



**CUY-90-14.90**

**PID 77332/85531**

---

**APPENDIX EX-03**

---

**Original ROW Plans  
(Reference Document)**

State of Ohio  
Department of Transportation  
Jolene M. Molitoris, Director

**Innerbelt Bridge  
Construction Contract Group 1 (CCG1)**

6-0

CUY-90-15.45 ±

FED. RD. DIVISION	STATE	FEDERAL AID PROJECT	FISCAL YEAR
2	OHIO	U1-1057(1)	

1/5

CENTRAL VIADUCT PART 1  
CITY OF CLEVELAND

Cuy-42

# STATE OF OHIO DEPARTMENT OF HIGHWAYS

## U.S. ROUTE 42 RELOCATION CENTRAL VIADUCT-INNER BELT FREEWAY PART 1-PRELIMINARY TEST PILES CUYAHOGA COUNTY CITY OF CLEVELAND

### CONVENTIONAL SIGNS

CORPORATION LINE	_____
CENTER LINE	_____
RAILROAD	_____

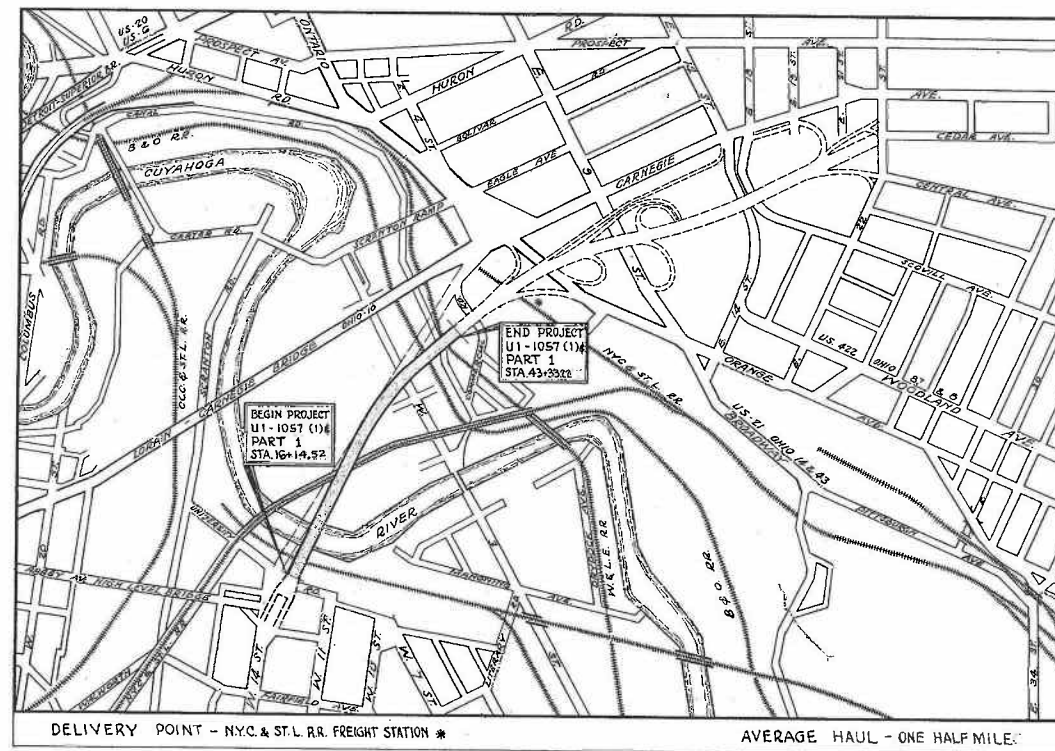
### INDEX OF SHEETS

TITLE SHEET	SHEET NO 1
LOCATION PLANS	2-3
CROSS SECTIONS	4
NOTES & QUANTITIES	5

### LINE DATA

Begin Project U-I-1057(1) Sta. 16+14.52  
 End Project U-I-1057(1) Sta. 43+33.22  
 Net Length of Project U-I-1057(1) = 2718.70' or 0.514 mi.

Begin Work Sta. 16+00  
 End Work Sta. 43+75  
 Net Length of Work = 2775' or 0.525 mi.



### LOCATION PLAN

SCALE: 1" = 800'

PORTION TO BE IMPROVED	_____
STATE HIGHWAYS	_____
OTHER HIGHWAYS	_____

CONSTRUCTION  
 BU. 11  
 MAR 27 1953  
 SURVEYING PHOTOCOPY

STANDARD DRAWINGS  
(None)

SUPPLEMENTAL SPECIFICATIONS  
 T-171.19 Rev. 3-19-53  
 M-101.7 1-24-53

The standard specifications of The State of Ohio, Department of Highways, including changes and supplemental specifications listed in the proposal shall govern this improvement.

I hereby approve these plans and declare that the making of this improvement will not require the closing to traffic of the highway, and that provisions for the maintenance and safety of traffic will be as set forth on the plans and estimates.

The right of way for this improvement will be provided by the State of Ohio.

Approved Louis R. Drasler  
 Date 4/6/53 Division Deputy Director

Approved \_\_\_\_\_  
 Date \_\_\_\_\_ Chief Engineer, Bureau of Maintenance

Approved John J. Heine  
 Date 4-15-53 Chief Engineer, Bureau of Planning & Programming

Approved Richard C. O'Neil  
 Date 4-13-53 Chief Engineer, Bureau of Bridges & R.R. Crossings

Approved Walter J. ...  
 Date 4-16-53 Chief Engineer, Bureau of Location & Design

Approved V. P. ...  
 Date 4-16-53 First Asst Director & Chief Engineer

Approved ...  
 Date 4-16-53 Director of Highways

Approved for The City of Cleveland Samuel J. Davis  
 Date 4-6-53 Director of Public Service

DEPARTMENT OF COMMERCE BUREAU OF PUBLIC ROADS	
RECOMMENDED FOR APPROVAL:	
DISTRICT ENGINEER	DATE
APPROVED	
FOR THE DIVISION ENGINEER	DATE

FILE NO	CUY -
	CUYAHOGA COUNTY CITY OF CLEVELAND
	DATE OF LETTING _____ 1953
	CONTRACT NO _____

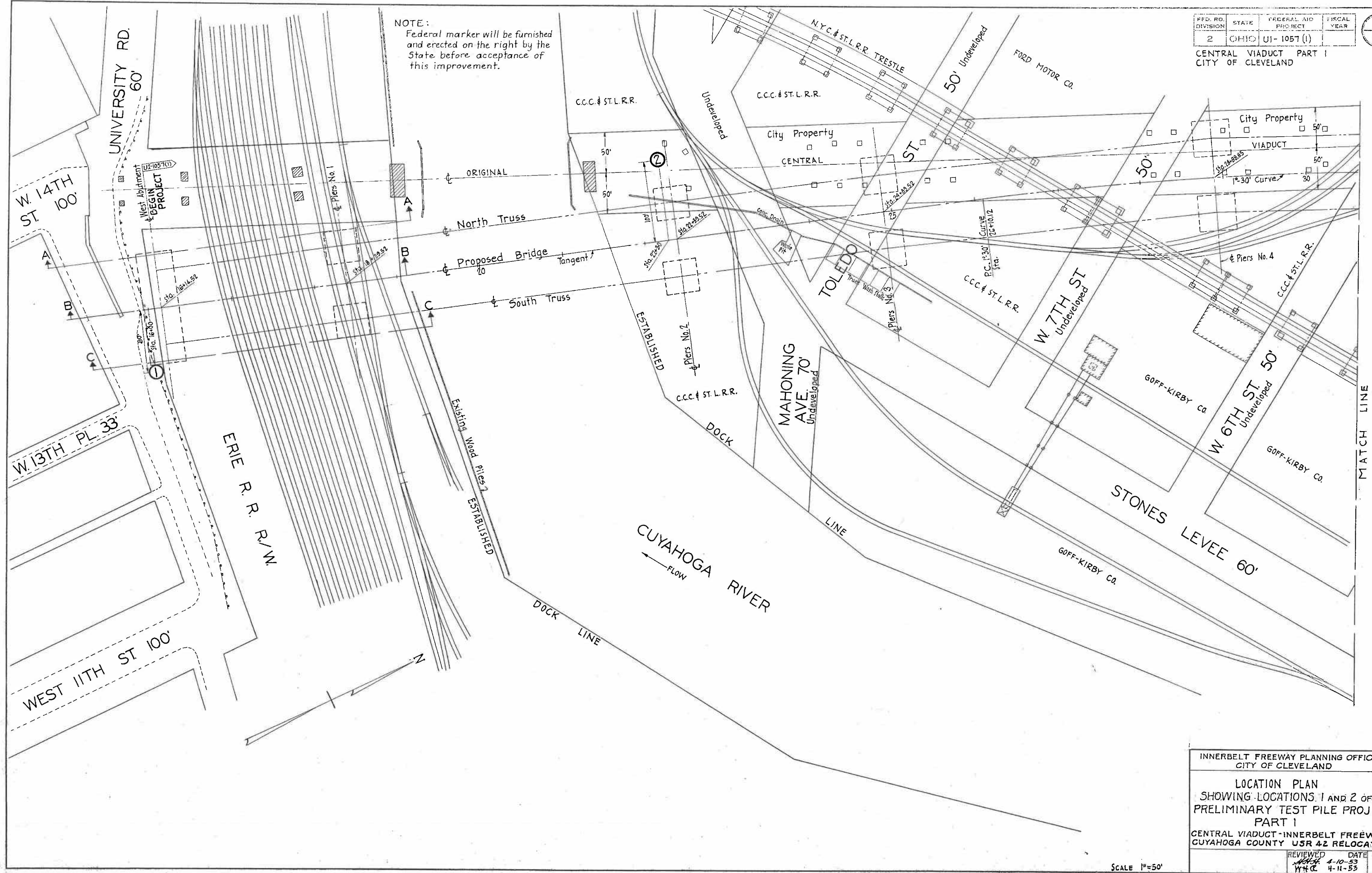
00062

FED. RD. DIVISION	STATE	FEDERAL AID PROJECT	FISCAL YEAR
2	OHIO	UI-1057 (1)	

2  
5

CENTRAL VIADUCT PART I  
CITY OF CLEVELAND

NOTE:  
Federal marker will be furnished and erected on the right by the State before acceptance of this improvement.



INNERBELT FREEWAY PLANNING OFFICE  
CITY OF CLEVELAND

LOCATION PLAN  
SHOWING LOCATIONS 1 AND 2 OF  
PRELIMINARY TEST FILE PROJECT  
PART I  
CENTRAL VIADUCT - INNERBELT FREEWAY  
CUYAHOGA COUNTY USR 42 RELOCATED

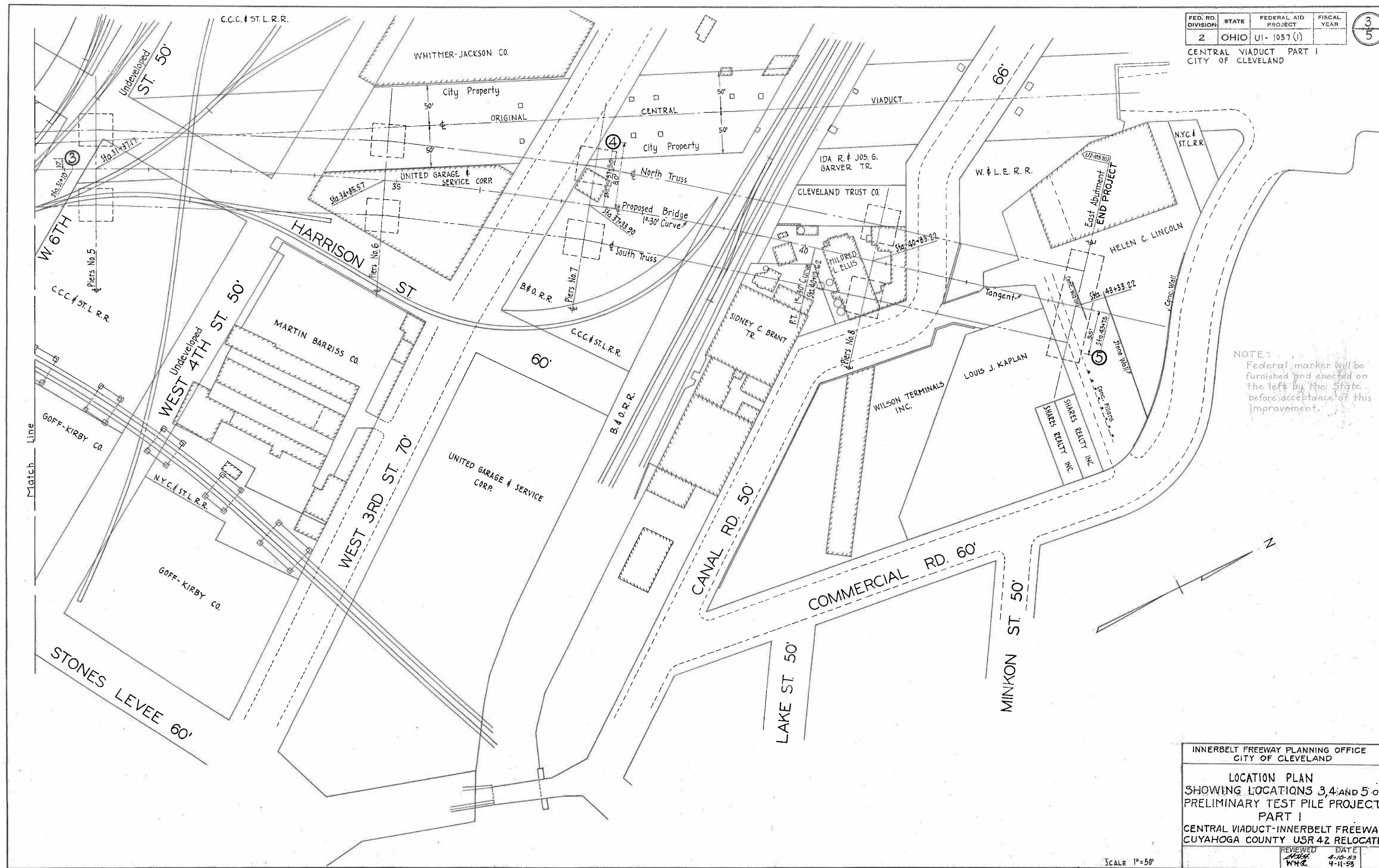
REVIEWED	DATE
WTC	4-10-53
WTC	4-11-53

SCALE 1"=50'

FED. RD. DIVISION	STATE	FEDERAL AID PROJECT	FISCAL YEAR
2	OHIO	UI-1057 (1)	

CENTRAL VIADUCT PART I  
CITY OF CLEVELAND

3  
5



NOTE:  
Federal marker will be furnished and erected on the left by the State before acceptance of this improvement.

INNERBELT FREEWAY PLANNING OFFICE  
CITY OF CLEVELAND

LOCATION PLAN  
SHOWING LOCATIONS 3, 4, AND 5 OF  
PRELIMINARY TEST PILE PROJECT  
PART I  
CENTRAL VIADUCT-INNERBELT FREEWAY  
CUYAHOGA COUNTY USR 42 RELOCATE

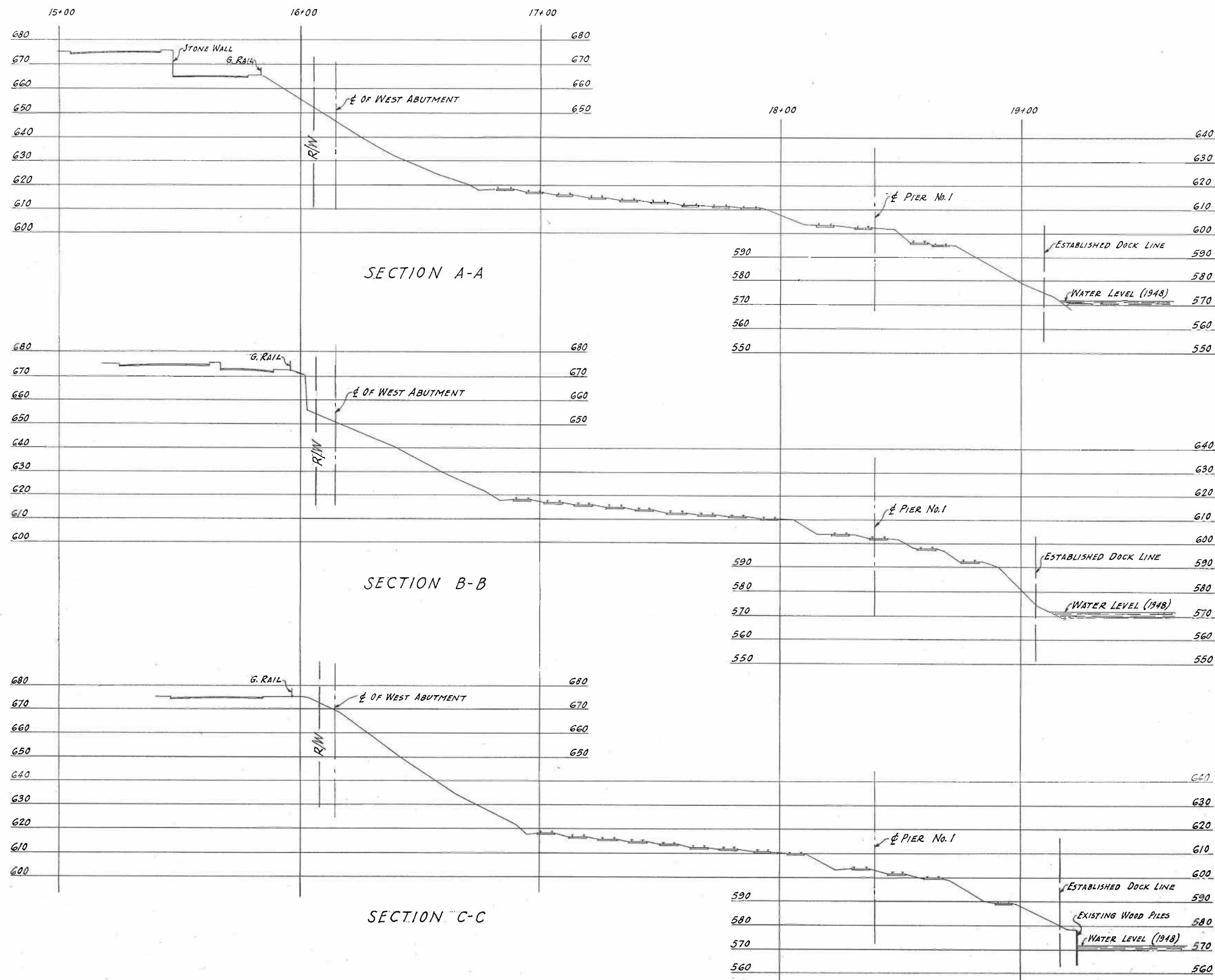
REVIEWED	DATE
W.H.G.	4-10-53
	4-11-53

SCALE: 1"=50'

FED. RD. DIVISION	STATE	FEDERAL AID PROJECT	FISCAL YEAR
2	OHIO	UI-1057(1)	

4  
5

CENTRAL VIADUCT PART I  
CITY OF CLEVELAND



INNERBELT FREEWAY PLANNING OFFICE  
CITY OF CLEVELAND

CROSS-SECTIONS  
AT WEST ABUTMENT & ERIE R. R.  
PRELIMINARY TEST PILE PROJECT  
PART I

CENTRAL VIADUCT-INNERBELT FREEWAY  
CUYAHOGA COUNTY USR A2 RELOCATED

REVIEWED	DATE
W.H.C.	4-10-53
	4-11-53

SCALE  
HOR. 1" = 20'  
VER. 1" = 20'

### GENERAL NOTES AND QUANTITIES

**1. SCOPE AND PURPOSE**

This contract consists of (a) the furnishing and driving of test piles at the locations shown on the general plan and (b) the test loading of these test piles under the direction of the Engineer, according to Items S-17 and S-18 of the Construction and Material Specifications and the following special provisions.

The purpose of this test pile project is to provide information for use in the design and construction of the pile foundations of a proposed viaduct at this site.

**2. TYPE OF PILES**

The types of piles to be tested by driving and loading are 14" cast-in-place concrete piles and 12" steel H piles (12BP53).

The cast-in-place concrete piles shall be of the type for which the casing is left in place and is driven without a mandrel. The casing of the first pile of this type driven shall be of approximately 7 gauge thickness, and this thickness of casing shall be used for subsequent piles unless and until appreciable constriction of the casing due to earth pressure occurs, as determined by the Engineer. If and after such constriction occurs, subsequently driven casings for this type shall be of approximately 5 gauge thickness. If a casing of 5 gauge thickness begins to constrict, as determined by the Engineer, the Contractor shall fill the lower part of the casing with high-early-strength cement concrete, to the extent directed by the Engineer, and after this concrete has set for 3 days driving shall be resumed. (It is contemplated that this testing of the type that is driven without a mandrel also will provide information regarding the required depth of penetration of left-in-place casings that are driven with a mandrel.)

**3. TEST METHODS**

The first testing shall be at Location No. 2 and the next at Location No. 3. At each of these two locations, two or more piles of the 14" cast-in-place type and two or more of the 12" steel H type shall be tested under two conditions: first, by providing a sheeted hole 20 feet deep and at least 24 inches in diameter, either by driving an open-end steel pipe and excavating all materials from within the pipe, or by any other suitable method and then driving the pile within the sheeted hole; second, by driving another pile of the same type and size, and by the same hammer, in an adjacent location beginning at the natural ground elevation (without the use of such sheeted hole). From a comparison of the penetration behavior of piles driven under these two conditions (at the same depth of penetration below the upper surface of the ground and with the same total length of pile in the leads) the difference in penetration (with and without the use of the sheeted hole) will be determined by the Engineer, and at the remaining locations the test piles shall be driven without the use of the sheeted hole.

Each pile shall be driven to a capacity of 65 tons according to the capacity formula in Sec. S-18.05. For the first two or three piles of each type driven the coefficient value of 2 in the numerator of the formula will be used. For the driving of subsequent piles, or for additional driving of a pile (after test loading thereon has been completed and if another test load at greater penetration is requested by the Engineer), this coefficient probably will be changed to a greater or lesser value by the Engineer, depending upon test load indications. Since one of the purposes of this test pile project is to determine the evaluation of the capacity formula to cause correct penetration for the contemplated design capacity of 65 tons, the driving shall be stopped immediately after a capacity of 65 tons according to the formula, has been attained.

The maximum magnitude of the test loads shall be 3R unless the yield point is reached at a smaller load. The test loading may be applied by use of a loaded platform or by the use of anchor piles or by a combination of these means. A pile driven as a test pile may be used as an anchor pile (for another, adjacent test pile) only after all test loading on it has been completed.

Any pile used as an anchor pile shall be located at least 6 feet from the pile being test loaded.

Except as noted above, the casing of a cast-in-place concrete pile shall not be filled with concrete until after the test loading has been completed, in order to facilitate subsequent driving of the casing to a greater penetration if so directed by the Engineer.

**4. PILE DRIVING HAMMERS**

The pile driving hammers used shall be of the single-acting steam type. The Contractor shall provide one hammer of approximately 15,000 ft. lb. (not less than 14,000, nor more than 16,500) and another of approximately 25,000 ft. lb. (not less than 24,000, nor more than 26,500) rated capacity, to be used as directed by the Engineer. It is expected that the 15,000 ft. lb. hammer will be adequate for this pile driving but the Contractor will be required to use both it and the 25,000 ft. lb. hammer, to ascertain the relative advantages and disadvantages of these two sizes. These hammers shall be applied on different piles or both shall be used in driving the same pile, as directed by the Engineer, but the use of both sizes of hammer on the same pile will not be required on more than six piles and the change from the one size to the other, where both sizes are used to drive the same pile, need be made not more than eight times altogether.

**5. FINAL CLEANING UP**

At the conclusion of each test, the cast-in-place piles shall be filled with concrete to within one foot of existing ground elevation and burned off at that elevation, or may be extracted and removed from the site, at the option of the Contractor. Likewise, unless extracted and removed from the site, the steel bearing piles shall be burned off at one foot below existing ground elevation. If steel pipe or other sheeting is left in the ground, the hole shall be filled with concrete to within one foot of existing ground elevation and burned off at that elevation. If piles, pipe or sheeting are extracted from the ground the holes shall be filled with concrete or with sand, flushed with water to eliminate voids. This also shall be done if a depression is created alongside a pile, pipe or sheet pile that is left in place. All equipment and material brought to the site by the Contractor except that which is left below ground, shall be removed from the site. All removed material, including extracted piles, pipes, and sheeting shall be considered the property of the Contractor.

**6. REPORTS**

A report on these tests will be prepared and submitted by the Engineer. The Contractor shall cooperate with the Engineer in the making of measurements, readings and observations.

**7. METHOD OF MEASUREMENT AND BASIS OF PAYMENT**

The lump sum item of "Pile Testing Overhead" is provided principally because of possible great variations in the final pay quantities of the unit price items (S-17, S-18 and the special item of 20ft depth sheeted holes) from the estimated quantities, in view of the research character of this project. This item shall be considered as providing payment for the following: (1) Moving all pile-driving and test-loading equipment to the indicated locations at the site and away from the site. (2) Furnishing at

the site, pile, pipe and sheeting materials which is not used and not paid for under the unit price items. (3) Cleaning up the site and removing all materials. The payment of the lump sum price bid for this item together with the payment at the unit prices bid for the quantities of work actually done under Item S-17, the two S-18 items and the special item of 20-ft. depth sheeted holes, shall constitute complete compensation for this work, including all labor, material, equipment and incidentals.

No Item S-16 is provided in the quantities since all of the piles are test piles and are to be paid for per linear foot under Item S-18 and since the item of Pile Testing Overhead is provided.

All pile test loads shall be paid for at the unit price per Pile Test Load bid regardless of whether it be a first or subsequent load and regardless of whether it be another loading applied on the same pile or the loading of another pile at the same or another location.

For the purposes of payment the point of cutoff for the piles under Item S-18 shall be considered as one foot below the existing ground.

