS SHALL BE PLACED AT 50 FOOT INTERVALS ON EACH SIDE OF NORMAL CROWNED SECTIONS, STAGGERED SO THAT EACH DRAIN IS 25 FEET FROM THE ADJACENT DRAIN ON THE OPPOSITE SIDE, AND AT 25 FOOT INTERVALS ON THE LOW SIDE ONLY OF SUPERELEVATED SECTIONS. AN AGGREGATE DRAIN SHALL BE PLACED AT THE LOW POINT OF EACH SLAG VERTICAL CURVE. ALL QUANTITIES NEEDED TO EXCAVATE AND REPLACE ANY PAVEMENT OR SHOULDER IMPACTED BY THE INSTALLATION OF THE AGGREGATE DRAINS SHALL BE CONSIDERED INCIDENTAL TO AND PAID FOR UNDER THIS ITEM.

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GENERAL NOTES

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BUTT JOINTS

BUTT JOINTS SHALL NOT BE CUT AND LEFT OPEN TO TRAFFIC. THEY SHALL BE FILLED IN WITH A TEMPORARY ASPHALT CONCRETE WEDGE USING ITEM 614 ASPHALT CONCRETE FOR MAINTAINING TRAFFIC.

CONSTRUCTION "BUMP" (W8-1-36) AND "ADVISORY SPEED" (W13-1-24) SIGNS SHALL BE ERECTED AND MAINTAINED DURING THE PERIOD THE BUTT JOINT IS LEFT OPEN. THESE SIGNS SHALL BE PAID FOR UNDER THE LUMP SUM ITEM FOR ITEM 614 MAINTAINING TRAFFIC.

PROFILE AND ALIGNMENT

PLACE THE PROPOSED PAVEMENT TO FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT. SEE EXISTING PLANS NOTE. PLACE THE PROPOSED ASPHALT CONCRETE OVERLAY AS SHOWN WITHIN THESE PLANS.

CROSSINGS AND CONNECTIONS TO EXISTING PIPES AND UTILITIES

WHERE PLANS PROVIDE FOR A PROPOSED CONDUIT TO BE CONNECTED TO, OR CROSS OVER OR UNDER AN EXISTING SEWER OR UNDERGROUND UTILITY, THE CONTRACTOR SHALL LOCATED THE EXISTING PIPES OR UTILITIES BOTH AS TO LINE AND GRADE BEFORE STARTING TO LAY THE PROPOSED CONDUIT.

IF IT IS DETERMINED THAT THE ELEVATION OF THE EXISTING CONDUIT, OR EXISTING APPURTENANCE TO BE CONNECTED, DIFFERS FROM THE PLAN ELEVATION OR RESULTS IN A CHANGE IN THE PLAN CONDUIT SLOPE, THE ENGINEER SHALL BE NOTIFIED BEFORE STARTING CONSTRUCTION OF ANY PORTION OF THE PROPOSED CONDUIT WHICH WILL BE AFFECTED BY THE VARIANCE IN THE EXISTING ELEVATIONS.

IF IT IS DETERMINED THAT THE PROPOSED CONDUIT WILL INTERSECT AN EXISTING SEWER OR UNDERGROUND UTILITY IF CONSTRUCTED AS SHOWN ON THE PLAN, THE ENGINEER SHALL BE NOTIFIED BEFORE STARTING CONSTRUCTION OF ANY PORTION OF THE PROPOSED CONDUIT WHICH WOULD BE AFFECTED BY THE INTERFERENCE WITH AN EXISTING FACILITY.

PAYMENT FOR ALL THE OPERATIONS DESCRIBE ABOVE SHALL BE INCLUDED IN THE CONTRACT PROCE FOR THE PERTINENT CONDUIT ITEM.

REVIEW OF DRAINAGE FACILITIES

BEFORE ANY WORK IS STARTED ON THE PROJECT AND AGAIN BEFORE FINAL ACCEPTANCE BY THE STATE, REPRESENTATIVES OF THE STATE AND THE CONTRACTOR, ALONG WITH LOCAL REPRESENTATIVES, SHALL MAKE AN INSPECTION OF ALL EXISTING SEWERS WHICH ARE TO REMAIN IN SERVICE AND WHICH MAY BE AFFECTED BY THE WORK. THE CONDITION OF THE EXISTING CONDUITS AND THEIR APPURTENANCE SHALL BE DETERMINED FROM FIELD OBSERVATIONS. RECORDS OF THE INSPECTION SHALL BE KEPT IN WRITING BY THE STATE.

ALL NEW CONDUITS, INLETS, CATCH BASINS, AND MANHOLES CONSTRUCTED AS A PART OF THE PROJECT SHALL BE FREE OF ALL FOREIGN MATTER AND IN A CLEAN CONDITION BEFORE THE PROJECT WILL BE ACCEPTED BY THE STATE.

ALL EXISTING SEWERS INSPECTED INITIALLY BY THE ABOVE MENTIONED PARTIES SHALL BE MAINTAINED AND LEFT IN A CONDITION REASONALBLY COMPARABLE TO THAT DETERMINED BY THE ORIGINAL INSPECTION. ANY CHANGE IN THE CONDITION RESULTING FROM THE CONTRACTOR'S OPERATIONS SHALL BE CORRECTED BY THE CONTRACTOR TO THE SATISFACTION OF THE ENGINEER.

