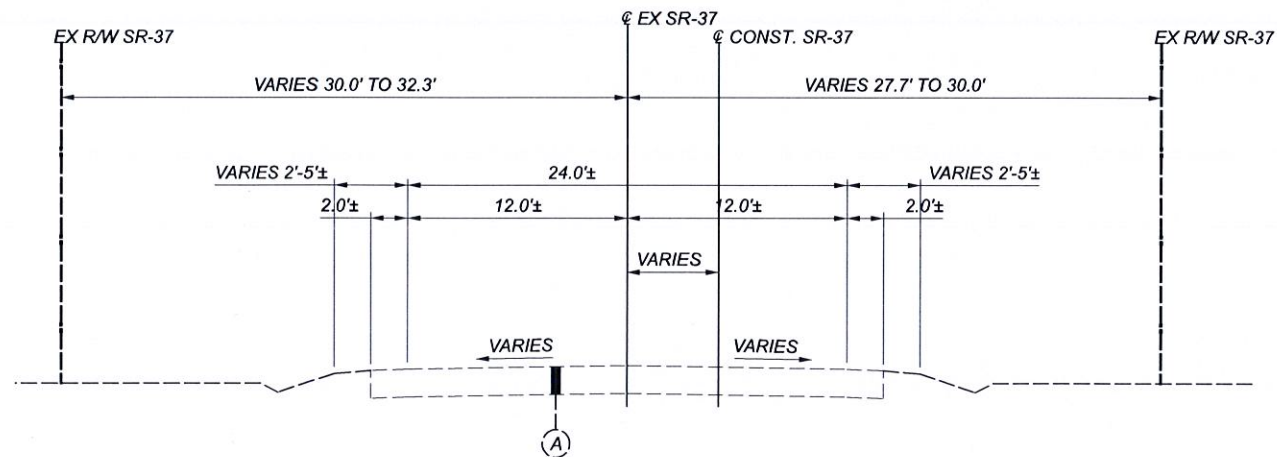


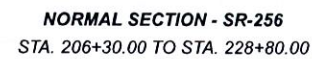
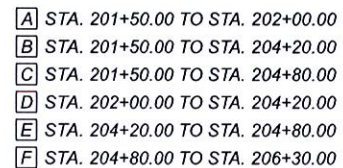
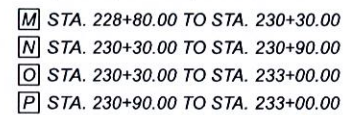
EXISTING SECTION - SR-256
 STA. 201+50.00 TO STA. 233+00.00



EXISTING SECTION - SR-37
 STA. 100+35.00 TO STA. 132+22.00

EXISTING LEGEND

(A) 12"-23"± ASPHALT CONCRETE



(A) 12"-23"± ASPHALT CONCRETE

① ITEM 441 - 1.5" ASPHALT CONCRETE SURFACE COURSE, TYPE 1 (448), PG70-22M	⑨ ITEM 204 - SUBGRADE COMPACTION
② ITEM 407 - NON-TRACKING TACK COAT	⑩ ITEM 204 - PROOF ROLLING
③ ITEM 441 - 1.75" ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1 (448)	⑪ ITEM 609 - CURB, TYPE 6
④ ITEM 302 - 7" ASPHALT CONCRETE BASE, PG 64-22	⑫ ITEM 605 6" BASE PIPE UNDERDRAIN
⑤ ITEM 304 - 15" AGGREGATE BASE	⑬ ITEM 659 - SEEDING AND MULCHING, CLASS 2
⑥ ITEM 304 - 9" AGGREGATE BASE	⑭ ITEM 254 - PAVEMENT PLANING, ASPHALT CONCRETE, AS PER PLAN
⑦ ITEM 408 - PRIME COAT, AS PER PLAN	⑮ ITEM 441 - ASPHALT CONCRETE INTERMEDIATE COURSE, TYPE 1 (448), (FOR LEVELING)
⑧ ITEM 617 - COMPACTED AGGREGATE, AS PER PLAN	⑯ ITEM 204 - LIME STABILIZED SUBGRADE, 14 INCHES DEEP

ITEM 614, MAINTAINING TRAFFIC (AT ALL TIMES)

A MINIMUM OF ONE LANE OF TRAFFIC IN EACH DIRECTION SHALL BE MAINTAINED AT ALL TIMES BY USE OF THE EXISTING PAVEMENT, THE COMPLETED PAVEMENT, ITEM 502 STRUCTURE FOR MAINTAINING TRAFFIC, ITEM 615 PAVEMENT FOR MAINTAINING TRAFFIC, ITEM 615 ROADS FOR MAINTAINING TRAFFIC, AND TEMPORARY SURFACES USING ITEMS 410 AND 614.

ITEM 614, MAINTAINING TRAFFIC (LANE CLOSURE/REDUCTION REQUIRED)

LENGTH AND DURATION OF LANE CLOSURES AND RESTRICTIONS SHALL BE AT THE APPROVAL OF THE ENGINEER. IT IS THE INTENT TO MINIMIZE THE IMPACT TO THE TRAVELING PUBLIC. LANE CLOSURES OR RESTRICTIONS OVER SEGMENTS OF THE PROJECT IN WHICH NO WORK IS ANTICIPATED WITHIN A REASONABLE TIME FRAME, AS DETERMINED BY THE ENGINEER, SHALL NOT BE PERMITTED. THE LEVEL OF UTILIZATION OF MAINTENANCE OF TRAFFIC DEVICES SHALL BE COMMENSURATE WITH THE WORK IN PROGRESS.

ITEM 614, MAINTAINING TRAFFIC (NOTICE OF CLOSURE SIGN)

NOTICE OF CLOSURE SIGNS (W20-H13) SHALL BE ERECTED BY THE CONTRACTOR PRIOR TO THE SCHEDULED ROAD OR RAMP CLOSURE IN ACCORDANCE WITH THE NOTICE OF CLOSURE TIME TABLE BELOW. AT THE APPROVAL OF THE ENGINEER, PORTABLE CHANGEABLE MESSAGE SIGNS MAY BE USED IN LIEU OF THE STANDARD FLATSHEET SIGN FOR CLOSURE DURATIONS OF LESS THAN 1 WEEK.

THE SIGNS SHALL BE ERECTED ON THE RIGHT-HAND SIDE OF THE ROAD FACING TRAFFIC. THEY SHALL BE PLACED SO AS NOT TO INTERFERE WITH THE VISIBILITY OF ANY OTHER TRAFFIC CONTROL SIGNS. ON ROADWAYS, THEY SHOULD BE ERECTED AT OR NEAR THE POINT OF CLOSURE.

NOTIFICATION TIME FRAME TABLE		
ITEM	DURATION OF CLOSURE	NOTIFICATION DUE TO FAIRFIELD COUNTY
ROAD CLOSURE	>=2 WEEKS	14 CALENDAR DAYS PRIOR TO CLOSURE
	>12 HOURS & <2 WEEKS	7 CALENDAR DAYS PRIOR TO CLOSURE
	<12 HOURS	2 BUSINESS DAYS PRIOR TO CLOSURE

THE SIGN SHALL DISPLAY THE DATE OF THE CLOSURE IN MMM-DD FORMAT AND THE NUMBER OF DAYS OF THE CLOSURE. THE LAST LINE OF THE W20-H13 SIGN LISTS A PHONE NUMBER WHICH A MOTORIST MAY CALL FOR ADDITIONAL INFORMATION. THIS IS TO BE A SPECIFIC OFFICE WITHIN THE DISTRICT RATHER THAN THE GENERAL SWITCHBOARD NUMBER.