The basis of payment for the 20-ft. depth sheeted holes, obtained by use of a driven steel pipe or other approved method, shall be the unit price bid per hole and shall be considered to provide compensation for furnishing all material, equipment, labor and incidentals and for driving, excavating, filling with concrete, or extracting and filling the hole.

Payment will be made, at the price per lin. ft. bid, for piles damaged in driving, if driven with the approval of the Engineer and if such damage is not caused by the Contractor's failure to operate properly, even though such piles must be abandoned and are not subjected to test loading.

The estimated quantities in the S-18 items are based on a possibility of 400 linear feet of 14" cast-in-place concrete and 400 linear feet of 12" steel H piling at each of Locations 2 and 3, and 200 linear feet at each of the other three Locations, a total of 1400 lin. ft. of each type. It is contemplated that there will be two piles of each type driven at each of Locations 2 and 3, and perhaps one pile of each type at each of the other three Locations. There is a possibility of more than this number because of additional piles needed to replace damaged piles that are not suitable for test loading or because of the need for a subsequent shorter pile of the same type where the test loading indicates that a pile has been driven to a penetration greater than required. However, if the results of the testing at Locations 2 and 3 so indicate, the Engineer may consider it sufficient to test only one type of pile at each of Locations 4 and 5, but it is anticipated that it will be necessary to test both the cast-in-place concrete and the steel H pile at Location No. 1.

The estimated number of 20-ft. depth sheeted holes is based on at least two such holes at each of Locations 2 and 3, with a possibility of twice this number if additional piles must be driven for the reasons stated above.

The estimated quantity of pile test loads is based on from 4 to 8 test loads at each of Locations 2 and 3, and from 1 to 4 test loads at each of the other three locations.

**ESTIMATED QUANTITIES**

As Built	ITEM	TOTAL	UNIT	DESCRIPTION
	Special Lump Sum			Pile Testing Overhead
CO. 3 - 4	4	8	Each	20-ft. Depth Sheeted Holes
CO. 3 - 9	19	28	Each	Pile Test Loads
CO. 3 - 767.6	632.4	1400	Lin. Ft.	14" Cast-in-place Concrete Piles
CO. 3 - 777.4	622.6	1400	Lin. Ft.	Steel Piles (12 BP 53)

INNERBELT FREEWAY PLANNING OFFICE  
CITY OF CLEVELAND

GENERAL NOTES & QUANTITIES  
PRELIMINARY TEST PILE PROJECT  
PART 1

CENTRAL VIADUCT - INNERBELT FREEWAY  
CUYAHOGA COUNTY USR 42 RELOCATED

REVIEWED WHE	DATE 4-10-53 4-11-53
-----------------	----------------------------

Revised As-Built  
WHE - 12-23-53

CUY-90-15.45

6-0

STATE OF OHIO  
DEPARTMENT OF HIGHWAYS

UI-1057 (4)

FED. ROAD DIV. NO.	STATE	FEDERAL AID PROJECT NO.	TYPE FUNDS
2	OHIO	UI 1057 (4)	

CUYAHOGA COUNTY  
CITY OF CLEVELAND  
INNER BELT FREEWAY  
CENTRAL VIADUCT  
CUY - 42R - 17.50

PART 2

CUY - 42R - 17.50

INNER BELT FREEWAY - CENTRAL VIADUCT

BR. NO. CU - 42R - 175

CUYAHOGA COUNTY

CITY OF CLEVELAND

PART 2 SUBSTRUCTURE

MAR 3 1952  
GROUND PHOTOGRAPH

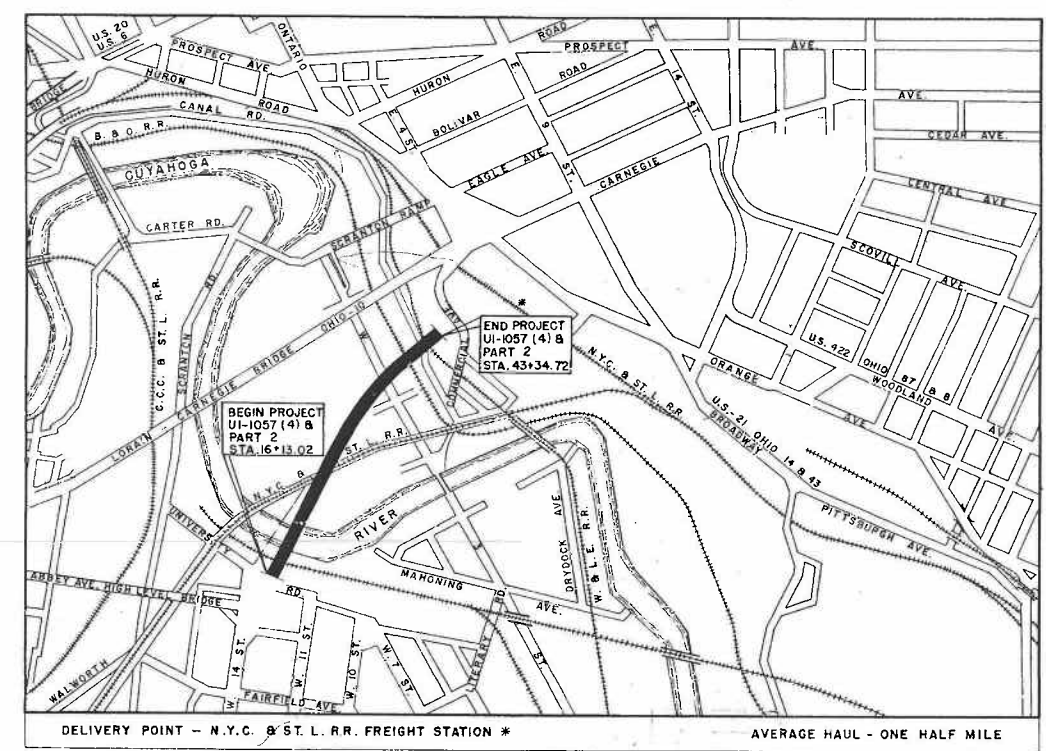
THIS IMPROVEMENT HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR OF HIGHWAYS IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 REVISED CODE OF OHIO.

INDEX OF SHEETS

1	TITLE SHEET	23-28	PIER SHAFT DETAILS
2	QUANTITIES	29	ANCHOR BOLT PLAN AND DETAILS
3	GENERAL NOTES	30-32	REINFORCEMENT SCHEDULES
4,5	GENERAL PLAN AND ELEVATION	33-37	STORM SEWER AND DRAINAGE DETAILS
6-9	GENERAL LAYOUT, TEST HOLE BORINGS, EXISTING CONDITIONS	38,39	DOCK WALL
10,11	RAILROAD TRACK MODIFICATIONS	40-43	CONSTRUCTION IN ERIE RAILROAD YARD
12,13	EXISTING CONDITIONS AT PIER LOCATIONS	10,11,35	R. R. FORCE ACCOUNT WORK
14-22	PIER FOOTINGS	36A	REVISED SHEET no. 36

LINE DATA

BEGIN PROJECT	UI-1057 (4)	STA. 16 + 13.02
END PROJECT	UI-1057 (4)	STA. 43 + 34.72
NET LENGTH	UI-1057 (4) AND WORK	2,721.7 LIN. FT. or 0.515 MILES



THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO DEPARTMENT OF HIGHWAYS, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH IN THE PLANS AND ESTIMATE.

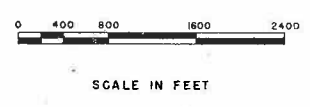
THE RIGHT OF WAY FOR THIS IMPROVEMENT WILL BE PROVIDED BY THE STATE OF OHIO.

- APPROVED  
DATE 9-2-54  
DIRECTOR OF PUBLIC SERVICE, CITY OF CLEVELAND
- APPROVED  
DATE 9-2-54  
DIVISION DEPUTY DIRECTOR
- APPROVED  
DATE 7/14/54  
DEPUTY DIRECTOR OF PLANNING AND PROGRAMMING
- APPROVED  
DATE 9-8-54  
ENGINEER OF BRIDGES
- APPROVED  
DATE  
ENGINEER OF LOCATION & DESIGN
- APPROVED  
DATE 9-10-54  
DEPUTY DIRECTOR OF DESIGN & CONSTRUCTION
- APPROVED  
DATE 9-14-54  
FIRST ASSISTANT DIRECTOR
- APPROVED  
DATE 9-15-54  
DIRECTOR OF HIGHWAYS
- PRINT APPROVED BY  
DATE 8-31-54  
CHIEF ENGINEER, ERIE RAILROAD
- PRINT APPROVED BY  
DATE 8-31-54  
CHIEF ENGINEER, NEW YORK CENTRAL SYSTEM
- APPROVED  
DATE  
CHIEF ENGINEER, NEW YORK, CHICAGO AND SAINT LOUIS RAILROAD CO.
- PRINT APPROVED BY  
DATE 9-2-54  
CHIEF ENGINEER, BALTIMORE AND OHIO RAILROAD

HOWARD NEEDLES TAMMEN & BERGENDOFF  
CONSULTING ENGINEERS  
KANSAS CITY CLEVELAND NEW YORK

H.G. SOURS  
ASSOCIATE  
COLUMBUS

LOCATION PLAN



PORTION TO BE IMPROVED

SUPPLEMENTAL SPECIFICATIONS			
NUMBER	DATE	NUMBER	DATE
CE-107	5-21-53		
F-171.19	R. 3-19-53		
M-101.7	1-24-53		

STANDARD DRAWINGS			
NUMBER	DATE	NUMBER	DATE
I-8 M.H. No. 7	5-1-52		
G-7.07	1-2-53		
I-8 M.H. No. 1-A	6-1-54		
I-1,2,3,4 & 5	2-20-45		

MAR 3 1952  
GROUND PHOTOGRAPH

DEPARTMENT OF COMMERCE  
BUREAU OF PUBLIC ROADS

APPROVED

DISTRICT ENGINEER

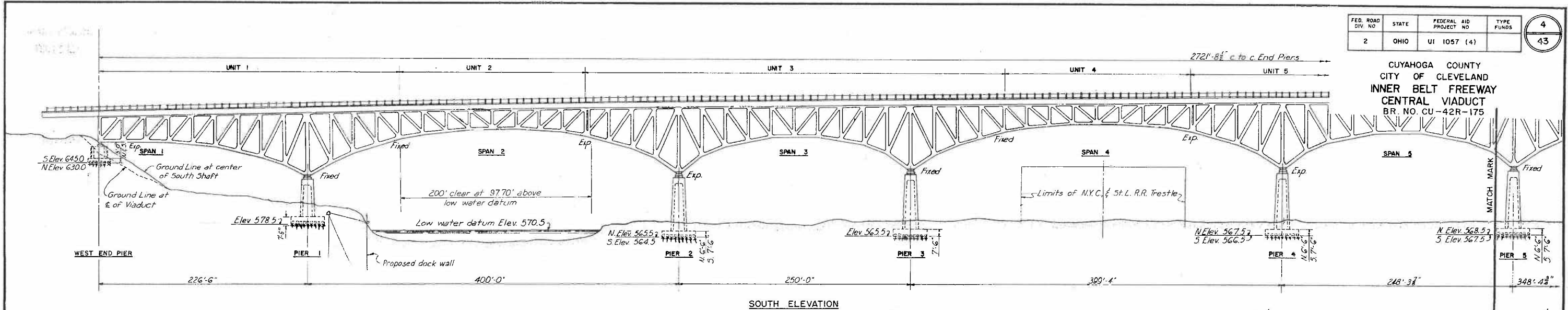
DATE

FILE NO.	CUYAHOGA COUNTY
SEC	00077
DATE OF LETTING	_____, 195__
CONTRACT NO.	

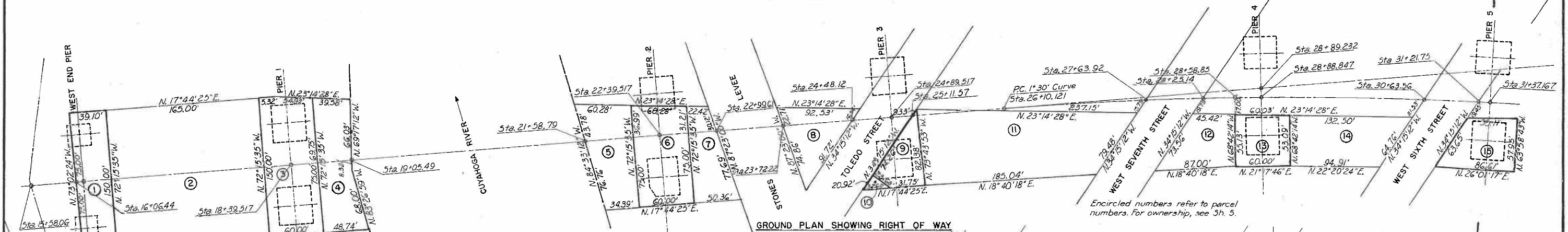
\* Accepted. \*\* Submitted to B.P.R. for approval.

FED. ROAD DIV. NO.	STATE	FEDERAL AID PROJECT NO.	TYPE FUNDS	4 43
2	OHIO	UI 1057 (4)		

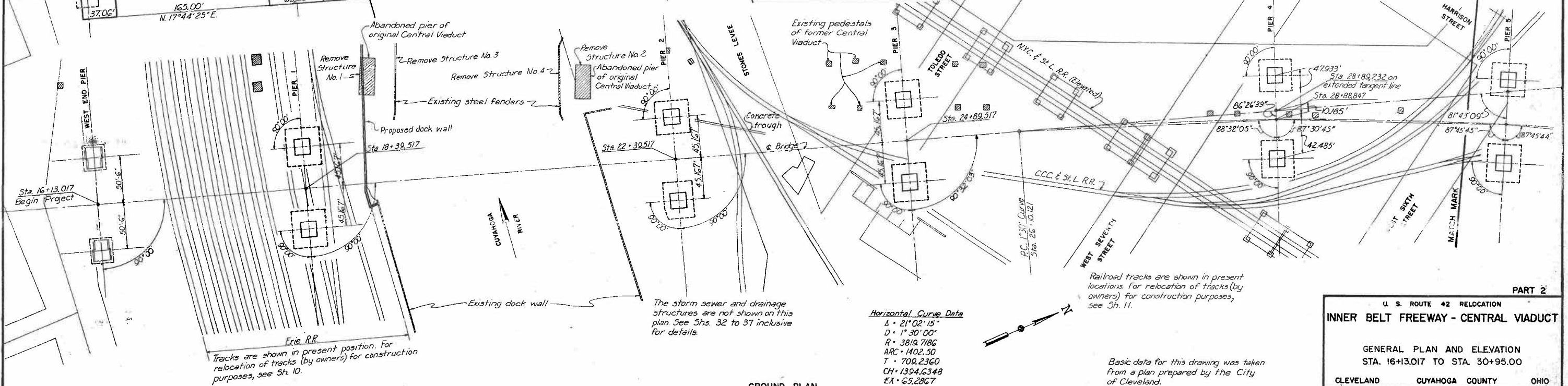
CUYAHOGA COUNTY  
CITY OF CLEVELAND  
INNER BELT FREEWAY  
CENTRAL VIADUCT  
BR. NO. CU-42R-175



SOUTH ELEVATION

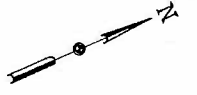


GROUND PLAN SHOWING RIGHT OF WAY



GROUND PLAN

**Horizontal Curve Data**  
 Δ • 21° 02' 15"  
 D • 1° 30' 00"  
 R • 3819.7186  
 ARC • 1402.50  
 T • 709.2360  
 CH • 1394.6348  
 EA • 65.2867



Railroad tracks are shown in present locations. For relocation of tracks (by owners) for construction purposes, see Sh. 11.

Basic data for this drawing was taken from a plan prepared by the City of Cleveland.

PART 2

U. S. ROUTE 42 RELOCATION  
INNER BELT FREEWAY - CENTRAL VIADUCT

GENERAL PLAN AND ELEVATION  
STA. 16+13.017 TO STA. 30+95.00

CLEVELAND	CUYAHOGA COUNTY	OHIO
-----------	-----------------	------

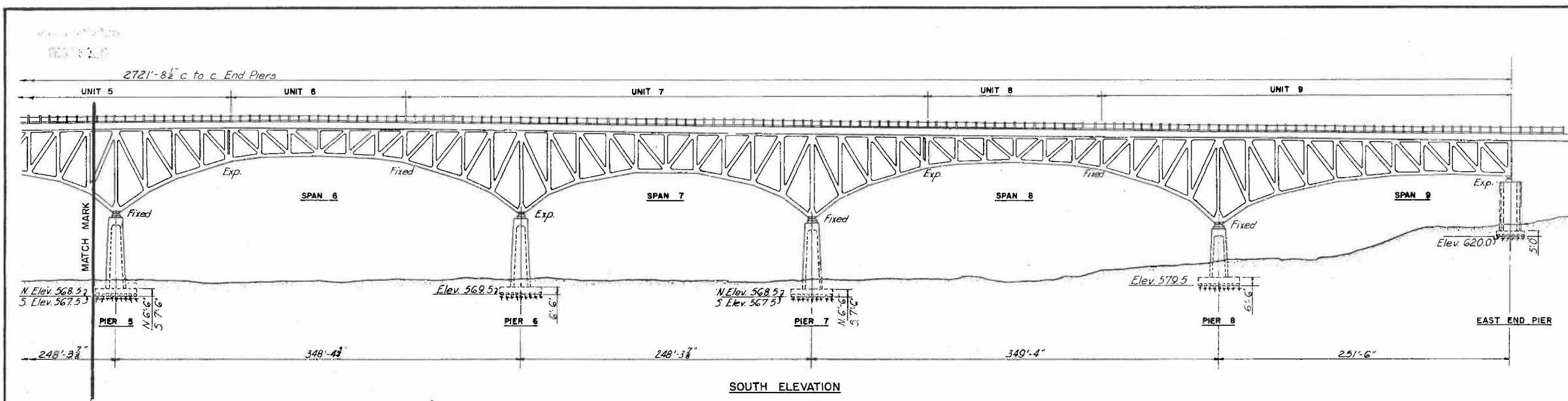
SCALE: 1" = 50'-0"

MADE N.A.M. DATE: 1-10-54	HOWARD, NEEDLES, TAMMEN & BERGENDOFF
TRCD. N.A.M. DATE: 6-12-54	CONSULTING ENGINEERS
CHK. G.A. DATE: 7-6-54	KANSAS CITY CLEVELAND NEW YORK

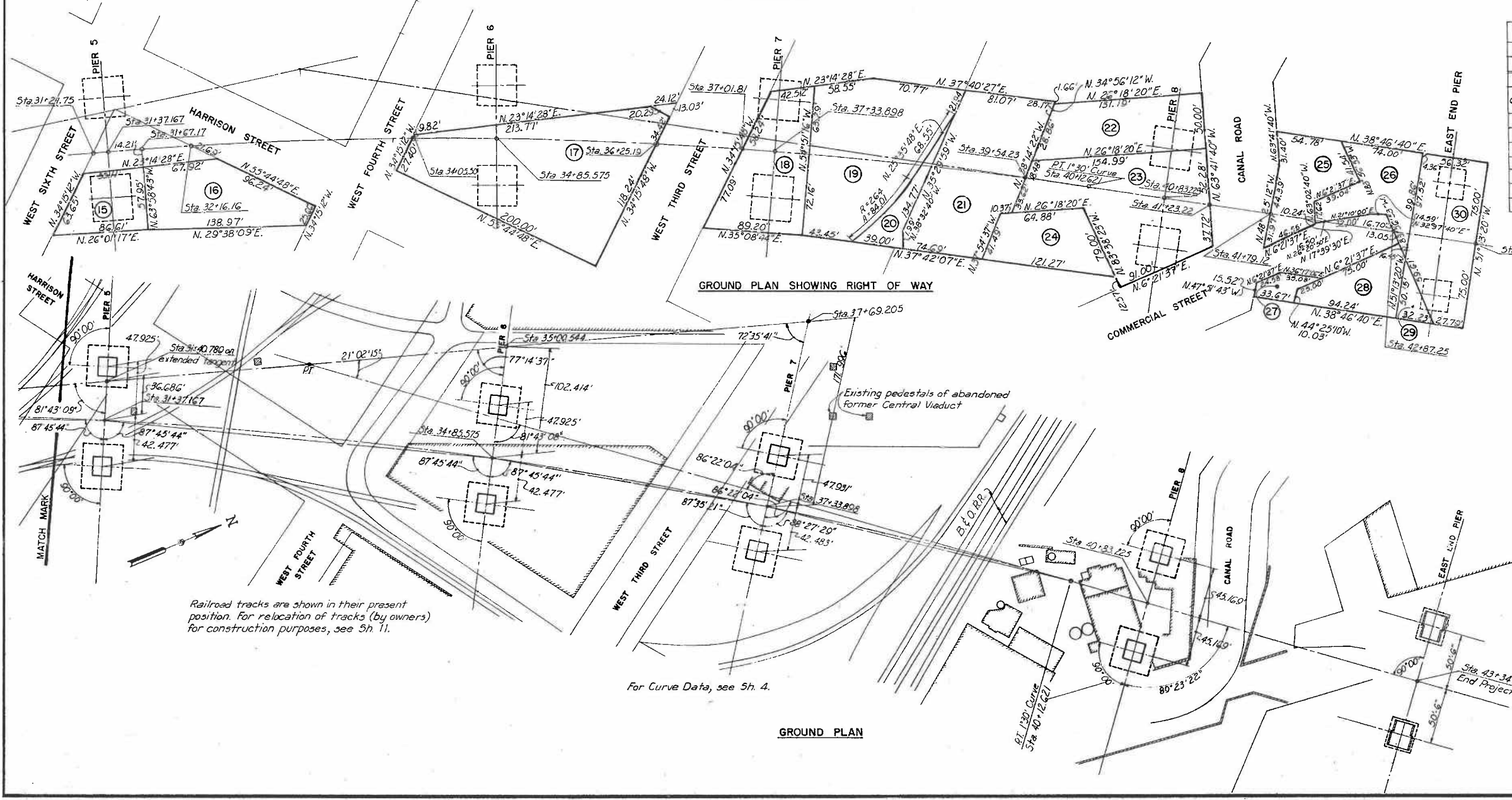
914-1A SHEET 1.04



CUYAHOGA COUNTY  
CITY OF CLEVELAND  
INNER BELT FREEWAY  
CENTRAL VIADUCT  
BR. NO. CU-42R-175



SOUTH ELEVATION



GROUND PLAN SHOWING RIGHT OF WAY

GROUND PLAN

RIGHT OF WAY ACQUISITION REQUIREMENTS			
PERMANENT ACQUISITION		AERIAL RIGHTS ONLY	
TRACT	OWNER	TRACT	OWNER
1, 3	Erie Railroad	2, 4	Erie Railroad
6, 9, 13, 15	C.C.C. & St. L. Railroad	3, 7, 8, 11, 12, 14, 16, 20	C.C.C. & St. L. Railroad
10	C.C.C. & St. L. Railroad	19, 21	B. & O. Railroad
17	United Garage & Service Corp.	24	Sidney C. Brant
18	B. & O. Railroad	25	W. & L. E. Railroad
22	Cleveland Trust Co.	26	Helen C. Lincoln
23	Mildred L. Richards	27	Wilson Terminals, Inc.
29	Louis J. Kaplan	28	Louis J. Kaplan
30	Helen C. Lincoln		

- Work included in this contract:
1. Two end piers
  2. Piers 1 to 8 inclusive
  3. Complete drainage and sewer system
  4. Removal of existing structures 1, 2, 3, and 4 from the Cuyahoga River
  5. Removal of 7 pedestals of former Central Viaduct
  6. Track modifications (force account by railroads)

Railroad tracks are shown in their present position. For relocation of tracks (by owners) for construction purposes, see Sh. 11.

For Curve Data, see Sh. 4.

