

CUY-90-14.90

PID 77332/85531

APPENDIX EX-03

Original ROW Plans (Reference Document)

State of Ohio
Department of Transportation
Jolene M. Molitoris, Director

Innerbelt Bridge
Construction Contract Group 1 (CCG1)

Revision Date: May 9, 2009

6-0

STATE OF OHIO DEPARTMENT OF HIGHWAYS U.S. ROUTE 42 RELOCATION CENTRAL VIADUCT-INNER BELT FREEWAY PART 1-PRELIMINARY TEST PILES **CUYAHOGA COUNTY**

CITY OF CLEVELAND

CITY OF CLEVELAND

2 OHIO U1-1057(I)

CENTRAL VIADUCT PART 1

FED. RD. STATE

CONVENTIONAL SIGNS

CORPORATION LINE CENTER LINE RAILROAD

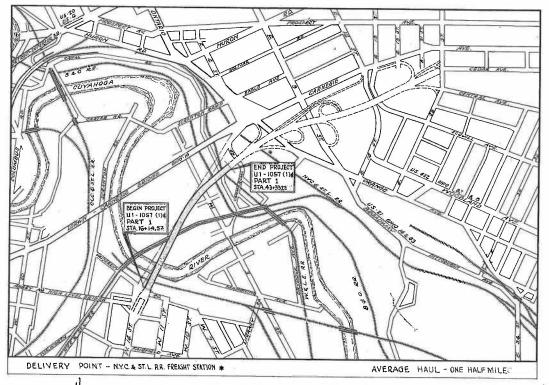
INDEX OF SHEETS

TITLE SHEET SHEET Nº 1 LOCATION PLANS 2-3 CROSS SECTIONS NOTES & QUANTITIES TO A SAME

LINE DATA

Begin Project UI-1057(1) Sta. 16+14.52 End Project U-1-1057(1) Sta. 43+33.22 Net Length of Project U-1-1057(1) = 2718.70 or 0.514 mi.

Begin Work Sta 16+00 End Work Sta 43+75 Net Length of Work = 2775' or 0.525 mi.



LOCATION

PLAN

SCALE :

PORTION TO BE IMPROVED STATE HIGHWAYS OTHER HIGHWAYS

STANDARD DRAWINGS (None)

6 THE PHOTOLAB

CONSTRUCTION

BU, L.J MAR 2 7 1958

SUPPLEMENTAL SPECIFICATIONS T-171.19 Rev. 3-19-53 M-101.7 1-24-53

The standard specifications of The State of Ohio, Department of Highways, including changes and supplemental specifications listed in the proposal shall govern this improvement.

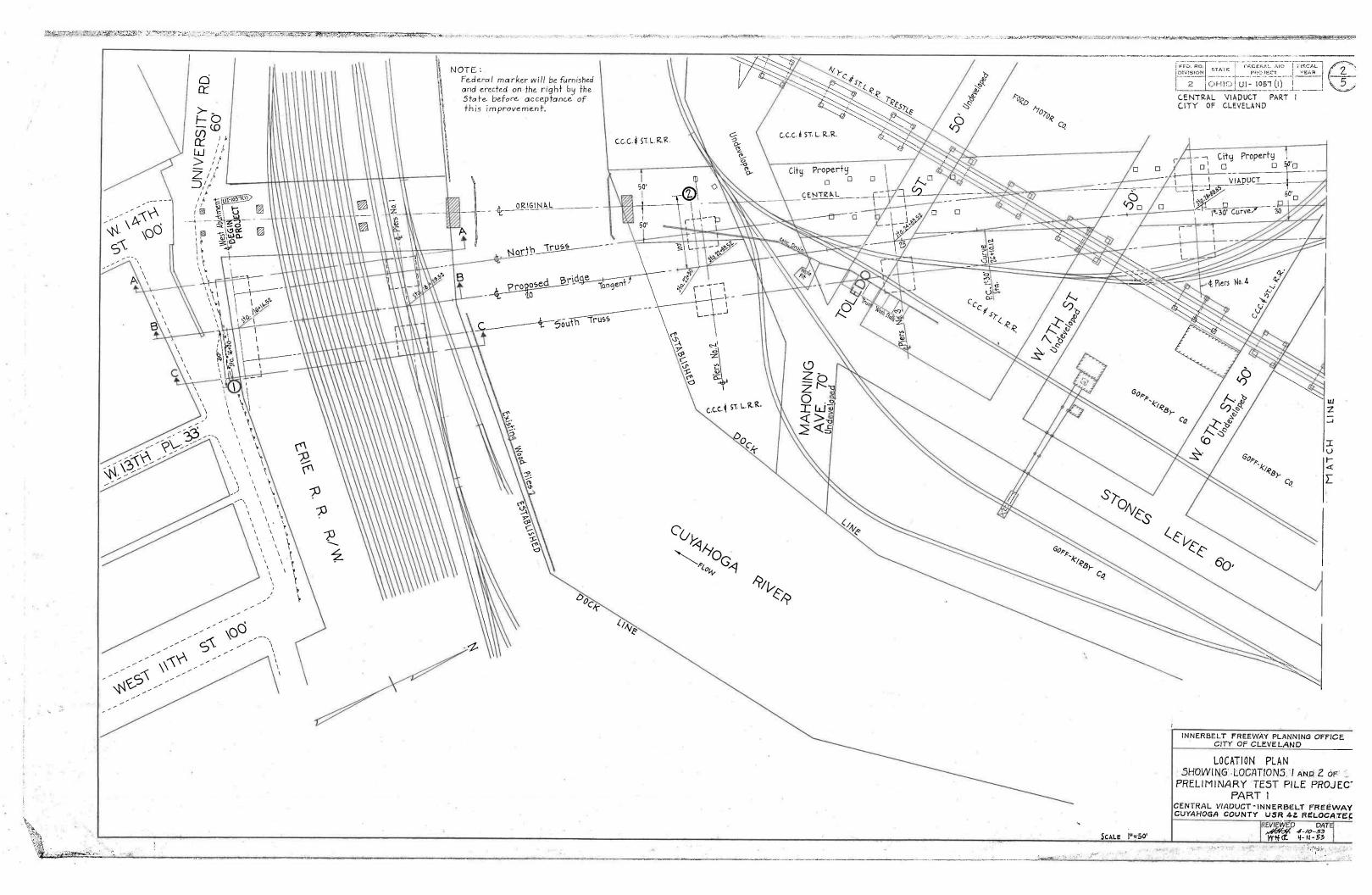
I hereby approve these plans and declare that the making of this improvement will not require the closing to traffic of the high way, and that provisions for the maintenance and safety of traffic will be as set forth on the plans and estimates.

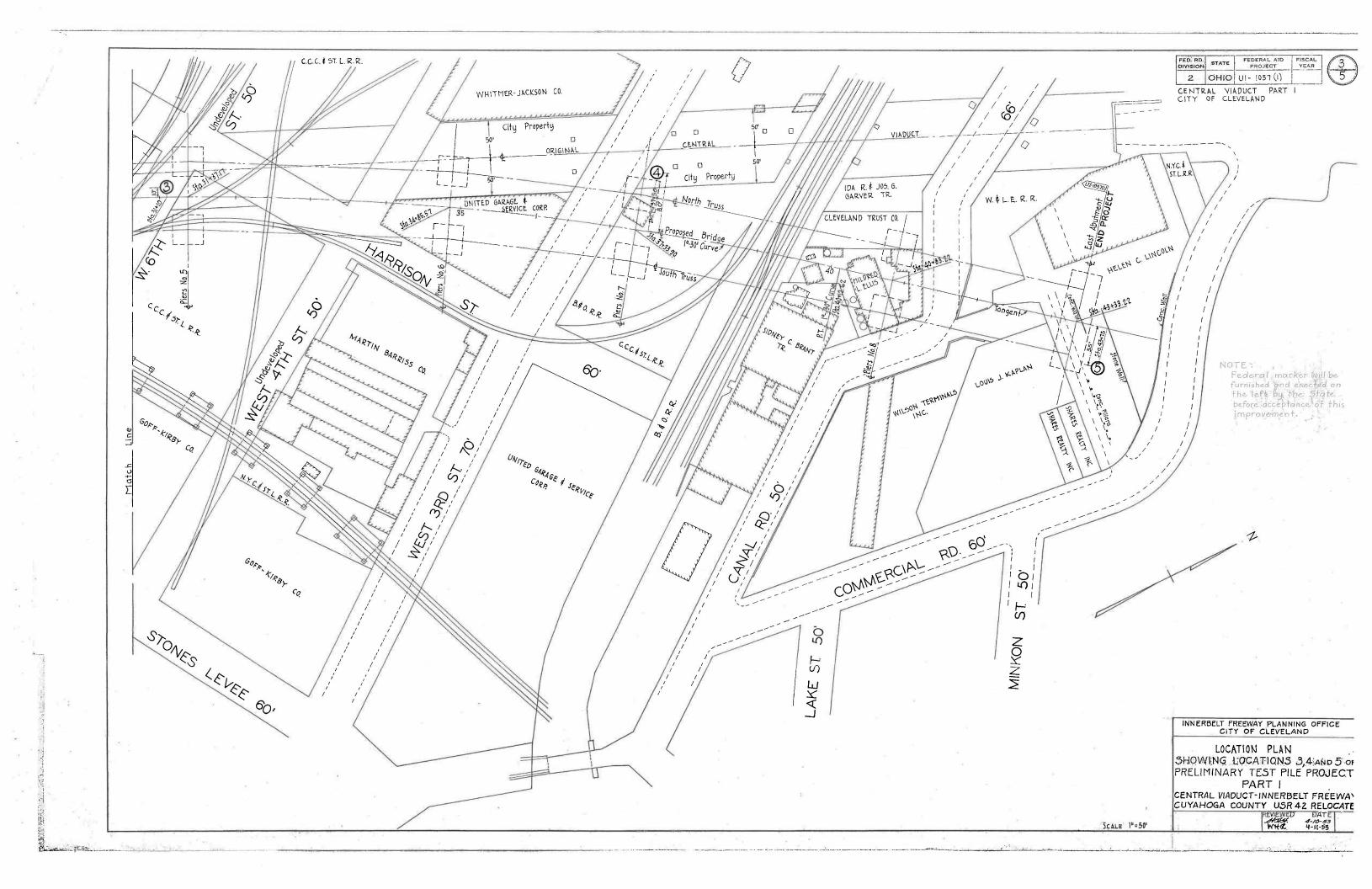
The right of way for this improvement will be provided by the State of Ohio.

Approved <u>Lewis L. Drasler</u> Date <u>4/6/53</u> Division Deputy Director
Approved Chief Engineer, Bureau of Maintenance
Approved
Approved Later Date -13-53 Chief Engineer, Bureau of Bridges & R.R. Crossings
Date 4-16-53 Chief Engineer, Bureau of Location & Design
Approved V. 4 χ Chaeldin Date 2-16-53 First Ass't Director & Chief Engineer
Approved
Approved for The City of Cleveland family 1 David Date 4-6-53 Director of Public Service

	ARTMENT OF COMM		Tal.
RECOMMENDED F	FOR APPROVAL:	51	
	11		(8 54
DISTRIC	T ENGINEER	a.	DATE
APPROVED			
FOR THE DIVISIO	N ENGINEER	_	DATE

FILE CUY -CUYAHOGA COUNTY CITY of CLEVELAND DATE OF LETTING___ CONTRACT Nº





FED. RD. STATE FEDERAL AID FISCAL DIVISION STATE PROJECT YEAR 15+00 16+00 17+00 2 OHIO UI- 1057 (I) CENTRAL VIADUCT PART I STONE WALL G. RAIL A OF WEST ABUTMENT 18+00 19+00 # PIER NO.1 ESTABLISHED DOCK LINE SECTION A-A (WATER LEVEL (1948) G. RAIL & OF WEST ABUTMENT & PIER NO.1 ESTABLISHED DOCK LINE SECTION B-B (WATER LEVEL (1948) G. RAIL OF WEST ABUTMENT & PIER No. 1 INNERBELT FREEWAY PLANNING OFFICE CITY OF CLEVELAND ESTABLISHED DOCK LINE CROSS- SECTIONS AT WEST ABUTMENT & ERIE R. R. SECTION C-C EXISTING WOOD PILES PRELIMINARY TEST PILE PROJECT WATER LEVEL (1948) 570 PART I CENTRAL VIADUCT-INNERBELT FREEWAY CUYAHOGA COUNTY USR 42 RELOCATED SCALE HOR. 1"= 20" VER. 1"= 20"

GENERAL NOTES AND QUANTITIES

SCOPE AND PURPOSE
This contract consists of (a) the furnishing and driving of
test piles at the locations shown on the general plan and
(b) the test loading of these test piles under the direction
of the Engineer, according to Items S-17 and S-18 of the
Construction and Material. Specifications and the following special provisions.

The purpose of this test pile project is to provide information for use in the design and construction of the pile foundations of a proposed viaduct at this site.

2. TYPE OF PILES
The types of piles to be tested by driving and loading are 14"
cast-in-place concrete piles and 12" steel H piles (128P53).

The cast-in-place concrete piles shall be of the type for which the casing is left in place and is driven without a mandrel. The casing of the first pile of this type driven shall be of approximately 7 gauge thickness, and this thickness of casing shall be used for subsequent piles unless and until appreciable constriction of the casing due to earth pressure occurs, as determined by the Engineer. If and after such constriction occurs, subsequently driven casings for this type shall be of approximately 5 gauge thickness. If a casing of 5 gauge thickness begins to constrict, as determined by the Engineer, the Contractor shall fill the lower part of the casing with high-early-strength cement concrete, to the extent directed by the Engineer, and after this concrete has set for 3 days driving shall be resumed. (It is contemplated that this testing of the type that is driven without a mandrel also will provide information regarding the required depth of penetration of left-in-place casings that are driven with a mandrel.)

3. TEST METHODS

The first testing shall be at Location No. 2 and the next at Location No.3. At each of these two locations, two armore piles of the 14" cast-in-place type and two or more of the 12" steel H type shall be tested under two conditions: first, 20" co. 1 by providing a sheeted hole 20 feet deep and at least 24 inches in diameter, either by driving an open-end steel pipe and excavating all materials from within the pipe, or by any other suitable method and then driving the pile within the sheeted hole; second, by driving another pile of the same type and size, and by the same hammer, in an adjacent location beginning at the natural ground elevation (without the use of such sheeted hole). From a comparison of the penetration behavior of piles driven under these two conditions (at the same depth of penetration below the upper surface of the ground and with the same total length of pile in the leads) the difference in penetration (with and without the use of the sheeted hole) will be determined by the Engineer, and at the remaining locations the test piles shall be driven without the use of the sheeted hole.

Each pile shall be driven to a capacity of 65 tons according to the capacity formula in Sec. S-18.05. For the first two or three piles of each type driven the coefficient value of 2 in the numerator of the formula will be used. For the driving of subsequent piles, or for additional driving of a pile (after test loading thereon has been completed and if another test load at greater penetration is requested by the Engineer), this coefficient probably will be changed to a greater or lesser value by the Engineer, depending upon test load indications. Since one of the purposes of this test pile project is to determine the evaluation of the capacity formula to cause correct penetration for the contemplated design capacity of 65 tons, the driving shall be stopped immediately after a capacity of 65 tons according to the formula, has been attained.

The maximum magnitude of the test loads shall be 3R unless the yield point is reached at a smaller load. The test loading may be applied by use of a loaded platform or by the use of anchor piles or by a combination of these means. A pile driven as a test pile may be used as an anchor pile (for another, adjacent test pile) only after all test loading on it has been completed.

Any pile used as an anchor pile shall be located at least 6 feet from the pile being test loaded.

Except as noted above, the casing of a cost-in-place concrete pile shall not be filled with concrete until after the test loading has been completed, in order to facilitate subsequent driving of the cosing to a greater penetration if so directed by the Engineer.

4. PILE DRIVING HAMMERS

The pile driving hammers used shall be of the single-acting steam type. The Contractor shall provide one hammer of approximately 15,000 ft. lb. (not less than 14,000, nor more than 16,500) and another of approximately 25,000 ft. lb. (not less than 24,000, nor more than 26,500) rated capacity, to be used as directed by the Engineer. It is expected that the 15,000 ft. lb. hammer will be adequate for this pile driving but the Contractor will be required to use both it and the 25,000 ft. lb. hammer, to ascertain the relative advantages and disadvantages of these two sizes. These hammers shall be applied on different piles or both shall be used in driving the same pile, as directed by the Engineer, but the use of both sizes of hammer on the same pile will not be required on more than six piles and the change from the one size to the other, where both sizes are used to drive the same pile, need be made not more than eight times altogether.

5. FINAL CLEANING UP

At the conclusion of each test, the cast-in-place piles shall be filled with concrete to within one foot of existing ground elevation and burned off at that elevation, or may be extracted and removed from the site, at the option of the Contractor. Likewise, unless extracted and removed from the site, the steel bearing piles shall be burned off at one foot below existing ground elevation. If steel pipe or other sheeting is left in the ground, the hole shall be filled with concrete to within one foot of existing ground elevation and burned off at that elevation. If piles, pipe or sheeting are extracted from the ground the holes shall be filled with concrete or with sand, flushed with water to eliminate voids. This also shall be done if a depression is created alongside a pile, pipe or sheet pile that is left in place. All equipment and material brought to the site by the Contractor except that which is left below ground, shall be removed from the site. All removed material, including extracted piles, pipes, and sheeting shall be considered the property of the Contractor.

6. REPORTS

A report on these tests will be prepared and submitted by the Engineer. The Contractor shall cooperate with the Engineer in the making of measurements, readings and observations.

7. METHOD OF MEASUREMENT AND BASIS OF PAYMENT
The lump sum item of "Pile Testing Overhead" is provided principally because of possible great variations in the final pay quantities of the unit price items (\$5-17, \$5-18 and the special item of 20ft depth sheeted holes) from the estimated quantities, in view of the research character of this project. This item shall be considered as providing payment for the following:(1) Moving all pile-driving and test-loading equipment to the indicated locations at the site and away from the site. (2) Furnishing at

the site, pile, pipe and sheeting materials which is not used and not paid for under the unit price items. (3) Cleaning up the site and removing all materials. The payment of the lump sum price bid for this item together with the payment at the unit prices bid for the quantities of work actually done under Item 5-17, the two 5-18 items and the special item of 20-ft. depth sheeted holes, shall constitute complete compensation for this work, including all labor material, equipment and incidentals.

No Item S-16 is provided in the quantities since all of the piles are test piles and are to be paid for per linear foot under Item S-18 and since the item of Pile Testing Overhead is provided.

All pile test loads shall be poid for at the unit price per Pile Test Load bid regardless of whether it be a first or subsequent load and regardless of whether it be another loading applied on the same pile or the loading of another pile at the same or another location.

For the purposes of payment the point of cutoff for the piles under Item S-18 shall be considered as one foot below the existing ground.

The basis of payment for the 20-ft. depth sheeted holes, obtained by use of a driven steel pipe or other approved method, shall be the unit price bid per hole and shall be considered to provide compensation for furnishing all material, equipment, labor and incidentals and for driving, excavating, filling with concrete, or extracting and filling the hole.

Payment will be made, at the price per lin. ft bid, for piles damaged in driving, if driven with the approval of the Engineer and if such damage is not caused by the Contractor's failure to operate properly, even though such piles must be abandoned and are not subjected to test loading.

The estimated quantities in the S-18 items are based on a possibility of 400 linear feet of 14" cast-in-place concrete and 400 linear feet of 12" steel H piling at each of Locations 2 and 3, and 200 linear feet at each of the other three Locations, a total of 1400 lin.ft. of each type. It is contemplated that there will be two piles of each type driven at each of Locations 2 and 3, and perhaps one pile of each type at each of the other three Locations. There is a possibility of more than this number because of additional piles needed to replace damaged piles that are not suitable for test loading or because of the need for a subsequent shorter pile of the same type where the test loading indicates that a pile hos been driven to a penetration greater than required. However, if the results of the testing at Locations 2 and 3 so indicate, the Engineer may consider it sufficient to test only one type of pile at each of Locations 4 and 5, but it is anticipated that it will be necessary to test both the cast-in-place concrete and the steel H pile at Location No.1.

The estimated number of 20-ft. depth sheeted holes is based on at least two such holes at each of Locations 2 and 3, with a possibility of twice this number if additional piles must be driven for the reasons stated above.

The estimated quantity of pile test loads is based on from 4 to 8 test loads at each of Locations 2 and 3, and from 1 to 4 test loads at each of the other three locations.

ESTIMATED QUANTITIES

As B	vi-It	ITEM	TOTAL	UNIT	DESCRIPTION
		Special	LumpSum		Pile Testing Overhead
c.o. 3 -4	4	Special	8	Each	20-ft. Depth Sheeted Holes
- 9	19	5-17	28	Each	Pile Test Loads
- 767.6	632.4	5-18	1400	Lin.ft.	14" Cast-in-place Concrete Piles
77774	622.6	S-18	1400	Lin.ft.	Steel Piles (12 BP 53)
	c.o. 3 - 4 c.o. 3 - 9 co. 3 - 767.6	co. 3 /9 co. 3 - 767.6 632.4	Special 4 4 Special 3 19 S-17 3 632.4 5-18	As Built ITEM TOTAL Special LumpSum	Special LumpSum

W Norted As-Buil

WHJ - 12-23-55

INNERBELT FREEWAY PLANNING OFFICE
CITY OF CLEVELAND

FEDERAL AID FISCAL PROJECT YEAR

FED. RD. STATE

2 OHIO UI-1057(1) | CENTRAL VIADUCT PART 1 CITY OF CLEVELAND

GENERAL NOTES & QUANTITIES
PRELIMINARY TEST PILE PROJECT
PART I

CENTRAL VIADUCT-INNERBELT FREEWAY CUYAHOGA COUNTY USR 42 RELOCATED

SHALAT.

DATE 4-10-53 4-11-53

CUY-90-15.45

STATE OF OHIO DEPARTMENT OF HIGHWAYS

UI - 1057(4)

KAR 15 19 R

2 OHIO UI 1057 (4)

CUYAHOGA COUNTY CITY OF CLEVELAND INNER BELT FREEWAY CENTRAL VIADUCT

PART 2

CUY - 42R - 17.50

INNER BELT FREEWAY - CENTRAL VIADUCT

BR. NO. CU - 42R-175 CUYAHOGA COUNTY

PART 2 SUBSTRUCTURE

THIS IMPROVEMENT HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR OF HIGHWAYS IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511. 02 REVISED CODE OF OHIO.

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I TITLE SHEET

QUANTITIES

3 GENERAL NOTES

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EXISTING CONDITIONS

IO, II RAILROAD TRACK MODIFICATIONS

23-28 PIER SHAFT DETAILS 29 ANCHOR BOLT PLAN AND DETAILS 30-32 REINFORCEMENT SCHEDULES 33-37 STORM SEWER AND DRAINAGE DETAILS

40-43 CONSTRUCTION IN ERIE RAILROAD YARD 10,11,35 R.R. FORCE ACCOUNT WORK

12,13 EXISTING CONDITIONS AT PIER LOCATIONS 36A REVISED SHEET NO. 36

14-22 PIER FOOTINGS

LINE DATA

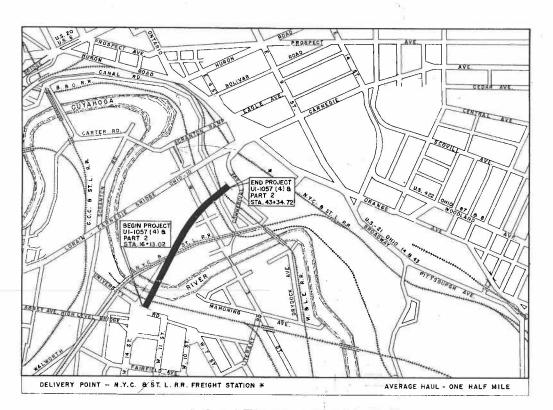
BEGIN PROJECT U1-1057 (4) END PROJECT

UI - 1057 (4)

STA. 16 + 13.02 STA . 43 + 34.72

NET LENGTH

UI- 1057 (4) AND WORK 2,721.7 LIN. FT. or 0 515 MILES



THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO DEPARTMENT OF HIGHWAYS INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH IN THE PLANS AND ESTIMATE.