PAYMENT FOR ALL OPERATIONS DESCRIBED ABOVE SHALL BE INCLUDED IN THE CONTRACT PRICE FOR THE PERTINENT 611 CONDUIT ITEMS.

ITEM 614 - MAINTAINING TRAFFIC: GENERAL

ONE 11' LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES. ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH ITEM 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, PLAN DETAILS, STANDARD DRAWINGS, AND AS OUTLINED IN THE CONSTRUCTION AND MAINTENANCE SECTION OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES CURRENT EDITION WITH THE LATEST REVISIONS. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614 - MAINTAINING TRAFFIC UNLESS SEPARATELY ITEMIZED ON THIS PLAN.

THE FOLLOWING REQUIREMENTS SHALL ALSO APPLY: THE CONTRACTOR SHALL SUBMIT, IN WRITING, A SCHEDULE OF OPERATIONS TO THE ENGINEER AND RECEIVE APPROVAL BEFORE WORK IS STARTED ON THE PROJECT. PRIOR TO BEGINNING WORK, THE CONTRACTOR SHALL COORDINATE THE MAINTENANCE OF TRAFFIC OPERATIONS WITH THE LOCAL STATE HIGHWAY PATROL.

NIGHT WORK IS PERMITTED.

THE CONTRACTOR IS REQUIRED TO MAINTAIN ALL PAVEMENT THROUGHOUT THE PROJECT UNDER ITEM 614 ASPHALT CONCRETE FOR MAINTAINING TRAFFIC DURING THE PERIOD FROM THE START OF WORK TO THE COMPLETION OF ALL WORK.

ITEM 614 - ASPHALT CONCRETE FOR MAINTAINING TRAFFIC

TEMPORARY WEDGES AT END OF RAMPS, PAVEMENT LAYER ENDS, APPROACH SLABS OR BRIDGE DECKS ARE TO BE CONSTRUCTED AS PER STANDARD DRAWING BP-3.1.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER FOR MAINTENANCE OF TRAFFIC.

ITEM 614 - ASPHALT CONCRETE FOR MAINTAINING TRAFFIC 250 CY.

ITEM 614 - MAINTAINING TRAFFIC

ALL ADVANCE WARNING SIGNS FOR ANY CONDITION WHICH RESTRICTS TRAFFIC SHALL BE ERECTED BEFORE ANY SUCH RESTRICTION IS PUT INTO EFFECT. ALL SUCH SIGNS SHALL BE COVERED OR REMOVED FROM THE VIEW OF TRAFFIC WHEN THEY ARE NOT APPLICABLE, WITH THE APPROVAL OF THE ENGINEER.

IF THE CONTRACTOR FAILS TO COMPLY WITH THE PROVISIONS FOR TRAFFIC CONTROL AS SET FORTH IN THESE PLANS OR WITH PROVISIONS OF THE OMUTCD, AND SUCH FAILURE RESULTS IN A CONDITION AT THE WORK SITE WHICH IS UNSAFE FOR TRAFFIC, THE ENGINEER SHALL SUSPEND WORK UNTIL THE CONTRACTOR COMPLIES WITH THE NECESSARY REQUIREMENTS.

ALL MAINTENANCE OF TRAFFIC SIGNS ARE PAID UNDER ITEM 614 - MAINTAINING TRAFFIC.

ITEM 614 - WORKSITE TRAFFIC SUPERVISOR

SUBJECT TO APPROVAL OF THE ENGINEER, THE CONTRACTOR SHALL EMPLOY AND IDENTIFY (SOMEONE OTHER THAN THE SUPERINTENDENT) A CERTIFIED WORKSITE TRAFFIC SUPERVISOR (WTS) BEFORE STARTING WORK IN THE FIELD. THE WTS MAY BE CERTIFIED FROM ONE OF THE FOLLOWING ORGANIZATIONS:

1. AMERICAN TRAFFIC SAFETY SERVICE ASSOCIATION (ATSSA), PHONE NUMBER 1-800-272-8772, CERTIFIED TRAFFIC CONTROL SUPERVISOR (TCS).
2. NATIONAL HIGHWAY INSTITUTE, DESIGN AND OPERATION OF WORK ZONE TRAFFIC CONTROL, PHONE NUMBER 1-703- 235-0528.
3. THE OHIO CONTRACTORS ASSOCIATION, TRAFFIC CONTROL SUPERVISOR (OCA/TCS) WORK ZONE CLASS, ONLY IF TAKEN AFTER MAY 5, 2004, PHONE NUMBER 1-614-599-7915.
4. OHIO LABORERS TRAINING, TRAFFIC CONTROL SUPERVISORS CLASS, PHONE NUMBER 1-740-599-7915.

A COPY OF EACH WTS'S CERTIFICATION AND 24-HOUR CONTACT INFORMATION SHALL BE PROVIDED TO THE ENGINEER AT THE PRECONSTRUCTION CONFERENCE. IF THE DESIGNATED WTS WILL NOT BE AVAILABLE FULL TIME (24/7) THE CONTRACTOR MAY DESIGNATE AN ALTERNATE WTS TO BE AVAILABLE WHEN THE PRIMARY IS OFF DUTY. EACH WTS SHALL HAVE A CURRENT WTS CERTIFICATION (WITH AN EXPIRATION DATE NO MORE THAN 5 YEARS FROM THE DATE OF ISSUE) FROM ANY OF THE APPROVED ORGANIZATIONS.