PART 2

U. S. ROUTE 42 RELOCATION  
INNER BELT FREEWAY - CENTRAL VIADUCT

GENERAL PLAN AND ELEVATION  
STA. 30+95.00 TO STA. 43+34.725

CLEVELAND	CUYAHOGA COUNTY	OHIO
-----------	-----------------	------

SCALE 1" = 50'-0"

MADE AND DATE 1-10-54  
TRCD AND DATE 6-12-54  
CHK. SA. DATE 7-8-54

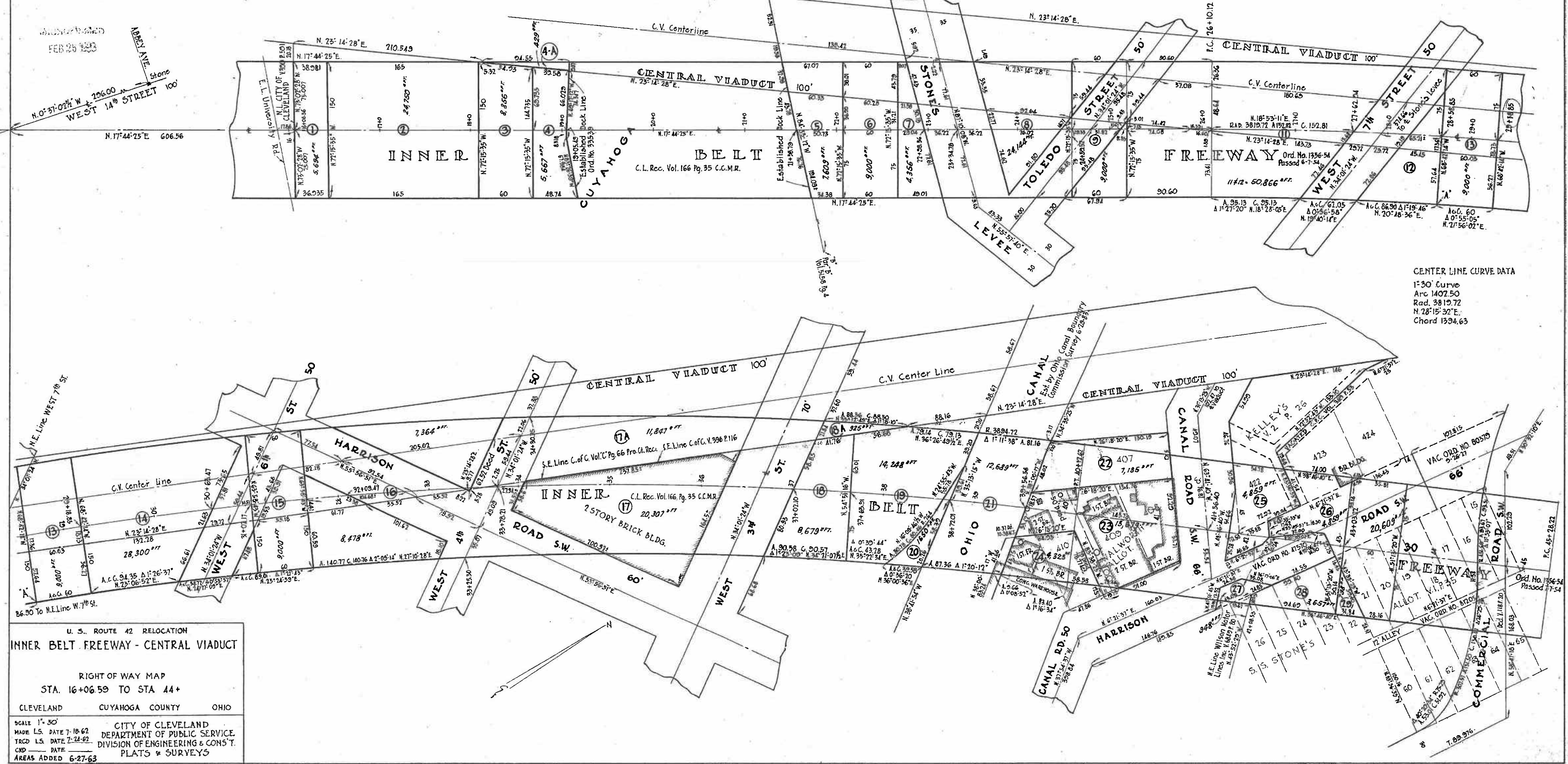
HOWARD, NEEDLES, TAMMEN & BERGENDOFF  
CONSULTING ENGINEERS  
CLEVELAND NEW YORK  
KANSAS CITY  
914-1A SHEET 1.05

FED. RD. DIVISION	STATE	FEDERAL AID PROJECT	FISCAL YEAR
2	OHIO	UI 1057 (4)	43

5-A

CUYAHOGA COUNTY  
CITY OF CLEVELAND  
INNER BELT FREEWAY  
CENTRAL VIADUCT  
C.U.Y. - 42 R. - 17.50

RIGHT OF WAY ACQUISITION REQUIREMENTS					RIGHT OF WAY ACQUISITION REQUIREMENTS				
PARCEL	GRANTOR	GRANTEE	DEED	RIGHTS	PARCEL	GRANTOR	GRANTEE	DEED	RIGHTS
1	Erie Railroad Company	City of Cleveland	V.8455 P.11	Permanent R. of W.	20	C.C.C. & S.L. R.R. Co. & Real Estate Imp. Co.	City of Cleveland	V.8673 P.24	Aerial Rights & Sewer Easement
2	Erie Railroad Company	City of Cleveland	Par.2 V.8455 P.11	Aerial Rights	21	The Baltimore & Ohio R.R. Co.	City of Cleveland	V.8651 P.471	Aerial Rights
3	Erie Railroad Company	City of Cleveland	Par.3 V.8455 P.11	Permanent R. of W.	27	The Cleveland Trust Co.	City of Cleveland	V.8723 P.23	Permanent acquisition
4	Erie Railroad Company	City of Cleveland	Par.4 V.8455 P.11	Aerial Rights	23	Mildred L. Richards	City of Cleveland	V.8455 P.40	Permanent acquisition
5-7-8-11-12-14	The C.C.C. & S.L. & N.Y.C. R.R. Co's.	City of Cleveland	V.8673 P.23	Aerial Rights	24	Sidney C. Brant	City of Cleveland	V.8441 P.61	Permanent acquisition
6	The C.C.C. & S.L. & N.Y.C. R.R. Co's.	City of Cleveland	Par.1 V.8673 P.23	Permanent acquisition	25	Wheeling & Lake Erie R.R.	City of Cleveland		Aerial Rights
9	The C.C.C. & S.L. & N.Y.C. R.R. Co's.	City of Cleveland	Par.2 V.8673 P.23	Permanent acquisition	26-30	Helen C. Lincoln	City of Cleveland	V.8455 P.42	Permanent acquisition
13	The C.C.C. & S.L. & N.Y.C. R.R. Co's.	City of Cleveland	Par.3 V.8673 P.23	Permanent acquisition	27	Wilson Terminals Inc.	City of Cleveland	V.8486 P.33	Permanent acquisition
15	The C.C.C. & S.L. & N.Y.C. R.R. Co's.	City of Cleveland	Par.4 V.8673 P.23	Permanent acquisition	28	Louis J. Kaplan	City of Cleveland	Vol.8430 P.12	Permanent acquisition
16	The C.C.C. & S.L. & N.Y.C. R.R. Co's.	City of Cleveland	V.8673 P.23	Aerial Rights	29	Louis J. Kaplan	City of Cleveland	V.8430 P.12	Permanent acquisition
17	United Garage & Service Corp. d/b/a	City of Cleveland	V.8068 P.460	Permanent acquisition	4A	N.Y. Penna and Ohio R.R.	City of Cleveland	Vol.900 P.54	Permanent acquisition
18	The Baltimore & Ohio R.R. Co.	City of Cleveland	V.8835 P.126	Permanent acquisition	17A	Margaret Stone, S.L.M. Barlow, Cass, Mrs. Elizabeth Smith	City of Cleveland	V.396 P.116	Permanent acquisition
19	The Baltimore & Ohio R.R. Co.	City of Cleveland	V.8835 P.126	Permanent acquisition	18A	W.J. Gordon	City of Cleveland	Par.4 V.8673 P.23	Permanent acquisition



CENTER LINE CURVE DATA  
1°30' Curve  
Arc 1407.50  
Rad. 3812.72  
N. 28°15'32"E.  
Chord 1934.63

U. S. ROUTE 42 RELOCATION  
INNER BELT FREEWAY - CENTRAL VIADUCT

RIGHT OF WAY MAP  
STA. 16+06.59 TO STA. 44+

CLEVELAND CUYAHOGA COUNTY OHIO

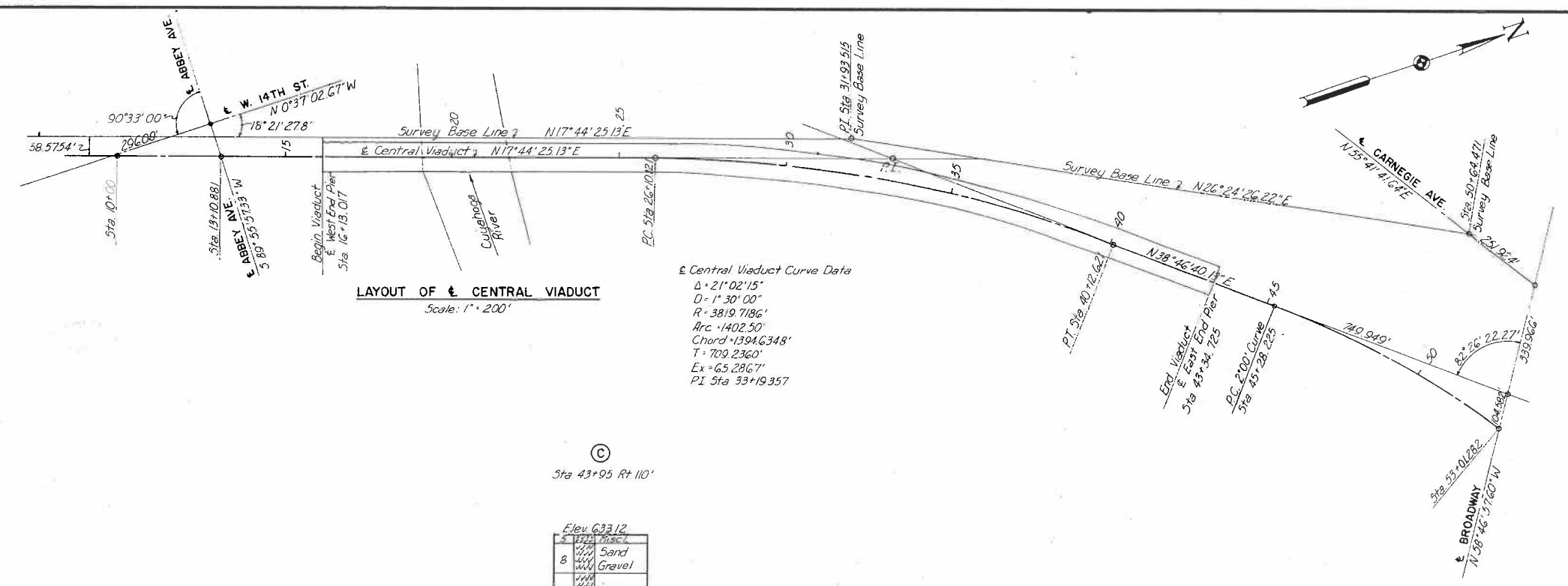
SCALE 1" = 50'  
MADE L.S. DATE 7-18-62  
TRCD L.S. DATE 7-24-62  
CND DATE  
AREAS ADDED 6-27-63

CITY OF CLEVELAND  
DEPARTMENT OF PUBLIC SERVICE  
DIVISION OF ENGINEERING & CONST.  
PLATS & SURVEYS

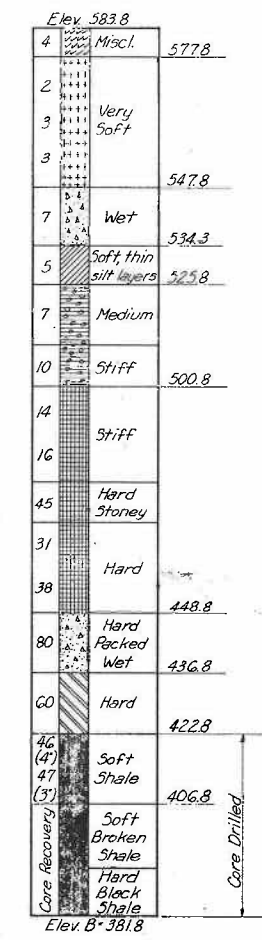
RECORDED  
FEB 15 1954

FED. ROAD DIV. NO.	STATE	FEDERAL AID PROJECT NO.	TYPE FUNDS	7
2	OHIO			122

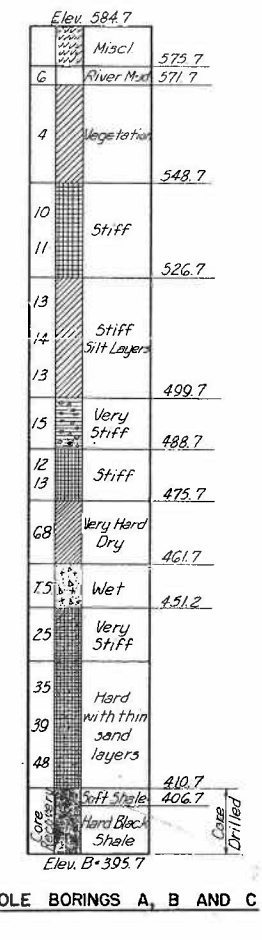
CUYAHOGA COUNTY  
CITY OF CLEVELAND  
INNER BELT FREEWAY  
CENTRAL VIADUCT  
CUY - 42 R - 17.50



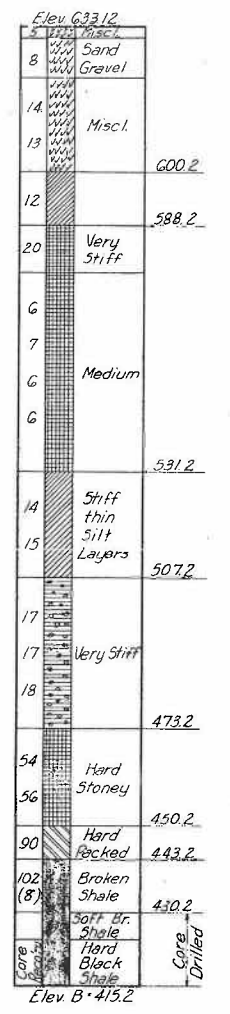
(A)  
Sta. 29+53 Rt. 265



(B)  
Sta. 37+10 Rt. 205'



(C)  
Sta. 43+95 Rt. 110'



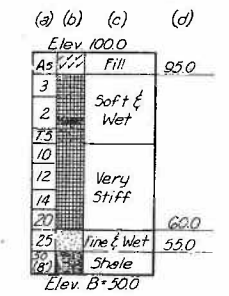
TEST HOLE BORINGS A, B AND C

GENERAL NOTES

SOIL LEGEND

- Misc.
- Fill
- Clay
- Sand
- Silt
- Silty Sand
- Silty Clay
- Sandy Clay
- Pebbly Clay
- Sand and Gravel
- Silt, Sand and Gravel
- Sand, Clay and Gravel
- Rock
- Ground Water
- Trap Sample T.S.
- Ruger Sample A.S.
- Tube Sample T.T.

BORING LEGEND



With reference to above example;  
 1. In column (a) the figures 2, 3, etc. are the hammer blows required to advance the casing one foot unless otherwise noted.  
 2. Column (b) shows the legend of soil types and ground water elevation.  
 3. Column (c) shows soil classification. Elev. 100.0 is the ground level and Elev. B = 50.0 is the bottom of the boring.  
 4. Column (d) shows the intermediate elevations of limits of different soil layers.

Misc. Notes: Vertical scale for boring 1" = 20'.  
 All samples taken with Split Type Tube, 2" O.D. x 15" I.D. and 4" steel casing unless otherwise noted.  
 Hammer wt. 140. Average fall 30'.

U. S. ROUTE 42 RELOCATION  
**INNER BELT FREEWAY - CENTRAL VIADUCT**  
 BR. NO. CU - 42 R - 17.5.  
**GENERAL LAYOUT AND TEST HOLE BORINGS**

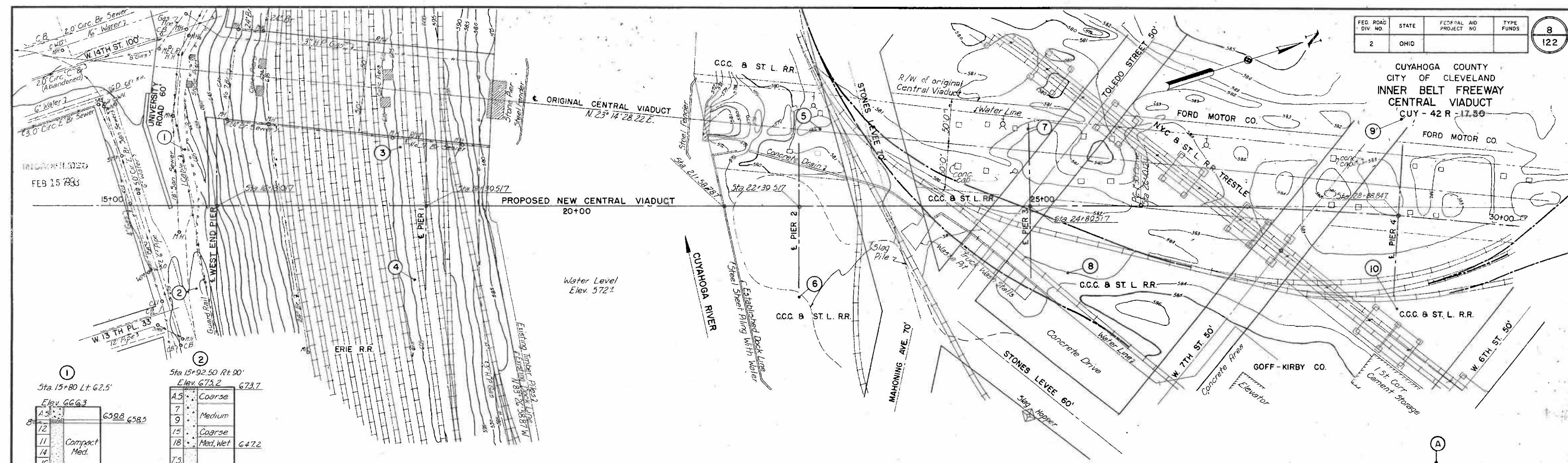
CLEVELAND	CUYAHOGA COUNTY	OHIO
-----------	-----------------	------

SCALE: Vertical 1" = 20'  
 MADE: 6.5. DATE: 1-27-54  
 TRCD: M.A.M. DATE: 2-6-54  
 C.D.: C.L.G. DATE: 2-25-54

HOWARD, NEEDLES, TAMMEN & BERGENDOFF  
 CONSULTING ENGINEERS  
 KANSAS CITY CLEVELAND NEW YORK

914-1A SHEET 2.07

CUYAHOGA COUNTY  
CITY OF CLEVELAND  
INNER BELT FREEWAY  
CENTRAL VIADUCT  
'CUY - 42 R - 17.5'



1  
Sta. 15+80 Lt. 62.5'  
Elev. 666.3

12	Compact Med.	659.8
11	Compact Med.	658.5
14	Compact Med.	
16	Compact Med. Wet	
18	Compact Med. Wet	
15	Compact Fine Wet	615.0
17	Stiff	
11	Stiff	
6	Medium	
16		
18		
15	Very Stiff	
15		
20		

Elev. B = 546.3

2  
Sta. 15+92.50 Rt. 90'  
Elev. 673.2

15	Coarse	673.7
7	Medium	
9	Medium	
15	Coarse	
18	Med. Wet	647.2
7.3	Med. Wet	
18	Med. Wet	
35	Fine Wet	617.2
7.3	Fine Wet	614.2
9	Stiff	
15	Very Stiff	
14		
14		
15	Very Stiff	
12		
17	Very Stiff	
9	Stiff	
7	Medium	
14		
15	Very Stiff	
12		
17	Very Stiff	
9	Stiff	
7	Medium	482.5
7.5	Coarse & Wet	

Elev. B = 545.2

3  
Sta. 18+11.50 Lt. 64'  
Elev. 604.0

4	Cinder & Slag	592.0
7	Fine, Brown	578.0
8	Grey	578.0
6	Med Grey	573.0
6	Med Grey	
14	Very Stiff	
9	Stiff	
7	Medium	
14		
15	Very Stiff	
12		
17	Very Stiff	
9	Stiff	
7	Medium	482.5
7.5	Coarse & Wet	

Elev. B = 474.0

4  
Sta. 18+27.50 Rt. 80'  
Elev. 604.2

5	Cinder	591.7
12	Fine, Brown	585.2
8	Grey	578.0
6	Medium	572.2
6	Soft	
7	Medium	
11	Stiff	
11	Stiff	
17	Very Stiff	
11	Stiff	
25	Hard Stony	
26		
25	Very Stiff	
7.1	Stiff	486.2
7.5	Coarse Wet	475.2
2.2	Very Stiff with Thin Silt Layers	461.7
7.5	Wet	

Elev. B = 454.2

5  
Sta. 22+62 Lt. 85'  
Elev. 581.5

3	Misc.	577.5
4	Loam	574.2
4	Wet	570.5
7	Wet	564.0
7.5	Med. Wet	
7.5	Coarse Wet	548.5
9	Stiff	538.5
7	Medium	528.5
8	Medium	
8	Medium	
12	Stiff	
15	Very Stiff	
37	Hard	479.5
9	Medium	468.0
7.5	Coarse Wet	457.5
18	Hard	447.5
18	Hard	446.0
18	Hard	443.0
50 (8)	Hard	429.0

Elev. B = 427.0

6  
Sta. 22+40 Rt. 100.5'  
Elev. 580.6

4.5	Brown Wet @ 45'	575.1
2	Soft	569.6
7	Medium	559.1
8	Coarse	548.1
7.5	Coarse	548.1
9	Stiff	538.6
11	Stiff	
12		
18	Very Stiff	

Elev. B = 500.6

7  
Sta. 24+89 Lt. 85'  
Elev. 581.9

3	Wet under Clay	573.4
2	Loam	569.9
2	Fine Wet	563.9
2	Fine Wet	559.6
4	Med Wet	
7.5	Wet	548.9
12	Stiff with Thin Silt Layers	
13		
8	Med with Thin Silt Layers	518.9
9	Medium	
12	Stiff	
12	Stiff	501.9
12	Stiff	

8  
Sta. 25+30 Rt. 75'  
Elev. 583.0

4	Slag & Clay	579.0
4	Loam	574.0
6	Medium	568.0
2	Fine Wet	564.0
5	Fine Wet	558.0
6		
7.5	Wet	544.0
14	Stiff with Thin Silt Layers	
13	Stiff with Thin Silt Layers	524.0
20	Very Stiff	514.0
16	Very Stiff	
14	Stiff	

Elev. B = 503.0

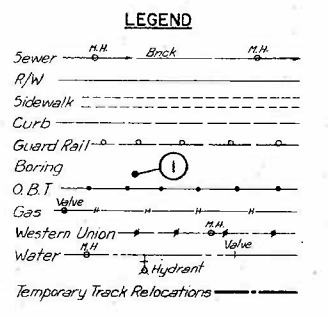
9  
Sta. 28+85 Lt. 105.5'  
Elev. 582.0

4	Misc. Cinder etc	576.0
3	Soft Wet	
5	Med with Vegetation	565.0
2	Soft	560.0
2	Soft Lit. Clay	552.0
4	Wet & Gravel Loose	533.0
19		
22	Very Stiff	
19		
15	Stiff	

Elev. B = 502.0

10  
Sta. 28+94 Rt. 104.5'  
Elev. 583.3

4	Misc.	575.3
6	Brn & Grey	
10	Med. Veg.	566.3
2		
2	Very Soft	
2		548.3
6		
7.5		531.3
16		
15	Very Stiff	
15	Very Stiff	503.3



Notes: For general notes on borings and soil and boring legends, see Sheet 7.

PART 3

U. S. ROUTE 42 RELOCATION  
INNER BELT FREEWAY - CENTRAL VIADUCT  
BR. NO. CU - 42 R - 17.5'

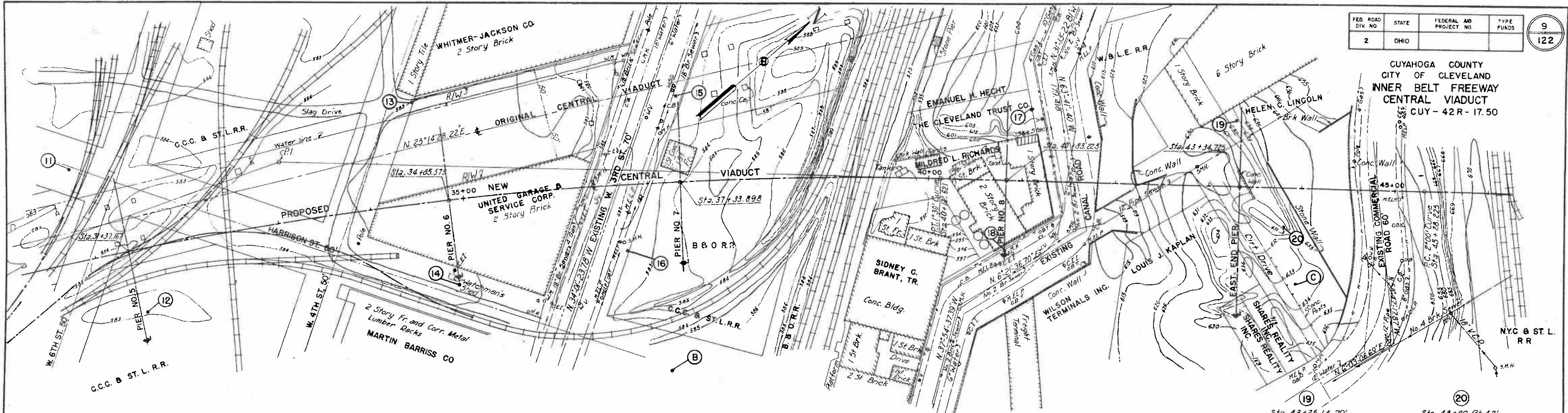
EXISTING CONDITIONS AND  
TEST HOLE BORINGS

CLEVELAND CUYAHOGA COUNTY OHIO

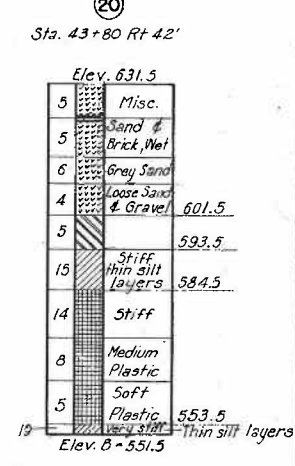
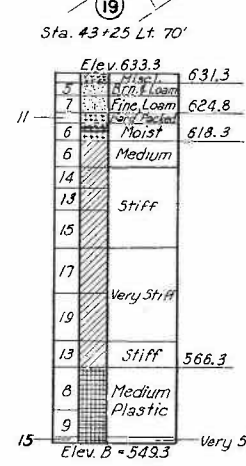
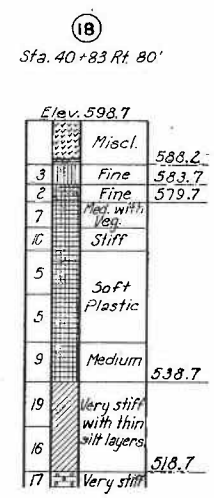
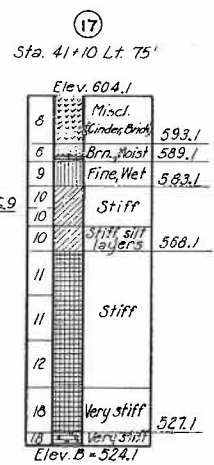
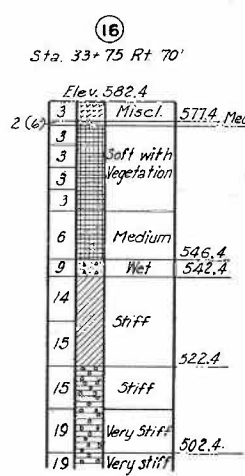
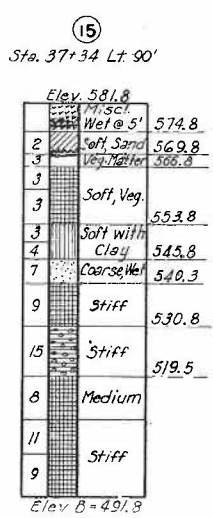
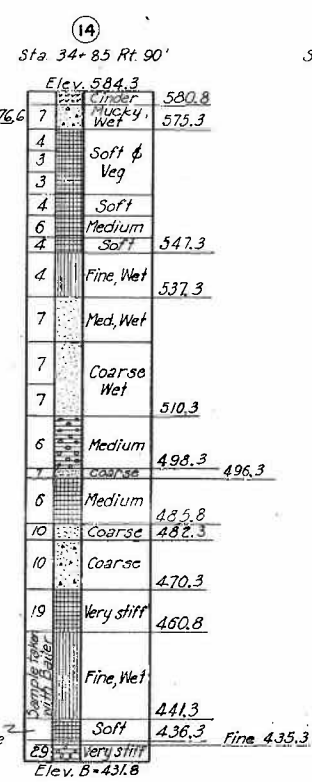
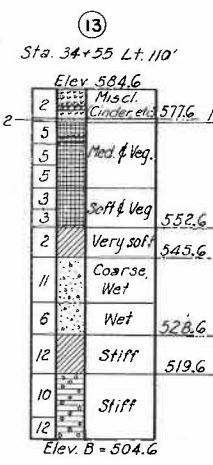
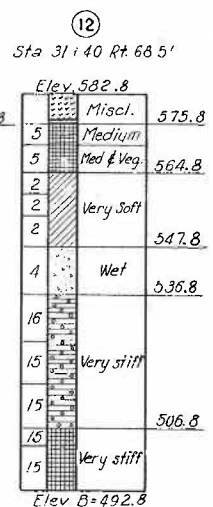
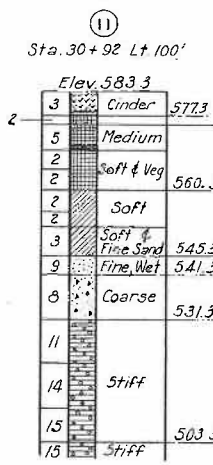
SCALE 1" = 40', 20'  
MADRID, G.S. DATE 1/12/54  
TRCD 6.1. DATE 2-11-54  
CKD C.L.C. DATE 2-25-54

HOWARD, NEEDLES, TAMMEN & BERGENDOFF  
CONSULTING ENGINEERS  
CLEVELAND NEW YORK  
KANSAS CITY CLEVELAND NEW YORK  
914-1A SHEET 2 OF 68

CUYAHOGA COUNTY  
CITY OF CLEVELAND  
INNER BELT FREEWAY  
CENTRAL VIADUCT  
CUY-42R-17.50



REVISED  
FEB 15 1953



Notes: For general notes on borings and soil and boring legend, see Sheet 7.  
Vertical scale for borings: 1" = 20'  
For existing conditions legend, see Sheet 8.