THE RIGHT OF WAY FOR THIS, IMPROVEMENT WILL BE PROVIDED BY THE STATE

Louis L. Drasler DATE 9-2-54 DIRECTOR OF PUBLIC SERVICE, CITY OF CLEVELAND DIVISION DEPUTY DIRECTOR APPROVED APPROVED

DATE 7/1454 DEPUT SHEETOR OF PLANING AND PROGRAMMING

APPROVED

DATE 9-8-54 ENGINEER OF BRIDGES APPROVED DATE ____ ENGINEER OF LOCATION & DESIGN APPROVED

DATE 9-10-54 DEPUTY DIRECTOR OF DESIGN B CONSTRUCTION

APPROVED

DATE 9-14-54 INST ASSISTANY DIRECTOR

APPROVED PRINT APPROVED BY DATE 8-31-54 CHIEF ENGINEER, ERIE RAHLROAD PRINT APPROVED BY F. H. Simpson
DATE 8-31-54 CHIEF ENGINEER, NEW YORK CENTRAL SYSTEM

DATE CHIEF ENGINEER, NEW YORK, CHICAGO AND SAINT LOUIS RAILROAD CO.
PRINT APPROVED BY K ! Man----PRINT APPROVED BY

DATE 9-2-54 CHIEF ENGINEER, BALTIMORE AND OHIO RAIL ROAD

HOWARD NEEDLES TAMMEN & BERGENDOFF CONSULTING ENGINEERS

KANSAS CITY CLEVELAND NEW YORK

H.G. SOURS ASSOCIATE **COLUMBUS**

LOCATION PLAN SCALE IN FEET PORTION TO BE IMPROVED

			-
NUMBER	DATE	NUMBER	DATE
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7-171.19	R. 3-19-53		
M-101.7	1-24-53	- Marcon	

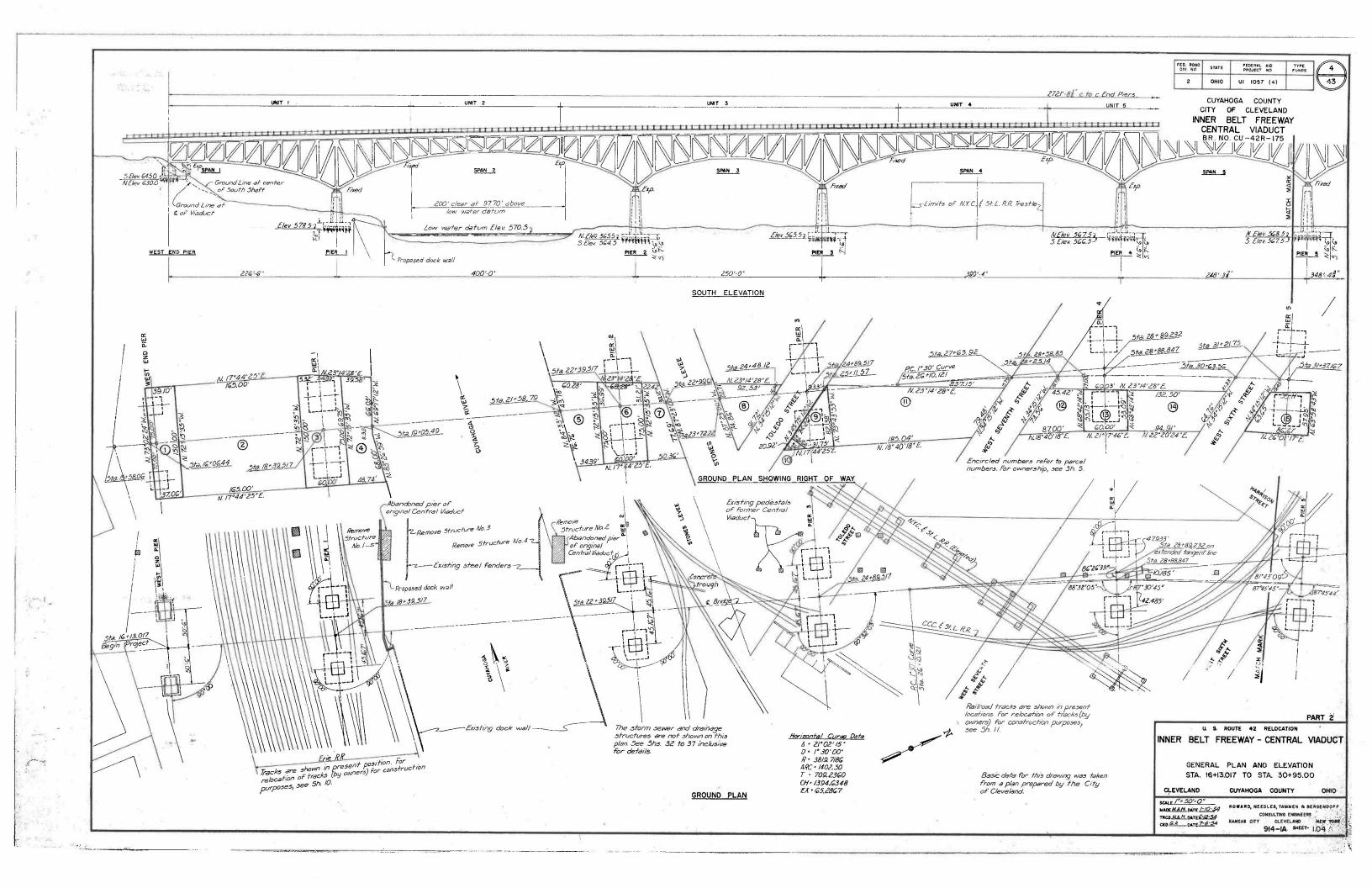
STANDARD DRAWINGS NUMBER DATE * Accepted. ** Submitted to B.P.R. for approval DEPARTMENT OF COMMERCE BUREAU OF PUBLIC ROADS

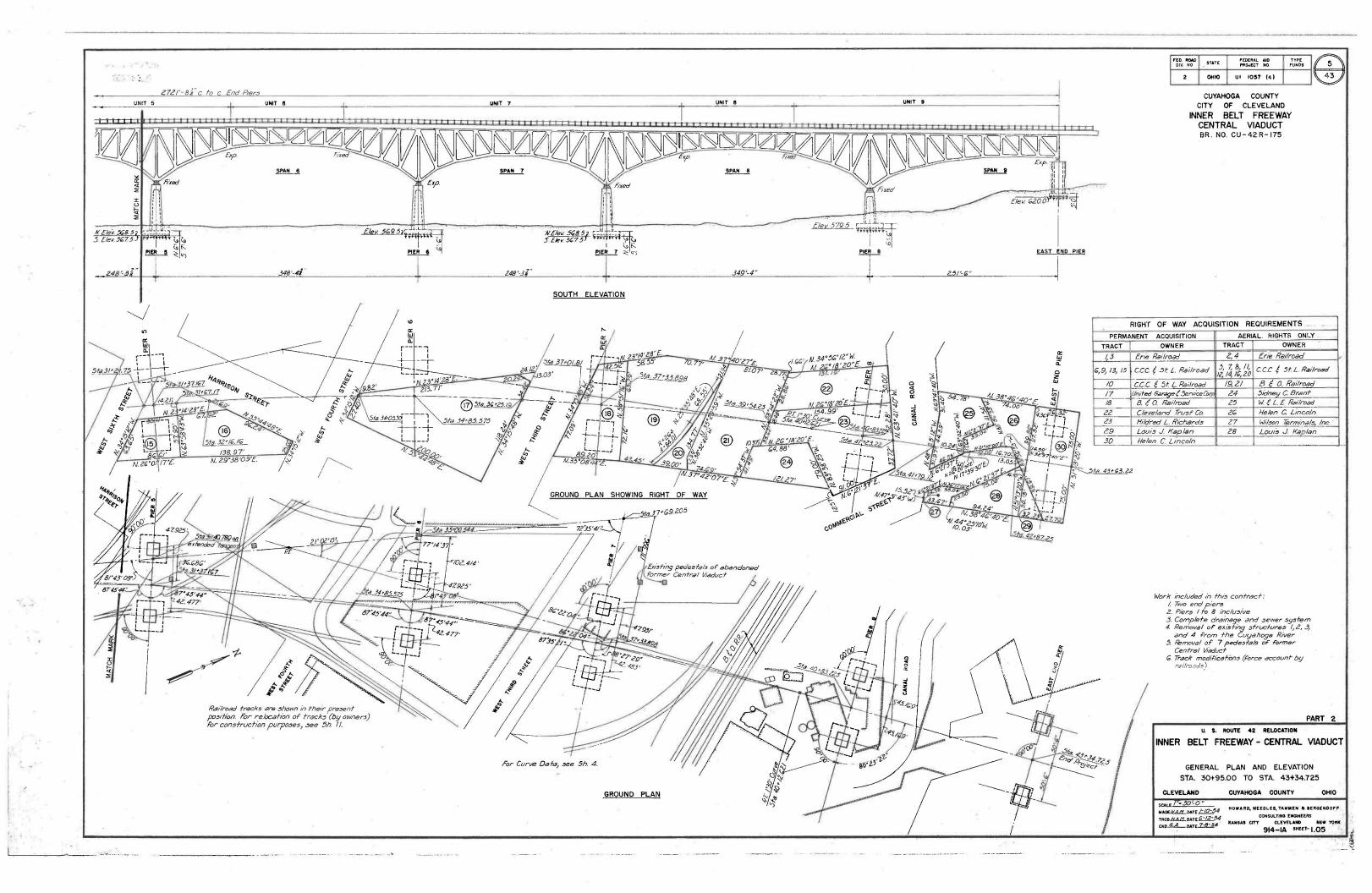
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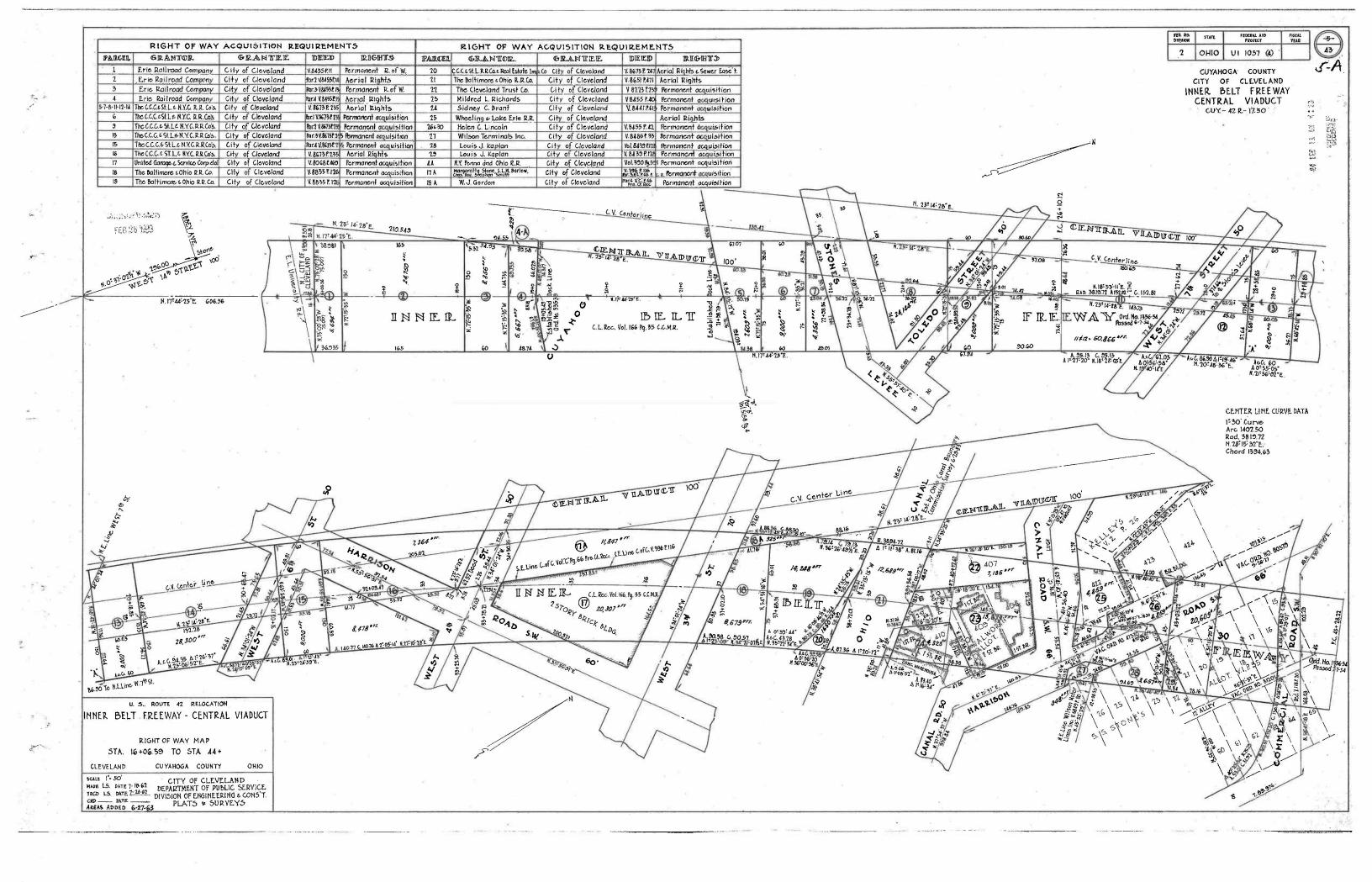
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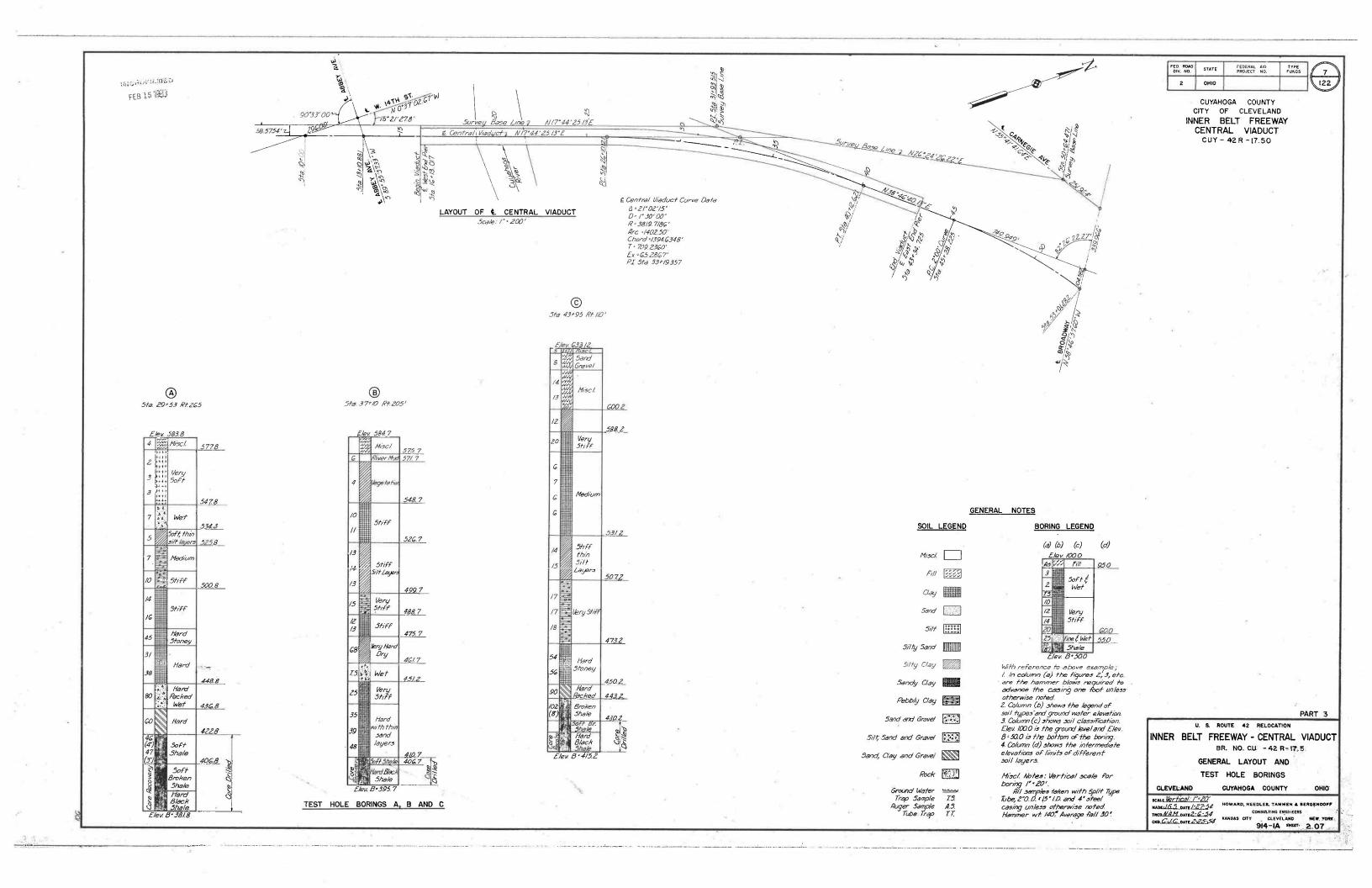
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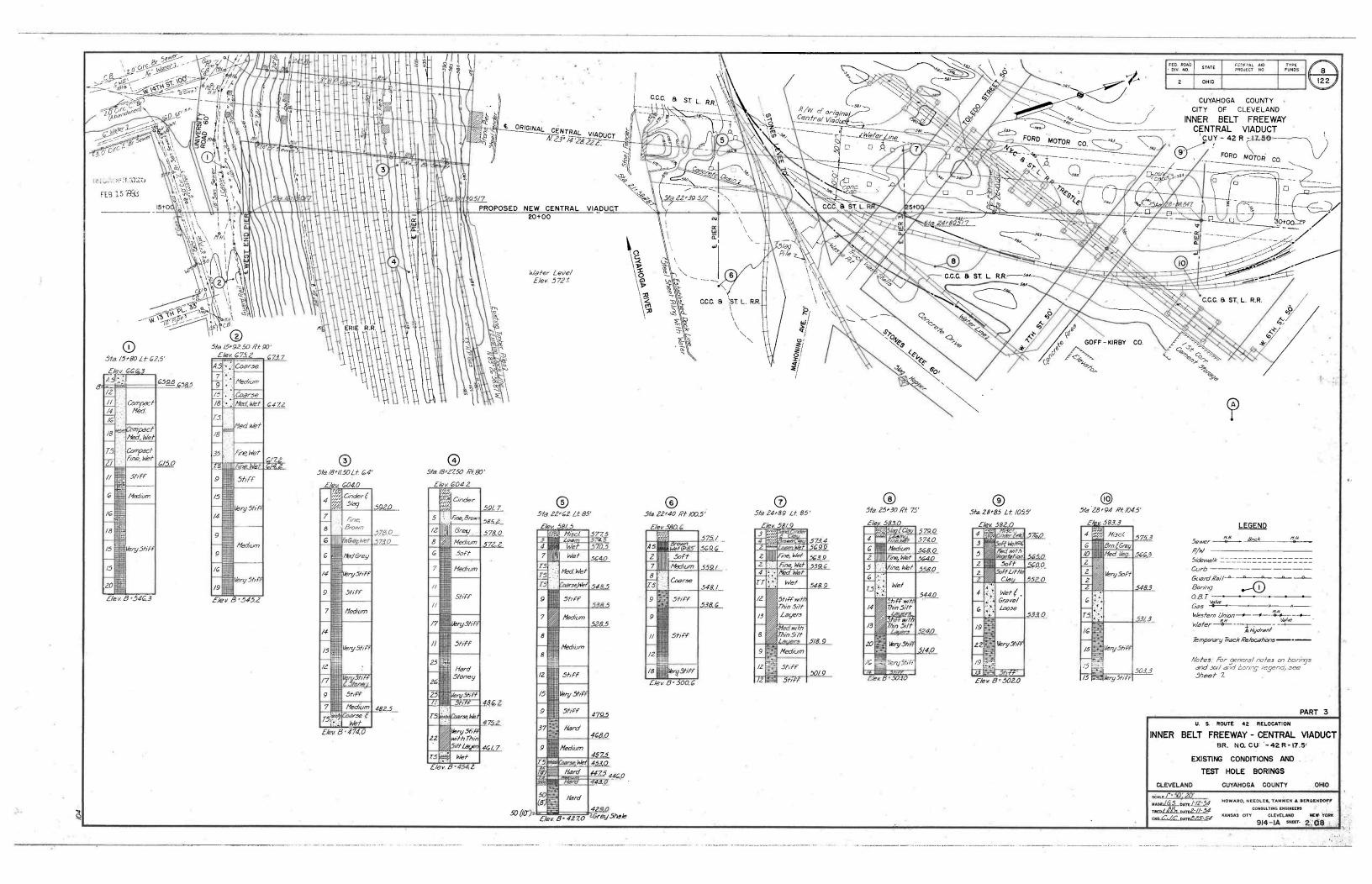
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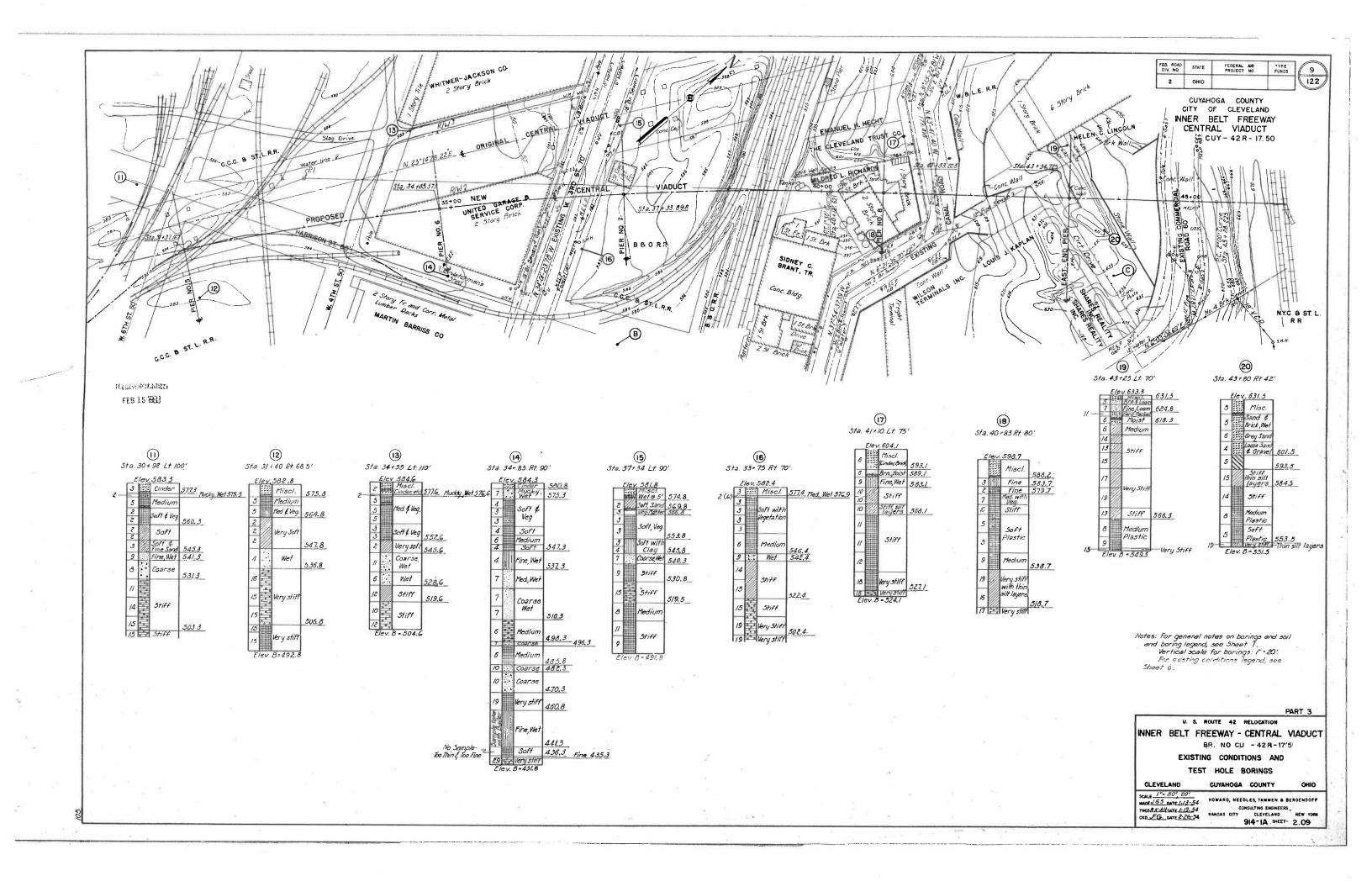


