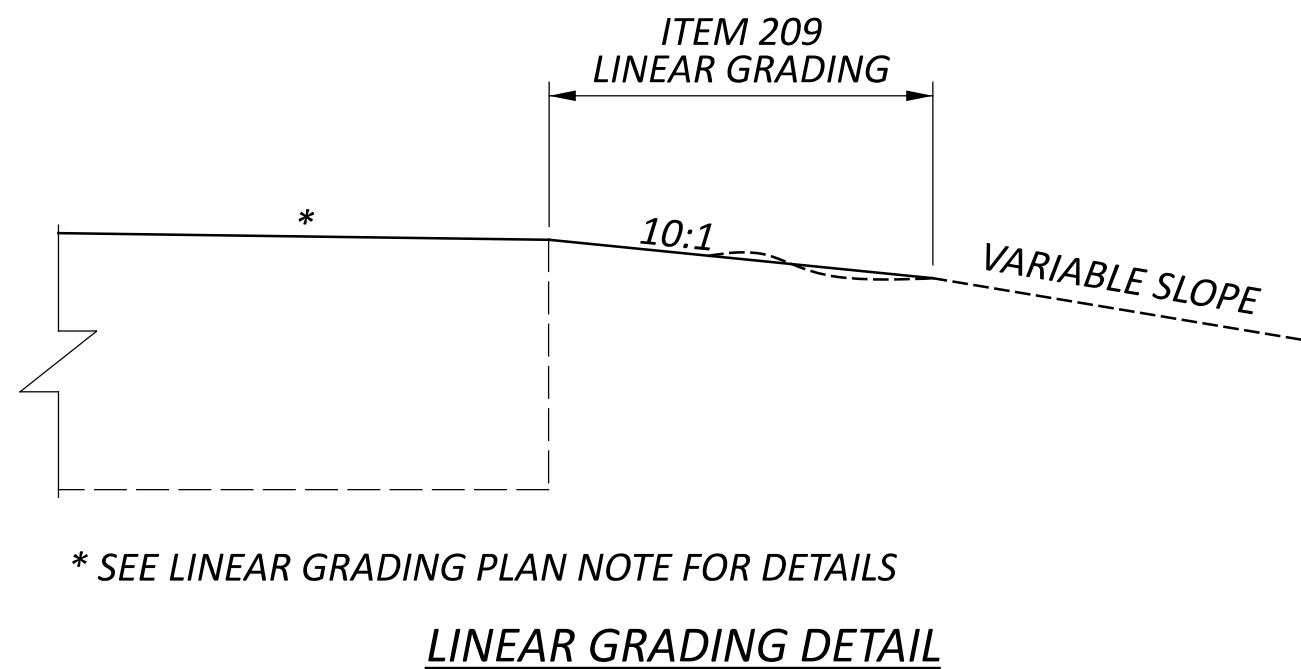
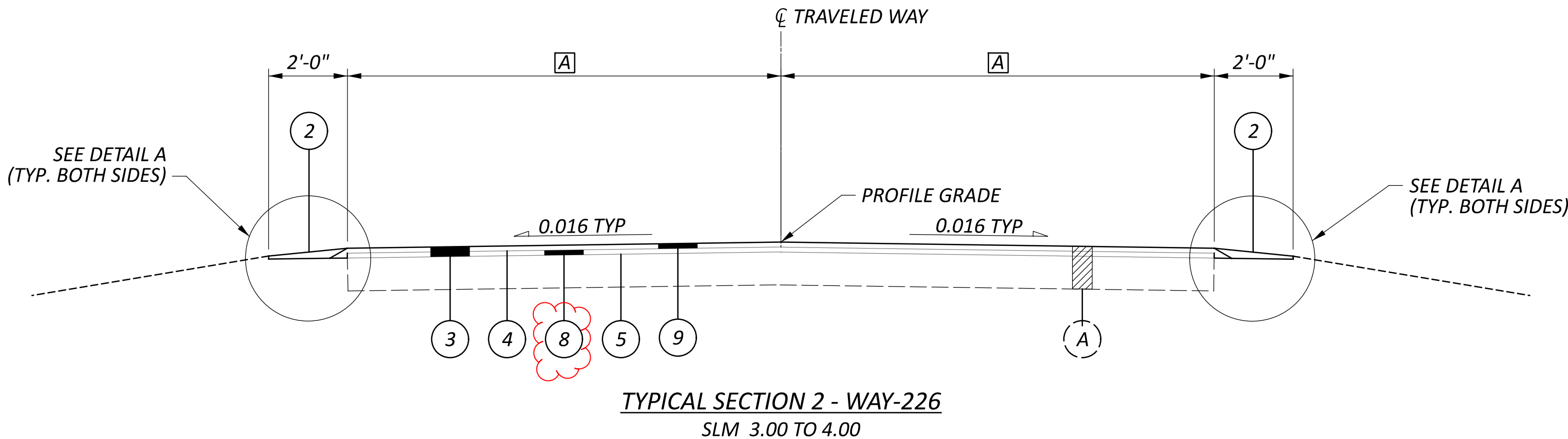
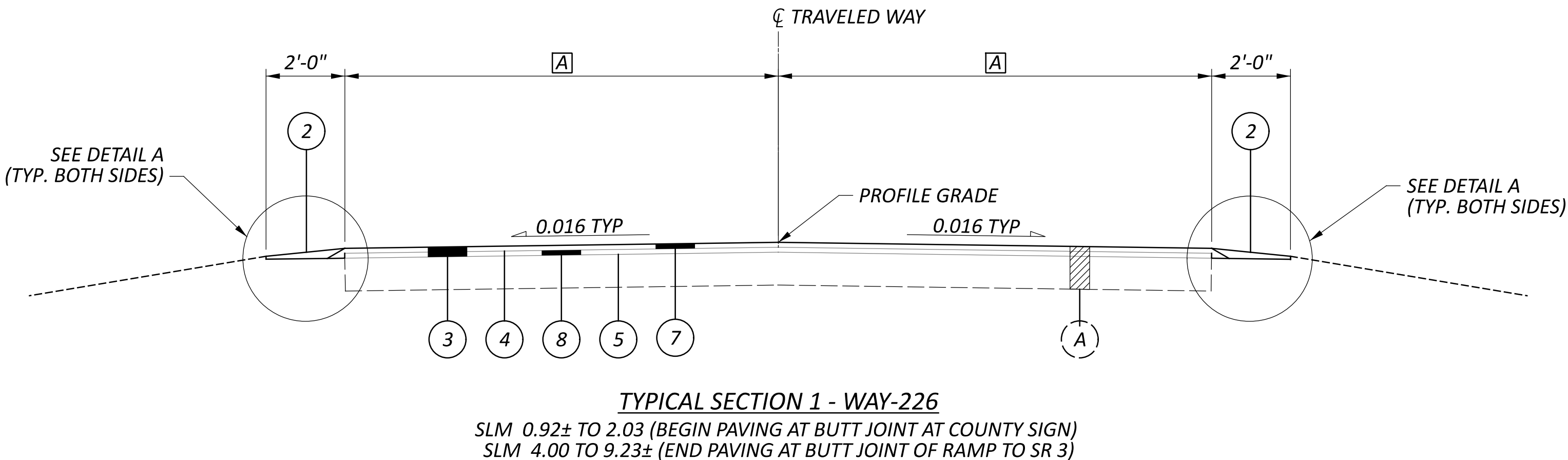
