



CUY-90-14.90

PID 77332/85531

APPENDIX ED-04

**Environmental Consultation
(Reference Document)**

State of Ohio
Department of Transportation
Jolene M. Molitoris, Director

**Innerbelt Bridge
Construction Contract Group 1 (CCG1)**



U.S. Department
of Transportation
**Federal Highway
Administration**

Ohio Division

January 21, 2010

200 North High Street
Room 328
Columbus, Ohio 43215
614-280-6896
614-280-6876 Fax
Ohio.FHWA@fhwa.dot.gov

Director Jolene M. Molitoris
Ohio Department of Transportation
1980 West Broad Street
Columbus, OH 43223

In Reply Refer To:
HEO-OH

Subject: Documentation for Environmental Consultation

Dear Director Molitoris:

Reference is made to the Ohio Department of Transportation (ODOT) Documentation for Environmental Consultation documentation submissions for the Cleveland Innerbelt Project, CUY – 71/90 – 16.79/14.90, PID 77510, submitted pursuant 23 CFR 771.129(c), requesting Federal Highway Administration (FHWA) acceptance of the Project changes, and concurrence that such changes result in no new significant environmental impacts and find that the Project September 18, 2009 Record of Decision (ROD) remains valid. The Consultation submissions, letter dated January 7, 2010, and e-mails dated January 12 and 13, 2010 provide for documented disclosure and environmental assessment of Project changes developed for incorporation into the Project work.

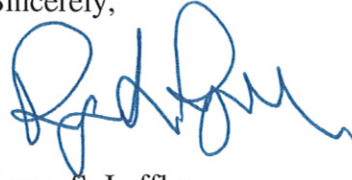
Please be advised that FHWA has completed its review and processing of the submitted Environmental Consultation documentation. Based upon the review, FHWA finds the disclosure of the Project changes, which came about during FHWA's and ODOT's logical progression of Project detailed design and right of way work activities, to be acceptable. FHWA also finds that the Consultation documentation provides for appropriate and acceptable environmental study and disclosure of their resultant impacts. In addition, FHWA finds that the Project changes will either result in the refinement of, or result in the lessening of, Project impacts, in terms of context and intensity, evaluated in the Environmental Impact Statement (EIS) without causing other environmental impacts that are significant and were not evaluated in the EIS. It is thus FHWA's determination that a supplemental EIS and/or the preparation of a revised ROD are not required or necessary. The Project changes as disclosed, assessed, and documented do not substantially change the selected Project alternative, project mitigation measures, or the findings discussed in the September 18, 2009 ROD.

Based upon FHWA's above findings and determinations, the ODOT may advance the Cleveland Innerbelt Project, Construction Contract Group 1 Design Build Contract PS&E without further environmental review, analysis, or documentation. In addition, the Project changes as disclosed within the Environmental Consultation documentation may be incorporated into the Project work.



Should you have any questions regarding the FHWA's consideration and management of the Environmental Consultation documentation, please contact Mr. Michael B. Armstrong, Civil Engineer (Highway) at (614) 280-6896, or by email Michael.Armstrong@dot.gov.

Sincerely,



For:

Laura S. Leffler
Division Administrator

Ecc:

Michael.Armstrong@dot.gov

Patrick.Bauer@dot.gov

Larry.Hoffman@dot.state.oh.us

Mark.Carpenter@dot.state.oh.us

Craig.Hebebrand@dot.state.oh.us

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Roger.Ryder@dot.gov

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David.Snyder@dot.gov

Laurie.Leffler@dot.gov

Bill.Ujvari@dot.state.oh.us

Project File: CUY – 71/90 – 16.79/14.90, PID 77510

File Name: 2010 01 21 CUY – Cleveland Innerbelt PID 77510 Envi Consultation Acceptance
(Ver 1).doc



<Michael.Armstrong@dot.gov
>

01/13/2010 09:14 AM

To <Larry.Hoffman@dot.state.oh.us>

cc <Craig.Hebebrand@dot.state.oh.us>,
<Mark.Carpenter@dot.state.oh.us>,
<patrick.bauer@dot.gov>, <Tim.Hill@dot.state.oh.us>,
<Richard.Henry@dot.gov>, <Laurie.Leffler@dot.gov>,
<Roger.Ryder@dot.gov>, <Andy.Blalock@dot.gov>,
<Michael.Armstrong@dot.gov>

bcc

Subject Informal acceptance of --> Documentation for Environmental
Consultation, CUY-71/90, PID 77510 (CIB)

Larry,

Thank you. The submission satisfactorily addresses and resolves all prior FHWA comment associated with Cleveland Innerbelt Project modifications/enhancements/refinements etc... made since the issuance of the ROD. FHWA will draft and provide ODOT with a formal replay with appropriate acceptance citations as soon as possible. No further environmental documentation is necessary in order to manage the Project modifications/enhancements/refinements etc... as we currently understand them.

The FHWA will validate the ODOT's appropriate management of the modifications/enhancements/refinements etc... within the Innerbelt Project contract documents during our final review of the Innerbelt CCG1 Design Build Contract PS&E submission for construction authorization.

Signed: _____

For: **Laura S. Leffler, Division Administrator**

**United States Department of Transportation
Federal Highway Administration
Ohio Federal-aid Division
200 North High Street, Room 328
Columbus, OH 43215**

Telephone: (614) 280-6896 Fax: (614) 280-6876 E-mail: ohio.fhwa@dot.gov

**Submit information and action requests to the above mailing address or to the FHWA Ohio Federal-aid Division electronic mailbox: ohio.fhwa@dot.gov
Submissions to the FHWA are typically processed within 30 days of receipt.**

Media, Congressional, Legal, Security Inquiries, and Request for Interview are directed to contact the FHWA Office of Public Affairs at the following address:

**United States Department of Transportation
Federal Highway Administration
Office of Public Affairs
1200 New Jersey Ave., SE**

Washington, DC 20590

Telephone: (202) 366-0660

From: Larry.Hoffman@dot.state.oh.us [mailto:Larry.Hoffman@dot.state.oh.us]
Sent: Wednesday, January 13, 2010 8:21 AM
To: Armstrong, Michael (FHWA)
Cc: Craig.Hebebrand@dot.state.oh.us; Mark.Carpenter@dot.state.oh.us; Armstrong, Michael (FHWA); Bauer, Patrick (FHWA); Tim.Hill@dot.state.oh.us
Subject: Re: Question --> RE: Update to 1/8/10 Documentation for Environmental Consultation, CUY-71/90, PID 77510 (CIB)

Mike,

Correct.

Larry

<Michael.Armstrong@dot.gov> To<Larry.Hoffman@dot.state.oh.us>
cc<Craig.Hebebrand@dot.state.oh.us>, <Mark.Carpenter@dot.state.oh.us>,
01/13/2010 08:08 <Tim.Hill@dot.state.oh.us>, <patrick.bauer@dot.gov>, <Michael.Armstrong@dot.gov>
AM SubjQuestion --> RE: Update to 1/8/10 Documentation for Environmental Consultation, CUY-71/90, PID
ect77510 (CIB)

Larry,

Is it correct to state that the assessment of environmental impacts for the Abbey Avenue bike lanes within the January 8, 2010 submission covers the improvement from West 11th Street to Gehring Avenue, (e.g. "Gehring Avenue" should replace all references to "West 20th Street" as the bike lane western termini.)?



Signed: _____
For: Laura S. Leffler, Division Administrator

United States Department of Transportation

**Federal Highway Administration
Ohio Federal-aid Division
200 North High Street, Room 328
Columbus, OH 43215**

Telephone: (614) 280-6896 Fax: (614) 280-6876 E-mail: ohio.fhwa@dot.gov

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**United States Department of Transportation
Federal Highway Administration
Office of Public Affairs
1200 New Jersey Ave., SE
Washington, DC 20590**

Telephone: (202) 366-0660

From: Larry.Hoffman@dot.state.oh.us [mailto:Larry.Hoffman@dot.state.oh.us]
Sent: Tuesday, January 12, 2010 4:16 PM
To: Armstrong, Michael (FHWA); Bauer, Patrick (FHWA)
Cc: Craig.Hebebrand@dot.state.oh.us; Mark.Carpenter@dot.state.oh.us; Tim.Hill@dot.state.oh.us
Subject: Update to 1/8/10 Documentation for Environmental Consultation, CUY-71/90, PID 77510 (CIB)

Mike,

Per this morning's CCG1 PS&E review meeting, we are sending this email to serve as an Update to the January 8, 2010 Documentation for Environmental Consultation, CUY-71/90, PID 77510, (CIB).

The following text and figure serves to clarify Abbey Avenue bike accommodations found on pages 1 -3 of the 1/8/10 Documentation.

The proposed bike lanes and widened bridge shoulder included in CCG 1 will provide connectivity between West 11th Street and the RTA station on Gehring Street. The project will also include bike route signage along the City of Cleveland's designated directional bike route between the Lorain-Carnegie bridge and Abbey Avenue.

The signage will direct bike riders from Abbey Avenue northbound on West 20th Street to the existing eastbound bike route on the Lorain-Carnegie bridge. From the westbound Lorain-Carnegie bridge the signage will direct bike riders north on West 20th Street, east on Lorain Avenue, south on West 18th Street, west on Smith Court, and south on West 19th Street to Abbey Avenue. See Figures X1, X2 & Y.

