

STATE OF OHIO DEPARTMENT OF TRANSPORTATION

WOO-582-5.43/10.24

WOOD COUNTY MIDDLETON, WEBSTER & TROY TOWNSHIPS VILLAGE OF LUCKEY

FEDERAL PROJECT NUMBER

E140761

RAILROAD INVOLVEMENT

CSX RAILROAD

PROJECT DESCRIPTION

RESURFACE SR 582 IN WOOD COUNTY FROM FROM SR 199 TO PEMBERVILLE RD.; PERFORM NECESSARY RELATED WORK.

Description Revised

EARTH DISTURBED AREAS

PROJECT EARTH DISTURBED AREA: N/A ACRES
ESTIMATED CONTRACTOR EARTH DISTURBED AREA: N/A ACRES
NOTICE OF INTENT EARTH DISTURBED AREA: N/A ACRES

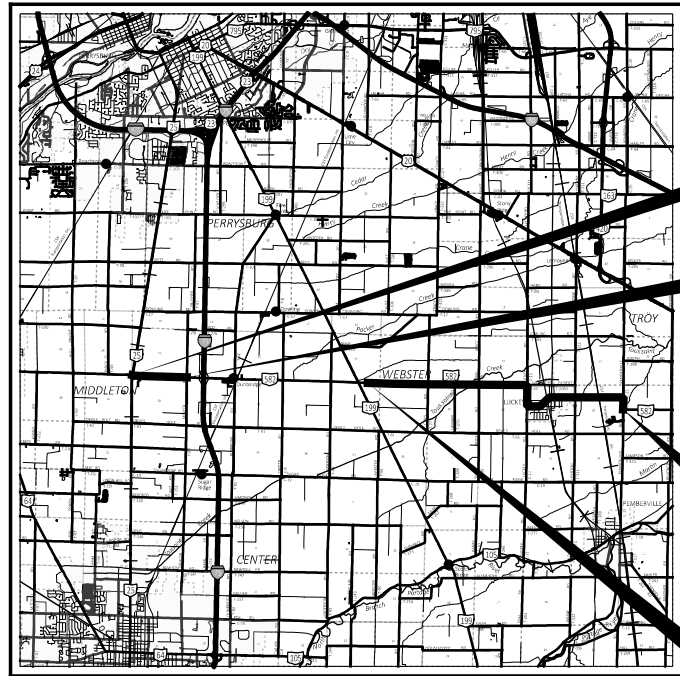
2023 SPECIFICATIONS

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO, DEPARTMENT OF TRANSPORTATION, INCLUDING SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PLANS, CHANGES LISTED IN THE PROPOSAL, AND THE SUPPLEMENTAL SPECIFICATION 800 VERSION INDICATED ON THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH ON THE PLANS AND ESTIMATES.

Pat McColley, PE
Pat McColley, P.E., S.I.
District 02 Deputy Director

Jack Marchbanks, PhD
Jack Marchbanks, PhD
Director, Department of Transportation



LOCATION MAP

LATITUDE: 41°27'11" N LONGITUDE: 83°29'00" W



PORTION TO BE IMPROVED	—————
INTERSTATE HIGHWAY	=====
FEDERAL ROUTES	=====
STATE ROUTES	=====
COUNTY & TOWNSHIP ROADS	=====
OTHER ROADS	—————

DESIGN DESIGNATION

CURRENT ADT (2025)	1800
DESIGN YEAR ADT (2037)	1900
DESIGN HOURLY VOLUME (2027)	200
DIRECTIONAL DISTRIBUTION	62%
TRUCKS (24 HOUR B&C)	5%
DESIGN SPEED	VARIES
LEGAL SPEED	VARIES
DESIGN FUNCTIONAL CLASSIFICATION:	RURAL MAJOR COLLECTOR

WOO-582 (10.24-16.35)
1800
1900
200
62%
5%
VARIES
VARIES
RURAL MAJOR COLLECTOR

NHS PROJECT ----- NO

DESIGN EXCEPTIONS

NONE REQUIRED

ADA DESIGN WAIVERS

NONE REQUIRED

UNDERGROUND UTILITIES
Contact Two Working Days
Before You Dig

OHIO811.org
Before You Dig

OHIO811, 8-1-1, or 1-800-362-2764
(Non members must be called directly)

PLAN PREPARED BY:
OHIO DEPARTMENT OF
TRANSPORTATION
DISTRICT 2

INDEX OF SHEETS:

- TITLE SHEET
- TYPICAL SECTIONS
- GENERAL NOTES
- MAINTENANCE OF TRAFFIC NOTES
- GENERAL SUMMARY
- PAVEMENT CALCULATIONS
- MISC. SUBSUMMARIES
- PLAN SHEETS

- 1
- 2-3
- 4
- 5-6
- 7-8
- 9-10
- 11-12
- 13-26

Sht. No. Revised

Deleted

STANDARD CONSTRUCTION DRAWINGS							SUPPLEMENTAL SPECIFICATIONS		SPECIAL PROVISIONS	
BP-3.1	1/21/22	MT-97.10	4/19/19	TC-61.30	7/19/19		800	1/19/2024		
BP-3.2	1/18/19	MT-97.12	1/20/17	TC-64.10	7/21/23		832	7/21/23		
BP-5.1	7/15/22	MT-99.20	4/19/19	TC-65.10	1/17/14		872	1/21/22		
BP-7.1	7/21/23	MT-101.60	4/21/23	TC-65.11	7/15/22		874	4/17/20		
		MT-101.90	7/17/20	TC-71.10	4/21/23		875	1/18/19		
DM-4.3	1/15/16	MT-105.10	1/17/20	TC-74.10	7/21/23					
DM-4.4	1/15/16			TC-82.10	7/19/19					
MGS-1.1	7/16/21									
MGS-2.1	1/19/18									
MGS-2.3	1/20/23									
MGS-5.2	7/15/16									
RM-1.1	1/20/23									

ENGINEER'S SEAL

ROADWAY



Revised

TITLE SHEET

DESIGN AGENCY



DESIGNER ALF

REVIEWER JMF

PROJECT NO. 95800

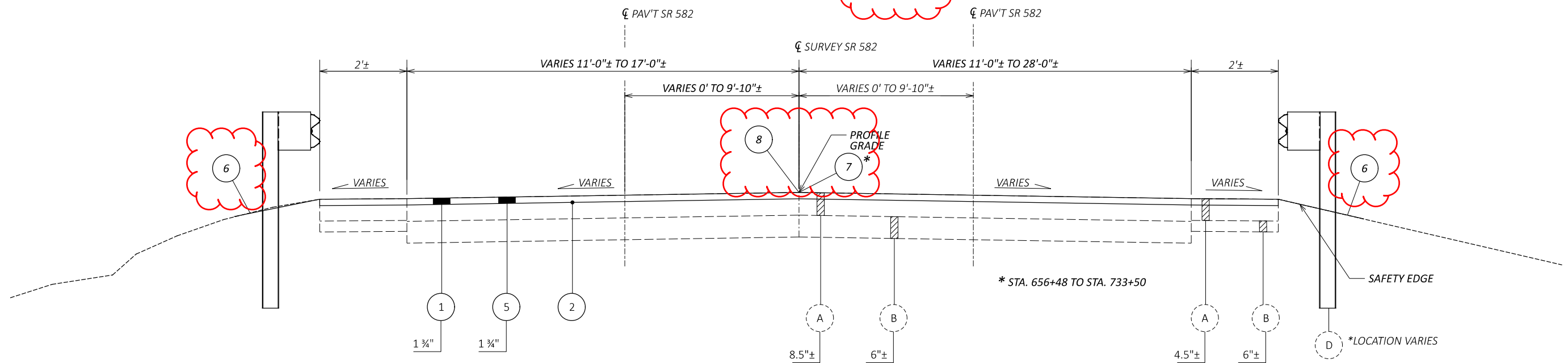
SHEET 1

TOTAL 26

WOO-582-5.43/10.05

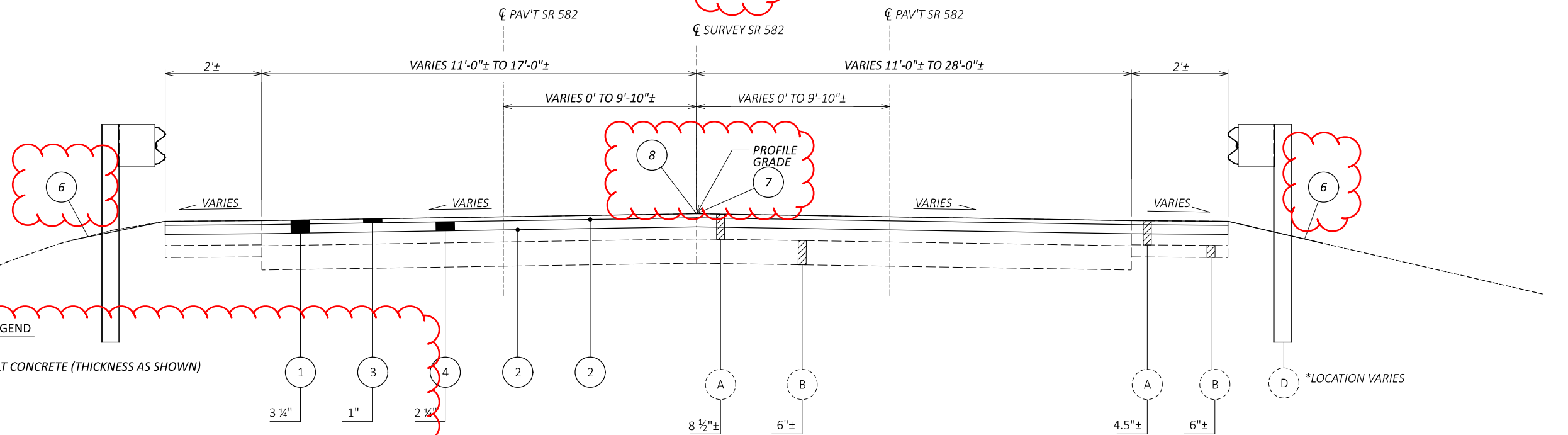
MODEL: Sheet PAPER: 17x11 (in.) DATE: 8/14/2024 TIME: 1:03:02 PM USER: afinal pwc:\hobol-pw-bentley.com\shahid-pw-02\Documents\01 Active Projects\District 02\Wood\95800\400-Engineering\Roadway\Sheets\95800_G1001.dgn

TYPICAL SECTION - A



TYPICAL SECTION APPLIES:
 STA. 541+59.20 TO STA. 654+90.00 = 11330.80 FT.

TYPICAL SECTION - B



TYPICAL SECTION APPLIES:
 STA. 656+48.00 TO STA. 740+74.00 = 8426.00 FT.

PROPOSED PAVEMENT LEGEND

- 1 ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE (THICKNESS AS SHOWN)
- 2 ITEM 407 - NON-TRACKING TACK COAT
- 3 ITEM 424 - 1" FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, (448), AS PER PLAN
- 4 ITEM 441 - ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (446) (THICKNESS AS SHOWN)
- 5 ITEM 442 - ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A (446) (THICKNESS AS SHOWN)
- 6 ITEM 617 - COMPACTED AGGREGATE & ITEM 209 - LINEAR GRADING
- 7 ITEM 618 - RUMBLE STRIPES, EDGE LINE (ASPHALT CONCRETE) & RUMBLE STRIPES, CENTERLINE (ASPHALT CONCRETE)
- 8 ITEM 874 - LONGITUDINAL JOINT PREPARATION
- 9 ITEM 875 - LONGITUDINAL JOINT ADHESIVE

EXISTING PAVEMENT LEGEND

- A ASPHALT CONCRETE
- B ASPHALT CONCRETE BASE
- C AGGREGATE BASE
- D GUARDRAIL

Revised

WOO-582-5.43/10.05

MODEL SHEET PAPER SIZE: 17x11 (in.) DATE: 8/14/2024 TIME: 1:03:10 PM USER: ahintel
 p:\ohiodot-pw-bentley.com\ohiodot-pw-02\Documents\01.Active Projects\District 02\Wood-58200\400-Engineering\Roadway\Sheets\95800_GY002.dgn

TYPICAL SECTION
SR 582

DESIGN AGENCY



DESIGNER

ALF

REVIEWER

JMF

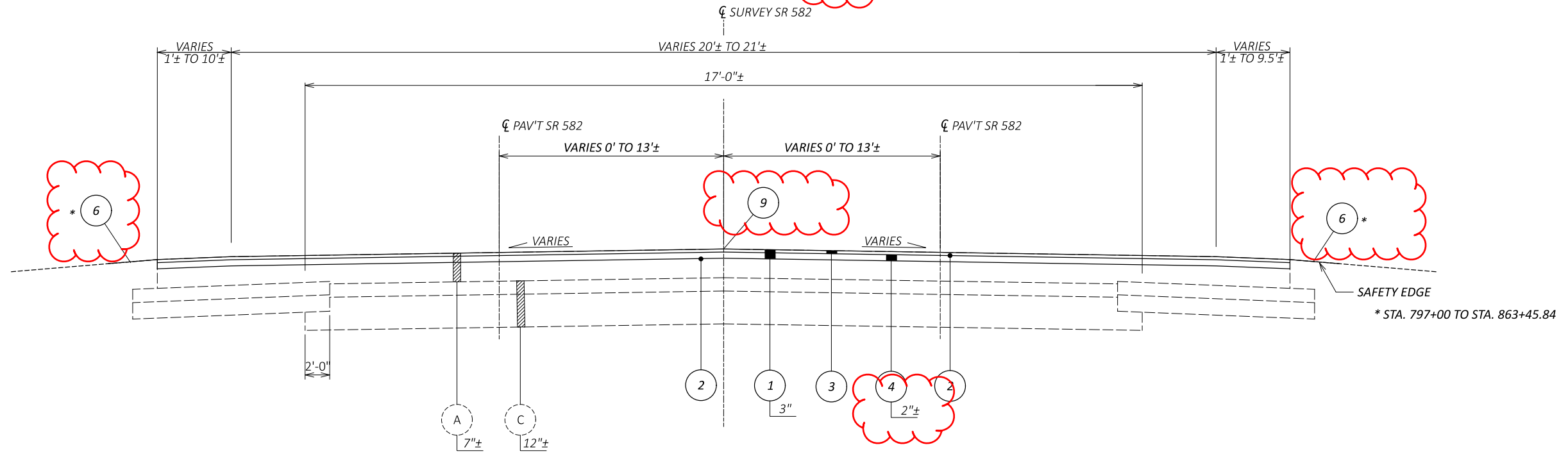
PROJECT ID

95800

SHEET TOTAL

2 26

TYPICAL SECTION - C



TYPICAL SECTION APPLIES:

- STA. 740+74.00 TO STA. 797+00.00 = 5626.00 FT.
- * STA. 797+00.00 TO STA. 823+22.00 = 2622.00 FT.
- * STA. 823+42.00 TO STA. 863+45.84 = 4003.84 FT.

TYPICAL SECTION
SR 582

DESIGN AGENCY



DESIGNER

ALF

REVIEWER

JMF

PROJECT ID

95800

SHEET TOTAL

3 26

UTILITIES

LISTED BELOW ARE ALL UTILITIES LOCATED WITHIN THE PROJECT CONSTRUCTION LIMITS TOGETHER WITH THEIR RESPECTIVE OWNERS:

AT&T BUCKEYE CABLEVISION
130 N. ERIE ST. 2700 OREGON RD.
TOLEDO, OH 43624 NORTHWOOD, OH 43519
419.245.7304 419.724.3713

CENTURYLINK COLUMBIA GAS OF OHIO, INC.
175 ASHLAND RD. 2901 E. MANHATTAN BLVD.
MANSFIELD, OH 44902 TOLEDO, OH 43611
419.755.7183 419.539.6066

FRONTIER ODOT-DISTRICT 2
300 W. GYSPY LN. RD. 317 E. POE RD.
BOWLING GREEN, OH 43402 BOWLING GREEN, OH 43402
419.354.9452 419.353-8131

SUBURBAN NATURAL GAS CO. TOLEDO EDISON
P.O. BOX 130 6099 ANGOLA RD.
CYGNET, OH 43413-0130 HOLLAND, OH 43528
419.655.2345 419.249.5218

NORTHWESTERN WATER & SEWER DIST.
P.O. BOX 348
BOWLING GREEN, OH 43402
419.354.9090

WORK LIMITS

THE WORK LIMITS SHOWN ON THESE PLANS ARE FOR PHYSICAL CONSTRUCTION ONLY. PROVIDE THE INSTALLATION AND OPERATION OF ALL WORK ZONE TRAFFIC CONTROL AND WORK ZONE TRAFFIC CONTROL DEVICES REQUIRED BY THESE PLANS WHETHER INSIDE OR OUTSIDE THESE WORK LIMITS.