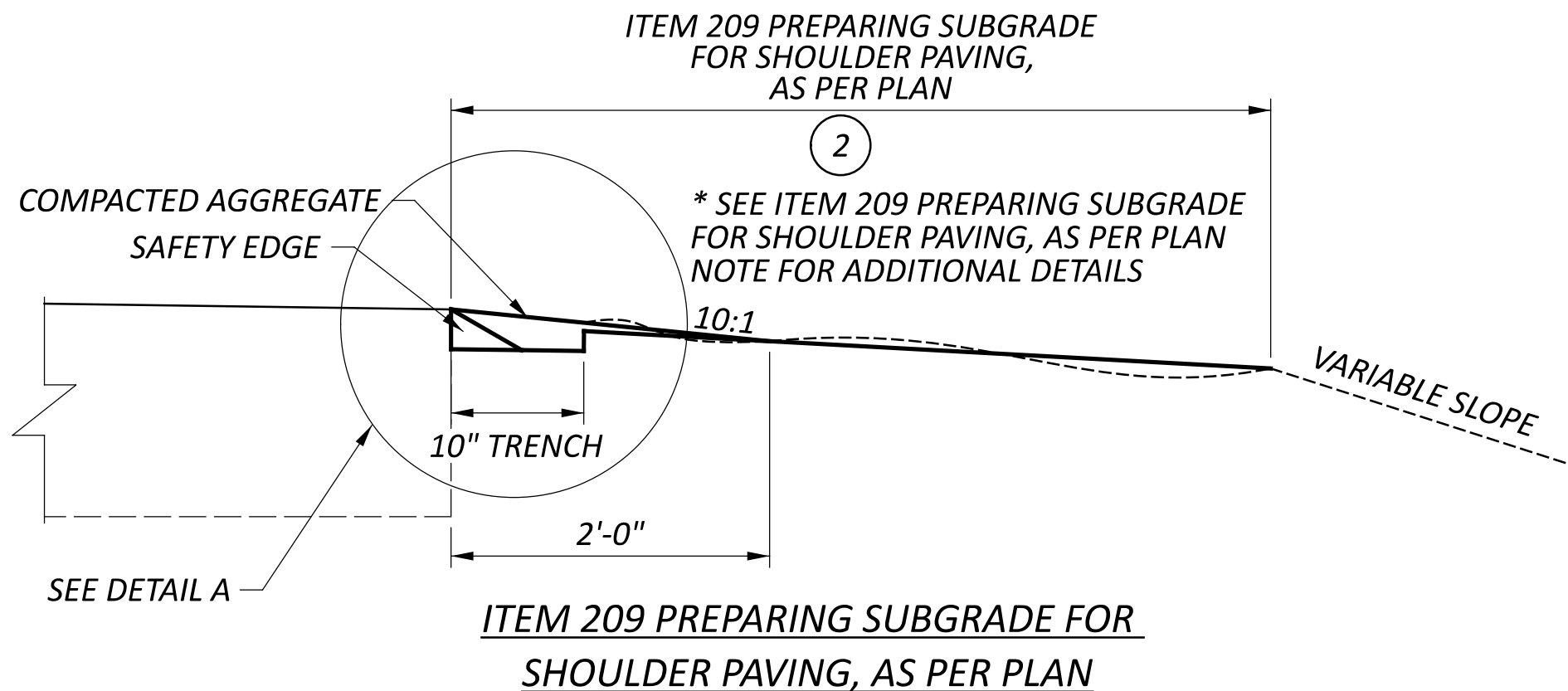
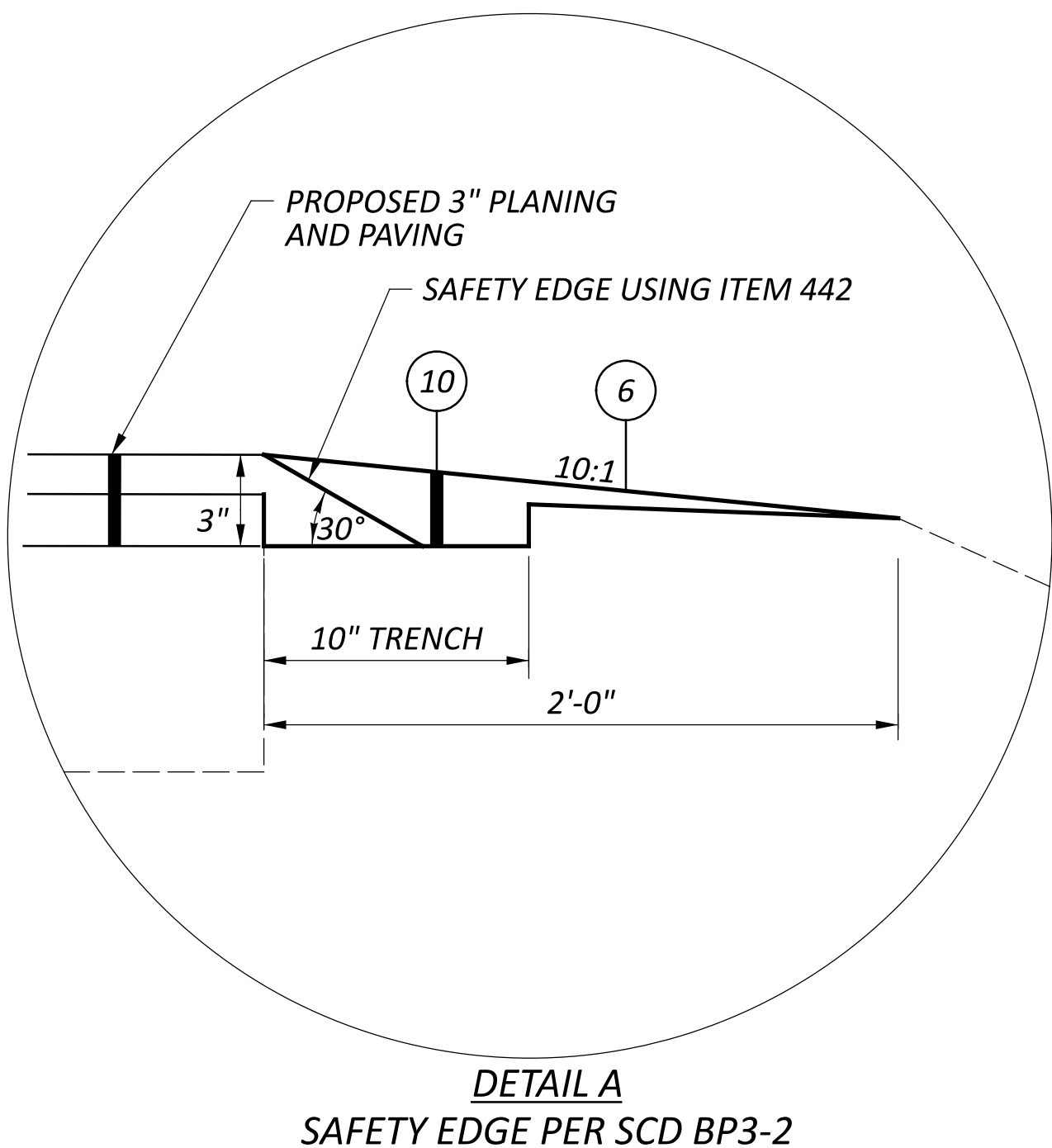