THE WTS POSITION HAS THE RESPONSIBILITY OF MONITORING TRAFFIC CONTROL DEFICIENCIES FOR THE ENTIRE WORK ZONE. THE DUTIES OF THE WTS ARE AS FOLLOWS:

1. BE AVAILABLE ON A 24-HOUR PER DAY BASIS, AND BE ABLE TO BE ON SITE FOR ALL EMERGENCY TRAFFIC CONTROL NEEDS WITHIN ONE HOUR OF NOTIFICATION BY POLICE OR PROJECT STAFF AND BE PREPARED TO EFFECT CORRECTIVE MEASURES IMMEDIATELY ON EXISTING WORK ZONE TRAFFIC CONTROL DEVICES.
2. ATTEND PRECONSTRUCTION MEETING AND ALL PROJECT MEETINGS WHERE TRAFFIC CONTROL MANAGEMENT IS DISCUSSED.
3. BE AVAILABLE FOR MEETINGS OR DISCUSSIONS WITH THE ENGINEER UPON REQUEST OR WITHIN 36 HOURS.
4. BE AWARE OF, AND COORDINATE IF NECESSARY, ALL TRAFFIC CONTROL OPERATIONS, INCLUDING THOSE OF SUBCONTRACTORS AND SUPPLIERS.
5. COORDINATE PROJECT ACTIVITIES WITH ALL LAW ENFORCEMENT OFFICERS (LEOS). A WTS SHALL ALSO BE THE MAIN CONTACT PERSON WITH THE LEO'S WHILE THEY ARE ON THE PROJECT.
6. COORDINATE MEETINGS WITH ODOT PERSONNEL, LEO'S AND OTHER APPLICABLE ENTITIES BEFORE EACH PLAN PHASE SWITCH TO DISCUSS WORK ZONE TRAFFIC CONTROL.
7. ENSURE COMPLIANCE WITH THE CONTRACT DOCUMENTS FOR SIGNS, BARRICADES, TEMPORARY CONCRETE BARRIER, PAVEMENT MARKINGS, PORTABLE MESSAGE SIGNS, AND OTHER TRAFFIC CONTROL DEVICES ON A DAILY BASIS; AND FACILITATE ANY CORRECTIVE ACTION NECESSARY.
8. NOTIFY THE CONTRACTOR OF THE NEED FOR CLEANING AND MAINTENANCE OF ALL TRAFFIC CONTROL DEVICES, INCLUDING THE COVERING AND REMOVAL OF INAPPLICABLE SIGNS.
9. INSPECT, EVALUATE, PROPOSE NECESSARY MODIFICATIONS TO, AND DOCUMENT THE EFFECTIVENESS OF, THE TRAFFIC CONTROL DEVICES AND/OR TRAFFIC OPERATIONS ON A DAILY BASIS (7 DAYS A WEEK). IN ADDITION, A WEEKLY NIGHT INSPECTION OF THE WORK ZONE SETUP FOR DAYTIME WORK OPERATIONS; AND ONE DAYTIME INSPECTION PER WEEK FOR NIGHTTIME PROJECTS. THIS SHALL INCLUDE (BUT NOT BE LIMITED TO) DOCUMENTATION ON THE FOLLOWING PROJECT EVENTS:
 - A. INITIAL TRAFFIC CONTROL SETUP (DAY AND NIGHT REVIEW).
 - B. DAILY TRAFFIC CONTROL SETUP AND REMOVAL.
 - C. WHEN CONSTRUCTION STAGING CAUSES A CHANGE IN THE TRAFFIC CONTROL SETUP.
 - D. CRASH OCCURRENCES WITHIN THE CONSTRUCTION AREA.
 - E. REMOVAL OF TRAFFIC CONTROL DEVICES AT THE END OF A PHASE OR PROJECT.
 - F. ALL OTHER EMERGENCY TRAFFIC CONTROL NEEDS.
10. COMPLETE THE DEPARTMENT APPROVED LONG TERM INSPECTION FORM (CA-D-8) AFTER EACH INSPECTION AS REQUIRED IN # 9 AND SUBMIT IT TO THE ENGINEER THE FOLLOWING WORK DAY. THESE REPORTS SHALL INCLUDE A CHECKLIST OF ALL TRAFFIC CONTROL MAINTENANCE ITEMS TO BE REVIEWED. A COPY OF THE FORM WILL BE PROVIDED AT THE PRE-CONSTRUCTION MEETING. ANY DEFICIENCIES OBSERVED SHALL BE NOTED, ALONG WITH RECOMMENDED CORRECTIVE ACTIONS AND THE DATES BY WHICH SUCH CORRECTIONS WERE, OR WILL BE, COMPLETED. A COPY OF THIS DOCUMENT CAN BE FOUND IN THE DEPARTMENT OF TRANSPORTATION CONSTRUCTION INSPECTION FORMS MANUAL DATED 10/15/06 OR CURRENT REVISION.
11. VERIFY THAT ALL FLAGGING OPERATIONS ARE BEING CONDUCTED PER THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
12. HAVE COPIES OF THE ODOT TEMPORARY TRAFFIC CONTROL MANUAL AND APPLICABLE STANDARDS AND SPECIFICATIONS INCLUDED IN THE CONTRACT DOCUMENTS AVAILABLE AT ALL TIMES ON THE PROJECT.