PROFILE AND ALIGNMENT

PLACE THE PROPOSED PAVEMENT TO FOLLOW THE ALIGNMENT AND PROFILE OF THE EXISTING PAVEMENT.

PLANED SURFACES

NO PLANED SURFACES SHALL BE OPEN TO THE PUBLIC FOR MORE THAN 5 DAYS. IF THE PLANED SURFACE IS OPEN FOR MORE THAN 5 DAYS, THEN IT IS THE CONTRACTOR'S RESPONSIBILITY TO REPAIR THE PAVEMENT FAILURES THAT OCCURED AFTER THE 5 DAYS.

ITEM 209 - PREPARING SUBGRADE FOR SHOULDER PAVING

THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR ITEM 209 FOR CONSTRUCTION OF THE SAFETY EDGE:

ITEM 209 - PREPARING SUBGRADE FOR SHDLR PAVING						
LOCATION	ROUTE	PLAN SPLIT CODE	STA TO STA	SIDE	QUANTITY (MILES)	
WOO	582	01/STR/05	656+48	740+74	RT/LT	3.2
WOO	582	01/STR/05	740+74	863+70.84	RT/LT	2.3
TOTAL CARRIED TO GENERAL SUMMARY						5.5

ITEMS ADJUSTED TO GRADE

THE FOLLOWING ESTIMATED QUANTITIES ARE TO BE USED FOR ADJUSTMENTS REQUIRED FOR THE FOLLOWING ITEMS, AS DIRECTED BY THE ENGINEER.

ITEM 611 - CATCH BASIN ADJUSTED TO GRADE			
LOCATION	ROUTE	PLAN SPLIT CODE	EACH
WOO	582	01/STR/05	4
TOTAL CARRIED TO GENERAL SUMMARY			4

ITEM 638 - VALVE BOX ADJUSTED TO GRADE			
LOCATION	ROUTE	PLAN SPLIT CODE	EACH
WOO	582	01/STR/05	1
TOTAL CARRIED TO GENERAL SUMMARY			1

ITEM 623 - MONUMENT ASSEMBLY RECONSTRUCTED TO GRADE			
LOCATION	ROUTE	PLAN SPLIT CODE	EACH
WOO	582	01/STR/05	2
TOTAL CARRIED TO GENERAL SUMMARY			2

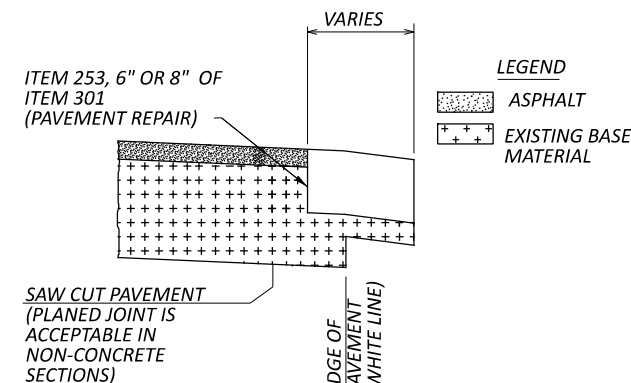
ITEM 611-MANHOLE ADJUSTED TO GRADE			
LOCATION	ROUTE	PLAN SPLIT CODE	EACH
WOO	582	01/STR/05	9
TOTAL CARRIED TO GENERAL SUMMARY			9

ITEM 253 - PAVEMENT REPAIR

ALL EXISTING PAVEMENT AREAS WHICH WILL BE IN CONTACT WITH THE PAVEMENT REPAIR SHALL BE COATED WITH PG GRADE LIQUID ASPHALT (SIDES AND BOTTOM) AT AN APPLICATION RATE OF 0.25 GAL. PER SQ. YD.

REPAIRS SHOULD BE DONE BEFORE MILLING BETWEEN SR 199 (SLM 10.50) TO TAUSSAINT CREEK BRIDGE (SLM 12.90).

THE FOLLOWING ESTIMATED QUANTITIES FOR SR 582 ARE TO BE USED FOR 8" OR 6" PAVEMENT REPAIR AS DIRECTED BY THE ENGINEER AND BASED ON THE PERCENTAGE SHOWN.



ITEM 253, FULL DEPTH PAVEMENT REPAIR 6" (CY)									
LOCATION	ROUTE	PLAN SPLIT CODE	STA TO STA	SIDE	LENGTH (FT)	WIDTH	AREA (SQ)	% REPAIR AREA	QUANTITY (CY)
WOO	582	01/STR/05	282+42.71	342+38.32	RT/LT	5995.61	23196		400
WOO	582	01/STR/05	656+48	740+74	RT/LT	8426.00	23.2	15%	543
WOO	582	01/STR/05	740+74	863+45.84	RT/LT	12271.84	23.2	10%	527
TOTAL CARRIED TO GENERAL SUMMARY									1470

ITEM 253, FULL DEPTH PAVEMENT REPAIR 8" (CY)										
LOCATION	ROUTE	PLAN SPLIT CODE	STA TO STA	SIDE	LENGTH (FT)	WIDTH	AREA (SQ)	% REPAIR AREA	QUANTITY (CY)	
WOO	582	01/STR/05	541+49.2	654+90	RT/LT	11340.80	47.5	59854	8%	1064
TOTAL CARRIED TO GENERAL SUMMARY									1064	

NOTE: THE ENGINEER SHALL FIELD VERIFY ALL LOCATIONS PRIOR TO THE BEGINNING OF WORK. ANY ADJUSTMENTS NECESSARY SHALL BE AS DIRECTED BY THE ENGINEER.

ITEM 424 - FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, (448), AS PER PLAN

PER CMS 424.04, 448 DENSITY APPLIES TO THIS PROJECT. DENSITY WILL BE TESTED ACCORDING TO SUPPLEMENT 1055 PER CMS 448.02. THE DENSITY DISINCENTIVE PORTION OF TABLE 448.04-3, WILL BE WAIVED PROVIDING THAT THE CONTRACTOR MAKES EVERY EFFORT TO OBTAIN DENSITY AND DOES NOT USE VIBRATORY ROLLERS.

PAVEMENT FOR SAFETY EDGE

THE FOLLOWING QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR CONSTRUCTION OF THE SAFETY EDGE:

ITEM 424 - FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B (448), AS PER PLAN							
LOC	ROUTE	PLAN SPLIT CODE	STA. FROM	STA. TO	AVG. DEPTH	SQ FT	SURFACE SAFETY EDGE (CY)
WOO	582	01/STR/05	656+40	740+74	0.0093	78.44	6
WOO	582	01/STR/05	740+74	863+70.84	0.0093	114.36	8
TOTAL CARRIED TO GENERAL SUMMARY							14

ITEM 442 - ASPHALT CONCRETE, MISC. : BUTT JOINTS AT INTERSECTIONS

THIS NOTE IS TO BE USED FOR THE INTERSECTION IN THE AREA WHERE ITEM 424 IS BEING PLACED.

FOR THE WORK AT THE INTERSECTIONS THE CONTRACTOR SHALL PLACE ONE OF FOLLOWING TREATMENTS:

MILL THE BUTT JOINT AREA OF THE INTERSECTION THE THICKNESS OF THE ASPHALT BEING PLACED, PLACE ITEM 407 NON TRACKING COAT ON THE MILLED SURFACE AND PLACE INTERMEDIATE COURSE AND 1" ITEM 424 FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B (449)

OR

MILL THE BUTT JOINT AREA OF THE INTERSECTION THE THICKNESS OF THE ASPHALT BEING PLACED, PLACE ITEM 407 NON TRACKING COAT ON THE MILLED SURFACE AND PLACE INTERMEDIATE COURSE AND 1 1/2" ITEM 442, ASPHALT CONCRETE SURFACE COURSE, 9.5MM, TYPE A, (449)

WHICH EVER METHOD THE CONTRACTOR CHOOSES ALL WORK SHALL BE PAID FOR UNDER ITEM 442 ASPHALT CONCRETE, MISC: BUTT JOINT INTERSECTION CY. FOR QUANTITY CALCULATIONS A THICKNESS OF 1 1/2" WAS USED.

INTERMEDIATE COURSE WILL BE PAID SEPARATELY.

SURVEYING PARAMETERS

PRIMARY PROJECT CONTROL MONUMENTS GOVERN ALL POSITIONING ON ODOT PROJECTS.

USE THE FOLLOWING PROJECT CONTROL, VERTICAL POSITIONING, AND HORIZONTAL POSITIONING PARAMETERS FOR ALL SURVEYING:

PROJECT CONTROL

POSITIONING METHOD: ODOT VRS
MONUMENT TYPE: TYPE B

VERTICAL POSITIONING

ORTHOMETRIC HEIGHT DATUM: NAVD88
GEOID: GEOID018

HORIZONTAL POSITIONING

REFERENCE FRAME: NAD83 (2011)
ELLIPSOID: GRS80
MAP PROJECTION: LAMBERT CONFORMAL CONIC
COORDINATE SYSTEM: OHIO STATE PLANE NORTH
COMBINED SCALE FACTOR: 1.000000 (GRID)
ORIGIN OF COORDINATE SYSTEM: 0,0,0

USE THE POSITIONING METHODS AND MONUMENT TYPE USED IN THE ORIGINAL SURVEY TO RESTORE ALL MONUMENTS RELATED TO PRIMARY PROJECT CONTROL THAT ARE DAMAGED OR DESTROYED BY CONSTRUCTION ACTIVITIES. RESTORE THE DAMAGED OR DESTROYED MONUMENTS IN ACCORDANCE WITH CMS 623.

UNITS ARE IN U.S. SURVEY FEET.

PAVING AT RAILROAD CROSSING

WORK THE CROWN OUT OF THE PROPOSED PAVEMENT ON EACH SIDE OF THE RAILROAD CROSSING, BEGINNING 50 FEET FROM THE NEAREST RAIL, BY RAISING THE EDGES OF THE NEW PAVEMENT TO MEET THE PLATFORM ELEVATION.

IF ANY ISSUE OR INCIDENT OCCURS WITHIN CSXT ROW, PLEASE CONTACT THE CSXT PUBLIC SAFETY COORDINATION CENTER AT 1-800-232-0144.

ASPHALT CONCRETE FOR DRIVEWAYS

THE FOLLOWING ESTIMATED QUANTITY FOR ASPHALT CONCRETE IS TO BE USED FOR ADJUSTING DRIVEWAYS AS DIRECTED BY THE ENGINEER:

ITEM 441 - ASPHALT CONCRETE SURFACE COURSE, TYPE 1 (449)

THE JOB WILL NOT BE CONSIDERED COMPLETE UNTIL ALL DRIVEWAYS HAVE BEEN TREATED AS DIRECTED BY THE ENGINEER.

SR 582 75 CU. YD.

TOTALS CARRIED TO GENERAL SUMMARY.

DESIGN AGENCY



DESIGNER

ALF

REVIEWER

JMF

PROJECT ID

95800

SHEET TOTAL

4 26

ITEM 614, MAINTAINING TRAFFIC

(WOO-582-(5.34-6.48) BEGINNING OF THE PROJECT TO MERCER RD

TWO-WAY TRAFFIC SHALL BE MAINTAINED AT ALL TIMES EXCEPT THAT ONE-WAY TRAFFIC WILL BE PERMITTED FOR MINIMUM PERIODS OF TIME CONSISTENT WITH THE REQUIREMENTS OF THE SPECIFICATIONS FOR PROTECTION OF COMPLETED ASPHALT CONCRETE COURSES.

(WOO-582- (10.24-16.35) SR 199 TO END OF THE PROJECT

A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES, EXCEPT FOR A PERIOD NOT TO EXCEED 45 CONSECUTIVE CALENDAR DAYS, WHEN THROUGH TRAFFIC MAY BE DETOURED AS DESCRIBED ON THIS SHEET DURING ACTIVE WORK HOURS ONLY. A DISINCENTIVE SHALL BE ASSESSED IN THE AMOUNT OF \$1500 PER DAY FOR EACH CALENDAR DAY THE ROADWAY REMAINS CLOSED TO TRAFFIC BEYOND THE SPECIFIED LIMIT. IN ORDER TO AVOID CONCURRENT AND/OR NEAR CONSECUTIVE CLOSURES OF SR 582 DUE TO ANOTHER ODOT PROJECT ALONG SR 582 AND AS PER THE WINDOW CONTRACT TABLE, THE ROAD CLOSURE SHALL NOT BEGIN UNTIL AFTER 4/1/2025.

DURING ACTIVE WORK HOURS WHEN THE ROAD MAY BE CLOSED, THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN 48x30 ROAD CLOSED SIGNS, SIGN SUPPORTS, BARRICADES AND LIGHTS, AS DETAILED IN TRAFFIC SCD MT-101.60 AT EACH END OF THE ACTIVE WORK AREA. THE ACTIVE WORK AREA SHALL BE LIMITED TO BETWEEN TWO ADJACENT CROSSROADS, AND LOCAL TRAFFIC SHALL BE MAINTAINED AT ALL TIMES. OUTSIDE OF WORKING HOURS, THE CONTRACTOR SHALL REMOVE THE CLOSURE BARRICADES AND ENSURE THAT THE TRAVELED WAY IS FREE OF EQUIPMENT AND ALL DROPOFFS ARE MAINTAINED PER MT-101.90.

POSTED DETOUR ROUTE:

SR-582 EB: SR-199 SOUTHBOUND TO SR-105 EASTBOUND TO US-23 NB
SR-582 WB: THE REVERSE OF THE ROUTE LISTED ABOVE.

THE DEPARTMENT SHALL FURNISH, ERECT, MAINTAIN, AND SUBSEQUENTLY REMOVE THE DETOUR SIGNS AND SUPPORTS.

Window Contract Table				
Description of Critical Work	Calendar Days to Complete	Disincentive \$ per Day	Work Window	
			Start	End
All pavement repairs, and paving operations up to the surface course	45	\$1500	4/1/2025	Project Completion Date
All work on project (including work listed above)	-	Per C&MS 108.07	4/1/2025	Project Completion Date

WORK NEAR THE RAILROAD:

- ROADWAY FLAGGERS MUST BE PRESENT ON EACH SIDE OF THE RAILROAD CROSSING, ANYTIME TRAFFIC IS DIRECTED INTO OPPOSING TRAFFIC LANES AT THE RAILROAD CROSSING.

- CHANNELIZATION/MOT/EROSION CONTROL DEVICES SHALL NOT BE PLACED A MINIMUM OF 15 FT. FROM CENTER LINE OF RAILROAD TRACKS.

NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. [AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK.]

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD/RAMP FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE. THE SIGNS MAY BE ERECTED ANYWHERE ON RAMPS AS LONG AS THEY ARE VISIBLE TO THE MOTORISTS USING THE RAMP. ON ENTRANCE RAMPS, THE SIGN SHALL BE ERECTED WELL IN ADVANCE OF THE MERGE AREA TO AVOID DISTRACTING MOTORISTS.

NOTICE OF CLOSURE SIGN TIME TABLE
ITEM DURATION SIGN DISPLAYED
OF CLOSURE TO PUBLIC

RAMP & >= 2 WEEKS 14 CALENDAR DAYS
PRIOR TO CLOSURE

ROAD > 12 HOURS 7 CALENDAR DAYS
& < 2 WEEKS PRIOR TO CLOSURE

CLOSURES <= 12 HOURS 2 BUSINESS DAYS
PRIOR TO CLOSURE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION. THIS IS TO BE A SPECIFIC OFFICE WITHIN THE DISTRICT RATHER THAN THE GENERAL SWITCHBOARD NUMBER.