ITEM 614, MAINTAINING TRAFFIC (ESTIMATED QUANTITIES)

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE GENERAL SUMMARY FOR USE AS DETERMINED BY THE ENGINEER FOR THE MAINTENANCE OF TRAFFIC.

ITEM 410, TRAFFIC COMPACTED SURFACE, TYPE A OR B xxx CU. YD.

ITEM 616, WATER xxx M. GAL.

ITEM 614, MAINTAINING TRAFFIC (ROAD CLOSED SIGN)

THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN STANDARD 48 X 30 INCH ROAD CLOSED SIGNS, SIGN SUPPORTS, BARRICADES AND LIGHTS, AS DETAILED IN SCD MT-101.60 AT THE LOCATIONS SHOWN IN THE PLANS DURING PERIODS IN WHICH THE AFFECTED ROADS ARE CLOSED TO TRAFFIC.

ITEM 614, MAINTAINING TRAFFIC (SIGNS AND BARRICADES)

THE CONTRACTOR SHALL PROVIDE, ERECT AND MAINTAIN SIGNS AND SIGN SUPPORTS, AS DETAILED IN THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES, AND TYPE III BARRICADES OF THE TYPE AND LOCATION AS SHOWN ON THE PLANS.

ITEM 614, MAINTAINING TRAFFIC (CLOSING PARAGRAPH FOR NOTE)

ALL WORK AND TRAFFIC CONTROL DEVICES SHALL BE IN ACCORDANCE WITH C&MS 614 AND OTHER APPLICABLE PORTIONS OF THE SPECIFICATIONS, AS WELL AS THE OHIO MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES. PAYMENT FOR ALL LABOR, EQUIPMENT AND MATERIALS SHALL BE INCLUDED IN THE LUMP SUM CONTRACT PRICE FOR ITEM 614, MAINTAINING TRAFFIC, UNLESS SEPARATELY ITEMIZED IN THE PLAN.

TRENCH FOR WIDENING

TRENCH EXCAVATION FOR BASE WIDENING SHALL BE ONLY ON ONE SIDE OF THE PAVEMENT AT A TIME. THE OPEN TRENCH SHALL BE ADEQUATELY MAINTAINED AND PROTECTED WITH DRUMS OR BARRICADES AT ALL TIMES. PLACEMENT OF PROPOSED SUBBASE AND BASE MATERIAL SHALL FOLLOW AS CLOSELY AS POSSIBLE BEHIND EXCAVATION OPERATIONS. THE LENGTH OF WIDENING TRENCH WHICH IS OPEN AT ANY ONE TIME SHALL BE HELD TO A MINIMUM AND SHALL AT ALL TIMES BE SUBJECT TO APPROVAL OF THE ENGINEER.

OVERNIGHT TRENCH CLOSING

THE BASE WIDENING SHALL BE COMPLETED TO A DEPTH OF NO MORE THAN TWO INCHES BELOW THE EXISTING PAVEMENT BY THE END OF EACH WORK DAY. NO TRENCH SHALL BE LEFT OPEN OVERNIGHT EXCEPT FOR A SHORT LENGTH (25 FEET OR LESS) OF A WORK SECTION AT THE END OF THE TRENCH. IN CASE WORK MUST BE SUSPENDED BECAUSE OF INCLEMENT WEATHER OR OTHER REASONS, THE TRENCH FOR THE UNCOMPLETED BASE WIDENING SHALL BE BACKFILLED AT THE DIRECTION OF THE ENGINEER.

DELINEATION OF PORTABLE AND PERMANENT BARRIER

BARRIER REFLECTORS AND OBJECT MARKERS SHALL BE INSTALLED ON ALL PORTABLE BARRIER (PB) USED FOR TRAFFIC CONTROL; AND, ON PERMANENT CONCRETE BARRIER (INCLUDING BRIDGE PARAPETS) LOCATED WITHIN 5 FEET OF THE EDGE OF THE ADJACENT TRAVEL LANE.

BARRIER REFLECTORS SHALL CONFORM TO C&MS 626, EXCEPT THAT THE SPACING SHALL BE AS PER TRAFFIC SCD MT-101.70. OBJECT MARKERS AND THEIR INSTALLATION SHALL CONFORM TO C&MS 614.03 AND SCD MT-101.70. WHEN THE PB CONTAINS GLARE SCREEN, ONE SET OF THREE VERTICAL STRIPES OF SHEETING SHALL BE CONSIDERED EQUIVALENT TO AN OBJECT MARKER, ONE-WAY.

[INCREASED BARRIER DELINEATION, AS SPECIFIED HEREIN, SHALL BE INSTALLED ON ALL PB AND PERMANENT CONCRETE BARRIER LOCATED WITHIN 5 FEET OF THE EDGE OF THE TRAVELED LANE UNDER EITHER OF THE FOLLOWING CONDITIONS: ALONG TAPERS AND TRANSITION AREAS; OR ALONG CURVES (OUTSIDE ONLY) WITH DEGREE OF CURVATURE GREATER THAN OR EQUAL TO 3 DEGREES.]

[THE INCREASED BARRIER DELINEATION SHALL CONSIST OF EITHER DELINEATION PANELS OR THE TRIPLE STACKING OF WORK ZONE BARRIER REFLECTORS.]

[DELINEATION PANELS SHALL CONSIST OF PANELS OF DELINEATION, APPROXIMATELY 34 INCHES LONG AND 6 INCHES WIDE AND SHALL BE "CRIMPED." PANELS SHALL BE INSTALLED AND SPACED PER TRAFFIC SCD MT-101.70.]

DELINEATION OF PORTABLE AND PERMANENT BARRIER (CONT.)

[TRIPLE-STACKED BARRIER REFLECTORS SHALL CONSIST OF ALIGNING THREE BARRIER REFLECTORS VERTICALLY, AT LOCATIONS WHERE A SINGLE BARRIER REFLECTOR WOULD BE OTHERWISE ATTACHED. THERE SHALL BE NO OPEN SPACE BETWEEN THE ADJACENT BARRIER REFLECTORS. THE TRIPLESTACKED BARRIER REFLECTORS SHALL CONFORM TO C&MS 626, EXCEPT THAT THEY SHALL BE SPACED AND ALIGNED PER TRAFFIC SCD MT-101.70.]

THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN INCLUDED IN THE PLANS AND CARRIED TO THE GENERAL SUMMARY:

ITEM 614, BARRIER REFLECTOR, TYPE 1 (ONE-WAY) ____ EACH

ITEM 614, OBJECT MARKER, ONE-WAY ____ EACH

[ITEM 614, INCREASED BARRIER BELINEATION ____ FEET]

PAYMENT SHALL BE FULL COMPENSATION FOR ALL MATERIAL, LABOR, INCIDENTALS AND EQUIPMENT NECESSARY FOR FURNISHING, INSTALLING, MAINTAINING AND REMOVING EACH OF THE ABOVE ITEMS.

[ALONG RUNS OF INCREASED BARRIER DELINEATION WHERE THIS ITEM IS PROVIDED, THE QUANTITY SHALL BE MEASURED AS THE ENTIRE LENGTH OF THE RUN OF INCREASED BARRIER DELINEATION, INCLUDING THE SPACES BETWEEN THE INDIVIDUAL DELINEATION PANELS OR STACKS OF BARRIER REFLECTORS.]