PROPOSED LEGEND:

- 1 ITEM 209 - LINEAR GRADING
- 2 ITEM 209 - PREPARING SUBGRADE FOR SHOULDER PAVING, AS PER PLAN (TYP.)
- 3 ITEM 254 - PAVEMENT PLANNING, ASPHALT CONCRETE (3.0")
- 4 ITEM 407 - TACK COAT (0.06 GAL/SY)
- 5 ITEM 407 - TACK COAT (0.09 GAL/SY)
- 6 ITEM 408 - PRIME COAT, AS PER PLAN (TYP.)
- 7 ITEM 442 - ASPHALT CONCRETE SURFACE COURSE, 9.5MM, TYPE A (446), AS PER PLAN (PG64-22) (1.25")
- 8 ITEM 442 - ASPHALT CONCRETE INTERMEDIATE COURSE, 12.5MM, TYPE A (448), AS PER PLAN (1.75")
- 9 ITEM 442 - ASPHALT CONCRETE SURFACE COURSE, 9.5MM, TYPE A (446), (PG70-22M) (1.25")
- 10 ITEM 617 - COMPACTED AGGREGATE (2" AVG.) (TYP.)

EXISTING LEGEND:

- A EXISTING ASPHALT CONCRETE PAVEMENT (THICKNESS VARIES 11"-13")
- B EXISTING COMPACTED AGGREGATE (1.5" AVG.) (TYP.)
- C EXISTING CONCRETE PAVEMENT (THICKNESS UNKNOWN)



A VARIES; SEE PAVEMENT AND SHOULDER DATA SHEETS

GUARDRAIL NOT SHOWN FOR CLARITY SEE GUARDRAIL DETAIL SHEETS

SCALE EXAGGERATED BY A FACTOR OF 2



GENERAL

UTILITIES  
(G102A)

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS:

VILLAGE VILLAGE OF SHREVE 150 WEST MCCONKLEY STREET P.O. BOX 604 SHREVE, OHIO 44676 330.567.2601	FIBER OPTIC AMERICAN ELECTRIC POWER 700 MORRISON ROAD GAHANNA, OH 43230 614.716.2531
FIBER OPTIC SPRINT 11370 ENTERPRISE PARK DRIVE SHARONVILLE, OH 45241 513.612.4204	COMMUNICATION BRIGHTSPEED 203 W. 9 <sup>TH</sup> STREET LORAIN, OH 44052 440.244.8330
CABLE CHARTER COMMUNICATIONS 5520 WHIPPLE AVENUE NW NORTH CANTON, OH 44720 330.494.9200	CABLE MASSILLON CABLE TEVELVISION P.O. BOX 917 WOOSTER, OH 44691 330.345.5110
ELECTRIC AEP OHIO 301 CLEVELAND AVENUE SW CANTON, OH 44702 800.672.2231	GAS ASPIRE ENERGRY 300 TRACY BRIDGE ROAD ORRVILLE, OH 44667 330.682.7726
GAS COLUMBIA GAS OF OHIO 1021 NORTH MAIN STREET MANSFIELD, OH 44903 419.528.1134	GAS ENBRIDGE GAS 320 SPRINGSIDE DRIVE, SUITE 320 AKRON, OH 44333 800.362.7557
GAS ENERGEX POWER 353 E LINCOLN HIGHWAY COATESVILLE, PA 19320 724.622.0093	GAS KENOIL, INC 1537 BLACHEYVILLE ROAD WOOSTER, OH 44691 330.262.1144
GAS KINDER MORGAN 605 WESTLAKE DRIVE ASHLAND, OH 44805 714.560.4967	GAS MARATHON PIPELINE 539 SOUTH MAIN STREET FINDLEY, OH 45840 419.884.0800
GAS ROVER PIPELINE 1300 MAIN STREET HOUSTON, TX 77002 501.322.9622	GAS TC ENGERGY 589 N STATE ROAD MEDINA, OH 44256 330.721.4163
TRAFFIC ODOT DISTRICT THREE 906 CLARK AVENUE ASHLAND, OH 44805 419.207.2868	SEWER WAYNE COUNTY ENGINEERS 3151 WEST OLD LINCOLN WAY WOOSTER, OH 44691 330.287.5500

THE AFOREMENTIONED UTILITY COMPANIES AND AGENCIES HAVE VARIOUS FACILITIES IN THE AREA THAT WILL REMAIN IN PLACE DURING CONSTRUCTION.

EXTREME CAUTION SHOULD BE EXERCISED IN AREAS WITH UTILITIES. SECTIONS 105.07 AND 107.16 OF THE DEPARTMENT OF TRANSPORTATION CONSTRUCTION AND MATERIAL SPECIFICATIONS REQUIRE, AMONG OTHER THINGS, THAT THE CONTRACTOR COOPERATE WITH ALL UTILITIES LOCATED WITHIN THE LIMITS OF THIS CONSTRUCTION PROJECT AND TAKE RESPONSIBILITY FOR THE PROTECTION OF THE UTILITY PROPERTY AND SERVICES.

EXISTING PLANS  
(G103)

EXISTING PLANS ENTITLED WAY-226-0.45 PID 25655, DATED 2013 MAY BE INSPECTED IN THE ODOT DISTRICT THREE OFFICE IN ASHLAND.

CONSTRUCTION NOISE  
(G104)

ACTIVITIES AND LAND USE ADJACENT TO THIS PROJECT MAY BE AFFECTED BY CONSTRUCTION NOISE. IN ORDER TO MINIMIZE ANY ADVERSE CONSTRUCTION NOISE IMPACTS, DO NOT OPERATE POWER-OPERATED CONSTRUCTION-TYPE DEVICES BETWEEN THE HOURS OF 7:00PM AND 7:00AM. IN ADDITION, DO NOT OPERATE AT ANY TIME ANY DEVICE IN SUCH A MANNER THAT THE NOISE CREATED SUBSTANTIALLY EXCEEDS THE NOISE CUSTOMARILY AND NECESSARILY ATTENDANT TO THE REASONABLE AND EFFICIENT PERFORMANCE OF SUCH EQUIPMENT.

WORK LIMITS  
(G106)

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

ROUTINE MAINTENANCE

BETWEEN THE TIME THAT BIDS ARE TAKEN AND THE START OF CONSTRUCTION, THE MAINTAINING AGENCY MAY ENTER UPON THE PROJECT AND PERFORM ROUTINE MAINTENANCE SUCH AS CRACK SEALING, PATCHING, AND BERM AND SHOULDER REPAIR. THE EFFECTS, IF ANY, OF THE PERFORMANCE OF ROUTINE MAINTENANCE SHALL BE CONSIDERED AS INHERENT IN WORK OF THE CHARACTER PROVIDED FOR IN THE PLAN AND THE RESULTING CONDITIONS SHALL NOT BE CONSIDERED AS DIFFERING MATERIALLY FROM THOSE EXISTING AT THE TIME BIDS WERE TAKEN.

ADA WAIVER  
(R128)

AN APPROVED ADA DESIGN WAIVER IS REQUIRED ON THIS PROJECT. THE FOLLOWING FEATURES LISTED BELOW CANNOT FEASIBLY BE CONSTRUCTED TO MEET ADA GUIDELINES.

ADA DESIGN WAIVERS			
RAMP REFERENCE	ADA FEATURE (ID NUMBER)	APPROVAL DATE:	PEDESTIRAN FACILITIES SUBSET NUMBER
B1	RMP0015889	2025/08/27	4
C2	RMP0015890	2025/08/27	5
C3	RMP0015891	2025/08/27	5
J1	RMP0029902	2025/08/27	12
J2	RMP0029907	2025/08/27	12
J3	RMP0029903	2025/08/27	12
J4	RMP0029908	2025/08/27	12
K3	RMP0015920	2025/08/27	13
K4	RMP0015921	2025/08/27	13
L1	RMP0029922	2025/08/27	14
L2	RMP0029919	2025/08/27	14
N1	RMP0029928	2025/08/27	16
N2	RMP0029927	2025/08/27	16
Q1	RMP0029933	2025/08/27	19
Q2	RMP0029932	2025/08/27	19
Q3	RMP0029934	2025/08/27	19
Q4	RMP0029935	2025/08/27	19
R1	RMP0015941	2025/08/27	20
R2	RMP0015940	2025/08/27	20
S1	RMP0029939	2025/08/27	21
S2	RMP0029940	2025/08/27	21
T1	RMP0029941	2025/08/27	21
T2	RMP0029942	2025/08/27	21

PRECONSTRUCTION PEDESTRIAN FACILITY LAYOUT INSPECTION

THE PROPOSED LAYOUT OF THE PEDESTRIAN FACILITIES INCLUDED IN THESE PLANS IS TO BE FIELD REVIEWED AND VERIFIED FOR COMPLIANCE WITH THE PLANS AND APPROPRIATE STANDARDS PRIOR TO PERFORMING ANY ASSOCIATED REMOVAL OR CONSTRUCTION. THIS MEETING IS INTENDED TO REVIEW PROPOSED WORK AS LAID OUT BY THE CONTRACTOR PRIOR TO THE MEETING; THIS MEETING IS NOT INTENDED TO LAYOUT ALL LOCATIONS IN CONJUNCTION WITH THE CONTRACTOR. THE CONTRACTOR SHOULD ADHERE TO THE PROJECT PLANS ON INITIAL LAYOUT PRIOR TO THIS MEETING, DETERMINE IF THERE ARE QUESTIONS, CONCERNS, OR CONTRACTOR-PROPOSED MODIFICATIONS TO THE DESIGN AT EACH LOCATION, AND BE PREPARED TO DISCUSS ANY SUCH LOCATIONS.