The Department acknowledges that parking lots are not eligible for federal transportation enhancement funds. Original parking lot discussion can be found on pages 5-10 of the 1/8/10 Documentation.

The below text serves as an update to the impact on the Scranton Averell property as was discussed on page 9 and 10 of the 1/8/10 Documentation. The property is located at 1515 Farfield Avenue, Cleveland, OH, 44113. Post- ROD engineering work has identified this property as a necessary take in order to accommodate the CCG 1 project. This property and buildings are not of an environmental concern given the site is not of a natural or social importance. The property will be acquired per the Uniform Act and will be available to the contractor to design and build on as required to construct the overall CCG 1 project, including possible retaining walls and parking area.

We believe this satisfies your request per this mornings meeting .

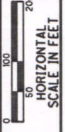
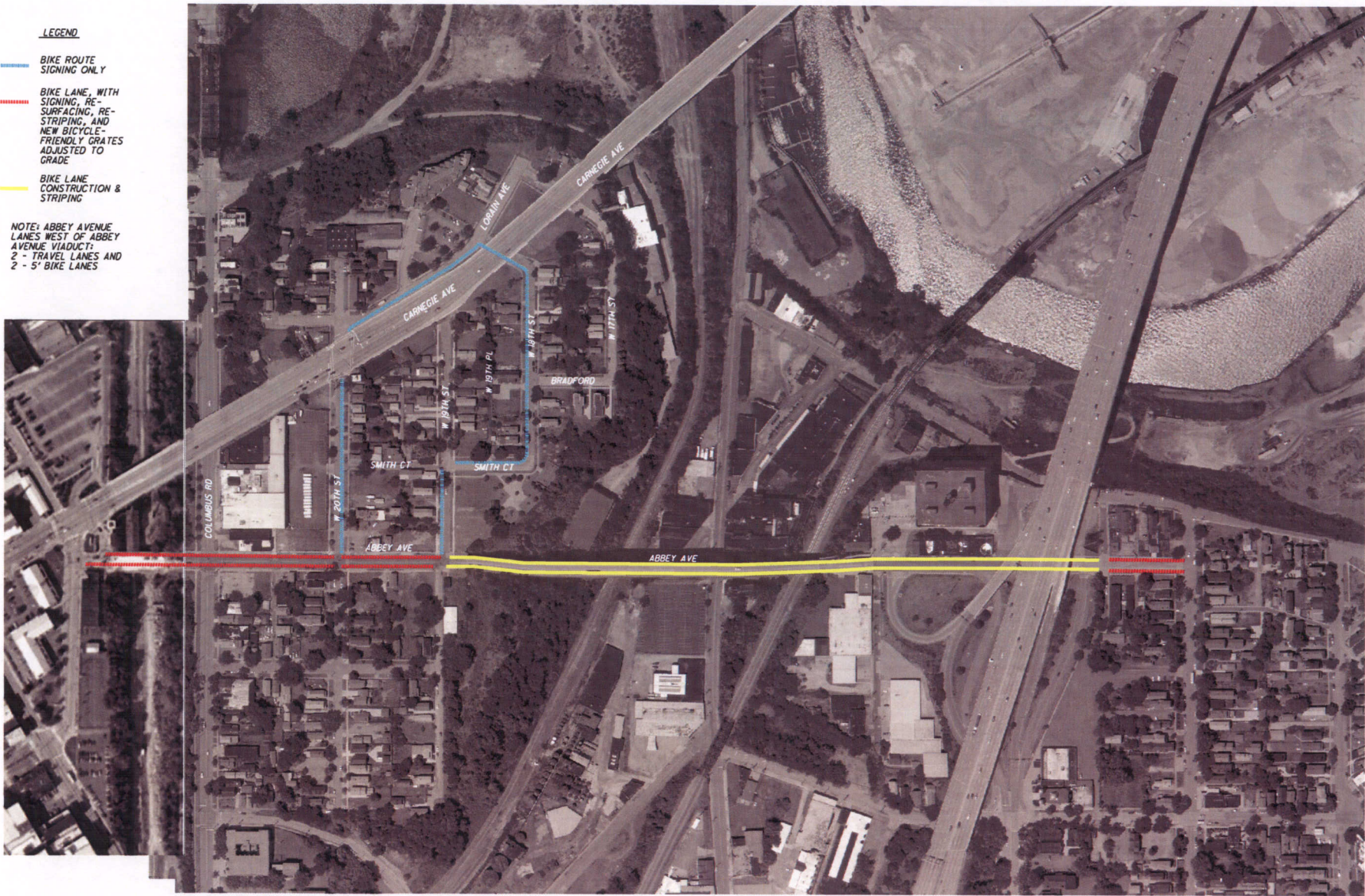
Thanks for your consideration ,

Larry

466-6439[attachment "image001.png" deleted by Larry Hoffman/Environmental/CEN/ODOT]



image001.png

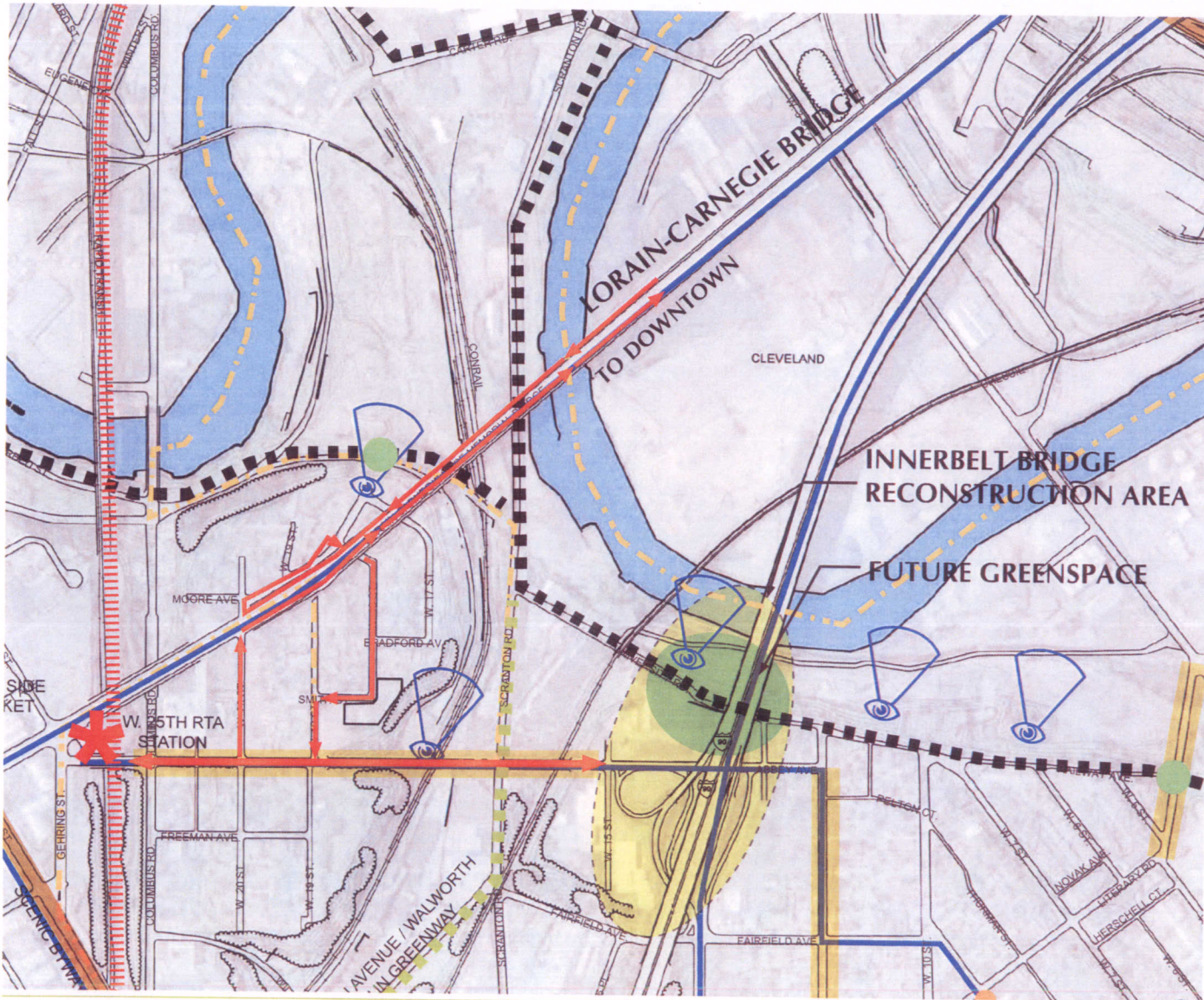


TREMONT BIKE CONNECTIVITY PLAN

CUY-9-14.52



FIGURE X1



ROUTE ANALYSIS MAP - ABBEY ROAD TO DOWNTOWN CLEVELAND

Key

- Tremont Service Area
- Bus Route
- Red Line
- Scenic Byway
- Proposed Towpath Trail
- Proposed Train Avenue / Walworth Run Greenway
- Streetscape Improvement
- Connection Improvement
- Towpath Trailhead
- Scenic Views