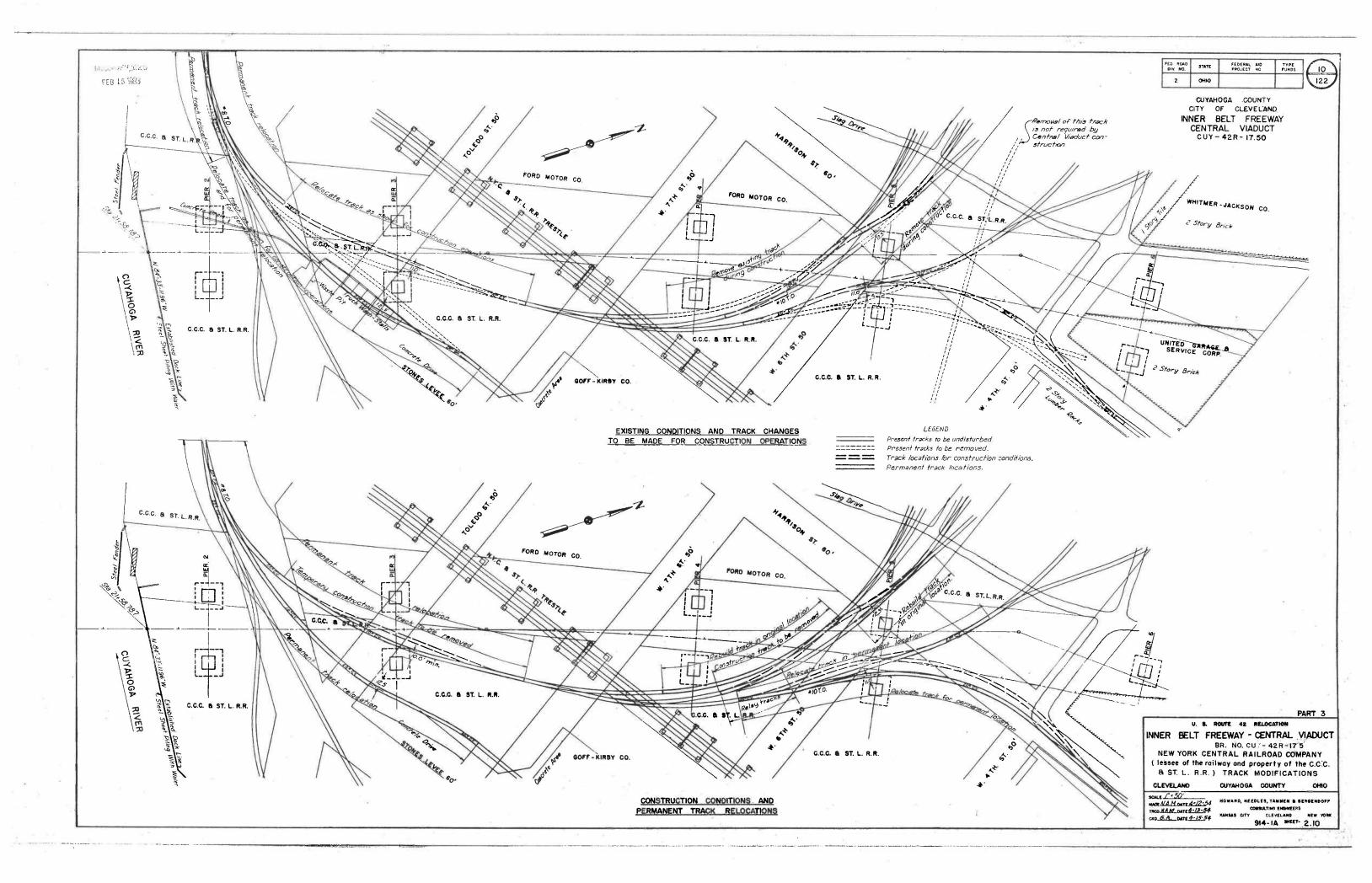












I - 1057(2)

STÂTE 2 OHIO 1-1057(2 CUY - 42 R - 17. 43

ROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR OF HIGHWAYS IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 REVISED CODE OF

MidroFilder FEB.25 1903

STATE OF OHIO DEPARTMENT OF HIGHWAYS

CUY-42R-17.43 CUYAHOGA COUNTY CITY OF CLEVELAND



IMITED ACCESS

PART 4-WEST APPROACH TO CENTRAL VI

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LINE DATA

67

BEGIN PROJECT STA. 12 +42.50 END PROJECT STA.16+13.02 NET LENGTH OF PROJECT (STRUCTURE)= ADDITIONS

33-35 REINFORCING SCHEDULE

36 ABUTMENT W-I

FREEWAY STA.5+00(±) TO STA.12+42.50

ABBEY AVE. STA. 0+97.42 TO STA. 7+86.87 TOTAL NET LENGTH OF WORK

370.52 LIN. FT. OR 0.070 MILE.

HANDRAIL DETAILS

742.50 ± LIN. FT. 689.45 LIN. FT.

1802.47 LIN. FT. OR 0.341 MILE

THIS SET OF PLANS, PART 4 OF THE INNER BELT FREEWAY, EXTENOS FROM WEST END PIER OF CENTRAL VIADUCT WESTWARD TO STA. 12+42.50. PARTS I THRU 3 ARE UNDER PREVIOUS

DELIVERY POINT: N.Y.C. & ST.L.RR.

LOCATION PLAN

AVERAGE HAUL. 1/2 MILE

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO DEPARTMENT OF HIGHWAYS, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH IN THE PLANS AND ESTIMATE.

THE RIGHT OF WAY FOR THIS IMPROVEMENT WILL BE PROVIDED BY THE STATE OF OHIO.

DIRECTOR OF PUBLIC SERVICE, CITY OF CLEVELAND **APPROVED** APPROVED DIVISION DEPUTY DIRECTOR DATE 6-7-56 _____ Orling of PLANNING AND PROGRAMMING APPROVED. DATE 6-21-56 DEPUTY DIRECTOR OF PLANNING AND PROGRAMMING
APPROVED
DATE 6-18-56 ENGINEER OF BRIDGES
APPROVED
DATE 6-18-56 ENGINEER OF LOCATION AND DESIGN
APPROVED
DATE 6-19-56 DEPUTY DIRECTOR OF DESIGN AND CONSTRUCTION
APPROVED
DATE 6-21-56 FIRST ASSISTANT DIRECTOR
APPROVED
DATE 6-21-56 DIRECTOR OF HIGHWAYS DATE 6. 21.56

> APR 1 1 1962 GROUND PHOTOLAS

PREPARED AND RECOMMENDED BY

HOWARD, NEEDLES, TAMMEN & BERGENDOFF CONSULTING ENGINEERS

KANSAS CITY / CLEVELAND NEW YORK

FILE NO. CUYAHOGA COUNTY 00079R SEC. DATE OF LETTING CONTRACT NO.

H. G. SOURS ASSOCIATE COLUMBUS

		MENTAL CATIONS		
NUMBER	DATE	NUMBER	16	DATE
. B-119 REV.	12-14-55	M-206.14		7-15-49
· L-209.12	7-17-54	M-109.23	REV	
5	6-8-55			
M-110.27	9-9-52			
5-114	8-30-55			



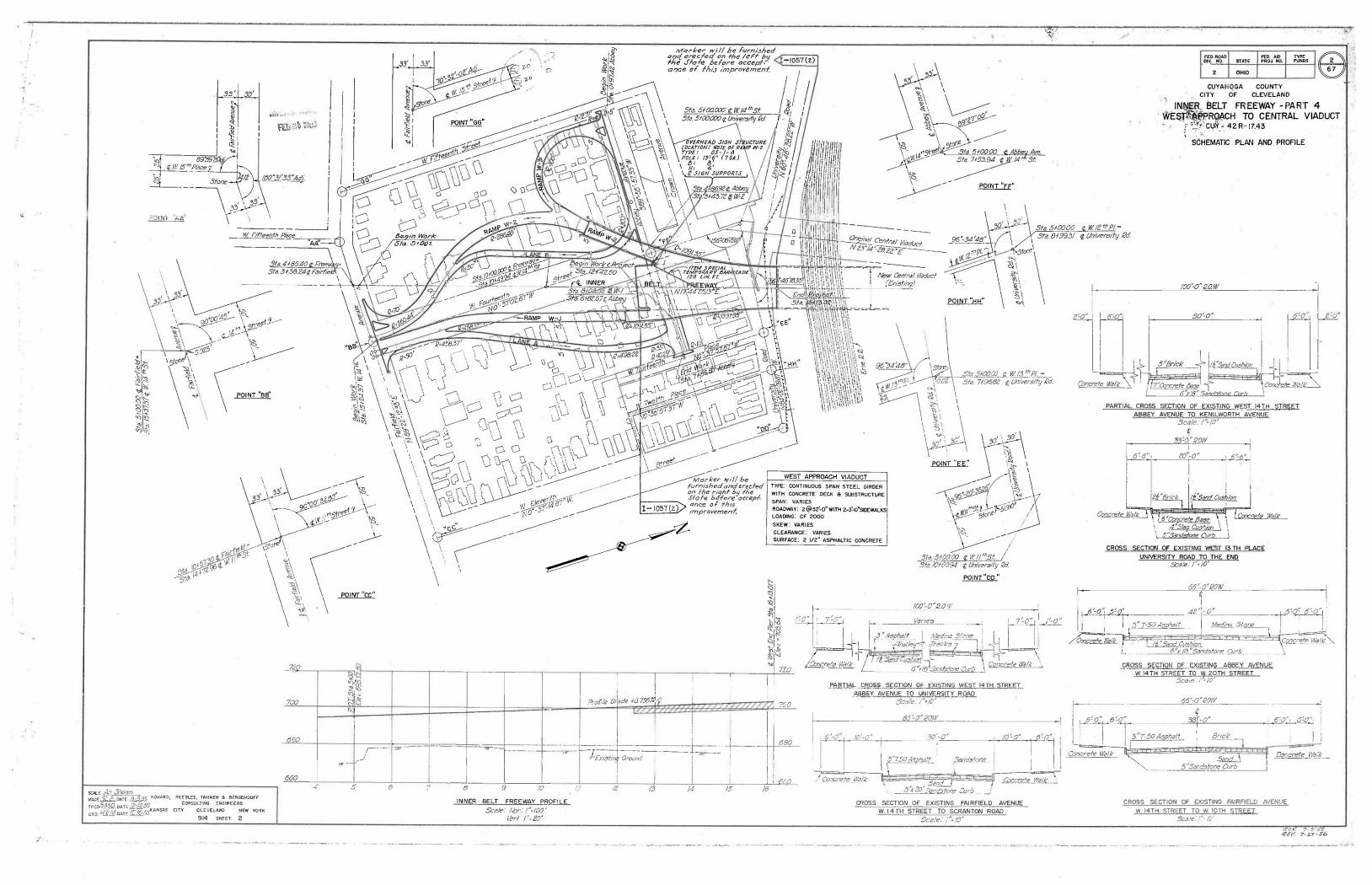
NUMBER	DATE	NUMBER	DATE
L-3	4-1-50	I-8 M.H. NO.1	5-1-52
L-3-A	4-1-50	1-15 NO. 1	8-1-55
RI-I	1-3-55	I-15 NO. 2	12-1-54
B-T-71R	3-2-53	BT 50-70-71E	10-1-47
LJ NO	7-1-55	AS-1-54	12-1-54
1-1, 2, 3, 4, 8 5	2-20-45	G-7.07	6-1-56
1-8CBN02-2A8B	5-1-52	I-8MH NO.I-A	1-3-55
I-80B NO.3	5-1-52	I-12	7-1-54
I-8CB NO.3 A	5-1-52	L-I	4-1-50
I-8 I NO. 2	12-1-54	·TJ	5-1 56
VS-1	7-1-55	1-8 M.H.NO.2	5-1-52

DEPARTMENT OF COMMERCE BUREAU OF PUBLIC ROADS APPROVED

DISTRICT ENGINEER

REV. 7.5-56 REV. 7-27-56

DATE



FED. ROAD STATE FED. AID TYPE PROJ. NO. FUNDS 2 OHIO (16) CUYAHOGA COUNTY Ā (1) 2-St 2-St Fr. Fr CITY OF CLEVELAND 1-5t. 1-5t Fr. Fr 2-5t. Br 2.St Fr 2.37. Fr. 5 В ... INNER BELT FREEWAY-PART 4 2-5t.Fr (7) (B) WEST APPROACH TO CENTRAL VIADUCT 8 HEROFILMED Proposed_ Right of Way 2-5t.Fr. 4 CUY-42 R-17.43 1 1-5t 9 (10) (4) (15) FEB 25 1437 (3) RIGHT OF WAY 2.5t. Fr. 3 2-5t.Br 990' 2-St. Fr. 2-5t. Fr. 1 1-St. Fr. 1-9t. Fr. (21) (a) F (a) 2-5t. Fr. (31) 26) (22) - P 2.5t.Fr. 25) PR.A Participation Construction 1-St. Conc. 3-St. Fr (32) RAMP W-2 2-St. Fr 2-St. Fr. 2-5t. 1 2-5t. Fr (27) 2-St. Fr. 2-5t. Fr. 1 St. Br. DEED TAKE PARCEL NO. Gas Station RESIDUE OWNER (33) 16,352.00 4,844.00 3,360.00 ALL (24) Received Cheaning Co. Hillcomb Realty Co. 11,508.00 Thos & J. Barylski Mary Babinchak Meribetts & Wm. Mihalik Mike & Anna Fekete 0.00 3,360.00 3,584.00 3,472.00 1,038.14 R.F. f. Sapienza & J.P. Arcara 926 29 16-B Homer & Agnes Cox John R & Birdle T Meltsch Eather T Williams Dale & Vera Yeager Roy & Helen Bartoszek **Ç W. 14TH.** ∦ST. 928.29 16-C 921.94 923.05 1,233.29 16-D 16-E 16-F FREEWAY 6,144.12 Rev. Andrew Chemyshin Joseph & Theodora Boriczuk 9.153.20 21,063.40 " 30,233.28 29,208.28 26 27 John F. & Helen Marshal Michael Mizenko RAMP 30,25,28, 22,208,28 15,122,25 12,861,75 7,562,94 5,962,44 22,693,77 16,011,27 15,133,14 9,754,14 9,174,20 5,914,20 John Lies Sam Broszko Anthony J. Toman Rose Thomas f. E.C. Zigsir Cerl f Anna Lemmermann Sinclair Refining Co. 6,682.50 5,379.00 2-54. Fr. LANE 2-5t. Br. 2-St. Fr. 3,260.00 22-St Fr. 2-St. Br 6,900.00 ALL 15,048.00 230.50 18,468.00 1,630.12 0.00 2:51 Fr. Greek Orthodox Church Ceroline Rice Estelle Zwolinski & Agnes S. Tornen Dr. Martin Luther Evangelical Church Savak Luthuran Church Congregation Same Owner,Parc. 3 1-5t Br. 2-St.Fr. 1-9t. Br. 48 16,837.87 9,760.12 11,055.00 12,474.00 3,102.00 42) 4 2-St.Fr (47) 11,628.00 1,867.87 2.5t.Br Right of Way 1-St. Br 2-5t. Br | 43) 2.5t Br. 7,524.00 4,422.00 8,778.00 5,813.50 8,778.00 6,525.75 15,048.00 12,870.00 39 40 41 Sturak Latheran Church-Congregation Same Owner Parc 37
Devid Mured (51) 44 2,964.50 2,252.25 2,178.00 0.00 45) David Murad Walter & Amy R. Higs Constatine Nicholan 1-St. Fr. 46 2-5t, Fr. 257.8 7980.00 ALL 7980.00 " 39 Outgo to Incorona Julius, Margt, & Win.F. McGuire Nellie Bukhair Mervin & Ethel M. Cary Egrl T. Benjamin, 785 Cleveland Trust Co. 1- St. Br 34) /-St.Fr 38 35) 1-St. Conc. 11.019.24 0.00 37) 1-5% Fr. 2-St. Br. 1-3t.Fr 0.00 0.00 0.00 0.00 0.00 0.00 1-St.Fr (53) 11,019.24 89 660 _ € W. 13 TH. PL 10,228.96 6,124.61 5,375.63 Vincent & Mary Triner Catherine Zemba Petro Petrick 88 5 375 63 52 (56) 57 (59) 3,000.00 0.00 39 - A Same Owner, Parcel 37 3,000.00 54) (55) (58) 87 39A 60) 30'-Sophia M. Maske W.G. Lewicky 0.00 12,656 25 ALL & W. 12 TH PL. Describing Belitto H. Cesar & G. M. Finesilver
Emesto Rodriguez Nelson N. Moss 0.00 (86) **61** 85 62 84 83 82 79 76 80 78 77 63 75 74 (73) 64) 72 (71) (70) (69) 68 67 66 65) € W. II TH. ST. SCALE 1"-50"

MADE 1 20 55" COARD, NEEDLES, TAMMEN & BERGENDOFF
CONSULTING ENGINEERS
CKD 1111. DATE 1 2 55 KANSAS CITY CLEVELAND NEW YORK Y CLEVELAND NEW YORK 914 SHEET 3

Rev 2-27-62

WEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR OF HIGHWAYS IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 REVISED CODE OF

> MICHOFILMED FEB 23 MA

STATE OF OHIO DEPARTMENT OF HIGHWAYS

INNER BELT FREEWAY

CUY - 42-(17.43-18.02) CUYAHOGA COUNTY CITY OF CLEVELAND

MAR & 5 1967 GROUND PHOTOLAR

> I -1057(5) LIMITED ACCESS

GRADE SEPARATION WITH NEW YORK, CHICAGO AND ST. LOUIS RAILROAD AND CLEVELAND UNION TERMINALS COMPANY

INDEX OF SHEETS

100	IIIOLA OI	<u> </u>	<u> </u>	
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3.	SEQUENCE OF CONSTRUCTION	60.	DELETED	INCLUDES THE SURFACING OF
4.	GENERAL NOTES	61-63.	DELETED	THE WEST APPROACH VIADUCT,
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44-45	PIER IS DETAILS	108-109	DRAINAGE DETAILS	
46-47	PIER 2B DETAILS	110	HANDRAIL DETAILS	
48-49	PIER 3B & IE-2 DETAILS	113	LIGHTING DETAILS	
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52-55	PIER 5B-8B DETAILS	113-117	WEARING SURFACE DETAILS	
			The second second	

LOCATION PLAN

DELIVERY POINT: N.Y.C. & ST. L. R.R

AVERAGE HAUL: 1/2 MILE

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO DEPARTMENT OF HIGHWAYS, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT.

I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH IN THE PLANS AND ESTIMATE.

THE RIGHT OF WAY FOR THIS IMPROVEMENT WILL BE PROVIDED BY THE STATE OF OHIO.

Louis L. Drasler APPROVED. DATE 1/9/57 DIRECTOR OF PUBLIC SERVICE, CITY OF CLEVELAND APPROVED

DATE 7.2.57 DIVISION DEPUTY DIRECTOR DEPUTY DIRECTOR OF PLANNING AND PROGRAMMING APPROVED DATE 10.25-57 ENGINEER OF BRIDGES APPROVED DATE__ 10-18-57 APPROVED DATE _ 10-22-57 _ ENGINEER OF LOCATION AND DESIGN APPROVED

DATE 10:22-51 DEPUTY DIRECTOR OF DESIGN AND CONSTRUCTION APPROVED FIRST ASSISTANT DIRECTOR DATE.... APPROVED DATE 10/28/52 ACTING DIRECTOR OF HIGHWAYS APPROVED _ CHIEF ENGINEER NEW YORK, CHICAGO AND ST. LOUIS RAILROAD DATE _ 11-7557. DATE 10:30-57 CHIEF ENGINEER CLEVELAND UNION TERMINALS COMPANY

LINE DATA

REGIN PROJECT STA. 12 ± 42 50 FND PROJECT STA. 58 ± 00 00 NET LENGTH OF PROJECT 4.557.50 LIN. FT. OR 0.863 MILES ADDITIONS: STA.58+00 TO STA.60+20 \$57A 10+70 TO 12+42.5 392.50 LIN.FT.
..NEW EAST NINTH ST. STA.10+752 TO STA 19+61±= 8860 \(\) LIN.FT. BROADWAY WEST BOUND STA. 37+8986 TO STA. 49+60±= 1,170.14 LIN. FT. NET LENGTH OF WORK = 7,006.14 "LIN. FT. OR 1.326 MILES

PREPARED AND RECOMMENDED BY

HOWARD, NEEDLES, TAMMEN & BERGENDOFF CONSULTING ENGINEERS

KANSAS CITY CLEVELAND NEW YORK

E NO. CUYAHOGA COUNTY - OC 051-K SEC. CUY-42-17.43-18.02) DATE OF LETTING CONTRACT NO.