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

DESIGNATED LOCAL DETOUR ROUTE

DEVILS HOLE RD. BETWEEN SR-199 AND US-23.

IN ADDITION TO THE OFFICIAL, SIGNED DETOUR ROUTE, A LOCAL ROUTE HAS BEEN DETERMINED TO BE THE SECONDARY, UNSIGNED DETOUR ROUTE OR "DESIGNATED LOCAL DETOUR ROUTE." THIS ROUTE IS LISTED ABOVE. DURING THE TIME THAT TRAFFIC IS DETOURED, THE CONTRACTOR SHALL MAINTAIN THIS ROUTE IN A CONDITION WHICH IS REASONABLY SMOOTH AND FREE FROM HOLES, RUTS, RIDGES, BUMPS, DUST AND STANDING WATER. ONCE THE DETOUR IS REMOVED AND TRAFFIC RETURNED TO ITS NORMAL PATTERN, THE DESIGNATED LOCAL DETOUR ROUTE SHALL BE RESTORED TO A CONDITION THAT IS EQUIVALENT TO THAT WHICH EXISTED PRIOR TO ITS USE FOR THIS PURPOSE. ALL SUCH WORK SHALL BE PERFORMED WHEN AND AS DETERMINED BY THE ENGINEER.

THE FOLLOWING ESTIMATED QUANTITIES ARE PROVIDED FOR USE AS DETERMINED BY THE ENGINEER TO MAINTAIN AND SUBSEQUENTLY RESTORE THE DESIGNATED LOCAL DETOUR ROUTE.

ITEM 202, 1 1/2" PAVEMENT REMOVED, ASPHALT	3520 SQ. YD.
ITEM 407, NON-TRACKING TACK COAT	299 GAL.
ITEM 442 ASPHALT CONCRETE SURFACE COURSE, 12.5MM, TYPE A (446)	147 CU. YD.
ITEM 617, COMPACTED AGGREGATE	49 CU. YD.
ITEM 642, CENTER LINE, TYPE 1	0.25 MILE

WORK ZONE MARKINGS AND SIGNS

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY FOR USE AT LOCATIONS IDENTIFIED BY THE ENGINEER FOR WORK ZONE PAVEMENT MARKINGS AND SIGNS PER THE REQUIREMENTS OF C&MS 614.04 AND 614.11.

ITEM 614 - WORK ZONE MARKING SIGN	34 EACH
ITEM 614 - WORK ZONE CENTER LINE, CLASS I, 642 PAINT	12.20 MILE
ITEM 614 - WORK ZONE STOP LINE, CLASS I, 642 PAINT	74 FEET
ITEM 614 - WORK ZONE EDGE LINE, CLASS I, 6", 642 PAINT	24.40 MILE
ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS I, 12", 642 PAINT	1104 FT
ITEM 614 - WORK ZONE RAILROAD SYMBOL MARKING, CLASS I, 642 PAINT	4 EACH

ITEM 614 - WORK ZONE CENTER LINE, CLASS III, 642 PAINT	2.33 MILE
ITEM 614 - WORK ZONE STOP LINE, CLASS III, 642 PAINT	37 FEET
ITEM 614 - WORK ZONE EDGE LINE, CLASS III, 6", 642 PAINT	4.66 MILE
ITEM 614 - WORK ZONE CROSSWALK LINE, CLASS III, 12", 642 PAINT	552 FT
ITEM 614 - WORK ZONE RAILROAD SYMBOL MARKING, CLASS III, 642 PAINT	2 EACH

NOTIFICATION OF TRAFFIC RESTRICTIONS

THROUGHOUT THE DURATION OF THE PROJECT, THE CONTRACTOR SHALL NOTIFY THE PROJECT ENGINEER IN WRITING OF ALL TRAFFIC RESTRICTIONS AND UPCOMING MAINTENANCE OF TRAFFIC CHANGES. THE CONTRACTOR SHALL ENSURE THE WRITTEN NOTIFICATION IS SUBMITTED IN A TIMELY MANNER TO ALLOW THE PROJECT ENGINEER TO MEET THE REQUIRED TIME FRAMES SET FORTH IN THE TABLE BELOW TO INFORM THE SPECIAL HAULING PERMITS SECTION (HAULING.PERMITS@DOT.OHIO.GOV) AND THE DISTRICT PUBLIC INFORMATION OFFICE (PIO). THIS NOTIFICATION SHALL BE RECEIVED BY THE PROJECT ENGINEER PRIOR TO THE PHYSICAL SETUP OF ANY APPLICABLE SIGNS OR MESSAGE BOARDS.

INFORMATION SHOULD INCLUDE, BUT IS NOT LIMITED TO, ALL CONSTRUCTION ACTIVITIES THAT IMPACT OR INTERFERE WITH TRAFFIC AND SHALL LIST THE SPECIFIC LOCATION, TYPE OF WORK, ROAD STATUS, DATE AND TIME OF RESTRICTION, DURATION OF RESTRICTION, NUMBER OF LANES MAINTAINED, NUMBER OF LANES CLOSED, MINIMUM VERTICAL CLEARANCE, MINIMUM WIDTH OF DRIVABLE PAVEMENT, DETOUR ROUTES, IF APPLICABLE, AND ANY OTHER INFORMATION REQUESTED BY THE PROJECT ENGINEER.

NOTIFICATION OF TRAFFIC RESTRICTIONS TIME TABLE
ITEM DURATION OF NOTICE DUE TO
CLOSURE PERMITS & PIO

RAMP & >= 2 WEEKS 21 CALENDAR DAYS
ROAD CLOSURES PRIOR TO CLOSURE

> 12 HOURS 14 CALENDAR DAYS
& < 2 WEEKS PRIOR TO CLOSURE

<= 12 HOURS 4 CALENDAR DAYS
PRIOR TO CLOSURE

LANE >= 2 WEEKS 14 CALENDAR DAYS
CLOSURES & PRIOR TO CLOSURE
RESTRICTIONS
< 2 WEEKS 5 BUSINESS DAYS
PRIOR TO CLOSURE

START OF N/A 14 CALENDAR DAYS
CONSTRUCTION & PRIOR TO
TRAFFIC PATTERN IMPLEMENTATION
CHANGES

ANY UNFORESEEN CONDITIONS NOT SPECIFIED IN THE PLANS REQUIRING TRAFFIC RESTRICTIONS SHALL ALSO BE REPORTED TO THE PROJECT ENGINEER USING THE NOTIFICATION TIME TABLE.

DESIGN AGENCY	
DESIGNER	ALF
REVIEWER	JMF
PROJECT ID	95800
SHEET	TOTAL
5	26

ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMTUCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMTUCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC, OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMTUCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

FOR OPERATIONS WITHOUT POSITIVE PROTECTION OCCURRING WITHIN 10 FEET OF AN OPEN TRAVELED LANE THAT MEET ALL OF THE FOLLOWING CRITERIA:

- ON A MULTI-LANE DIVIDED INTERSTATE, OTHER FREEWAY OR EXPRESSWAY; AND
- AN AUTHORIZED SPEED LIMIT OF 45 MPH OR GREATER THAT IS IN EFFECT AT THE TIME OF THE OPERATION; AND,
- AAOT OF 50,000 (OR AADT OF 30,000 WITH 25% OR HIGHER PERCENT TRUCKS)

"WITHOUT POSITIVE PROTECTION" MEANS USE OF DRUMS, CONES, SHADOW VEHICLE, ETC, WITHOUT PROTECTION FROM PORTABLE BARRIER OR OTHER RIGID BARRIER ALONG THE WORK AREA. THIS PHRASE DOES NOT APPLY TO CASES WHERE POSITIVE PROTECTION IS REQUIRED. MOBILE OPERATIONS ARE REGARDED AS "WITHOUT POSITIVE PROTECTION". FOR WORK ZONES USING A COMBINATION OF BARRIER AND TEMPORARY TRAFFIC CONTROL DEVICES (CONES, DRUMS, ETC), THE DESIGNATION SHALL BE BASED UPON THE TYPE OF DEVICES USED IN THE AREA THAT WORKERS ARE LOCATED.

IF MULTIPLE ACTIVE LOCALIZED QUALIFYING WORK AREAS OCCUR WITHOUT POSITIVE PROTECTION, PER MAINLINE TRAFFIC DIRECTION, PROVIDE A UNIFORMED LEO AND OFFICIAL PATROL CAR IN ADVANCE OF:

- THE FIRST ACTIVE WORK AREA THAT DRIVERS WILL ENCOUNTER; OR
- THE ACTIVE WORK AREA LATERALLY CLOSEST TO THE OPEN TRAVELED LANE; OR
- OTHER LOCATION AS APPROVED BY THE ENGINEER.

THE UNIFORMED LEO AND OFFICIAL PATROL CAR MAY RELOCATE AMONG THE LISTED LOCATIONS AS APPROPRIATE AS THE OPERATIONS PROCEED IN THE LOCALIZED QUALIFYING WORK AREAS.

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION (OR AT THE POINT OF ROAD CLOSURE), AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE THAT SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY.

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE 40 HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF A LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGNS, AS PER PLAN

THE CONTRACTOR SHALL FURNISH, INSTALL, MAINTAIN AND REMOVE, WHEN NO LONGER NEEDED, A CHANGEABLE MESSAGE SIGN. THE SIGN SHALL BE OF A TYPE SHOWN ON A LIST OF APPROVED PCMS UNITS AVAILABLE ON THE OFFICE OF MATERIALS MANAGEMENT WEB PAGE. THE LIST CONTAINS CLASS A AND B UNITS WITH MINIMUM LEGIBILITY DISTANCES OF 800 FEET AND 650 FEET, RESPECTIVELY.

EACH SIGN SHALL BE TRAILER-MOUNTED AND EQUIPPED WITH A FUNCTIONAL DIMMING MECHANISM, TO DIM THE SIGN DURING DARKNESS, AND A TAMPER AND VANDAL PROOF ENCLOSURE. EACH SIGN SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ON-SITE PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT. THE SIGN SHALL ALSO BE CAPABLE OF BEING POWERED BY AN ELECTRICAL SERVICE DROP FROM A LOCAL UTILITY COMPANY. THE PCMS SHALL BE DELINEATED IN ACCORDANCE WITH C&MS 614.03.

PLACEMENT, OPERATION, MAINTENANCE AND ALL ACTIVATION OF THE SIGNS BY THE CONTRACTOR SHALL BE AS DIRECTED BY THE ENGINEER. THE PCMS SHALL BE LOCATED IN A HIGHLY VISIBLE POSITION YET PROTECTED FROM TRAFFIC. THE CONTRACTOR SHALL, AT THE DIRECTION OF THE ENGINEER, RELOCATE THE PCMS TO IMPROVE VISIBILITY OR ACCOMMODATE CHANGED CONDITIONS. WHEN NOT IN USE, THE PCMS SHALL BE TURNED OFF. ADDITIONALLY, WHEN NOT IN USE FOR EXTENDED PERIODS OF TIME, THE PCMS SHALL BE TURNED AWAY FROM ALL TRAFFIC.

THE ENGINEER SHALL BE PROVIDED ACCESS TO EACH SIGN UNIT AND SHALL BE PROVIDED WITH APPROPRIATE TRAINING AND OPERATION INSTRUCTIONS TO ENABLE ODOT PERSONNEL TO OPERATE AND TROUBLESHOOT THE UNIT, AND TO REVISE SIGN MESSAGES, IF NECESSARY.

THE CONTRACTOR SHALL IMPLEMENT A SYSTEM WHEREBY CHANGEABLE MESSAGES WILL BE IMPLEMENTED WITHIN 4 HOURS FOLLOWING TELEPHONE NOTIFICATION FROM THE PROJECT ENGINEER TO A DESIGNATED PHONE.

ALL MESSAGES TO BE DISPLAYED ON THE SIGN WILL BE PROVIDED BY THE ENGINEER. A LIST OF ALL REQUIRED PRE-PROGRAMMED MESSAGES WILL BE GIVEN TO THE CONTRACTOR AT THE PROJECT PRECONSTRUCTION CONFERENCE. THE SIGN SHALL HAVE THE CAPABILITY TO STORE UP TO 99 MESSAGES. MESSAGE MEMORY OR PRE-PROGRAMMED DISPLAYS SHALL NOT BE LOST AS A RESULT OF POWER FAILURES TO THE ON-BOARD COMPUTER. THE SIGN LEGEND SHALL BE CAPABLE OF BEING CHANGED IN THE FIELD. THREE-LINE PRESENTATION FORMATS WITH UP TO SIX MESSAGE PHASES SHALL BE SUPPORTED. PCMS FORMAT SHALL PERMIT THE COMPLETE MESSAGE FOR EACH PHASE TO BE READ AT LEAST TWICE.

THE PCMS SHALL CONTAIN AN ACCURATE CLOCK AND PROGRAMMING LOGIC WHICH WILL ALLOW THE SIGN TO BE ACTIVATED, DEACTIVATED OR MESSAGES CHANGED AUTOMATICALLY AT DIFFERENT TIMES OF THE DAY FOR DIFFERENT DAYS OF THE WEEK.

THE PCMS UNIT SHALL BE MAINTAINED IN GOOD WORKING ORDER BY THE CONTRACTOR IN ACCORDANCE WITH THE PROVISIONS OF C&MS 614.07. THE CONTRACTOR SHALL, PRIOR TO ACTIVATING THE UNIT, MAKE ARRANGEMENTS, WITH AN AUTHORIZED SERVICE AGENT FOR THE PCMS, TO ASSURE PROMPT SERVICE IN THE EVENT OF FAILURE. ANY FAILURE SHALL NOT RESULT IN THE SIGN BEING OUT OF SERVICE FOR MORE THAN 12 HOURS, INCLUDING WEEKENDS. FAILURE TO COMPLY MAY RESULT IN AN ORDER TO STOP WORK AND OPEN ALL TRAFFIC LANES AND/OR IN THE DEPARTMENT TAKING APPROPRIATE ACTION TO SAFELY CONTROL TRAFFIC. THE ENTIRE COST TO CONTROL TRAFFIC, ACCRUED BY THE DEPARTMENT DUE TO THE CONTRACTOR'S NONCOMPLIANCE, WILL BE DEDUCTED FROM MONEYS DUE, OR TO BECOME DUE THE CONTRACTOR ON HIS CONTRACT.

THE CONTRACTOR SHALL BE RESPONSIBLE FOR 24-HOUR-PER-DAY OPERATION AND MAINTENANCE OF THESE SIGNS ON THE PROJECT FOR THE DURATION OF THE PHASES WHEN THE PLAN REQUIRES THEIR USE.

PAYMENT FOR THE ABOVE DESCRIBED ITEM SHALL BE AT THE CONTRACT UNIT PRICE. PAYMENT SHALL INCLUDE ALL LABOR, MATERIALS, EQUIPMENT, FUELS, LUBRICATING OILS, SOFTWARE, HARDWARE AND INCIDENTALS TO PERFORM THE ABOVE DESCRIBED WORK.

ITEM 614, PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN 4 SIGN MONTH

ASSUMING 2 PCMS SIGN(S) FOR 2 MONTH(S)

ITEM 614, REPLACEMENT DRUM

DRUMS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT DRUMS SHALL BE NEW.

PAYMENT FOR THE NEW DRUMS SHALL BE MADE AT THE CONTRACT PRICE PER EACH FOR ITEM 614, REPLACEMENT DRUM, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF THE DAMAGED DRUM, AND PROVIDING AND MAINTAINING THE REPLACEMENT DRUM IN ACCORDANCE WITH THE CONTRACT REQUIREMENTS FOR THE ORIGINAL DRUM.