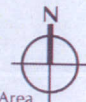
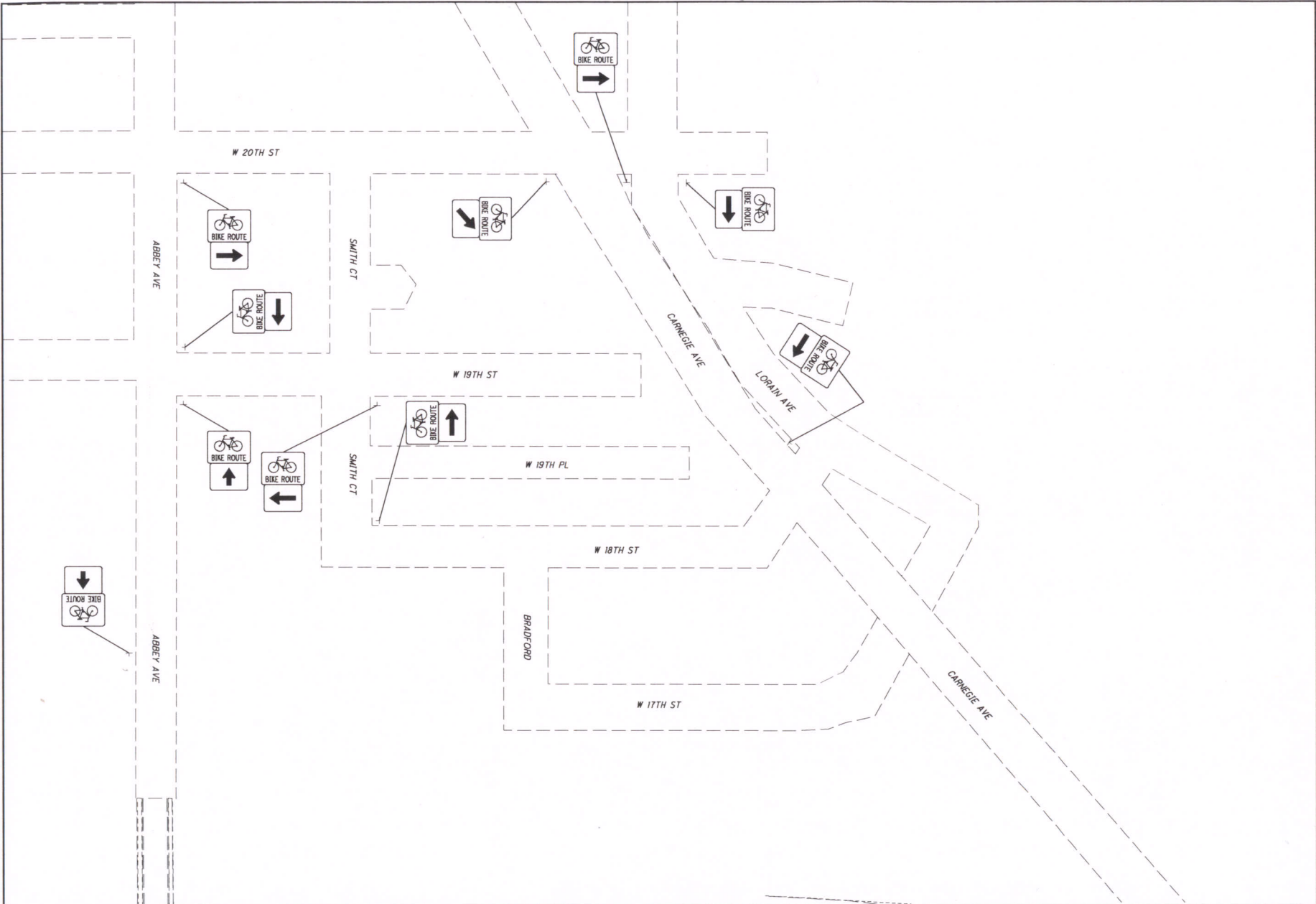
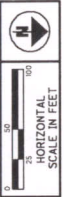


FIGURE X2

Figure Y



Conceptual Signing Sheet.dgn 1/5/2010 3:15:13 PM



CONCEPTUAL SIGNING PLAN

CUY-90-14.52

3
13



OHIO DEPARTMENT OF TRANSPORTATION

CENTRAL OFFICE • 1980 WEST BROAD STREET • COLUMBUS, OH 43223

TED STRICKLAND, GOVERNOR • JOLENE M. MOLITORIS, DIRECTOR

January 7, 2010

Laura Leffler, PE
Division Administrator
Federal Highway Administration
200 North High Street
Columbus, Ohio 43215

Attn: Michael B. Armstrong

Re: Documentation for Environmental Consultation
CUY-71/90, PID 77510, Cleveland Innerbelt (CIB)

Dear Ms. Leffler:

This letter supersedes our November 3, 2009 letter to you on the subject project. Pursuant to 23 CFR 771.129(c), please consider the information contained herein as Documentation for Environmental Consultation for the subject project. FHWA issued the Record of Decision (ROD) for the CIB project on September 18, 2009. Since July, ODOT has been preparing the design-build scope of services for the Cleveland Innerbelt Construction Contract Group 1 (CCG1), PID 85531, to build the new westbound bridge over the Cuyahoga River Valley. Five preliminary design decisions for CCG1 that were not specifically addressed in the Final Environmental Impact Statement (FEIS) have been made and are discussed below:

1. Abbey Avenue bike accommodations
2. East 9th Street Multi-Use Path
3. Commercial Road - removal of cul-de-sac
4. Lorain-Carnegie Bridge over GCRTA modification to roadway, sidewalks and vandal protection fence.
5. Parking

Abbey Avenue bike accommodations

Based on the public preference that improved pedestrian and bicycle connectivity be included as a part of the project, the Aesthetics Enhancement and Mitigation (AEM) Subcommittee identified the connection between the existing bike route on West 11th Street and the proposed bike route on West 20th Street as a priority. The AEM Subcommittee recommended including this work in the CCG1 project. ODOT has agreed to fund the accommodations as enhancement work to CCG1 and is intending to use ARRA money.

The FEIS did not specifically discuss bike lanes on Abbey Avenue nor were pavement markings shown on Abbey Avenue in the FEIS graphics. The FEIS did discuss that bike facilities are not included on the new Innerbelt Bridge and that Abbey Avenue to Lorain Avenue was the alternative route for bicycles and pedestrians.

The existing pavement width of Abbey Avenue will remain at 40-feet. The existing cross sectional width of Abbey Avenue between W 11th Street and West 20th Street, not including the Abbey Avenue Bridge over Scranton Road, will be redistributed to provide a 5-foot bike lane, 11-foot travel lane, 11-foot travel lane, 5-foot bike lane, and 8-foot parking lane. The proposed typical section is attached Figure A. The Abbey Avenue Bridge over the Scranton Road will be reconfigured also. The bridge currently has two 5-foot sidewalks, two 14-foot lanes, and two 18-inch splash guards for a total width of 41-feet, face-to-face of parapet. The splash guards and parapet will be removed and the existing bridge width will be redistributed to provide for a 5-foot-sidewalk, 5-foot bike lane, 11-foot travel lane, 11-foot travel lane, 5-foot bike lane, and 5-foot sidewalk. A higher parapet with vandal protection fence will replace the existing. The existing and proposed typical section of the Abbey Avenue Bridge is attached Figure B.

The proposed bike lanes and widened bridge shoulder will provide connectivity between W 11th Street and W 20th Street. ODOT will include the extension of the bike lanes along Abbey Avenue to Gehring Avenue in CCG1. The project will also include bike lane signage along the City of Cleveland's designated bike route on West 20th Street and West 19th Street. These improvements will connect to existing bike lanes and or bike routes as envisioned by the City of Cleveland in their Bikeway Master Plan.

All proposed work will be within existing pavement or structure and simply involve the redistribution of the existing width to better accommodate all modes. No new right-of-way will be required to add the bike lanes to Abbey Avenue. The area of the Abbey Avenue bike lanes between West 11th Street and the Abbey Avenue Bridge was included in the original study area for the entire project as shown in the FEIS. This area includes significant excavation to stabilize the slope. The work required to provide the bike lanes will be within the limits of the existing pavement: therefore, no new environmental impacts are anticipated. The work on the Abbey Avenue Bridge and westward to West 20th Street is outside the study area identified in the FEIS. However, the work required to provide the wide shoulder on the Abbey Avenue Bridge and the bike lanes between the bridge and West 20th Street are completely on the deck of the structure or within existing pavement limits. The only work near or in the National Register Tremont Historic District are the connection of West Thirteenth Place (one way NB) into University Road (one way EB), the painting of bike lane lines and symbols along Abbey Avenue east from West Fourteenth Street to West 11th Street. The first involves work extending the west side curb of West Thirteenth Place along a new curve return into the north side curb of University Road just outside the District.

The next includes painting of bike lane markings within the existing pavement to connect to the West 11th Street bike route. Given that the lane painting will not take or impact any of the elements which contribute to the historic district, no environmental impacts will occur. The planned bike accommodations for CCG1 are in compliance with the CIB PA (Agreement Number 15498).