H. G. SOURS ASSOCIATE COLUMBUS

CUIDDI EMENITAL

		ATIONS	#2
NUMBER	DATE	NUMBER	DATE
-		S 207	4-28-55
E-101	1-1-57		
5	6-8-55		1.0
S-114	8-/-57		
M 206.14	7-15-49		

0 400	80 0	1600	2400	3200
7-5-5-	SCALE	IN FEE	T	
	I TO BE			

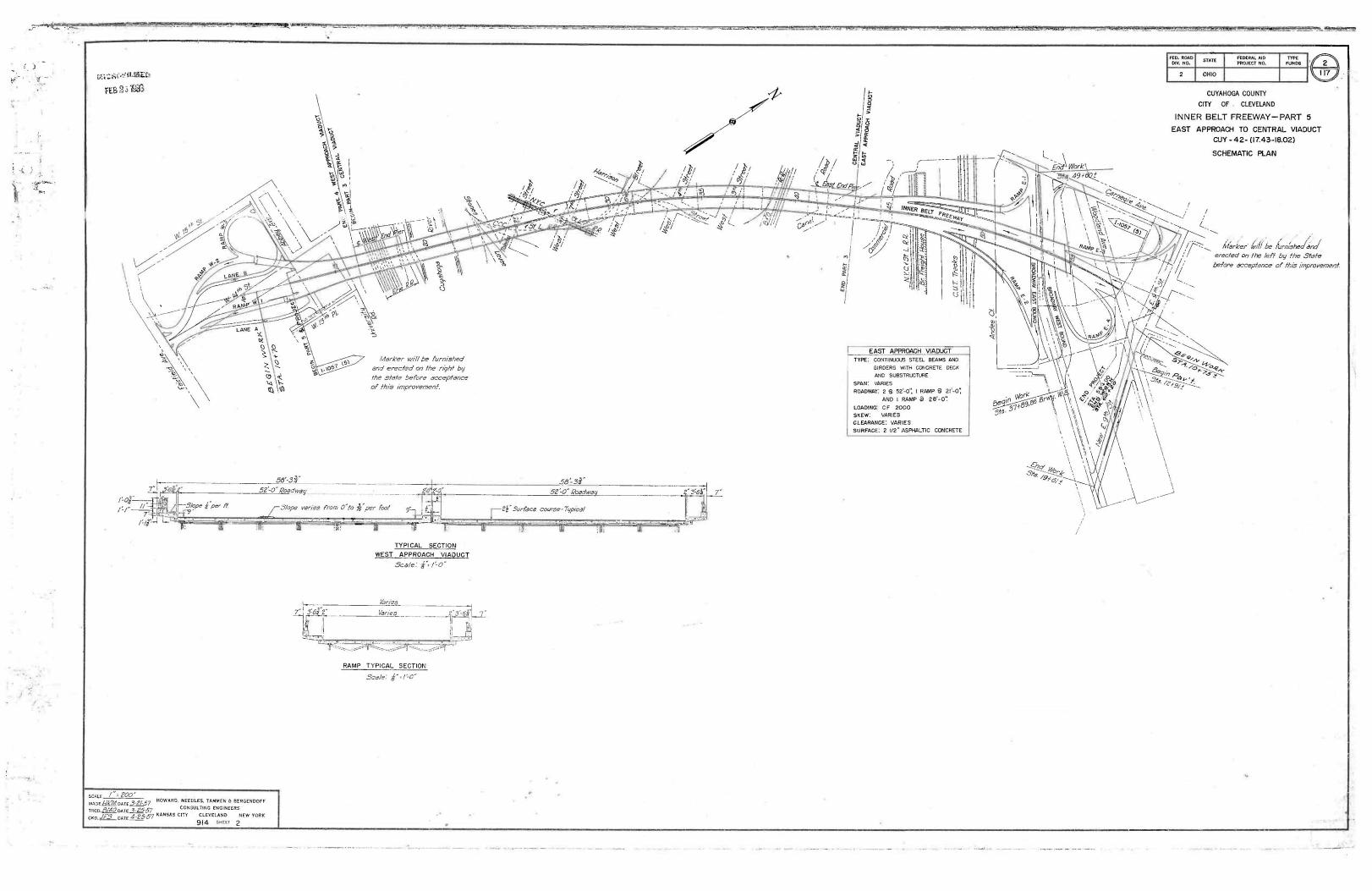
STAND	DARD	DRAWINGS	S
NUMBER	DATE	NUMBER	DATE
L-3	4-1-50	05-2	12:17-50
L-3-A	4-1-50	1-5 NO. 2-B	6-1-57
R1-1	1-3-55	8T-50-70-71E NO.1	10-1-47
8-T-71 R	3-2-53	AS-1-54	12-1-54
LJ NO I	7-1-55	G -7.07	6-1-56
1-1, 2, 3, 4, 8 5	2-20-45	I-BMH NO. I-A	1-3-55
1-808 NO. 2-2488	8-1-56	1-12	7-1-54
1-8CB 2-3 8 2-4	5-1-52	. L- 1	4-1-50
I-81 NO.2	12-1-54	TJ	5-1-56
1-8 MH NO. I	5-1-52	.OS-I	12-17-56
1-8 MH NO. 2	5-1-52	1-21:23	.856
I-15 NO. I	8-1-55	1-14-G	1-22-52
		1-35	1-2 56

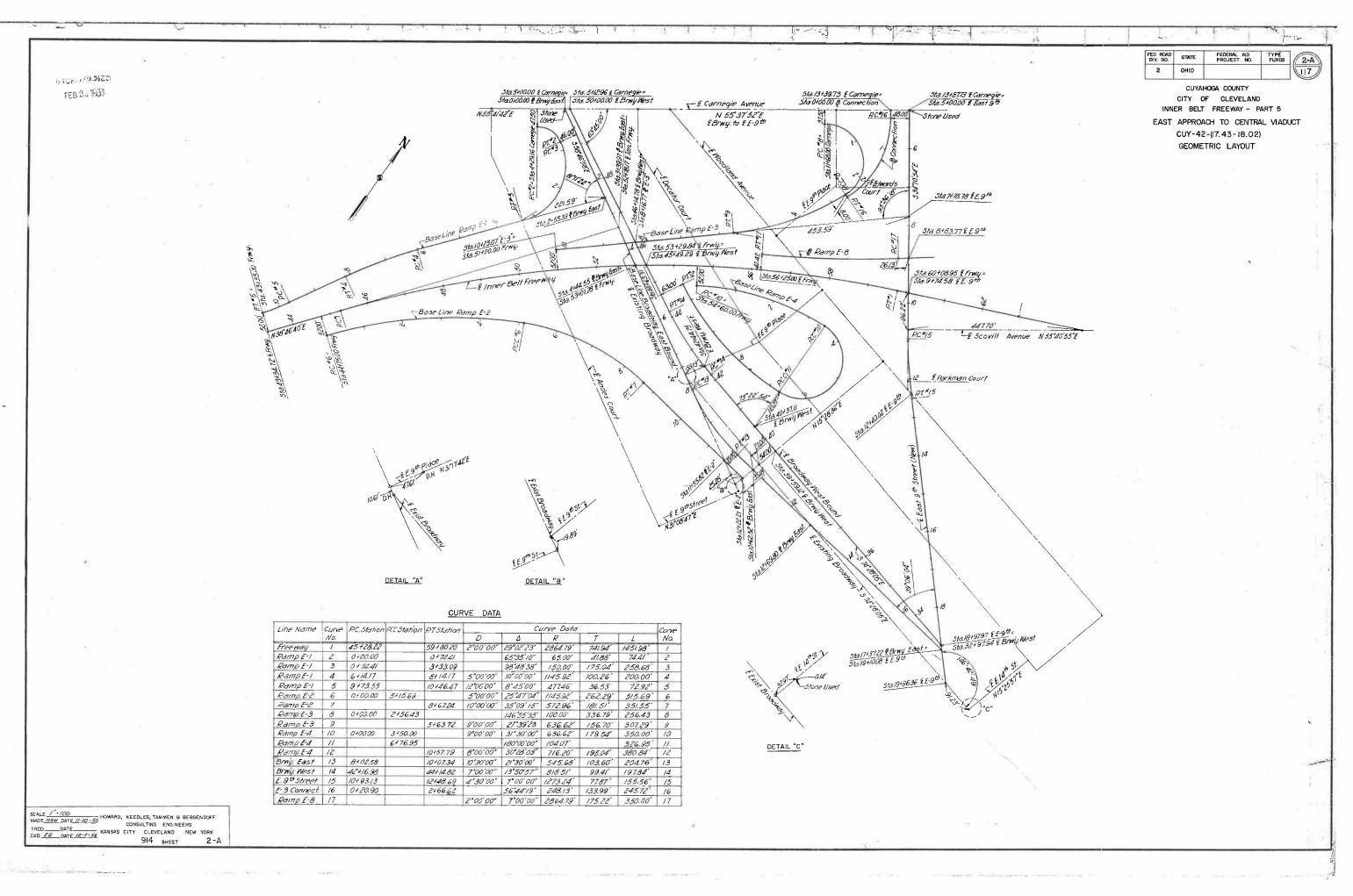
68 151972 GROUND PROTOLAS DEPARTMENT OF COMMERCE BUREAU OF PUBLIC ROADS

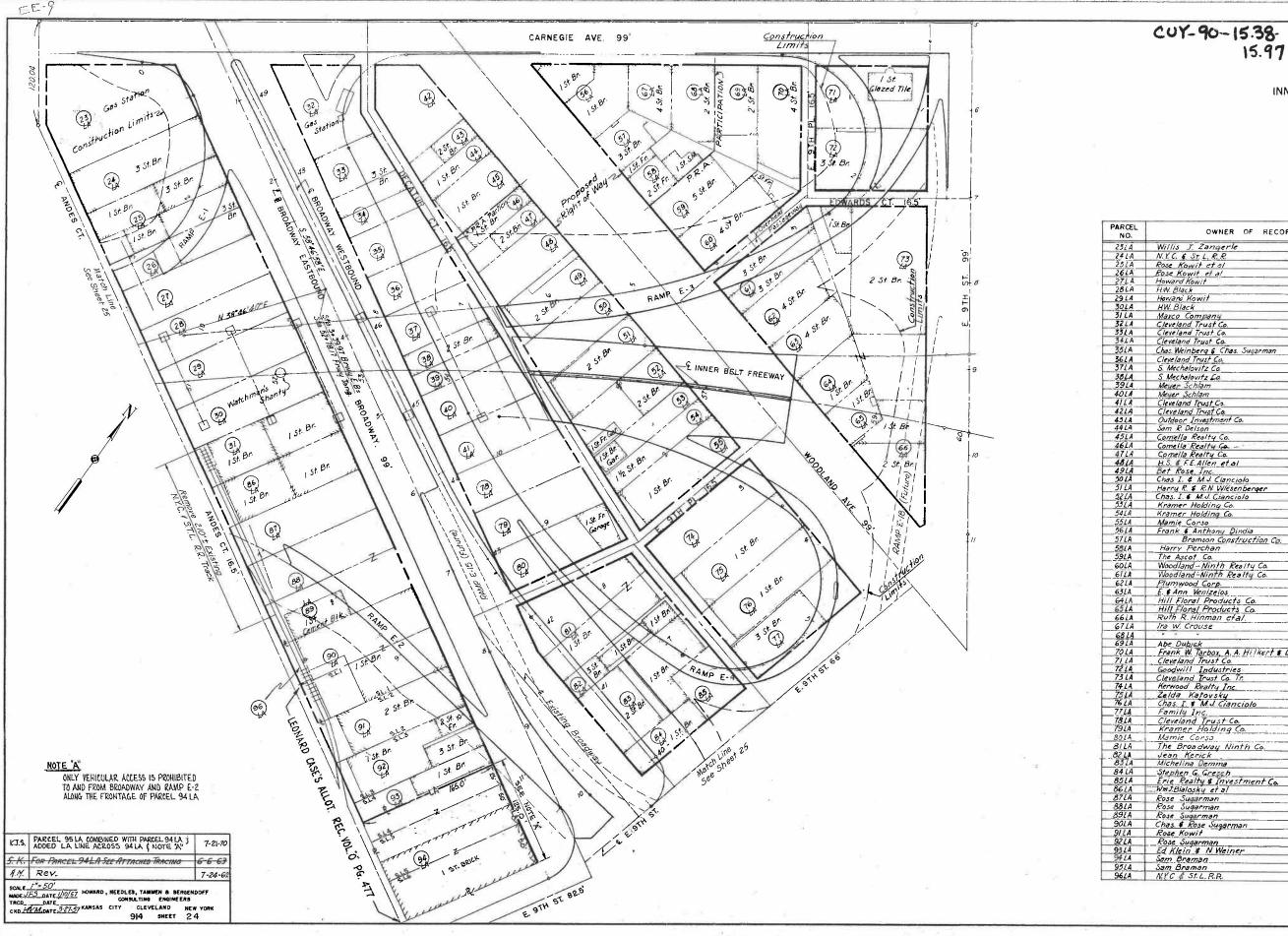
APPROVED:

DIVISION ENGINEER

DATE







15 . S

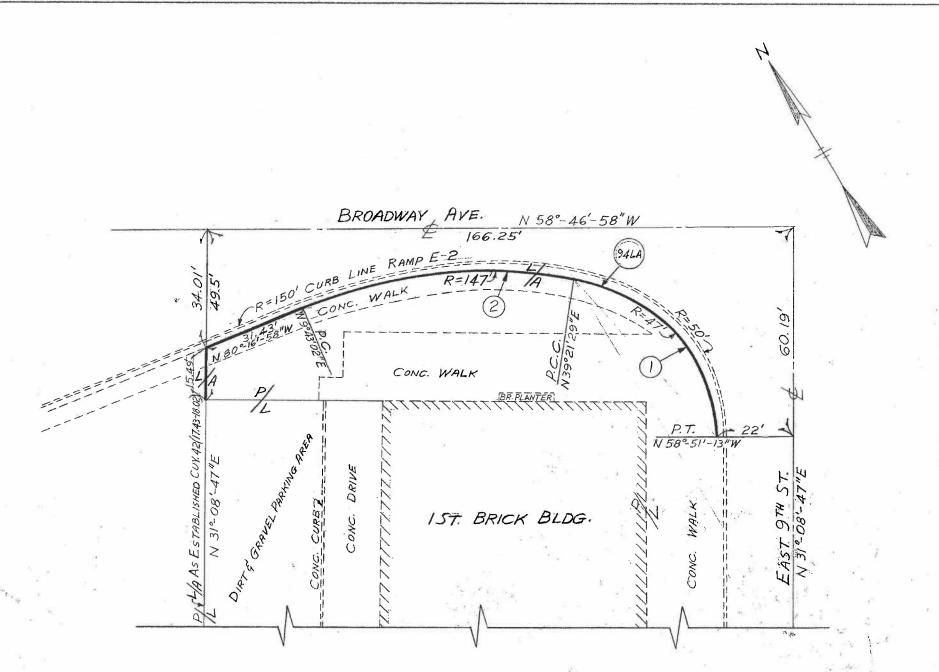
FED ROAD DIV NO.	STATE	FED. AID PROJ. NO	TYPE FUNDS	1
2	OHIO	7-1567(5)		1

CUYAHOGA COUNTY CITY OF CLEVELAND

INNER BELT FREEWAY - PART 5 APPROACH TO CENTRAL VIADUCT CUY- 42-(17.43-18.02)

RIGHT OF WAY

PARCEL	OWNER OF RECORD	AREA
NO.	OWNER OF RECORD	SQ. FT.
25LA	Willis J. Zangerle	15.683.1
24 LA	N.Y.C. & St. L. R.R.	8,250.0
25 LA 26 LA	Rose Kowit et al Rose Kowit et al	10,725.0
27/ A	Howard Kowit	5,775.0 8,250.0
27LA 28LA	H.W. Black	8.250.0
29 LA	Howard Kowit	8.250.0
30LA	HW. Black	8.250.0 8.250.0
31 LA	Maxco Company	8.250.0
32 L A 33 L A	Cleveland Trust Co. Cleveland Trust Co.	8.421.1 3.9050
34 LA	Cleveland Trust Co.	4.180.0
35LA	Chas. Weinberg & Chas. Sugarman	4.520.8
36 L A	Cleveland Trust Co.	4.941.0
37LA	S. Mechalovitz Co	4,941.0 5,287.0
38LA	S Mechalovitz Co.	2,773.3
39LA 40LA	Meyer Schlam	2.859.4 5977.5
41LA	Meyer Schlam	6323.0
421A	Cleveland Trust Co. Cleveland Trust Co.	7.085.5
43 LA	Outdoor Investment Co.	2.08/.8
44 LA	Sam R. Delson	2,168.3
45LA	Comella Realty Co.	4 7866
46LA	Comella Realty Co	2.14.2.5
47 LA	Comeila Realty Co.	2,607.5
48 (A 49 (A	H.S. & F.E. Allen et al Bet Rose Inc.	5.287.0 5.632.8
50 LA	Chas I. & M.J. Cianciolo	5,977.5
51 LA	Harry R. & R.N. Wiesenberger	6 3250
52 LA 53 LA	Chas I & M.J. Cianciala	6.668.8
55LA	Kramer Holding Co. Kramer Holding Co. Mamie Corso Frank & Anthony Dindia	3464.1
54LA 55LA	Mamie Conta	3,550.5 7360.5
56 LA	Frank & Anthony Dindia	5757.2
57 LA	Bramson Construction Co.	5,757.2 3,170.8
581A	_ Harry Perchan	4.335.2
59LA	The Ascot Co.	4.335.2 5.500.3
60LA	Woodland - Ninth Realty Co.	6,665,0
61LA	Woodland-Ninth Realty Co.	6,581.2
62LA 63LA	Plumwood Corp. E. & Ann Venizelos	3668.7 4.756.2 9.267.0 2.755.1
64 LA	Hill Floral Products Co.	92670
65 LA	Hill Floral Products Co.	2755./
66LA	Ruth R. Hinman et al.	8,155.5
67LA	Ira W. Crouse	3,170.8
68 LA 69 LA	" " "	4 3 3 5, 2 5, 500. 3
70 LA	Abe Dubick Frank W Tachay A A Hilkest & / Botzum	6,665.0
7114	Cleveland Trust Co.	95248
72 LA	Frank W. Tarbox, A. A. Hilkert & L. Botzum Cleveland Trust Co. Goodwill Industries	9,524.8 9505.4
73 LA	Cleveland Trust Co Ir	17006.2
74 LA	Kerwood Realty Inc.	7961.3
75 LA 76 LA	Zelda Katovsky	7961.3
77 LA	Chas. I. # M.J. Cianciolo Family Inc.	7,961.3 7,961.3
78 LA	Cleveland Trust Co.	6.668.8
79 LA	Cleveland Trust Co. Kramer Holding Co. Mamie Corso	7.014.5
801A	Mamie Corso	7,360.5
BILA	The Broadway Ninth Co.	12,828.0
82 LA 83 LA	Jean Kerick Michelina Demma	4,075.5 11,137.5
84 LA	Stenhen G Green	3,300.0
85 LA	Stephen G. Gresch Erie Realty & Investment Co.	3.745.5
86 LA	wmJ.Dialosky et al	8.250.0
87LA	Rose Sunarman	8.250.0 8.250.0
88 LA	Rose Sugarman .	8.250.0
891A 901A	Kose Sugarman	8,250.0
91LA	Chas. & Rose Sugarman Rose Kowit	14,900.0
92 LA	Rose Sugarman	8.250.0 8,172.5
93 LA	Ed Klein & N Weiner	4,125.0
93 LA 94 LA	Sam Braman	LA ONLY (SEE NOTE A
951A	Sam Braman Sam Braman N.Y.C. & St. L. R.R.	COMBINED WITH PAR.94LA
964A	N.Y.C. & St. L. R.R.	1,600-0



Fed. Road Division	STATE	FED. AIP PROJ. NO.	TYPE FUNDS	(2A-A)
2	OHO	T-1057(5)	X - 1-3	117

CUYAHOGA COUNTY
CITY OF CLEVELAND

INNER BELT FREEWAY - PART 5

APPROACH TO CENTRAL VIAPUCT

CUY -42-(17.43-18.02)

RIGHT OF WAY

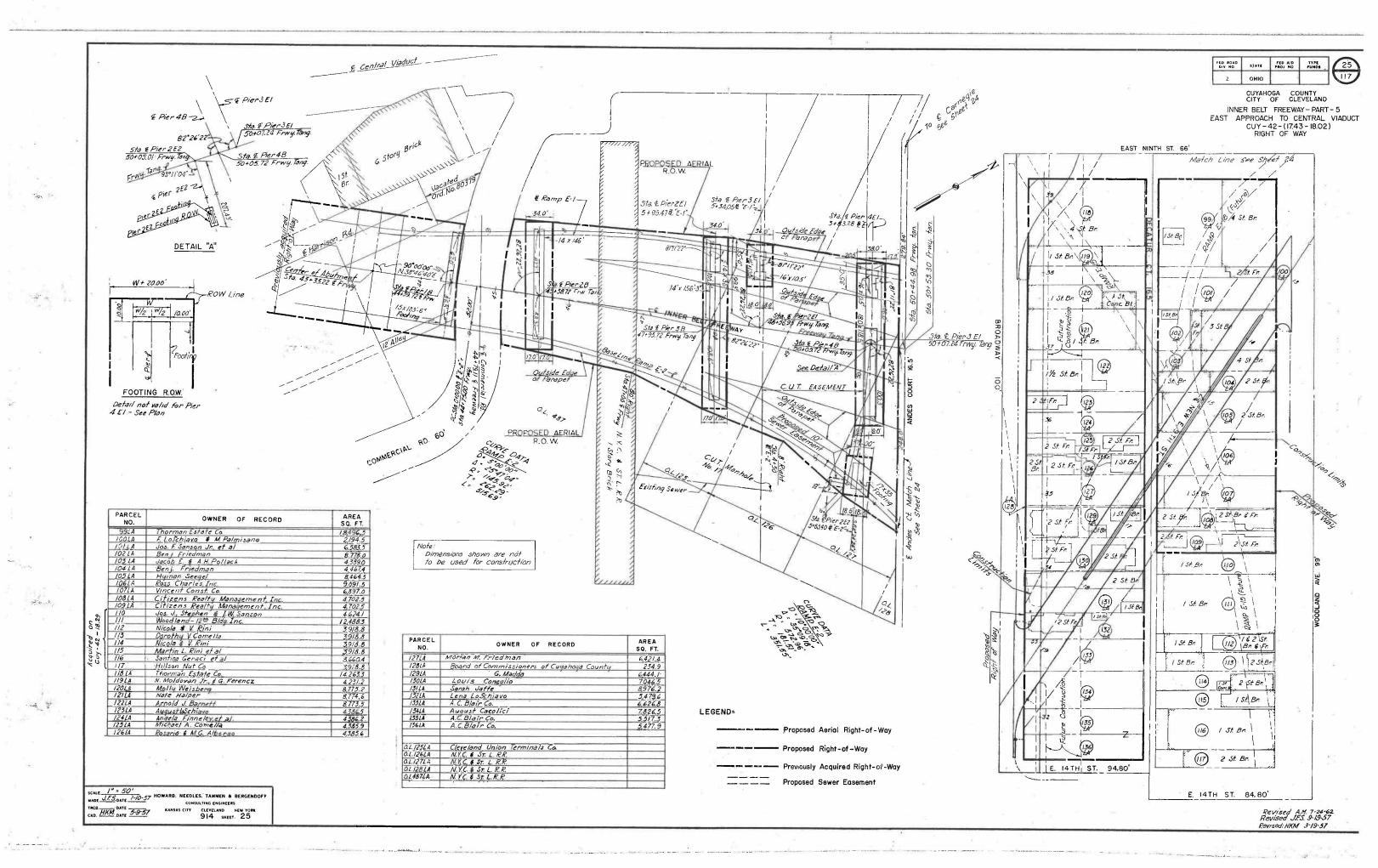
. /	LIMITED	ACCE55	1
	CURVE	DATA	
	7)	2	
	81°-47'-18"	Δ=29°-38	
R = 4	17.00'	R= 147.0	
T = 4	10.70	T = 38.8:	
Ch = 6		Ch= 75.2	
A = e	57.09'	A = 76.0.	5
Ch.Bear	ing = N92-44'-52'W	Ch. Bearing = NGS	-27'-44"W

PARCEL 94 LA

STATE OF OHIO
DEPARTMENT OF HIGHWAYS
PLAT OF
LIMITED ACCESS RIGHT OF WAY
FOR PROPERTY OWNED BY
SAM BRAMAN
CITY OF CLEVELAND
CUYAHOGA COUNTY

SCALE: 1"=20'

JUNE 6,1963



PROVEMENT IS ESPECIALLY DESIGNED FOR THROUGH TRAFFIC AND HAS BEEN DECLARED A LIMITED ACCESS HIGHWAY OR FREEWAY BY ACTION OF THE DIRECTOR OF HIGHWAYS IN ACCORDANCE WITH THE PROVISIONS OF SECTION 5511.02 REVISED CODE OF OHIO.

STATE OF OHIO **DEPARTMENT OF HIGHWAYS** CUY-90-16.24=

APPROVED DATE 12/12/59

APPROVED DATE 12/11/59 APPROVED' DATE 3-11-60

APPROVED DATE 3-4-60

APPROVED

STANDARD DRAWINGS

DATE

NUMBER

DATE 3 . 9 . 60

ungeto 144 f 140 ravisad G-14-10. Sweet in the sale V-10-10. Skeets 153 f 154 ravisad 6-29-44. Just 182 NOV 18 ravisad 7-15-10

Specto 12, 45-50 revised 8-22-61 AEC

incore 148 6 43 revised resident.