AN ESTIMATED QUANTITY OF 6 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

ITEM 614, REPLACEMENT SIGN

FLATSHEET SIGNS FURNISHED BY THE CONTRACTOR IN ACCORDANCE WITH THE REQUIREMENTS OF THE PLANS, SPECIFICATIONS AND PROPOSAL WHICH BECOME DAMAGED BY THE TRAFFIC FOR REASONS BEYOND THE CONTROL OF THE CONTRACTOR SHALL BE REPLACED IN KIND WHEN ORDERED BY THE ENGINEER. REPLACEMENT SIGNS SHALL BE NEW. OTHER MATERIALS MAY BE IN USED, BUT GOOD, CONDITION SUBJECT TO APPROVAL BY THE ENGINEER.

PAYMENT FOR THE NEW SIGNS SHALL BE MADE AT THE CONTRACT PRICE PER EACH ITEM 614, REPLACEMENT SIGN, AND SHALL INCLUDE THE COST OF REMOVING AND DISPOSING OF DAMAGED SIGNS, HARDWARE AND SUPPORTS, AND PROVIDING NECESSARY REPLACEMENT HARDWARE, SUPPORTS, ETC.

AN ESTIMATED QUANTITY OF 6 EACH HAS BEEN PROVIDED IN THE GENERAL SUMMARY.

DESIGN AGENCY



DESIGNER

ALF

REVIEWER

JMF

PROJECT ID

95800


SHEET TOTAL

6 26

SHEET NUM.							PART.	ITEM	ITEM	GRAND	UNIT	DESCRIPTION	SEE SHEET NO.
4	5	6	9	10	11	12	01/STR/05	EXT	TOTAL				
ROADWAY													
	3,520		25										
							3,545	202	23000	3,545	SY	PAVEMENT REMOVED	
							1,113	202	30000	1,113	SF	WALK REMOVED	
							23	202	32000	23	FT	CURB REMOVED	
					412.5		412.5	202	38000	412.5	FT	GUARDRAIL REMOVED	
					2		2	202	42010	2	EACH	ANCHOR ASSEMBLY REMOVED, TYPE E	
					3		3	202	42040	3	EACH	ANCHOR ASSEMBLY REMOVED, TYPE T	
						3	3	203	10000	3	CY	EXCAVATION	
						21	21	203	20000	21	CY	EMBANKMENT	
					5		5	209	15000	5	STA	RESHAPING UNDER GUARDRAIL	
			12.18	0.79			12.97	209	60500	12.97	MILE	LINEAR GRADING	
5.5							5.5	209	72050	5.5	MILE	PREPARING SUBGRADE FOR SHOULDER PAVING	
					350		350	606	15050	350	FT	GUARDRAIL, TYPE MGS	
					2		2	606	26150	2	EACH	ANCHOR ASSEMBLY, MGS TYPE E, MASH 2016	
					3		3	606	26550	3	EACH	ANCHOR ASSEMBLY, MGS TYPE T	
						32	32	608	10000	32	SF	4" CONCRETE WALK	
						871	871	608	52000	871	SF	CURB RAMP	
						23	23	609	26000	23	FT	CURB, TYPE 6	
2							2	623	39600	2	EACH	MONUMENT ASSEMBLY RECONSTRUCTED TO GRADE	
EROSION CONTROL													
					184	44	228	659	10000	228	SY	SEEDING AND MULCHING	
					0.02		0.02	659	20000	0.02	TON	COMMERCIAL FERTILIZER	
					1		1	659	35000	1	MGAL	WATER	
							1,000	832	30000	1,000	EACH	EROSION CONTROL	
DRAINAGE													
4							4	611	98630	4	EACH	CATCH BASIN ADJUSTED TO GRADE	
9							9	611	99654	9	EACH	MANHOLE ADJUSTED TO GRADE	
PAVEMENT													
1,470							1,470	253	02000	1,470	CY	PAVEMENT REPAIR, 6"	
1,064							1,064	253	02000	1,064	CY	PAVEMENT REPAIR, 8"	
			27,741	194			27,935	254	01000	27,935	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 1 3/4"	
			31,763				31,763	254	01000	31,763	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 3"	
			20,855				20,855	254	01000	20,855	SY	PAVEMENT PLANING, ASPHALT CONCRETE, 3 1/4"	
	299		9,725	16			10,040	407	20000	10,040	GAL	NON-TRACKING TACK COAT	
14			1,462				1,476	424	14000	1,476	CY	FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, (448)	
			3,068	116			3,184	441	10200	3,184	CY	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (446)	
75							75	441	70500	75	CY	ASPHALT CONCRETE SURFACE COURSE, TYPE 1, (449), (DRIVEWAYS)	
					81		81	441	90000	81	CY	ASPHALT CONCRETE, MISC.: BUTT JOINT AT INTERSECTION	4
	147		1,349	8			1,504	442	10000	1,504	CY	ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (446)	
	49		1,031	42			1,122	617	10100	1,122	CY	COMPACTED AGGREGATE	
			3.73				3.73	618	43000	3.73	MILE	RUMBLE STRIPES, CENTER LINE (ASPHALT CONCRETE)	
			19,662				19,662	874	20000	19,662	FT	LONGITUDINAL JOINT PREPARATION	
			3,093				3,093	875	10000	3,093	LB	LONGITUDINAL JOINT ADHESIVE	
WATER WORK													
1							1	638	10800	1	EACH	VALVE BOX ADJUSTED TO GRADE	

GENERAL SUMMARY

DESIGN AGENCY



DESIGNER
ALF


REVIEWER
JMF

PROJECT ID
95800

SHEET TOTAL
7 26

SHEET NUM.							PART.	ITEM	ITEM	GRAND	UNIT	DESCRIPTION	SEE SHEET NO.
4	5	6	9	10	11	12	01/STR/05	EXT	TOTAL				
												TRAFFIC CONTROL	
					415		415	621	54000	415	EACH	RAISED PAVEMENT MARKER REMOVED	
					415		415	621	00100	415	EACH	RPM	
					9		9	626	00116	9	EACH	BARRIER REFLECTOR, TYPE 5, BIDIRECTIONAL	
					12.2		12.2	642	00104	12.2	MILE	EDGE LINE, 6", TYPE 1	
	0.25				6.1		6.35	642	00300	6.35	MILE	CENTER LINE, TYPE 1	
					37		37	644	00500	37	FT	STOP LINE	
					552		552	644	00630	552	FT	CROSSWALK LINE, 24"	
					2		2	644	01000	2	EACH	RAILROAD SYMBOL MARKING	
												MAINTENANCE OF TRAFFIC	
		40					40	614	11110	40	HOUR	LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE	
	34						34	614	12460	34	EACH	WORK ZONE MARKING SIGN	
		6					6	614	12500	6	EACH	REPLACEMENT SIGN	
		6					6	614	12600	6	EACH	REPLACEMENT DRUM	
		4					4	614	18601	4	SNMT	PORTABLE CHANGEABLE MESSAGE SIGN, AS PER PLAN	6
	12.2						12.2	614	21100	12.2	MILE	WORK ZONE CENTER LINE, CLASS I, 642 PAINT	
	2.33						2.33	614	21550	2.33	MILE	WORK ZONE CENTER LINE, CLASS III, 642 PAINT	
	24.4						24.4	614	22110	24.4	MILE	WORK ZONE EDGE LINE, CLASS I, 6", 642 PAINT	
	4.66						4.66	614	22360	4.66	MILE	WORK ZONE EDGE LINE, CLASS III, 6", 642 PAINT	
	74						74	614	26200	74	FT	WORK ZONE STOP LINE, CLASS I, 642 PAINT	
	37						37	614	26610	37	FT	WORK ZONE STOP LINE, CLASS III, 642 PAINT	
	1,104						1,104	614	27050	1,104	FT	WORK ZONE CROSSWALK LINE, CLASS I, 12", 642 PAINT	
	552						552	614	27250	552	FT	WORK ZONE CROSSWALK LINE, CLASS III, 12", 642 PAINT	
	4						4	614	32200	4	EACH	WORK ZONE RAILROAD SYMBOL MARKING, CLASS I, 642 PAINT	
	2						2	614	32210	2	EACH	WORK ZONE RAILROAD SYMBOL MARKING, CLASS III, 642 PAINT	
												INCIDENTALS	
							100	00300	LS			PREMIUM ON RAILROADS' PROTECTIVE PUBLIC LIABILITY AND PROPERTY DAMAGE LIABILITY INSURANCE	
							103	05000	LS			PREMIUM FOR CONTRACT PERFORMANCE BOND AND FOR PAYMENT BOND	
							614	11000	LS			MAINTAINING TRAFFIC	
							623	10000	LS			CONSTRUCTION LAYOUT STAKES AND SURVEYING	
							624	10000	LS			MOBILIZATION	

DESIGN AGENCY



DESIGNER
ALF

REVIEWER
JMF

PROJECT ID
95800

SHEET TOTAL
8 | 26

STATION RANGE			SIDE	DISTANCE (D) FT	AVERAGE PAVEMENT WIDTH (W) FT	AVERAGE SHOULDER WIDTH (S) FT	CADD GENERATED AREA SY	202	209	254			407	407	424	441	441	442	617	618	874	875
								PAVEMENT REMOVED (CURB RAMP AREA) SY	LINEAR GRADING MILE	PAVEMENT PLANING, ASPHALT CONCRETE, 1 3/4" SY	PAVEMENT PLANING, ASPHALT CONCRETE, 3" SY	PAVEMENT PLANING, ASPHALT CONCRETE, 3 1/4" SY	NON-TRACKING TACK COAT (0.085) GAL	NON-TRACKING TACK COAT (0.055) GAL	FINE GRADED POLYMER ASPHALT CONCRETE, TYPE B, (448), 1" CY	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (446), 2" CY	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (446), 2 1/4" CY	ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (449), 3 1/4" CY	COMPACTED AGGREGATE CY	RUMBLE STRIPES, CENTER LINE (ASPHALT CONCRETE) MILE	LONGITUDINAL JOINT PREPARATION FT	LONGITUDINAL JOINT ADHESIVE LB
541+59.2	TO	541+84.2	RT/LT	25	19.00	3.00	61		0.01	61			5				3	1	0.01	25		
541+84.2	TO	654+49	RT/LT	11265	19.00	3.00	27536		4.27	27536			2341				1339	243	2.13	11265		
654+49	TO	654+90	RT/LT	41	19.00	12.50	144		0.02	144			12				7	1	0.01	41		
656+48	TO	656+91	RT/LT	43	19.00	12.50	151		0.02				13	8	4			2	0.01	43		
656+91	TO	716+18	RT/LT	5927	19.00	3.00	14488		2.25				1231	797	402			238	1.12	5927		
716+18	TO	717+44	RT/LT	126	24.33	3.45	389		0.05				33	21	11			5	0.02	126		
717+44	TO	739+79	RT/LT	2235	19.00	3.00	5463		0.85				464	300	152			83	0.42	2235		
739+79	TO	740+74	RT/LT	95.00	31.50	3.00	364		0.04				31	20	10			4			24	
740+74	TO	745+21	RT/LT	447	20.50	2.00	1118		0.17				95	61	31	62		17			112	
745+21	TO	746+77	RT/LT	156	20.50	10.50	537		0.06				46	30	15	30		6			39	
746+77	TO	753+65	RT/LT	688.00	20.50	7.50	2140		0.26				182	118	59	119		25			172	
753+65	TO	755+09	RT/LT	144	44.25	7.50	828		0.05				70	46	23	46		5			36	
755+09	TO	758+67	RT/LT	358	20.50	1.00	855		0.14				73	47	24	48		13			90	
758+67	TO	761+31	RT/LT	264	20.50	2.50	675		0.10				57	37	19	37		10			66	
761+31	TO	763+63	RT/LT	232	20.50	3.50	619	7	0.09				53	34	17	34		9			58	
763+63	TO	768+26	RT/LT	463	20.50	9.50	1543		0.18				131	85	43	86		17			116	
768+26	TO	797+00	RT/LT	2874	20.50	1.00	6866	18	1.09				584	378	191	381		106			719	
797+00	TO	822+75	RT/LT	2575	20.50	1.00	6151		0.98				523	338	171	342		95			644	
822+75	TO	823+22	RT/LT	47	20.50	2.00	118		0.02				10	6	3	7		2			12	
823+42	TO	823+74	RT/LT	32	20.50	2.00	80		0.01				7	4	2	4		1			8	
823+74	TO	849+75	RT/LT	2601	20.50	2.00	6503		0.99				553	358	181	361		96			650	
849+75	TO	850+77	RT/LT	102	20.50	36.00	640		0.04				54	35	18	36		4			26	
850+77	TO	863+45.84	RT/LT	1269	20.50	1.00	3031		0.48				258	167	84	168		47			317	
863+45.84	TO	863+70.84	RT/LT	25	20.50	1.00	60		0.01				5	3	2	3		1			6	
01/STR/05 SUB-TOTALS								25	12.18	27741	31763	20855	6831	2894	1462	1765	1393	1349	1031	3.73	19662	3093
01/STR/05 TOTALS CARRIED TO GENERAL SUMMARY								25	12.18	27741	31763	20855	9725	1462	3068		1349	1031	3.73	19662	3093	

PAVEMENT CALCULATIONS

DESIGN AGENCY



DESIGNER
ALF
REVIEWER
JMF
PROJECT ID
95800

SHEET 9 TOTAL 26

STATION RANGE	SIDE	CADD GENERATED AREA	209	254	254		407			441	441	441	442	442	617		
			LINEAR GRADING	PAVEMENT PLANING, ASPHALT CONCRETE, 1 3/4"	PAVEMENT PLANING, ASPHALT CONCRETE, 3 1/4"		NON-TRACKING TACK COAT			ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (446), 2"	ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 2, (446), 2 1/4"	ASPHALT CONCRETE, MISC.: BUTT JOINT AT INTERSECTION	ASPHALT CONCRETE SURFACE COURSE, 12.5 MM, TYPE A (446) 1 1/2"	ASPHALT CONCRETE INTERMEDIATE COURSE, 12.5 MM, TYPE A (446), 1 3/4"	COMPACTED AGGREGATE		
		SY	MILE	SY	SY		GAL			CY	CY	CY	CY	CY	CY		
INTERSECTIONS																	
MERCER RD	RT	12	0.02		12		2						1	1	1		
MERCER RD	LT	107	0.04		107		18						4	6	4		
CARIS RD	RT	61	0.02	61		5							3		2		
CARIS RD	LT	60	0.02	60		5							2		2		
LAYMAN RD	RT	73	0.03							5	3				3		
STONY RIDGE RD	RT	83	0.03							5	3				3		
STONY RIDGE RD	LT	123	0.05							8	5				5		
LUCKEY RD	LT	144	0.05							9	6				5		
GILBERT RD	LT	91	0.03							6	4				3		
LUCKEY RD	RT/LT	166	0.06							10	7				6		
MAPLE ST	LT	38	0.01						2		2						
PARK DR	RT	94	0.04						5		4						
ASH ST	RT	39	0.01						2		2						
MAIN ST	RT/LT	100	0.04						6		4						
LIME ST	RT	78	0.03						4		3						
LIME ST	LT	76	0.03						4		3						
WALNUT ST	RT	63	0.02						4		3						
OAK ST	RT	78	0.03						4		3						
BASIC ST	LT	58	0.02						3		2						
ADAMS ST	RT	119	0.05						7		5						
ADAMS ST	LT	56	0.02						3		2						
RUCH ST	RT	40	0.02						2		2						
SCHOOL ST	RT	59	0.02						3		2						
LEMOYNE RD	RT	92	0.03						5		4						
LEMOYNE RD	LT	114	0.04						6		5				4		
PEMBERVILLE RD	LT	125	0.05						7		5				5		
MIDDLETON PIKE	LT	116	0.04						6		5				4		
01/STR/05 SUB-TOTALS			0.87	121	119		30			73	43	84	10	7	48		
01/STR/05 TOTALS CARRIED TO GENERAL SUMMARY			0.87	121	119		30			116		84	10	7	48		

PAVEMENT CALCULATIONS

DESIGN AGENCY



DESIGNER

ALF

REVIEWER

JMF

PROJECT ID

95800

SHEET

9

TOTAL

26

REF. NO.	STATION	ROUTE	SIDE	202				209	606				626	
				GUARDRAIL REMOVED FEET	ANCHOR ASSEMBLY REMOVED, TYPE E EACH	ANCHOR ASSEMBLY REMOVED, TYPE T EACH			RESHAPING UNDER GUARDRAIL STA	GUARDRAIL, TYPE MGS FEET	ANCHOR ASSEMBLY, MGS TYPE E, MASH 2016 EACH	ANCHOR ASSEMBLY, MGS TYPE T EACH		BARRIER REFLECTOR, TYPE 5, BIDIRECTIONAL EACH
GR-1	650+94	SR 582	LT	300.0	1	1		3	237.5	1	1			6
GR-2	795+39	SR 582	RT	112.5	1	2		2	112.5	1	2			3
TOTALS CARRIED TO GENERAL SUMMARY				412.5	2	3		5	350.0	2	3			9

NOTE: CONTRACTOR MUST CALL OUPS TO LOCATE UTILITIES IN VICINITY OF ANY GUARDRAIL RUN

NOTE: Caution shall be used when placing proposed Guardrail, as to avoid damaging any existing drainage (pipes, culverts, etc.) within the work area of Any Run of Guardrail.