THE MEETING PARTICIPANTS WILL REVIEW EACH LOCATION AS REQUESTED BY THE CONTRACTOR, ADHERING TO THE ABOVE DETAILS. ADDITIONAL LOCATIONS WILL BE VERIFIED BY DISTRICT PERSONNEL FOR ADHERENCE TO THE PLANS AND SPECIFICATIONS.

COORDINATE WITH THE PROJECT ENGINEER TO SCHEDULE THE MEETING WITH ALL APPROPRIATE STAKEHOLDERS IN ORDER TO PROVIDE A MINIMUM OF 14 CALENDAR DAY NOTICE TO ALL MEETING ATTENDEES. THE REQUIRED STAKEHOLDERS ARE THE DISTRICT ADA ENGINEER, DISTRICT ADA COORDINATOR, MUNICIPAL REPRESENTATIVE (IF APPLICABLE), PROJECT ENGINEER, AND CONTRACTOR REPRESENTATIVE. THE ENGINEER OF RECORD, ODOT PROJECT MANAGER, ODOT DESIGNERS, AND CONSTRUCTION AREA ENGINEER SHOULD BE INVITED AS OPTIONAL ATTENDEES.

ALL MATERIAL, EQUIPMENT, LABOR, AND INCIDENTALS NEEDED TO COMPLETE THIS MEETING ARE TO BE INCLUDED IN THE CONTRACT BID PRICE FOR THE APPROPRIATE PEDESTRIAN FACILITY ASSOCIATED WITH THIS WORK.

ROADWAY

ITEM 201 – CLEARING AND GRUBBING

REMOVE ALL TREES AND STUMPS SPECIFICALLY MARKED FOR REMOVAL WITHIN THE CONSTRUCTION LIMITS UNDER THE LUMP SUM BID FOR ITEM 201, CLEARING AND GRUBBING. THE FOLLOWING IS AN APPROXIMATE ESTIMATE OF THE NUMBER OF TREES TO BE REMOVED UNDER A SEPARATE TREE PRUNING CONTRACT BY [DATE] [XXX].

<u>SIZES</u>	<u>NO. TREES</u>	<u>NO. STUMPS</u>	<u>TOTAL</u>
48"	0	1	1

A LUMP SUM QUANTITY IS INCLUDED IN THE GENERAL SUMMARY FOR ITEM 201 – CLEARING AND GRUBBING FOR AREAS NOT INCLUDED IN THE ABOVE LIST. ALL PROVISIONS AS SET FORTH IN THE SPECIFICATIONS UNDER THIS ITEM ARE INCLUDED IN THE LUMP SUM BID PRICE FOR ITEM 201 – CLEARING AND GRUBBING.

ITEM 623 – MONUMENT ASSEMBLY ADJUSTED/RECONSTRUCTED TO GRADE

THE CONTRACTOR AND THE ENGINEER SHALL FIELD VERIFY THE LOCATION OF ALL EXISTING MONUMENT ASSEMBLIES LISTED IN THE PLANS PRIOR TO BEGINNING ANY WORK ON THE MONUMENT ASSEMBLIES. THE USE OF A METAL DETECTOR AND/OR GPS SURVEY EQUIPMENT MAY BE NECESSARY TO LOCATE BURIED MONUMENT ASSEMBLIES. ANY MONUMENT ASSEMBLY THAT IS IMMEDIATELY VISIBLE ON THE SURFACE OF THE EXISTING PAVEMENT, OR IS UNCOVERED DURING THE PLANING PROCESS, SHALL BE ADJUSTED TO GRADE IF WITHIN TOLERANCE OF THE ADJUSTMENT COLLAR. ANY MONUMENT NOT FITTING THIS CRITERIA SHALL BE TREATED AS RECONSTRUCTED TO GRADE.

THE ENGINEER SHALL MAKE THE FINAL DETERMINATION OF WHETHER EACH MONUMENT ASSEMBLY IS TO BE RECONSTRUCTED OR ADJUSTED AFTER THE PLACEMENT OF THE FINAL ASPHALT OR CONCRETE PAVEMENT SURFACE. ANY MONUMENT ASSEMBLY THAT DOES NOT HAVE AN EXISTING ADJUSTABLE FRAME AND LID, OR THAT EXHIBITS SUBSTANTIAL DETERIORATION AS DETERMINED BY THE ENGINEER REQUIRING MORE WORK THAN WOULD BE CONSIDERED NORMAL FOR ITEM 623 – MONUMENT ASSEMBLY ADJUSTED TO GRADE SHALL BE RECONSTRUCTED. ANY EXISTING MONUMENT THAT DOES NOT HAVE AN EXISTING SALVAGEABLE MONUMENT ASSEMBLY AROUND THE PIN SHALL BE RECONSTRUCTED USING A NEW MONUMENT BOX AS PER RM-1.1, MAINTAINING THE EXISTING MONUMENTATION LOCATION.

ALL WORK RELATED TO ADJUSTING OR RECONSTRUCTING MONUMENT ASSEMBLIES TO GRADE WILL BE IN ACCORDANCE WITH SPECIFICATIONS 611.10.C AND 623 OF THE ODOT C&MS.


ALL LABOR, MATERIAL, EQUIPMENT, AND INCIDENTALS NEEDED TO COMPLETE THIS WORK IS TO BE PAID USING THE CONTRACT BID PRICE PER EACH FOR ITEM 623 – MONUMENT ASSEMBLY ADJUSTED TO GRADE- AS PER PLAN OR MONUMENT ASSEMBLY RECONSTRUCTED TO GRADE, AS PER PLAN. A LIST OF KNOWN LOCATIONS IS SHOWN BELOW. A TOTAL QUANTITY OF MONUMENT ASSEMBLIES REQUIRING WORK IS CARRIED TO THE GENERAL SUMMARY.

SLM CHART FOR MONUMENT BOXES									
1.79	3.33	4.08	4.87	5.65	6.49	7.32	8.16	8.76	
1.87	3.49	4.26	5.03	5.81	6.64	7.49	8.33	8.81	
2.89	3.61	4.42	5.18	5.98	6.81	7.64	8.48	8.91	
3.02	3.76	4.58	5.35	6.15	6.98	7.81	8.66	9.06	
3.14	3.89	4.73	5.5	6.32	7.15	7.99	8.71	9.18	

ITEM 623 – MONUMENT ASSEMBLY ADJUSTED TO GRADE, AS PER PLAN 34 EACH  
ITEM 623 – MONUMENT ASSEMBLY RECONSTRUCTED TO GRADE, AS PER PLAN 11 EACH

DESIGN AGENCY

DISTRICT 3



ENGINEERING  
TEAM TWO

DESIGNER

JEC

REVIEWER

NRF 8-15-25

PROJECT ID

121723

SHEET

P.5

TOTAL

69



**ITEM 606 – ANCHOR ASSEMBLY, MGS TYPE B (R112A)**

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING ANY OF THE MASH 2016 GUARDRAIL END TERMINALS FOR TYPE MGS GUARDRAIL AS LISTED ON ROADWAY ENGINEERING’S WEB PAGE UNDER ROADSIDE SAFETY DEVICES FOR APPROVED GUARDRAIL END TREATMENTS. INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS, IN ACCORDANCE WITH THE MANUFACTURER’S SPECIFICATIONS.

REFER TO THE MANUFACTURER’S INSTRUCTIONS REGARDING THE INSTALLATION OF, AND THE GRADING AROUND, THE FOUNDATION TUBES AND GROUND STRUT. THE TOP OF ANY FOUNDATION TUBE SHOULD BE LESS THAN 4 INCHES ABOVE THE GROUND. THE PLACEMENT OF THE FOUNDATION TUBES SHOULD BE AN APPROPRIATE DEPTH BELOW THE LEVEL LINE IN ORDER TO MAINTAIN THE FINISHED GUARDRAIL HEIGHT OF 31 INCHES FROM THE EDGE OF THE SHOULDER.

ON-SITE GRADING IS REQUIRED IF THE TOP OF THE FOUNDATION TUBES OR TOP OF THE GROUND STRUT DOES PROJECT MORE THAN 4 INCHES ABOVE THE GROUND LINE.  
THE FACE OF THE TYPE B IMPACT HEAD SHALL BE COVERED WITH TYPE J, ASTM D4956 TYPE XI REFLECTIVE SHEETING, PER CMS 730.193.