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ITEM 614 - WORKSITE TRAFFIC SUPERVISOR (CONTINUED)

THE DEPARTMENT WILL NOT PAY THE UNIT PRICE BID FOR THE WTS FOR ANY DAY ON WHICH THE CONTRACTOR FAILS TO PERFORM THE DUTIES SET FORTH ABOVE. SHOULD THE CONTRACTOR'S FAILURE TO PERFORM ANY OF THE DUTIES DESCRIBED ABOVE RESULT IN A MAINTENANCE OF TRAFFIC SAFETY ISSUE, THE DEPARTMENT WILL DEDUCT THE PRORATED DAILY AMOUNT FOR ITEM 614 MAINTENANCE OF TRAFFIC FROM THE CONTRACTOR'S NEXT SCHEDULED ESTIMATE.

IF THREE OR MORE FAILURES TO PERFORM THE DUTIES SET FORTH ABOVE OCCUR, THE WTS SHALL BE IMMEDIATELY REMOVED FROM THE WORK IN ACCORDANCE WITH C&MS 108.05.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN INCLUDED FOR THE WORKSITE TRAFFIC SUPERVISOR:

ITEM 614 - WORKSITE TRAFFIC SUPERVISOR 5 MNTH

MAINTENANCE OF TRAFFIC SCHEME

THE CONTRACTOR SHALL SCHEDULE THEIR WORK AND METHODS IN ORDER TO MEET THE INTENT OF THE PLANS. THE PAVEMENT SURFACES TO BE USED BY THE TRAVELING PUBLIC SHALL BE ABLE TO DRAIN FREELY. ALL COSTS TO MAINTAIN THE ROADWAY AS PER THE CONSTRUCTION AND MATERIALS SPECIFICATIONS AND THE PLANS SHALL BE INCLUDED IN ITEM 614 LUMP SUM MAINTAINING TRAFFIC UNLESS SEPARATELY ITEMIZED.

OVERNIGHT TRENCH CLOSING

THE BASE WIDENING SHALL BE COMPLETED TO A DEPTH OF NO MORE THAN 1.5 INCHES BELOW THE EXISTING PAVEMENT BY THE END OF EACH WORK DAY. NO TRENCH SHALL BE LEFT OPEN OVERNIGHT. IN CASE WORK MUST BE SUSPENDED BECAUSE OF INCLEMENT WEATHER OR OTHER REASONS, THE TRENCH FOR THE UNCOMPLETED BASE WIDENING SHALL BE BACKFILLED AT THE DIRECTION OF THE ENGINEER.

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

IN ADDITION TO THE REQUIREMENTS OF CMS 614 AND THE LATEST EDITION OF THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES (OMUTCD), A UNIFORMED LAW ENFORCEMENT OFFICER (AND OFFICIAL PATROL CAR WITH MOUNTED EMERGENCY FLASHING LIGHTS) SHALL BE PROVIDED FOR CONTROLLING TRAFFIC FOR THE FOLLOWING TASKS AS DIRECTED BY THE ENGINEER:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED.

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

DURING A TRAFFIC SIGNAL INSTALLATION.

LAW ENFORCEMENT OFFICERS (LEO'S) SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED. THE LEO'S ARE CONSIDERED TO BE EMPLOYED BY THE CONTRACTOR AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THEIR ACTIONS. ALTHOUGH THEY ARE EMPLOYED BY THE CONTRACTOR, THE PROJECT ENGINEER SHALL HAVE CONTROL OVER THEIR PLACEMENT. THE OFFICIAL PATROL CAR SHALL BE A PUBLIC SAFETY VEHICLE AS REQUIRED BY THE OHIO REVISED CODE. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEO'S SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE CONTRACTOR SHALL MAKE ARRANGEMENTS FOR THESE SERVICES AND PROVIDE 72 HOURS ADVANCE NOTICE AS REQUIRED BY THE HIGHWAY PATROL LISTED BELOW:

OHIO STATE HIGHWAY PATROL
WOOSTER POST
1786 DOVER ROAD
WOOSTER, OHIO 44691
330.264.0575

LAW ENFORCEMENT OFFICERS WITH PATROL CAR REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 614 - LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 200 HOURS

THE HOURS PAID SHALL INCLUDE MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

IF THE CONTRACTOR WISHES TO UTILIZE LEO'S FOR FLAGGING AND TRAFFIC CONTROL OTHER THAN FOR THAT REQUIRED IN THESE PLANS, THEY MAY DO SO AT THEIR OWN EXPENSE.

CONSTRUCTION EQUIPMENT MEDIAN CROSSING

CONSTRUCTION EQUIPMENT SHALL CROSS THE MEDIAN ONLY AT THE EXISTING INTERSECTIONS AND U-TURN CROSSOVERS AND AT OTHER ADDITIONAL LOCATIONS APPROVED BY THE ENGINEER. A MAXIMUM OF TWO (2) ADDITIONAL EQUIPMENT CROSSINGS MAY BE ALLOWED.

THE CONTRACTOR SHALL BE RESPONSIBLE, AT HIS EXPENSE, FOR THE RESTORATION OF THE ADDITIONAL EQUIPMENT CROSSINGS TO A CONDITION AT LEAST EQUAL TO THAT EXISTING PRIOR TO HIS WORK OPERATIONS.