East 9th Street Multi-Use Path

In order to improve pedestrian and bicycle connectivity between the Cuyahoga River Valley and the Central Business District as preferred by the local stakeholders, the AEM Subcommittee recommended the inclusion of a 10-foot wide multi-use path parallel to the proposed extension of East 9th Street between Canal Road and Broadway, then along Broadway from East 9th Street to Carnegie Avenue. With the exception of the portion of the multi-purpose trail along Broadway between East 9th Street and the new westbound bridge, the proposed multi-purpose trail will be constructed as a part of Construction Contract Group 1. The remaining portion between East 9th Street and the new westbound bridge will be constructed as a part of Construction Contract Group 2; however, a temporary connection will be constructed during Construction Contract Group 1. A drawing of the proposed extension of East 9th Street and the multi-use path are attached Figure C.

The FEIS did not include the proposed multi-use path. The design concept in the FEIS included 5-foot sidewalks on either side of the extension of East 9th Street. At a July 29, 2009 meeting, the City of Cleveland indicated their agreement with the recommendation to provide a single multi-use path instead of the sidewalks on both sides of the road. The multi-use trail is more desired by the City of Cleveland compared to sidewalks because it is a better connection to the Cleveland Bikeway Master Plan. It also provides a corridor for pedestrians and bicycles separated from heavy trucks going up and down the steep hill. There are no residential properties on this extension of East 9th Street and minimal driveways, so there will be no impact to the public by incorporating the multi-use path.

The area of the proposed multi-use path along the extension of East 9th Street was included in the original study area for the entire project as shown in the FEIS. The area needed to construct the extension of East 9th Street and the multi-use path is within the area identified in the FEIS to construct the extension of East 9th Street and two-5-foot sidewalks. There was no graphic showing the roadway and sidewalk configuration in the FEIS, only proposed construction limits. The multi-use path will be constructed within these construction limits. Since the project construction limits do not change, there are no new environmental impacts.

Commercial Road – removal of cul-de-sac

Commercial Road will remain a continuous public street between Carnegie Avenue and the extension of East 9th Street in order to provide access to the existing fire station, the proposed fire museum, the CEI Hazel Substation, and existing privately owned parcels.

The design concept in the FEIS utilized a cul-de-sac and driveway connection under the existing IR-90 Bridge in order to provide access. Consistent with the FEIS, the primary connection between the Cuyahoga River Valley and Central Business District will be by way of an extension of East 9th Street from Broadway to Canal Road. Commercial Road will be realigned to intersect the extension of East 9th Street, providing access to the fire station, fire museum, utility substation and other privately owned parcels. See attached Figure C.

The cul-de-sac adjacent to the CEI substation site would have required a large amount of cut to create a relatively flat circle to allow all vehicle movements. CEI then identified two 138 kV pipe cable systems that contains a pressurized insulating oil system at this location. To relocate these cable lines, the insulating system would require the replacement of the entire segment of cable. This segment of cable extends 8000-feet between the Hazel Substation and the Hamilton Substation in the northeast corner of downtown. Due to the 'high critical' nature of this facility and adjacent steep terrain, ODOT removed the cul-de-sac concept. At a July 29, 2009 meeting, the City of Cleveland indicated their approval of maintaining Commercial Road as through street under the bridge.

The proposed work in the area of Commercial Road was included in the original study area for the entire project as shown in the FEIS. The area needed to maintain Commercial Road as a continuous public street is within the area identified in the FEIS to construct the Commercial Road with two cul-de-sacs. The continuous Commercial Road concept will be constructed within the study area shown in the FEIS. This roadway concept change will not have an operational impact on the project as documented in the approved IJS. This determination was made by ODOT in conjunction with the engineering consultant team and is based in part by the fact that an increase of more than 400 vehicles in the peak (PM) hours would need to exit to Commercial Road and attempt to enter I-90 west bound at Ontario Street before the level of service would be degraded. Given that all of the truck traffic will be required to use the new East 9th Street extension and given that only about 1500 automobiles per day exit Commercial (source Flats Transportation Study Data, 1998). So even without the reduction in the volume, no change in the level of service is reasonably anticipated to occur. Furthermore, the project construction limits do not change, thus there are no new environmental impacts.

Lorain-Carnegie Bridge over GCRTA modification to roadway, sidewalks and vandal protection fence

The Commercial Road intersection with Carnegie Avenue will be shifted to the west and the existing pavement area will be reconstructed to narrow the intersection to reflect the new roadway configuration. Parts of the former roadway are to include a raised sidewalk/plaza area. In addition, the vandal protection fence will be replaced. See Figure C. The replacement of vandal protection fence was also not specifically discussed in the FEIS.

The change from pavement to sidewalk/plaza area on top of the NRHP bridge was shown in the FEIS graphics, although not discussed in the narrative. The area was proposed by the AEM Subcommittee to be designed as an open plaza for public displays, i.e. vintage fire truck or public art. The aesthetic, enhancement, and mitigation items recommended by the AEM Subcommittee were accepted by the City of Cleveland Planning Commission. ODOT will ensure that all work is performed as stipulated in the Programmatic Agreement for historic impacts on the Innerbelt Project (Agreement Number 15498).

The area of work on the Lorain-Carnegie Bridge was included in the original study area for the entire project as shown in the FEIS. The subject work on the bridge will be constructed within the construction limits shown in the FEIS. Per recent discussions with your office it was recognized that no changes will occur to the structural elements that contribute to the bridges historic eligibility. And since the project construction limits do not change, there are no new environmental impacts.

Parking

New parking areas are included as enhancements with the Cleveland Innerbelt Construction Contract Group 1 (CCG1). A Parking Impact matrix attached, see Figure D. Each parking area is discussed below.

Gateway Animal Clinic

Existing

There is private parking between the Gateway Animal Clinic building and the Cold Storage building north of Crown Avenue. All of the existing parking spaces are on private property owned by Scranton Averell. West 15th Street is adjacent to the Gateway Animal Clinic building and connects Abbey Avenue to Crown Avenue.

Project Impact

Sixty (60) parking spaces will be removed for the project in order to stabilize the slope. West 15th Street and Crown Avenue will be abandoned and the pavement removed. Without the replacement of parking, this would require a full take of the Gateway Animal Clinic.

Proposed

The project is designed to allow a paved parking lot adjacent to the Gateway Animal Clinic Building on the area of existing West 15th Street and Crown Avenue. This will provide 31 parking spaces. This would remain private property owned by Scranton Averell. (Figure E)

The project design allows for another 30-space parking lot on the south side of Abbey Avenue across from the Gateway Animal Clinic. This is currently privately owned property and will continue to be privately owned by Scranton Averell. (Figure E)

Right-of-way Cost to Cure

The appraisal of the Scranton Averill property will be determined for 2 situations. One will be determined on financially compensating for the total take of the land owned by Scranton Averill and the building owned by Gateway Animal Clinic due to loss of parking causing the loss of economic viability of the existing business. The second will be determined on constructing replacement parking so that a total take can be avoided. ODOT will proceed with the replacement parking using federal funding up to the amount of damages for a total take. If the replacement parking exceeds the amount of damages for total take, ODOT will fund the excess cost with 100% state funding.

Public Parking under the Innerbelt Bridge in Tremont

Existing

There is no regulated off-street public parking under the existing Innerbelt Bridge or in the area of the new westbound bridge in Tremont. The current condition under the bridge is an open dirt area that is not maintained for any particular use. It is dark under the bridge with minimal lighting. The area currently attracts the homeless. While there are no curb cuts for access, the area is not restricted by fence or barrier, and vehicles do haphazardly park in the area.

Project Impact

The project will not impact regulated off-street public parking under the Innerbelt Bridge in Tremont. The area under the bridge will need to be regraded and stabilized due to construction activities. The unregulated parking that occurs would not be allowed during construction. The unregulated areas under the existing bridge will also be eliminated during the Construction Contract Group 2 (CCG2).

Proposed

The local stakeholders requested that the area be paved and lighted to prevent the homeless from occupying the area. The stakeholders also request that it be constructed to allow parking for the various events in Tremont. The Tremont West Development Corporation will maintain the parking lot in the future under a maintenance agreement with ODOT. ODOT will continue to own the property. (Figure E)

The area under the Innerbelt Bridge in Tremont will be graded, paved, fenced, lighted and equipped with security cameras for safety and to prevent homeless people inhabiting the area. In addition, as requested by the stakeholders, the paved area will be designed to allow public parking. The parking design would follow the city parking standards. Approximately 55 parking spaces will be constructed under CCG1. Another 82 parking spaces will be added under CCG2. Tremont West Development Corporation already maintains a security camera system and the proposed cameras will be added to their system.

Enhancement

The Aesthetics, Enhancement and Mitigation Subcommittee chose to include the parking lot under the Innerbelt Bridge in the ARRA Enhancement funding identified for the project. The City of Cleveland Planning Commission approved the parking lot as part of the enhancements for the project.

Public Parking on Abbey Avenue

Existing

On-street public parking is allowed on both sides of Abbey Avenue between West 11th Street and West 15th Street. There are no pavement markings to regulate the parking. All parking is within City of Cleveland right-of-way.

Project Impact

Parking will not be allowed on the south side of Abbey Avenue. This will result in the loss of approximately 11 parking spaces on the south side of Abbey Avenue.