The Contractor Shall Exercise Caution When Working in the Proximity of any Underground Utilities. All Existing Underground Utilities Shall Remain Active and In Place During Construction of Any Guardrail Run, Unless Otherwise Noted in the Plan.

Caution Must be Used When Removing and Replacing Guardrail As to Maintain the Existing Shoulders and Embankment.

The Following Items are to be used As Directed by the Engineer. The Estimated Quantities will be Carried to the General Summary and are to be Used for Proposed Guardrail Runs:

Item 659	184	SQ YD	Seeding and Mulching
Item 659	0.02	TON	Commercial Fertilizer
Item 659	1	M GAL	Water

COUNTY	LOCATION	STATION TO STATION	621	621	642	642	644	644	644				
			RAISED PAVEMENT MARKER REMOVED EACH	RPM, YELLOW-YELLOW EACH	EDGE LINE, 6", TYPE 1, WHITE MILE	CENTER LINE, TYPE 1 MILE	STOP LINE FT	CROSSWALK LINE, 24" FT	RAILROAD SYMBOL MARKING EACH				
WOO	582	541+59.2 TO 610+50		104	2.61	1.31							
WOO	582	610+50 TO 655+00		67	1.69	0.84							
WOO	582	655+00 TO 665+00		15	0.38	0.19							
WOO	582	665+00 TO 722+50		87	2.18	1.09	37						
WOO	582	722+50 TO 746+00		24	0.89	0.45		30					
WOO	582	746+00 TO 756+00			0.38	0.19		138					
WOO	582	756+00 TO 766+00			0.38	0.19		126					
WOO	582	766+00 TO 775+50	415		0.36	0.18		138					
WOO	582	775+50 TO 785+50			0.38	0.19		96					
WOO	582	785+50 TO 795+50		15	0.38	0.19		24					
WOO	582	795+50 TO 827+50		48	1.21	0.61				1			
WOO	582	827+50 TO 851+00		36	0.89	0.45				1			
WOO	582	851+00 TO 861+00		15	0.38	0.19							
WOO	582	861+00 TO 863+71.86		4	0.10	0.05							
01/STR/05 TOTALS CARRIED TO GENERAL SUMMARY			415	415	12.20	6.10		37	552		2		

DESIGN AGENCY



DESIGNER

ALF

REVIEWER

JMF

PROJECT ID


95800

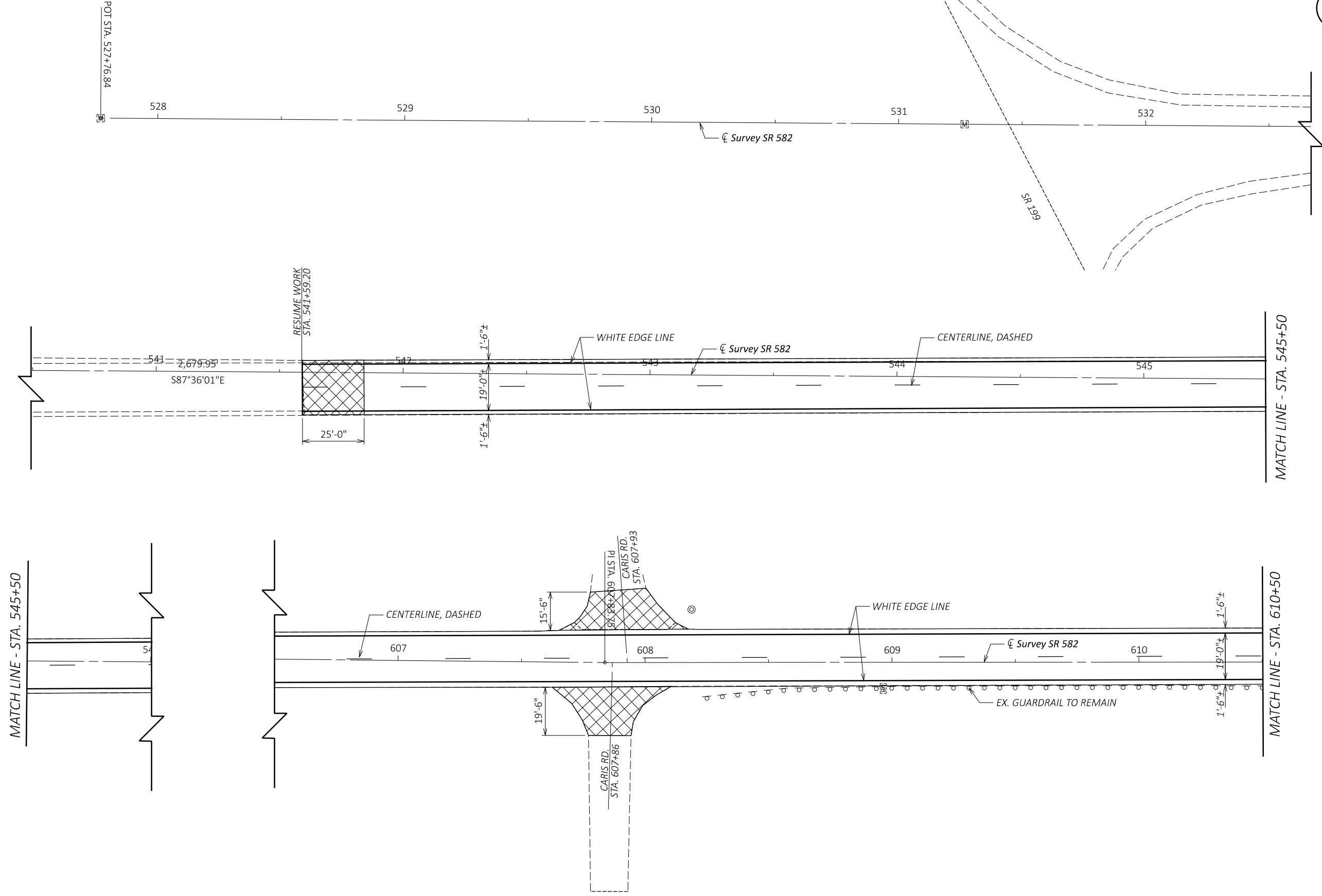
SHEET TOTAL

11 | 26


REFERENCE NO.	ROUTE	STATION	SIDE	NEAREST CROSS STREET NAME	LENGTH	WIDTH	202	202		203	203		608	608		609		659
							CURB REMOVED	WALK REMOVED		EXCAVATION	EMBANKMENT		4" CONCRETE WALK	CURB RAMP		CURB, TYPE 6		SEEDING AND MULCHING
							FT	SF		CY	CY		SF	SF		FT		SY
CR-1	SR 582	745+30	LT	MAPLE ST	6.0	5.0		26			1			23				3
CR-2	SR 582	745+55	LT	MAPLE ST	9.0	5.0	11	35			1			30		11		3
CR-3	SR 582	748+02	RT	PARK DR	16.0	5.0		80			1			67				2
CR-4	SR 582	748+37	RT	PARK DR	7.0	5.0		43			1			31				2
CR-5	SR 582	753+05	RT	ASH ST	7.0	5.0		35			1			26				2
CR-6	SR 582	753+30	RT	ASH ST	7.0	5.0		38			1			27				2
CR-7	SR 582	754+52	LT	MAIN ST	8.5	5.0		44			1			34				2
CR-8	SR 582	754+96	RT	MAIN ST	11.0	6.0	12	74			1			61		12		1
CR-9	SR 582	758+61	RT	LIME ST	9.0	5.0		53			1			36				2
CR-10	SR 582	759+21	RT	LIME ST	8.0	5.0		44			1			31				2
CR-11	SR 582	761+29	RT	WALNUT ST	8.0	5.0		37			1			29				1
CR-12	SR 582	761+74	RT	WALNUT ST	12.0	5.0		66	1		1			41				3
CR-13	SR 582	763+57	RT	OAK ST	6.0	5.0		33			1			30				1
CR-14	SR 582	773+69	RT	ADAMS ST	13.0	5.0		65			1		16	37				1
				ADAMS ST	4.0	5.0		16					16					
CR-15	SR 592	774+19	RT	ADAMS ST	24.0	5.0		69						69				
CR-16	SR 582	774+34	LT	ADAMS ST	8.0	5.0		39			1							
				ADAMS ST	4.0	5.0			1					26				
CR-17	SR 582	774+81	RT	ADAMS ST	12.0	8.0		67	1		1			83				1
CR-18	SR 582	779+48	RT	RUCH ST	8.0	5.0		43			1			33				2
CR-19	SR 582	779+86	RT	RUCH ST	10.0	5.0		54			1			40				2
CR-20	SR 582	782+14	RT	SCHOOL ST	9.0	5.0		39			1			36				1
CR-21	SR 582	782+60	RT	SCHOOL ST	9.0	5.0		46						46				
CR-22	SR 582	787+76	RT	MID BLOCK/PARK	3.0	5.0		22			1			11				2
CR-23	SR 582	787+83	LT	MID BLOCK/PARK	6.0	5.0		45	2		1							9
				MID BLOCK/PARK	6.0	6.0								24				
01/STR/05 TOTALS CARRIED TO GENERAL SUMMARY							23	1113		3	21		32	871		23		44

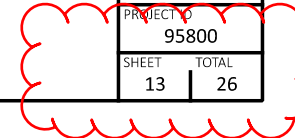
CURB RAMP SUBSUMMARIES

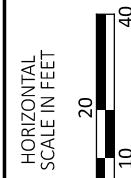
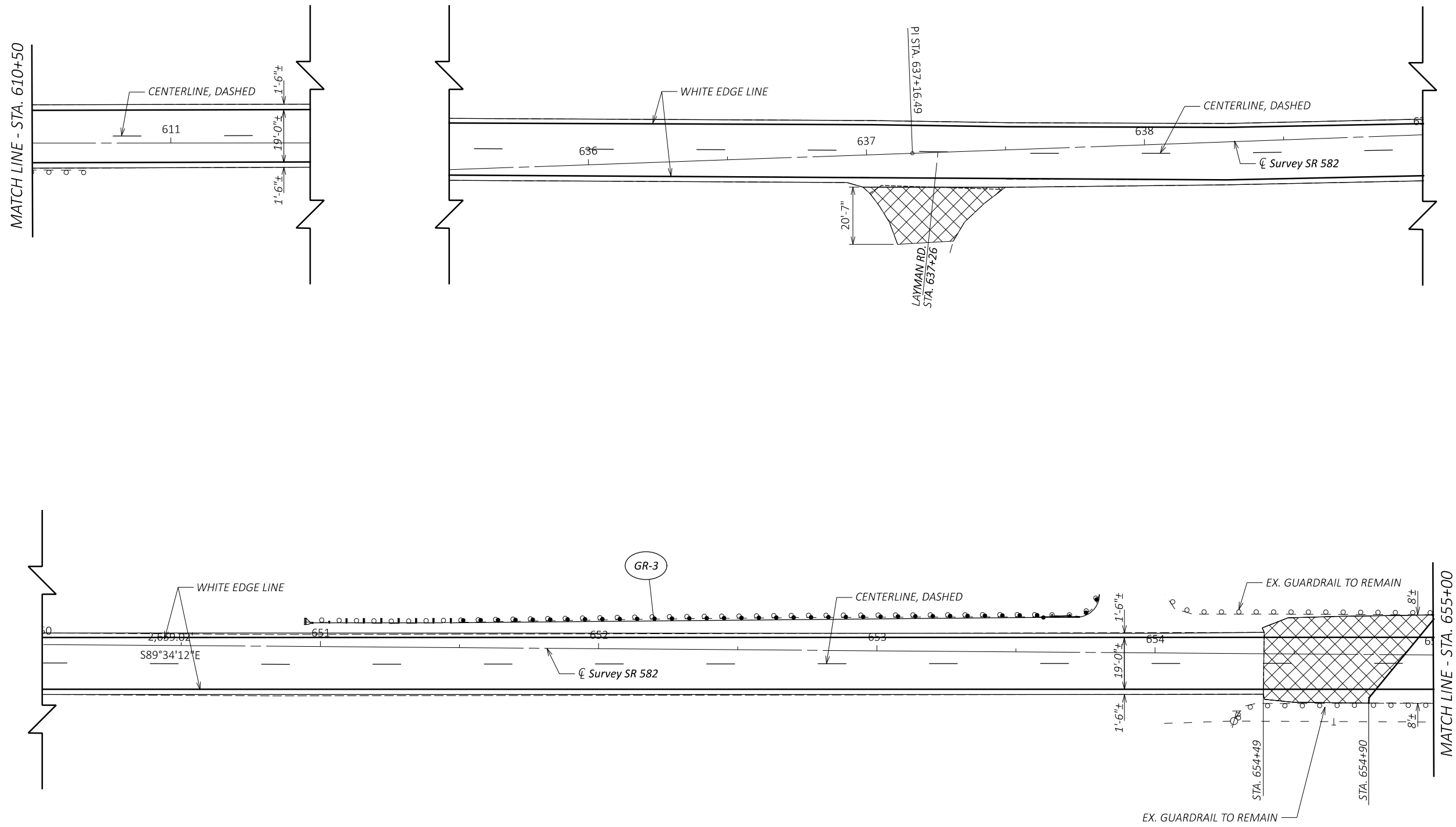
DESIGN AGENCY

 DESIGNER
 ALF
 REVIEWER
 JMF
 PROJECT ID
 95800
 SHEET TOTAL
 12 | 26



PLAN SHEET
 STA. 541+59.20 TO STA. 610+50 SR 582

DESIGN AGENCY	
	
DESIGNER	ALF
REVIEWER	JMF
PROJECT #	95800
SHEET	TOTAL
13	26





PLAN SHEET
STA. 610+50 TO STA. 655+00 SR 582

DESIGN AGENCY



DESIGNER

ALF

REVIEWER

JMF

PROJECT NO.