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS

USE OF LAW ENFORCEMENT OFFICERS (LEOS) BY CONTRACTORS OTHER THAN THE USES SPECIFIED BELOW WILL NOT BE PERMITTED AT PROJECT COST. LEOS SHOULD NOT BE USED WHERE THE OMUTCD INTENDS THAT FLAGGERS BE USED.

IN ADDITION TO THE REQUIREMENTS OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHALL BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS:

DURING THE ENTIRE ADVANCE PREPARATION AND CLOSURE SEQUENCE WHERE COMPLETE BLOCKAGE OF TRAFFIC IS REQUIRED.

DURING A TRAFFIC SIGNAL INSTALLATION WHEN IMPACTING THE NORMAL FUNCTION OF THE SIGNAL OR THE FLOW OF TRAFFIC, OR WHEN TRAFFIC NEEDS TO BE DIRECTED THROUGH AN ENERGIZED TRAFFIC SIGNAL CONTRARY TO THE SIGNAL DISPLAY (E.G., DIRECTING MOTORISTS THROUGH A RED LIGHT).

IN ADDITION TO THE REQUIREMENT OF C&MS 614 AND THE OMUTCD, A UNIFORMED LEO WITH AN OFFICIAL PATROL CAR (CAR WITH TOP-MOUNTED EMERGENCY FLASHING LIGHTS AND COMPLETE MARKINGS OF THE APPROPRIATE LAW ENFORCEMENT AGENCY) SHOULD BE PROVIDED FOR THE FOLLOWING TRAFFIC CONTROL TASKS AS APPROVED BY THE ENGINEER:

FOR LANE CLOSURES: DURING INITIAL SET-UP PERIODS, TEAR DOWN PERIODS, SUBSTANTIAL SHIFTS OF A CLOSURE POINT OR WHEN A NEW LANE CLOSURE ARRANGEMENTS ARE INITIATED FOR LONG-TERM LANE CLOSURES/SHIFTS (FOR THE FIRST AND LAST DAY OF MAJOR CHANGES IN TRAFFIC CONTROL SETUP).

IN GENERAL, LEOS SHOULD BE POSITIONED IN ADVANCE OF AND ON THE SAME SIDE AS THE LANE RESTRICTION OR AT THE POINT OF ROAD CLOSURE, AND TO MANUALLY CONTROL TRAFFIC MOVEMENTS THROUGH SIGNALIZED INTERSECTIONS IN WORK ZONES.

ITEM 614 - LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE DURING CONSTRUCTION OPERATIONS (CONT.)

LEOS SHOULD NOT FORGO THEIR TRAFFIC CONTROL RESPONSIBILITIES TO APPREHEND MOTORISTS FOR ROUTINE TRAFFIC VIOLATIONS. HOWEVER, IF A MOTORIST'S ACTIONS ARE CONSIDERED TO BE RECKLESS, THEN PURSUIT OF THE MOTORIST IS APPROPRIATE.

THE LEOS WORK AT THE DIRECTION OF THE CONTRACTOR. THE CONTRACTOR IS RESPONSIBLE FOR SECURING THE SERVICES OF THE LEOS WITH THE APPROPRIATE AGENCIES AND COMMUNICATING THE INTENTIONS OF THE PLANS WITH RESPECT TO DUTIES OF THE LEOS. THE ENGINEER SHALL HAVE FINAL CONTROL OVER THE LEOS' DUTIES AND PLACEMENT, AND WILL RESOLVE ANY ISSUES THAT MAY ARISE BETWEEN THE TWO PARTIES.

ENSURE PROVIDED LEOS HAVE BEEN TRAINED APPROPRIATE TO THE JOB DECISIONS THEY ARE REQUIRED TO MAKE WHILE ON THE PROJECT, IN ACCORDANCE WITH C&MS 614.03.

THE LEO SHALL REPORT IN TO THE CONTRACTOR PRIOR TO THE START OF THE SHIFT, IN ORDER TO RECEIVE INSTRUCTIONS REGARDING THE SPECIFIC WORK ASSIGNMENTS DURING HIS/HER SHIFT. THE LEO IS EXPECTED TO STAY AT THE PROJECT SITE FOR THE ENTIRE DURATION OF HIS/HER SHIFT. THE LEO SHALL REPORT TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT. SHOULD IT BE NECESSARY TO LEAVE THE PROJECT SITE, THE LEO SHALL NOTIFY THE ENGINEER. THE CONTRACTOR SHALL PROVIDE THE LEO WITH A TWO-WAY COMMUNICATION DEVICE WHICH SHALL BE RETURNED TO THE CONTRACTOR AT THE END OF HIS/HER SHIFT.

LEOS (WITH PATROL CAR) REQUIRED BY THE TRAFFIC MAINTENANCE TASKS ABOVE SHALL BE PAID FOR ON A UNIT PRICE (HOURLY) BASIS UNDER ITEM 614, LAW ENFORCEMENT OFFICER (WITH PATROL CAR) FOR ASSISTANCE. THE FOLLOWING ESTIMATED QUANTITIES HAVE BEEN CARRIED TO THE GENERAL SUMMARY:

ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE ____ HOURS

THE HOURS PAID SHALL INCLUDE ANY MINIMUM SHOW-UP TIME REQUIRED BY THE LAW ENFORCEMENT AGENCY INVOLVED.

ANY ADDITIONAL COSTS (ADMINISTRATIVE OR OTHERWISE) INCURRED BY THE CONTRACTOR TO OBTAIN THE SERVICES OF A LEO ARE INCLUDED WITH THE BID UNIT PRICE FOR ITEM 614, LAW ENFORCEMENT OFFICER WITH PATROL CAR FOR ASSISTANCE.

DESIGN AGENCY
Mead & Hunt

CLIENT


DESIGNER
EGD

REVIEWER
MJH 07/07/20

PROJECT ID
110412

SHEET
P.07

TOTAL
136

MAINTENANCE OF TRAFFIC SEQUENCE OF CONSTRUCTION

THE CONTRACTOR SHALL MAINTAIN A MINIMUM TRAVEL LANE WIDTH OF 11 FEET ON SR 37 AND 11 FEET ON SR 256. THESE LANE WIDTHS MATCH THE EXISTING LANE WIDTHS ON SR 37 AND SR 256. THE LANE WIDTHS WILL BE INCREASED IF POSSIBLE.

THE CONTRACTOR SHALL MAINTAIN ACCESS TO ALL PROPERTIES AT ALL TIMES. IF THE CONTRACTOR NEEDS TO RESTRICT ACCESS TO A PARCEL, THE PROPERTY OWNER OR TENANT, IF DIFFERENT FROM THE PROPERTY OWNER, SHALL BE NOTIFIED 48 HOURS IN ADVANCE. THE CONTRACTOR SHALL NOT RESTRICT ACCESS MORE THAN 1 HOUR IN A 24 HOUR PERIOD.

THE CONTRACTOR WILL NEED TO ORDER THE TRAFFIC SIGNAL EQUIPMENT AS SOON AS POSSIBLE IN ORDER TO BEGIN THE TRAFFIC SIGNAL RELOCATION DURING PHASE 2 CONSTRUCTION.

THE DESIGN SPEED FOR BOTH SR 37 AND SR 256 IS 60 MPH.