MICROFILMED

FED. ROAD STATE OHIO 1-71-5(6)247

CUY-42-18.29

MAR 6 1965 POWER PLOTOLAR

PART 6

CUY 42-18.29

CUYAHOGA COUNTY CITY OF CLEVELAND

WILLOW- INNERBELT FREEWAY PART 6

DELIVERY POINT - N.Y.C. ST.L. R.R. BROADWAY TEAM TRACKS AVERAGE HAUL 1/2 MILE

I-71-5(6)247 LIMITED ACCESS

Since the Construction Plans for PART Nº 6, PART Nº 7-A and PART Nº 7-B are now Combined into ONE CONTRACT and PROJECT; the General Notes and Traffic Maintenance Notes of each of the aforesaid PARTS, shall apply in the general execution and completion of this COMBINED PROJECT.

For Part 7-A, see plans for CUY-21-15.32; CUY-42-18.42 For Part 7-B, seeplans for CUY-42-18.77

THE STANDARD SPECIFICATIONS OF THE STATE OF OHIO DEPARTMENT OF HIGHWAYS, INCLUDING CHANGES AND SUPPLEMENTAL SPECIFICATIONS LISTED IN THE PROPOSAL SHALL GOVERN THIS IMPROVEMENT. I HEREBY APPROVE THESE PLANS AND DECLARE THAT THE MAKING OF THIS IMPROVEMENT WILL NOT REQUIRE THE CLOSING TO TRAFFIC OF THE HIGHWAY AND THAT PROVISIONS FOR THE MAINTENANCE AND SAFETY OF TRAFFIC WILL BE AS SET FORTH IN THE PLANS AND ESTIMATES.

THE RIGHT OF WAY FOR THIS IMPROVEMENT WILL BE PROVIDED BY THE STATE OF OHIO.

Louis L. Drasler DIRECTOR OF PUBLIC SERVICE, CITY OF CLEVELAND DIVISION DEPUTY DIRECTOR
Stuy C. Weeper DEPUTY BIRECTOR OF PLANNING AND PROGRAMMING Stoverman ENGINEER OF BRIDGES Wrenco ENGINEER OF LOCATION AND DESIGN
DEPUTY DIRECTOR OF DESIGN AND CONSTRUCTION DATE 3-9-60 APPROVED DATE 3-9-60 DIRECTOR OF HIGHWAYS APPROVED DATE 3-9-60

LOCATION PLAN

CONVENTIONAL SIGNS

PAVEMENT PLANS SHEFT 16 DRAINAGE PLANS SHEET 31 LIGHTING PLANS SHEET 45 EXISTING UTILITIES

SCALE IN FEET

PORTION TO BE IMPROVED OTHER HIGHWAYS & STREETS

NUMBER

MAR SPY SROWN TO ILLE

> DEPARTMENT OF COMMERCE BUREAU OF PUBLIC ROADS

APPROVED:

DIVISION ENGINEER

INDEX OF SHEETS

ı	SMEE	NO.				
l	1 =	TITLE SHEET 37-	41,414: "C'C	DRAINAGE PROFILES	116-120	BR. NO. CUY-42-1854
ı	2	SCHEMATIC PLAN	42-43	DRAINAGE DETAILS	121-131	RAMP E-15 OVER
ı	3	SEQUENCE OF CONSTRUCT	110N 44-50	LIGHTING PLANS		W.B. BROADWAY
١	4-7	TYPICAL SECTIONS	51	LIGHTING NOTES	132-143	RAMPS E-15 & E-18
l	8-9	GEOMETRICS 52,	52-A,53,53-A	LIGHTING DETAILS		OVER E. 9TH ST.
١	10	GENERAL NOTES	54-60	EXISTING UTILITIES	144-149	BR. NO. CUY-21-1573
l	11-13	QUANTITY CALCULATIONS	61	CROSS-SECTION KEY SHEET	150-153	BR. NO. CUY-21-1559
l	14	GENERAL SUMMARY OF	62-83 3-4,83 83-C,83 3-83-E	CROSS-SECTIONS	154-155	BR. NO. CUY-21-1544
ı	5	QUANTITIES 84	,84-A,85-90	RIGHT OF WAY	156	END DAM DETAILS
	15	SUMMARY OF DRAINAGE	91-91A	GENERAL NOTES FOR	157	DRAINAGE DETAILS
ŀ		QUANTITIES		STRUCTURES	158	RAILING AND GUARD
	16-22	PAVEMENT DETAILS	92-92A	ESTIMATED QUANTITIES		RAIL DETAILS
	23-27	ROADWAY PROFILES	93-99	E.B. SCOVILL OVER W.B.	159-160	LIGHTING DETAILS
	28-29	MISCELLANEOUS DETAILS		EAST 14TH ST.	161	RETAINING WALLS ALONG
	27-4,5	APPROACH ELABS	100-115	BR. NO. CUY-42-1832		E. 14TH ST.
	31-36	DRAINAGE PLANS			162-175	OMITTED
	10.00	3X		LINE DATA		

LENGTH OF PROJECT

LINNER BELT		STA	. 58 + 00	то	STA.64+68.53	= 668.53'
2	I-71-5(6)	247	<u> </u>	ENGTH	OF PROJECT	= <u>668.53</u> '
LENGTH OF WORK		((4))				
& INNER SELT	8	31 A.	53+45	10	STA. 64+68.53	= //23.53'
ADD 00000 (Co.)			4- 1			

ADD E. 1444 ST SOUTH GOTTAND) STA 0+33 = 752' STA.14 +00 STA.10+38

I-71-5(6)247

= 3885.53' = 0.735 MILE

= 0.126 MILE

= 0.126 M'LE

PREPARED AND RECOMMENDED BY

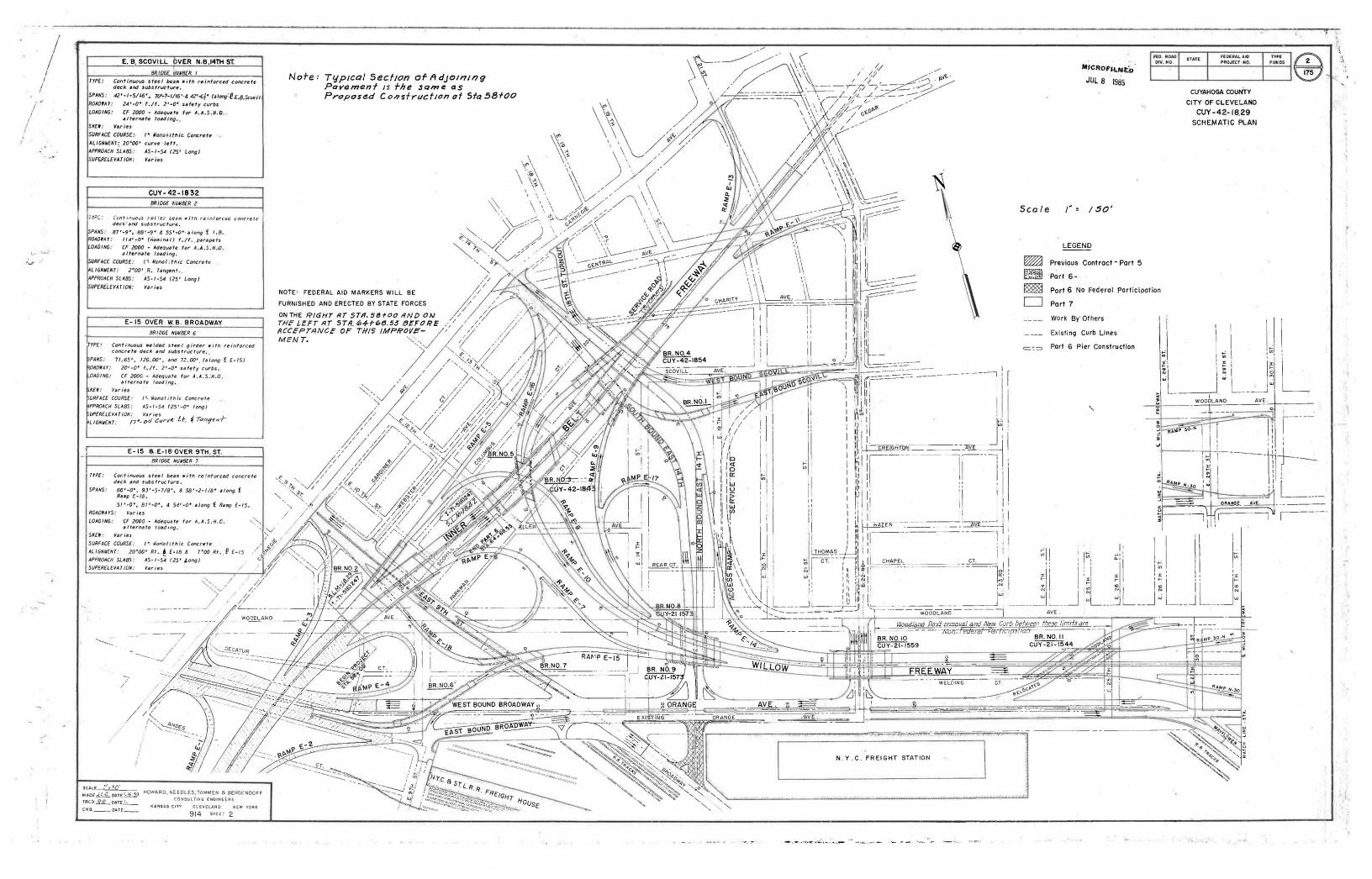
HOWARD, NEEDLES, TAMMEN & BERGENDOFF CONSULTING ENGINEERS KANSAS CAY CLEVELAND NEW YORK

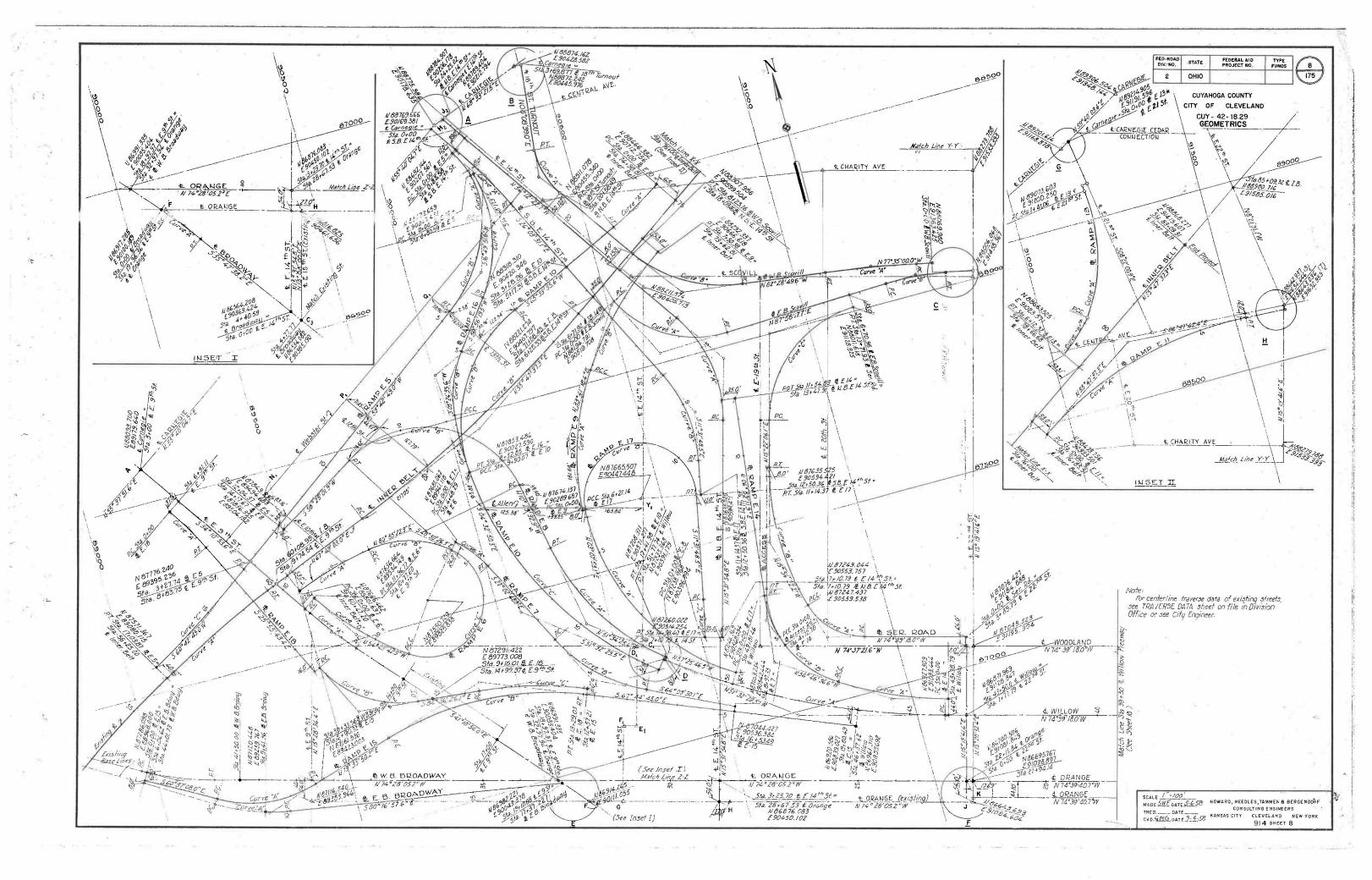
00124-1

FILE NO. CUYANGGA COUNTY SEC. CUY-42-18.29 DATE OF LETTING CONTRACT NO.

ADD RAMP E-18

H.G. SOURS ASSOCIATE COLUMBUS





OCCODED TO THE	0.00	BEGINNING	OF CURVE	P.I.	OF CURVE	ENDING (OF CURVE				COLUMN TERM		
DESCRIPTION	CURVE	STATION	COORD.	STATION	COORD.	STATION	COORD.	Δ	D	R	T	L	REMARKS
FREEWAY	S												
NNER BELT	"A"	45+ 28, 225	N 86831.706 E-88267.935	52 + 70 . 170	N87410.112 E88732.616	59 + 80.200	N 87690.243 E 89419.645	29°02'23.0"	2°00'	2864.789	741.945'	1451.980	
WILL DEET	"B"	62+98.330	N 87810.354 E 89714.228	67+00.601	N 81962.236 E 90086.125	70+99.918	N 88188.383 E 90419.411	120 01 25.7	1030'	38 19. 719	402.271	801.588	
MILLOW	"A"	44+.07.286	N 66887.127 E 91073.603	47+97.821	N 86.990, 476 E 90696, 991	51+77.735	N 87233.368 E 90391, 179	23°06'48.5"	3° 00'	1909.859	390.535	770.499	
RAMPS											1		-
RAMP E 5	"A"	0+00.000	N 88692.144 E 90207.567	1+09.796	N 88601.340 E 90269.292	2+04.877+	N 88496.247 E 90237.501	51°02'14.3"		230.000'	109.796	204 877	
	"B"	2+04.877†	N 88496.247 E 90237.501	3+41.346	N 88365.624 E 90197.986	4+68.324	N 88284859 E 90087.983	36°52′574	14°00'	409.256	136.469	263.447	
	"C" ·	13+53.866	N 87760. 775 E 89374.176	15+29.084	N 87657, 079 E 89232, 937	17+03.866	N 87571.367 E 89080.115	7°00'00.0"	z°00'	2864.789	175.218	350.000	
RAMP E 6	"A"	0+00.000	N 87666.462 £ 89501,697	1.09.690	N 87707 877 £ 89603.268	2+18.423 t	E 03/11, 503	13°06' 19.3"	6°00'	954.930	109.690	218.423	
	6	2+16 +23 r	£ 89711 585	3:41,446	1. 5 ⁷ 42. 590 £ 8 9 533 067	4+48 256 t	E 03325.211	50°34 11.3	22° CO'	260.435	123.023	229.863	
	"C"	4+48.286 t	N 87663.085 E 89925.217		N 87550.729 E 89825.838 [‡]	9 + 03.238 t	N 87428.265 E 89739.220	173~46'43.8		150.000		454.952	
2112	"D"	9+03.238 +	N87428.265 E89739.220	10 + 51.595	N 87513.934 E 89618.098	11+96.766	N 87636.664 E 89534.749	20°32'48.9	7000	818.511	148.357	293.528	
QAMP E7	"A"	0+00.000	N 87681.06Z £ 89941.118	0+54.287	N 87645.095 E 89981.781	1+01.302	N 87596.745 E 90006.467	21°21'37.5	' 20°00'	286.479	54.287	107.302	
211.2	"8"	3+73.218	N 87359.898 E 90127.392 N 87238 965	5+20.736	N 87 28 514 E 90194.471	6+57.922	N 87163. 582 E 90327. 124	37°00'41.3	13°00'	440.737	147.518	284.704	Arabo-Survey are seen
RAMP E8	"A"	0+00.000	E 90396.994	2+26.383	N 87379 663 E 90219.644	4+33 /37	# 87602.309 £ 90178.677	41°08'52.7	9"30'	603,113	226.383	433. /37	
0.440 5.0	*8"	6+17.137	N87783.262 E 90145.379		N 87732,593 E 89870,002*	11+60.353	N 87971. 121 E 89723.358	111°09'25.2		280,000	408 601	543 216	
RAMP E 9	Ά"	0+00.000	N 87674.137 E 90289.687	1+95,640	N81869,485 E90300.374	3+81.963 t	1188032 271 E90408.891	30°33'253'	8°00'	716./97	195.640	381.963	
	"B"	3+81.965 t	N 88032,271 E 90408,891	5+68.492	N 88187 475 E 90512.355	7 + 50.384	K 58292, 337 E 90666.618	22°06′ 18.9°	6000	954.930	186.529	368. 421	
RAMP E 10	Α"	0+00.000	N 88466.382 E 907/2.756	0+81.629	N88420.493 E 90645.247	1+62.862	N 88386.727 E 90570.929	9°46' 18.3°	6.00.	954.930	81.629	162.862	
	" <i>B</i> "	5+06.536	N 88244.565 £ 90258.037	7+83 798	N88129.875 = 90005,608	9+90.072+	N 87853.486 E 90027.590	70°06'45.9	14° 30'	395.143	277.262	483.536	
	"C"	9+90.072 +	N 87853.486 E 90027.590	13+88.623	N 87456.189 E 90059.188	17+41.980	N 87208.311 E 90371.277	46°59'39.2	* 6° 15'	916.732	398.551	751.908'	
RAMP E 11	"A"	0+00.000	N 88473.756 E 0933.501 N 8 073.603	3+24,879	N 88656. 394 E 91202.182	6+26.297	N 88637, 288 E 91526,498	37°34'403	6°00'	954.930	324.879	626.297	
PAMP E 13	"A"	1+41.058	E 91200.250	3+21.916	N88874.670 E91226.567	4+83.653 †	N 88750.281 E 91117.656	45°23′37.8°	13°15'00.0°	432.421	180.858	342.595'	
	`B"	4 + 83.653 †	N 88750. 281 E 91117. 656	5+48.689	N88698 360 E 91078.492	6 + 12.323	N88663.525 E 91023.571	20"35"13.9"	16°00'00.0"	358.099	65.036	128.670	
RAMP E 14	*A"	0+00.000	N 86927 309 E 91093 444	1+50.205	N 86967 058	2+97.969 t	N87049.355	17052'41.4	6.00.	954.930	150 205	297.969	

6+16.786

4+73.210

3+44.957

6+73.599+

8+32.854

6+21.139.1

11 + 14.366

14+98.396

1+37.547

13+19.180

12+48.686

16+05.361

2+93.432

11 159 292

1+38.363

2+85 526

8+23.162

3-30.016

2 + 32.632

4+50.336 1

8+41.684

13+71.927

1+61.734

10+07 340

1+28 514

E 90527.654 N 87907.461 E 89261.395 N 87310.653

N87538,606 E89556,620 N87969,753 E90760,286 N85611,077 E90277,188 N87849,928 £90643,385 N86570,099

90413.451

153203.210

90682.795

12+57.214

63°45'48.5" 20° 00'

45°49'01.8 17°00'

38°41'245 7°00'

51"44'368 15° 00'

31°31'43.9 18°00'

9°51'25.9 6°15'

60°33'39.6 9°45'

04000'063 4000

8°15'10.2" 6°00'

7°00'00.0 4°30'

49°44'175 16°00'

08°08'516 4° 00'

37°35'19.6 15 60'

41°30'32.4" 30°00'

4°53'49.6" 2°00'

47°06'56.2" 15°00'

66000112 20000

14' 28'423 9"00

31018283 8000

72"34"11.6" 18" 00"

37°11'56.1 23°00'

21°30'00.0 10°30'

5° 23'158" 5° 00'

58°42'55.8

188°23'554

E 89291.519 0 10.2 6 00 N87295.520 58°40 427 20°00'

187213.040 E 90166.573 16°51'41.1 6°00'

87/56.582

E 90028.788

E 90623.402 1188642.058 E 90421 197 V 38121.094

954.930 150.205 297.969

286.479 178.190 318.817

337.034 142.428 269.513

287.362 552.718

185 243 344.957

89.859 175,160

79.858 159,315

343126 621.139

50.042' 100.044'

165.989 310.864

954.930' 68.893' 137.547'

286.479 161.023' 293.393'

954.930 141.535 281.024

77.875

102.013'

72 375

186-053 330.018

30.366 160.871

98.437 179.336

200.692 391.348

233 633 403.166

83.8331 161.734

103.593 2.04.760

53.917' 107.754

122,502

166.545

493.227

155.556

203.692

250.592

138.363

2.44.855

3.14.104

818.511

381.972

318,310

587.649

150 000

1432.395

12/3.240

358.090

1432395

2864 789

381.972

286 479

636.620

175 000

716.197

318.310

249.112'

545.674

1145,916'

4+76.159

3+46. 125

9+91.858

1+85.243

5+88.238

7 + 53 397

3+43.126

14 + 48.394

0+68.893

7+40.040

11+79.691

11+71.005

14 160 486

1+91.758

10+38.692

1+63.173

6+75.603

1+86.053

8+52 627

3+69.437

6+51.028

12+02 454

0+83 833

9+06.179

0+74.677

72.375

90822 942

N 88173.878 E 90091 262

N 88012.596 E 90028.788

N 87803.028 E 89512.868 N 87809.828 E 90715.839

E 91323 705 N 88150 347

E 90861.009 N 88194.791

E 90519,708

N 57268 847 E 90801 597 N 87777.674

5 90713.970

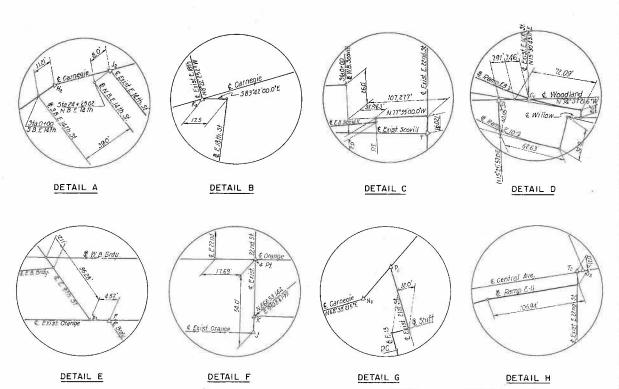
PER ROAD BIV. NO.	STATE	PROJECT NO.	TYPE FUND:
	01110		

9 175

CUYAHOGA COUNTY CITY OF CLEVELAND CUY - 42-18.29

CURVE DATA TABULATION

DESCRIP-	CURVE	BEGINNIN	IG OF CURVE	P.I. OF CURVE		END OF CURVE						
TION		STATION	COORD.	STATION	COORD.	STATION	COORD	Δ	D	R	т	L
Relocated	"A"	0+00.000	N86680.881 E 92416.604	1+84.390	N86729.676 E 92238.787	3+57.895	N86670.695 E92064.084	3400 00.0"	9°30'	603,//3	184.390	357.895
Woodland	<i>"8"</i>	5+65.5/6	N.86637,444 E.91856,175	7+49.870	N 86578.475 E 91681.507	9+23.344	N.86627.741 E.91503,720	33°59'37.3"	9°30′	603.1/3	184.354	357.828
RAMP	"A"	0+00,000	N.86606.956 E.91928.320	1+83.898	N.86558.291 E.92105.662	3+66.667	N.86476,682 E92270,460	11°00'00.0"	3°00'	1909.859	183.898	366,667
N-30	<i>"B"</i>	9+41.743	N.86235815 E 92792908	9+96.944	N.86211.318 E.92842.376	10 + 51.806	N.86196.716 E.92895.611	11.00, 22.74	10°00'	572.958	55.201	110.063
30-N	"Д"	7+ 95.695	N. 6606.207 E. 92328.691	9+24.252	N.86623.307 E.92201.276	10 + 52.423	N.86657.327 E.92077.302	7°42' 06.6"	3,00	1909.859	128.557	256.728



Note + = P.C.C + = Center of Curve

·8"

"B"

"A"

"R"

.C.