FLOODLIGHTING

FLOODLIGHTING OF THE WORK SITE FOR OPERATIONS CONDUCTED DURING NIGHTTIME PERIODS SHALL BE ACCOMPLISHED SO THAT THE LIGHTS DO NOT CAUSE GLARE TO THE DRIVERS ON THE ROADWAY. TO ENSURE THE ADEQUACY OF THE FLOODLIGHT PLACEMENT, THE CONTRACTOR AND THE ENGINEER SHALL DRIVE THROUGH THE WORK SITE EACH NIGHT WHEN THE LIGHTING IS IN PLACE AND OPERATIVE PRIOR TO COMMENCING ANY WORK. IF GLARE IS DETECTED, THE LIGHT PLACEMENT AND SHIELDING SHALL BE ADJUSTED TO THE SATISFACTION OF THE ENGINEER BEFORE WORK PROCEEDS.

PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT UNIT PRICE FOR ITEM 614 - MAINTAINING TRAFFIC.

WORK OPERATIONS

IN ADDITION TO THE REQUIREMENTS OF SECTION 614 OF THE CONSTRUCTION AND MATERIAL SPECIFICATIONS THE FOLLOWING SHALL APPLY:

THE CONTRACTOR'S EQUIPMENT SHALL BE OPERATED IN THE DIRECTION OF TRAVEL WHERE PRACTICAL. A FLAGGER SHALL BE USED WHERE THE CONTRACTOR'S EQUIPMENT MUST MERGE WITH THE TRAFFIC STREAM.

THE CONTRACTOR SHALL ARRANGE CONSTRUCTION OPERATIONS SO AS TO PREVENT ANY INTERFERENCE TO THE CONTINUOUS FLOW OF TRAFFIC. ALL VEHICLES, EQUIPMENT, WORKERS AND THEIR ACTIVITIES ARE RESTRICTED AT ALL TIMES TO THE CLOSED LANES UNLESS OTHERWISE APPROVED BY THE ENGINEER.

EQUIPMENT MAY BE PARKED IN AREAS ALONG THE HIGHWAY A MINIMUM OF SIX FEET BEHIND GUARDRAIL OR 30 FEET FROM THE NEAREST EDGE OF PAVEMENT WHEN VARIOUS OPERATIONS ARE SCHEDULED TO CONTINUE THE NEXT WORKDAY. ON WEEKENDS OR AT OTHER TIMES OF SUSPENSION OF WORK, THE EQUIPMENT SHALL BE STORED AT A STORAGE AREA OUTSIDE OF THE ROADWAY RIGHT-OF-WAY. THE LOCATION SHALL HAVE PRIOR APPROVAL OF THE ENGINEER. ADEQUATE BARRICADES AND LIGHTS SHALL BE PLACED ON THE PAVEMENT SIDE OF THE EQUIPMENT TO IDENTIFY THE LIMITS OF THE EQUIPMENT. ALL OTHER EQUIPMENT, INCLUDING PRIVATE VEHICLES, SHALL BE STORED AT THE APPROVED CONTRACTOR'S STORAGE AREA.

ITEM 614 - PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN, ON SITE, FOR THE DURATION OF THE PROJECT. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS MAINTAINED BY THE DIRECTOR (OFFICE OF MATERIALS MANAGEMENT). THE APPROVED LIST OF PORTABLE CHANGEABLE MESSAGE SIGNS CAN BE FOUND ON THE ODOT WEBSITE BY CLICKING ON THE SERVICES MENU, THEN CLICKING ON MATERIALS MANAGEMENT. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 650 FT. AND 475 FT., RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. PCMS TRAILERS SHALL BE DELINEATED ON A PERMANENT BASIS BY AFFIXING CONSPICUITY TAPE CONFORMING TO CMS 614.03, IN A CONTINUOUS LINE ON THE FACE OF THE TRAILER AS SEEN BY ONCOMING ROAD USERS.

THE PROBABLE PCMS LOCATIONS WILL BE DETERMINED BY THE ENGINEER PRIOR TO BEGINNING WORK ON THIS PROJECT. PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED, FACING AWAY FROM ALL TRAFFIC, AND SHALL DISPLAY ONE OR MORE YELLOW RETROREFLECTIVE SHEETING SURFACES OF 9-INCH BY 15-INCH MINIMUM SIZE FACING TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

ITEM 614 - PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN (CONTINUED)

(THE CONTRACTOR SHALL IMPLEMENT A SYSTEM WHEREBY CHANGEABLE MESSAGES WILL BE IMPLEMENTED WITHIN FOUR (4) HOURS FOLLOWING TELEPHONE NOTIFICATION FROM THE PROJECT ENGINEER TO A DESIGNATED PHONE.)

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PREPROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PREPROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

(THE PCMS SHALL CONTAIN A CELLULAR TELEPHONE DATA LINK WHICH WILL (IN ACTIVE CELLULAR PHONE AREAS) ALLOW REMOTE SIGN ACTIVATION, MESSAGE CHANGES, MESSAGE ADDITIONS AND REVISIONS TO TIME OF DAY PROGRAMS. THE SYSTEM SHALL ALSO PERMIT VERIFICATION OF CURRENT AND PROGRAMMED MESSAGES. ONE REMOTE DATA INPUT DEVICE (LAPTOP COMPUTER PLUS MODEM OR EQUIVALENT) SHALL BE FURNISHED FOR USE BY THE DISTRICT TRAFFIC ENGINEER, OR EQUIVALENT, AND SHALL BE INSURED AGAINST THEFT.)