THE FACE OF THE TYPE B IMPACT HEAD SHALL BE COVERED WITH REROUNDABLE RETROREFLECTIVE SHEETING, PER CMS 730.191.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID FOR ITEM 606, ANCHOR ASSEMBLY, MGS TYPE B, EACH, AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT A COMPLETE AND FUNCTIONAL ANCHOR ASSEMBLY SYSTEM, INCLUDING REFLECTIVE SHEETING AND ALL RELATED HARDWARE, GRADING, EMBANKMENT AND EXCAVATION NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER.

**ITEM 606 – ANCHOR ASSEMBLY, MGS TYPE E (R113A)**

THIS ITEM SHALL CONSIST OF FURNISHING AND INSTALLING ANY OF THE MASH 2016 GUARDRAIL END TERMINALS FOR TYPE MGS GUARDRAIL AS LISTED ON ROADWAY ENGINEERING’S WEB PAGE UNDER ROADSIDE SAFETY DEVICES FOR APPROVED GUARDRAIL END TREATMENTS. INSTALLATION SHALL BE AT THE LOCATIONS SPECIFIED IN THE PLANS, IN ACCORDANCE WITH THE MANUFACTURER’S SPECIFICATIONS.

THE FACE OF THE TYPE E IMPACT HEAD SHALL BE COVERED WITH REROUNDABLE RETROREFLECTIVE SHEETING, PER CMS 730.191.

REFER TO THE MANUFACTURER’S INSTRUCTIONS REGARDING THE INSTALLATION OF, AND THE GRADING AROUND THE FOUNDATION TUBES AND GROUND STRUT. THE TOP OF ANY FOUNDATION TUBE SHOULD BE LESS THAN 4 INCHES ABOVE THE GROUND. THE PLACEMENT OF THE FOUNDATION TUBES SHOULD BE AN APPROPRIATE DEPTH BELOW THE LEVEL LINE IN ORDER TO MAINTAIN THE FINISHED GUARDRAIL HEIGHT OF 31 INCHES FROM THE EDGE OF THE SHOULDER.

ON-SITE GRADING IS REQUIRED IF THE TOP OF THE FOUNDATION TUBES OR TOP OF THE GROUND STRUT DOES PROJECT MORE THAN 4 INCHES ABOVE THE GROUND LINE.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID FOR ITEM 606, ANCHOR ASSEMBLY, MGS TYPE E, EACH, AND SHALL INCLUDE ALL LABOR, TOOLS, EQUIPMENT AND MATERIALS NECESSARY TO CONSTRUCT A COMPLETE AND FUNCTIONAL ANCHOR ASSEMBLY SYSTEM, INCLUDING ALL RELATED TRANSITIONS, REFLECTIVE SHEETING, HARDWARE, GRADING, EMBANKMENT AND EXCAVATION NOT SEPARATELY SPECIFIED, AS REQUIRED BY THE MANUFACTURER.

**ITEM 606 – BRIDGE TERMINAL ASSEMBLY, TYPE 4, AS PER PLAN**

THIS ITEM SHALL BE USED TO REPLACE THE EXISTING TYPE 4 BRIDGE TERMINAL ASSEMBLIES THAT ARE ONLY PARTIAL ASSEMBLIES (POSTS 1 TO 7 OR LESS). SEE GUARDRAIL SUBSUMMARY SHEET FOR LOCATIONS.

PAYMENT FOR THE ABOVE WORK SHALL BE MADE AT THE UNIT PRICE BID PER EACH FOR ITEM 606 – BRIDGE TERMINAL ASSEMBLY, TYPE 4, AS PER PLAN, AND SHALL INCLUDE ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS NECESSARY TO COMPLETE THE ABOVE WORK (REPLACEMENT FROM POST 1 TO 7 OR LESS).

**DRAINAGE**

**ITEM 611 – CASTINGS ADJUSTED TO GRADE**  
**ITEM 638 – VALVE BOX ADJUSTED TO GRADE**

THE CASTING TO BE ADJUSTED MAY OR MAY NOT HAVE AN EXISTING FRAME. THE WORK SHALL CONSIST OF ADJUSTING THE EXISTING CASTING TO THE SATISFACTION OF THE ENGINEER. IT IS NOT INTENDED TO PLACE NEW FRAMES WHERE NONE CURRENTLY EXIST. THE CONTRACTOR IS REMINDED TO FIELD CHECK ALL ADJUSTMENT TO GRADE ITEMS PRIOR TO BIDDING, AS NO ADDITIONAL COMPENSATION WILL BE GRANTED FOR LABOR AND MATERIALS REQUIRED TO SATISFACTORILY ADJUST CASTINGS WITHOUT FRAMES.

ITEM 611 – INLET ADJUSTED TO GRADE	18 EACH
ITEM 611 – MANHOLE ADJUSTED TO GRADE	15 EACH
ITEM 611 – CATCH BASIN ADJUSTED TO GRADE	6 EACH
ITEM 638 – VALVE BOX ADJUSTED TO GRADE	16 EACH

**ITEM SPECIAL – MISCELLANEOUS METAL (D119)**

EXISTING CASTINGS MAY PROVE TO BE UNSUITABLE FOR REUSE, AS DETERMINED BY THE ENGINEER. IT SHALL BE THE CONTRACTOR’S RESONSIBILITY TO PROVIDE THE CASTINGS OF THE REQUIRED TYPE, SIZE AND STRENGTH (HEAVY OR LIGHT DUTY) FOR THE PARTICULAR STRUCTURE IN QUESTION. ALL MATERIAL SHALL MEET ITEM 611 OF THE SPECIFICATIONS AND SHALL HAVE THE PRIOR APPROVAL OF THE ENGINEER.

THE FOLLOWING ESTIMATED QUANTITY HAS BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AS DIRECTED BY THE ENGINEER.

SPECIAL - MISCELLANEOUS METAL	1500 LB
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THE CONTRACTOR IS CAUTIONED TO USED EXTREME CARE IN THE REMOVAL, STORAGE AND REPLACEMENT OF ALL EXISTING CASTINGS. CASTINGS DAMAGED BY THE NEGLIGENCE OF THE CONTRACTOR, AS DETERMINED BY THE ENGINEER, SHALL BE REPLACED WITH THE PROPER NEW CASTINGS AT THE EXPENSE OF THE CONTRACTOR.

**LOCATIONS OF CASTINGS)**  
SEE JTTHE FOLLOWING TABLE SHOWS THE APPROXIMATE LOCATIONS AND QUANTITIES OF CASTINGS TO BE ADJUSTED TO GRADE. LOCATIONS AND QUANTITY SHOULD BE FIELD VERIFIED AND ARE AS DIRECTED BY THE ENGINEER.

WAY-226 CASTINGS ADJUSTED TO GRADE				
LOCATION	VALVE BOX	CATCH BASIN	INLET	MANHOLE
PROSPECT ST.	3			3
EAGLE NEST LN.				1
MAIN ST.				2
SLM 2.23				1
MCCONKEY ST	3	2		3
JONES ST.	4	1		2
SLM 2.43	1			1
ROBINSON ST.				1
WOODS ST.	1	1	1	1
CHURCH ST.	4	2		1
HIGH ST.				1
PAYNE ST.			1	1
SLM 2.85				1
TOTALS	16	6	2	19

**PAVEMENT**

**ITEM 251 – PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN**  
THIS ITEM OF WORK SHALL CONSIST OF THE REMOVAL OF THE EXISTING PAVEMENT OR PAVED BERM WHICH MAY BE ASPHALT, BRICK, CONCRETE, OR A COMBINATION OF EACH, IN AREAS OF EXISTING PAVEMENT FAILURE. PAVEMENT REPAIR SHALL BE PERFORMED AFTER PAVEMENT PLANING AND BEFORE PLACEMENT OF THE OVERLAY COURSE(S). THE DEPTH OF REMOVAL SHALL BE AS DIRECTED BY THE ENGINEER WITH A MAXIMUM DEPTH OF 6”. THE MINIMUM WIDTH OF REPAIR SHALL BE 4FT. REPLACEMENT MATERIAL SHALL BE ITEM 301 ASPHALT CONCRETE BASE (449) AND SHALL BE PLACED AND COMPACTED TO FINISH FLUSH WITH THE ADJACENT PAVEMENT SURFACE.