PART 3

U. S. ROUTE 42 RELOCATION  
INNER BELT FREEWAY - CENTRAL VIADUCT  
BR. NO CU - 42R-17.5

EXISTING CONDITIONS AND  
TEST HOLE BORINGS

CLEVELAND CUYAHOGA COUNTY OHIO

SCALE: 1" = 40', 20'  
MADE 6.5 DATE 1.13.54  
TRD:R.K. DATE 1.12.54  
CKD: F.G. DATE 2.26.54

HOWARD, NEEDLES, TAMMEN & BERENDOFF  
CONSULTING ENGINEERS,  
KANSAS CITY CLEVELAND NEW YORK

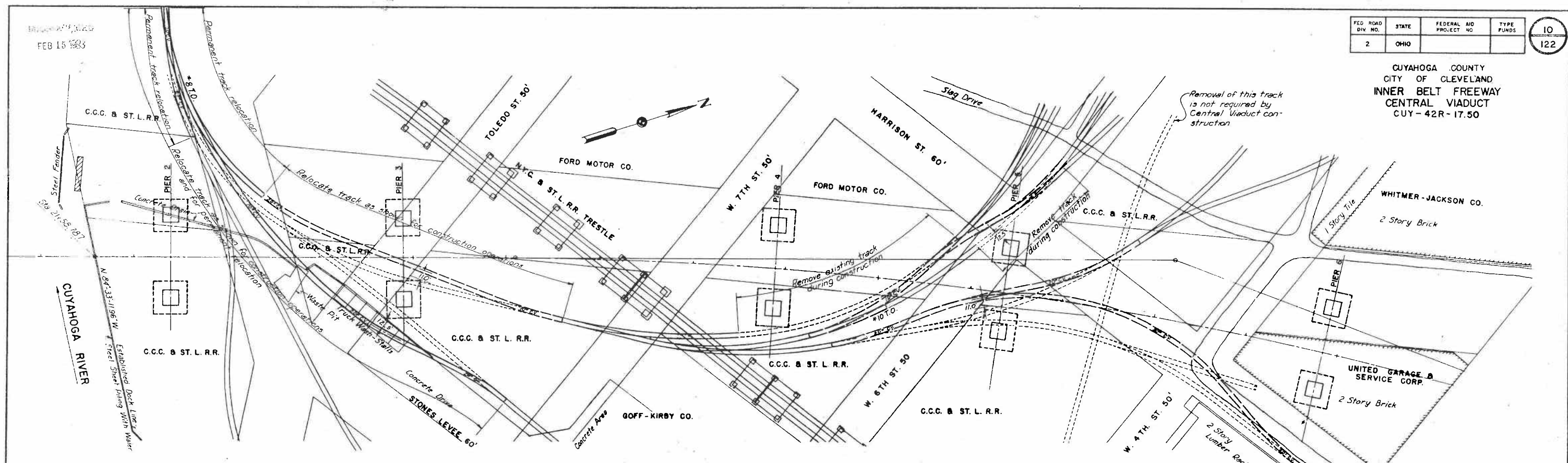
914-1A SHEET 2.09

105

ENGR. 10120  
FEB 15 1953

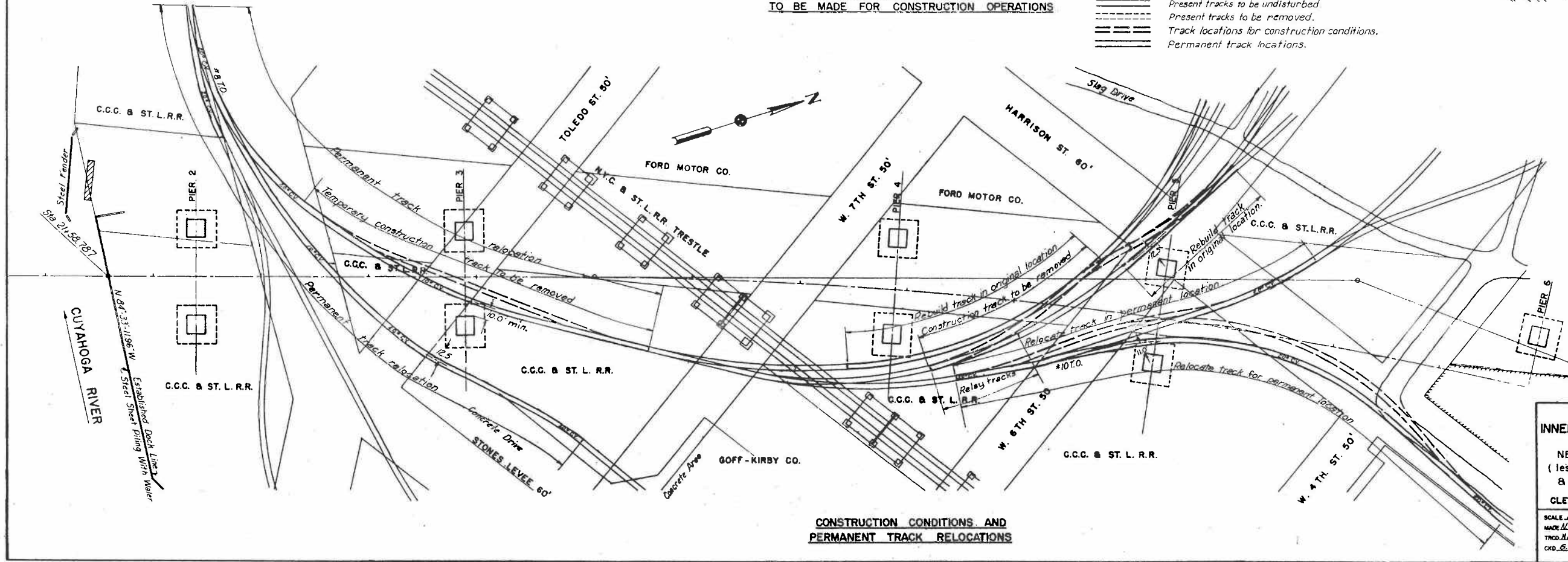
FED. ROAD DIV. NO.	STATE	FEDERAL AID PROJECT NO.	TYPE FUNDS	10
2	OHIO			122

CUYAHOGA COUNTY  
CITY OF CLEVELAND  
INNER BELT FREEWAY  
CENTRAL VIADUCT  
CUY-42R-17.50



EXISTING CONDITIONS AND TRACK CHANGES  
TO BE MADE FOR CONSTRUCTION OPERATIONS

LEGEND  
 ——— Present tracks to be undisturbed  
 - - - Present tracks to be removed.  
 - - - Track locations for construction conditions.  
 ——— Permanent track locations.



CONSTRUCTION CONDITIONS AND  
PERMANENT TRACK RELOCATIONS

PART 3

U. S. ROUTE 42 RELOCATION  
 INNER BELT FREEWAY - CENTRAL VIADUCT  
 BR. NO. CUY-42R-17.50  
 NEW YORK CENTRAL RAILROAD COMPANY  
 (lessee of the railway and property of the C.C.C. & ST. L. R.R.) TRACK MODIFICATIONS  
 CLEVELAND CUYAHOGA COUNTY OHIO

SCALE 1" = 50'  
 MADE N.A.M. DATE 4-12-54 HOWARD, NEEDLES, TAMMEN & BERSENDORFF CONSULTING ENGINEERS  
 TRCD N.A.M. DATE 8-13-54 KANSAS CITY CLEVELAND NEW YORK  
 CND G.A. DATE 4-15-54 914-1A SHEET 2.10

STATE OF OHIO  
DEPARTMENT OF HIGHWAYS

I-1057(2)

APR 11 1962  
GROUND PHOTOLAB

INNER BELT FREEWAY

CUY-42R-17.43  
CUYAHOGA COUNTY  
CITY OF CLEVELAND

LIMITED ACCESS

PART 4 - WEST APPROACH TO CENTRAL VIADUCT

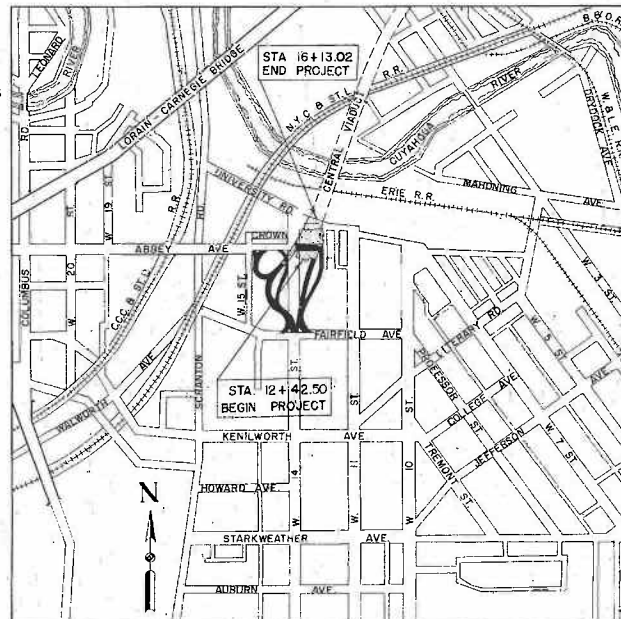
IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR OF HIGHWAYS IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 REVISED CODE OF OHIO.

DATE OF DESIGN  
FEB 25 1962

INDEX OF SHEETS

1 TITLE SHEET	37 RAMP W-1 RETAINING WALL
2 SCHEMATIC PLAN AND PROFILE	38 ABUTMENT W-2
3 RIGHT OF WAY PLAN	39-44 PIERS
4-5 GENERAL NOTES	45 FRAMING PLAN
6 SUMMARY OF QUANTITIES	46 BEAM ELEVATIONS
7-8 TYPICAL CROSS SECTIONS	47-49 GIRDER ELEVATIONS
9 PAVEMENT DETAILS	50 GIRDER FIELD SPLICES
10-11 PROFILES-RAMPS AND LANES	51 STIFFENER AND JACKING DIAPHRAGM DETAILS
12 INTERSECTION DETAILS	52 DEAD LOAD DEFLECTION DIAGRAMS
13-14 MISCELLANEOUS DETAILS	53 CROSS FRAMES
15 EXISTING UTILITIES	54 CROSS FRAMES AND SUPERSTRUCTURE DETAILS
16-16A LIGHTING DETAILS	55-58 BOX GIRDER
17 DRAINAGE PLAN	57-58 EXPANSION JOINTS
18-19 DRAINAGE PROFILES	59 SHOES
19A SPECIAL FRAME AND MANHOLE COVERS	60-61 SLAB PLANS
20-28 CROSS SECTIONS	62 SLAB DETAILS
	63 DECK CONTOURS AND HANDRAIL SPACING
	64 SLAB PLAN RAMP W-2
	65 DRAINAGE DETAILS
	66 LIGHTING DETAILS
	67 HANDRAIL DETAILS
<b>STRUCTURE</b>	
29 GENERAL PLAN AND ELEVATION	
30 GENERAL NOTES	
31 ESTIMATED QUANTITIES	
32 GROUND PLAN AND BORINGS	
33-35 REINFORCING SCHEDULE	
36 ABUTMENT W-1	

NOTE:  
THIS SET OF PLANS, PART 4 OF THE INNER BELT FREEWAY, EXTENDS FROM WEST END PIER OF CENTRAL VIADUCT WESTWARD TO STA. 12+42.50.  
PARTS 1 THRU 3 ARE UNDER PREVIOUS CONTRACTS.



DELIVERY POINT: N.Y.C. & ST.L.R.R. AVERAGE HAUL: 1/2 MILE

LOCATION PLAN

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO DEPARTMENT OF HIGHWAYS, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.  
I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH IN THE PLANS AND ESTIMATE.  
THE RIGHT OF WAY FOR THIS IMPROVEMENT WILL BE PROVIDED BY THE STATE OF OHIO.

APPROVED DATE 5-7-56 Louis R. Drasler DIRECTOR OF PUBLIC SERVICE, CITY OF CLEVELAND  
APPROVED DATE 6-7-56 Arthur A. ... DIVISION DEPUTY DIRECTOR  
APPROVED DATE 6-21-56 John D. ... DEPUTY DIRECTOR OF PLANNING AND PROGRAMMING  
APPROVED DATE 6-18-56 ... ENGINEER OF BRIDGES  
APPROVED DATE 6-18-56 ... ENGINEER OF LOCATION AND DESIGN  
APPROVED DATE 6-19-56 H.F. ... DEPUTY DIRECTOR OF DESIGN AND CONSTRUCTION  
APPROVED DATE 6-21-56 ... FIRST ASSISTANT DIRECTOR  
APPROVED DATE 6-21-56 ... DIRECTOR OF HIGHWAYS

APR 11 1962  
GROUND PHOTOLAB

LINE DATA

BEGIN PROJECT STA. 12+42.50	
END PROJECT STA. 16+13.02	
NET LENGTH OF PROJECT (STRUCTURE) ADDITIONS	370.52 LIN. FT. OR 0.070 MILE
FREEWAY STA. 5+00(±) TO STA. 12+42.50	742.50 ± LIN. FT.
ABBEY AVE. STA. 3+97.42 TO STA. 7+86.87	689.45 LIN. FT.
TOTAL NET LENGTH OF WORK	1802.47 LIN. FT. OR 0.341 MILE

PREPARED AND RECOMMENDED BY  
HOWARD, NEEDLES, TAMMEN & BERGENDOFF  
CONSULTING ENGINEERS  
KANSAS CITY CLEVELAND NEW YORK

H. G. SOURS  
ASSOCIATE  
COLUMBUS

SUPPLEMENTAL SPECIFICATIONS

NUMBER	DATE	NUMBER	DATE
B-119 REV	12-14-55	M-206.14	7-15-49
L-209.12	7-17-54	M-108.23	REV 4-20-56
5	6-8-55		
M-110.27	9-9-52		
S-114	6-30-55		



PORTION TO BE IMPROVED  
OTHER HIGHWAYS & STREETS

STANDARD DRAWINGS

NUMBER	DATE	NUMBER	DATE
L-3	4-1-50	I-8 M.H. NO.1	5-1-52
L-3-A	4-1-50	I-15 NO.1	8-1-55
RI-1	1-3-55	I-15 NO.2	12-1-54
B-T-71R	3-2-53	BT 50-70-71E	10-1-47
LJ NO.1	7-1-56	AS-1-54	12-1-54
I-1, 2, 3, 4, & 5	2-20-49	G-7.07	6-1-56
I-8CB NO.2-2A&B	5-1-52	I-8MH NO.1-A	1-3-55
I-8CB NO.3	5-1-52	I-12	7-1-54
I-8CB NO.3A	5-1-52	L-1	4-1-50
I-8 I NO.2	12-1-54	TJ	5-1-56
S-1	7-1-58	I-8 M.H. NO.2	5-1-52

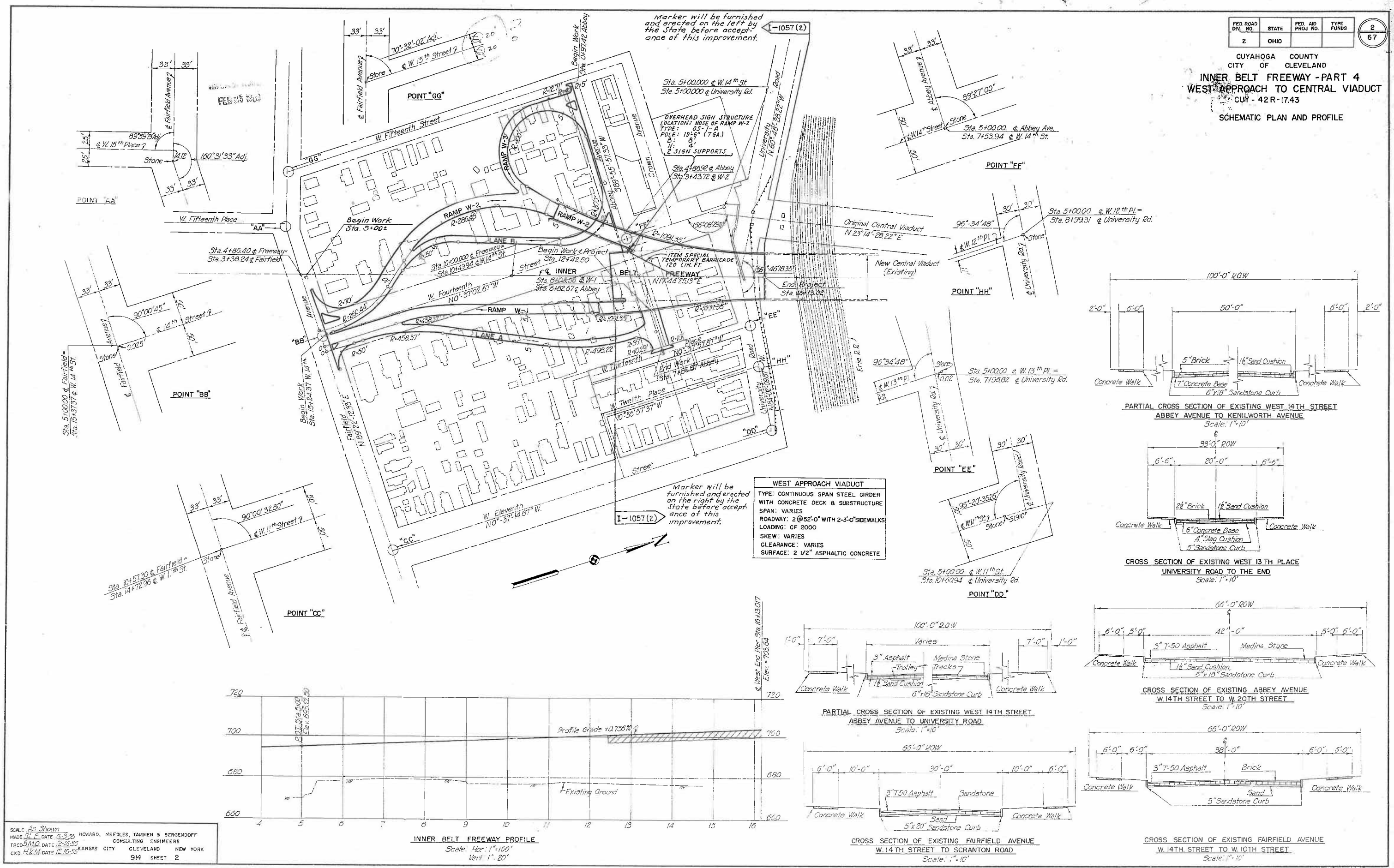
DEPARTMENT OF COMMERCE  
BUREAU OF PUBLIC ROADS

APPROVED \_\_\_\_\_ DATE \_\_\_\_\_  
DISTRICT ENGINEER

FILE NO. CUYAHOGA COUNTY 00079R  
SEC. \_\_\_\_\_  
DATE OF LETTING \_\_\_\_\_, 195\_\_\_\_  
CONTRACT NO. \_\_\_\_\_

FED. ROAD DIV. NO.	STATE	FED. AID PROJ. NO.	TYPE FUNDS	2
2	OHIO			67

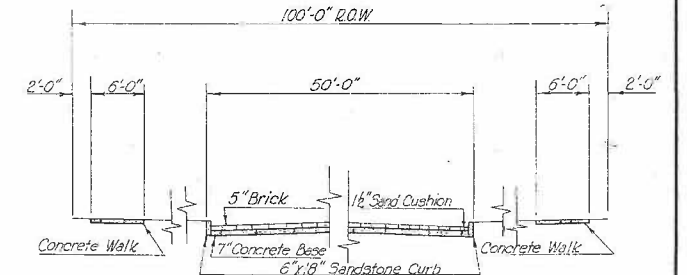
CUYAHOGA COUNTY  
CITY OF CLEVELAND  
**INNER BELT FREEWAY - PART 4**  
**WEST APPROACH TO CENTRAL VIADUCT**  
CUY - 42R-1743  
SCHEMATIC PLAN AND PROFILE



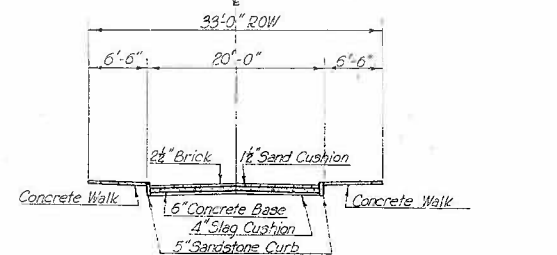
Marker will be furnished and erected on the left by the State before acceptance of this improvement.

Marker will be furnished and erected on the right by the State before acceptance of this improvement.