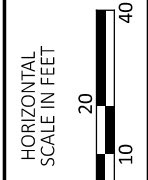
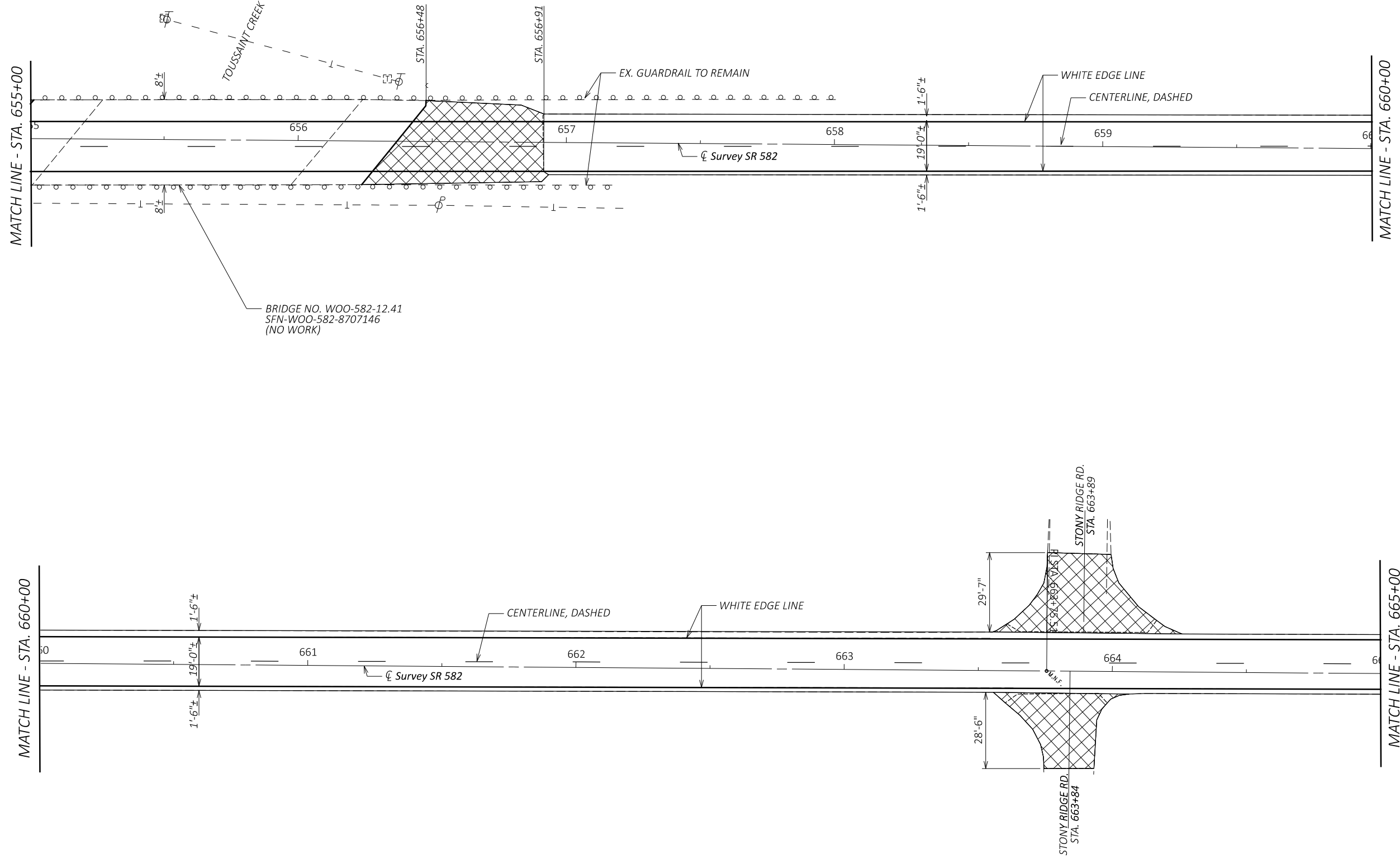
95800

SHEET

14

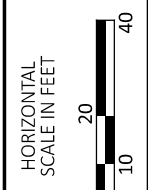
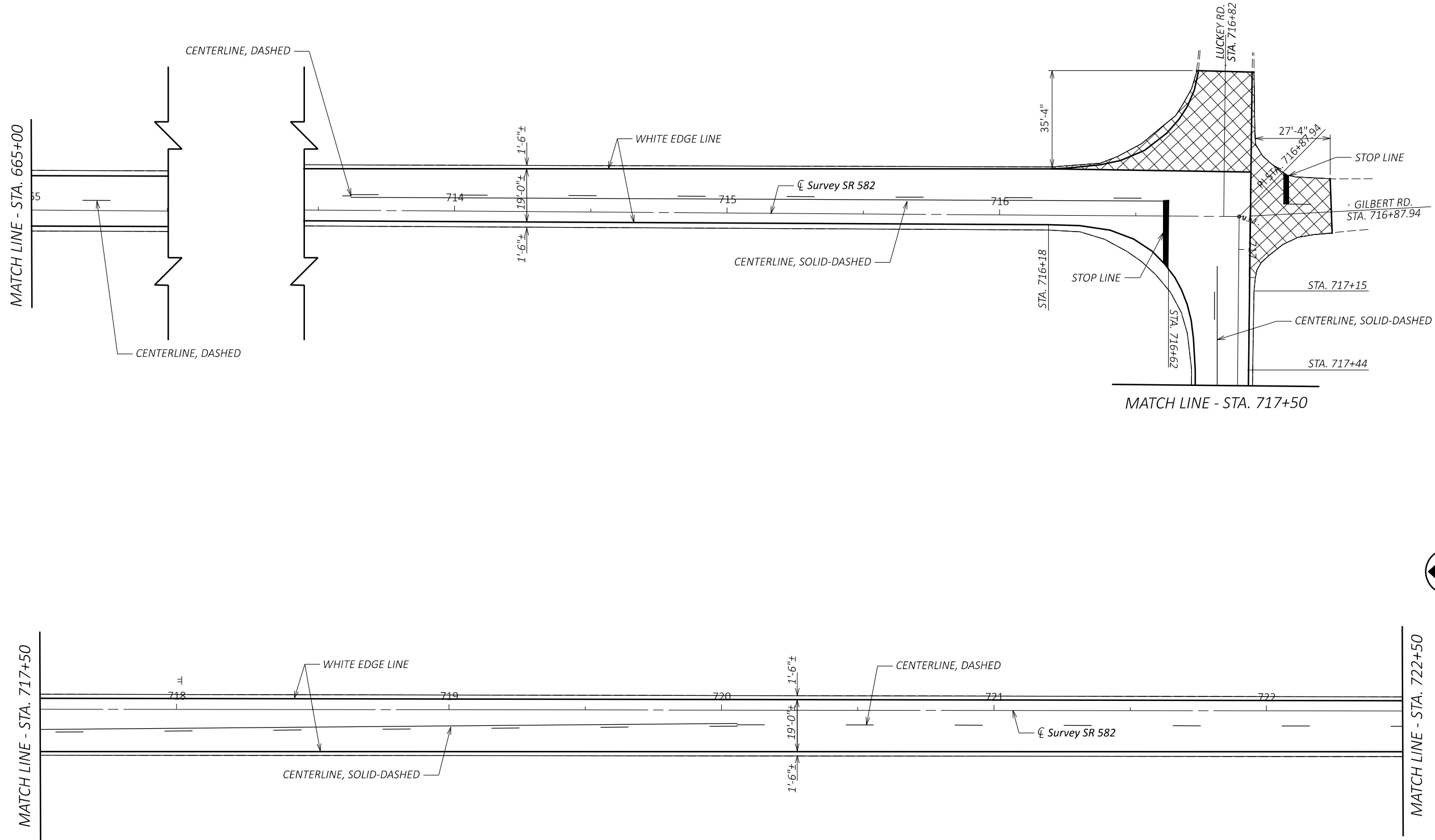
TOTAL

26



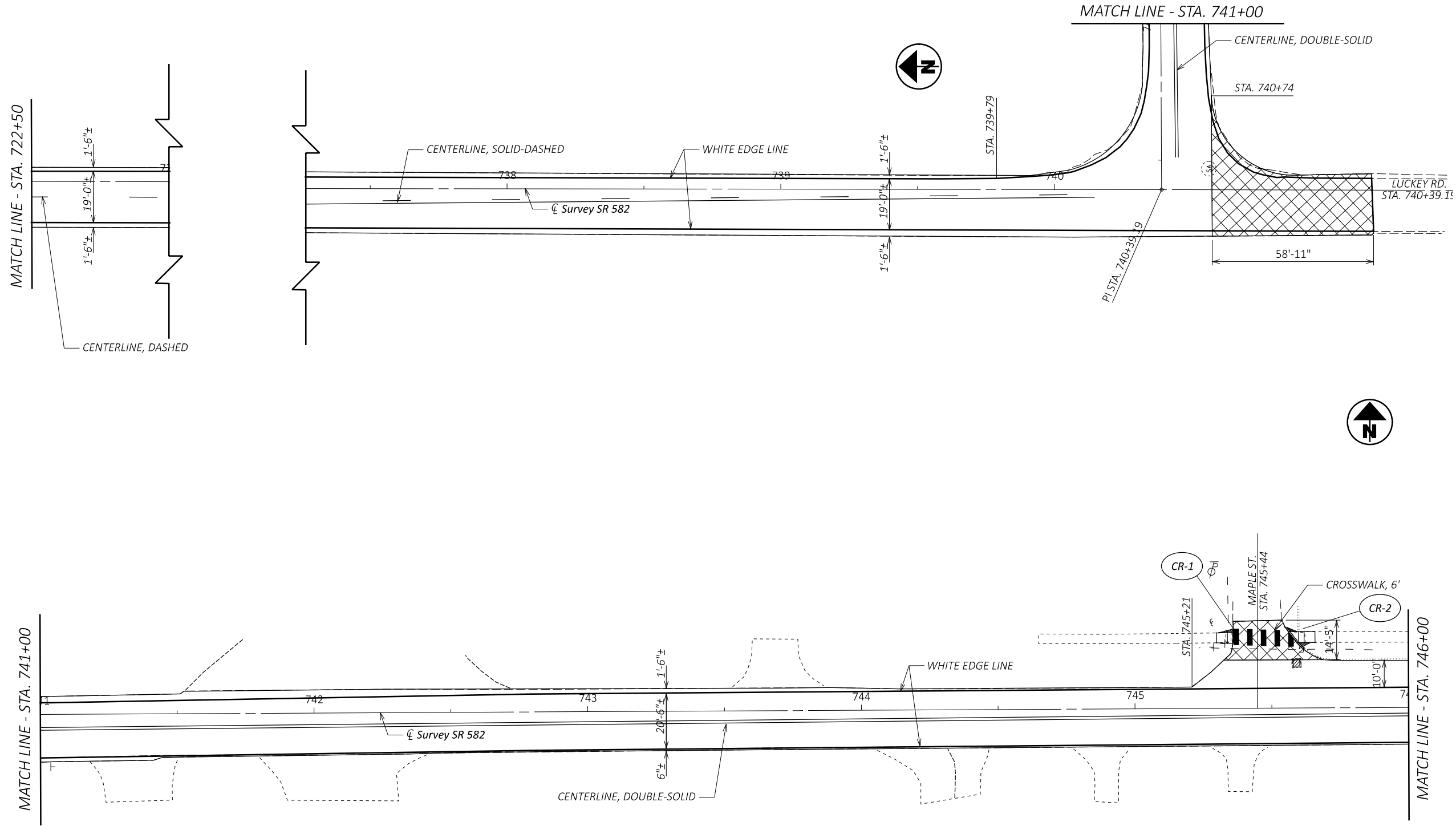
PLAN SHEET
 STA. 610+50 TO STA. 655+00 SR 582

DESIGN AGENCY	
DESIGNER	ALF
REVIEWER	JMF
PROJECT ID	95800
SHEET	TOTAL
15	26



PLAN SHEET
STA. 665+00 TO STA. 722+50 SR 582

DESIGN AGENCY	
DESIGNER	ALF
REVIEWER	JMF
PROJECT ID	95800
SHEET	TOTAL
16	26



PLAN SHEET
 STA. 722+50 TO STA. 746+00 SR 582

DESIGN AGENCY



DESIGNER

ALF

REVIEWER

JMF

PROJECT ID

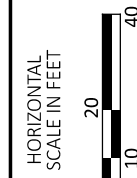
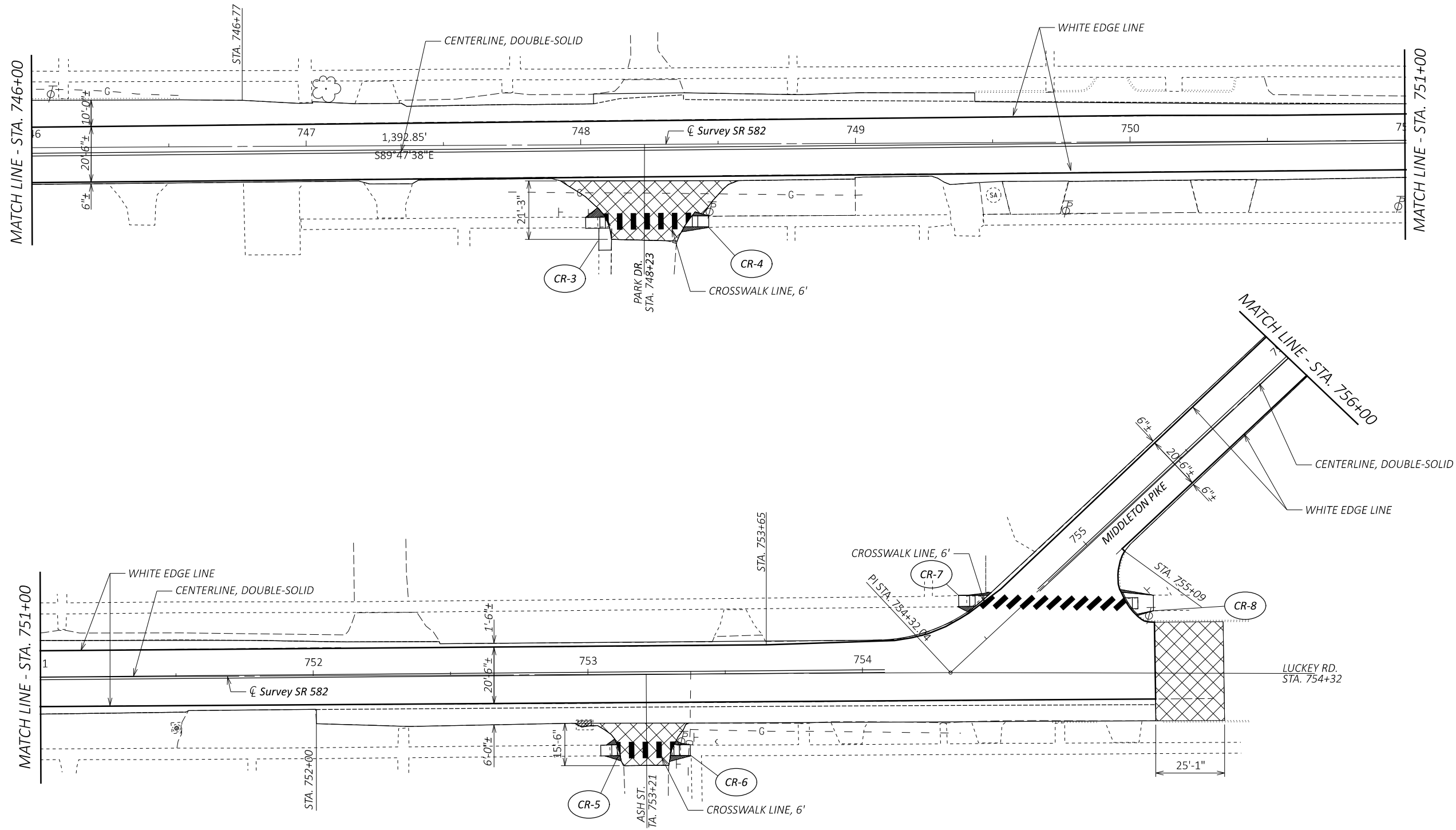
95800