PHASE 1: THE CONTRACTOR SHALL SET UP THE DETOUR ROUTES FOR NORTHBOUND SR 37 AND EASTBOUND SR 256 AS SHOWN ON THE DETOUR MAPS AND CLOSE NORTHBOUND SR 37 AND EASTBOUND SR 256 TO TRAFFIC. USING THE EXISTING PAVEMENT, THE CONTRACTOR SHALL PLACE ALL SIGNS AND OTHER TRAFFIC CONTROL DEVICES TO MAINTAIN SOUTHBOUND SR 37 AND WESTBOUND SR 256 TRAFFIC AS SHOWN ON THE PHASE 1 MOT PLANS. THE CONTRACTOR SHALL CONSTRUCT THE FULL DEPTH PAVEMENT ALONG THE NORTHBOUND SIDE OF SR 37, THE EASTBOUND SIDE OF SR 256 AND A PORTION OF THE INTERSECTION UP TO AND INCLUDING THE ASPHALT CONCRETE INTERMEDIATE COURSE AS SHOWN ON THE PHASE 1 PLANS. THE CONTRACTOR SHALL ALSO PLACE THE SMALL PORTION OF WORK ZONE PAVEMENT ALONG THE NORTHBOUND SIDE OF SR 37 AS SHOWN ON THE PLANS. PLACE A 4:1 TRANSVERSE WEDGE OF ASPHALT TO TRANSITION FROM THE EXISTING PAVEMENT SURFACE TO THE NEWLY CONSTRUCTED ASPHALT CONCRETE INTERMEDIATE COURSE AS REQUIRED. THE CONTRACTOR SHALL ERECT THE TRAFFIC SIGNAL STRAIN POLES AND RELOCATE THE MESSENGER WIRE AND TRAFFIC SIGNAL HEADS. THE CONTRACTOR MAY NEED TO ERECT TEMPORARY POLES AND SIGNALS IF THE NEW STRAIN POLES CANNOT BE FABRICATED AND DELIVERED TO THE PROJECT SITE IN TIME FOR ERECTION.

PHASE 2A: FOR PHASE 2A, THE CONTRACTOR SHALL MAINTAIN THE DETOUR ROUTES FOR NORTHBOUND SR 37 AND EASTBOUND SR 256 AS SHOWN ON THE DETOUR MAPS. AT THE COMPLETION OF ALL PHASE 1 WORK, THE CONTRACTOR SHALL SET UP AN ADDITIONAL DETOUR ROUTE FOR WESTBOUND SR 256 AS SHOWN ON THE DETOUR MAPS. THE CONTRACTOR SHALL MAINTAIN THE SOUTHBOUND SR 37 TRAFFIC ON THE SOUTHBOUND SIDE AS SHOWN IN PHASE 1. BETWEEN THE HOURS OF 7 PM FRIDAY TO 6AM MONDAY, THE CONTRACTOR SHALL DETOUR WESTBOUND SR 256 TRAFFIC AND CONSTRUCT THE PAVEMENT AND SHOULDER WIDENING AND THE PAVEMENT OVERLAY IN THE INTERSECTION UP TO AND INCLUDING THE ASPHALT CONCRETE INTERMEDIATE COURSE AS SHOWN ON THE PHASE 2A PLANS. PLACE A 4:1 TRANSVERSE WEDGE OF ASPHALT AND A 35:1 LONGITUDINAL WEDGE OF ASPHALT AS SHOWN ON SCD BP-3.1 TO TRANSITION FROM THE EXISTING PAVEMENT SURFACE TO THE NEWLY CONSTRUCTED ASPHALT CONCRETE INTERMEDIATE COURSE AS REQUIRED. DO NOT PLACE THE ASPHALT CONCRETE SURFACE COURSE. THE CONTRACTOR SHALL CONTINUE WITH ANY REMAINING TRAFFIC SIGNAL WORK. ALL PHASE 2A WORK MUST BE COMPLETED WITH SUFFICIENT TIME REMAINING TO ALLOW THE PHASE 2B WORK TO BE COMPLETED BY 6 AM MONDAY. THE CONTRACTOR WILL BE PERMITTED TO DO ANY PHASE 2B WORK CONCURRENTLY WITH THE PHASE 2A WORK AS LONG AS IT DOES NOT IMPACT SOUTHBOUND SR 37 TRAFFIC.

PHASE 2B: FOR PHASE 2B, THE CONTRACTOR SHALL MAINTAIN THE DETOUR ROUTES FOR NORTHBOUND SR 37, EASTBOUND SR 256 AND WESTBOUND SR 256 AS SHOWN ON THE DETOUR MAPS. AT THE COMPLETION OF ALL PHASE 2A WORK, THE CONTRACTOR SHALL

MAINTENANCE OF TRAFFIC SEQUENCE OF CONSTRUCTION (CONT.)

PHASE 2B (CONT.): PLACE THE REQUIRED WORK ZONE PAVEMENT MARKINGS, SIGNS, AND TRAFFIC CONTROL DEVICES AND RELOCATE THE TRAFFIC SIGNAL HEADS AS REQUIRED AND SHIFT SOUTHBOUND SR 37 TRAFFIC TO THE NEWLY COMPLETED PAVEMENT ON THE NORTHBOUND SIDE OF SR 37. BETWEEN THE HOURS OF 7 PM FRIDAY TO 6AM MONDAY, THE CONTRACTOR SHALL CONSTRUCT THE PAVEMENT AND SHOULDER WIDENING AND THE PAVEMENT OVERLAY IN THE INTERSECTION UP TO AND INCLUDING THE ASPHALT CONCRETE INTERMEDIATE COURSE AS SHOWN ON THE PHASE 2B PLANS. PLACE A 4:1 TRANSVERSE WEDGE OF ASPHALT AND A 35:1 LONGITUDINAL WEDGE OF ASPHALT AS SHOWN ON SCD BP-3.1 TO TRANSITION FROM THE EXISTING PAVEMENT SURFACE TO THE NEWLY CONSTRUCTED ASPHALT CONCRETE INTERMEDIATE COURSE AS REQUIRED. DO NOT PLACE THE ASPHALT CONCRETE SURFACE COURSE. THE CONTRACTOR SHALL CONTINUE WITH ANY REMAINING TRAFFIC SIGNAL WORK. ALL PHASE 2B WORK ALONG WITH ALL PHASE 2A WORK MUST BE COMPLETED BY 6 AM MONDAY. AT 6AM MONDAY THE CONTRACTOR SHALL OPEN WESTBOUND SR 256 TO TRAFFIC AND REMOVE THE DETOUR SIGNING. THE CONTRACTOR SHALL MAINTAIN THE DETOUR ROUTES FOR NORTHBOUND SR 37 AND EASTBOUND SR 256 AS SHOWN ON THE DETOUR MAPS.

PHASE 3: FOR PHASE 3, THE CONTRACTOR SHALL MAINTAIN THE DETOUR ROUTES FOR NORTHBOUND SR 37 AND EASTBOUND SR 256 AS SHOWN ON THE DETOUR MAPS. THE CONTRACTOR SHALL MAINTAIN SOUTHBOUND SR 37 TRAFFIC ON THE NORTHBOUND SIDE OF SR 37 AND SHIFT WESTBOUND SR 256 TRAFFIC ONTO THE NEWLY CONSTRUCTED EASTBOUND LANE OF SR 256 AS SHOWN ON THE PHASE 3 MOT PLANS. THE CONTRACTOR SHALL CONSTRUCT THE PAVEMENT AND SHOULDER WIDENING IMPROVEMENTS ALONG THE SOUTHBOUND SIDE OF SR 37 AND THE WESTBOUND SIDE OF SR 256. THE CONTRACTOR SHALL CONTINUE WITH ANY REMAINING TRAFFIC SIGNAL WORK.