Proposed

The project will redistribute the pavement to include parallel parking on the north side of Abbey Avenue. The new off-street parking lot (B) under the bridge will replace the loss of on street parking in this location. Abbey Avenue will continue to be within City of Cleveland roadway right-of-way. The off-street parking will be owned by ODOT and maintained by the Tremont West Development Corporation under an agreement. (Figure E)

Enhancement

The improved parking on Abbey Avenue was included in the recommendations from the Aesthetics, Enhancement and Mitigation Sub-Committee and approved by the Cleveland planning Commission. It is included in the allotted enhancement funding for the project.

Parking lot on Commercial Road owned by NS Railroad

Existing

The NS Railroad property under the Innerbelt Bridge located on Commercial Road is leased to the Gateway Corporation for employee parking. Public parking is not allowed on this parcel at this time. The parcel is privately owned by NS.

Project Impact

A portion of the property will be taken to construct the East 9th Street extension into the valley. There will be no vehicle access to the property from Commercial Road during construction. Vehicle access from Commercial Road in the future will be allowed but limited due to turning restrictions at the Carnegie Avenue/Commercial Road intersection. Pedestrian access to Commercial Road from the property will be maintained during and after construction. The project will eliminate approximately 133 parking spaces from the NS property between Commercial road and the existing Innerbelt Bridge. The property impacted by the East 9th Street extension does not have marked parking spaces. It is estimated that 130 parking spaces in this location will be eliminated. Therefore, a total of 263 parking spaces on the NS Railroad property will be eliminated.

Proposed

The local stakeholders requested that the area under the bridge be designed to include public parking. The project will sign the area for vehicle access to the property from south of the Innerbelt via the new East 9th Street extension. The area under the Innerbelt Bridge will be constructed for about sixty-four (64) public parking spaces under CCG1. Another seventy-two (72) parking spaces are planned under CCG2 (Figure F). The area under the Innerbelt Bridge will be owned by ODOT. The remainder of the parcel to the south will remain privately owned by NS Railroad. Three bus parking slots will be constructed on the north/west side of Commercial Road separate from the parking lot to simplify the movements for buses. (Figure F) Driveway access will be provided to the parcel from the new East 9th Street Extension and well as from the Commercial Road.

Enhancement

The Aesthetics, Enhancement and Mitigation Subcommittee chose to include the parking lot under the Innerbelt Bridge in the ARRA Enhancement funding identified for the project. The City of Cleveland Planning Commission approved the parking lot as part of the enhancements for the project.

Right-of-way Compensation

The 130 spaces impacted on the south end of the parcel for the East 9th Street Extension will be compensated for through the right-of-way process. No replacement parking will be constructed with the project.

Cuyahoga Community College Administration Building

Existing

The Cuyahoga Community College (CCC) Administration Building has a parking lot with 103 spaces all located on the western and southern portion of their property located at 700 Carnegie Avenue. The parking is on private property owned by CCC.

Project Impact

In order to construct the new WB Innerbelt Bridge, approximately 31 parking spaces on the south side of the parking lot will be eliminated. Without the replacement of parking, this would require a full take of the CCC property.

Proposed

The Carnegie Avenue westbound entrance ramp will be removed. The project was designed to allow a replacement parking lot to be built on the removed ramp area between East 9th Street and the Administration Building. The replacement parking will be on private property owned by CCC. (Figure G)

Right-of-way Cost to Cure

The appraisal of the Cuyahoga Community College property will be determined for 2 situations. One will be determined on financially compensating for the loss of parking. The second will be determined on constructing replacement parking as a cost to cure. ODOT will proceed with the replacement parking using federal funding up to the amount of compensation determined for the loss of parking. If the replacement parking exceeds the compensation amount, ODOT will fund the excess cost with 100% state funding.

Scranton Averell on Fairfield Avenue

Existing

Scranton Averell has parking lot on West 15th Street and Fairfield Avenue with 31 parking spaces. The parking is for employees and visitors to the adjacent businesses. The parking lot is privately owned.

Project Impact

The new WB Innerbelt bridge alignment will eliminate all 31 parking spaces. Without the replacement of parking, this would require a full take of the adjacent property. West 15th Street place will be removed also.

Proposed

A retaining wall can be built adjacent to the property to provide flat area for parking. The project was designed to allow a replacement parking lot to be built with an entrance off of Fairfield Avenue. This will provide 34 replacement parking spaces. The replacement parking will be on private property owned by Scranton Averell. (Figure H)

Right-of-Way Cost to Cure

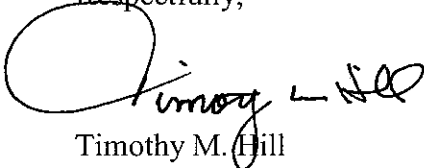
The loss of parking facilities will be compensated for through the right-of-way acquisition process with the current property owner. The mitigation of the parking was designed by ODOT to determine if enough parking could be provided on-site.

The proposed design will construct a retaining wall within ODOT right-of-way. The replacement parking will not be built with the project. The design indicates that sufficient parking can be provided on-site. The property owner will be compensated for the loss of parking. It is the decision of the property owner after right-of-way acquisition whether the parking is built as it has been designed.

In conclusion, we believe the four above preliminary design decisions have been investigated and determined that there are no new environmental impacts. Thus, we believe that we have satisfied the requirements of 23 CFR 771.129(c). Therefore, the environmental commitments from the FEIS remain valid and appropriate for the CCG1 and the Record of Decision remains valid. And furthermore, we do not believe that a Supplemental EIS is necessary per 23 CFR 771.130 since there are no new significant environmental impacts associated with these design decisions.

If you have any questions, please call Mark Carpenter at (216) 584-2089 or Larry Hoffman at (614) 466-6439.

Respectfully,

A handwritten signature in black ink, appearing to read "Timothy M. Hill", with a large circular flourish on the left side.

Timothy M. Hill
Administrator
Office of Environmental Services

Attachments

TMH:LJH:MAC

c: P. Bauer, FHWA – reading file – file PID 77510, PID 85331

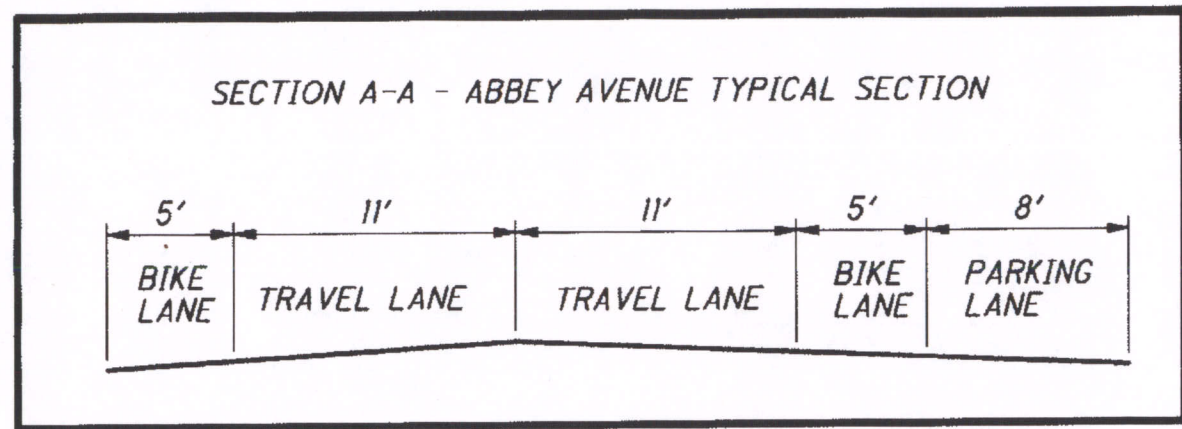
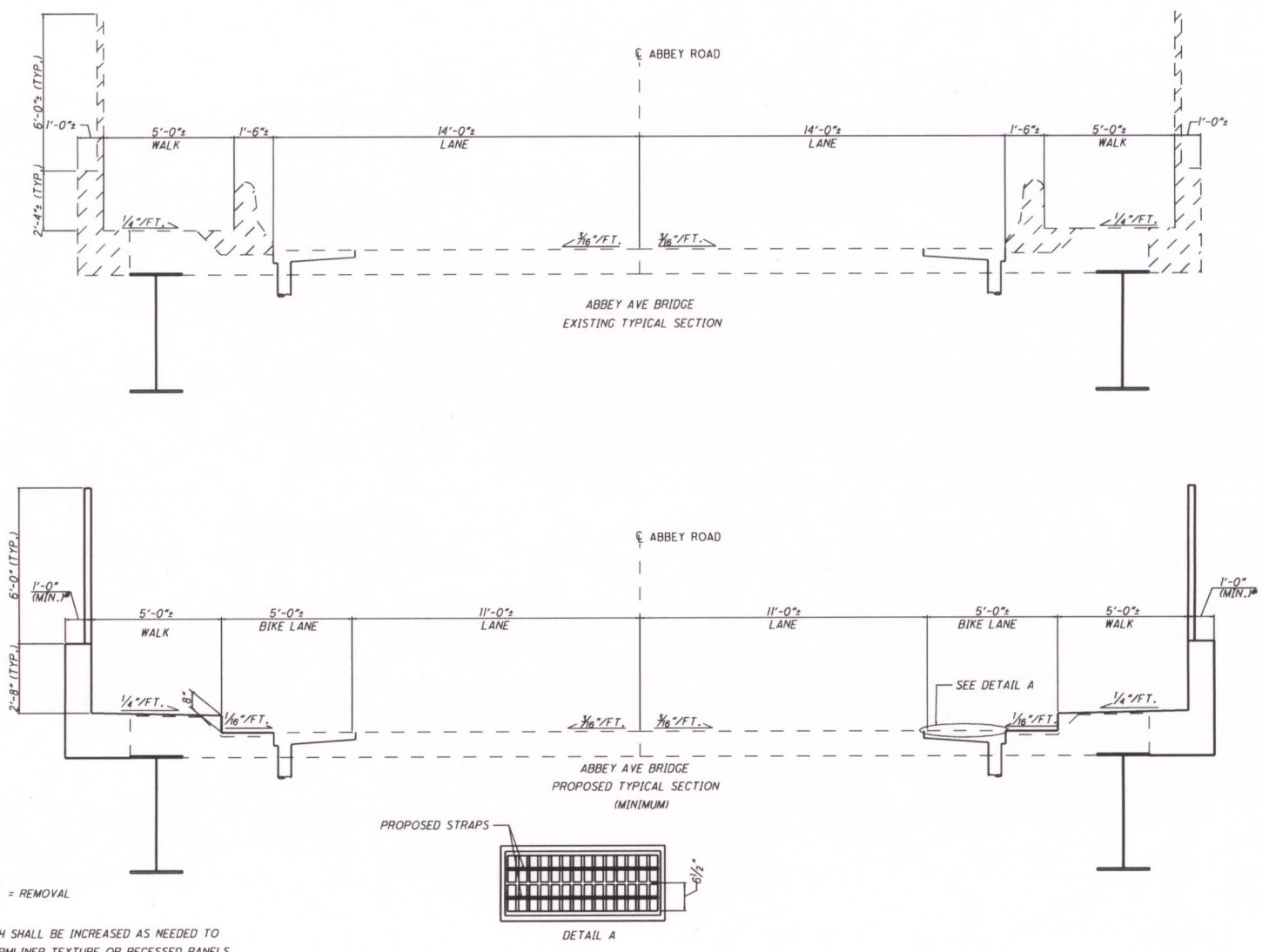