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF CMS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK. THE CONTRACTOR SHALL ONLY BE PAID FOR PCMS UNITS WHEN THEY ARE IN OPERATION ON THE PROJECT AS SPECIFIED IN THE PLANS OR BY THE ENGINEER.

ITEM 614 - PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 10 SNMT

ITEM 614. MAINTAINING TRAFFIC (LANES OPEN DURING HOLIDAYS OR SPECIAL EVENTS)

NO WORK SHALL BE PERFORMED AND ALL EXISTING LANES SHALL BE OPEN TO TRAFFIC DURING THE FOLLOWING DESIGNATED HOLIDAYS:

CHRISTMAS	FOURTH OF JULY
NEW YEARS	LABOR DAY
MEMORIAL DAY	THANKSGIVING

THE PERIOD OF TIME THAT THE LANES ARE TO BE OPEN DEPENDS ON THE DAY OF THE WEEK ON WHICH THE HOLIDAY OR EVENT FALLS. THE FOLLOWING SCHEDULE SHALL BE USED TO DETERMINE THIS PERIOD:

DAY OF THE WEEK	TIME ALL LANES MUST BE OPEN TO TRAFFIC
SUNDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY
MONDAY	12:00N FRIDAY THROUGH 6:00 AM TUESDAY
TUESDAY	12:00N MONDAY THROUGH 6:00 AM WEDNESDAY
WEDNESDAY	12:00N TUESDAY THROUGH 6:00 AM THURSDAY
THURSDAY	12:00N WEDNESDAY THROUGH 6:00 AM MONDAY
FRIDAY	12:00N THURSDAY THROUGH 6:00 AM MONDAY
SATURDAY	12:00N FRIDAY THROUGH 6:00 AM MONDAY

SHOULD THE CONTRACTOR FAIL TO MEET ANY OF THESE REQUIREMENTS, THE CONTRACTOR SHALL BE ASSESSED A DISINCENTIVE IN THE AMOUNT OF \$50 FOR EACH MINUTE THE ABOVE DESCRIBED LANE CLOSURE RESTRICTIONS ARE VIOLATED.

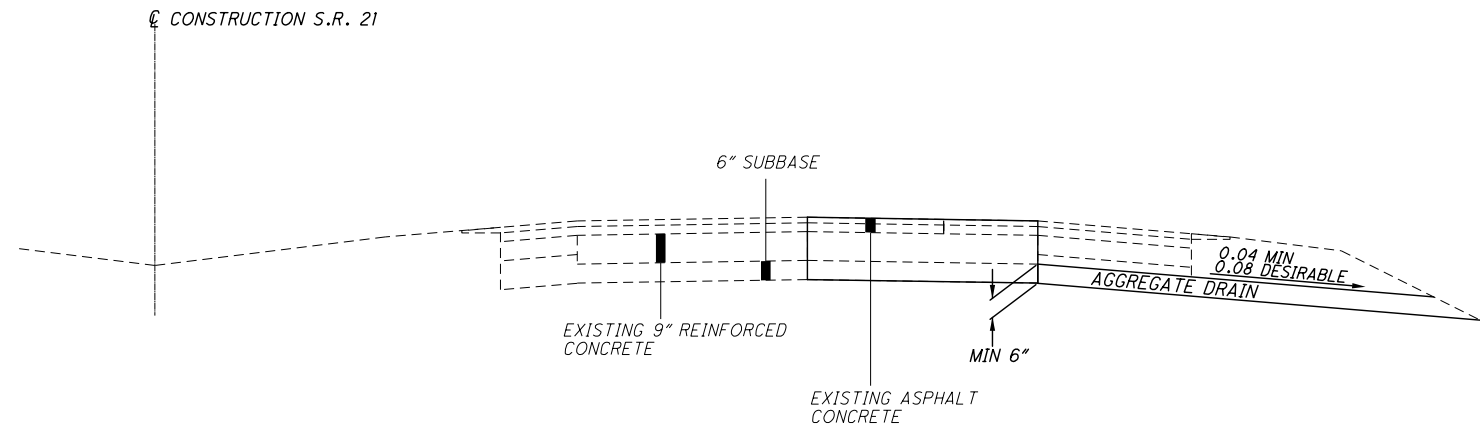
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SHEET NUM.							PART.	ITEM	ITEM	GRAND	UNIT	DESCRIPTION	SEE SHEET NO.
6	7	8	9	11	12	13	01/NHS/ PV	EXT	TOTAL				
												ROADWAY	
						8	8	202	35100	8	FT	PIPE REMOVED, 24" AND UNDER	
						1	1	202	58100	1	EACH	CATCH BASIN REMOVED	
125							125	203	20000	125	CY	EMBANKMENT	
				300			300	204	13000	300	CY	EXCAVATION OF SUBGRADE	
												EROSION CONTROL	
1608							1608	601	21050	1608	SY	TIED CONCRETE BLOCK MAT, TYPE 1	
							1500	832	30000	1500	EACH	EROSION CONTROL	
												DRAINAGE	
						LUMP	LUMP	503	11100	LS		COFFERDAMS AND EXCAVATION BRACING	
				500			500	605	31101	500	FT	AGGREGATE DRAINS, AS PER PLAN	7, 11
						8	8	611	06100	8	FT	15" CONDUIT, TYPE C, 706.01 OR 706.02	
						1	1	611	98410	1	EACH	CATCH BASIN, NO. 8	
												PAVEMENT	
	275						275	251	01010	275	CY	PARTIAL DEPTH PAVEMENT REPAIR	
	3575						3575	253	02000	3575	CY	PAVEMENT REPAIR	
				300			300	304	20000	300	CY	AGGREGATE BASE	
63							63	617	10100	63	CY	COMPACTED AGGREGATE	
												TRAFFIC CONTROL	
						888	888	621	00100	888	EACH	RPM	
						888	888	621	54000	888	EACH	RAISED PAVEMENT MARKER REMOVED	
						23.19	23.19	642	00104	23.19	MILE	EDGE LINE, 6", TYPE 1	
						11.6	11.6	642	00204	11.6	MILE	LANE LINE, 6", TYPE 1	
						156	156	642	00400	156	FT	CHANNELIZING LINE, 8", TYPE 1	
						50	50	642	00500	50	FT	STOP LINE, TYPE 1	
						3	3	642	01300	3	EACH	LANE ARROW, TYPE 1	
						0.25	0.25	646	10010	0.25	MILE	EDGE LINE, 6"	
						0.13	0.13	646	10110	0.13	MILE	LANE LINE, 6"	
												MAINTENANCE OF TRAFFIC	
						200	200	614	11110	200	HOUR	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	
						5	5	614	11500	5	MNTH	WORKSITE TRAFFIC SUPERVISOR	
		250					250	614	13000	250	CY	ASPHALT CONCRETE FOR MAINTAINING TRAFFIC	
						10	10	614	18600	10	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN	
					11.72		11.72	614	20100	11.72	MILE	WORK ZONE LANE LINE, CLASS I, 642 PAINT	
							LS	614	11000	LS		MAINTAINING TRAFFIC	
							5	619	16010	5	MNTH	FIELD OFFICE, TYPE B	
							LS	623	10000	LS		CONSTRUCTION LAYOUT STAKES AND SURVEYING	
							LS	624	10000	LS		MOBILIZATION	