PHASE 4: THE CONTRACTOR SHALL MAINTAIN THE DETOUR ROUTES FOR NORTHBOUND SR 37 AND EASTBOUND SR 256 AS SHOWN ON THE DETOUR MAPS. AT THE COMPLETION OF ALL PHASE 3 WORK, THE CONTRACTOR SHALL MILL THE EXISTING ASPHALT SURFACE COURSE FROM SR 37 AND SR 256 ALONG WITH THE 4:1 PAVEMENT WEDGES CONSTRUCTED DURING PREVIOUS CONSTRUCTION PHASES. REMOVE THE WORK ZONE PAVEMENT ALONG THE NORTHBOUND SIDE OF SR 37 THAT WAS CONSTRUCTED DURING PHASE 1 AND CONSTRUCT THE CURB AND GUTTER SECTION. PLACE THE ASPHALT CONCRETE SURFACE COURSE ON THE EXISTING PAVEMENT AND THE ASPHALT CONCRETE INTERMEDIATE COURSE THAT WAS CONSTRUCTED DURING PREVIOUS CONSTRUCTION PHASES. THE CONTRACTOR SHALL USE FLAGGERS PER SCD MT-97.12 TO DIRECT TRAFFIC AROUND THE PAVING OPERATION. PLACE THE REQUIRED TEMPORARY PAVEMENT MARKINGS. THE CONTRACTOR SHALL CONTINUE ANY TRAFFIC SIGNAL WORK REQUIRED.

PHASE 5: AT THE COMPLETION OF ALL PHASE 4 WORK, THE CONTRACTOR SHALL PLACE THE REQUIRED PERMANENT PAVEMENT MARKINGS AND SIGNS. COMPLETE ANY REMAINING GRADING AND SEEDING AND MULCHING REQUIRED.