FIGURE A

FIGURE A

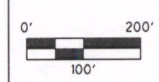
Figure B



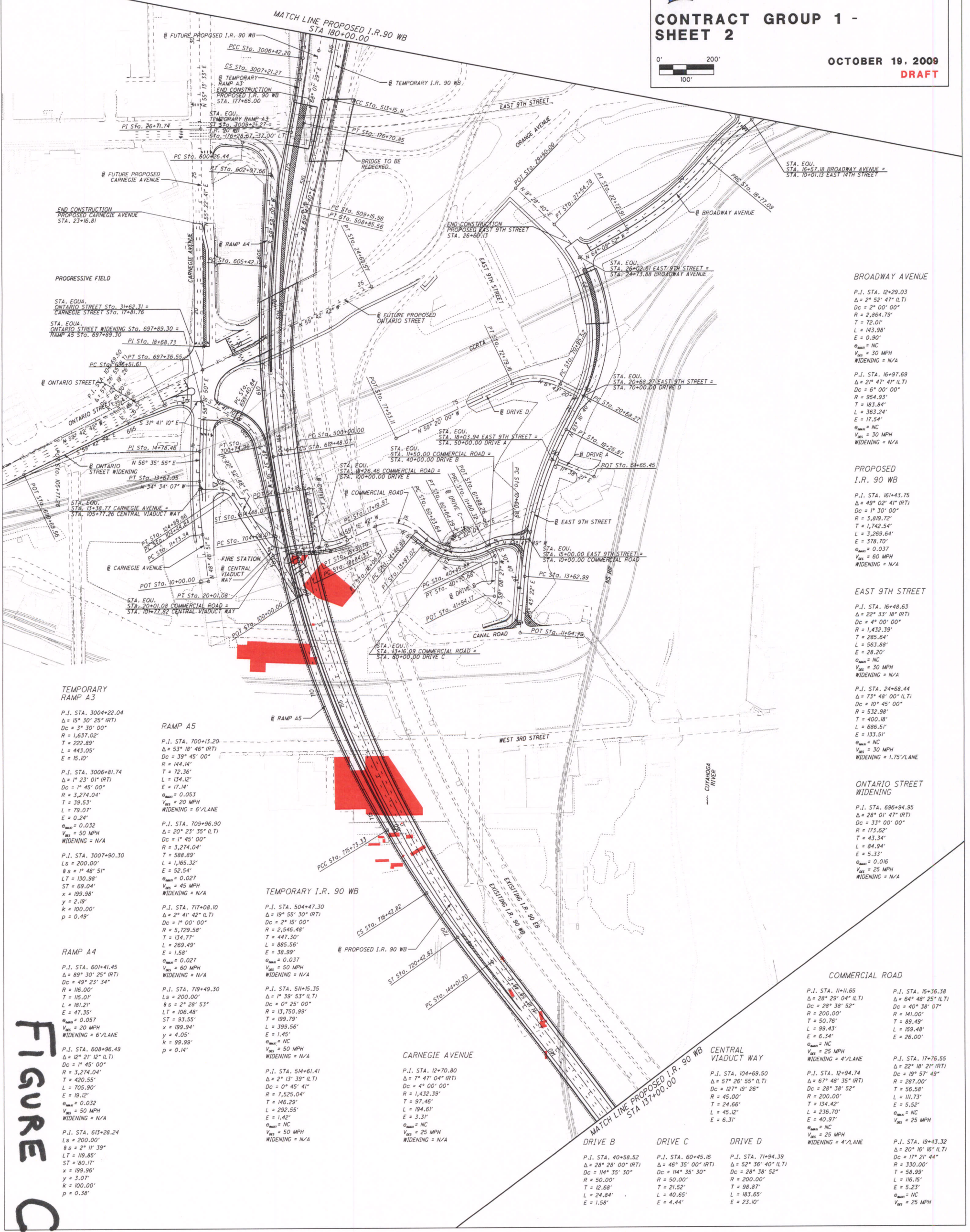
* PARAPET WIDTH SHALL BE INCREASED AS NEEDED TO ACCOMMODATE FORMLINER TEXTURE OR RECESSED PANELS



CONTRACT GROUP 1 - SHEET 2



OCTOBER 19, 2009
DRAFT



BROADWAY AVENUE

P.I. STA. 12+29.03
 $\Delta = 2^\circ 52' 47''$ (LT)
 $Dc = 2^\circ 00' 00''$
 $R = 2,864.79'$
 $T = 72.01'$
 $L = 143.98'$
 $E = 0.90'$
 $\theta_{max} = NC$
 $V_{85} = 30$ MPH
 WIDENING = N/A

P.I. STA. 16+97.69
 $\Delta = 21^\circ 47' 41''$ (LT)
 $Dc = 6^\circ 00' 00''$
 $R = 954.93'$
 $T = 183.84'$
 $L = 363.24'$
 $E = 17.54'$
 $\theta_{max} = NC$
 $V_{85} = 30$ MPH
 WIDENING = N/A

PROPOSED I.R. 90 WB

P.I. STA. 16+43.75
 $\Delta = 49^\circ 02' 41''$ (RT)
 $Dc = 1^\circ 30' 00''$
 $R = 3,819.72'$
 $T = 1,742.54'$
 $L = 3,269.64'$
 $E = 378.70'$
 $\theta_{max} = 0.037$
 $V_{85} = 60$ MPH
 WIDENING = N/A

EAST 9TH STREET

P.I. STA. 16+48.63
 $\Delta = 22^\circ 33' 18''$ (RT)
 $Dc = 4^\circ 00' 00''$
 $R = 1,432.39'$
 $T = 285.64'$
 $L = 563.88'$
 $E = 28.20'$
 $\theta_{max} = NC$
 $V_{85} = 30$ MPH
 WIDENING = N/A

P.I. STA. 24+68.44
 $\Delta = 73^\circ 48' 00''$ (LT)
 $Dc = 10^\circ 45' 00''$
 $R = 532.98'$
 $T = 400.18'$
 $L = 686.51'$
 $E = 133.51'$
 $\theta_{max} = NC$
 $V_{85} = 30$ MPH
 WIDENING = 1.75'/LANE

ONTARIO STREET WIDENING

P.I. STA. 696+94.95
 $\Delta = 28^\circ 01' 47''$ (RT)
 $Dc = 33^\circ 00' 00''$
 $R = 173.62'$
 $T = 43.34'$
 $L = 84.94'$
 $E = 5.33'$
 $\theta_{max} = 0.016$
 $V_{85} = 25$ MPH
 WIDENING = N/A

COMMERCIAL ROAD

P.I. STA. 11+11.65
 $\Delta = 28^\circ 29' 04''$ (LT)
 $Dc = 28^\circ 38' 52''$
 $R = 200.00'$
 $T = 50.76'$
 $L = 99.43'$
 $E = 6.34'$
 $\theta_{max} = NC$
 $V_{85} = 25$ MPH
 WIDENING = 4'/LANE