"C"

"A"

'A "

"B"

"A "

"A"

"B"

"B"

"A"

"A"

RAMP E 15

RAMP E 16

RAMP E 17

RAMP E 18

STREETS EAST 9th ST.

N. B. E. 14th 57 "A"

5. B. E. 14 th 57.

18th ST. Turnout

W. B. SCOVILL

E.B. SCOVILL

ROAD

ACCESS

E. B. BROADWAY

BROADWAY

2+97.969 t

2+03.697

7+04.496

0+00.000

4+98.379

6+73.539·t

0+00.000

6+21.139 t

13:98.352

0+00 000

5+79.017

10+38,156

10+93 130

12+94.497

0+89.740

9+08.700

0+00.000

0+40.671

5+09.058

0+00.000

7+71.761

2+71.000

4+50.336 t

9+68.761

0+00 000

8+02.580

0 + 20.760

INTERSECTION DETAILS No Scale

SCALE No. SCALE CONSULTING ENGINEERS TRCD. DATE CKD GMG DATE 7-8-58 KANSAS GITY CLEVELAND NEW YORK 914 SHEET 9

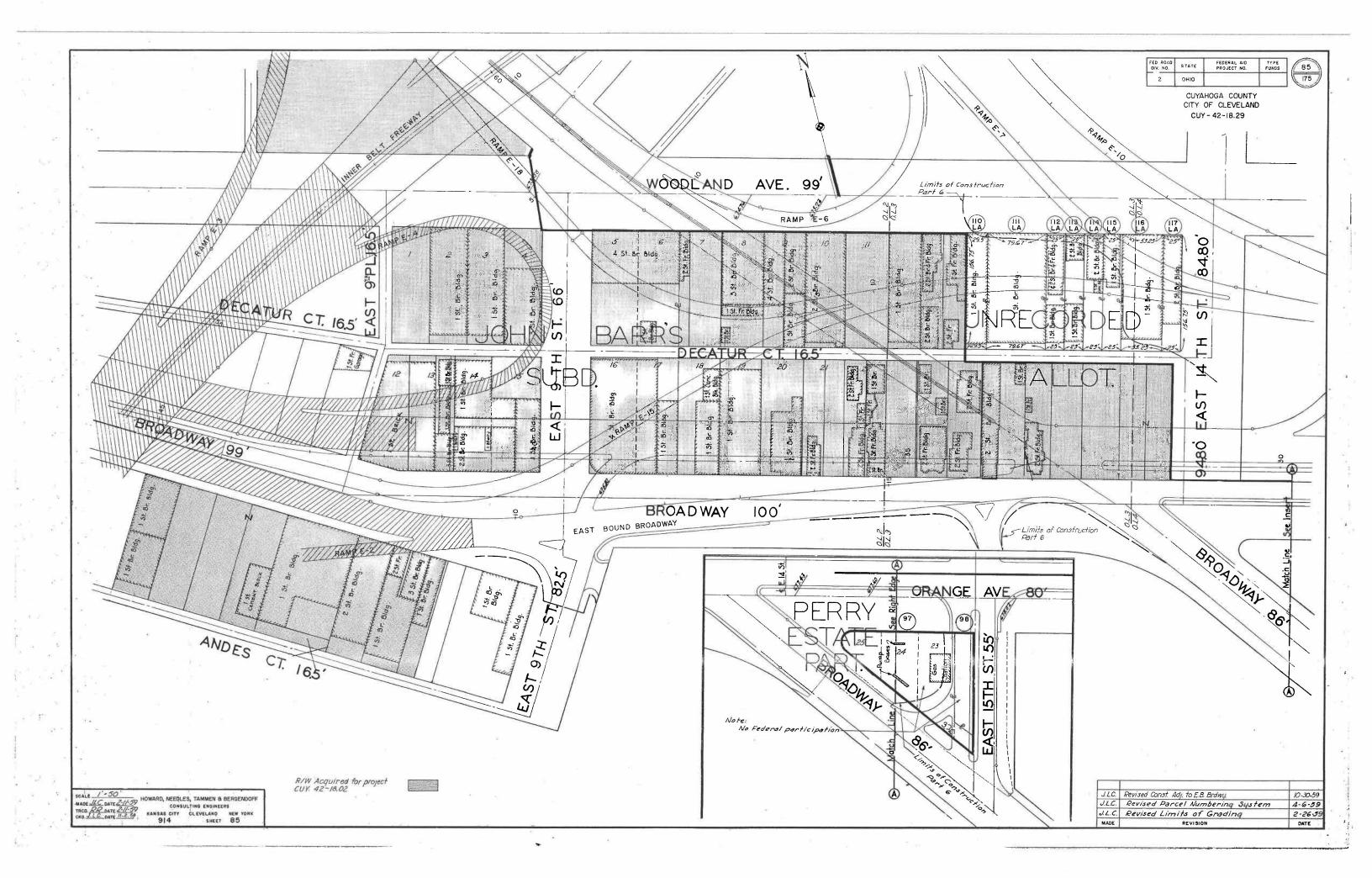
CHEET	PARCEL	RIGHT OF WAY SU			
NO.	NO.	OWNER	AREA	BLDGS	REMARKS
85	97	STANDARD OIL CO. NEW YORK CENTRAL RAILROAD	ALL	YES NO	100% CITY FUNDS
	lista a				
35	110LA	JOS. J., STEPHEN, & I. W. SANSON WOODLAND = 12TH BLDG. INC.	ALL ALL	YES	
**		NICOLA. 8 V. RINI	ALL	YES	
••		DOROTHY V. COMELLA	ALL	YES	
	114LA	NICOLA & V. RINI	ALL	YES	
	115LA 116LA	MARTIN L. RINI, ET AL. SANTINA GERACI, ET AL.	ALL	YES	
	117LA	HILLSON NUT COMPANY	ALL	YES	
88	151LA	MYRTLE KELLER	ALL	YES	
**	152LA	JOHN TILDEN	ALL	YES	
	153LA	CLEVELAND TRUST COMPANY, TR.	ALL	NO	
86	155LA	JOSEPH SHAIA	ALL	NO	
**	156LA 157LA	MICHAEL W. HELLER	ALL	YES	
2	157LA 158LA	MILK PRODUCERS FED. OF CLEVELAND DOMINIC BUZZELLI	ALL	AE2	
**	159 LA	ARTHUR & H. BERCHAK	ALL	YES	
	160LA	THE LEE H. GOULD CO.	ALL	YES	
**	161LA	OHIO IGNITION COMPANY	ALL	YES	
	163LA	J.B. PEARCE CO.	ALL	YES	
86	166LA	ANGELO MAROTTA	ALL	YES	
96	169LA	J.H. GREEN	ALL	NO	
.,	170LA	SCOVILL-NINTH CO.	ALL	YES	
- 7	171LA	EMMA WOLKOFF & ELEANOR L.SCHNAEDER		YES	
*	172LA	W. F. HYDE	ALL	YE2	
	173LA 174LA	THE PARLA CORP. HAROLD B. MC MULLEN	ALL		
"	175LA	MICHAEL COHEN, ET AL.	ALL		
"	176LA	JOHN H. BRAMLEY	ALL	ОИ	
:	178LA	A.C. BLAIR CO.	ALL	NO	
ii	179LA 180LA	MARIE MANCUSO PARKMAN COURT INC.	ALL	YES YES	
	181LA	EUGENE & A. DUBICK	ALL	YES	
	182LA	F. A. & MARY DINDIA	ALL	YES	
"	183LA	JULIUS B. GARSON, ET AL.	ALL	YES	
**	184LA	CLEVELAND GROWERS MARKETING CO.	ALL	YES	
344	185LA 186LA	NATIONAL CITY BANK OF CLEVELAND	ALL	NO	
	187LA	H.E. & H.V. BING & L.H. ENSTEN, JR. CECILIA SPANO, ET AL.	ALL	YES	
	188LA	SALVATORE ARTINO, ET AL.	ALL	NO	
	189LA	CECILIA SPANO, ET AL.	ALL	NO	
**	190LA	FRANK ANTONY & MARY DINDIA	ALL	YES	
	191LA	PAULINE KLANN	ALL	NO	
.,	192LA 193LA	MARY PEPPEREL, TR. JEAN WEISER, ET AL.	ALL ALL	YES	
	194LA	CITY OF CLEVELAND	ALL	NO	
87	195LA 196LA	CITY OF CLEVELAND	ALL	NO NO	
		SAYDE M. KAPLAN	ALL		
87	198LA 199LA	MATTEO & MARTHA RINI WM. M. BASSICHIS	ALL	NO ON	
**	200 LA	JACOB' & WM. BASSICHIS	ALL	NO	
87	202 LA	CITY OF CLEVELAND	ALL	YES	
"	203 L A	F. & B. INC.	ALL	YES	
	204LA 205LA	JOSEPH M. SWEENY (SHERIFF) ZELVY BROS.	ALL	NO YES	
87	208LA	ASHER & BESSIE HOFFMAN	ALL	YES	
87	210LA	THE M-S-M REALTY CO.	ALL	YES	
"	211LA	ZELVY BROS.	ALL	NO	
.,	212LA 213LA	JOSEPHINE ZINGALE STRATIS CALOGERAS	ALL	YES	
	213LA 214LA	LOUIE OLIVER	ALL	AE2	
87	218LA	RAY L. EHRLICH	ALL	YES	
	219LA	E. F. ROMANO & F. LITTLE	ALL	YES	
87	227LA	JAS. BARBERA	ALL	YES	
	228LA 229LA	CAESAR W. PUCCI SAYDE M. KAPLAN	ALL	YES YES	
	232LA	VIOLA F. DEERING, ET AL.	ALL	YES	
* 1	232LA 233LA	MORRIS UDELF, ET AL.	ALL .	YES	
**	234LA	ED. A. ZINGALE	ALL	YES	
	235LA	GRAZIA COSENZA	ALL	YES	
	236LA	SALVATORE SALUPO	ALL	YES	
	237 L A 238 L A	SALVATORE & C. SALUPO ANDREAS CONIGLIO	ALL	YES	
	239LA	MICHAEL GANCI, JR.	ALL	YES	
::	240 L.A	HARRY KERTZ	ALL	YES	
:		GUISEPPINA PORELLO	ALL	YES	
:	241LA	ERANK ANTHONY & M DINOLA		153	
:	242LA	FRANK ANTHONY & M. DINDIA		1000	
:	242LA 244LA	AMERICA & CONCETTA GIOVANETTI	ALL	YES	4
:	242LA	AMERICA & CONCETTA GIOVANETTI PAUL A. RINI MEDA RINI			
:::::::::::::::::::::::::::::::::::::::	242LA 244LA 245LA	AMERICA & CONCETTA GIOVANETTI PAUL A. RINI	ALL ALL	YES	wit.
: : : : : : : : : : : : : : : : : : : :	242LA 245LA 245LA 246LA 247LA 248LA	AMERICA & CONCETTA GIOVANETTI PAUL A. RINI MEDA RINI THE MITTLEMAN CO. LOUIS CULOTTA	ALL ALL ALL . ALL ALL	YES YES YES	
	242LA 244LA 245LA 246LA 247LA 248LA 249LA	AMERICA & CONCETTA GIOVANETTI PAUL A. RINI MEDA RINI THE MITTLEMAN CO. LOUIS CULOTTA ANNA J. GREGG	ALL ALL ALL ALL ALL ALL	YES YES YES YES	od Y.
: : : : : : : : : : : : : : : : : : : :	242LA 245LA 245LA 246LA 247LA 248LA	AMERICA & CONCETTA GIOVANETTI PAUL A. RINI MEDA RINI THE MITTLEMAN CO. LOUIS CULOTTA	ALL ALL ALL . ALL ALL	YES YES YES	

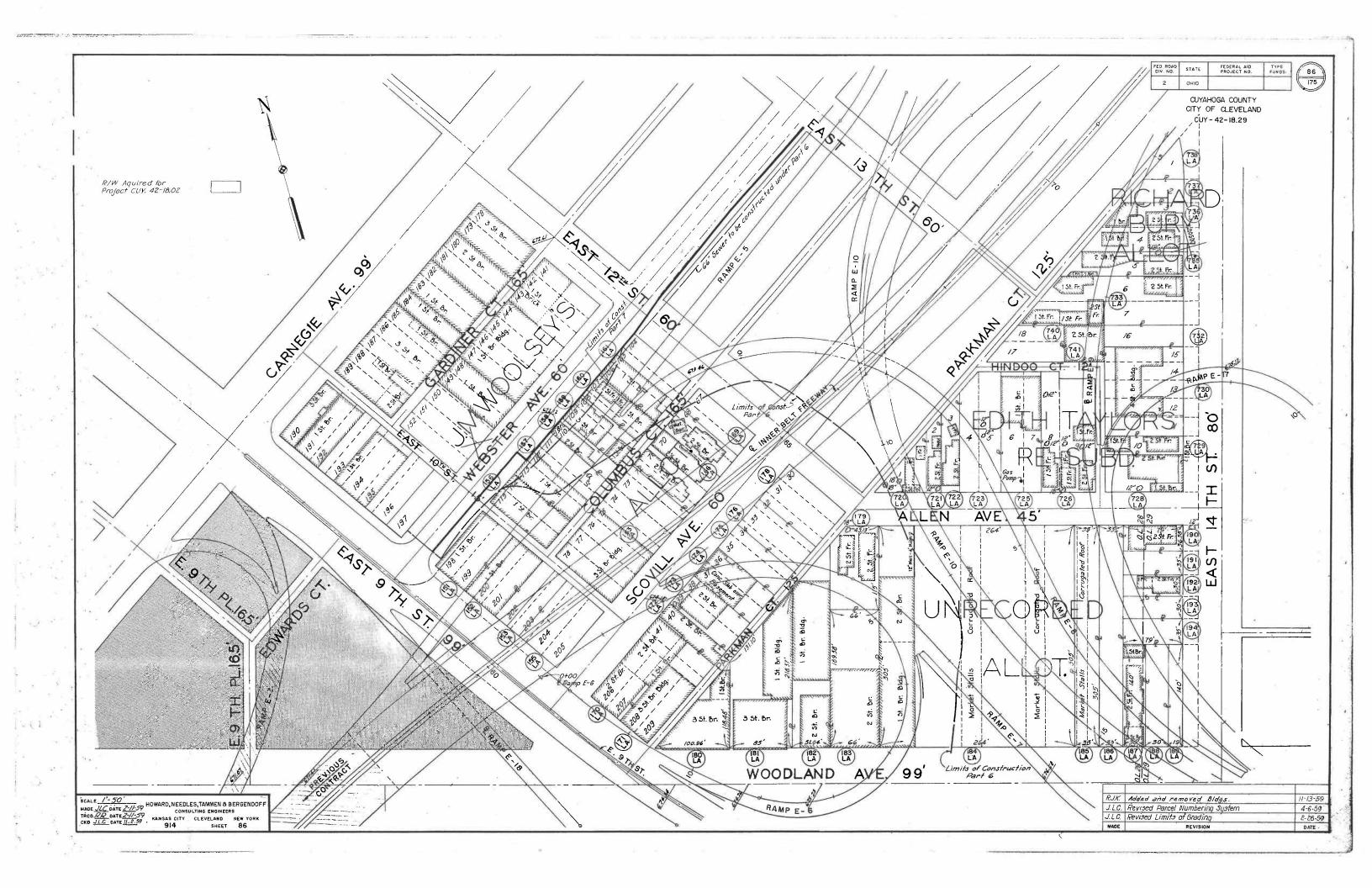
	PARCEL		JMMARY		
NO.	NO.	OWNER	AREA	BLDGS	REMARKS
87	254LA	THE MITTLEMAN CO.	ALL	YES	
44	255LA	JACOE & M. BISHKO	ALL	YES	
- 295	256 LA	R.F. CARBONE	ALL	YES	
1440	257LA	M.C. & I. N. STONE CO.	ALL	YES	
200	A 7852	MARIE J. KOCH	ALL	YES	
	259L A	LAKEWOOD CENTER CO.	ALL	NO	
	260 L A	EDITH B. COOPER JOSEPHINE LOMBARDO, ET AL.	ALL	YES	
	262LA	ELEUTERIO AND R. FLORES	ALL	YES	
***	263LA	GAETANO ZINGALES	ALL	YES	
***	254LA	MARIA CIRESI	ALL	YES	
370	265 L A	FRANCES RINI, ET AL.	ALL	YES	
	266L A	ANTHONY BLANDING	ALL	YES	
**	267LA 268LA	ANTONIO CRIMALDI ROSE RINI	ALL ALL	YES	1.0
**	269L A	MATTEO RINI DI GIOVANNI	ALL	YES	
365	271LA	ANTHONY M. LOMBARDO, ET AL.	ALL	YES	
**	272LA	MARIA MERCURIO	ALL	YES	
**	273LA	MARIA MERCURIO	ALL	YES	
	274L # 275L A	DOMENICO & ANTONIA SURACE	ALL	YES	
	276LA	G. GAGLIONE & ROSE PLATAMONE, ET AL. MICHELENA DEMMA	ALL	NO VEC	
	277LA	ANNA C. DOTTORE	ALL	YES	
300	278 L A	CLEVELAND TRUST CO.	ALL	NO	
	275 L A	CLEVELAND TRUST CO.	ALL	NO	
	280LA	JOHN & M.J. GREEN	ALL	YES	
87	281LA 286	SHIRLEE BLONDER NOT USED	ALL	YES	
Fi. [287	NOT USED			
-15	288	EC De			
	289	n n			
**	290	" "	l l		
**	291 292	" "		4	
1990	293	37 11			
	294				
	29.5	** 00			
:	296 297 LA	III III		1	
	297 L.A 298 L.A	JIMMIE RICHARDSON EDITH B. COOPER	851 ALL	YES YES	
87	300 LA 301 LA	MACK & ROSE STEVENS JAMES & J. L. HONER	ALL	YES YES	
87	307LA	J. & M. MASON	ALL	YES	
87	317	IDEAL MACARONI CO.	8977	YES	
87	319	SALVATORE A. PRECARIO	2511	YES	
87	334	GREEK CATHOLIC CONGREGATION	8800	YES	
				, 53	
88	359 L A 360	NOT USED (BILLBOARD)	ALL	NO	
	361	,, ,,		1	
	362				
::	363 364				
88	2/8/ 4	1.1000.000			
**	368L A 369L A	J. LOMBARDO M. GOTTLIEB	3342 ALL	YES	
	370 L A	LAWRENCE A. WHITMAN	ALL	NO	
	371LA	E. B. CULLEN	ALL	YES	
88	375LA	WM., E.D., & ESTHER BASSICHIS	ALL	YES	
"	376LA	F. MERCURIO & F. MINNITI	ALL	AE2	
88		D. KOSLIN	ALL	YES	
"	380L A	J. DIMETRO, ET AL.	ALL	YES	
.	381LA 382LA	S. RINI	ALL	YES	
**	382LA 383LA	J. GERACI LOUANNA CABBIL	ALL	YES YES	
		EVA ALTMAN	ALL	AE2	
	385LA	H. J. & F. D. JOHANEK	ALL	YES	2
	387LA	7 704140 57 44	ALL	NO	
88	387 L A 388 L A	R. ROMANO, ET AL. N. Z. FISHER	ALL	NO	
		THOS. & ELSIE SNEED	ALL	NO YES	
22	390 L. A	DELIA MC MYLER	ALL	YES	
:		PROSPECT RADIATOR CO.	ALL	YES	
::		OUTDOOR INVESTMENT CO.	ALL	NO	
#		CITY OF CLEVELAND NICK H. LO CONTI	ALL	NO YES	
.		E. MASTERSON	ALL	NO	
.	396LA	JEANETTE K. REDMOND, TR.	ALL	YES	
	398L A	ROSE MORELLI M. KOCH	ALL	YES NO	
"		ANTHONY SANSON, TR.	ALL	NO	
88		GUND REALTY CO. M. RINI	ALL	YES YES	
		THE STRATHMORE CLEVELAND HOMES CO.	ALL	YES	
••	406LA	WM. & O. BONDI	ALL	YES	
	407LA	J. BONDI	ALL	YES	
	407 L A 408 L A	ARTHUR KRAUSE	ALL	YES	
:	407 L A 408 L A 409 L A	ARTHUR KRAUSE WM. H. HAZLETT, ET AL.	ALL	YES NO	
** ** **	407LA 408LA 409LA 410LA 411LA	ARTHUR KRAUSE	ALL	YES	