FOR BID AND ESTIMATING PURPOSES, APPROXIMATELY 70% OF THE REPAIRS ARE TO BE CONSIDERED LONGITUDINAL REPAIRS AND 30% ARE TO BE CONSIDERED TRANSVERSE REPAIRS UNLESS OTHERWISE STATED. LONGITUDINAL IS DEFINED AS ANY REPAIR THAT HAS A GREATER MEASUREMENT PARALLEL TO THE CENTERLINE THAN THE MEASUREMENT PERPENDICULAR TO THE CENTERLINE. TRANSVERSE IS DEFINED AS ANY REPAIR THAT HAS A GREATER MEASUREMENT PERPENDICULAR TO THE CENTERLINE THAN THE MEASUREMENT PARALLEL TO THE CENTERLINE.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT REPAIR. FOR PAYMENT PURPOSES, ITEM 251 PARTIAL DEPTH PAVEMENT REPAIR (ASPHALT CONCRETE BASE), AS PER PLAN IS TO BE A MAXIMUM OF 6” DEEP. THE FOLLOWING ITEMS AND QUANTITIES ARE PROVIDED IN THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER:

ITEM 251 - PARTIAL DEPTH PAVEMENT REPAIR (251)			
LOCATION	PLAN SPLIT	(TRANSVERSE)	(LONGITUDINAL)
		CY	CY
SR 226	01/STR (0.905 to 7.751)	365	852
	02/S5K (7.751 to 9.306)	83	194
	03/STR (1.814 to 2.972)	62	145
GRAND TOTAL		510	1191

**ITEM 253 – PAVEMENT REPAIR, AS PER PLAN**  
THIS ITEM OF WORK SHALL CONSIST OF THE REMOVAL OF THE EXISTING PAVEMENT OR PAVED BERM WHICH MAY BE ASPHALT, BRICK, CONCRETE, OR A COMBINATION OF EACH, IN AREAS OF EXISTING PAVEMENT FAILURE. PAVEMENT REPAIR SHALL BE PERFORMED AFTER PAVEMENT PLANING AND BEFORE PLACEMENT OF THE OVERLAY COURSE(S). THE DEPTH OF REMOVAL SHALL BE SUFFICIENT TO REMOVE ALL DETERIORATED PAVEMENT WITH A MINIMUM DEPTH OF 6” AND A MAXIMUM DEPTH OF 12” AND A MINIMUM WIDTH OF 4FT. FOR FULL DEPTH REPAIRS WHERE CONCRETE IS UNDERLYING ASPHALT BUT CONCRETE IS NOT BEING REPLACED AS PART OF THE REPAIR, REMOVE ALL ASPHALT TO THE TOP OF CONCRETE THEN COMPLETE FLEXIBLE REPAIR ON TOP OF EXISTING CONCRETE. REPLACEMENT MATERIAL SHALL BE ITEM 301 ASPHALT CONCRETE BASE (449) AND SHALL BE PLACED AND COMPACTED TO FINISH FLUSH WITH THE ADJACENT PAVEMENT SURFACE. THE MAXIMUM PAVEMENT LIFT THICKNESS IS 6”.

FOR BID AND ESTIMATING PURPOSES, APPROXIMATELY 70% OF THE REPAIRS ARE TO BE CONSIDERED LONGITUDINAL REPAIRS AND 30% ARE TO BE CONSIDERED TRANSVERSE REPAIRS UNLESS OTHERWISE STATED. LONGITUDINAL IS DEFINED AS ANY REPAIR THAT HAS A GREATER MEASUREMENT PARALLEL TO THE CENTERLINE THAN THE MEASUREMENT PERPENDICULAR TO THE CENTERLINE. TRANSVERSE IS DEFINED AS ANY REPAIR THAT HAS A GREATER MEASUREMENT PERPENDICULAR TO THE CENTERLINE THAN THE MEASUREMENT PARALLEL TO THE CENTERLINE.

THE REQUIREMENTS OF SCD MT-101.90 (DROP-OFFS IN WORK ZONES) APPLY. UNLESS SPECIFIED AND ACCOUNTED FOR OTHERWISE IN THESE PLANS, FILL REPAIR AREAS WITH EITHER THE FINAL MATERIAL OR A TEMPORARY MATERIAL AT THE DISCRETION AND APPROVAL OF THE ENGINEER WHEN WORK IS NOT ACTIVE IN THE AREA OF THE REPAIR.

PAYMENT SHALL INCLUDE ALL LABOR, EQUIPMENT, AND MATERIALS NECESSARY TO COMPLETE THE PAVEMENT REPAIR. FOR PAYMENT PURPOSES, ITEM 253 PAVEMENT REPAIR, AS PER PLAN IS TO BE GREATER THAN 6” DEEP. THE FOLLOWING ITEMS AND QUANTITIES ARE PROVIDED IN THE GENERAL SUMMARY TO BE USED AS DIRECTED BY THE ENGINEER:

ITEM 253 - PAVEMENT REPAIR (FULL DEPTH FLEXIBLE)			
LOCATION	PLAN SPLIT	(TRANSVERSE)	(LONGITUDINAL)
		CY	CY
SR 226	01/STR (0.905 to 7.751)	37	86
	02/S5K (7.751 to 9.306)	8	19
	03/STR (1.814 to 2.972)	6	14
GRAND TOTAL		51	119



TRAFFIC CONTROL

STOP BAR PLACEMENT

IN ORDER TO COMPLY WITH THE REQUIREMENTS OF THE TRAFFIC ENGINEERING MANUAL AND THE O MUTCD, AT NORMAL STOP CONTROLLED INTERSECTIONS WITHOUT CROSSWALK, PLACE THE LEADING EDGE OF THE STOP BAR (CLOSEST TO THE CENTER OF THE INTERSECTION) IN ACCORDANCE WITH THE BELOW TABLE UNLESS SPECIFIED OTHERWISE IN THESE PLANS:

<u>SHOULDER WIDTH OF INTERSECTED ROADWAY</u>	<u>PLACE THE LEADING EDGE OF STOP BAR ON INTERSECTING/APPROACH ROADWAY</u>
0 FEET < SHOULDER WIDTH ≤ 2 FEET	4 FEET BACK FROM EDGE OF PAVED SHOULDER OF INTERSECTED ROADWAY
2 FEET < SHOULDER WIDTH ≤ 4 FEET	2 FEET BACK FROM EDGE OF PAVED SHOULDER OF INTERSECTED ROADWAY
SHOULDER WIDTH > 4 FEET	IN LINE WITH EDGE OF PAVED SHOULDER OF INTERSECTED ROADWAY