P.I. STA. 15+36.38
 $\Delta = 64^\circ 48' 25''$ (LT)
 $Dc = 40^\circ 38' 07''$
 $R = 141.00'$
 $T = 89.49'$
 $L = 159.48'$
 $E = 26.00'$
 $\theta_{max} = NC$
 $V_{85} = 25$ MPH

P.I. STA. 17+76.55
 $\Delta = 22^\circ 18' 21''$ (RT)
 $Dc = 19^\circ 57' 49''$
 $R = 287.00'$
 $T = 56.58'$
 $L = 111.73'$
 $E = 5.52'$
 $\theta_{max} = NC$
 $V_{85} = 25$ MPH

DRIVE B

P.I. STA. 40+58.52
 $\Delta = 28^\circ 28' 00''$ (RT)
 $Dc = 114^\circ 35' 30''$
 $R = 50.00'$
 $T = 12.68'$
 $L = 24.84'$
 $E = 1.58'$

DRIVE C

P.I. STA. 60+45.16
 $\Delta = 48^\circ 35' 00''$ (RT)
 $Dc = 114^\circ 35' 30''$
 $R = 50.00'$
 $T = 21.52'$
 $L = 40.65'$
 $E = 4.44'$

DRIVE D

P.I. STA. 71+94.39
 $\Delta = 52^\circ 36' 40''$ (LT)
 $Dc = 28^\circ 38' 52''$
 $R = 200.00'$
 $T = 98.87'$
 $L = 183.65'$
 $E = 23.10'$

TEMPORARY RAMP A3

P.I. STA. 3004+22.04
 $\Delta = 15^\circ 30' 25''$ (RT)
 $Dc = 3^\circ 30' 00''$
 $R = 1,637.02'$
 $T = 222.89'$
 $L = 443.05'$
 $E = 15.10'$

RAMP A5

P.I. STA. 3006+81.74
 $\Delta = 1^\circ 23' 01''$ (RT)
 $Dc = 1^\circ 45' 00''$
 $R = 3,274.04'$
 $T = 39.53'$
 $L = 79.07'$
 $E = 0.24'$
 $\theta_{max} = 0.032$
 $V_{85} = 50$ MPH
 WIDENING = N/A

RAMP A4

P.I. STA. 601+41.45
 $\Delta = 89^\circ 30' 25''$ (RT)
 $Dc = 49^\circ 23' 34''$
 $R = 116.00'$
 $T = 115.01'$
 $L = 181.21'$
 $E = 47.35'$
 $\theta_{max} = 0.057$
 $V_{85} = 20$ MPH
 WIDENING = 6'/LANE

TEMPORARY I.R. 90 WB

P.I. STA. 504+47.30
 $\Delta = 18^\circ 55' 30''$ (RT)
 $Dc = 2^\circ 15' 00''$
 $R = 2,546.48'$
 $T = 447.30'$
 $L = 885.56'$
 $E = 38.99'$
 $\theta_{max} = 0.037$
 $V_{85} = 50$ MPH
 WIDENING = N/A

TEMPORARY I.R. 90 WB

P.I. STA. 717+08.10
 $\Delta = 2^\circ 41' 42''$ (LT)
 $Dc = 1^\circ 00' 00''$
 $R = 5,729.58'$
 $T = 134.77'$
 $L = 269.49'$
 $E = 1.58'$
 $\theta_{max} = 0.027$
 $V_{85} = 45$ MPH
 WIDENING = N/A

TEMPORARY I.R. 90 WB

P.I. STA. 719+49.30
 $\Delta = 20^\circ 23' 35''$ (LT)
 $Dc = 1^\circ 45' 00''$
 $R = 3,274.04'$
 $T = 588.89'$
 $L = 1,165.32'$
 $E = 52.54'$
 $\theta_{max} = 0.027$
 $V_{85} = 45$ MPH
 WIDENING = N/A

TEMPORARY I.R. 90 WB

P.I. STA. 719+49.30
 $\Delta = 20^\circ 23' 35''$ (LT)
 $Dc = 1^\circ 45' 00''$
 $R = 3,274.04'$
 $T = 588.89'$
 $L = 1,165.32'$
 $E = 52.54'$
 $\theta_{max} = 0.027$
 $V_{85} = 45$ MPH
 WIDENING = N/A

TEMPORARY I.R. 90 WB

P.I. STA. 719+49.30
 $\Delta = 20^\circ 23' 35''$ (LT)
 $Dc = 1^\circ 45' 00''$
 $R = 3,274.04'$
 $T = 588.89'$
 $L = 1,165.32'$
 $E = 52.54'$
 $\theta_{max} = 0.027$
 $V_{85} = 45$ MPH
 WIDENING = N/A

FIGURE C

CUY-Cleveland Innerbelt (PID 77510) & CUY - 90 - 14.90 (PID 85531)

Parking for CCG1

The numbers on this table are based on ODOT Preliminary Engineering. The numbers are subject to change based on the design-build engineering to be done.

Area	Location	# of parking spaces to be eliminated	# of replacement parking spaces	R/W Cost to Cure or Enhancement	Replacement Parking: Public or Private owned	Cost Estimate
A	Gateway Animal Clinic	60	61	R/W Cost to Cure	Private	\$275,000
B	Off-street under Innerbelt Bridge in Tremont	0	55 *	Enhancement	Public	\$470,000
C	Abbey Avenue on-street	23	12	Existing	Public	\$70,000
D	NS Railroad Property on Commercial Road	263 **	64 ***	Enhancement	Public	\$470,000
E	Cuyahoga Community College	31	48	R/W Cost to Cure	Private	****
F	Scranton Averell on Fairfield Avenue	31	34	R/W Cost to Cure	Private	****

* Another 82 parking spaces are planned for CCG2 under the new eastbound bridge.

** 133 parking spaces eliminated at north end. At the south end, it is an open gravel lot with no marked parking spaces. It was estimated that there would be 130 parking spaces eliminated, for total of 263.

*** Another 72 parking spaces planned for CCG2 under the new eastbound bridge.

**** No cost estimates have been done for this area.

FIGURE D

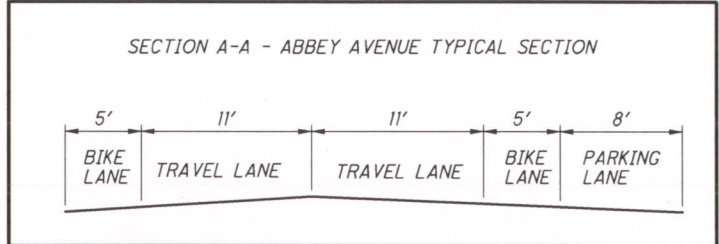
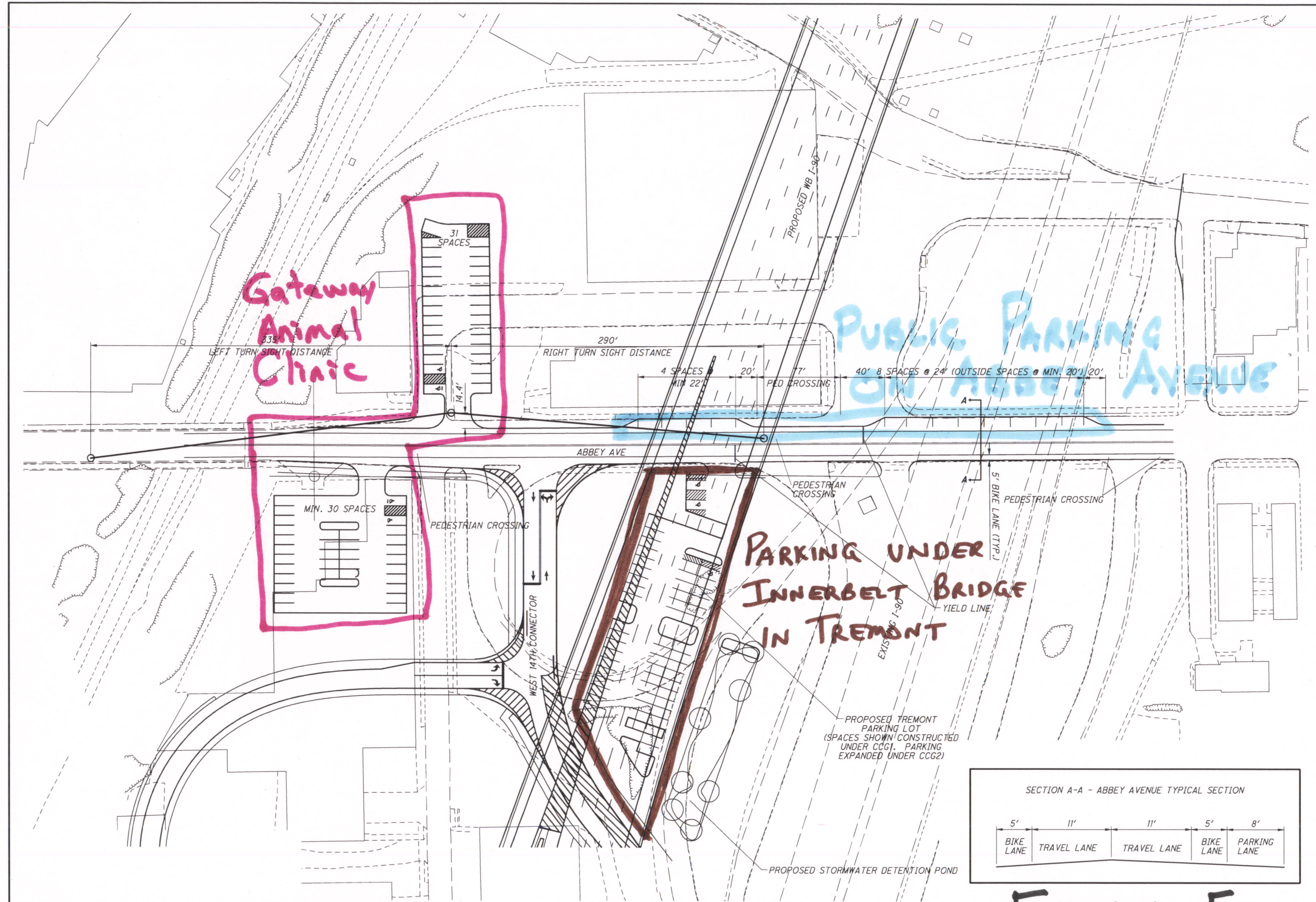