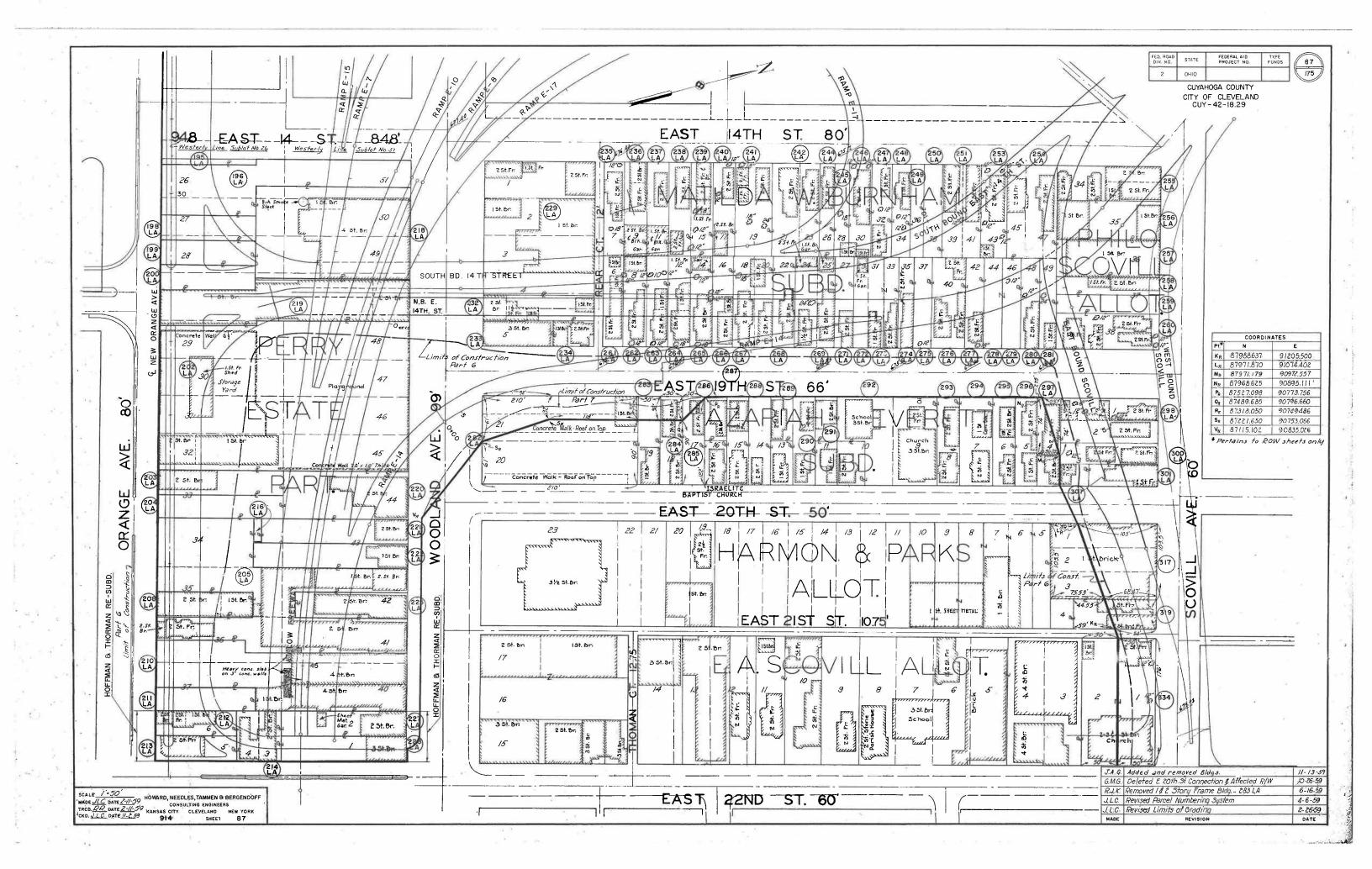
	PARCEL	Aug.		LATES HE S	
NO.	NO.	OWNER	AREA	BLDGS	REMARK
88	414LA 415LA	M. 8 M. RINI ALBERT A. LEVIN	ALL	YES NO	l
	416LA	D. L. CORSO	ALL	NO	
**	417LA	M. A. LONG	ALL	YES	1
**	418LA	A. MARRALE	ALL	NO	
- 0	419LA	H. B. MC GUIRE	ALL	NO	
44	420LA 421LA	M. C. DI CORPO 8 AUGUSTINE C. DIASIO ESTATE OF J. ANTENNA	ALL	YES	
49	422LA	C. CONTI	ALL	YES	
*	423LA	MAZEL REALTY CO.	ALL	YES	
		T. & R. LOCOCO	ALL	YES	l.
	425 L.A 426 L.A	SUN OIL CO.	ALL 1722	NO YES	
100	437 L A	MILLER BROS. REALTY		YES	
	438LA	MILLER BROS REALTY		YES	
88	452LA	MILLER BROS. REALTY		NO	
88		INDEPENDENT TOWEL CO. CHRISTOPHER C. FLIEDNER, ET AL.	ALL	NO YES	
		C. C. & BENJ. FLIEDNER	ALL	NO	
	457 LA	DAVID L. SUID	- ALL	NO	
	458LA	HELEN E. ABOID	ALL	NO	
	459LA 460LA	MIKE & L. ABRAHAM PEARL G. SUID	ALL ALL	YES	
88 & 89	461LA	LAUS BAKING CO. (FRENCH BAKERY CO.)	ALL	YES	
88 & 89	461ALA	LAUS BAKING CO. (FRENCH BAKERY CO.)	12528		
88	467LA	CENTRAL NATIONAL BANK 1/3 DOROTHY B. LEHMAN, ET AL.			
			3604	YES	
1	468LA 469LA	RUTH WRIGHT PATRINA PIAZZA, ET AL.	3303 2600	YES	
**	470	ANNA WHITE	1213	YES	
**	471	MARY SIMONELLI, ET AL.	1583	YES	
	4/2	NOT USED			
	473	NOT USED			
88		NOT USED FREDA ABRAHAM	ALL	YES	
00		HELEN & A. KACMARIK	ALL	YES YES	
	478	JAS. A. LIA	ALL	YES	
1992	479	JOE CREA	910	YES	
90	547	C. C. C. HIGHWAY CO.	ALL	YES	
90		BEN & JENNIE CASTELLANA, ET AL.	ALL	YES	
	562LA 563LA	ABE & MIKE ORCHEN WILBUR LA GANKE	ALL	YES	
-0.1	564LA	ABRAHAM, ROSE, MIKE & IDA DRCHEN	ALL	YES	
	565LA	SALVATORE SALUPO	ALL	YES	
::		GIACOMA ZITO	ALL	YES	
- 1		B & L REALTY CO.	ALL	YES	
90	570LA 572LA	U. S. TRUCK LINES, INC. CLEVELAND, COLUMBUS & CINCINNATI	ALL	YES	
~	J. 20.A	HIGHWAY CO.	ALL	YES	
90	579LA	JACOB & WM. M. BASSICHIS	ALL	YE5	
90	583LA	B & L REALTY CO.	ALL	YES	
90		SARAH MIRSKY	ALL	NO	
	586LA 587LA	JACOB & WM. M. BASSICHIS WM. M. BASSICHIS	ALL	YES NO	
		FRANK SHAPIRO, JR.	ALL	YES	
86	720LA	DONALD J. LUCIDO	ALL	YES	
"	721LA	H. IACOVONE, ET AL.	ALL	YES	
**	722LA	V. IACOVONE	ALL	YES	
	723LA	J. C. & A. MAROTTS	ALL	NO	
-	,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,,				
	725 L.A	PAULINE KLANN L. M. BUCCI, ET AL.	ALL ALL	YES YES	
86	725LA 726LA	L. M. BUCCI, ET AL.	ALL	YES	
86	725LA 726LA 728LA 729LA	L. M. BUCCI, ET AL. FRANK, ANTHONY & MARY DINDIA D. & B. REALTY CO.	ALL ALL	YES YES YES	
86 86 	725LA 726LA 728LA 729LA 730LA	L.M. BUCCI, ET AL. FRANK, ANTHONY & MARY DINDIA D. & B. REALTY CO. E. D. REALTY CO.	ALL ALL ALL	YES YES YES YES	s
86	725LA 726LA 728LA 729LA 730LA 732LA	L. M. BUCCI, ET AL. FRANK, ANTHONY & MARY DINDIA D. & B. REALTY CO.	ALL ALL	YES YES YES	ε
86 86 	725LA 726LA 728LA 729LA 730LA 732LA 733LA	L. M. BUCCI, ET AL. FRANK, ANTHONY & MARY DINDIA D. & B. REALTY CO. E. D. REALTY CO. ANTONY S. CIRESI JOW ARCARA ANTHONY SFERRA	ALL ALL ALL ALL ALL ALL	YES YES YES YES NO YES YES	
86 86	725LA 726LA 728LA 729LA 730LA 732LA 733LA 735LA 736LA	L. M. BUCCI, ET AL. FRANK, ANTHONY & MARY DINDIA D. & B. REALTY CO. E. D. REALTY CO. ANTONY S. CIRESI JOW ARCARA ANTHONY SFERRA LOPRESTI PROP. CO.	ALL ALL ALL ALL ALL ALL ALL ALL ALL	YES YES YES NO YES YES YES YES	
86 	725LA 726LA 728LA 729LA 730LA 732LA 733LA 735LA 736LA 737LA	L. M. BUCCI, ET AL. FRANK. ANTHONY 8 MARY DINDIA D. 8 B. REALTY CO. E. D. REALTY CO. ANTONY S. CIRESI JOW ARCARA ANTHONY SFERRA LOPRESTI PROP. CO. ESTATE OF MARY ZECHMAN	ALL	YES	
86	725LA 726LA 728LA 729LA 730LA 732LA 733LA 735LA 736LA 737LA 737LA 738LA	L. M. BUCCI, ET AL. FRANK, ANTHONY & MARY DINDIA D. & B. REALTY CO. E. D. REALTY CO. ANTONY S. CIRESI JOW ARCARA ANTHONY SFERRA LOPRESTI PROP. CO. ESTATE OF MARY ZECHMAN CITY OF CLEVELAND UNION PROPERTIES CO.	ALL ALL ALL ALL ALL ALL ALL ALL ALL	YES YES YES NO YES YES YES YES	
86 86 	725LA 726LA 728LA 729LA 730LA 732LA 733LA 735LA 735LA 736LA 737LA 738LA 740LA	L. M. BUCCI, ET AL. FRANK, ANTHONY & MARY DINDIA D. & B. REALTY CO. E. D. REALTY CO. ANTONY S. CIRESI JOW ARCARA ANTHONY SFERRA LOPRESTI PROP. CO. ESTATE OF MARY ZECHMAN CITY OF CLEVELANO UNION PROPERTIES CO. HASEROT COMPANY	ALL	YES YES YES NO YES YES NO YES NO NO NO NO	
86	725LA 726LA 728LA 729LA 730LA 732LA 733LA 735LA 736LA 736LA 737LA 737LA 737LA 737LA 738LA	L. M. BUCCI, ET AL. FRANK, ANTHONY & MARY DINDIA D. & S. REALTY CO. E. D. REALTY CO. ANTONY S. CIRESI JOW ARCARA ANTHONY SFERRA LOPRESTI PROP. CO. ESTATE OF MARY ZECHMAN CITY OF CLEVELAND UNION PROPERTIES CO. HASEROT COMPANY MARTIN A. RINI	ALL	YES	
86 86 86	725LA 726LA 728LA 729LA 730LA 732LA 733LA 735LA 736LA 736LA 737LA 737LA 737LA 737LA 738LA	L. M. BUCCI, ET AL. FRANK, ANTHONY & MARY DINDIA D. & S. REALTY CO. E. D. REALTY CO. ANTONY S. CIRESI JOW ARCARA ANTHONY SFERRA LOPRESTI PROP. CO. ESTATE OF MARY ZECHMAN CITY OF CLEVELAND UNION PROPERTIES CO. HASEROT COMPANY MARTIN A. RINI	ALL	YES YES YES NO YES YES NO NO NO NO YES YES YES YES YES YES YES YES	
86	725LA 726LA 726LA 729LA 730LA 733LA 733LA 733LA 737LA 738LA 740LA 741LA 801LA 803LA 803LA	L. M. BUCCI, ET AL. FRANK, ANTHONY & MARY DINDIA D. & B. REALTY CO. E. D. REALTY CO. ANTONY S. CIRESI JOW ARCARA ANTHONY SFERRA LOPRESTI PROP. CO. ESTATE OF MARY ZECHMAN CITY OF CLEVELAND UNION PROPERTIES CO. HASEROT COMPANY MARTIN A. RINI CONSOLID. ÇART. & STORAGE CO. SALVATORE SALUPO	ALL	YES	
86	725LA 726LA 728LA 729LA 730LA 733LA 733LA 733LA 737LA 738LA 740LA 741LA 801LA 802LA 803LA 805LA 805LA	L. M. BUCCI, ET AL. FRANK, ANTHONY & MARY DINDIA D. & B. REALTY CO. E. D. REALTY CO. ANTONY S. CIRESI JOW ARCARA ANTHONY SFERRA LOPRESTI PROP. CO. ESTATE OF MARY ZECHMAN CITY OF CLEVELAND UNION PROPERTIES CO. HASEROT COMPANY MARTIN A. RINI CONSOLID. ÇART. & STORAGE CO. SALVATORE SALUPO SALVATORE SALUPO SALVATORE SALUPO SALVATORE SALUPO SALVATORE SALUPO WENHAM, INC.	ALL	YES	5
86	725LA 726LA 728LA 729LA 730LA 730LA 733LA 733LA 733LA 735LA 737LA 737LA 737LA 747LA 801LA 802LA 803LA 804LA 805LA	L. M. BUCCI, ET AL. FRANK, ANTHONY & MARY DINDIA D. & B. REALTY CO. E. D. REALTY CO. ANTONY S. CIRESI JOW ARCARA ANTHONY SFERRA LOPRESTI PROP. CO. ESTATE OF MARY ZECHMAN CITY OF CLEVELAND UNION PROPERTIES CO. HASEROT COMPANY MARTIN A. RINI CONSOLID. ÇART. & STORAGE CO. SALVATORE SALUPO	ALL	YES	
86 86 86 90	725LA 726LA 729LA 729LA 730LA 733LA 733LA 735LA 735LA 735LA 731LA 740LA 740LA 801LA 802LA 804LA 804LA 805LA 818LA 819LA	L. M. BUCCI, ET AL. FRANK, ANTHONY & MARY DINDIA D. & B. REALTY CO. E. D. REALTY CO. ANTONY S. CIRESI JOW ARCARA ANTHONY SFERRA LOPRESTI PROP. CO. ESTATE OF MARY ZECHMAN CITY OF CLEVELANO UNION PROPERTIES CO. HASEROT COMPANY MARTIN A. RINI CONSOLID. ÇART. & STORAGE CO. SAM C. & J. C. LOMBARDO WENHAM, INC. BARNET J. AXELROD	ALL	YES YES NO YES NO YES NO YES YES NO YES	

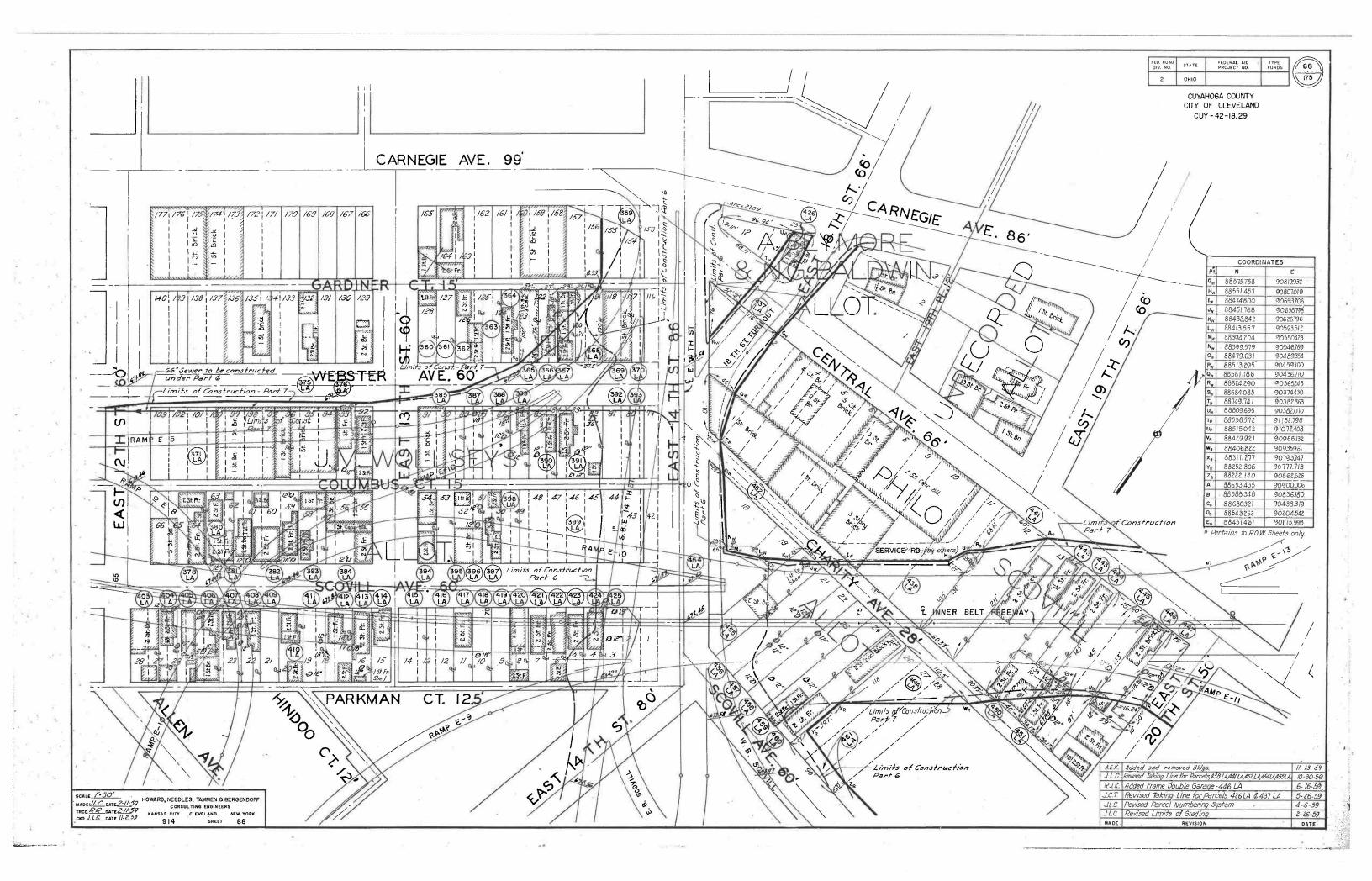
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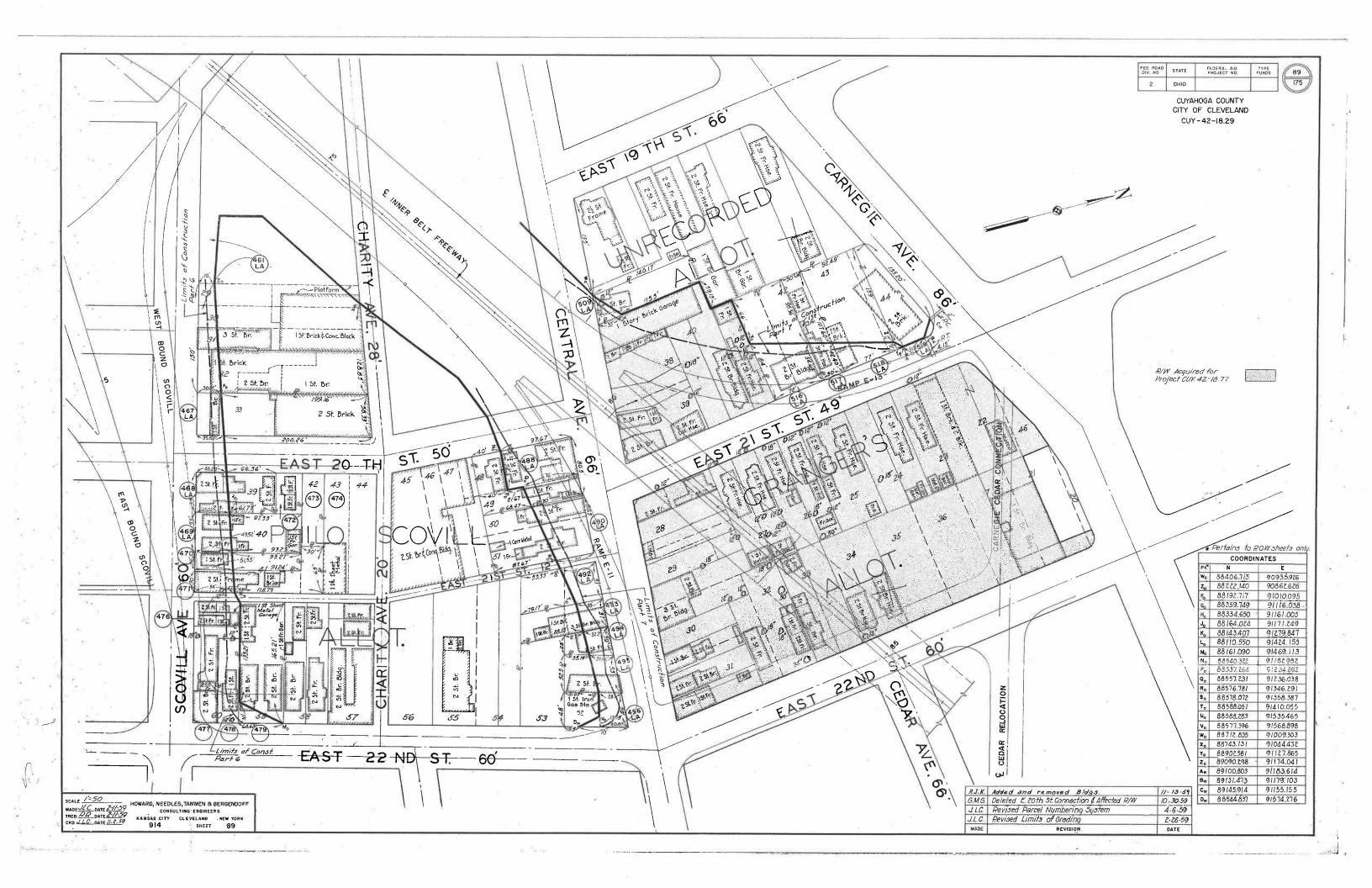
CUYAHOGA COUNTY CITY OF CLEVELAND CUY-42-18.29 RIGHT OF WAY SUMMARY

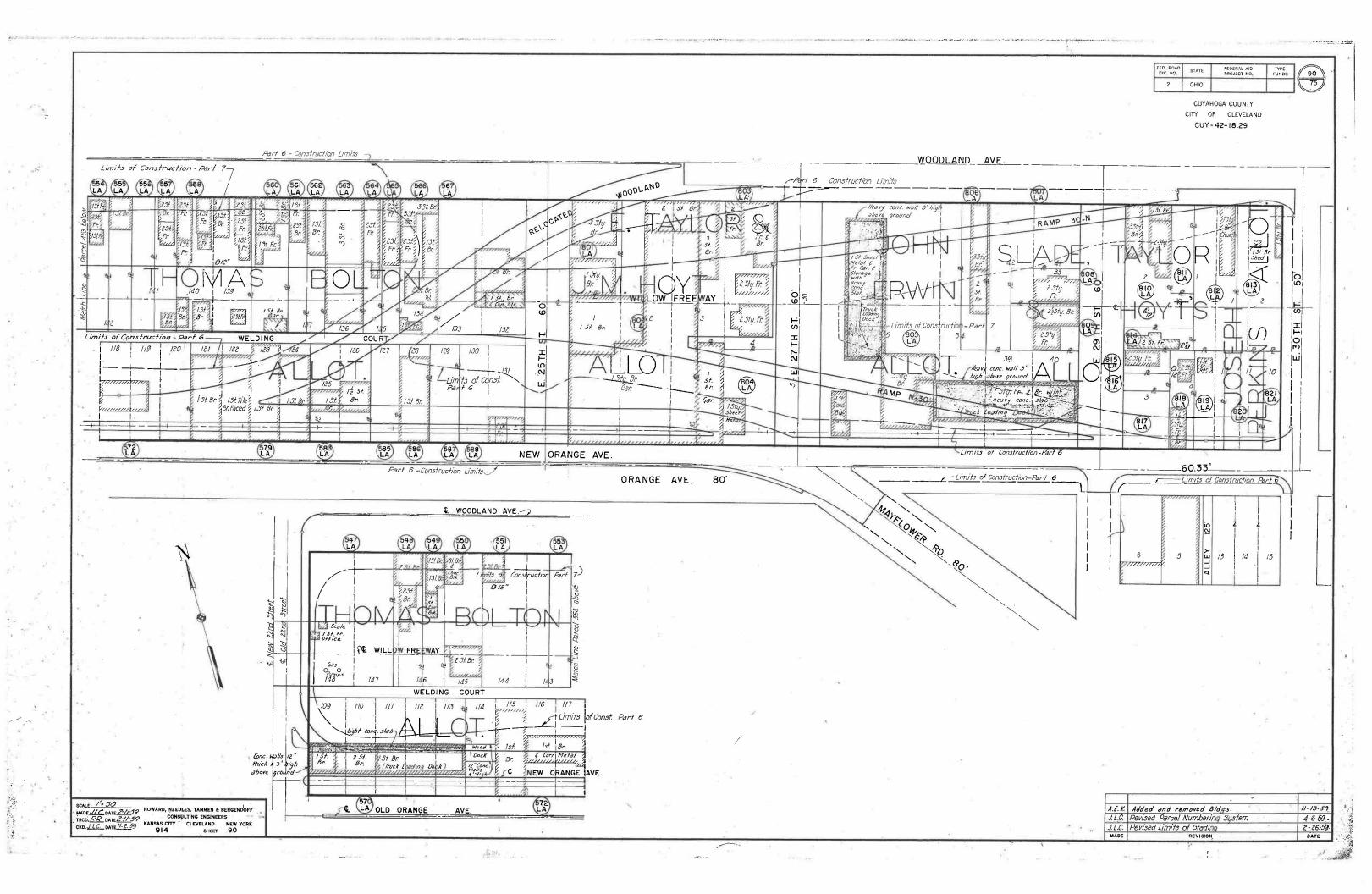












SUPPLEMENTAL SPECIFICATIONS

REFERENCE SHALL BE MADE TO SUPPLEMENTAL SPECIFICATIONS NO. S-207, HIGH STRENGTH STEEL BOLTS, DATED APRIL 28, 1955, NO. M-206.14 ASBESTOS CEMENT CONDUIT DATED JULY 15, 1949. AND NO. SS-18, FENCE AND SATES, DATED JUNE 15, 1959, AND S-197 DATED 12-2-54

REFERENCE DRAWINGS

REFERENCE SHALL BE MADE TO STANDARD DRAWING NUMBERS RB-1-55 REVISED 2/2/59, AR-1-57 REVISED 2/2/59, AND TO AS-1-54 REVISED 12/1/54.