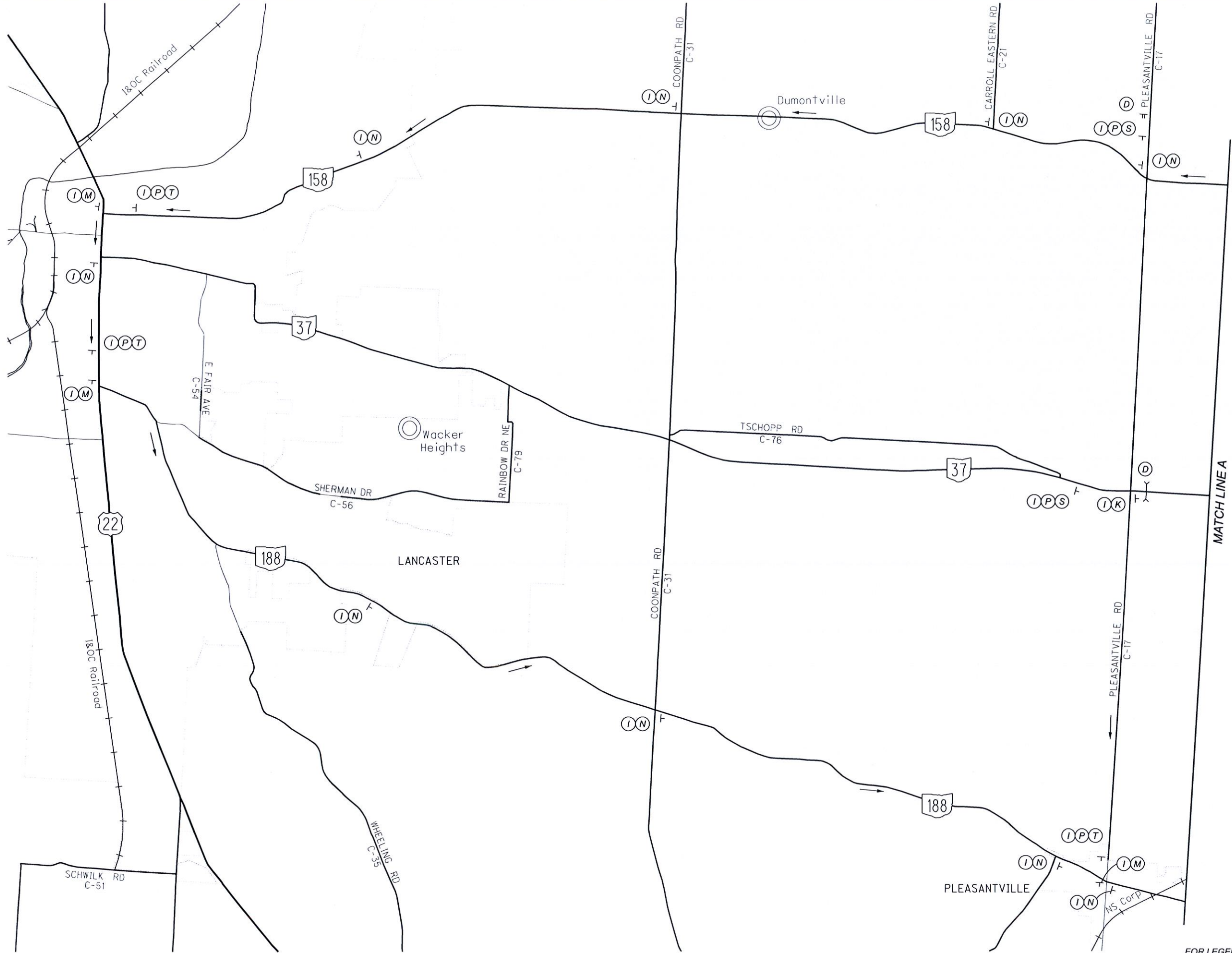
DESIGN AGENCY
Mead & Hunt
CLIENT


DESIGNER
EGD

REVIEWER
MJH 07/07/20

PROJECT ID
110412

SHEET TOTAL
P.08 136

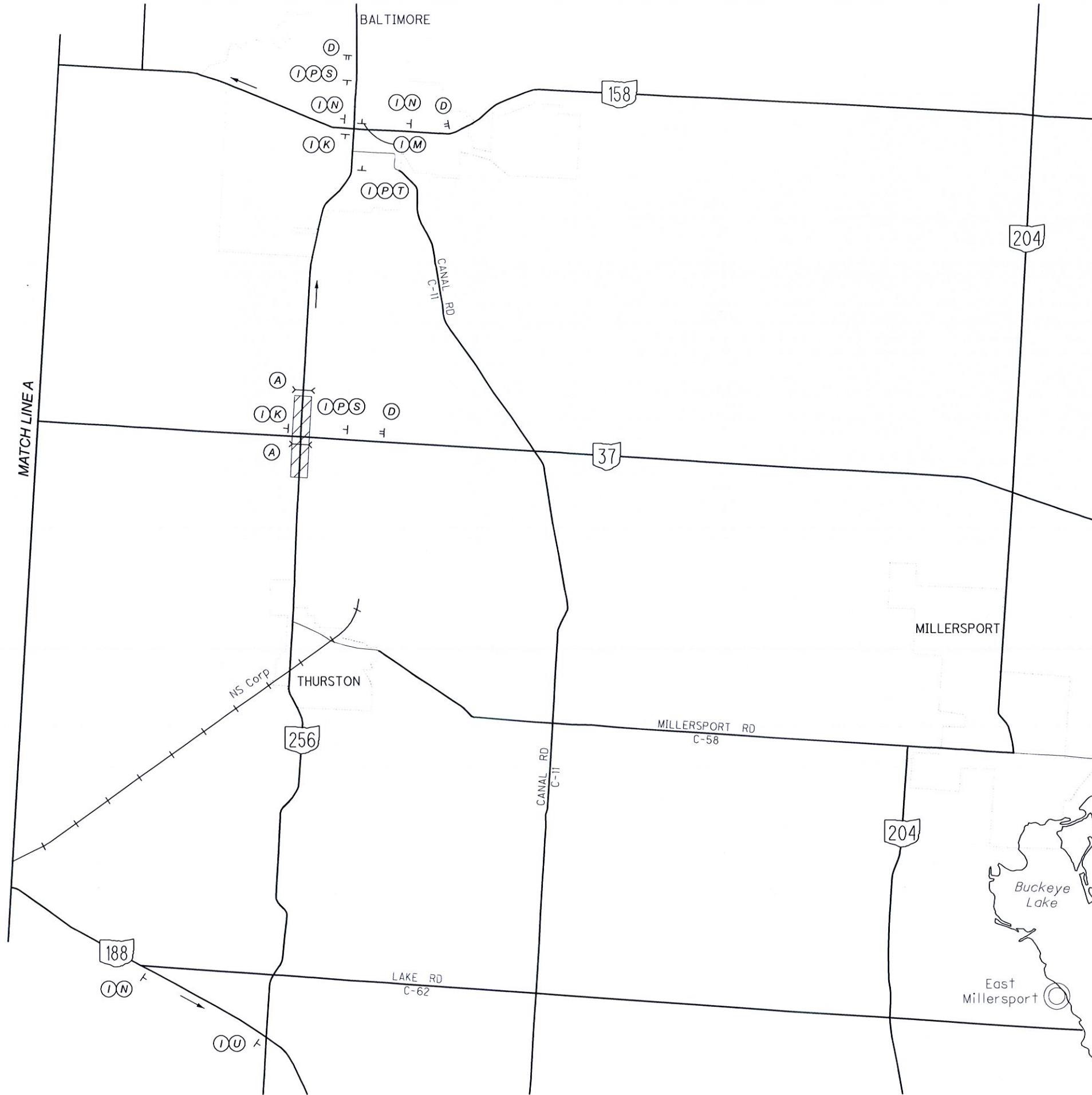


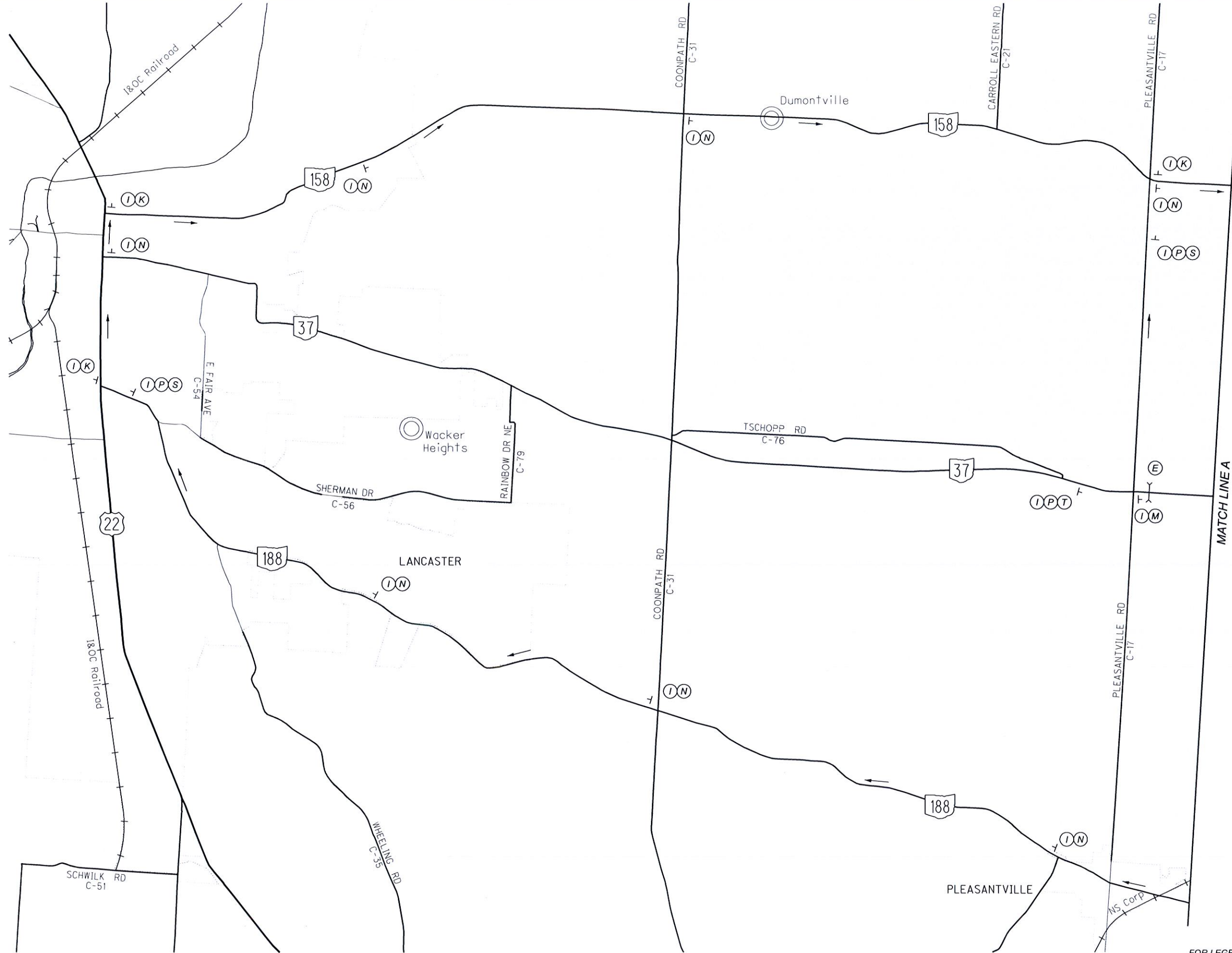
FOR LEGEND, SEE SHEET xx



MAINTNANCE OF TRAFFIC
DETOUR MAP - SR 256 EASTBOUND

DESIGN AGENCY	
Mead & Hunt	
CLIENT	
DESIGNER	EGD
REVIEWER	MJH
PROJECT ID	110412
SHEET	P.14
TOTAL	136



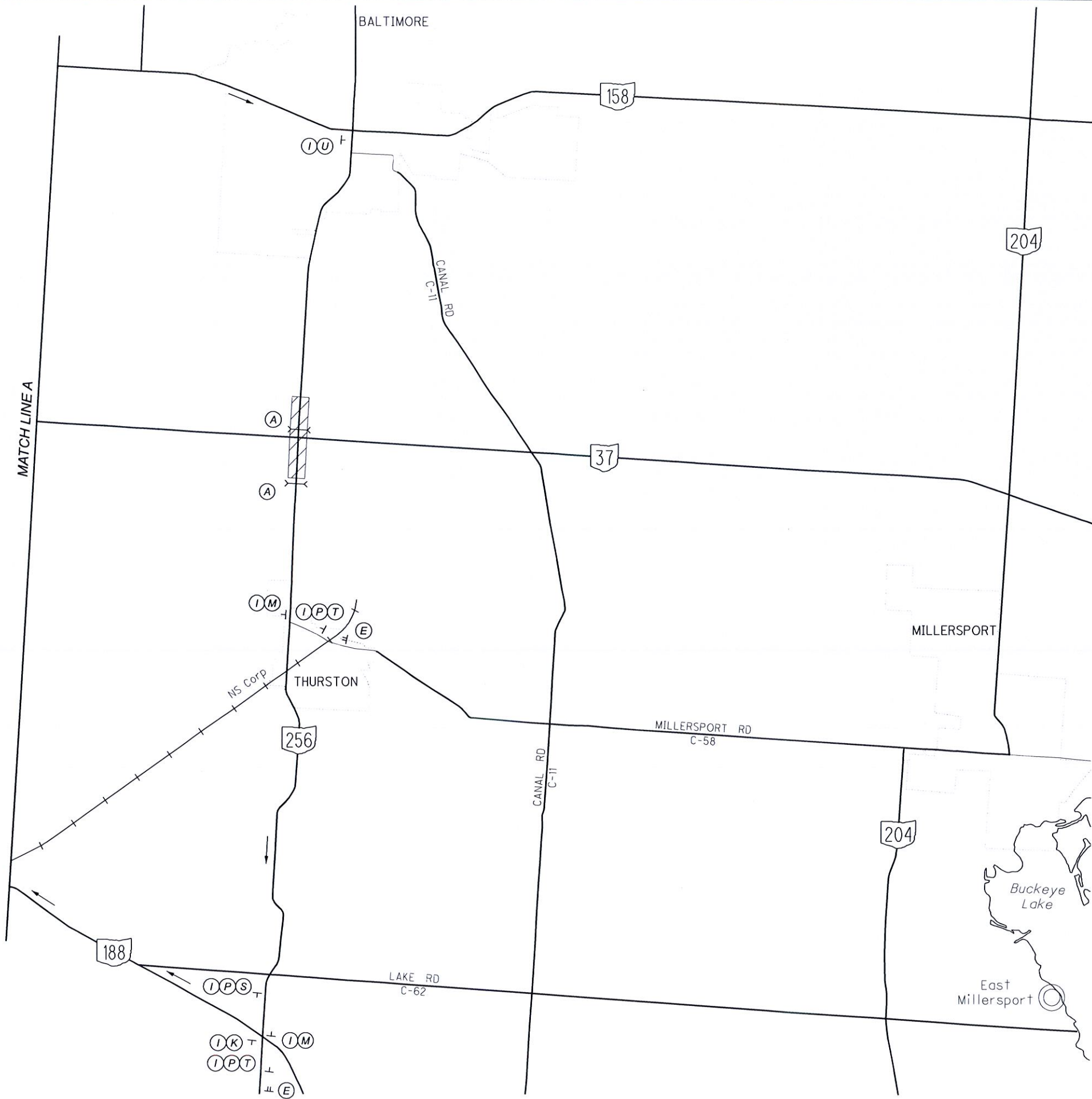


FOR LEGEND, SEE SHEET xx



MAINTNANCE OF TRAFFIC
DETOUR MAP - SR 256 WESTBOUND

DESIGN AGENCY	