PAVEMENT MARKING LOG

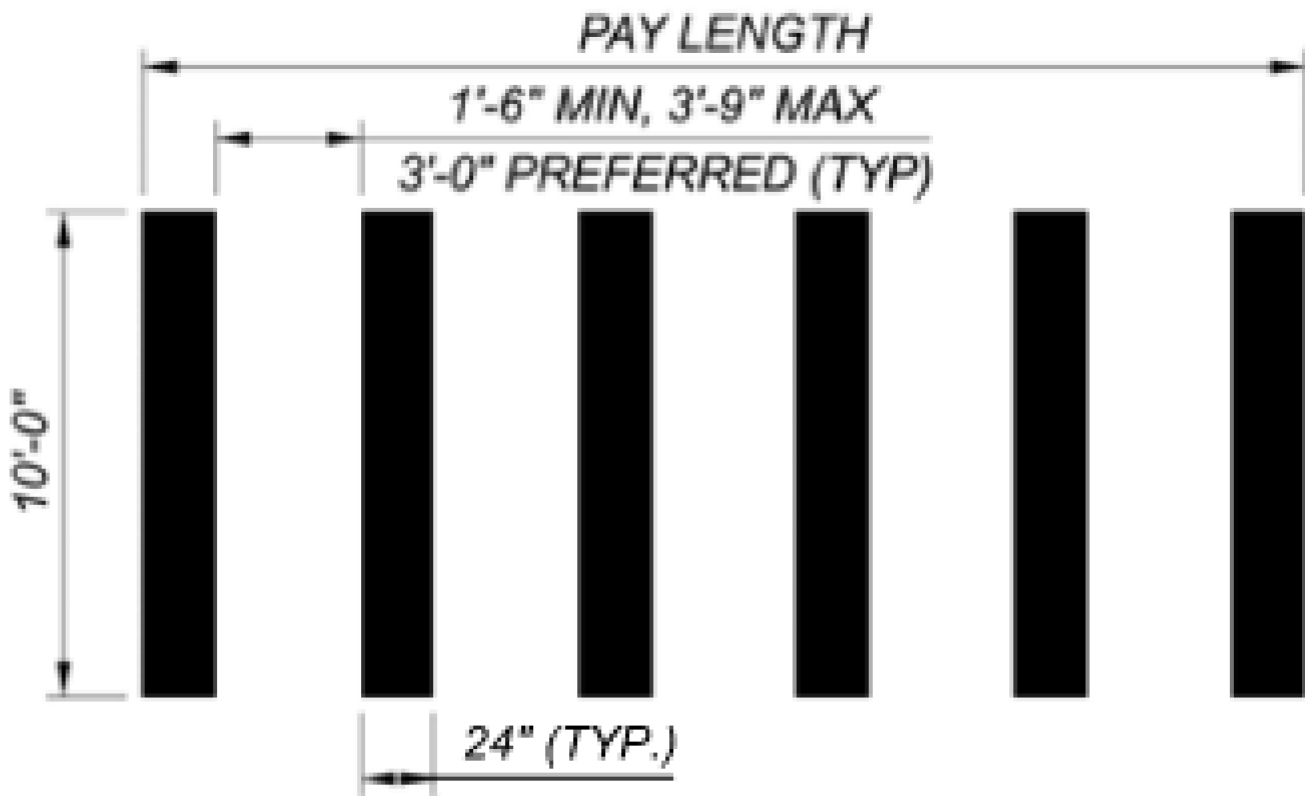
PRIOR TO REMOVING, GRINDING, OR OTHERWISE DESTROYING ANY EXISTING PAVEMENT MARKINGS, IT IS THE RESPONSIBILITY OF THE CONTRACTOR TO CREATE AN EXISTING PAVEMENT MARKING LOG IN ORDER TO PLACE THE PROPOSED PAVEMENT MARKINGS IN THE SAME LOCATION AS THEIR EXISTING CONFIGURATION. SUBMIT THE EXISTING PAVEMENT MARKING LOG TO THE ENGINEER AND OBTAIN HIS OR HER APPROVAL PRIOR TO REMOVING, GRINDING, OR OTHERWISE DESTROYING THE EXISTING PAVEMENT MARKINGS. [XXX – ADDITIONAL NOTE FOR CITY APPROVAL IF APPLICABLE. EXAMPLE: ADDITIONALLY, SUBMIT THE EXISTING PAVEMENT MARKINGS LOG TO THE CITY OF GALION AND OBTAIN THEIR APPROVAL PRIOR TO REMOVING, GRINDING, OR OTHERWISE DESTROYING THE EXISTING PAVEMENT MARKINGS.]

ALL LABOR, MATERIAL, EQUIPMENT, AND INCIDENTALS NEEDED TO COMPLETE THIS WORK SHOULD BE INCLUDED IN THE CONTRACT LUMP SUM BID PRICE FOR ITEM 614 – MAINTAINING TRAFFIC.

ITEM 644 – PAVEMENT MARKING, MISC.: CROSSWALK LINE, 24”, AS PER PLAN

INSTALL CROSSWALK LINE, AS PER PLAN ACCORDING TO O MUTCD 3B.18, SPECIFICALLY 3B.18.05, AND 3B.18.15. ORIENT THE MARKINGS PARALLEL WITH THE CENTERLINE OF THE ROADWAY. PLACE THE MARKINGS IN ORDER TO AVOID OTHER PAVEMENT MARKINGS AND WHEEL PATHS, WHERE PRACTICAL. VARY THE SPACING AS SHOWN BELOW AS NEEDED TO MEET THESE REQUIREMENTS, MAINTAINING IDENTICAL SPACING BETWEEN INDIVIDUAL BARS FOR EACH MARKING. ENSURE THE FULL WIDTH OF THE ROADWAY, LABELED AS THE PAY LENGTH IN THE BELOW DETAIL, IS UTILIZED IN THE PLACEMENT OF THE MARKINGS. ADD OR REMOVE THE NUMBER OF BARS, UTILIZING THE BELOW WIDTH AND SPACING REQUIREMENTS, AS NEEDED TO PROVIDE FULL ROADWAY COVERAGE.

INSTALL CROSSWALK LINE, AS PER PLAN AT ANY MID-BLOCK, NON-STOP CONTROLLED CROSSING WHERE THERE IS NOT OTHER GENERAL STOP CONTROL MEASURES FOR THE TRAFFIC ON THE CROSSED TRAVELLED WAY. SEE PLAN SHEET NO. 43 FOR LOCATION OF CROSSWALK LINE, 24”, AS PER PLAN. ALL OTHER CROSSWALKS ARE LOCATED WITHIN THE VILLAGE SHREVE AND ARE PAID UNDER ITEM 614 CROSSLINE , 24”.



LAYOUT OF WORK

PRIOR TO REMOVING OR ERECTING SIGNS OR SIGN SUPPORTS, THE CONTRACTOR SHALL FIELD LAYOUT AND IDENTIFY, BY TYPE OF WORK, SIGNS AND SIGN SUPPORTS TO BE ERECTED OR REMOVED. THIS LAYOUT MAY BE ACCOMPLISHED BY STAKING (USING WHITE STAKES OR WHITE FLAGS) OR BY PLACING CLEARLY DISCERNABLE WHITE PAINTED MARKINGS ON THE EDGE OF PAVEMENT. IN NO CASE SHALL THE CONTRACTOR PLACE ANY PERMANENT MARKINGS ON ANY EXISTING SIGN OR SIGN SUPPORT.

ITEMS OF WORK

ITEM 900 - SPECIAL - RAILROAD COORDINATION

THIS PROJECT INCLUDES WORK WITHIN THE RIGHT OF WAY OF ONE OR MORE RAILWAYS. THE CONTRACTOR SHALL COMPLETE AND SECURE ALL INSURANCE AND DOCUMENTATION REQUIREMENTS WITH EACH AFFECTED RAILROAD ON THE PROJECT. SEE THE PROJECT PROPOSAL FOR ADDITIONAL INFORMATION AND REQUIREMENTS AS PART OF THE RAILROAD AGREEMENT AND/OR RAILROAD CLAUSES INCLUDED WITH THE PROJECT.

RAILROAD FLAGGING WILL BE REQUIRED BY THE RAILWAY(S) WHEN ANY PROJECT OPERATIONS ARE BEING COMPLETED WITHIN RAILROAD RIGHTS OF WAY. IN ORDER TO LIMIT THE RISK OF DELAYS IN THE PROJECT SCHEDULE, THE CONTRACTOR SHALL MAKE EFFORTS TO SCHEDULE A RAILROAD FLAGGER WITH THE AFFECTED RAILROAD(S) AS EARLY AS POSSIBLE, PREFERRABLY WITHIN TWO WEEKS OF THE AWARD OF THE CONTRACT. THE PROJECT PROPOSAL AND/OR RAILROAD CLAUSES LISTS THE RAILWAY-REQUESTED MINIMUM NOTIFICATON REQUIREMENTS FOR EACH AFFECTED RAILROAD. IF IT IS DETERMINED BY THE ENGINEER THAT THE CONTRACTOR DID NOT NOTIFY THE RAILROAD WITHIN THE REQUIRED TIMEFRAME TO SECURE A RAILROAD FLAGGER, AND THUS INCURS A DELAY IN PROJECT OPERATIONS, ANY DELAY CLAIMS MADE BY THE CONTRACTOR AS A RESULT OF DELAYED COORDINATION WITH SUCH RAILROAD(S) WILL BE DENIED.