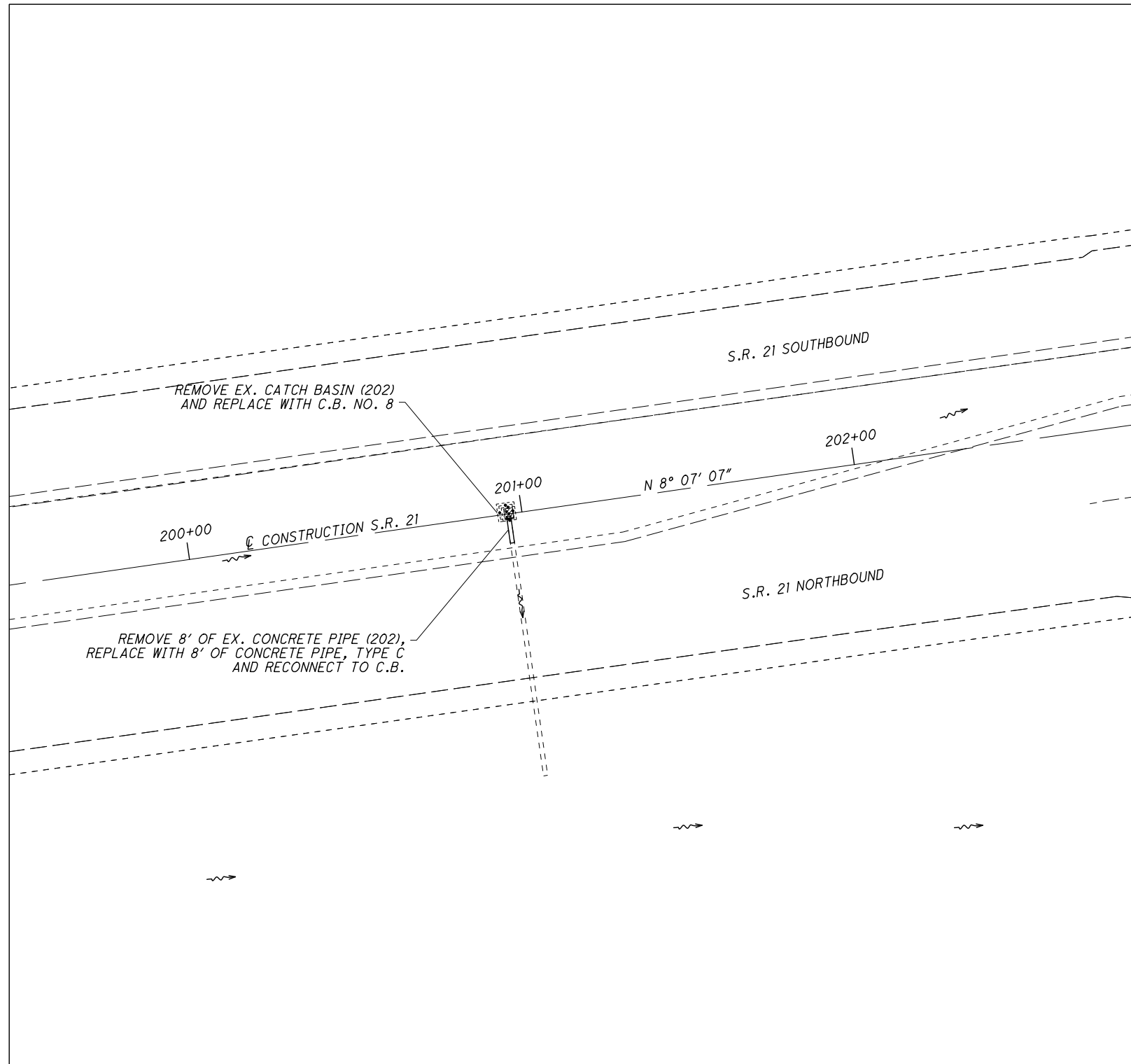
GENERAL SUMMARY

WAY - 21 - 0.00



NOTES

- THE BOTTOM OF THE AGGREGATE DRAINS SHALL BE AT OR BELOW THE BOTTOM OF THE PAVEMENT'S EXISTING AGGREGATE SUBBASE AT THE POINT OF CONTACT.
- THE AGGREGATE DRAINS SHALL OUTLET TO THE EXISTING SIDE SLOPE.
- A QUANTITY OF 500 FT OF ITEM 605 AGGREGATE DRAINS HAS BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER AT LOCATIONS WHERE WATER IS EVIDENT AFTER PAVEMENT REMOVAL.
- A QUANTITY OF 300 CY OF ITEM 304 AGGREGATE BASE AND 300 CY OF ITEM 204 EXCAVATION OF SUBGRADE HAS BEEN CARRIED TO THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER AT LOCATIONS WHERE SUBGRADE IS SATURATED AFTER PAVEMENT REMOVAL.
- PAVED SHOULDER REMOVAL AND REPLACEMENT SHALL BE INCLUDED IN THE COST OF ITEM 605 AGGREGATE DRAINS
- SEE GENERAL SUMMARY FOR ADDITIONAL FULL DEPTH REPAIR QUANTITIES, AS PER PLAN.



EXISTING STRUCTURE VERIFICATION

DETAILS AND DIMENSIONS ON THESE PROPOSED PLANS PERTAINING TO THE EXISTING STRUCTURE HAVE BEEN OBTAINED FROM THE PLANS OF THE EXISTING STRUCTURE AND FROM FIELD OBSERVATIONS AND MEASUREMENTS. CONSEQUENTLY, SUCH DETAILS AND DIMENSIONS ARE INDICATIVE OF THE EXISTING STRUCTURE AND THE PROPOSED WORK, BUT THEY SHALL BE CONSIDERED TENTATIVE AND APPROXIMATE. THE CONTRACTOR IS REFERRED TO CMS SECTIONS 