SHEET

17

TOTAL

26



PLAN SHEET
STA. 746+00 TO STA. 756+00 SR 582

DESIGN AGENCY



DESIGNER

ALF

REVIEWER

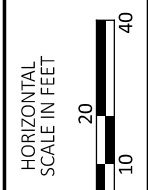
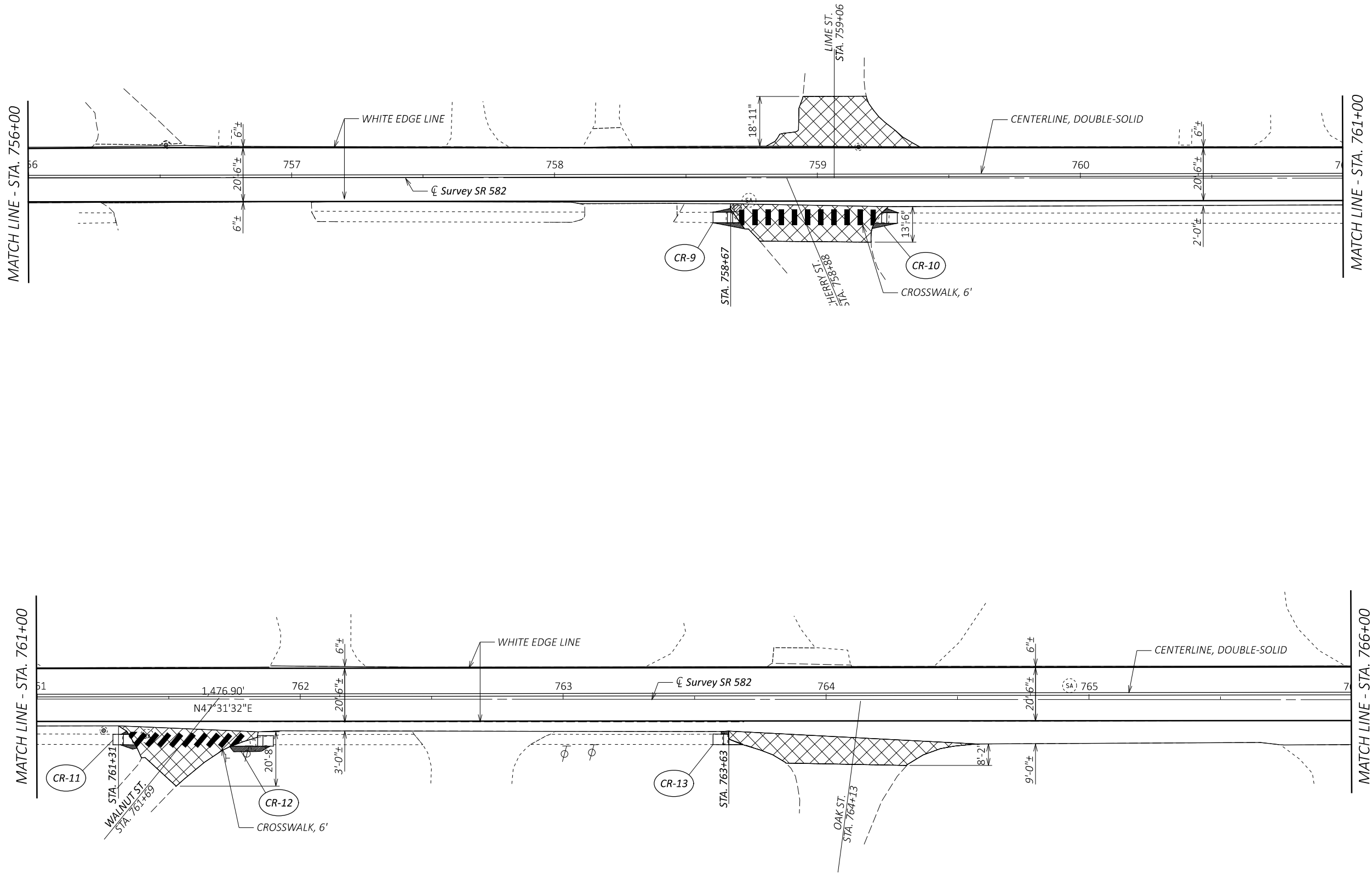
JMF

PROJECT NO.

95800

SHEET TOTAL

18 | 26



PLAN SHEET
STA. 756+00 TO STA. 766+00 SR 582

DESIGN AGENCY



DESIGNER

ALF

REVIEWER

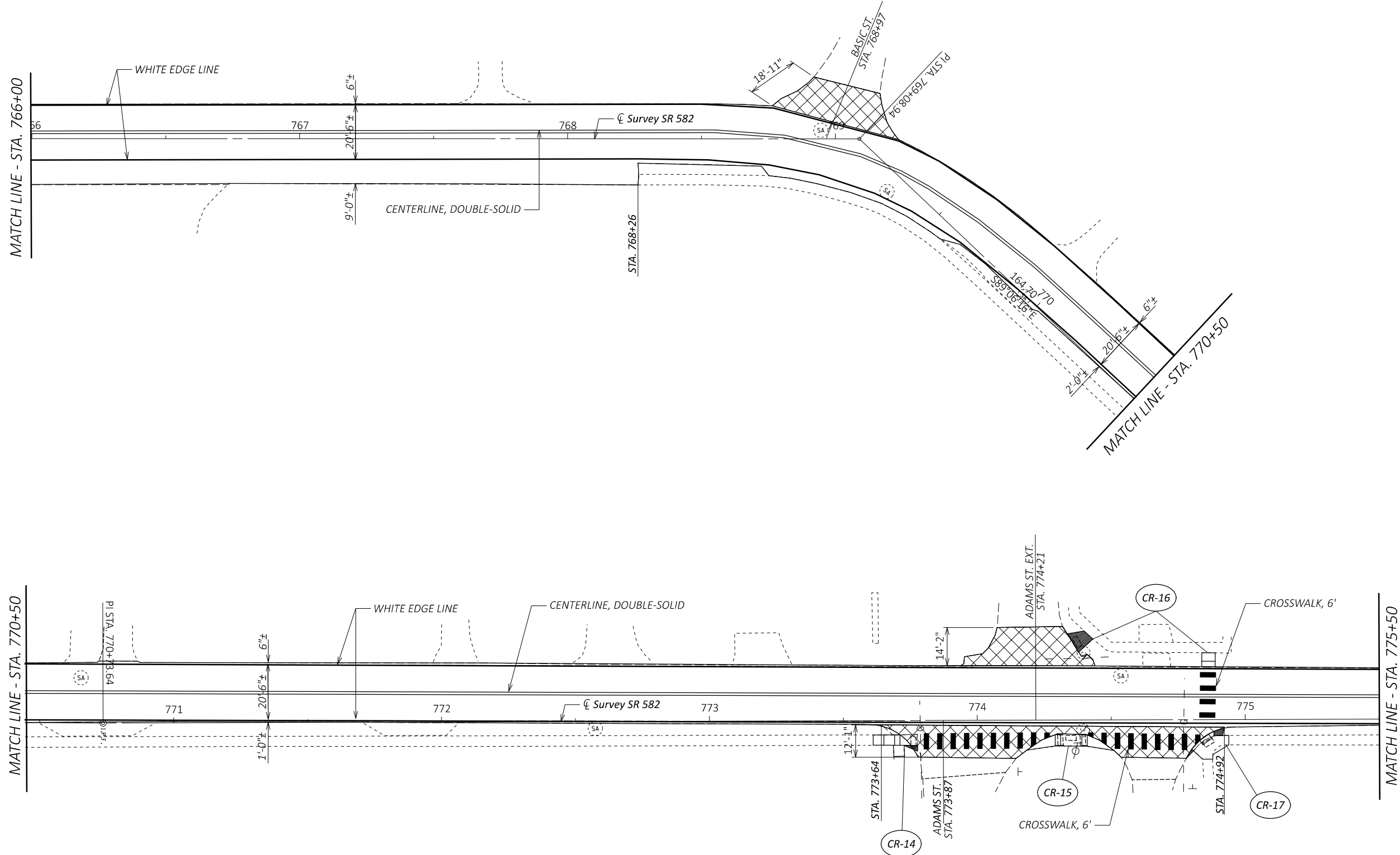
JMF

PROJECT #

95800

SHEET TOTAL

19 26



PLAN SHEET
STA. 766+00 TO STA. 775+50 SR 582

DESIGN AGENCY



DESIGNER

ALF

REVIEWER

JMF

PROJECT ID

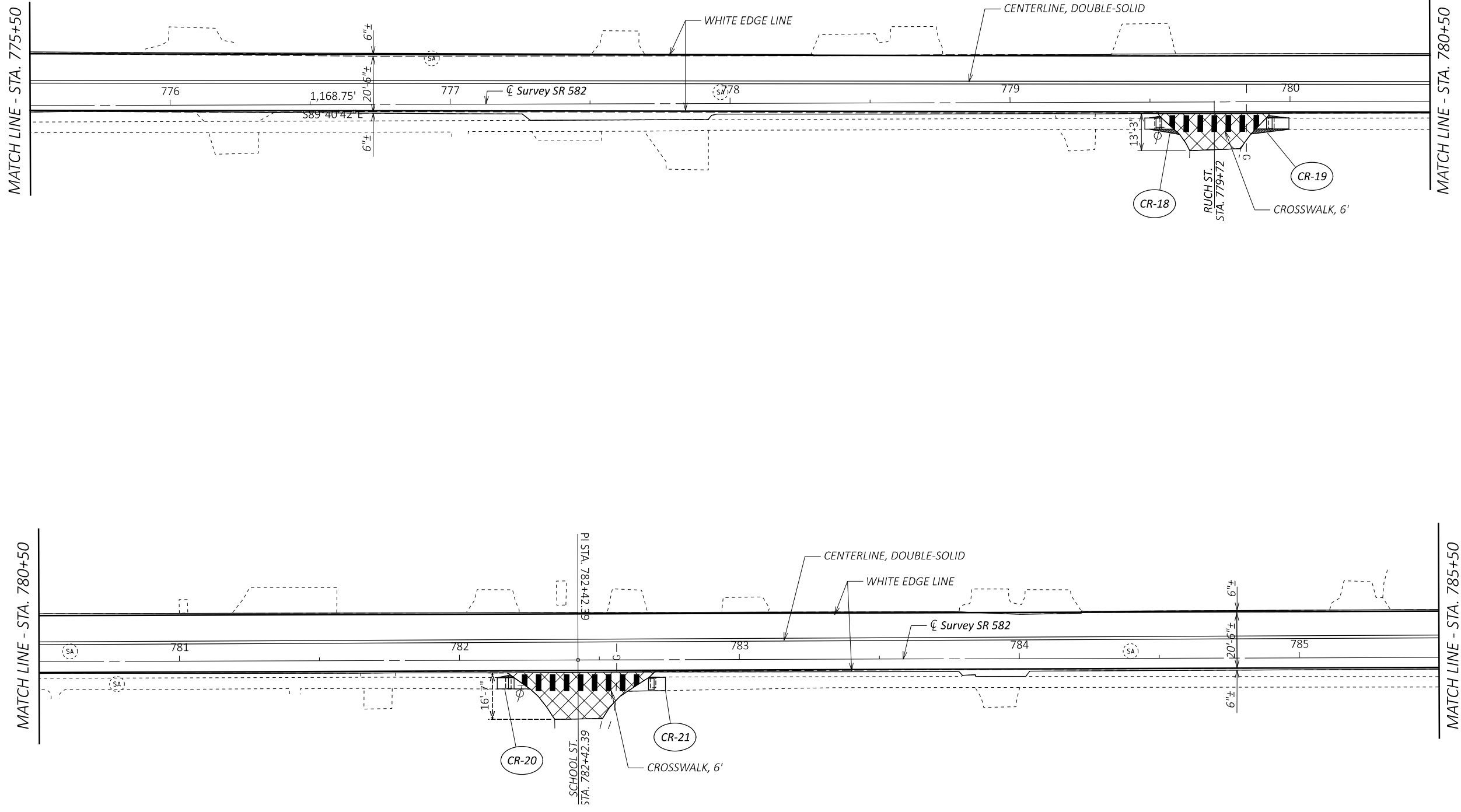
95800

SHEET

20

TOTAL

26



PLAN SHEET
STA. 775+50 TO STA. 785+50 SR 582

DESIGN AGENCY



DESIGNER

ALF

REVIEWER

JMF

PROJECT ID

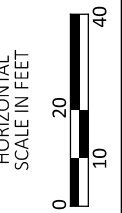
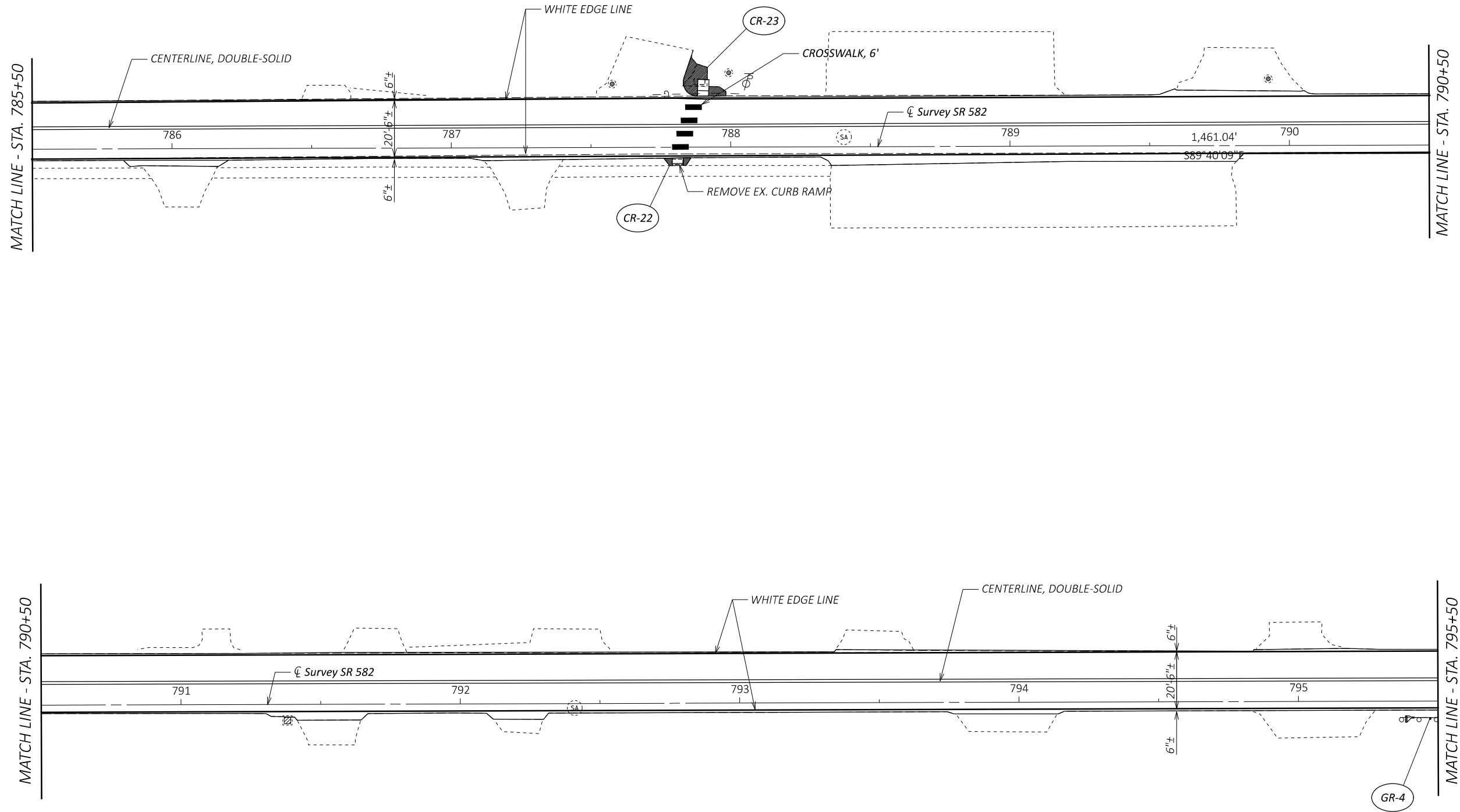
95800