IT MAY BE NECESSARY TO COMPLETE WORK WITHIN RAILROAD RIGHTS OF WAY UNDER A SEPARATE PHASE FROM THE REMAINDER OF THE PROJECT PENDING THE AVAILABILITY OF RAILROAD FLAGGING.

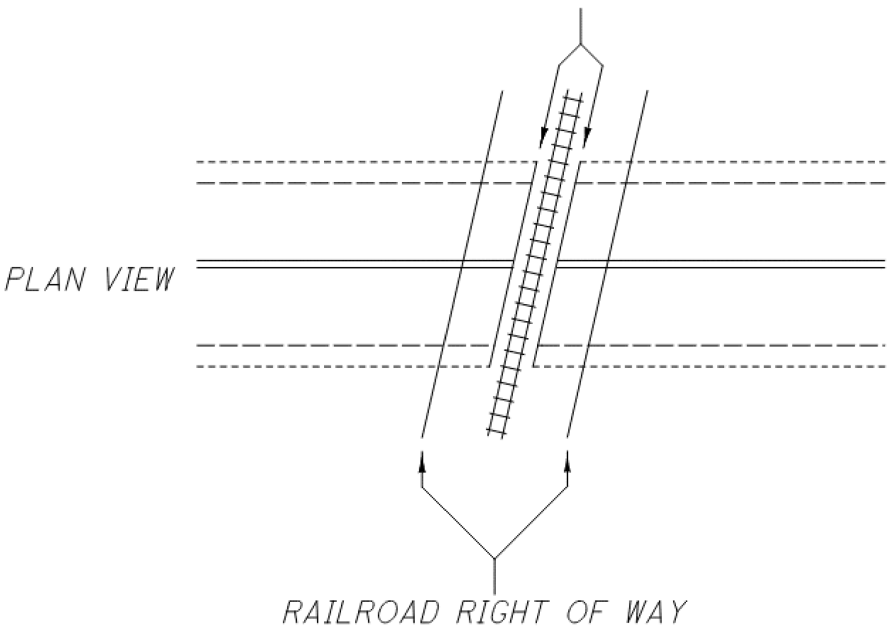
ALL WORK, INCLUDING LABOR, MATERIALS, EQUIPMENT, MOBILIZATION, AND INCIDENTALS, NEEDED TO COMPLETE THIS COORDINATION WITHIN THE RAILROAD RIGHT OF WAY SHALL BE INCLUDED IN THE CONTRACT LUMP SUM BID PRICE FOR ITEM 900 - SPECIAL - RAILROAD COORDINATION, UNLESS SEPARATELY ITEMIZED OR DETAILED IN THIS PLAN AND PROPOSAL.

PAVING AT RAILROAD CROSSING

WORK THE CROWN OUT OF THE PROPOSED PAVEMENT ON EACH SIDE OF THE RAILROAD CROSSING, BEGINNING 50 FEET FROM THE NEAREST RAIL, BY RAISING THE EDGES OF THE NEW PAVEMENT TO MEET THE PLATFORM ELEVATION.


DETAIL – PAVING AT RAILROAD CROSSING

BUTT JOINT/BEGIN AND END RESURFACING



- NOTE:
- 1.) DO NOT DISTURB RAILROAD GATES
  - 2.) RE-INSTALL PAVEMENT MARKINGS
  - 3.) RAILROAD MAY DIRECT ENGINEER ON THE LOCATION OF BUTT JOINTS. OTHERWISE OMIT AND RESUME RESURFACING AT AT THE EDGE OF THE EXISTING CROSSING SURFACE ON BOTH SIDES OF THE TRACK.

DESIGN AGENCY  
DISTRICT 3

  
ENGINEERING  
TEAM TWO

DESIGNER  
JEC

REVIEWER  
KCK 8-15-25

PROJECT ID  
121723

SHEET	TOTAL
P.10	69



WAY-226-0.905

SHEET NUMBER											PART.			ITEM	ITEM EXT	GRAND TOTAL	UNIT	DESCRIPTION	SEE SHEET NO.
5	6	7	8	11	12	15	16	17	34	56	01/STR	02/SSK	03/STR						
1											1			201	26520	1	EACH	ROADWAY	
									0.69		0.69			202	23000	0.69	SY	STUMP REMOVED, 48" DIAMETER	
									6,976		6,976			202	30000	6,976	SF	PAVEMENT REMOVED	
									838					202	32000	838	FT	WALK REMOVED	
									445		445			202	32500	445	FT	CURB REMOVED	
																		CURB AND GUTTER REMOVED	
								6,359.25			5,796.75	562.5		202	38000	6,359.25	FT	GUARDRAIL REMOVED	
								55			45	10		202	42000	55	EACH	ANCHOR ASSEMBLY REMOVED, TYPE A	
								3			3			202	42010	3	EACH	ANCHOR ASSEMBLY REMOVED, TYPE E	
								5			5			202	42040	5	EACH	ANCHOR ASSEMBLY REMOVED, TYPE T	
								12			9	3		202	47000	12	EACH	BRIDGE TERMINAL ASSEMBLY REMOVED	
								1,733			1,547	186		203	20001	1,733	CY	EMBANKMENT, AS PER PLAN(GUARDRAIL)	
								1.5			1.5			209	15001	1.5	STA	RESHAPING UNDER GUARDRAIL, AS PER PLAN	
						0.05					0.05			209	60500	0.05	MILE	LINEAR GRADING	
						12.99					10.05	2.94		209	72051	12.99	MILE	PREPARING SUBGRADE FOR SHOULDER PAVING, AS PER PLAN	
	63										58	5		209	80000	63	EACH	GRADING MAILBOX APPROACHES	
								1,486.25			992.25	494		606	13000	1,486.25	FT	GUARDRAIL, TYPE 5	
								4,237.5			4,237.5			606	15050	4,237.5	FT	GUARDRAIL, TYPE MGS	
								75			75			606	16000	75	FT	GUARDRAIL REBUILT	
								1,084.5			697	387.5		606	17000	1,084.5	FT	RAISING TYPE 5 GUARDRAIL	
								1				1		606	17700	1	EACH	REPLACE EXISTING GUARDRAIL BLOCKOUT	
								7			7			606	17900	7	EACH	GUARDRAIL POST	
								1			1			606	26050	1	EACH	ANCHOR ASSEMBLY, MGS TYPE B	
								31			21	10		606	26100	31	EACH	ANCHOR ASSEMBLY, TYPE E(MASH 2016)	
								25			25			606	26150	25	EACH	ANCHOR ASSEMBLY, MGS TYPE E(MASH 2016)	
								1			1			606	26500	1	EACH	ANCHOR ASSEMBLY, TYPE T	
								5			5			606	26550	5	EACH	ANCHOR ASSEMBLY, MGS TYPE T	
								3			1	2		606	27900	3	EACH	ANCHOR ASSEMBLY REBUILT, TYPE T	
								11			8	3		606	35140	11	EACH	BRIDGE TERMINAL ASSEMBLY, TYPE 4	
								1			1			606	35141	1	EACH	BRIDGE TERMINAL ASSEMBLY, TYPE 4, AS PER PLAN	
									2,921		2,921			608	10000	2,921	SF	4" CONCRETE WALK	
									4,938		4,938			608	52000	4,938	SF	CURB RAMP	
									145		145			609	12000	145	FT	COMBINATION CURB AND GUTTER, TYPE 2	
									512		512			609	26000	512	FT	CURB, TYPE 6	
									26		26			609	28000	26	FT	CURB, TYPE 7	
34											25	9		623	39501	34	EACH	MONUMENT ASSEMBLY ADJUSTED TO GRADE, AS PER PLAN	
11											9	2		623	39601	11	EACH	MONUMENT ASSEMBLY RECONSTRUCTED TO GRADE, AS PER PLAN	
	LS										LS			623	51000	LS		POST CONSTRUCTION SURVEY MONUMENT VERIFICATION AND REPORT	
									10		10			659	00301	10	CY	EROSION CONTROL	
											400	100		832	30000	500	EACH	EROSION CONTROL	