Mead & Hunt	
CLIENT	
DESIGNER	
EGD	
REVIEWER	
MJH 07/07/20	
PROJECT ID	
110412	
SHEET	TOTAL
P.16	136



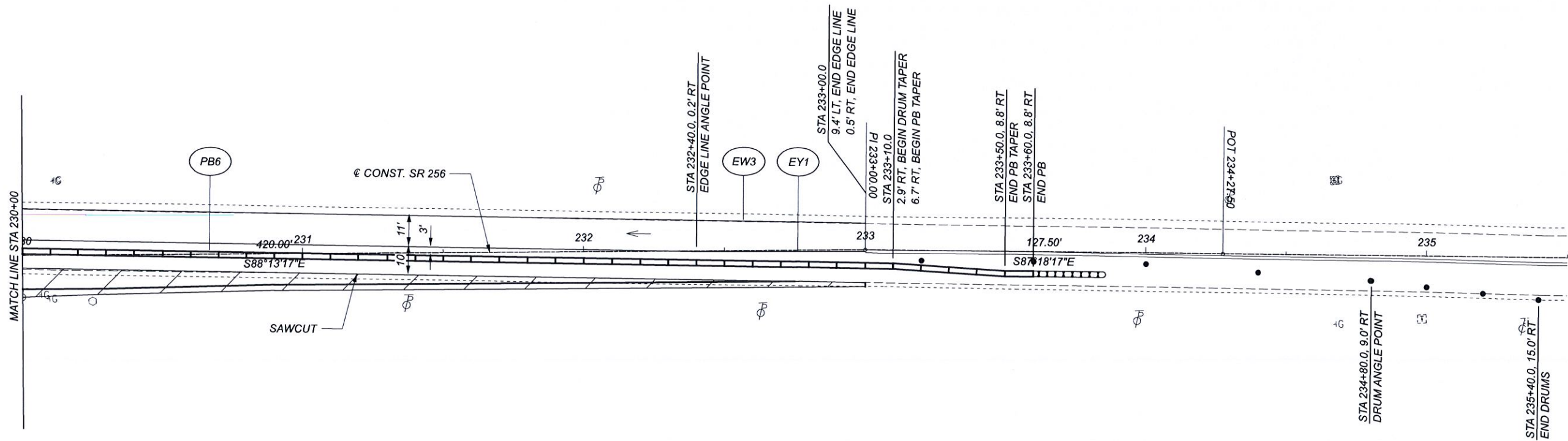
FOR LEGEND, SEE SHEET xx



MAINTNANCE OF TRAFFIC
DETOUR MAP - SR 256 WESTBOUND

DESIGN AGENCY	
Mead & Hunt	
CLIENT	
DESIGNER	
EGD	
REVIEWER	
MJH 07/07/20	
PROJECT ID	
110412	
SHEET	TOTAL
P.17	136





THE CONTRACTOR SHALL INSTALL THE FOLLOWING ADVANCE WARNING SIGNS AT THE LOCATIONS SHOWN PRIOR TO THE START OF PHASE 1 CONSTRUCTION. AT THE COMPLETION OF PHASE 1 THE SIGNS SHALL REMAIN IN PLACE FOR USE DURING SUBSEQUENT PHASES.