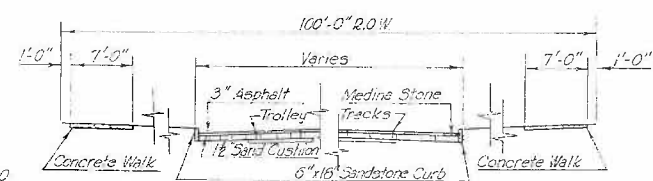
**WEST APPROACH VIADUCT**  
TYPE: CONTINUOUS SPAN STEEL GIRDER WITH CONCRETE DECK & SUBSTRUCTURE  
SPAN: VARIES  
ROADWAY: 2 @ 52'-0" WITH 2-3'-0" SIDEWALKS  
LOADING: OF 2000  
SKEW: VARIES  
CLEARANCE: VARIES  
SURFACE: 2 1/2" ASPHALTIC CONCRETE



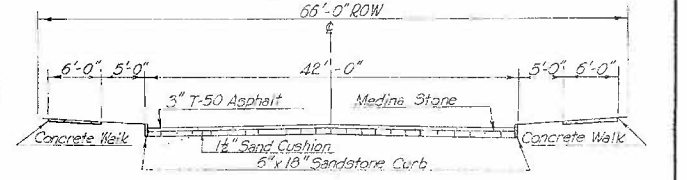
PARTIAL CROSS SECTION OF EXISTING WEST 14TH STREET  
ABBAY AVENUE TO KENILWORTH AVENUE  
Scale: 1"=10'



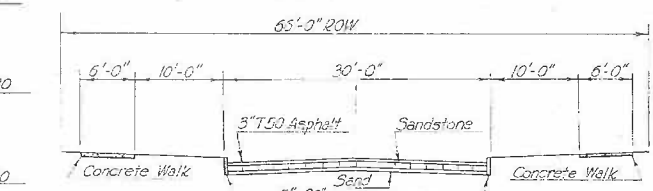
CROSS SECTION OF EXISTING WEST 13TH PLACE  
UNIVERSITY ROAD TO THE END  
Scale: 1"=10'



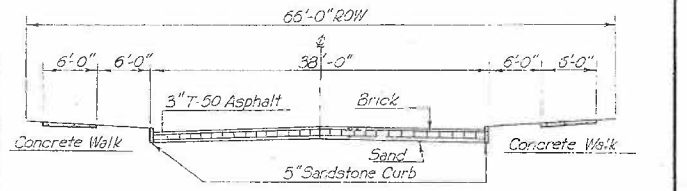
PARTIAL CROSS SECTION OF EXISTING WEST 14TH STREET  
ABBAY AVENUE TO UNIVERSITY ROAD  
Scale: 1"=10'



CROSS SECTION OF EXISTING ABBAY AVENUE  
W. 14TH STREET TO W. 20TH STREET  
Scale: 1"=10'



CROSS SECTION OF EXISTING FAIRFIELD AVENUE  
W. 14TH STREET TO SCRANTON ROAD  
Scale: 1"=10'



CROSS SECTION OF EXISTING FAIRFIELD AVENUE  
W. 14TH STREET TO W. 10TH STREET  
Scale: 1"=10'

SCALE AS SHOWN  
MADE BY DATE HOWARD, NEEDLES, TAMMEN & BERGENDOFF CONSULTING ENGINEERS  
TPCO 5/1/62 DATE 12/11/55 KANSAS CITY CLEVELAND NEW YORK  
CKD 4/1/61 DATE 12/10/55 914 SHEET 2

REV. 7-5-56  
REV. 7-27-56

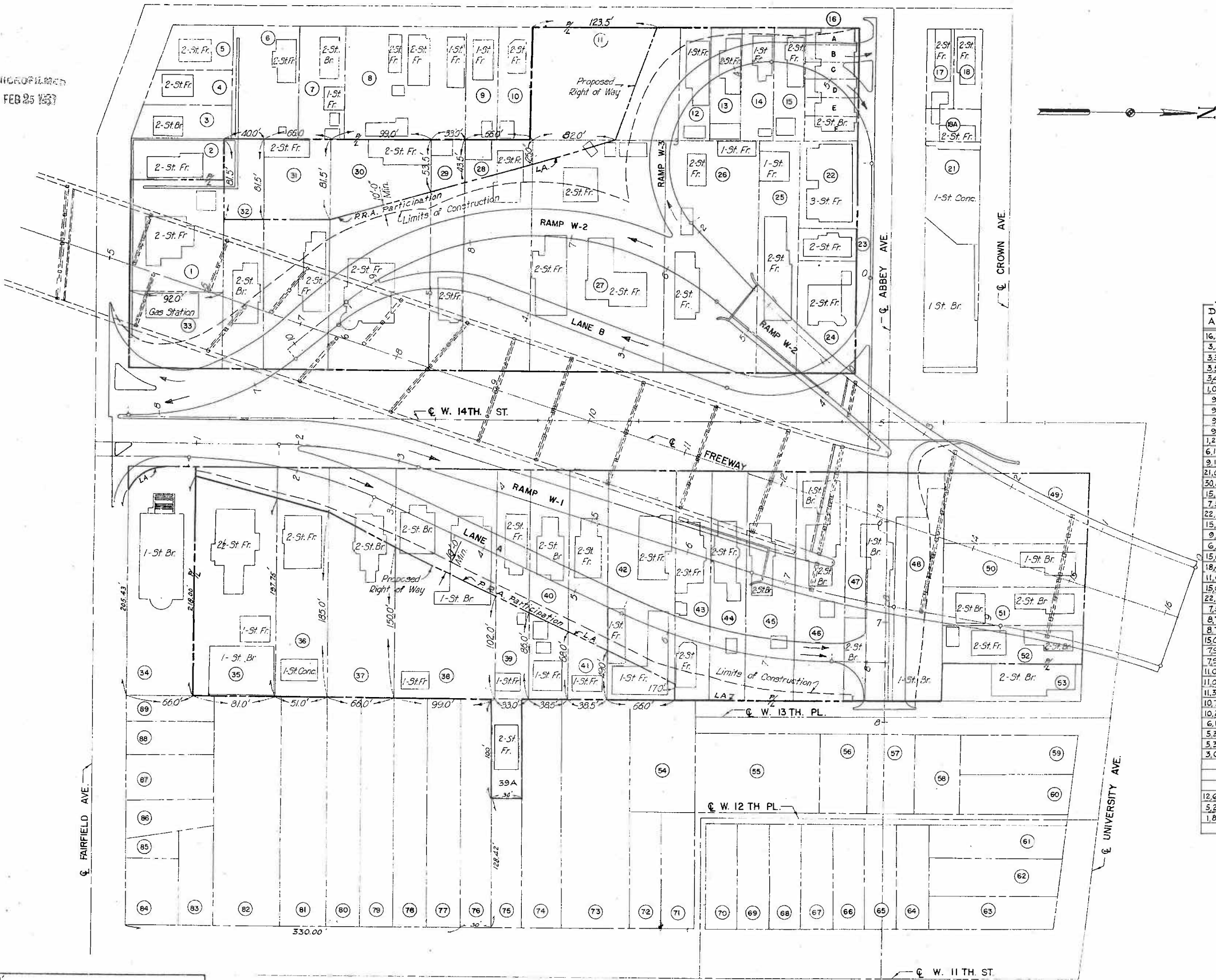


FED. ROAD DIV. NO.	STATE	FED. AID PROJ. NO.	TYPE FUNDS	3
2	OHIO			67

CUYAHOGA COUNTY  
CITY OF CLEVELAND  
**INNER BELT FREEWAY - PART 4**  
WEST APPROACH TO CENTRAL VIADUCT  
CUY-42 R-17.43

RIGHT OF WAY

MICROFILMED  
FEB 25 1987



DEED AREA	TAKE	PARCEL NO.	OWNER	RESIDUE
16,352.00	4,844.00	11	<del>Reeves Cleaning Co.</del> Hillcomb Realty Co.	11,508.00
3,360.00	ALL	12	Thos & J. Barulski	0.00
3,360.00	"	3	Mary Babincak	0.00
3,584.00	"	14	Meribetta & Wm. Mihalik	0.00
3,472.00	"	15	Mike & Anna Feketa	0.00
1,058.14	"	16-A	R.F.I. Sapienza & J.P. Arcara	0.00
926.29	"	16-B	Homer & Agnes Cox	0.00
928.29	"	16-C	John R. & Birdie T. Meltsch	0.00
921.94	"	16-D	Esther T. Williams	0.00
923.05	"	16-E	Dale & Vera Yeager	0.00
1,233.29	"	16-F	Roy & Helen Bartoszek	0.00
6,144.12	"	24	Rev. Andrew Chervashin	0.00
9,153.20	"	25	Joseph & Theodora Borczuk	0.00
21,063.40	"	26	John F. & Helen Marshal	0.00
30,233.28	29,208.28	27	Michael Mizenko	1,025.00
15,122.25	12,861.75	28	John Lias	2,260.50
7,562.94	5,962.44	29	Sam Broszko	1,600.50
22,693.77	16,011.27	30	Anthony J. Toman	6,682.50
15,135.14	9,754.14	31	Rose Thomas & E.C. Zigair	5,379.00
9,174.20	5,914.20	32	Carl & Anna Lemmerman	3,260.00
6,900.00	ALL	33	Sinclair Refining Co.	0.00
15,048.00	230.50	34	Greek Orthodox Church	14,817.50
18,468.00	1,630.12	35	Caroline Rice	16,837.87
11,628.00	1,867.87	36	Estelle Zwolinski & Agnes S. Toman	9,760.12
15,048.00	5,993.00	37	Dr. Martin Luther Evangelical Church	11,055.00
22,572.00	10,098.00	38	Slovak Lutheran Church - Congregation - Same Owner, Parc. 37	12,474.00
7,524.00	4,422.00	39	Slovak Lutheran Church - Congregation - Same Owner, Parc. 37	3,102.00
8,778.00	5,813.50	40	David Murad	2,964.50
8,778.00	6,525.75	41	David Murad	2,252.25
15,048.00	12,870.00	42	Walter & Amy R. Higs	2,178.00
7,980.00	ALL	43	Constantine Nicholan	0.00
7,980.00	"	44	<del>George &amp; Joseph Siebert</del> M & E. Adamczyk	0.00
11,019.24	"	45	Julius, Margt. & Wm. F. McGuire	0.00
11,019.24	"	46	Nellie Bukhair	0.00
11,343.00	"	47	Mervin & Ethel M. Cary	0.00
10,716.00	"	48	Earl T. Benjamin, TRS	0.00
10,228.96	"	49	Cleveland Trust Co.	0.00
6,124.61	"	50	Vincent & Mary Triner	0.00
5,375.63	"	51	Catherine Zemba	0.00
5,375.63	"	52	Petro Petrick	0.00
3,000.00	0.00	39-A	Same Owner, Parcel 37	3,000.00
12,656.25	ALL	1	<del>Sophia M. Mosko</del> W.G. Lewicky	0.00
5,263.78	"	22	<del>Josephine Belitto</del> H. Cesar & G.M. Finesilver	0.00
1,849.10	"	23	<del>Ernesto Rodriguez</del> Nelson N. Moss	0.00

SCALE 1" = 50'  
MADE BY DATE 12-19-55 GARD, NEEDLES, TAMMEN & BERGENDOFF  
TRCD. ENO. DATE 12-25 CONSULTING ENGINEERS  
CKD. LKAL. DATE 3-4-56 KANSAS CITY CLEVELAND NEW YORK  
914 SHEET 3

CUY-90-15.38-15.97 ±

FED. ROAD DIV. NO.	STATE	PROJ.
2	OHIO	I-1057(5)

117

IMPROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR OF HIGHWAYS IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 REVISED CODE OF OHIO.

MICROFILMED  
FEB 20 1968

STATE OF OHIO  
DEPARTMENT OF HIGHWAYS

INNER BELT FREEWAY

CUY - 42 - (17.43 - 18.02)  
CUYAHOGA COUNTY  
CITY OF CLEVELAND

MAR 5 1967  
GROUND PHOTOGRAPH

I-1057(5)  
LIMITED ACCESS

PART 5 EAST APPROACH TO CENTRAL VIADUCT

GRADE SEPARATION WITH NEW YORK, CHICAGO AND ST. LOUIS RAILROAD AND CLEVELAND UNION TERMINALS COMPANY

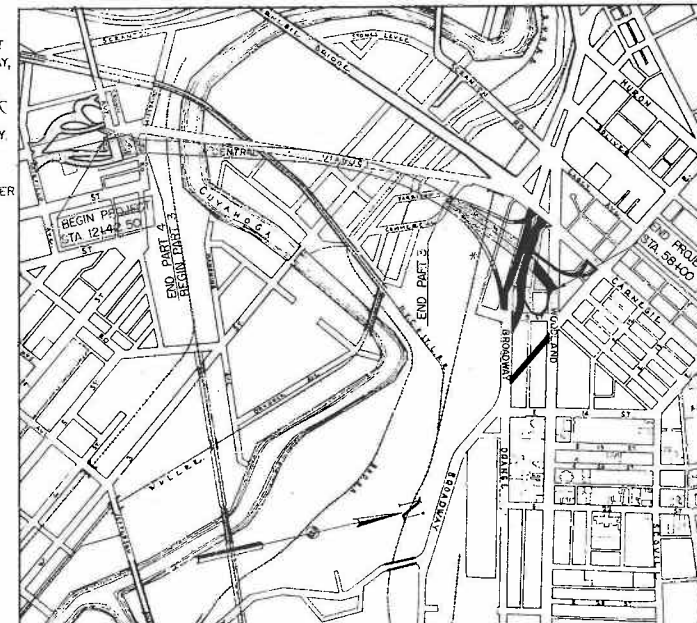
THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO DEPARTMENT OF HIGHWAYS, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH IN THE PLANS AND ESTIMATE.

THE RIGHT OF WAY FOR THIS IMPROVEMENT WILL BE PROVIDED BY THE STATE OF OHIO.

INDEX OF SHEETS

1. TITLE SHEET	56. REINFORCEMENT SCHEDULE-PIERS 88 AND 98	NOTE: THIS SET OF PLANS, PART 5 OF THE INNER BELT FREEWAY, INCLUDES THE SURFACING OF THE WEST APPROACH VIADUCT AND THE CENTRAL VIADUCT, PARTS 3 AND 4 RESPECTIVELY, AND EXTENDS FROM WEST PIER 3-A TO STA. 58+00. PARTS 1 THRU 4 ARE UNDER PREVIOUS CONTRACTS.
2. SCHEMATIC PLAN	57. PIER 98	
2-A GEOMETRIC LAYOUT	58-59. ABUTMENT 1B	
3. SEQUENCE OF CONSTRUCTION	60. DELETED	
4. GENERAL NOTES	61-63. DELETED	
4-A LIGHTING NOTES	64. PIER 2E-1	
5-6. GENERAL SUMMARY-QUANTITIES	65. PIERS 3E-1 AND 4E-1	
7-8. TYPICAL CROSS SECTIONS	66. ABUTMENT E-1	
9-13. PAVEMENT DETAILS	67. PIER 2E-2	
10-12. PROFILES	68-69. ABUTMENT E-2 DETAILS	
13-14. MISCELLANEOUS DETAILS	70-73. FRAMING PLANS	
15. LIGHTING PLAN	74-78, 78A, 79. GIRDER ELEVATIONS	
16. REGULATOR VAULT	80. UNIT 2 BEAM ELEVATIONS	
17-19. EXISTING UTILITIES	81-82. GIRDER SPLICES	
20. DRAINAGE PLAN	83. CROSS FRAMES	
21-22. DRAINAGE PROFILES	84. CROSS FRAME DETAILS UNIT 1	
23-23A. SPECIAL MANHOLE FRAME AND COVERS	85. STIFFENER AND JACKING FRAME DETAILS	
24-25. RIGHT OF WAY PLANS	86-86A. ROLLER AND CROSS FRAME DETAILS & MISC. DETAILS	
26-35. CROSS SECTIONS	87-88. BOX GIRDER AT EAST END PIER	
36. GENERAL NOTES-STRUCTURE	89. SHOES	
37. QUANTITIES	90. DEFLECTION DIAGRAM	
38. GENERAL PLAN AND ELEVATION	91-94. EXPANSION JOINT DETAILS	
39. ELEVATION-RAMPS E-1 AND E-2	95-101. SLAB PLANS	
40. GROUND PLAN	102. SLAB CROSS SECTIONS	
41. BORING LOGS	103-104. SUPERSTRUCTURE REINFORCEMENT SCHEDULE	
42-43. EAST END PIER DETAILS	105-107. CONTOUR PLAN	
44-45. PIER 1B DETAILS	108-109. DRAINAGE DETAILS	
46-47. PIER 2B DETAILS	110. HANDRAIL DETAILS	
48-49. PIER 3B & 1E-2 DETAILS	111. LIGHTING DETAILS	
50-51. PIER 4B DETAILS	112. APPROACH SLABS	
52-55. PIER 5B-8B DETAILS	113-117. WEARING SURFACE DETAILS	



LOCATION PLAN

APPROVED DATE 7/9/57 Louis L. Drasler DIRECTOR OF PUBLIC SERVICE, CITY OF CLEVELAND  
 APPROVED DATE 7-2-57 Arthur A. Edwards DIVISION DEPUTY DIRECTOR  
 APPROVED DATE 10-25-57 L. H. Mahoney DEPUTY DIRECTOR OF PLANNING AND PROGRAMMING  
 APPROVED DATE 10-18-57 W. H. Morrison ENGINEER OF BRIDGES  
 APPROVED DATE 10-22-57 R. E. Shultz ENGINEER OF LOCATION AND DESIGN  
 APPROVED DATE 10-22-57 R. E. Masterson DEPUTY DIRECTOR OF DESIGN AND CONSTRUCTION  
 APPROVED DATE \_\_\_\_\_ FIRST ASSISTANT DIRECTOR  
 APPROVED DATE 10/28/57 Wm. J. Sherman ACTING DIRECTOR OF HIGHWAYS  
 APPROVED DATE 11-25-57 H. W. Whitmore CHIEF ENGINEER NEW YORK, CHICAGO AND ST. LOUIS RAILROAD  
 APPROVED DATE 10-30-57 F. H. Simpson CHIEF ENGINEER CLEVELAND UNION TERMINALS COMPANY

LINE DATA

BEGIN PROJECT STA. 12+42.50  
 END PROJECT STA. 58+00.00  
 NET LENGTH OF PROJECT 4,557.50 LIN. FT. OR 0.863 MILES  
 ADDITIONS: STA. 58+00 TO STA. 60+20 & STA. 10+70 TO 12+42.5 392.50 LIN. FT.  
 NEW EAST NINTH ST. STA. 10+75 ± TO STA. 19+61 ± 8860 ± LIN. FT.  
 BROADWAY WEST BOUND STA. 37+89.86 TO STA. 49+60 ± 1,170.14 LIN. FT.  
 NET LENGTH OF WORK = 7,026.14 LIN. FT. OR 1.326 MILES

PREPARED AND RECOMMENDED BY  
 HOWARD, NEEDLES, TAMMEN & BERGENDOFF  
 CONSULTING ENGINEERS  
 KANSAS CITY CLEVELAND NEW YORK

H. G. SOURS  
 ASSOCIATE  
 COLUMBUS

SUPPLEMENTAL SPECIFICATIONS

NUMBER	DATE	NUMBER	DATE
E-101	1-1-57	S 207	4-28-55
5	6-8-55		
S-114	6-7-57		
M 206.14	7-15-49		



PORTION TO BE IMPROVED  
 OTHER HIGHWAYS & STREETS

STANDARD DRAWINGS		STANDARD DRAWINGS	
NUMBER	DATE	NUMBER	DATE
L-3	4-1-50	OS-2	12-17-56
L-3-A	4-1-50	I-15 NO. 2-B	6-1-52
RI-1	1-3-55	BT-50-70-71E NO. 1	10-1-47
B-T-71 R	3-2-53	AS-1-54	12-1-54
LJ NO. 1	7-1-55	G-707	6-1-56
I-1, 2, 3, 4, 8, 5	2-20-45	I-8 MH NO. 1-A	1-3-55
I-8 GB NO. 2-2A B	8-1-56	I-12	7-1-54
I-8 GB 2-3 A 2-4	5-1-52	L-1	4-1-50
I-81 NO. 2	12-1-54	TJ	5-1-56
I-8 MH NO. 1	5-1-52	OS-1	12-17-56
I-8 MH NO. 2	5-1-52	I-2123	8-1-56
I-15 NO. 1	8-1-55	I-14-G	1-22-52
		I-35	1-2-56

DEPARTMENT OF COMMERCE  
 BUREAU OF PUBLIC ROADS  
 APPROVED:  
 DIVISION ENGINEER \_\_\_\_\_ DATE \_\_\_\_\_

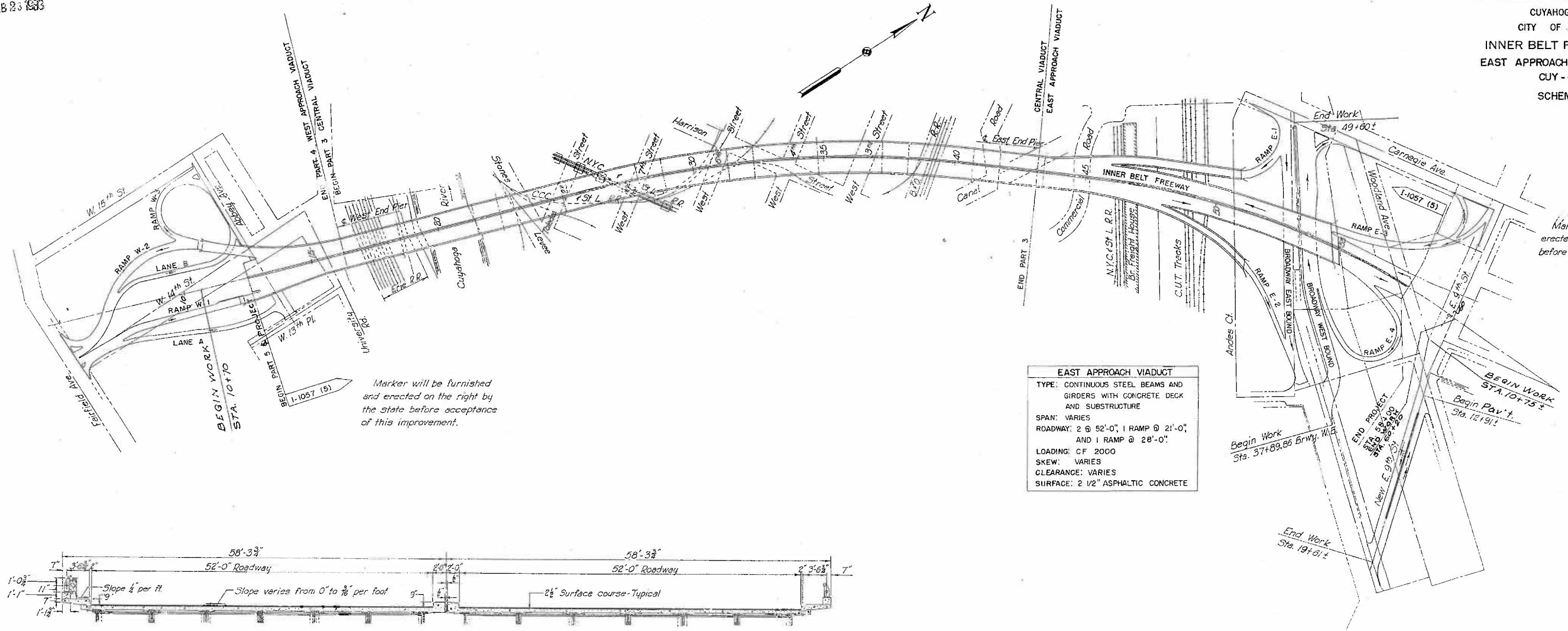
FILE NO. CUYAHOGA COUNTY 00051-K  
 SEC. CUY 42-17.43-18.02  
 DATE OF LETTING \_\_\_\_\_, 195\_\_\_\_  
 CONTRACT NO. \_\_\_\_\_

UNCORRECTED  
FEB 23 1958

FED. ROAD DIV. NO.	STATE	FEDERAL AID PROJECT NO.	TYPE FUNDS
2	OHIO		

2  
117

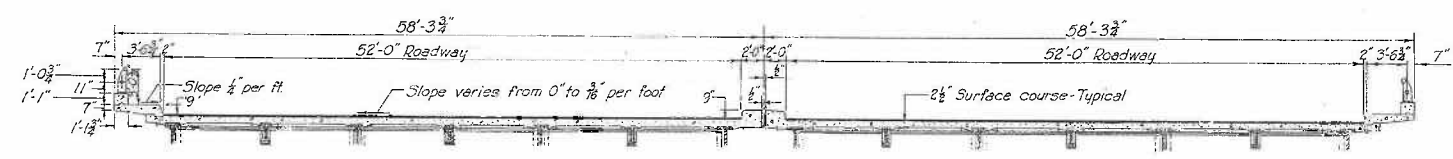
CUYAHOGA COUNTY  
CITY OF CLEVELAND  
INNER BELT FREEWAY—PART 5  
EAST APPROACH TO CENTRAL VIADUCT  
CUY - 42 - (17.43-18.02)  
SCHEMATIC PLAN



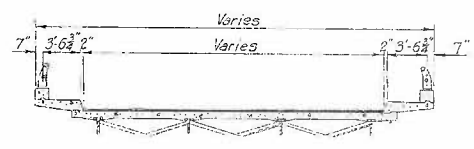
Marker will be furnished and erected on the right by the state before acceptance of this improvement.

Marker will be furnished and erected on the left by the State before acceptance of this improvement.