SHEET

21

TOTAL

26



PLAN SHEET
STA. 785+50 TO STA. 795+50 SR 582

DESIGN AGENCY

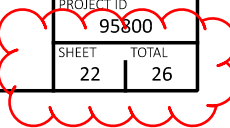


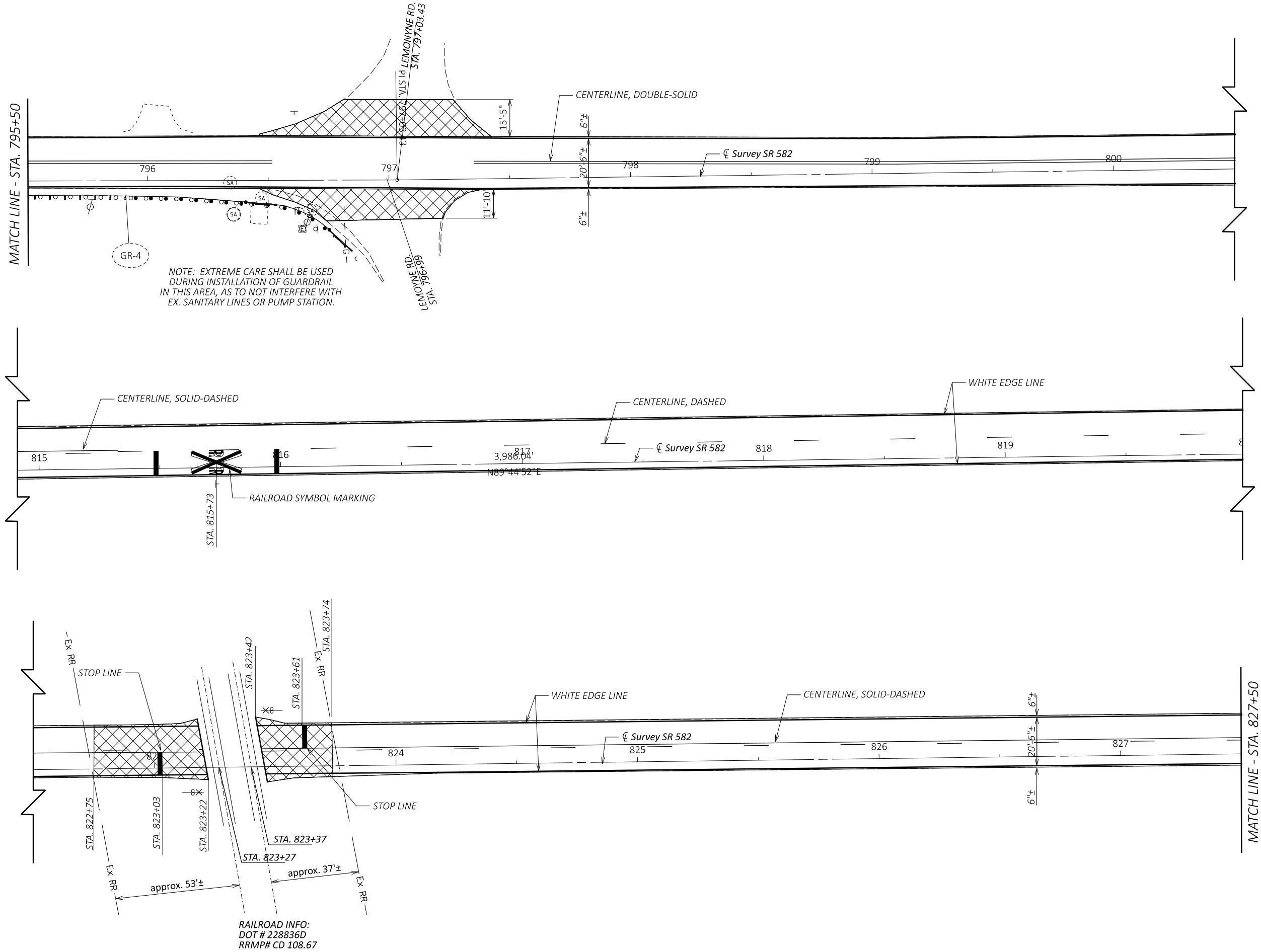
DESIGNER
ALF

REVIEWER
JMF

PROJECT ID
95800

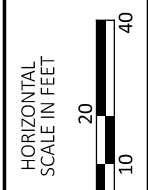
SHEET	TOTAL
22	26





NOTE: EXTREME CARE SHALL BE USED DURING INSTALLATION OF GUARDRAIL IN THIS AREA, AS TO NOT INTERFERE WITH EX. SANITARY LINES OR PUMP STATION.

RAILROAD INFO:
DOT # 228836D
RRMP# CD 108.67



PLAN SHEET
STA. 795+50 TO STA. 827+50 SR 582

DESIGN AGENCY

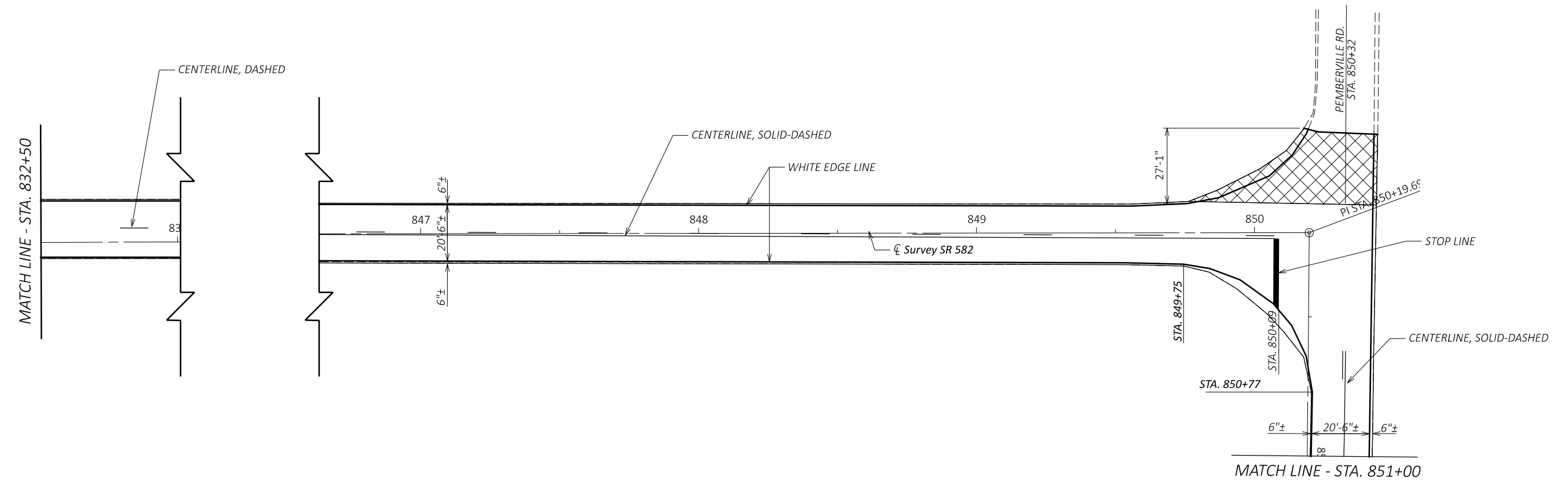
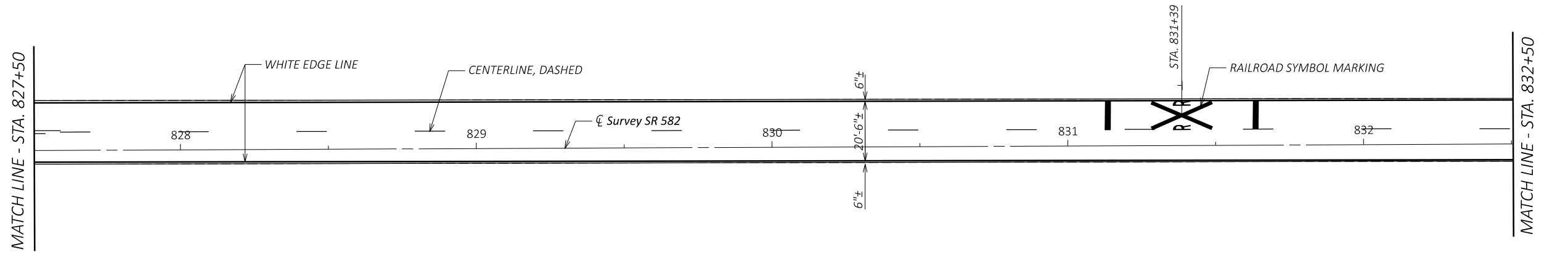


DESIGNER
ALF

REVIEWER
JMF

PROJECT ID
95800

SHEET	TOTAL
23	26



PLAN SHEET
 STA. 827+50 TO STA. 851+00 SR 582

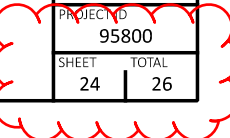
DESIGN AGENCY

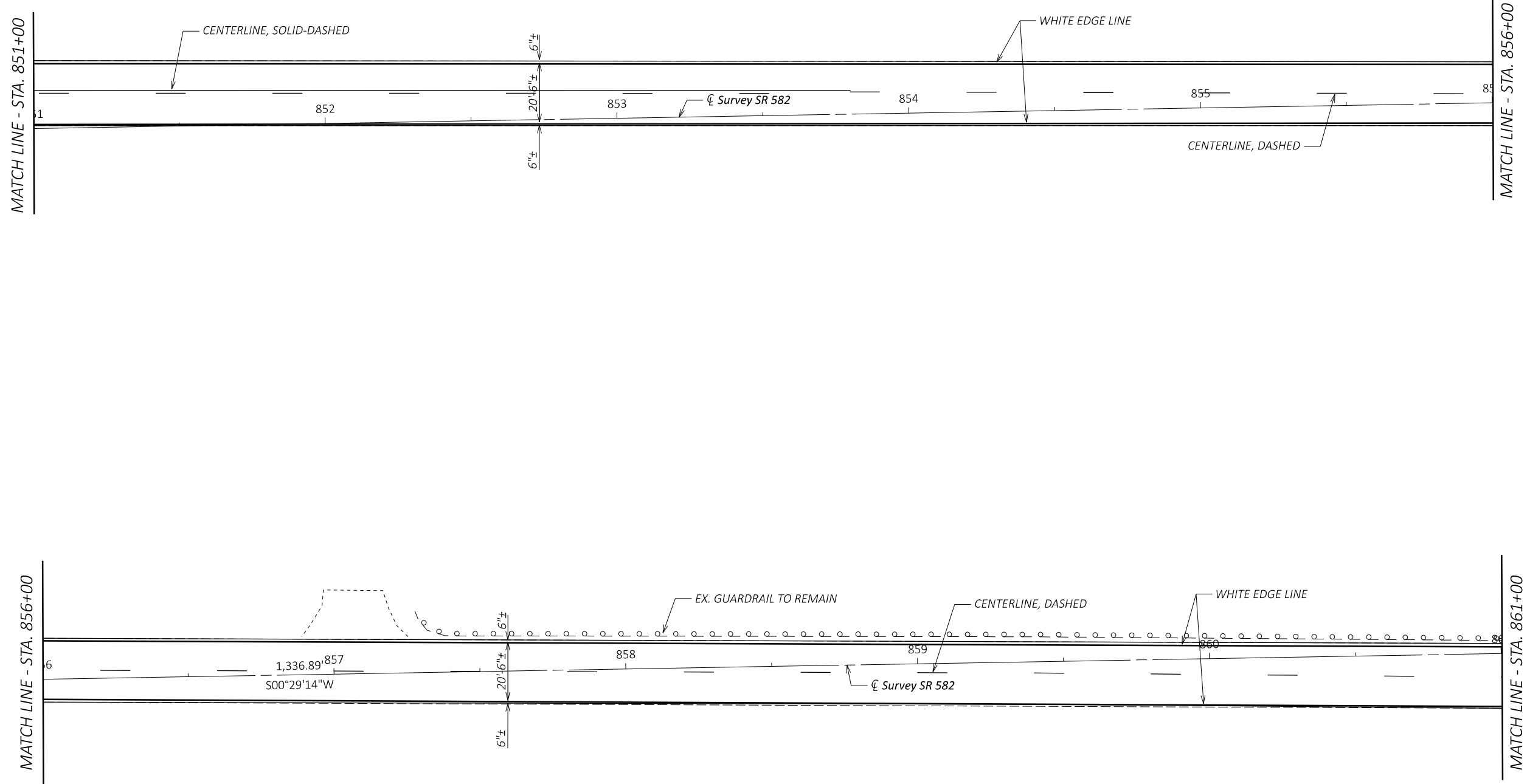


DESIGNER
 ALF
 REVIEWER
 JMF

PROJECT ID
 95800

SHEET TOTAL
 24 26





PLAN SHEET
STA. 851+00 TO STA. 861+00 SR 582

DESIGN AGENCY



DESIGNER

ALF

REVIEWER

JMF

PROJECT ID

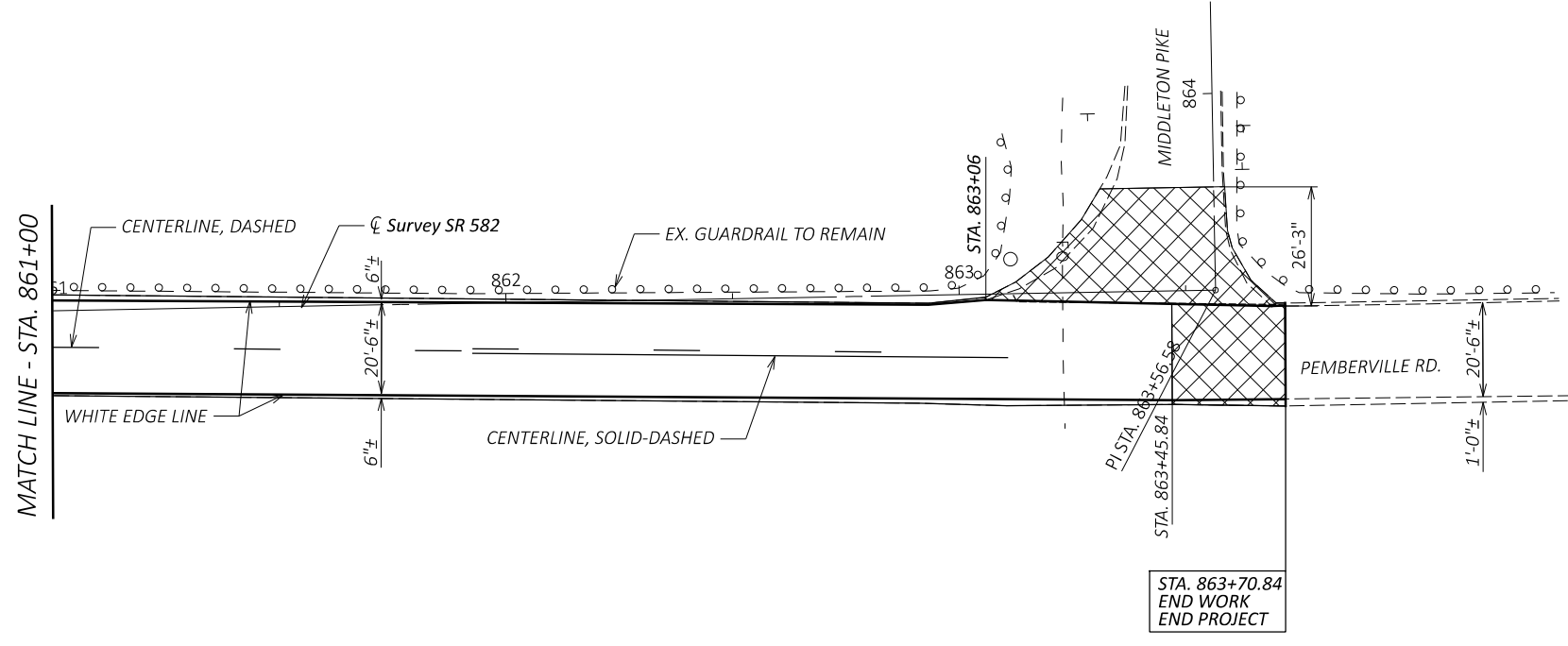
95800

SHEET

25

TOTAL

26



PLAN SHEET
STA. 861+00 TO STA. 863+71.86 SR 582

DESIGN AGENCY



DESIGNER

ALF

REVIEWER

JMF

PROJECT ID

95800

SHEET TOTAL

26

26