FIGURE E

DRAFT



Key Plan

Job No.	0910
Drawn By	DN, CR
Checked By	JB, SS
Date	11/02/09
Scale	1"=40'-0"

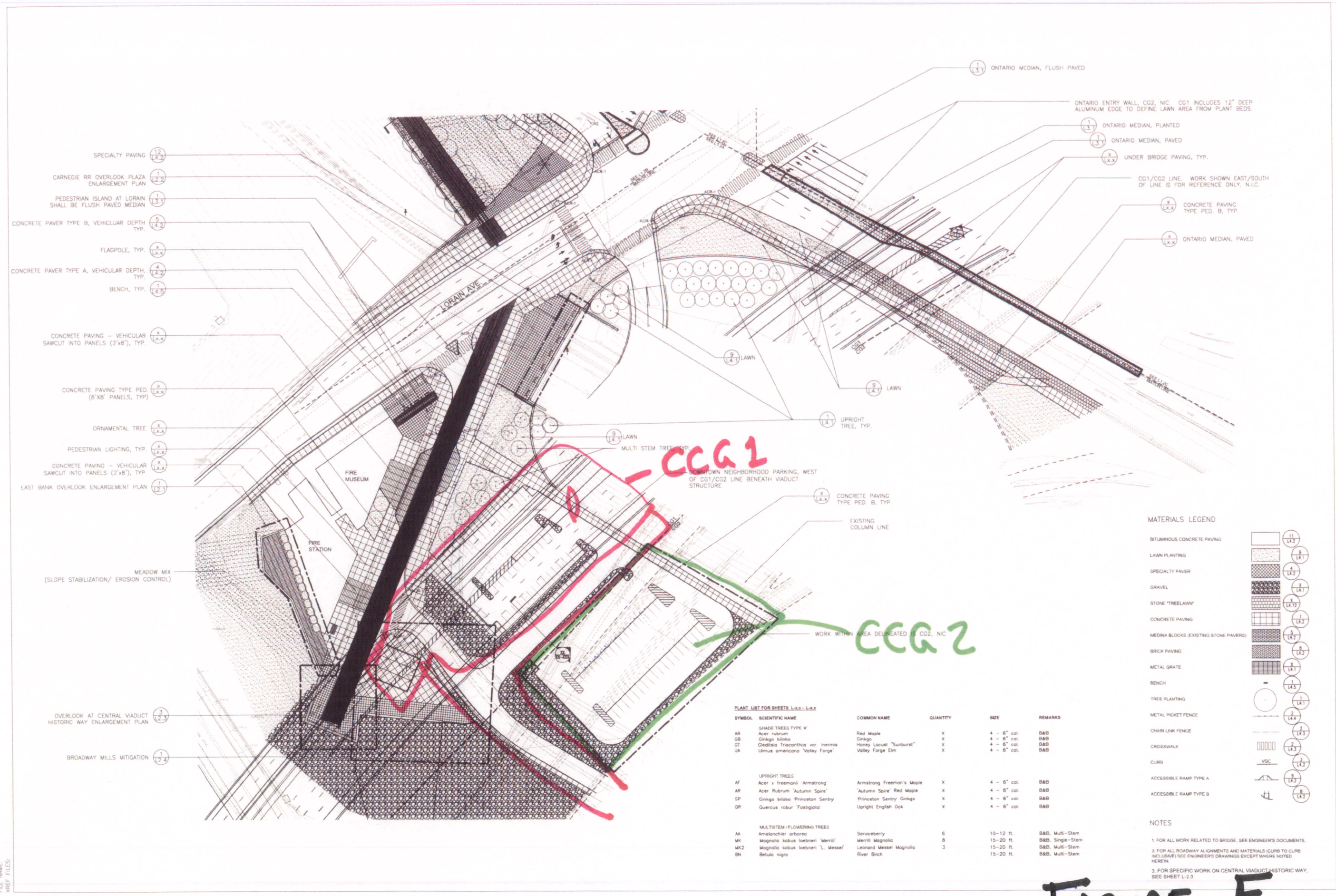
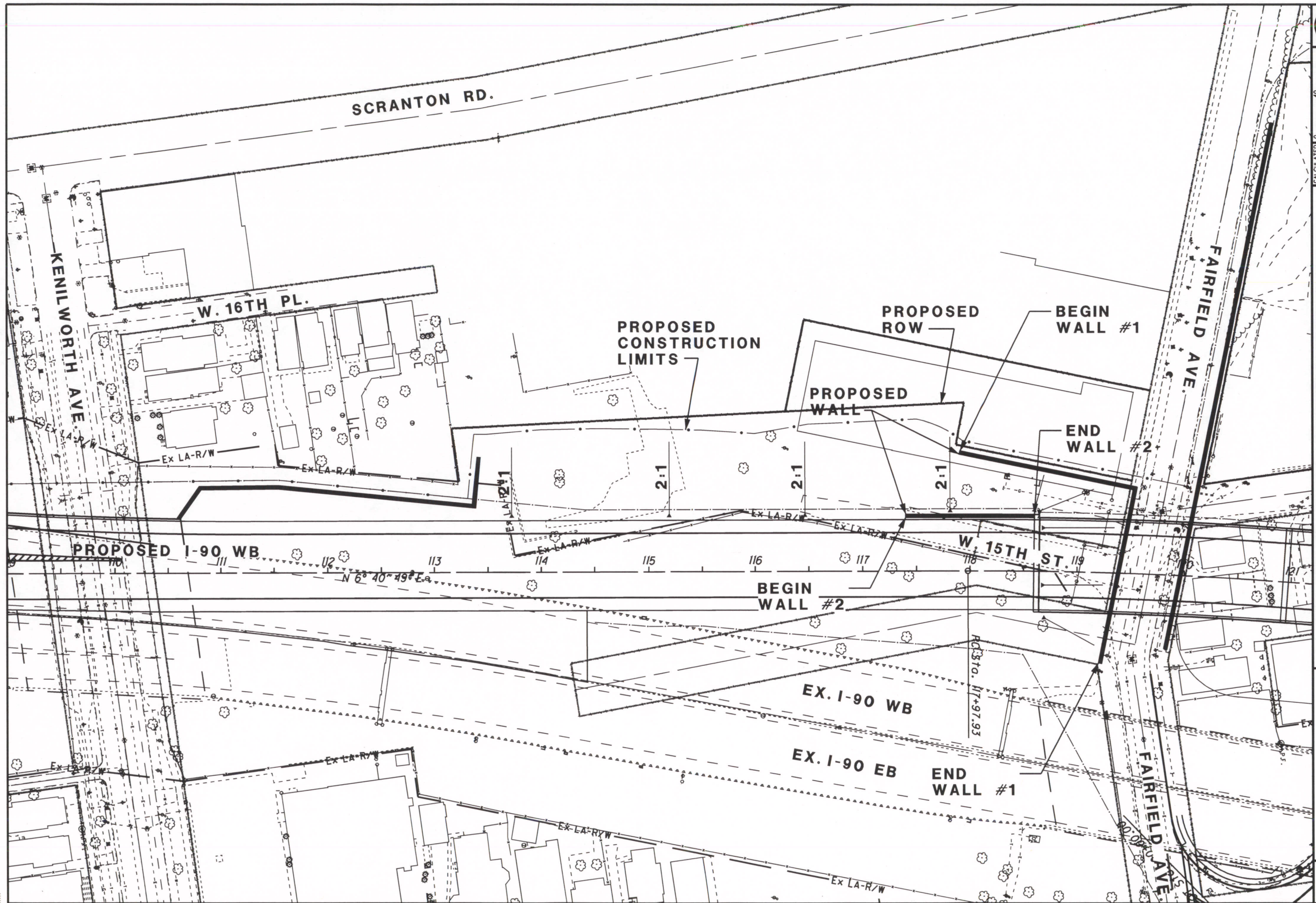



FIGURE F





 0 20 40 80

 HORIZONTAL

 SCALE IN FEET

CALCULATED _____

 CHECKED _____

RETAINING WALL JUSTIFICATION STUDY

I-90 WESTBOUND (STA. 116+00 TO 119+00)

CUY-90-INNERBELT

2

 4

FIGURE H