**EAST APPROACH VIADUCT**  
TYPE: CONTINUOUS STEEL BEAMS AND GIRDERS WITH CONCRETE DECK AND SUBSTRUCTURE  
SPAN: VARIES  
ROADWAY: 2 @ 52'-0", 1 RAMP @ 21'-0" AND 1 RAMP @ 28'-0"  
LOADING: CF 2000  
SKEW: VARIES  
CLEARANCE: VARIES  
SURFACE: 2 1/2" ASPHALTIC CONCRETE



TYPICAL SECTION  
WEST APPROACH VIADUCT  
Scale: 1/8" = 1'-0"

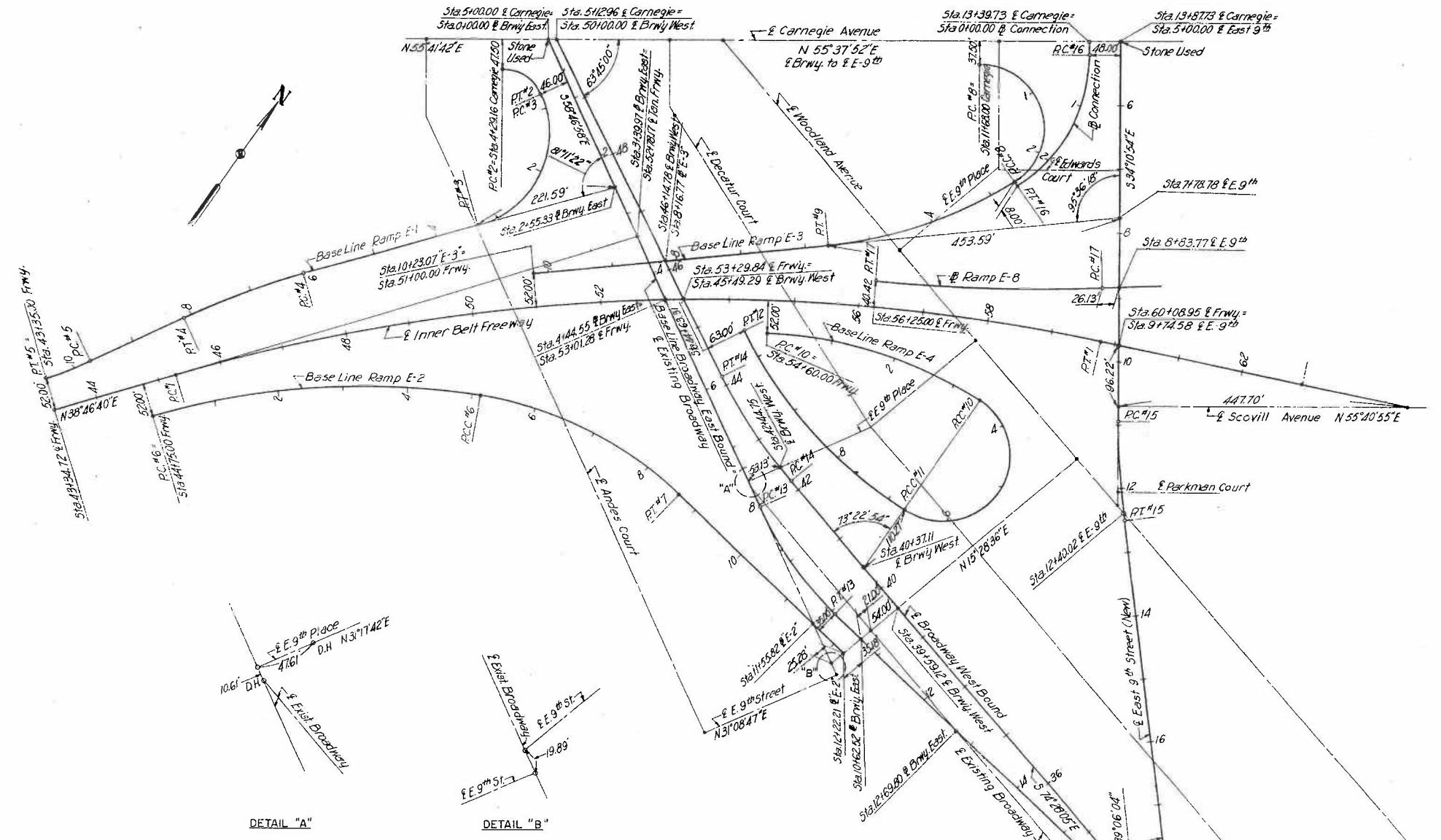


RAMP TYPICAL SECTION  
Scale: 1/8" = 1'-0"

SCALE 1" = 200'  
MADE BY DATE 3-21-57 HOWARD, NEEDLES, TAMMEN & BERGENDOFF  
TRCD. BY DATE 3-25-57 CONSULTING ENGINEERS  
CRD. JFS DATE 4-25-57 KANSAS CITY CLEVELAND NEW YORK  
914 SHEET 2

CUYAHOGA COUNTY  
CITY OF CLEVELAND  
INNER BELT FREEWAY - PART 5  
EAST APPROACH TO CENTRAL VIADUCT  
CUY-42-(17.43-18.02)  
GEOMETRIC LAYOUT

NOT REVISIONED  
FEB 20 1963



CURVE DATA

Line Name	Curve No.	PC Station	PT Station	Curve Data					Curve No.	
				D	Δ	R	T	L		
Freeway	1	45+28.22	59+80.20	2°00'00"	29°02'23"	2864.79'	741.94'	1451.98'	1	
Ramp E-1	2	0+00.00	0+74.41		65°35'10"	65.00'	41.88'	74.41'	2	
Ramp E-1	3	0+74.41	3+33.09		98°48'38"	150.00'	175.04'	258.68'	3	
Ramp E-1	4	6+14.17	8+14.17		5°00'00"	10°00'00"	1145.92'	100.26'	200.00'	4
Ramp E-1	5	9+73.55	10+46.47		12°00'00"	8°45'00"	477.46'	36.53'	72.92'	5
Ramp E-2	6	0+00.00	5+15.63		5°00'00"	25°47'04"	1145.92'	262.29'	515.69'	6
Ramp E-2	7		8+67.24		10°00'00"	35°09'18"	572.96'	181.51'	351.55'	7
Ramp E-3	8	0+00.00	2+56.43			146°35'33"	100.00'	336.79'	256.43'	8
Ramp E-3	9		5+63.72		9°00'00"	27°39'23"	636.62'	156.70'	307.29'	9
Ramp E-4	10	0+00.00	3+50.00		9°00'00"	31°30'00"	636.62'	179.54'	350.00'	10
Ramp E-4	11		6+76.95			180°00'00"	104.07'		326.95'	11
Ramp E-4	12		10+57.79		8°00'00"	30°28'03"	716.20'	195.04'	380.84'	12
Brwy. East	13	8+02.58	10+07.34		10°30'00"	21°30'00"	545.68'	103.60'	204.76'	13
Brwy. West	14	44+16.38	44+14.82		7°00'00"	13°50'57"	818.51'	99.41'	197.84'	14
E 9th Street	15	10+93.13	12+48.69		4°30'00"	7°00'00"	1273.24'	77.87'	155.56'	15
E 9th Connect	16	0+20.90	2+66.62			56°44'19"	248.13'	133.99'	245.72'	16
Ramp E-8	17				2°00'00"	7°00'00"	2864.79'	175.22'	350.00'	17

SCALE 1"=100'  
MADE H.M. DATE 11-10-56 HOWARD, NEEDLES, TAMMEN & BERGENDOFF CONSULTING ENGINEERS  
TRCD. DATE KANSAS CITY CLEVELAND NEW YORK  
CKD. DATE 12-7-56  
914 SHEET 2-A

CUY-90-1538  
15.97

FED ROAD DIV. NO.	STATE	FED AID PROJ. NO.	TYPE FUNDS	24
2	OHIO	1-1-56-10		117

CUYAHOGA COUNTY  
CITY OF CLEVELAND  
**INNER BELT FREEWAY - PART 5**  
APPROACH TO CENTRAL VIADUCT  
CUY-42-(17.43-18.02)  
RIGHT OF WAY



PARCEL NO.	OWNER OF RECORD	AREA SQ. FT.
23 LA	Willis J. Zangerle	15 683.1
24 LA	N.Y.C. & St. L. R.R.	8 250.0
25 LA	Rose Kowitz et al	10 725.0
26 LA	Rose Kowitz et al	5 775.0
27 LA	Howard Kowitz	8 250.0
28 LA	H.W. Black	8 250.0
29 LA	Howard Kowitz	8 250.0
30 LA	H.W. Black	8 250.0
31 LA	Maxco Company	8 250.0
32 LA	Cleveland Trust Co.	8 421.1
33 LA	Cleveland Trust Co.	19 050.0
34 LA	Cleveland Trust Co.	4 180.0
35 LA	Chas. Weinberg & Chas. Sugarman	4 520.8
36 LA	Cleveland Trust Co.	4 941.0
37 LA	S. Mechalovitz Co.	5 287.0
38 LA	S. Mechalovitz Co.	2 773.3
39 LA	Meyer Schlam	2 859.4
40 LA	Meyer Schlam	5 977.5
41 LA	Cleveland Trust Co.	6 323.0
42 LA	Cleveland Trust Co.	7 285.3
43 LA	Outdoor Investment Co.	2 081.8
44 LA	Sam R. Nelson	7 168.3
45 LA	Comella Realty Co.	4 786.6
46 LA	Comella Realty Co.	2 142.5
47 LA	Comella Realty Co.	2 607.5
48 LA	H.S. & F.E. Allen et al	5 287.0
49 LA	Bet Rose, Inc.	3 632.8
50 LA	Chas. I. & M.J. Cianciolo	5 977.5
51 LA	Harry R. & R.N. Wiesenberger	6 323.0
52 LA	Chas. I. & M.J. Cianciolo	6 668.8
53 LA	Kramer Holding Co.	3 464.1
54 LA	Kramer Holding Co.	3 530.5
55 LA	Mamie Corso	7 360.5
56 LA	Frank & Anthony Dindia	5 157.2
57 LA	Bramson Construction Co.	3 170.8
58 LA	Harry Perchan	4 335.2
59 LA	The Ascot Co.	5 500.3
60 LA	Woodland-Ninth Realty Co.	6 665.0
61 LA	Woodland-Ninth Realty Co.	6 581.2
62 LA	Plumwood Corp.	5 668.7
63 LA	E & Ann Venizelos	4 756.2
64 LA	Hill Floral Products Co.	9 267.0
65 LA	Hill Floral Products Co.	2 755.1
66 LA	Ruth R. Hinman et al.	8 153.5
67 LA	Ira W. Crouse	3 170.8
68 LA		4 335.2
69 LA	Abe Dubiek	5 500.3
70 LA	Frank W. Tarbox, A. A. Hilker & L. Botzum	6 665.0
71 LA	Cleveland Trust Co.	3 524.8
72 LA	Goodwill Industries	9 505.4
73 LA	Cleveland Trust Co. Tr.	17 006.2
74 LA	Kerwood Realty Inc.	7 961.3
75 LA	Zelda Katovsky	7 961.3
76 LA	Chas. I. & M.J. Cianciolo	7 961.3
77 LA	Family Inc.	7 961.3
78 LA	Cleveland Trust Co.	6 668.8
79 LA	Kramer Holding Co.	7 014.5
80 LA	Mamie Corso	7 360.5
81 LA	The Broadway Ninth Co.	12 828.0
82 LA	Jean Kerick	4 075.5
83 LA	Michalina Demma	11 377.5
84 LA	Stephen G. Gresh	3 300.0
85 LA	Erie Realty & Investment Co.	3 745.5
86 LA	Wm. J. Bilosky et al	8 250.0
87 LA	Rose Sugarman	8 250.0
88 LA	Rose Sugarman	8 250.0
89 LA	Rose Sugarman	8 250.0
90 LA	Chas. & Rose Sugarman	14 900.0
91 LA	Rose Kowitz	8 250.0
92 LA	Rose Sugarman	8 172.5
93 LA	Ed Klein & N. Weiner	4 125.0
94 LA	Sam Braman	
95 LA	Sam Braman	
96 LA	N.Y.C. & St. L. R.R.	16 000.0

**NOTE 'A'**  
ONLY VEHICULAR ACCESS IS PROHIBITED TO AND FROM BROADWAY AND RAMP E-2 ALONG THE FRONTAGE OF PARCEL 94 LA

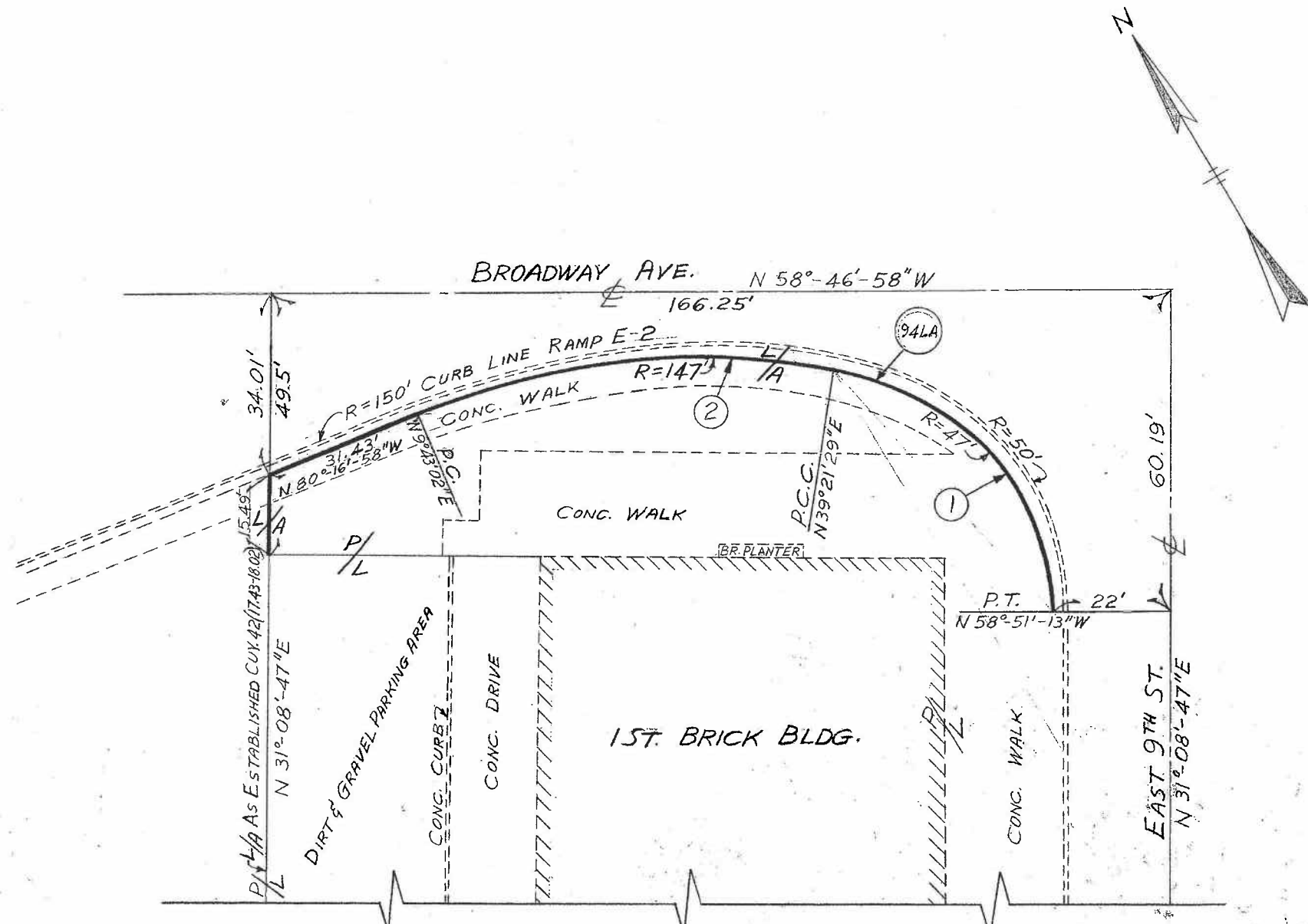
K.I.S.	PARCEL 95 LA COMBINED WITH PARCEL 94 LA & ADDED L.A. LINE ACROSS 94 LA & NOTE 'A'	7-21-70
S.K.	FOR PARCEL 94 LA SEE ATTACHED TRACING	6-6-63
A.H.	Rev.	7-24-62

SCALE 1" = 50'  
MADE J.E.S. DATE 1/10/57 HOWARD, NEEDLES, TAMMEN & BERENDOFF CONSULTING ENGINEERS  
TRCD DATE 3-21-57 KANSAS CITY CLEVELAND NEW YORK  
CKD L.H.M. DATE 3-21-57 914 SHEET 24

FED. ROAD DIVISION	STATE	FED. AID PROJ. NO.	TYPE FUNDS
2	OHIO	I-1657(5)	

2A-A  
117

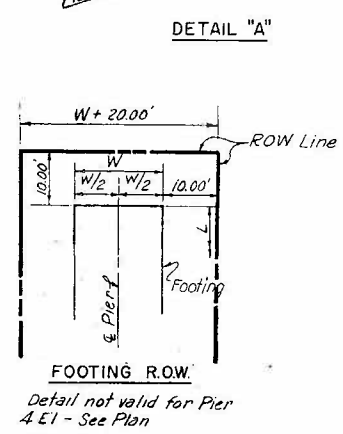
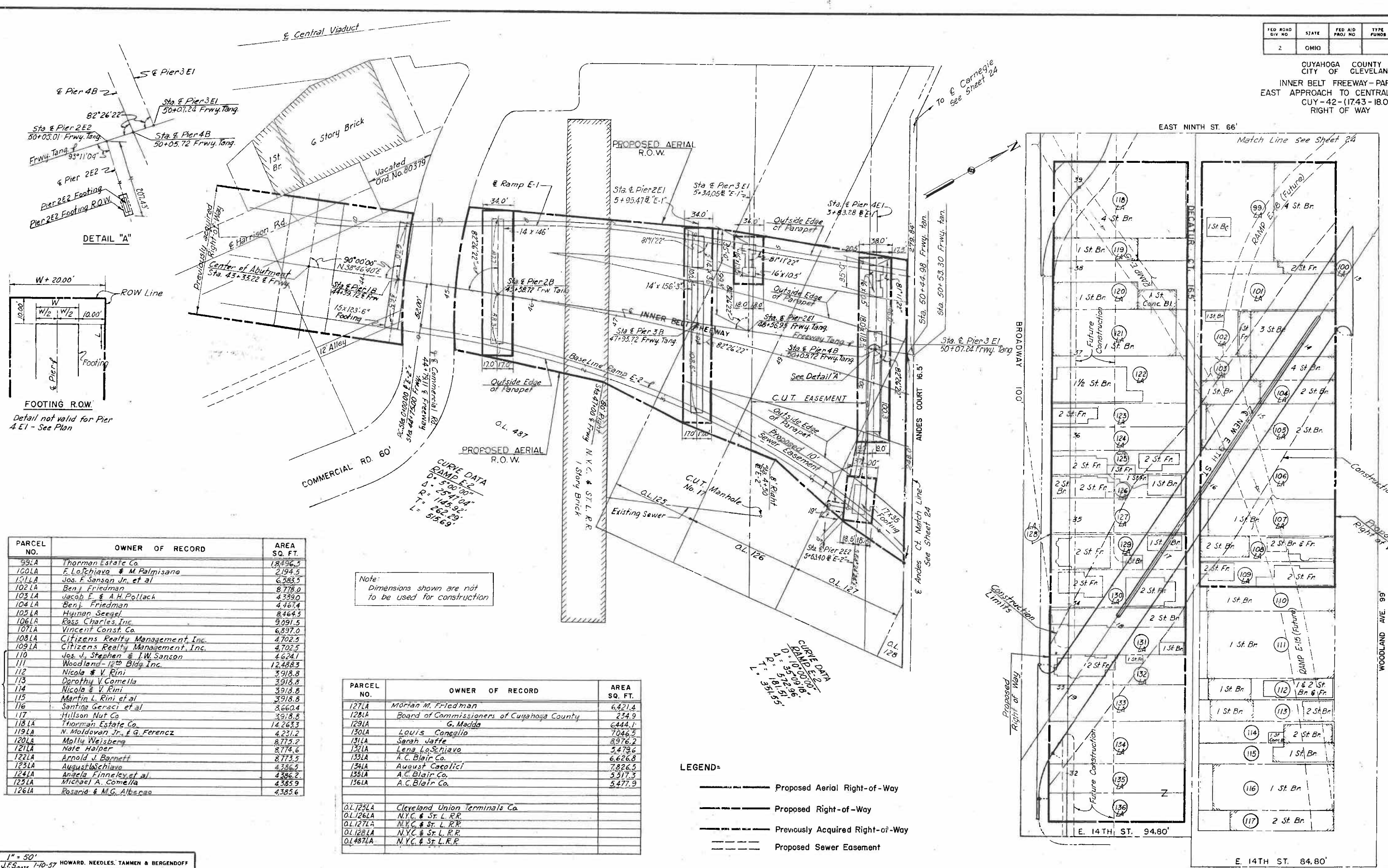
CUYAHOGA COUNTY  
CITY OF CLEVELAND  
INNER BELT FREEWAY - PART 5  
APPROACH TO CENTRAL VIADUCT  
CUY-42-(17.43-18.02)  
RIGHT OF WAY



LIMITED ACCESS CURVE DATA	
①	②
$\Delta = 81^{\circ}-47'-18''$	$\Delta = 29^{\circ}-38'-27''$
$R = 47.00'$	$R = 147.00'$
$T = 40.70'$	$T = 38.89'$
$Ch = 61.58'$	$Ch = 75.21'$
$A = 67.09'$	$A = 76.05'$
Ch. Bearing = N 3°-44'-52" W	Ch. Bearing = N 65°-27'-44" W

PARCEL 94 LA  
STATE OF OHIO  
DEPARTMENT OF HIGHWAYS  
PLAT OF  
LIMITED ACCESS RIGHT OF WAY  
FOR PROPERTY OWNED BY  
SAM BRAMAN  
CITY OF CLEVELAND  
CUYAHOGA COUNTY  
SCALE: 1"=20'  
JUNE 6, 1963

CUYAHOGA COUNTY  
CITY OF CLEVELAND  
INNER BELT FREEWAY - PART - 5  
EAST APPROACH TO CENTRAL VIADUCT  
CUY-42-(1743-18.02)  
RIGHT OF WAY



PARCEL NO.	OWNER OF RECORD	AREA SQ. FT.
951A	Thorman Estate Co.	18,456.5
1001A	F. LoSchiavo & M. Palmisano	2,194.5
1011A	Jos. F. Sanson Jr. et al	6,583.5
1021A	Benj. Friedman	8,778.0
1031A	Jacob E. & A.H. Pollack	4,399.0
1041A	Benj. Friedman	4,467.4
1051A	Hyman Seegal	8,464.5
1061A	Ross Charles Inc.	9,091.5
1071A	Vincent Const. Co.	6,837.0
1081A	Citizens Realty Management Inc.	4,702.5
1091A	Citizens Realty Management, Inc.	4,702.5
110	Jos. V. Stephen & I.W. Sanson	4,624.7
111	Woodland - 120 Bldg. Inc.	12,488.3
112	Nicola & V. Rini	3,918.8
113	Dorothy V. Comella	3,918.8
114	Nicola & V. Rini	3,918.8
115	Martin L. Rini et al	3,918.8
116	Santina Geraci et al	3,660.4
117	Hillson Nut Co.	3,918.8
1181A	Thorman Estate Co.	14,263.3
1191A	N. Moldovan Jr. & G. Ferencz	4,231.2
1201A	Molly Weisberg	8,775.2
1211A	Note Halper	8,774.6
1221A	Arnold J. Barnett	8,773.5
1231A	Augusto Schiavo	4,386.5
1241A	Angela Finneley et al	4,386.2
1251A	Michael A. Comella	4,385.9
1261A	Rosarie & M.G. Albanese	4,385.6

PARCEL NO.	OWNER OF RECORD	AREA SQ. FT.
1271A	Morian M. Friedman	6,421.4
1281A	Board of Commissioners of Cuyahoga County	234.9
1291A	G. Madda	6,444.1
1301A	Louis Conaglio	7,046.5
1311A	Sarah Jaffe	8,976.2
1321A	Lena LoSchiavo	5,479.6
1331A	A.C. Blair Co.	6,626.8
1341A	August Cacolici	7,826.5
1351A	A.C. Blair Co.	5,517.3
1361A	A.C. Blair Co.	5,477.9
OL 1251A	Cleveland Union Terminals Co.	
OL 1261A	N.Y.C. & St. L. R.R.	
OL 1271A	N.Y.C. & St. L. R.R.	
OL 1281A	N.Y.C. & St. L. R.R.	
OL 1291A	N.Y.C. & St. L. R.R.	

LEGEND:-  
 - - - - - Proposed Aerial Right-of-Way  
 - - - - - Proposed Right-of-Way  
 - - - - - Previously Acquired Right-of-Way  
 - - - - - Proposed Sewer Easement

Acquired on Cuy-42-18.29

SCALE 1" = 50'  
 MADE J.F.S. DATE 7-10-57 HOWARD, NEEDLES, TAMMEN & BERGENOFF  
 TRCD DATE 5-9-57 KANSAS CITY CLEVELAND NEW YORK  
 C.K.D. DATE 5-9-57 914 SHEET 25

Revised A.M. 7-26-62  
 Revised J.F.S. 9-19-57  
 Revised H.K.M. 3-19-57

01-0

PROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR OF HIGHWAYS IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 REVISED CODE OF OHIO.

STATE OF OHIO DEPARTMENT OF HIGHWAYS

CUY-90-16.24±

MICROFILMED JUL 6 1985

Table with columns: FED. ROAD DIV. NO., STATE, FEDERAL AID PROJECT NO., TYPE FUNDS. Values: 2, OHIO, 1-71-5(6)247, 175

CUY-42-18.29

PART 6

CUY 42-18.29 CUYAHOGA COUNTY CITY OF CLEVELAND WILLOW INNERBELT FREEWAY PART 6

I-71-5(6)247 LIMITED ACCESS

NOTE: Since the Construction Plans for PART N26, PART N27-A and PART N27-B are now Combined into ONE CONTRACT and PROJECT; the General Notes and Traffic Maintenance Notes of each of the aforesaid PARTS, shall apply in the general execution and completion of this COMBINED PROJECT.

For Part 7-A, see plans for CUY-21-15.32; CUY-42-18.42 For Part 7-B, see plans for CUY-42-18.77

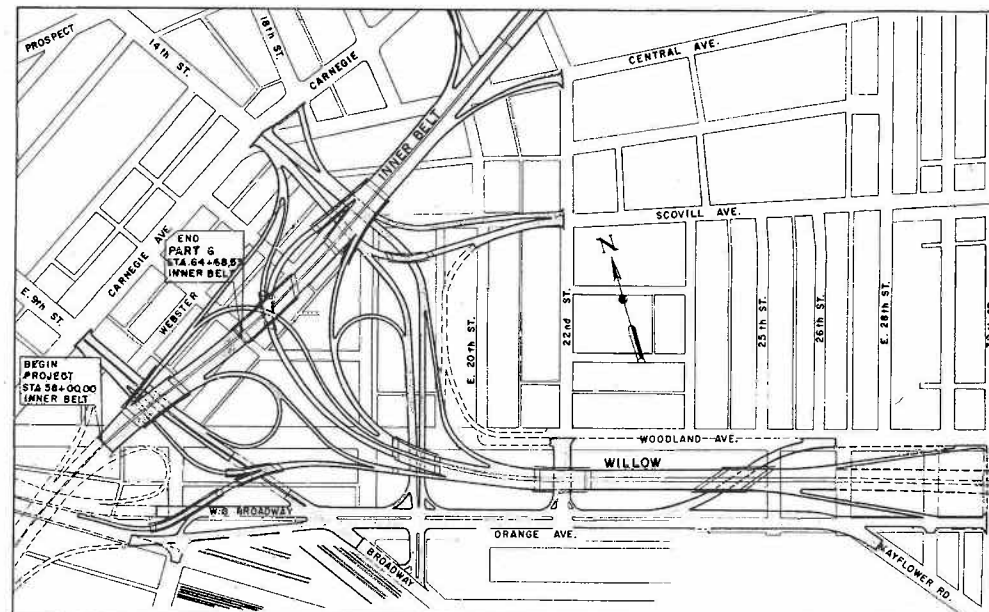
THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO DEPARTMENT OF HIGHWAYS, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT. I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH IN THE PLANS AND ESTIMATES. THE RIGHT OF WAY FOR THIS IMPROVEMENT WILL BE PROVIDED BY THE STATE OF OHIO.