DIMENSIONS

DIMENSIONS GIVEN ARE MEASURED HORIZONTALLY AND AT 60°F.

UTILITIES

ANY EXISTING UTILITY FACILITIES ENCOUNTERED AT THE SITE OF THE WORK WHICH WILL INTERFERE WITH PORTIONS OF THE FINISHED ROADWAYS OR STRUCTURES WILL BE REMOVED OR RELOCATED BY OTHERS, UNLESS OTHERWISE SHOWN. THE CONTRACTOR SHALL COORDINATE HIS OPERATIONS WITH THE WORK OF THE UTILITY OWNERS OR OTHERS WHO MAY BE MAKING THE RELOCATIONS, AND SHALL NOTIFY THE OWNERS OF THE UTILITIES OF HIS SCHEDULE SUFFICIENTLY IN ADVANCE TO PERMIT THEM TO MAKE THE NECESSARY ALTERATIONS.

EXISTING SEWERS WILL BE RELOCATED OR REMOVED BY THE CONTRACTOR AS SHOWN ON THE ROADWAY PLANS.

EXCAVATION

AT THE PEDESTALS OF BRIDGE NO. 6 AND THE PIERS OF ALL OTHER BRIDGES THE EXCAVATION QUANTITY FOR PAYMENT WILL BE COMPUTED FROM THE COMPLETED CONSTRUCTION CROSS SECTIONS AND GRADE LINES OF THE LOWER ROADWAY.

FOR THE ABUTMENTS THE EXCAVATION QUANTITY FOR PAYMENT WILL BE COMPUTED FROM THE COMPLETED CROSS SECTION OF THE LOWER ROADWAY AND/OR THE SURFACE OF THE PROPOSED EMBANKMENT.

THE EMBANKMENT SHALL BE PLACED AND COMPACTED UP TO THE FINISHED SPILL-THRU SLOPE AND TO THE LEVEL OF THE SUBGRADE, AFTER WHICH THE EXCAVATION SHALL BE MADE. BACKFILL BEHIND THE ABUT-MENTS SHALL BE MADE WITH MATERIAL MEETING THE REQUIREMENTS OF SEC. 1-22 AND SHALL BE COMPACTED IN ACCORDANCE WITH THE REQUIRE-MENTS FOR EMBANKMENT COMPACTION. THE PAYMENT FOR THIS 1-22 BACKFILL SHALL BE CONSIDERED AS INCLUDED IN THE PAYMENT FOR E-2. UNCLASSIFIED EXCAVATION.

BORINGS

BORING INFORMATION, LOGS AND SAMPLES OF MATERIALS EN-COUNTERED MAY BE EXAMINED AT THE DIVISION OFFICE IN GARFIELD HEIGHTS, OHIO AND AT THE BRIDGE BUREAU OFFICE IN COLUMBUS, OHIO, BUT THE STATE DOES NOT GUARANTEE THESE BORINGS TO PRESENT A COMPLETE PICTURE OF SUBSURFACE CONDITIONS TO BE ENCOUNTERED. FOUNDATION DESIGN AND FOUNDATION QUANTITIES ARE BASED ON A STUDY

PILES SHALL BE DRIVEN TO A MINIMUM BEARING CAPACITY OF 35 TONS FOR THE ABUTMENTS AND 45 TONS FOR THE PIERS.

CONCRETE

a. THE CONCRETE ROADWAY SLABS SHALL BE SO CONSTRUCTED THAT, AFTER COMPLETION AND AFTER REMOVAL OF FORMS AND ANY FALSEWORK, AND AFTER THE STEEL WORK HAS DEFLECTED UNDER THE CONFORM AS NEARLY AS PRACTICABLE TO THE ELEVATIONS AND CONTOUR LINES SHOWN ON THE PLANS.

b. THE TOTAL DEPTH OF THE BRIDGE SLAB AND HAUNCH OVER EACH BEAM(TOP OF CONCRETE TO TOP OF FLANGE) OR GIRDER (TOP OF CONCRETE TO TOP OF WEB) AT THE SUPPORTS IS GIVEN ON THE PLANS. THE STEEL BEAMS AND GIRDERS SHALL BE FABRICATED WITH THE CAMBER. AS SPECIFIED ON THE PLANS, TO COMPENSATE FOR THE DEFLECTIONS DUE TO WEIGHT OF

THE THEORETICAL DEFLECTIONS ARE TABULATED ON THE PLANS. TO COMPENSATE FOR DEFLECTIONS DUE TO THE DEAD LOAD OF THE CONCRETE THE SCREEDS USED TO STRIKE OFF THE SURFACE OF THE CON-CRETE SLAB SHALL BE SET UP WITH CAMBER ABOVE THE FINAL DESIRED GRADE LINE BY AMOUNTS EQUAL TO DEFLECTIONS SHOWN FOR THIS DEAD LOAD. SCREEDS MAY REQUIRE FURTHER ADJUSTMENT DUE TO IRREGULAR. ITIES IN THE FABRICATED STEEL. THE CONCRETE SLAB SHALL BE OF UNIFORM THICKNESS BETWEEN BEAMS WITH CAMBER SETTING OBTAINED BY VARYING THE THICKNESS OF THE HAUNCHES OVER THE BEAMS.

REINFORCING STEEL

BARS SHALL, UNLESS OTHER WISE SHOWN, BE 3 INCHES CLEAR FROM THE FACE OF CONGRETE IN FOOTINGS AND 2 INCHES CLEAR ELSEWHERE. EXCEPT IN SLABS WHERE BARS SHALL BE I INCH CLEAR ON THE BOTTOM AND I INCH PLUS THE THICKNESS OF THE MONOLITHIC WEARING SURFACE ON THE TOP.

ALL BARS ARE DESIGNATED ON THE PLANS BY BAR NUMBERS. THE BAR SIZE IS INDICATED BY THE FIRST DIGIT OF THREE-DIGIT NUMBERS AND BY THE FIRST TWO DIGITS OF FOUR-DIGIT NUMBERS.

IF REINFORGING BARS ARE FABRICATED FROM STOCK WHICH HAS PREVIOUSLY BEEN TESTED AND APPROVED BY THE OHIO HIGHWAY TESTING LABORATORY, TEST SAMPLES AS PROVIDED IN SEC. S-4.02 NEED NOT BE FURNISHED. AND REPLACEMENT BARS WILL NOT BE REQUIRED.

WATERPROOFING

ALL CONTRACTION AND EXPANSION JOINTS IN BACK FACE OF SUB-STRUCTURE AGAINST WHICH EARTH IS TO BE PLACED SHALL BE WATERPROOFED WITH A PRE-MOLDED SEALING STRIP AS SHOWN IN THE PLANS.

WELDING

ALL WELDING SHALL BE CLASS "A" EXCEPT AS NOTED IN THE TAIL OF THE WELDING SYMBOL.

RADIOGRAPHIC EXAMINATION OF WELDS

THIS WORK SHALL CONSIST OF THE PERFORMANCE AND INTERPRE-TATION OF A RADIOGRAPHIC EXAMINATION OF BUTT WELDS AS REQUIRED BY THESE SPECIFICATIONS, IT SHALL INCLUDE THE PREPARATION AND POSITIONING OF WELDS FOR EXAMINATION, THE RADIOGRAPHING OF WELDS, THE PROCESSING AND EXAMINATION OF RADIOGRAPHS, THE INTERPRETATION OF RADIOGRAPHS FOR COMPLIANCE WITH THESE SPECIFICATIONS. AND THE PERFORMANCE AND INTERPRETATION OF ANY RETAKES OF RADIO-GRAPHS REQUIRED FOR WELDS MADE TO REPLACE UNSATISFACTORY WELDS.

a. APPROVAL OF DIRECTOR

THE CONTRACTOR SHALL FURNISH EVIDENCE, ACCEPTABLE TO THE DIRECTOR, OF THE ADEQUACY OF THE EQUIPMENT TO BE USED AND THE COMPETENCE OF THE PERSONNEL.

THE INTERPRETATION OF RADIOGRAPHS AND THE CORRECTION OF DEFECTIVE WELDS SHALL BE SUBJECT TO THE APPROVAL OF THE DIRECTOR.

b. SCOPE OF EXAMINATION

BY MEANS OF RADIOGRAPHIC EXAMINATION. THE CONTRACTOR SHALL. FURNISH EVIDENCE OF THE ACCEPTABLE QUALITY OF THE BUTT WELDS
OF ALL GIRDERS. THE PARTS OF THESE MEMBERS TO BE RADIOGRAPHED ARE AS FOLLOWS

(1) THE COMPLETE BUTT WELDS IN THE FLANGES OF EACH GIRDER EXCEPT THE BOTTOM FLANGE OVER THE BEARING DEVICES

(2) ONE FOOT AT EACH END OF EACH OF THE WEB SPLICE WELDS.

THE SHOP EXAMINATION OF THE BUTT WELDS OF THE FLANGE PLATES AND OF THE WEB PLATES SHALL BE DETERMINED TO BE ACCEPTABLE BEFORE THESE FLANGE AND WEB PLATES ARE ASSEMBLED AND WELDED TO FORM THE GIRDERS. THE EXAMINATION OF FIELD WELDS SHALL BE MADE AS SOON AS PRACTICABLE AFTER WELDING AT EACH FIELD SPLICE IS COMPLETED.

RADIOGRAPHIC INSPECTION OF WELDS OF ROLLED BEAMS WILL NOT BE REQUIRED.

c. WELD CONDITION

ALL WELDED JOINTS WHICH ARE TO BE RADIOGRAPHED SHALL BE FREE OF PAINT, SCALE AND GREASE AND SHALL BE FOUND FREE OF ALL
WELD RIPPLES AND SURFACE IRREGULARITIES ON BOTH SIDES. THE DIRECTION
OF GRINDING SHALL BE PERPENDICULAR TO THE LENGTH OF THE WELD. THE WELDS SHALL BE GROUND TO SUCH A DEGREE THAT THE RESULTING RADIOGRAPHIC CONTRAST, DUE TO REMAINING IRREGULARITIES, CANNOT MASK OR BE CONFUSED WITH THAT OF ANY OBJECTIONABLE DEFECT AND THAT THE WELD SURFACE WILL MERGE SMOOTHLY INTO THE PLATE SURFACE. UNLESS SPECIFIED TO BE GROUND FLUSH, THE FINISHED SURFACE OF THE REINFORCEMENT MAY HAVE A CROWN EQUAL TO ONE-EIGHTH THE THICKNESS OF THE METAL BUT NOT MORE THAN ONE-EIGHTH INCH.

d. RADIOGRAPHIC TECHNIQUE

THE WELD SHALL BE RADIOGRAPHED WITH A TECHNIQUE WHICH WILL DETERMINE QUANTITATIVELY THE SIZE OF DEFECTS WITH THICKNESSES
EQUAL TO OR GREATER THAN 2 PER CENT OF THE THICKNESS OF THE BASE METAL. IN THE CASE OF A WELD JOINING PLATES OF UNEQUAL THICKNESS, BOTH PLATES MUST BE RADIOGRAPHED AT 2 PER CENT SENSITIVITY TOGET-HER OR SINGLY, WITH THE WELD JUNCTION EVIDENT IN BOTH VIEWS.

TO DETERMINE WHETHER THE RADIOGRAPHIC TECHNIQUE EMPLOYED IS DETECTING DEFECTS OF A THICKNESS EQUAL TO OR GREATER THAN 2 PER CENT OF THE THICKNESS OF THE BASE MATERIAL, THICKNESS GAGES OR PENETRAMETERS OF THE TYPE HEREINAFTER SPECIFIED SHALL BE PLACED ON THE SIDE OF THE WELDED PLATE NEAREST THE SOURCE OF RADIATION AT AN EXTREME EDGE OF THE RADIOGRAPHIC PLATE OR FILM.

THE MATERIAL OF THE PENETRAMETER SHALL BE SUBSTANTIALLY THE SAME AS THAT OF THE WELDED PLATE.

THE THICKNESS OF THE PENETRAMETER SHALL BE NOT MORE THAN 2 PER CENT OF THE THICKNESS OF THE PLATE EXCLUSIVE OF ANY WELD REINFORCEMENT. PENETRAMETERS DESIGNED FOR INCREMENTS OF $1/8^{\circ}$ OF PLATE THICKNESS ARE ACCEPTABLE.

IN EACH PENETRAMETER THERE SHALL BE THREE HOLES WITH DIAMETERS EQUAL RESPECTIVELY TO TWO, THREE, AND FOUR TIMES THE PENETRAMETER THICKNESS, BUT IN NO CASE SHALL LESS THAN $1/16^{\rm th}$ DIAMETER BE USED.

EACH PENETRAMETER SHALL CARRY AN IDENTIFYING NUMBER REPRE-SENTING IN TWO SIGNIFICANT FIGURES THE MINIMUM THICKNESS IN INCHES OF THE PLATE FOR WHICH IT MAY BE USED. PENETRAMETERS MAY BE ESTABLISHED FOR DIFFERENCES IN THICKNESS NOT TO EXCEED 1/8" SO THAT A SET OF PENETRAMETERS VARYING FOR INCREMENTS OF PLATE THICKNESS WILL BE ADEQUATE TO SERVE PLATES HAVING THICKNESSES BETWEEN THESE 1/8" DIMENSIONS.

THE IMAGES OF IDENTIFYING NUMBERS AND THE HOLES OF EACH PENETRAMETER MUST APPEAR CLEARLY ON THE RADIOGRAPH TO ESTABLISH THE 2 PER CENT SENSITIVITY.

FOR PLATES UP TO AND INCLUDING 2-1/2" IN THICKNESS, EACH PENETRAMETER SHALL BE 1-1/2" LONG AND 1/2" WIDE. FOR PLATES THICKER THAN 2-1/2", EACH PENETRAMETER SHALL BE 2-1/4" LONG AND 1" WIDE.

THE FILM DURING EXPOSURE SHALL BE AS CLOSE TO THE WELD AS PRACTICABLE. IF POSSIBLE. THIS DISTANCE SHALL BE NOT GREATER THAN I INCH. IN ANY EVENT, THE RATIO

> DISTANCE FROM SOURCE OF RADIATION TO WELD SURFACE TOWARD RADIATION
> DISTANCE FROM WELD SURFACE TOWARD RADIATION TO FILM

SHALL BE AT LEAST 7 TO 1.

ALL RADIOGRAPHS SHALL BE FREE FROM EXCESSIVE MECHANICAL PROCESSING DEFECTS WHICH WOULD INTERFERE WITH PROPER INTERPRE-TATION OF THE RADIOGRAPH

IDENTIFICATION MARKERS, THE IMAGES OF WHICH WILL APPEAR ON THE FILM, SHALL BE PLACED ADJACENT TO THE WELD, AND THEIR LOCATIONS SHALL BE ACCURATELY AND PERMANENTLY MARKED ON THE OUTSIDE SURFACE NEAR THE WELD SO THAT A DEFECT APPEARING ON THE RADIOGRAPH MAY BE ACCURATELY LOCATED.

THE SIZE OF FILM TO BE USED SHALL BE 4" WIDE x 15" LONG UNLESS PERMISSION TO USE A DIFFERENT SIZE IS OBTAINED IN WRITING FROM THE

e. STANDARDS OF ACCEPTABILITY

THE ACCEPTABILITY OF THE WELDS EXAMINED BY RADIOGRAPHY SHALL BE JUDGED BY THE FOLLOWING STANDARDS.

(I) CRACKS

DEFINITION - A DISCONTINUITY RESULTING FROM A VERY NARROW SEPARATION OF METAL.

- NO WELD CONTAINING CRACKS, REGARDLESS OF STANDARD LENGTH, SIZE OR LOCATION, SHALL BE CONSIDERED ACCEPTABLE.

(2) GAS POROSITY

DEFINITION - GAS POCKETS OR VOIDS IN METAL.

 THE MAXIMUM DIMENSION OF ANY INDIVIDUAL GAS POCKET SHALL NOT EXCEED 1/8 INCH. THE MAXIMUM ACCUMULATION OF GAS POCKETS SHALL NOT EXCEED THAT SHOWN IN THE "POROSITY STANDARDS" OF THE AMERICAN SOCIETY OF MECHANICAL ENGINEERS.

(3) SLAG INCLUSIONS

- NONMETALLIC, SOLID MATERIAL ENTRAPPED IN WELD METAL OR BETWEEN WELD METAL AND BASE

STANDARD-A - ELONGATED SLAG INCLUSIONS: NO ELONGATED SLAG INCLUSION SHALL EXCEED TWO-THIRDS OF THE THICKNESS OF THE THINNER PLATE OF THE JOINT IN LENGTH AND 1/16" IN WIDTH, EXCEPT THAT RE-GARDLESS OF THE PLATE THICKNESS, NO SUCH IN-CLUSION SHALL BE LONGER THAN 3/4" AND EXCEPT THAT NO SUCH INCLUSION WHICH IS SHORTER THAN 1/4" SHALL BE CAUSE FOR REJECTION.

> B - ISOLATED SLAG INCLUSIONS: IN ANY 12 INCH LENGTH OF WELD, THE MAXIMUM WIDTH OF ANY ISOLATED SLAG INCLUSION SHALL NOT EXCEED 1/8 INCH, THE SUMMATION OF LENGTHS OF ISOLATED SLAG IN-CLUSIONS SHALL NOT EXCEED 1 INCH, AND THERE SHALL BE NO MORE THAN FOUR ISOLATED SLAG INCLUSIONS OF THE MAXIMUM WIDTH OF 1/8 INCH. ANY TWO SUCH INCLUSIONS SHALL BE SEPARATED BY AT LEAST 2 INCHES OF SOUND WELD METAL.

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(4) - INCOMPLETE FUSION

FAILURE OF THE WELD METAL TO FUSE COMPLETELY DEFINITION WITH THE BASE METAL OR PRECEDING BEADS.

NO INDIVIDUAL LACK OF FUSION SHALL EXCEED STANDARD 1/2 INCH IN LENGTH. IN ANY 12 INCH LENGTH OF WELD, THE SUMMATION OF LENGTHS OF LACK OF FUSION SHALL NOT EXCEED 3/4 INCH AND INDIVIDUAL DEFECTS SHALL BE SEPARATED BY AT LEAST 6

(5) INCOMPLETE PENETRATION:

DEFINITION - ROOT PENETRATION WHICH IS LESS THAN COMPLETE OR FAILURE OF A ROOT PASS AND A BACKING PASS TO FUSE WITH EACH OTHER

INCHES OF SOUND METAL.

NO INDIVIDUAL LACK OF PENETRATION SHALL EX-CEED 1/2 INCH IN LENGTH. IN ANY 12 INCH LENGTH OF OF WELD, THE SUMMATION OF LENGTHS OF LACK

OF PENETRATION SHALL NOT EXCEED 3/4 INCH AND INDIVIDUAL DEFECTS SHALL BE SEPARATED BY AT LEAST 6 INCHES OF SOUND METAL.

f. REPAIR OF DEFECTIVE WELDS

DEFECTIVE WELDS SHALL BE REPAIRED BY CHIPPING OR MELTING OUT SUCH DEFECTS FROM ONE OR BOTH SIDES OF THE JOINT AS REQUIRED, RE-MOVING ONLY SUFFICIENT WELD METAL TO CORRECT THE DEFECT. THE JOINT SHALL THEN BE REWELDED AND AGAIN RADIOGRAPHED.

g. ADDIT IONAL RADIOGRAPHS

WHEREVER AN UNACCEPTABLE WELD OCCURS. A RADIOGRAPH SHALL BE MADE OF THE ADJOINING 12-INCH LENGTHS OF WELD TO DETERMINE IF THE FLAWS EXTEND BEYOND THE LIMITS OF THE ORIGINAL RADIOGRAPH. IF UNACCEPTABLE FLAWS OCCUR IN THESE ADJOINING LENGTHS OF WELD, THESE DEFECTIVE WELDS SHALL BE REPAIRED, AND THIS ENTIRE PROCEDURE RE-PEATED FOR THE NEXT ADJOINING 12-INCH LENGTH OF WELD.

h. CUSTODY OF RADIOGRAPHS

AS SOON AS THE RADIOGRAPHING OF THE WELDMENTS ON THE FULL LENGTH OF EACH FLANGE OR WEB PLATE BETWEEN FIELD SPLICES HAS BEEN COMPLETED, THE CONTRACTOR SHALL SEND TO THE STATE THE PROCESSED CONTACT FILM (THAT FILM CLOSEST TO THE SOURCE OF RADIATION) OF ALL ORIGINAL AND RETAKE RADIOGRAPHS. THESE RADIO-GRAPHS SHALL BE ACCOMPANIED BY A CERTIFICATION FROM THE CONTRACTOR THAT THE RADIOGRAPHIC EXAMINATION WAS PERFORMED IN CONFORMANCE WITH THESE SPECIFICATIONS. THE RADIOGRAPHS SHALL BECOME THE PROPERTY OF THE STATE. EACH RADIOGRAPH SHALL BE CLEARLY IDENTIFIED TO SHOW THE LOCATION ON THE STRUCTURE AT WHICH IT WAS TAKEN. UNACCEPTABLE DEFECTS SHALL BE IDENTIFIED IN EACH RADIOGRAPH IN WHICH THEY OCCUR, AND THE REPAIR OR REPLACEMENT OF EACH UNACCEPTABLE WELD DEFECT SHALL BE NOTED AND IDENTIFIED.

i. REPORT OF COST

AFTER THE COMPLETION OF THE RADIOGRAPHIC INSPECTION OF WELDS, THE CONTRACTOR SHALL FURNISH THE STATE A COMPLETE RE-PORT OF THE COST OF PERFORMING THIS WORK. SEPARATED INTO THE ITEMS MENTIONED IN THE FOLLOWING PARAGRAPH

j. BASIS OF PAYMENT

PAYMENT FOR THIS WORK, INCLUDING ALL LABOR, EQUIPMENT, MATERIALS, AND INCIDENTALS, SHALL BE INCLUDED IN THE UNIT PRICE BID FOR S-7. "STRUCTURAL STEEL."

PART 6

OHIO

HOWARD: NEEDLES, TAMMEN & BERGENDOFF CONSULTING ENGINEERS
KANSAS CITY CLEVELAND

GENERAL NOTES

WILLOW-INNER BELT FREEWAY

CLEVELAND CHYAHOGA COUNTY

DATE 11-10-59 DATE 11-10-09 DATE 10-4-59 DATE