102.05 AND 105.02. BASE THE CONTRACT BID PRICES UPON A RECOGNITION OF THE UNCERTAINTIES DESCRIBED ABOVE AND UPON A PRE BID EXAMINATION OF THE EXISTING STRUCTURE. HOWEVER, THE DEPARTMENT WILL PAY FOR ALL PROJECT WORK BASED UPON ACTUAL DETAILS AND DIMENSIONS THAT HAVE BEEN VERIFIED IN THE FIELD.

REHABILITATE WAY-21-3.81 CULVERT BY PERFORMING THE FOLLOWING WORK:

- 1) MAINTAIN ONE LANE OF TWO-WAY TRAFFIC AT ALL TIMES USING SHOULDER/LANE CLOSURES AS NEEDED.
- 2) CLEAR AND GRUB AS NEEDED TO ACCESS THE WORK.
- 3) REMOVE 8' OF PIPE AT CATCH BASIN AND CATCH BASIN AS SHOWN IN PLANS.
- 4) INSTALL 8' OF CONCRETE PIPE AND CATCHBASIN TO SAME ELEVATIONS OF PREVIOUS PIPE AND CATCH BASIN. ENSURE CONNECTIONS ARE WATER TIGHT.
- 5) SEED AND MULCH ANY DISTURBED AREAS (TO BE CONSIDERED INCIDENTAL TO THE APPLICABLE 611 ITEM).

EXISTING STRUCTURE	
TYPE:	CIRCULAR CONCRETE PIPE
SIZE:	15" X 87'
SKEW:	NO SKEW
ALIGNMENT:	TANGENT
COVER DEPTH:	APPROX. 4 FEET
DATE BUILT:	1961
CFN:	850210026

ESTIMATED QUANTITIES CARRIED TO GENERAL SUMMARY			
ITEM	QUANTITY	UNIT	DESCRIPTION
202	8	FT	PIPE REMOVED, 24" AND UNDER
202	1	EACH	CATCH BASIN REMOVED
503	LUMP		COFFERDAMS AND EXCAVATION BRACING
611	8	FT	15" CONDUIT, TYPE C, 706.01, 706.02, OR 706.03
611	1	EACH	CATCH BASIN, NO. 8



WAY -21-3.81 CULVERT DETAIL
STA. 200+96

WAY -21-0.00