APPROVED DATE 12/12/59 Louis L. Thresher DIRECTOR OF PUBLIC SERVICE, CITY OF CLEVELAND
APPROVED DATE 12/11/59 W. B. Henry DIVISION DEPUTY DIRECTOR
APPROVED DATE 3-11-60 Deputy Director of Planning and Programming
APPROVED DATE 3-4-60 Engineer of Bridges
APPROVED DATE 3-9-60 Engineer of Location and Design
APPROVED DATE 3-9-60 Deputy Director of Design and Construction
APPROVED DATE 3-9-60 First Assistant Director
APPROVED DATE 3-9-60 Director of Highways

INDEX OF SHEETS table with columns: SHEET NO., TITLE SHEET, DRAINAGE PROFILES, SCHEMATIC PLAN, SEQUENCE OF CONSTRUCTION, etc.

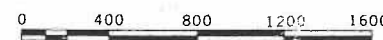
LINE DATA

Table with columns: LENGTH OF PROJECT, LENGTH OF WORK, ADD SCOVILL, ADD E. 14th ST, ADD E. 9th ST, ADD RAMP E-18. Includes stationing and mile calculations.



LOCATION PLAN

Table with columns: CONVENTIONAL SIGNS, PAVEMENT PLANS, DRAINAGE PLANS, LIGHTING PLANS, EXISTING UTILITIES. Includes sheet numbers.



PORTION TO BE IMPROVED OTHER HIGHWAYS & STREETS

STANDARD DRAWINGS table with columns: NUMBER, DATE, NUMBER, DATE. Includes drawing codes like L-1, RI-1, B-1, etc.

Notes: Sheets 114 & 140 revised 9-14-60, Sheet 115 revised 1-10-60, Sheet 116 & 117 revised 4-29-60, Sheet 118 & 119 revised 7-15-60, Sheet 120 revised 7-15-60, Sheets 14, 45-50 revised 8-22-61 REC, Sheet 145 & 146 revised 10-23-61, Sheet 59 revised 10-2-60

PREPARED AND RECOMMENDED BY HOWARD, NEEDLES, TAMMEN & BERGENDOFF CONSULTING ENGINEERS KANSAS CITY CLEVELAND NEW YORK

H.G. SOURS ASSOCIATE COLUMBUS

SUPPLEMENTAL SPECIFICATIONS table with columns: NUMBER, DATE, NUMBER, DATE. Includes entries like S-20, B-19, M-20, CE-101.

FILE NO. CUYAHOGA COUNTY SEC. CUY-42-18.29 DATE OF LETTING CONTRACT NO. 00124-1 19

DEPARTMENT OF COMMERCE BUREAU OF PUBLIC ROADS APPROVED: DIVISION ENGINEER DATE



**E. B. SCOVILL OVER N.B. 14TH ST.**

BRIDGE NUMBER 1

TYPE: Continuous steel beam with reinforced concrete deck and substructure.  
SPANS: 42'-1-5/16", 70'-7-1/16" & 42'-6 1/2" (along E.B. Scovill)  
ROADWAY: 24'-0" f./f. 2'-0" safety curbs  
LOADING: CF 2000 - Adequate for A.A.S.H.O. alternate loading.  
SKEW: Varies  
SURFACE COURSE: 1" Monolithic Concrete  
ALIGNMENT: 20°00' curve left.  
APPROACH SLABS: AS-1-54 (25' Long)  
SUPERELEVATION: Varies

Note: Typical Section of Adjoining Pavement is the same as Proposed Construction at Sta 58+00

**CUY-42-1832**

BRIDGE NUMBER 2

TYPE: Continuous rolled beam with reinforced concrete deck and substructure.  
SPANS: 87'-9", 88'-9" & 55'-0" along E. B.  
ROADWAY: 114'-0" (nominal) f./f. parapets  
LOADING: CF 2000 - Adequate for A.A.S.H.O. alternate loading.  
SURFACE COURSE: 1" Monolithic Concrete  
ALIGNMENT: 2°00' R. Tangent.  
APPROACH SLABS: AS-1-54 (25' Long)  
SUPERELEVATION: Varies

**E-15 OVER W.B. BROADWAY**

BRIDGE NUMBER 6

TYPE: Continuous welded steel girder with reinforced concrete deck and substructure.  
SPANS: 71.65', 120.00', and 72.00' (along E-15)  
ROADWAY: 20'-0" f./f. 2'-0" safety curbs.  
LOADING: CF 2000 - Adequate for A.A.S.H.O. alternate loading.  
SKEW: Varies  
SURFACE COURSE: 1" Monolithic Concrete  
APPROACH SLABS: AS-1-54 (25'-0" long)  
SUPERELEVATION: Varies  
ALIGNMENT: 17°-00' Curve Lt. & Tangent

**E-15 & E-18 OVER 9TH ST.**

BRIDGE NUMBER 7

TYPE: Continuous steel beam with reinforced concrete deck and substructure.  
SPANS: 66'-0", 93'-5-7/8", & 58'-2-1/8" along E Ramp E-18.  
51'-0", 81'-0", & 54'-0" along E Ramp E-15.  
ROADWAYS: Varies  
LOADING: CF 2000 - Adequate for A.A.S.H.O. alternate loading.  
SKEW: Varies  
SURFACE COURSE: 1" Monolithic Concrete  
ALIGNMENT: 20°00' Rt. & E-18 & 7°00' Rt. & E-15  
APPROACH SLABS: AS-1-54 (25' Long)  
SUPERELEVATION: Varies

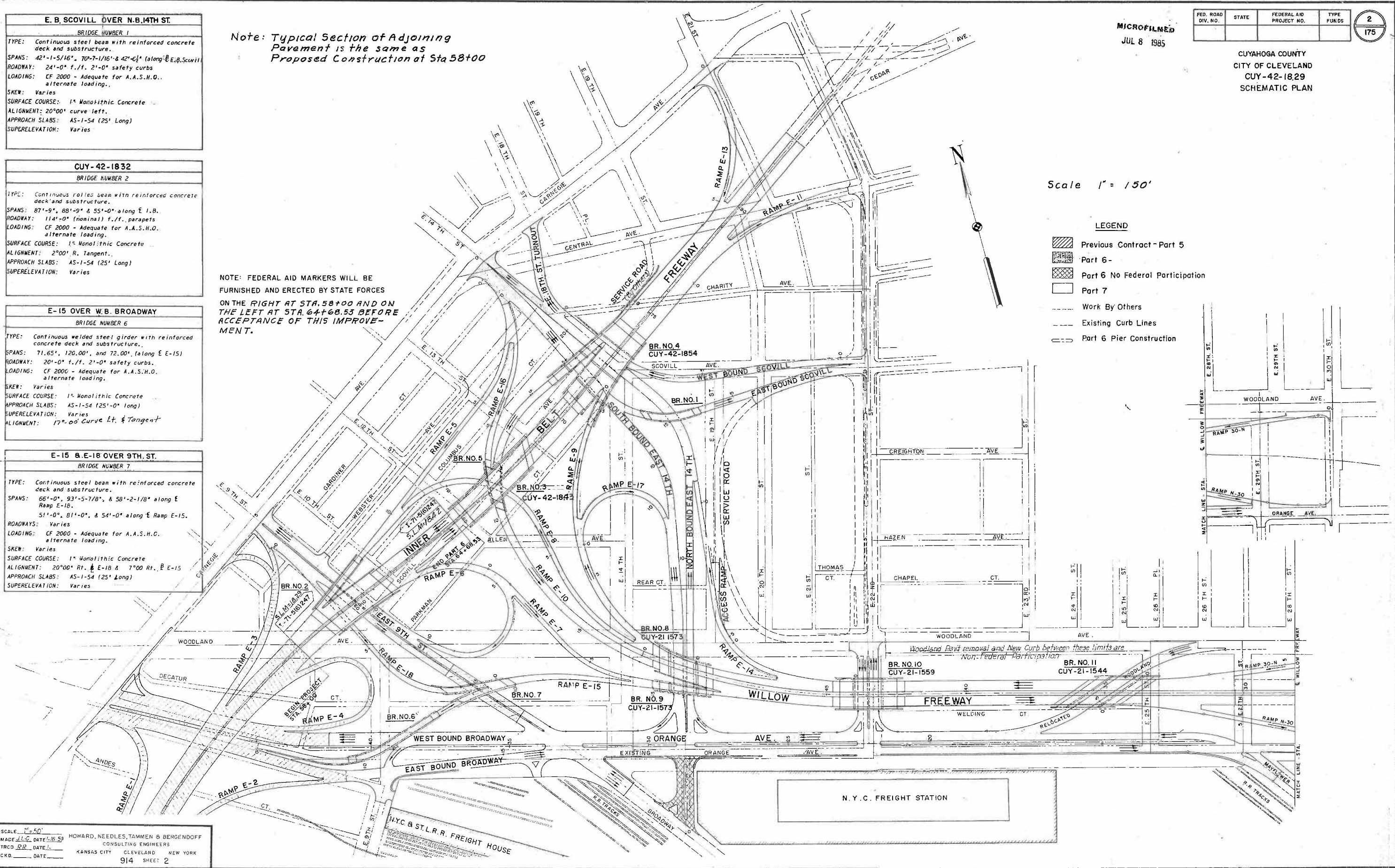
MICROFILMED  
JUL 8 1985

FED. ROAD DIV. NO.	STATE	FEDERAL AID PROJECT NO.	TYPE FUNDS	2 175
--------------------	-------	-------------------------	------------	----------

CUYAHOGA COUNTY  
CITY OF CLEVELAND  
CUY-42-1829  
SCHEMATIC PLAN

Scale 1" = 150'

- LEGEND**
- Previous Contract - Part 5
  - Part 6 -
  - Part 6 No Federal Participation
  - Part 7
  - Work By Others
  - Existing Curb Lines
  - Part 6 Pier Construction

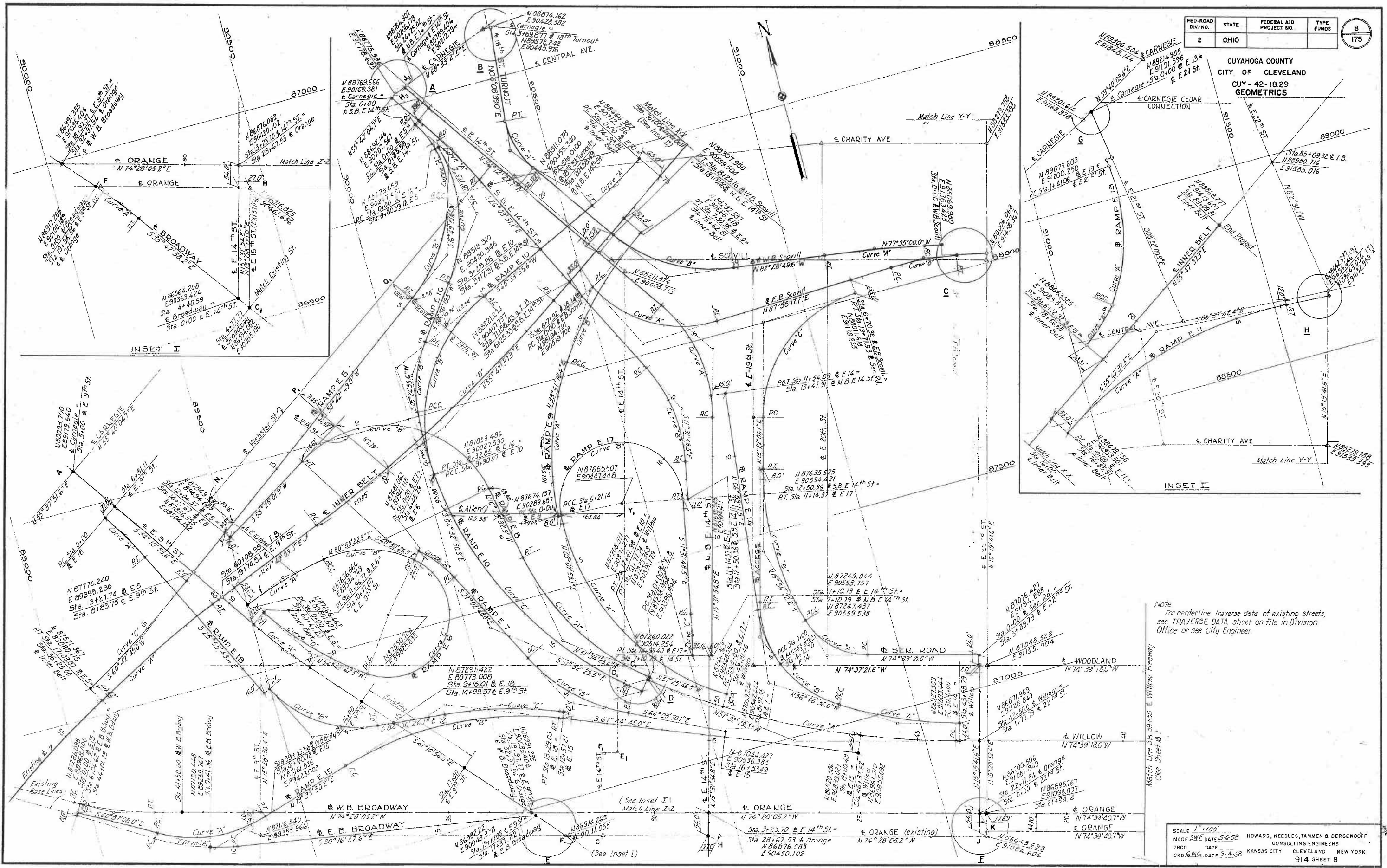


SCALE: 1" = 50'  
 MADE BY: HOWARD, NEEDLES, TAMMEN & BERGENDOFF  
 TRCD. DATE: CONSULTING ENGINEERS  
 CHK. DATE: KANSAS CITY CLEVELAND NEW YORK  
 914 SHEET 2

FED. ROAD DIV. NO.	STATE	FEDERAL AID PROJECT NO.	TYPE FUNDS
2	OHIO		

8  
175

CUYAHOGA COUNTY  
CITY OF CLEVELAND  
CUY - 42-18.29  
GEOMETRICS



INSET I

INSET II

Note:  
For centerline traverse data of existing streets,  
see TRAVERSE DATA sheet on file in Division  
Office or see City Engineer.

SCALE 1" = 100'  
MADE S.W.E. DATE 5-6-58 HOWARD, NEEDLES, TAMMEN & BERGENDORFF  
TRCD. DATE CONSULTING ENGINEERS  
CKD. DATE 7-4-58 KANSAS CITY CLEVELAND NEW YORK  
914 SHEET 8



EE-7

EE-9

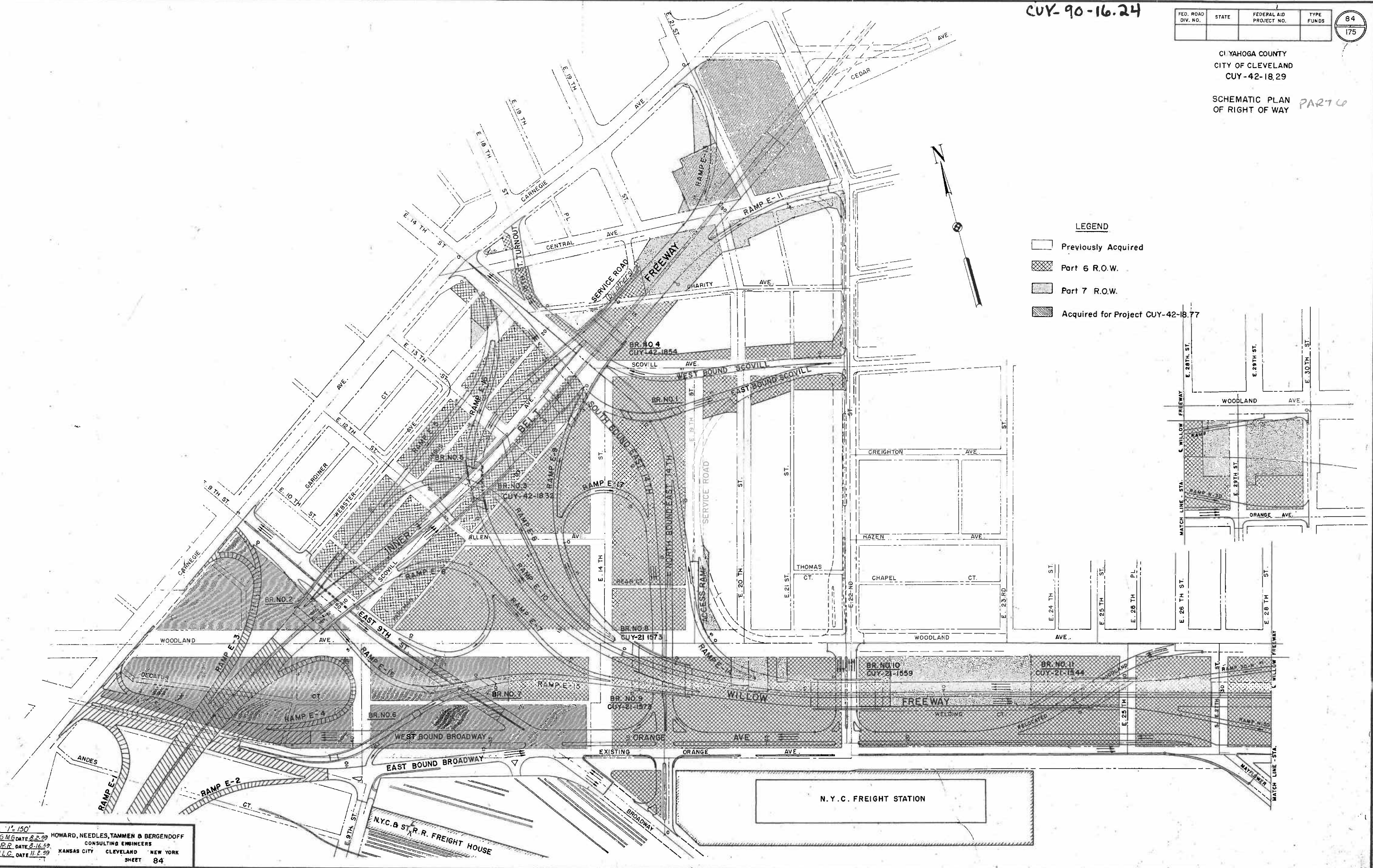
CUY-90-16.24

FED. ROAD DIV. NO.	STATE	FEDERAL AID PROJECT NO.	TYPE FUNDS

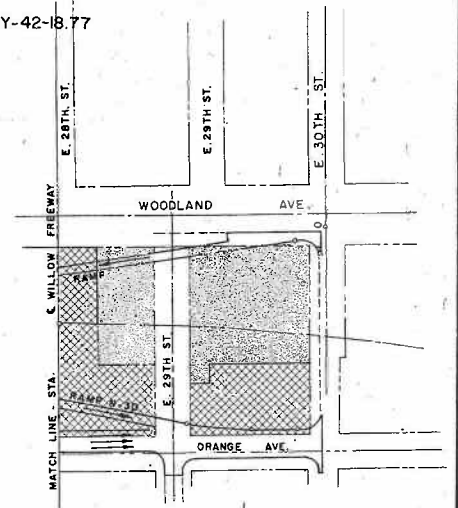
84  
175

CUYAHOGA COUNTY  
CITY OF CLEVELAND  
CUY-42-18.29

SCHEMATIC PLAN OF RIGHT OF WAY PART 6



- LEGEND**
- Previously Acquired
  - Part 6 R.O.W.
  - Part 7 R.O.W.
  - Acquired for Project CUY-42-18.77



SCALE 1" = 150'  
 MADE & DATE 8.2.59  
 TRCD. P.R. DATE 8.16.59  
 CKD. J.L.C. DATE 11.8.59

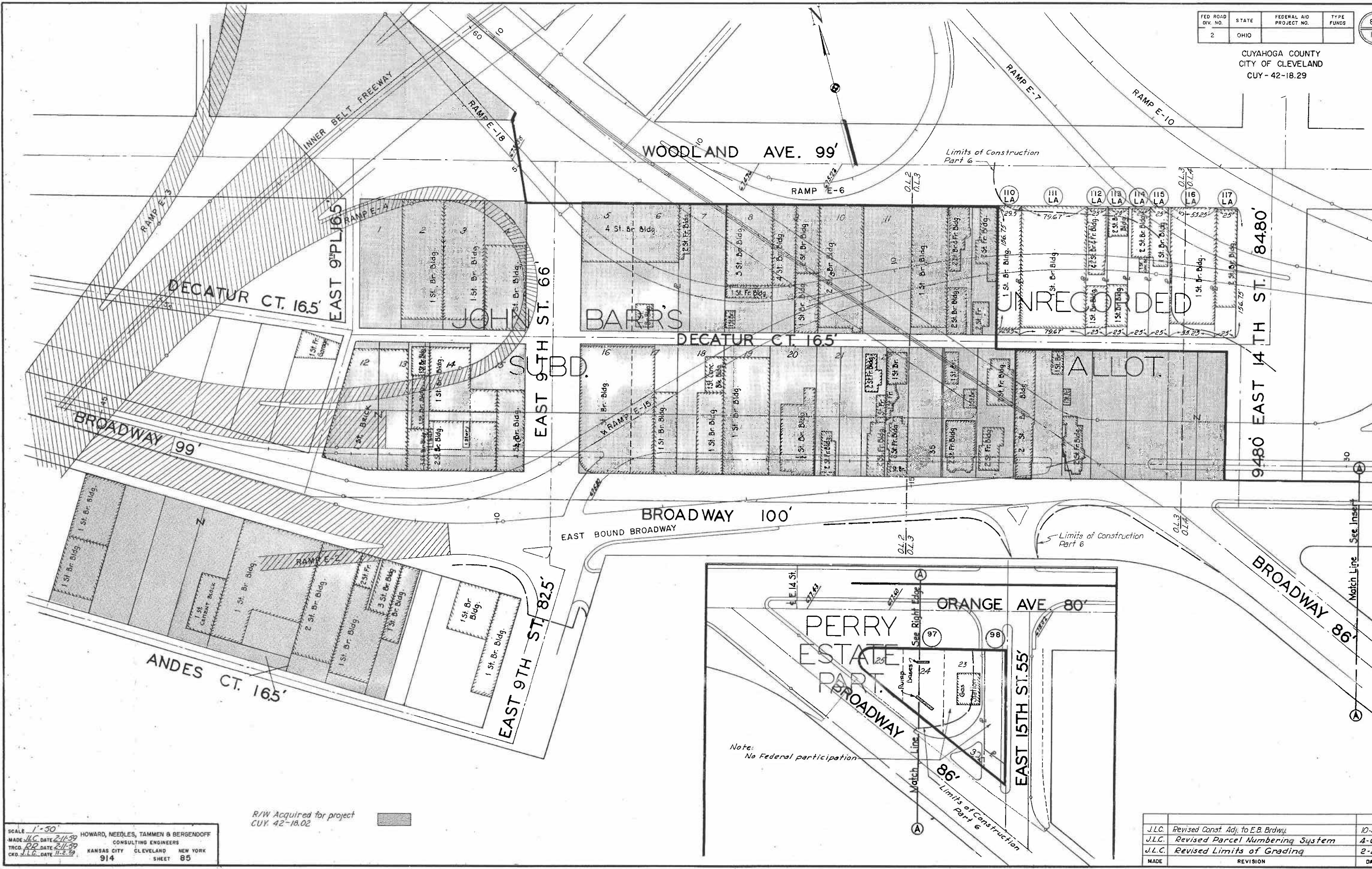
HOWARD, NEEDLES, TAMMEN & BERGENDOFF  
 CONSULTING ENGINEERS  
 KANSAS CITY CLEVELAND NEW YORK

SHEET 84



FED. ROAD DIV. NO.	STATE	FEDERAL AID PROJECT NO.	TYPE FUNDS	85
2	OHIO			175

CUYAHOGA COUNTY  
CITY OF CLEVELAND  
CUY-42-18.29



UNRECORDED

ALLOT.

PERRY ESTATE PART.