 W20-1-36 W/TYPE A WARNING LIGHT STA 238+00, LT	 W20-1-36 W/TYPE A WARNING LIGHT STA 243+00, LT	 W20-1-36 W/TYPE A WARNING LIGHT STA 248+00, LT
--	--	--



MAINTENANCE OF TRAFFIC - PHASE 1
SR256 - STA 230+00 TO STA 235+50

DESIGN AGENCY

Mead & Hunt

CLIENT



DESIGNER

EGD

REVIEWER

MJH 07/07/20

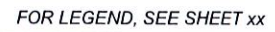
PROJECT ID

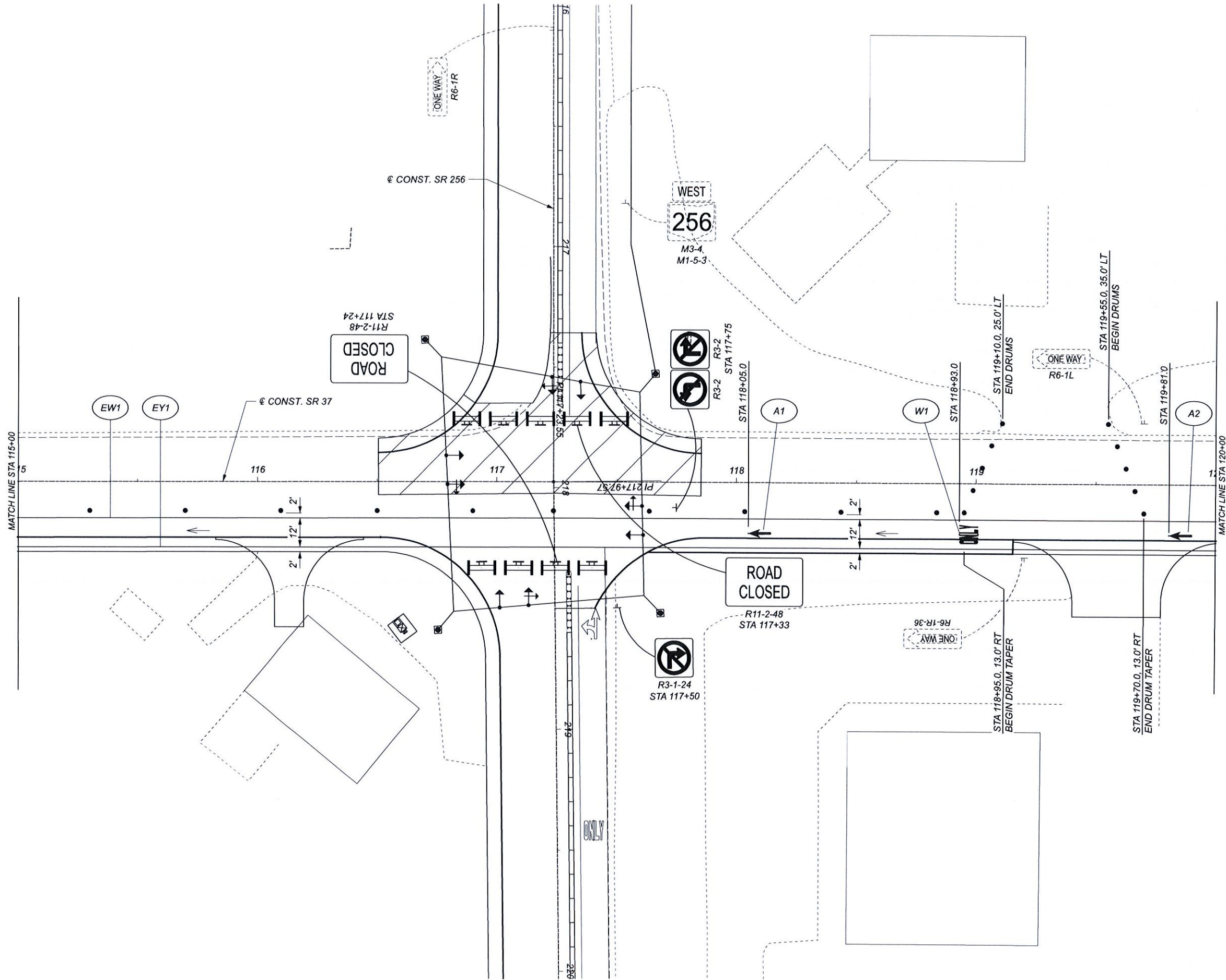
110412

SHEET TOTAL

P.31 136

FOR LEGEND, SEE SHEET xx





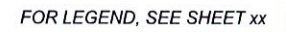
FOR LEGEND, SEE SHEET xx

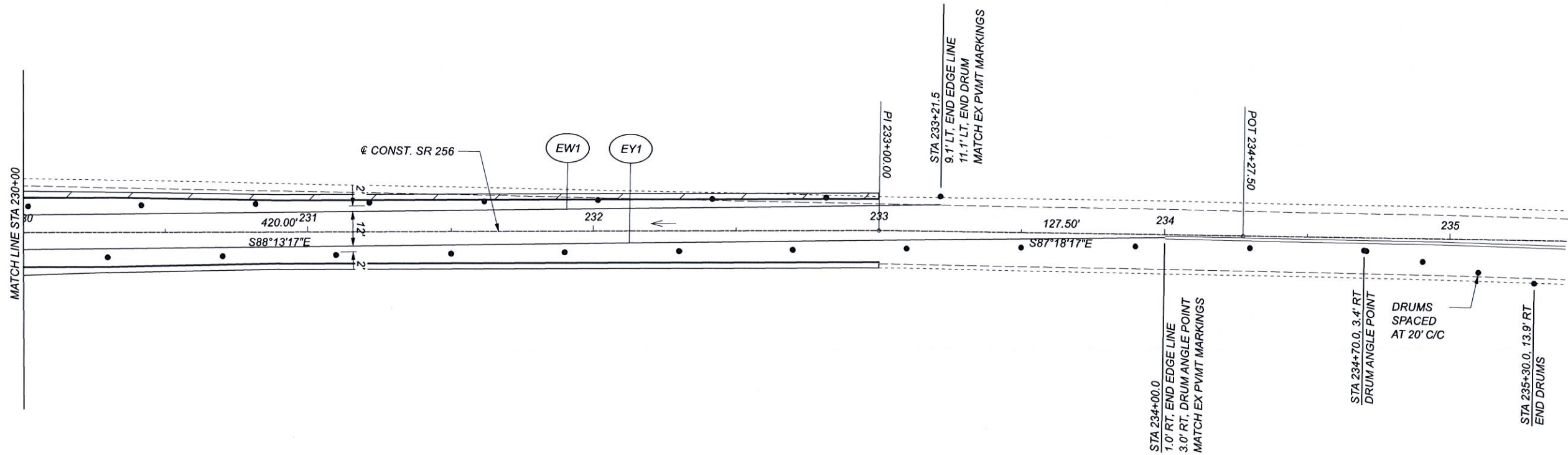


HORIZONTAL
SCALE IN FEET
0 10 20 40

MAINTENANCE OF TRAFFIC - PHASE 2B
SR 37 - STA 115+00 TO STA 120+00 & SR 256 - STA 216+00 TO STA 220+00

DESIGN AGENCY	Mead & Hunt
CLIENT	
DESIGNER	EGD
REVIEWER	MJH 07/07/20
PROJECT ID	110412
SHEET	P.37
TOTAL	136





THE CONTRACTOR SHALL INSTALL THE FOLLOWING ADVANCE WARNING SIGNS AT THE LOCATIONS SHOWN PRIOR TO THE START OF PHASE 1 CONSTRUCTION. AT THE COMPLETION OF PHASE 1 THE SIGNS SHALL REMAIN IN PLACE FOR USE DURING SUBSEQUENT PHASES.



MAINTENANCE OF TRAFFIC - PHASE 3
SR 256 - STA 230+00 TO STA 235+50

DESIGN AGENCY

Mead & Hunt

CLIENT



DESIGNER

EGD

REVIEWER

MJH 07/07/20

PROJECT ID

110412

SHEET TOTAL

P.54 136

FOR LEGEND, SEE SHEET xx