Note:  
No Federal participation

SCALE 1"=50'  
MADE J.L.C. DATE 2-11-59  
TRCD J.L.C. DATE 2-11-59  
CKD J.L.C. DATE 11-2-59

HOWARD, NEEDLES, TAMMEN & BERGENDOFF  
CONSULTING ENGINEERS  
KANSAS CITY CLEVELAND NEW YORK  
914 SHEET 85

R/W Acquired for project  
CUY. 42-18.02

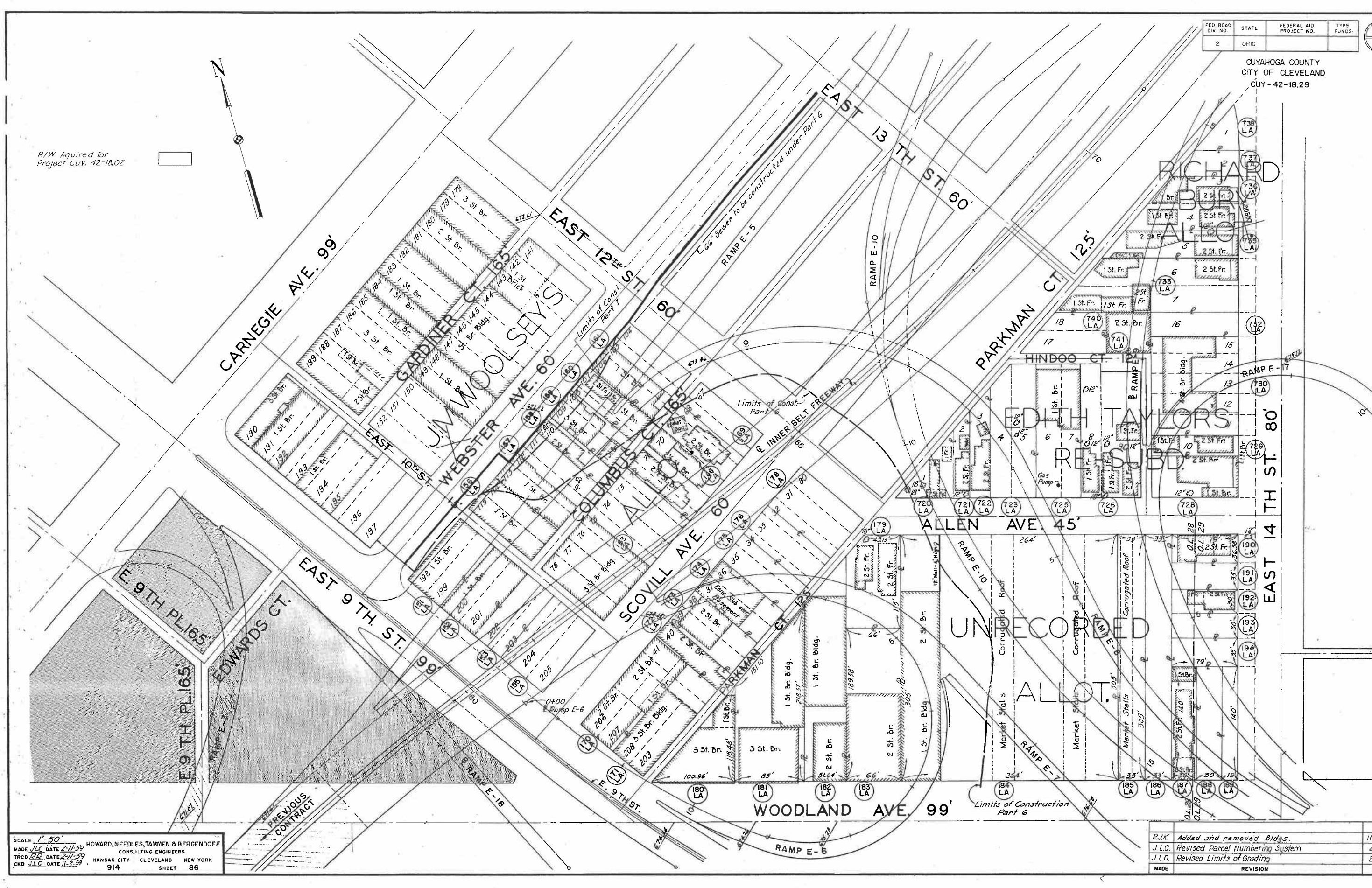
MADE	REVISION	DATE
J.L.C.	Revised Const. Adj. to E.B. Brdwy	10-30-59
J.L.C.	Revised Parcel Numbering System	4-6-59
J.L.C.	Revised Limits of Grading	2-26-59

FED. ROAD DIV. NO.	STATE	FEDERAL AID PROJECT NO.	TYPE FUNDS.
2	OHIO		

86  
175

CUYAHOGA COUNTY  
CITY OF CLEVELAND  
CUY-42-18.29

R/W Acquired for  
Project CUY-42-18.02

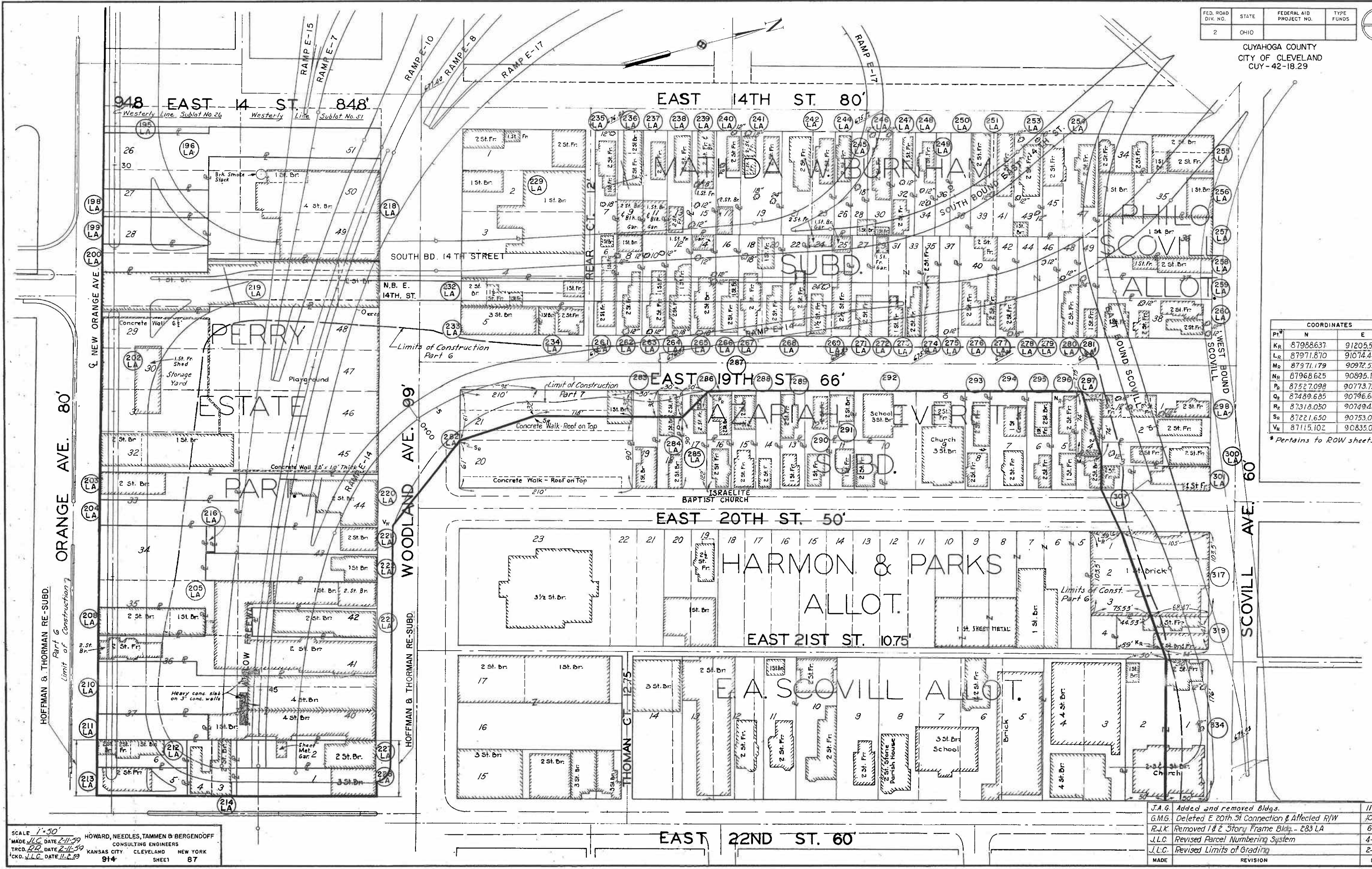


SCALE 1"=50'  
MADE J.L.C. DATE 2-11-59  
TRCO B.R. DATE 2-11-59  
CKD J.L.C. DATE 11-2-59

HOWARD, NEEDLES, TAMMEN & BERGENDOFF  
CONSULTING ENGINEERS  
KANSAS CITY CLEVELAND NEW YORK

914 SHEET 86

R.J.K.	Added and removed Bldgs.	11-13-59
J.L.C.	Revised Parcel Numbering System	4-6-59
J.L.C.	Revised Limits of Grading	2-26-59
MADE	REVISION	DATE



COORDINATES		
Pt.	N	E
K <sub>R</sub>	87988.637	91205.500
L <sub>R</sub>	87971.870	91074.402
M <sub>R</sub>	87971.179	90972.557
N <sub>R</sub>	87968.625	90895.111
P <sub>R</sub>	87527.098	90773.756
Q <sub>R</sub>	87489.685	90796.660
R <sub>R</sub>	87318.050	90749.486
S <sub>R</sub>	87221.650	90753.066
V <sub>R</sub>	87115.102	90835.026

\* Pertains to ROW sheets only

SCALE 1"=30'  
MADE J.L.C. DATE 2-11-59  
TRCD. R.R. DATE 2-11-59  
CKD. J.L.C. DATE 11-2-59

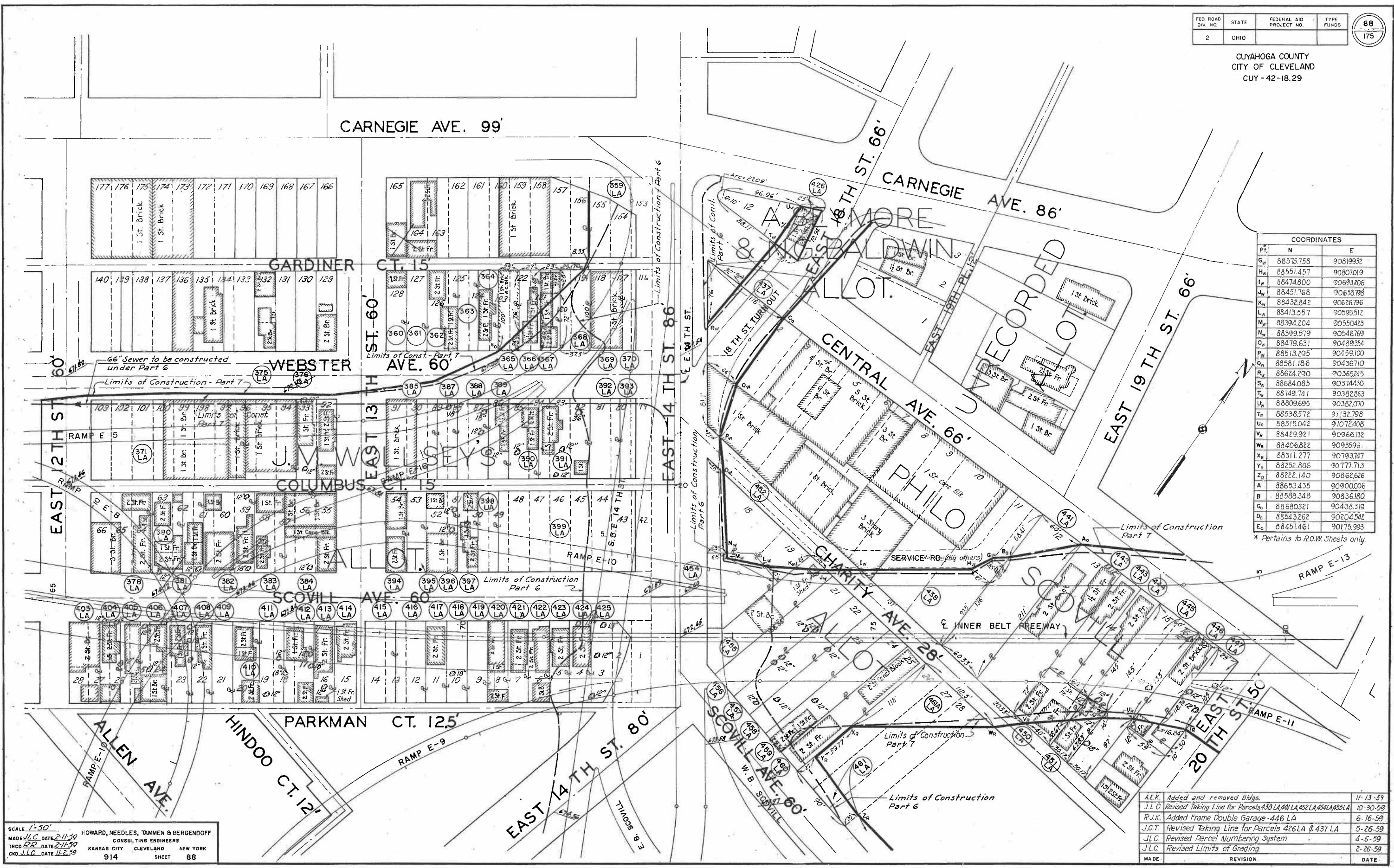
HOWARD, NEEDLES, TAMMEN & BERGENDOFF  
CONSULTING ENGINEERS  
KANSAS CITY CLEVELAND NEW YORK

914 SHEET 87

MADE	REVISION	DATE
J.A.G.	Added and removed Bldgs.	11-13-59
G.M.G.	Deleted E 20th St Connection & Affected R/W	10-26-59
R.J.K.	Removed 1 & 2 Story Frame Bldg. - 283 LA	6-16-59
J.L.C.	Revised Parcel Numbering System	4-6-59
J.L.C.	Revised Limits of Grading	2-26-59



CUYAHOGA COUNTY  
CITY OF CLEVELAND  
CUY-42-18.29



COORDINATES		
Pt.	N	E
G <sub>w</sub>	88575.758	90819932
H <sub>w</sub>	88551.457	90807019
I <sub>w</sub>	88474.800	90693206
J <sub>w</sub>	88451.768	90658798
K <sub>w</sub>	88432.842	90626796
L <sub>w</sub>	88413.557	90593512
M <sub>w</sub>	88394.204	90550423
N <sub>w</sub>	88399.579	90546769
O <sub>w</sub>	88479.631	90489354
P <sub>w</sub>	88513.295	90459100
Q <sub>w</sub>	88581.186	90436710
R <sub>w</sub>	88624.290	90365245
S <sub>w</sub>	88684.085	90374430
T <sub>w</sub>	88749.741	90382863
U <sub>w</sub>	88809.695	90382070
V <sub>w</sub>	88829.921	90966132
W <sub>w</sub>	88406.822	90935961
X <sub>w</sub>	88311.277	90793747
Y <sub>w</sub>	88252.806	90777713
Z <sub>w</sub>	88222.140	90862626
A	88653.435	90900006
B	88588.348	90836180
C <sub>w</sub>	88680.321	90438319
D <sub>w</sub>	88543.262	90204542
E <sub>w</sub>	88451.461	90175993

\* Pertains to R.O.W. Sheets only.

SCALE 1"=50'  
MADE I.L.C. DATE 2-11-59  
TRCD. R.P. DATE 2-11-59  
CKD J.L.C. DATE 11-2-59

HOWARD, NEEDLES, TAMMEN & BERGENOFF  
CONSULTING ENGINEERS  
KANSAS CITY CLEVELAND NEW YORK

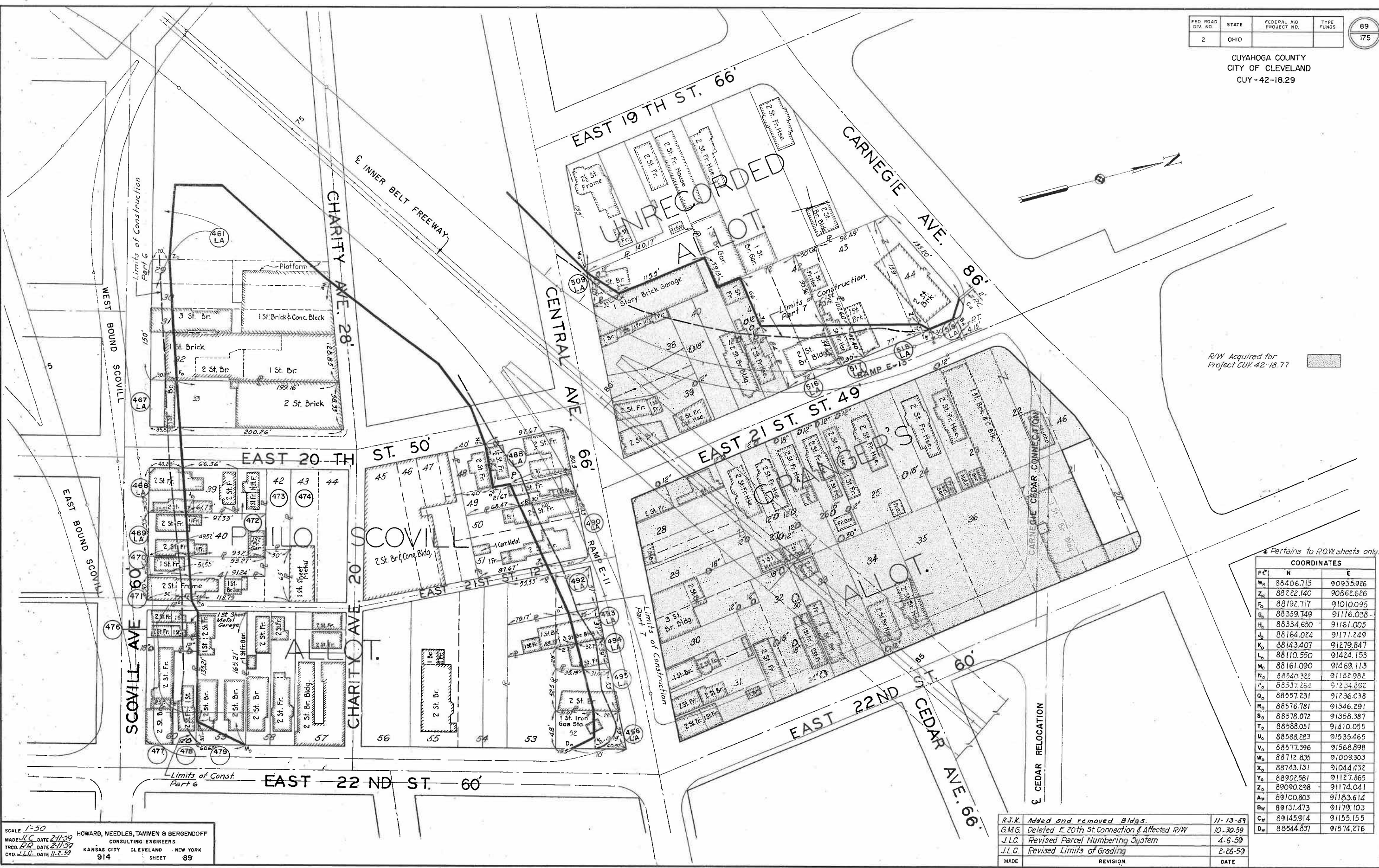
914 SHEET 88

MADE	REVISION	DATE
A.E.K.	Added and removed Blkgs.	11-13-59
J.L.C.	Revised Taking Line for Parcels 438 LA, 441 LA, 452 LA, 454 LA, 455 LA	10-30-59
R.J.K.	Added Frame Double Garage-446 LA	6-16-59
J.C.T.	Revised Taking Line for Parcels 426 LA & 437 LA	5-26-59
J.L.C.	Revised Parcel Numbering System	4-6-59
J.L.C.	Revised Limits of Grading	2-26-59

FED. ROAD DIV. NO.	STATE	FEDERAL AID PROJECT NO.	TYPE FUNDS
2	OHIO		

89  
175

CUYAHOGA COUNTY  
CITY OF CLEVELAND  
CUY-42-18.29



R/W Acquired for Project CUY-42-18.77

\* Pertains to R/W sheets only.

COORDINATES		
Point	N	E
W <sub>0</sub>	88406.715	90935.926
Z <sub>0</sub>	88222.140	90862.626
F <sub>0</sub>	88192.717	91010.095
G <sub>0</sub>	88359.749	91116.038
H <sub>0</sub>	88334.650	91161.005
J <sub>0</sub>	88164.024	91171.249
K <sub>0</sub>	88143.407	91279.847
L <sub>0</sub>	88110.550	91424.153
M <sub>0</sub>	88161.090	91469.113
N <sub>0</sub>	88540.322	91182.982
P <sub>0</sub>	88537.264	91234.892
Q <sub>0</sub>	88557.231	91236.038
R <sub>0</sub>	88576.781	91346.291
S <sub>0</sub>	88578.072	91358.387
T <sub>0</sub>	88588.051	91410.055
U <sub>0</sub>	88588.283	91535.465
V <sub>0</sub>	88577.396	91568.898
W <sub>0</sub>	88712.835	91009.303
X <sub>0</sub>	88743.131	91044.432
Y <sub>0</sub>	88902.581	91127.865
Z <sub>0</sub>	89090.298	91174.041
A <sub>0</sub>	89100.803	91183.614
B <sub>0</sub>	89131.473	91179.103
C <sub>0</sub>	89145.914	91155.155
D <sub>0</sub>	88544.637	91574.276

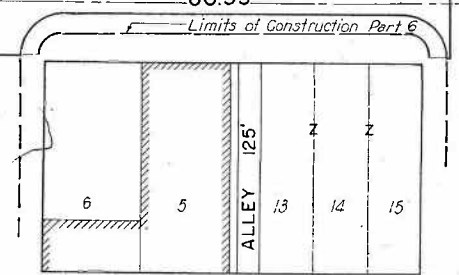
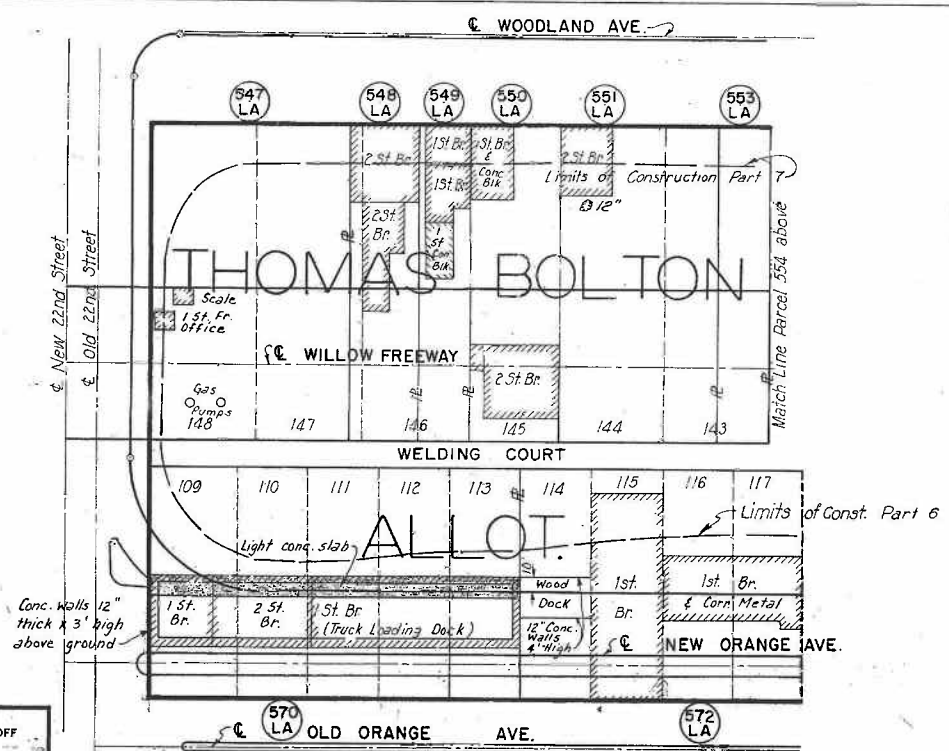
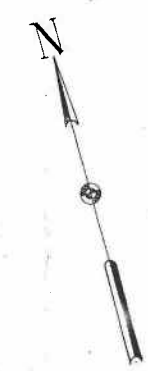
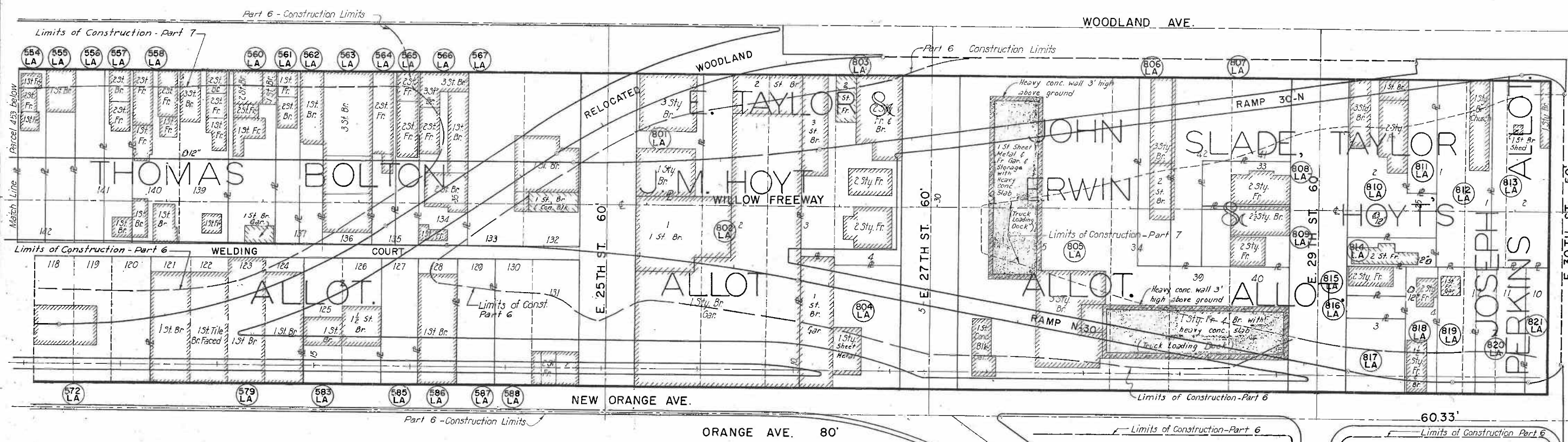
MADE	REVISION	DATE
R.J.K.	Added and removed Bldgs.	11-13-59
G.M.G.	Deleted E. 20th St. Connection & Affected R/W	10-30-59
J.L.C.	Revised Parcel Numbering System	4-6-59
J.L.C.	Revised Limits of Grading	2-26-59

SCALE 1"=50'  
HOWARD, NEEDLES, TAMMEN & BERGENDOFF  
CONSULTING ENGINEERS  
KANSAS CITY CLEVELAND NEW YORK  
914 SHEET 89

FED. ROAD DIV. NO.	STATE	FEDERAL AID PROJECT NO.	TYPE FUNDS
2	OHIO		

90  
175

CUYAHOGA COUNTY  
CITY OF CLEVELAND  
CUY-42-18.29



SCALE 1"=50'  
MADE I.L.C. DATE 2-11-59  
TRCD. R.R. DATE 2-11-59  
CKD. I.L.C. DATE 11-2-59

HOWARD, NEEDLES, TAMMEN & BERGENDOFF  
CONSULTING ENGINEERS  
KANSAS CITY CLEVELAND NEW YORK  
914 SHEET 90

A.E.K.	Added and removed Bldgs.	11-13-59
J.L.C.	Revised Parcel Numbering System	4-6-59
J.L.C.	Revised Limits of Grading	2-26-59
MADE	REVISION	DATE

